



REGULAR MEETING AGENDA

Date: 1/23/2023
Time: 7:00 p.m.
Location: Zoom.us/join – ID# 862 5880 9056 and
City Council Chambers
751 Laurel St., Menlo Park, CA 94025

NOVEL CORONAVIRUS, COVID-19, EMERGENCY ADVISORY NOTICE

Consistent with Cal. Gov. Code §54953(e), and in light of the declared state of emergency, and maximize public safety while still maintaining transparency and public access, members of the public can listen to the meeting and participate using the following methods.

How to participate in the meeting

- Access the live meeting, in-person, at the City Council Chambers
- Access the meeting real-time online at:
zoom.us/join – Meeting ID# 862 5880 9056
- Access the meeting real-time via telephone (listen only mode) at:
(669) 900-6833
Regular Meeting ID # 862 5880 9056
Press *9 to raise hand to speak
- Submit a written comment online up to 1-hour before the meeting start time:
PlanningDept@menlopark.gov
Please include the agenda item number related to your comment.

*Written comments are accepted up to 1 hour before the meeting start time. Written messages are provided to the Planning Commission at the appropriate time in their meeting.

Subject to change: Given the current public health emergency and the rapidly evolving federal, state, county and local orders, the format of this meeting may be altered or the meeting may be canceled. You may check on the status of the meeting by visiting the city website menlopark.gov. The instructions for logging on to the webinar and/or the access code is subject to change. If you have difficulty accessing the webinar, please check the latest online edition of the posted agenda for updated information (menlopark.gov/agendas).

Regular Meeting

A. Call To Order

B. Roll Call

C. Reports and Announcements

D. Public Comment

Under “Public Comment,” the public may address the Commission on any subject not listed on the agenda. Each speaker may address the Commission once under public comment for a limit of three minutes. You are not required to provide your name or City of residence, but it is helpful. The Commission cannot act on items not listed on the agenda and, therefore, the Commission cannot respond to non-agenda issues brought up under Public Comment other than to provide general information.

E. Consent Calendar

None

F. Public Hearing

- F1. Consider and adopt a resolution to deny a variance to increase the height of the daylight plane from 19 feet, six inches to approximately 23 feet, seven inches, and to deny a use permit to demolish an existing one-story, single-family residence and construct a new two-story residence with a basement on a substandard lot with regard to minimum lot area and width in the R-1-U (Single Family Urban Residential) zoning district, at 103 Dunsmuir Way; determine this action is categorically exempt under CEQA Guidelines Section 15303's Class 3 exemption for new construction or conversion of small structures. ([Staff Report #23-007-PC](#))
- F2. Consider and adopt a resolution to approve a use permit to remodel and construct first and second story additions to an existing nonconforming, one-story single-family residence on a substandard lot with regard to minimum lot width and area in the R-1-U (Single Family Urban Residential) zoning district, at 932 Peggy Lane; determine this action is categorically exempt under CEQA Guidelines Section 15303's Class 3 exemption for new construction or conversion of small structures. The project would exceed 50 percent of the existing floor area and is considered equivalent to a new structure. The value of the proposed project would also exceed 50 percent of the existing replacement value in a 12-month period. ([Staff Report #23-008-PC](#))
- F3. Consider and adopt a resolution to approve a use permit to demolish an existing one-story, single-family residence and construct a new two-story residence on a substandard lot with regard to minimum lot depth and area in the R-1-U (Single Family Urban Residential) zoning district, at 225 Lexington Drive; determine this action is categorically exempt under CEQA Guidelines Section 15303's Class 3 exemption for new construction or conversion of small structures. ([Staff Report #23-009-PC](#))

G. Study Session

- G1. Study session for the Parkline Master Plan project to comprehensively redevelop an approximately 63.2-acre site located at 301 and 333 Ravenswood Avenue and 555 and 565 Middlefield Road. The proposed project would redevelop SRI International's research campus by creating a new office/research and development, transit-oriented campus with no net increase in commercial square footage, up to 550 new rental housing units (with a minimum of 15% of the units available for below market rate households), new bicycle and pedestrian connections, and approximately 25 acres of publicly accessible open space. The proposed project would demolish all existing buildings, excluding Buildings P, S, and T, which would remain on-site and operational by SRI and its tenants. The proposed project would organize land uses generally into two land use districts within the Project site, including 1) an approximately 10-acre Residential District in the southwestern portion of the Project site; and 2) an approximately 53-acre Office/R&D (research and development) District that would comprise the remainder of the Project site. In total, the Proposed Project would result in a total of approximately 1,898,931 square feet, including approximately 1,380,332 square feet of office/R&D and approximately 518,599 square feet of residential uses (including up to 450 rental residential units). In addition, the proposed project would establish a separate parcel of land that is proposed to be leased to an affordable housing developer for the future construction of a 100 percent affordable housing or special needs project which would be separately rezoned as part of the proposed project for up to 100 residential units (in addition to the residential units proposed within the Residential District), and which is not included in residential square footage calculations as the square footage has not been determined. The EIR will study two potential project variants, one that includes an approximately 2 million gallon buried concrete water reservoir and associated facilities, and one that includes an additional 50 residential units for a total of up to 600 dwelling units, inclusive of the standalone affordable housing building. The Planning Commission previously held a public hearing on the scope and content of the EIR as part of the 30-day NOP (Notice of Preparation) comment period that ended on January 9, 2023. The project site is zoned "C-1(X)" (Administrative and Professional District, Restrictive) and governed by a Conditional Development Permit (CDP) approved in 1975, and subsequently amended in 1978, 1997, and 2004. The proposed project is anticipated to include the following entitlements: General Plan Amendment (Text and Map), Zoning Ordinance Amendment, Rezoning, Conditional Development Permit, Development Agreement, Architectural Control (for potential future Design Review), Heritage Tree Removal Permits, Vesting Tentative Map, Below Market Rate (BMR) Housing Agreement and Environmental Review. *Continued from the meeting of December 12, 2022.*
([Staff Report #22-073-PC](#))

H. Informational Items

- H1. Future Planning Commission Meeting Schedule – The upcoming Planning Commission meetings are listed here, for reference. No action will be taken on the meeting schedule, although individual Commissioners may notify staff of planned absences.
- Regular Meeting: February 6, 2023
 - Regular Meeting: February 23, 2023

I. Adjournment

At every regular meeting of the Planning Commission, in addition to the public comment period where the public shall have the right to address the Planning Commission on any matters of public interest not listed on the agenda, members of the public have the right to directly address the Planning Commission on any item listed on the agenda at a time designated by

the chair, either before or during the Planning Commission's consideration of the item.

At every special meeting of the Planning Commission, members of the public have the right to directly address the Planning Commission on any item listed on the agenda at a time designated by the chair, either before or during consideration of the item. For appeal hearings, appellant and applicant shall each have 10 minutes for presentations.

If you challenge any of the items listed on this agenda in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City of Menlo Park at, or before, the public hearing.

Any writing that is distributed to a majority of the Planning Commission by any person in connection with an agenda item is a public record (subject to any exemption under the Public Records Act) and is available by request by emailing the city clerk at jaherren@menlopark.gov. Persons with disabilities, who require auxiliary aids or services in attending or participating in Planning Commission meetings, may call the City Clerk's Office at 650-330-6620.

Agendas are posted in accordance with Cal. Gov. Code §54954.2(a) or §54956. Members of the public can view electronic agendas and staff reports by accessing the City website at menlopark.gov/agendas and can receive email notification of agenda postings by subscribing at menlopark.gov/subscribe. Agendas and staff reports may also be obtained by contacting City Clerk at 650-330-6620. (Posted: 1/18/2023)



STAFF REPORT

Planning Commission

Meeting Date:

1/23/2023

Staff Report Number:

23-007-PC

Public Hearing:

Consider and adopt a resolution to deny a variance to increase the height of the daylight plane from 19 feet, six inches to approximately 23 feet, seven inches, and to deny a use permit to demolish an existing one-story, single-family residence and construct a new two-story residence with a basement on a substandard lot with regard to minimum lot area and width in the R-1-U (Single Family Urban Residential) zoning district, at 103 Dunsmuir Way

Recommendation

Staff recommends that the Planning Commission adopt a resolution denying a variance to increase the height of the daylight plane from 19 feet, six inches to approximately 23 feet, seven inches, and denying a use permit to demolish an existing one-story, single-family residence and construct a new two-story residence with a basement on a substandard lot with regard to minimum lot area and width in the R-1-U (Single Family Urban Residential) zoning district. The draft resolution is included as Attachment A.

Policy Issues

Each use permit and variance request is considered individually. The Planning Commission should consider whether the required findings can be made for the proposal.

Background

Site location

The subject property is a corner lot located at the northeastern corner of the intersection of Hedge Road and Dunsmuir Way in the Suburban Park-Lorelei Manor-Flood Triangle neighborhood. All neighboring properties are similarly located in the R-1-U (Single Family Urban Residential) zoning district. A location map is included as Attachment B. This block of Dunsmuir Way features a mixture of one-story ranch-style residences and generally newer two-story residences in proximity to the subject site.

Analysis

Project description

The applicant is proposing to demolish the existing one-story, single-family residence and construct a new two-story, single-family residence with a basement. A data table summarizing parcel and project characteristics is included as Attachment C. The project plans and project description letter are included as Attachment A, Exhibits A and B, respectively.

The proposed residence would be a five-bedroom, six-bathroom home. The first floor would be primarily shared living space, including the kitchen, dining room, living room, laundry room, office, an art/bedroom, and one bathroom. Two covered porches, an uncovered patio, and a two-car garage providing the required parking for the home are located at the first floor. The second floor would contain four bedrooms and four bathrooms. The basement would include a gym, music room, theater/multipurpose room, one bathroom, and storage. The basement is proposed to be internally accessible with a staircase that extends all levels of the home and also via a lightwell with staircase at the west side of the residence, adjacent to the garage.

The proposal includes a variance request to increase the height of the daylight plane to approximately 23 feet, seven inches, where the daylight plane is measured from 19 feet, six inches (Municipal Code Chapter 16.67). In other words, at the increased height of approximately 23 feet, seven inches, the project, as proposed, would no longer intrude into the daylight plane—but as this increased daylight plane height is not permitted by the Municipal Code, a variance is requested. Municipal Code Section 16.67.020(2)(B) permits some gable and dormer intrusions, however, the extent of the intrusion is limited by the required side setback and in cases where the setback is eight feet or greater (the required side setback along Dunsmuir Way is 12 feet), no intrusions are permitted.

Aside from non-compliance with the daylight plane requirement, the proposed residence would meet all other Zoning Ordinance (Municipal Code Title 16) requirements for setbacks, lot coverage, floor area limit (FAL), and height. Of particular note, the project would have the following characteristics with regard to the Zoning Ordinance:

- The proposed floor area would be near the maximum, with 2,795.8 square feet proposed where 2,800 square feet is the maximum permitted. Note, the total square footage of buildings is 3,697.6 square feet, however, the proposed 901.8 square-foot basement area is excluded from the FAL calculation per Municipal Code Section 16.04.313(c).
- The proposed residence would be below the maximum building coverage, with 31.7 percent proposed where 35 percent is the maximum building coverage.
- The proposed second floor of the residence would be below the second floor limit, with 976.0 square feet proposed where 1,400 square feet is the maximum second-story floor area limit.
- The proposed residence would be below the maximum height, with 23 feet, seven inches proposed where 28 feet is the maximum permitted height.

The site is a corner lot fronting on two public streets, Hedge Road and Dunsmuir Way. Pursuant to Municipal Code Section 16.04.400, the front lot line is to the west (facing Hedge Road) and this thereby dictates the rear lot line (east, adjacent to 107 Dunsmuir Way) and side lot lines (north, adjacent to 124 Hedge Road, and south, facing Dunsmuir Way). The proposed residence would have a front and rear setback of 20 feet, where 20 feet is required in either case. The proposed residence would have an interior/north side setback of six feet, where six feet is required. The proposed residence would have a street/south side setback of 12 feet, four inches, where 12 feet is required. No balconies are proposed.

Design and materials

Within the project description letter (Attachment A, Exhibit B), the applicant indicates that the architectural style of the residence is California Craftsman. The exterior includes wood shingles, exposed wooden rafter tails, and copper gutters and downspouts. Dark grey composition shingles are proposed for the roof. The massing of the second level is located close to Dunsmuir Way. The south façade of the residence includes a step back in the center portion of the home to incorporate a protected front porch. The residence would also include metal clad windows with a gridding pattern (simulated divided lite with spacer bar) as well as copper round gutters and downspouts. All second-story windows would have a minimum sill height of at

least two feet, 10 inches, with greater sill heights proposed for certain windows at the north, east, and west elevations. It is noted that at the south elevation facing Dunsmuir Way, the second story window sill heights are 2 feet, 10 inches. Due to the extent of the setback from the street, staff believes the windows would not create privacy issues for neighbors across the street.

Staff believes that the scale, materials, and style of the proposed residence would result in a consistent aesthetic approach and are generally consistent with the broader neighborhood, given the similar and compatible architectural styles and sizes of structures in the area. However, Municipal Code Section 16.82.010 states, "The purpose of the use permit is to allow the proper integration into the community of uses which may be suitable only in specific locations in a zoning district, or if such uses are designed or laid out on the site in a particular manner." And further, Municipal Code Section 16.82.030 states, "In considering an application, the Planning Commission shall consider and give due regard to the nature and condition of all adjacent uses and structures, and to general and specific plans for the area in question and surrounding areas, and the impact of the application thereon." The design and layout of the proposed residence results in an intrusion into the daylight plane required by Municipal Code Chapter 16.67, and as such, staff believes that the use permit purpose and granting findings are not able to be met by the project as designed.

Variance

As part of this proposal, the applicant is requesting a variance to increase the height of the daylight plane to approximately 23 feet, seven inches, where the daylight plane is measured from 19 feet, six inches. The applicant has provided a variance request letter which is included as Attachment A, Exhibit C. The required variance findings (Municipal Code Section 16.82.340) are evaluated below in succession, with a high-level summary of the applicant-prepared response to finding followed by staff's analysis:

1. *That a hardship peculiar to the property and not created by any act of the owner exists. In this context, personal, family or financial difficulties, loss of prospective profits and neighboring violations are not hardships justifying a variance. Further, a previous variance can never have set a precedent, for each case must be considered only on its individual merits;*

The applicant states that the hardship is to the fact that the corner lot is in a unique orientation and relationship with neighbors that is not similarly experienced by the vast majority of other properties in the vicinity.

Staff believes that the conditions upon which the variance is requested would be applicable to other property in the same zoning district and beyond, whether they are a corner lot parcel or otherwise. Daylight planes are a Zoning Ordinance requirement for the R-E (Residential Estate), R-E-S (Residential Estate Suburban), R-2 (Low Density Apartment), R-1-S (Single Family Suburban Residential), and R-1-U (Single Family Urban Residential) zoning districts (Municipal Code Chapter 16.67).

2. *That such variance is necessary for the preservation and enjoyment of substantial property rights possessed by other conforming property in the same vicinity and that a variance, if granted, would not constitute a special privilege of the recipient not enjoyed by his/her neighbors;*

The applicant states that the requested variance is necessary for the preservation of substantial property rights enjoyed by other properties in the vicinity because for the vast majority of properties in the neighborhood, the front façade of the residence is not subject to the daylight plane limitation. The applicant

states that the vast majority of properties in the neighborhood have a side yard that faces a neighboring property, unlike the situation present at 103 Dunsmuir Way.

Staff believes that allowing the building intrusion into the daylight plane would constitute a special privilege for the owners because other properties in the same zoning district and beyond, whether they are a corner lot parcel or otherwise, are required to abide by this Zoning Ordinance requirement. The front façade of a residence is not dictated by property lines and owners are able to design their homes with orientation and layout as they see fit, subject to conformance with the Municipal Code. The proposal is a new two-story, single-family residence and there is ability for the residence to be designed without building intrusion into the daylight plane.

- 3. That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property; and*

Staff believes that the proposal would not impair the supply of light and air to the adjacent properties.

- 4. That the conditions upon which the requested variance is based would not be applicable, generally, to other property within the same zoning classification.*

The applicant states that the corner lot is in a unique orientation and relationship with neighbors that is not similarly experienced by the vast majority of other properties in the vicinity.

Staff believes that the conditions upon which the variance is requested would be applicable to other property in the same zoning district and beyond, whether they are a corner lot parcel or otherwise. Daylight planes are a Zoning Ordinance requirement for the R-E (Residential Estate), R-E-S (Residential Estate Suburban), R-2 (Low Density Apartment), R-1-S (Single Family Suburban Residential), and R-1-U (Single Family Urban Residential) zoning districts (Municipal Code Chapter 16.67). The lot is substandard with regard to minimum lot area (6,552 square feet provided where 7,000 square feet minimum is required) and minimum lot width (60 feet provided where 65 feet minimum is required) in the R-1-U (Single Family Urban Residential) zoning district. Existing lots that do not meet the minimum lot width, depth, and/or area in their respective zoning districts are considered to be substandard. Substandard lots are common within the city and are subject to the same development regulations, including daylight plane. The proposal is a new two-story, single-family residence and there is ability for the residence to be designed without building intrusion into the daylight plane.

- 5. That the condition upon which the requested variance is based is an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process.*

The property is not within any Specific Plan area and thereby a finding regarding an unusual factor does not apply.

Approval of a variance requires that all five findings be made. Staff believes not all the requisite findings can be made and thereby recommends denial of the variance request. Findings to this effect are included in the draft resolution (Attachment A).

Alternate plan

In preparation for the Planning Commission public hearing, staff recommended to the applicant the

preparation of an alternate plan without building intrusion into the daylight plane. As of the compilation of this report, no alternate plan has been submitted. Should the Commission feel it is appropriate, there is the option to continue the public hearing with direction to the applicant, if they are willing, to return with an alternate plan without building intrusion into the daylight plane, negating the need for a variance. On January 16, 2023, the property owners submitted correspondence for staff and the Planning Commission's consideration (Attachment A, Exhibit B). The correspondence notes the desire to proceed with the variance application and not provide an alternate plan at this time and also provides a summary of support for the variance.

Trees and landscaping

The applicant has submitted an arborist report (Attachment D), detailing the species, size, and conditions of on-site and nearby trees. There are no trees overhanging from neighboring properties. The arborist report lists a total of four trees on the subject property of which three are considered heritage trees (Trees #1, 2, and 3). Tree #1 is located on-site while Trees #2 and #3 are street trees along Dunsmuir Way and Hedge Road, respectively. No heritage trees are proposed for removal. There is one non-heritage persimmon tree (Tree #4) proposed for removal. The arborist report includes tree protection recommendations for the pre-construction, construction, and post-construction phases of the project, if approved. As part of the project review process, the arborist report was reviewed by the City Arborist. The applicant has not proposed any additional landscaping at this time.

Correspondence

Within the project description letter (Attachment A Exhibit B), the applicant indicates that the property owners have conducted extensive outreach to the neighbors, focusing their efforts on the immediate neighbors that may be most significantly affected. The applicant indicates that to date, no negative feedback has been received in relation to the proposed project. Letters of support from neighbors are provided, including, 124 Hedge Road (adjoining neighbor to the north), 119 Hedge Road (across street neighbor to the west), and 112 Dunsmuir Way (across street neighbor to the southeast).

As of the compilation of this report, staff has received correspondence from: Jon Wright and Annabel Chang, residents at 112 Dunsmuir Way (across street neighbor to the southeast); Martin de Jong and Eleanor de Jong, residents at 116 Dunsmuir Way (across street neighbor to the south); and Jerry and Carol Marsh, residents at 124 Hedge Road (adjoining neighbor to the north). All correspondence received has been in support of the proposed project (Attachment E).

Conclusion

Staff believes that the scale, materials, and style of the proposed residence would result in a consistent aesthetic approach and are generally consistent with the broader neighborhood, given the similar and compatible architectural styles and sizes of structures in the area. However, the design and layout of the proposed residence results in an intrusion into the daylight plane required by the Municipal Code, and as such, staff believes that the use permit purpose and granting findings are not able to be met by the project as designed, and further, not all required variance findings can be met.

Staff recommends that the Planning Commission adopt a resolution denying a variance to increase the height of the daylight plane from 19 feet, six inches to approximately 23 feet, seven inches, and denying a use permit to demolish an existing one-story, single-family residence and construct a new two-story residence with a basement on a substandard lot with regard to minimum lot area and width in the R-1-U (Single Family Urban Residential) zoning district. The draft resolution is included as Attachment A.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 3 (Section 15303, "New Construction or Conversion of Small Structures") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Draft Planning Commission Resolution Adopting Findings of Denial for project Use Permit and Variance
Exhibits to Attachment A
 - A. Project Plans
 - B. Project Description Letter
 - C. Variance Letter
- B. Location Map
- C. Data Table
- D. Arborist Report
- E. Correspondence

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings, and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:
Calvin Chan, Senior Planner

Report reviewed by:
Corinna Sandmeier, Principal Planner

PLANNING COMMISSION RESOLUTION NO. 2023-XX**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF MENLO PARK DENYING A VARIANCE TO INCREASE THE HEIGHT OF THE DAYLIGHT PLANE FROM 19 FEET, SIX INCHES TO APPROXIMATELY 23 FEET, SEVEN INCHES, AND DENYING A USE PERMIT TO DEMOLISH AN EXISTING ONE-STORY RESIDENCE AND CONSTRUCT A NEW TWO-STORY RESIDENCE WITH A BASEMENT ON A SUBSTANDARD LOT WITH REGARD TO MINIMUM LOT AREA AND WIDTH IN THE R-1-U ZONING DISTRICT**

WHEREAS, the City of Menlo Park (“City”) received an application requesting a use permit to construct a new two-story, single-family residence on a substandard lot in the R-1-U zoning district; the project includes a request for a variance to increase the height of the daylight plane from 19 feet, six inches to approximately 23 feet, seven inches (collectively, the “Project”) from John and Amy McGaraghan (“Owner” and “Applicant”), located at 103 Dunsmuir Way (APN 055-292-370) (“Property”). The use permit and variance are depicted in and subject to the development plans and documents which are attached hereto as Exhibit A through Exhibit C and incorporated herein by this reference; and

WHEREAS, the Property is located in the Single Family Urban Residential (R-1-U) zoning district, which supports the construction of single family residences; and

WHEREAS, the proposed Project does not comply with all objective standards of the R-1-U zoning district; and

WHEREAS, Municipal Code Section 16.67.020 includes requirements for daylight planes applicable to the R-1-U zoning district; and

WHEREAS, the applicant proposes a residence that includes building intrusion into the daylight plane and requests a variance for such; and

WHEREAS, the proposed Project was reviewed by the Engineering Division and found to be in compliance with standards from the Public Works Department; and

WHEREAS, the Applicant submitted an arborist report prepared by Aesculus Arboricultural Consulting which was reviewed by the City Arborist and found to be in compliance with the Heritage Tree Ordinance and proposes mitigation measures to adequately protect heritage trees in the vicinity of the project; and

WHEREAS, the Project, requires discretionary actions by the City as summarized above, and therefore the California Environmental Quality Act (“CEQA,” Public Resources Code Section §21000 et seq.) and CEQA Guidelines (Cal. Code of Regulations, Title 14, §15000 et seq.) require analysis and a determination regarding the Project’s environmental impacts; and

WHEREAS, the City is the lead agency, as defined by CEQA and the CEQA Guidelines, and is therefore responsible for the preparation, consideration, certification, and approval of environmental documents for the Project; and

WHEREAS, the Project is categorically exempt from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures); and

WHEREAS, all required public notices and public hearings were duly given and held according to law; and

WHEREAS, at a duly and properly noticed public hearing held on January 23, 2023, the Planning Commission fully reviewed, considered, and evaluated the whole of the record including all public and written comments, pertinent information, documents and plans, prior to taking action regarding the use permit and variance request.

NOW, THEREFORE, THE MENLO PARK PLANNING COMMISSION HEREBY RESOLVES AS FOLLOWS:

Section 1. Recitals. The Planning Commission has considered the full record before it, which may include but is not limited to such things as the staff report, public testimony, and other materials and evidence submitted or provided, and the Planning Commission finds the foregoing recitals are true and correct, and they are hereby incorporated by reference into this Resolution.

Section 2. Variance Findings. The Planning Commission of the City of Menlo Park does hereby make the following Findings per Section 16.82.340 of the Zoning Ordinance pertaining to the denial of a variance:

1. That a hardship peculiar to the property and not created by any act of the owner does not exist; in that, the conditions upon which the variance is requested would be applicable to other property in the same zoning district and beyond, whether they are a corner lot parcel or otherwise. Daylight planes are a Zoning Ordinance requirement for the R-E (Residential Estate), R-E-S (Residential Estate Suburban), R-2 (Low Density Apartment), R-1-S (Single Family Suburban Residential), and R-1-U (Single Family Urban Residential) zoning districts (Municipal Code Chapter 16.67).
2. That the variance is not necessary for the preservation and enjoyment of substantial property rights possessed by other conforming properties in the vicinity and that the variance, if granted, would constitute a special privilege of the recipient not enjoyed by his/her/their neighbors; in that, allowing the building intrusion into the daylight plane would constitute a special privilege for the owners because other properties in the same zoning district and beyond, whether they are a corner lot parcel or

otherwise, are required to abide by this Zoning Ordinance requirement. The front façade of a residence is not dictated by property lines and owners are able to design their homes with orientation and layout as they see fit, subject to conformance with the Municipal Code. The proposal is a new two-story, single-family residence and there is ability for the residence to be designed without building intrusion into the daylight plane.

3. That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property; in that, the proposal would not impair the supply of light and air to adjacent properties.
4. That the conditions upon which the requested variance is based would be applicable, generally, to property within the same zoning classification; in that, whether they are a corner lot parcel or otherwise, daylight planes are a Zoning Ordinance requirement for the R-E (Residential Estate), R-E-S (Residential Estate Suburban), R-2 (Low Density Apartment), R-1-S (Single Family Suburban Residential), and R-1-U (Single Family Urban Residential) zoning districts (Municipal Code Chapter 16.67). The lot is substandard with regard to minimum lot area (6,552 square feet provided where 7,000 square feet minimum is required) and minimum lot width (60 feet provided where 65 feet minimum is required) in the R-1-U (Single Family Urban Residential) zoning district. Existing lots that do not meet the minimum lot width, depth, and/or area in their respective zoning districts are substandard. Substandard lots are common within the city and are subject to the same development regulations, including daylight plane. The proposal is a new two-story, single-family residence and there is ability for the residence to be designed without building intrusion into the daylight plane.
5. That the condition upon which the requested variance is based is on an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process; in that, the subject parcel is not located within a Specific Plan area and thereby a finding regarding an unusual factor does not apply.

Section 3. Conditional Use Permit Finding. The Planning Commission of the City of Menlo Park does hereby make the following Finding per Section 16.82.030 of the Zoning Ordinance pertaining the denial of a use permit:

1. In considering an application, the Planning Commission must consider and give due regard to the nature and condition of all adjacent uses and structures, and to general and specific plans for the area in question and surrounding areas, and the impact of the application thereon. The Zoning Ordinance (Municipal Code Title 16) is an implementation tool of the City's general plan. The proposed Project does not comply with all objective standards of the R-1-U zoning district as it includes building intrusion into the daylight plane applicable to the R-1-U zoning district required by Municipal Code Section 16.67.020.

Section 4. Variance and Conditional Use Permit. The Planning Commission hereby denies the variance and denies the use permit No. PLN2022-00039, which variance and use permit are depicted in the project plans, project description letter, and variance letter which are attached hereto and incorporated herein by this reference as Exhibit A, Exhibit B, and Exhibit C, respectively.

Section 5. ENVIRONMENTAL REVIEW. The Planning Commission makes the following findings, based on its independent judgment after considering the Project, and having reviewed and taken into consideration all written and oral information submitted in this matter:

- A. The Project is categorically exempt from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures).

Section 6. SEVERABILITY

If any term, provision, or portion of these findings or the application of these findings to a particular situation is held by a court to be invalid, void or unenforceable, the remaining provisions of these findings, or their application to other actions related to the Project, shall continue in full force and effect unless amended or modified by the City.

I, Corinna Sandmeier, Acting Principal Planner and Planning Commission Liaison of the City of Menlo Park, do hereby certify that the above and foregoing Planning Commission Resolution was duly and regularly passed and adopted at a meeting by said Planning Commission on January 23, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

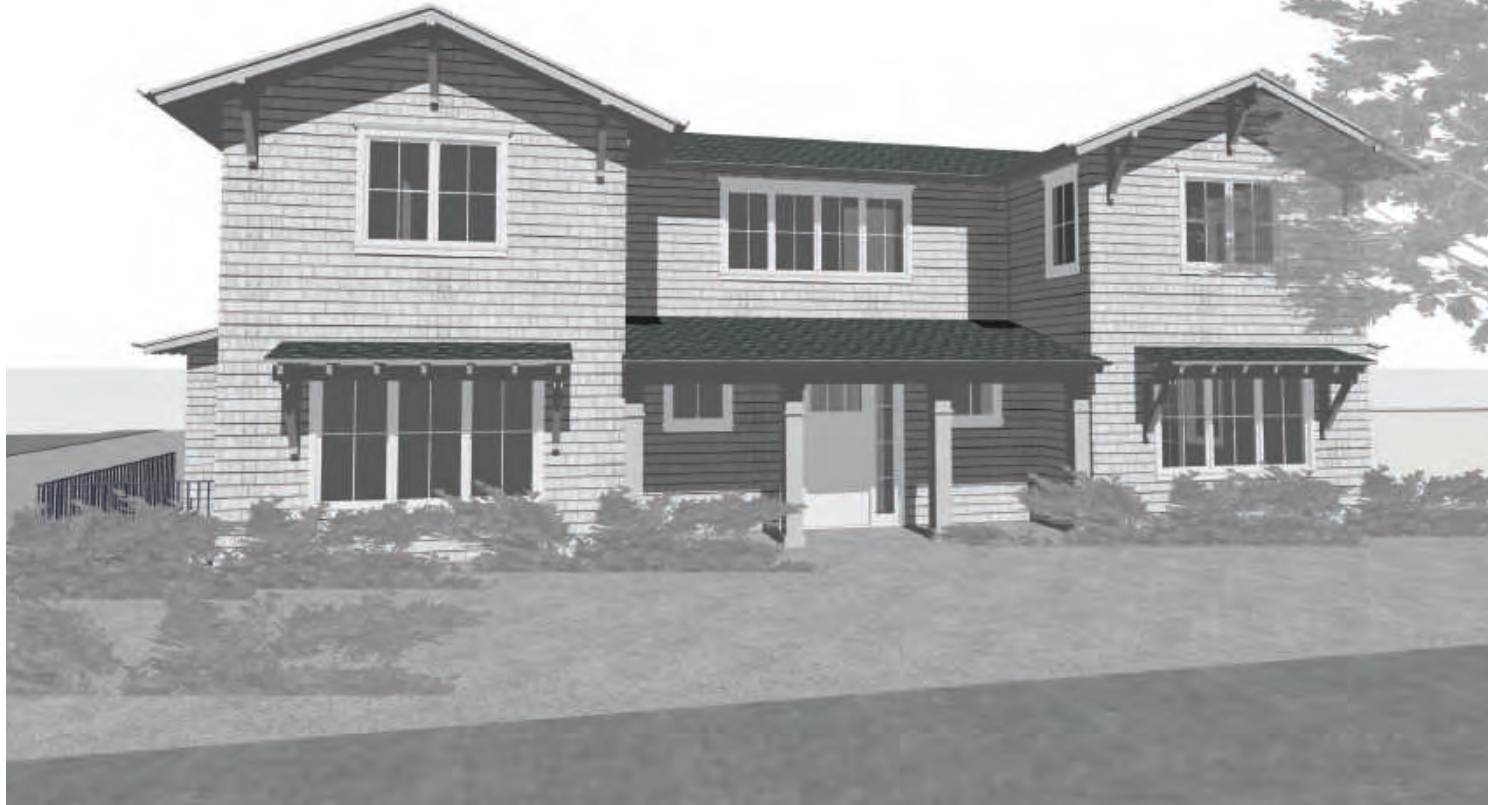
IN WITNESS THEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this 23rd day of January, 2023.

Corinna Sandmeier
Principal Planner and Planning Commission Liaison
City of Menlo Park

Exhibits

- A. Project Plans

- B. Project Description Letter
- C. Variance Letter



McGARAGHAN RESIDENCE

103 DUNSMUIR WAY



SCHNEIDER
DESIGN
ASSOCIATES

514 23rd AVENUE
SAN FRANCISCO, CALIFORNIA, 94121
415.845.5472

A.P.N. : 055-292-370
McGARAGHAN RESIDENCE
103 DUNSMUIR WAY
MENLO PARK, CA 94025



ISSUED:








PLANNING SUBMITTAL	07.01.22
▲ PLANNING RESPONSES	09.22.22
▲ PLANNING RESPONSES	12.15.22

SCALE:
JOB NO: 202128
A0.0

GENERAL NOTES

1. ALL WORK SHALL COMPLY WITH THE 2019 CALIFORNIA RESIDENTIAL BUILDING CODE. TYPE OF CONSTRUCTION: TYPE 5 NON-RATED. OTHER CODES: CPC, CMC, CEC, CFC (LATEST EDITIONS).
2. THE STANDARD A.I.A. GENERAL CONDITIONS ARE HEREBY MADE A PART OF THESE DRAWINGS.
3. DO NOT SCALE DRAWINGS FOR DIMENSIONS. WRITTEN DIMENSIONS SHALL TAKE PRECEDENCE OVER SCALED DIMENSIONS AND SHALL BE VERIFIED ON THE JOB SITE. ANY DISCREPANCY SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT PRIOR TO COMMENCEMENT OF ANY WORK.
4. CLARIFY ALL DISCREPANCIES BETWEEN THESE DRAWINGS AND ACTUAL SITE CONDITIONS WITH THE ARCHITECT BEFORE PERFORMING THE WORK.
5. THE CONTRACTOR SHALL:
 - A. FURNISH, PAY FOR AND FILE ALL NECESSARY PERMITS, FEES, INSPECTIONS, ETC.; EXCEPT FOR PLAN CHECK AND ZONING FEES, WHICH WILL BE PAID FOR BY THE OWNER.
 - B. INSTALL ALL MATERIALS AND EQUIPMENT IN ACCORDANCE WITH MANUFACTURER'S DIRECTIONS, AS APPLICABLE.
 - C. PROTECT EXISTING VEGETATION FROM DAMAGE DURING THE COURSE OF THE WORK. BRACE STRUCTURE AS REQUIRED DURING CONSTRUCTION.
 - D. PROVIDE STRICT CONTROL OF JOB CLEANUP TO REMOVE DUST AND DEBRIS FROM CONSTRUCTION AREA.
6. ALL INSPECTIONS ARE REQUIRED AS PER SFBC SEC. 109.
7. INSULATION SHALL MEET CALIFORNIA ENERGY COMMISSION QUALITY STANDARDS AND BE CERTIFIED BY THE MANUFACTURER.
8. FIRESTOPS SHALL BE PROVIDED IN ALL LOCATIONS SPECIFIED BY SFBC CHAPTER 9.
9. ALL ELECTRICAL OUTLETS IN EXTERNAL WALLS SHALL HAVE INSULATING GASKETS.

SYMBOLS

-  DIMENSION TO FACE OF FRAMING
-  DIMENSION TO CENTERLINE
-  DIMENSION TO FACE OF FINISH
-  EXTERIOR ELEVATION REFERENCE
-  ELEVATION DATUM POINT
-  DETAIL REFERENCE
-  SECTION REFERENCE

PROJECT INFORMATION

PARCEL NUMBER: 055-292-370

ZONING: R-1-U
 OCCUPANY TYPE: R-3
 CONSTRUCTION TYPE: V B

PROPERTY ADDRESS: 103 DUNSMUIR WAY
 MENLO PARK, CA 94025

PROPERTY OWNERS: JOHN & AMY McGARAGHAN

FLOOR AREA LIMIT

LOT SIZE: 6,552 S.F.
 ALLOWABLE HOUSE SIZE: 2,800 S.F.

PROPOSED HOUSE

FIRST STORY (INCLUDING GARAGE): 1,820 S.F.
 SECOND STORY: 976 S.F.
 PROPOSED TOTAL: 2,796 S.F.

PROPOSED BASEMENT

BASEMENT: 902 S.F.
 NOTE: SEE 1/A2.5 FOR FLOOR AREA LIMIT CALCULATIONS

SECOND STORY FLOOR AREA LIMIT

ALLOWABLE SECOND STORY LIMIT: 50% F.A.L.
 = 2,800 x 50% 1,400 S.F.
 PROPOSED SECOND STORY: 976 S.F.
 976 S.F. < 1,400 S.F.
 NOTE: SEE 1/A2.5 FOR FLOOR AREA LIMIT CALCULATIONS

BUILDING COVERAGE

LOT SIZE: 6,552 S.F.
 ALLOWABLE COVERAGE (35% LOT SIZE): 2,293 S.F.
 PROPOSED BUILDING COVERAGE: 2,079 S.F.

NOTE: SEE 2/A2.5 FOR BUILDING COVERAGE CALCULATION

BUILDING HEIGHT

ALLOWABLE HEIGHT: 28'-0"
 PROPOSED HEIGHT: 23'-7"

NOTE: SEE A3.1 & A3.2 FOR DAYLIGHT PLANE INFORMATION

ARCHITECTURAL DRAWINGS

- A0.0 COVER SHEET
- A0.1 INFORMATION SHEET SURVEY
- A1.0 AREA PLAN & STREETSCAPES
- A1.1 EXISTING SITE PLAN
- A1.2 PROPOSED SITE PLAN
- A1.3 TREE PROTECTION PLAN
- A2.0 EXISTING FIRST FLOOR PLAN
- A2.1 PROPOSED FIRST FLOOR PLAN
- A2.2 PROPOSED SECOND FLOOR PLAN
- A2.3 PROPOSED BASEMENT PLAN
- A2.4 ROOF PLAN
- A2.5 FLOOR AREA, BUILDING COVERAGE, & AVERAGE GRADE DIAGRAMS
- A3.0 EXISTING EXTERIOR ELEVATIONS
- A3.1 PROPOSED EXTERIOR ELEVATIONS
- A3.2 PROPOSED EXTERIOR ELEVATIONS

SECTIONS

- A5.1 PERSPECTIVES

DIRECTORY

OWNER: JOHN & AMY McGARAGHAN
 103 DUNSMUIR WAY
 MENLO PARK, CA 94025

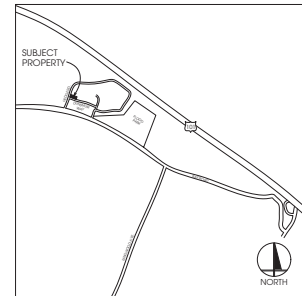
ARCHITECT: SCHNEIDER DESIGN ASSOCIATES
 MARSHALL SCHNEIDER
 514 23rd AVENUE
 SAN FRANCISCO, CA 94121
 TEL: 415.845.5472
 EMAIL: MSCHNEIDER@SCHNEIDERDESIGNINC.COM

CONTRACTOR: T.B.D.

DESCRIPTION OF WORK

- DEMOLISH (E) 1,737 S.F. SINGLE STORY HOUSE
- CONSTRUCT (N) 2,796 S.F. TWO-STORY HOME WITH 902 S.F. BASEMENT & 259 S.F. COVERED PORCHES

VICINITY MAP



CODE INFORMATION

ADOPTED CODE	EDITION
CALIFORNIA BUILDING CODE	2019
CALIFORNIA ENERGY STANDARDS	2019
CALIFORNIA PLUMBING CODE	2019
CALIFORNIA MECHANICAL CODE	2019
CALIFORNIA ELECTRICAL CODE	2019
CALIFORNIA FIRE CODE	2019
CALIFORNIA GREEN ENERGY CODE	2019



SCHNEIDER DESIGN ASSOCIATES
 514 23rd AVENUE
 SAN FRANCISCO, CALIFORNIA, 94121
 415.845.5472

A.P.N. : 055-292-370
MCGARAGHAN RESIDENCE
 103 DUNSMUIR WAY
 MENLO PARK, CA 94025



ISSUED:

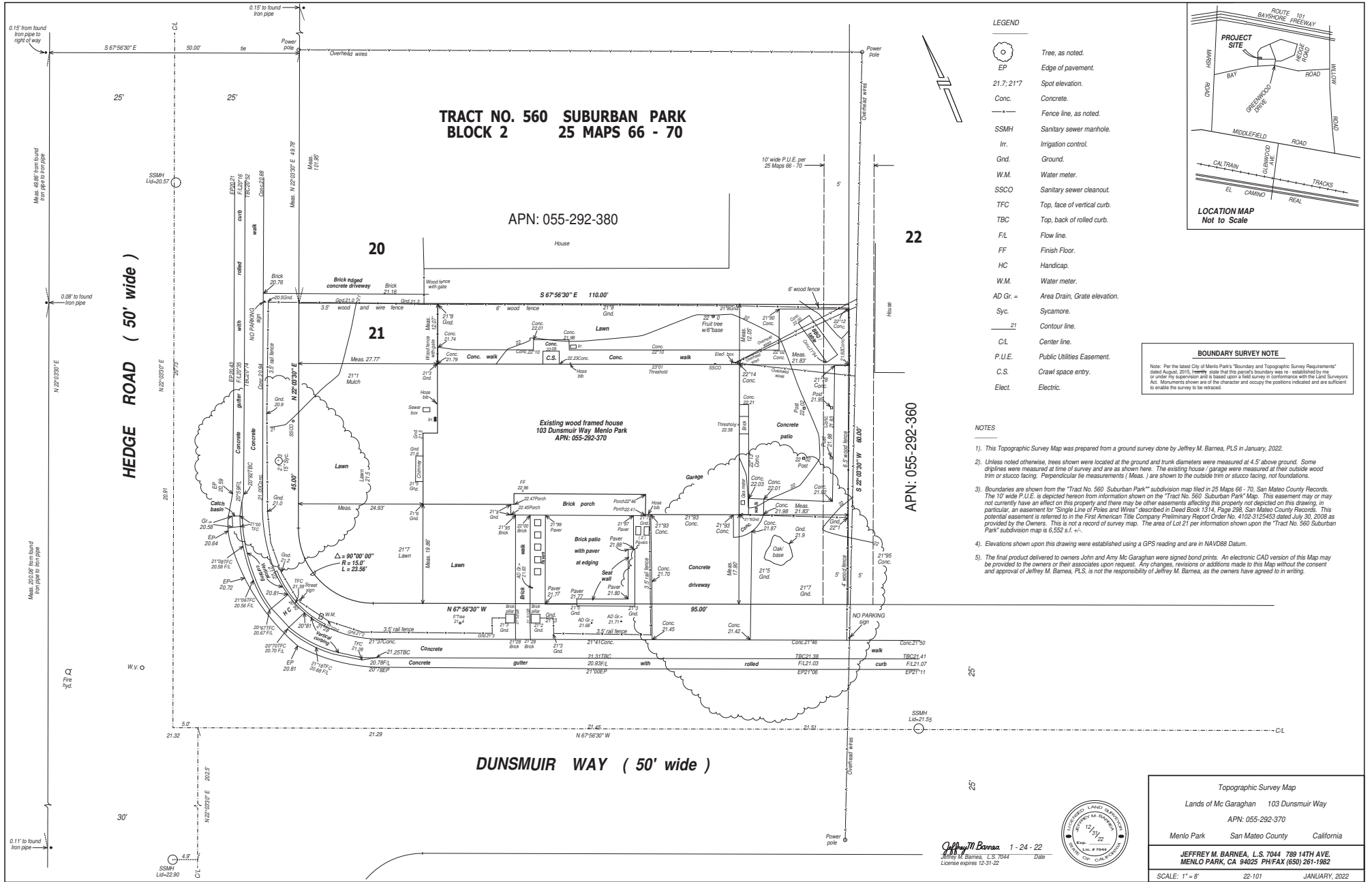
PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

INFORMATION SHEET

SCALE: AS NOTED

JOB NO.: 202128

A0.1



**TRACT NO. 560 SUBURBAN PARK
BLOCK 2
25 MAPS 66 - 70**

APN: 055-292-380

APN: 055-292-360

DUNSMUIR WAY (50' wide)

HEDGE ROAD (50' wide)

LEGEND

- Tree, as noted.
- Edge of pavement.
- 21.7, 21.7 Spot elevation.
- Conc. Concrete.
- Fence line, as noted.
- SSMH Sanitary sewer manhole.
- Ir. Irrigation control.
- Gnd. Ground.
- W.M. Water meter.
- SSCO Sanitary sewer cleanout.
- TFC Top, face of vertical curb.
- TBC Top, back of rolled curb.
- FL Flow line.
- FF Finish Floor.
- HC Handicap.
- W.M. Water meter.
- AD Gr. = Area Drain, Grate elevation.
- Syc. Sycamore.
- 21 Contour line.
- CL Center line.
- P.U.E. Public Utilities Easement.
- C.S. Crawl space entry.
- Elect. Electric.

BOUNDARY SURVEY NOTE

Note: Per the last City of Menlo Park's "Boundary and Topographic Survey Requirements" dated August, 2015, hereby state that this project's boundary was re-established by me, or under my supervision and is based upon a field survey in conformance with the Land Surveyors Act. Monuments shown are of the character and occupy the positions indicated and are sufficient to enable the survey to be retraced.

NOTES

- 1). This Topographic Survey Map was prepared from a ground survey done by Jeffrey M. Barnea, PLS in January, 2022.
- 2). Unless noted otherwise, trees shown were located at the ground and trunk diameters were measured at 4.5' above ground. Some driplines were measured at time of survey and are as shown here. The existing house / garage were measured at their outside wood trim or stucco facing. Perpendicular tie measurements (Meas.) are shown to the outside trim or stucco facing, not foundations.
- 3). Boundaries are shown from the "Tract No. 560 Suburban Park" subdivision map filed in 25 Maps 66 - 70, San Mateo County Records. The 10' wide P.U.E. is depicted hereon from information shown on the "Tract No. 560 Suburban Park" Map. This easement may or may not currently have an effect on this property and there may be other easements affecting this property not depicted on this drawing. In particular, an easement for "Single Line of Poles and Wires" described in Deed Book 1314, Page 298, San Mateo County Records. This potential easement is referred to in the First American Title Company Preliminary Report Order No. 4102-3125463 dated July 30, 2008 as provided by the Owners. This is not a record of a survey map. The area of Lot 21 per information shown upon the "Tract No. 560 Suburban Park" subdivision map is 6,532 s.f. +/-.
- 4). Elevations shown upon this drawing were established using a GPS reading and are in NAVD83 Datum.
- 5). The final product delivered to owners John and Amy Mc Garaghan were signed bond prints. An electronic CAD version of this Map may be provided to the owners or their associates upon request. Any changes, revisions or additions made to this Map without the consent and approval of Jeffrey M. Barnea, PLS, is not the responsibility of Jeffrey M. Barnea, as the owners have agreed to in writing.



Topographic Survey Map
Lands of Mc Garaghan 103 Dunsmuir Way
APN: 055-292-370
Menlo Park San Mateo County California

**JEFFREY M. BARNEA, L.S. 7044
MENLO PARK, CA 94025 PH/FAX (650) 261-1982**

SCALE: 1" = 8' 22-101 JANUARY, 2022



SCHNEIDER
DESIGN
ASSOCIATES

514 23rd AVENUE
SAN FRANCISCO, CALIFORNIA, 94121
415.845.5472

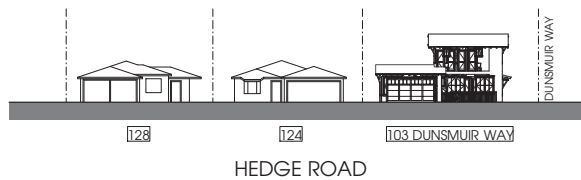
A.P.N. : 055-292-370
MCGARAGHAN RESIDENCE
103 DUNSMUIR WAY
MENLO PARK, CA 94025



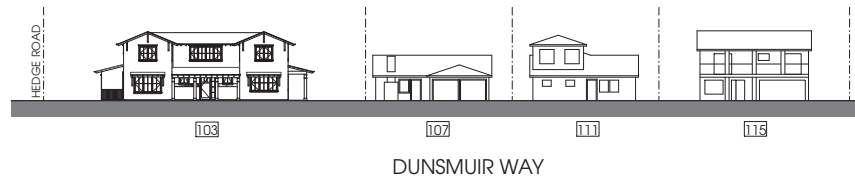
ISSUED:	
PLANNING SUBMITTAL:	07.01.22
PLANNING RESPONSES:	09.22.22
PLANNING RESPONSES:	12.15.22

AREA PLAN & STREETSCAPES
SCALE: AS NOTED
JOB NO.: 202128

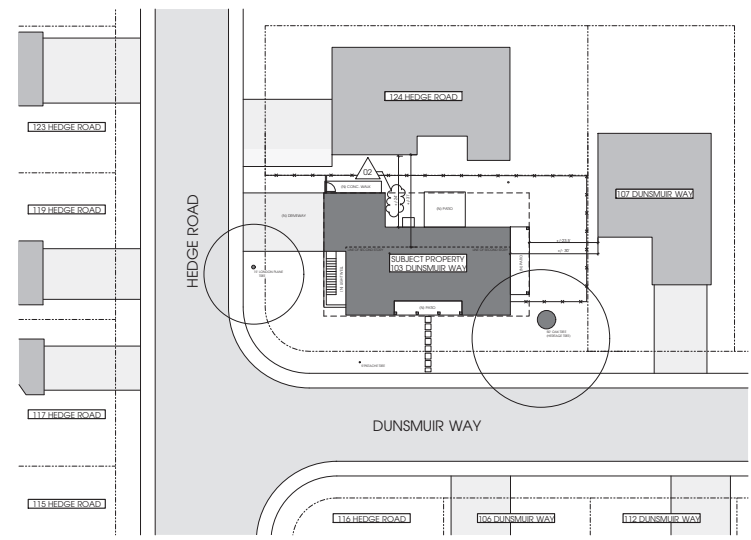
A1.0



3 WEST STREETSCAPE
SCALE: 1" = 20'



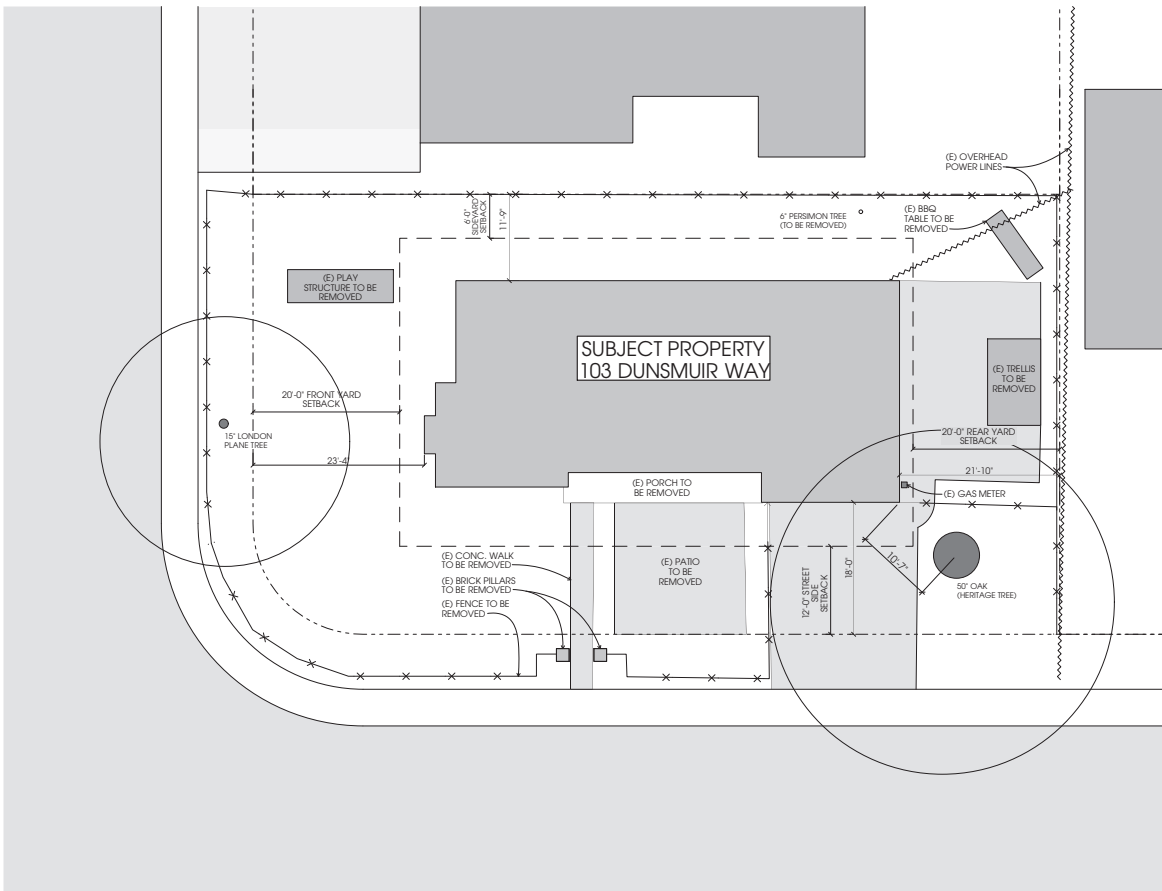
2 SOUTH STREETSCAPE
SCALE: 1" = 20'



1 AREA SITE PLAN
SCALE: 1" = 20'

LEGEND

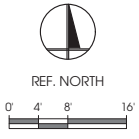
(E) STRUCTURES ON NEIGHBORING PROPERTIES	SETBACKS
(N) STRUCTURE ON SUBJECT PROPERTY	LINE OF 2 ND STORY ABOVE
	WOOD FENCE
	PROPERTY LINE



SITE ANALYSIS
ZONING : R-1-U

LOT AREA:	6,552 S.F.
ALLOWABLE FLOOR AREA: (PRESCRIBED BY CODE FOR LOTS BETWEEN 5,000 S.F. & 7,000 S.F.)	2,800 S.F.
EXISTING 1 ST FLOOR AREA:	1,737 S.F.
TOTAL EXISTING FLOOR AREA:	1,737 S.F.
LAND COVERED BY STRUCTURES:	27%
LANDSCAPING:	51%
PAVED AREAS:	22%
PARKING SPACES:	2 COVERED/2 UNCOVERED

1
A1.1
EXISTING SITE PLAN
SCALE: 1/8" = 1'-0"



ISSUED:

PLANNING SUBMITTAL:	07.01.22
PLANNING RESPONSES:	09.22.22
PLANNING RESPONSES:	12.15.22

EXISTING SITE PLAN

SCALE:	1/8" = 1'-0"
JOB NO.:	202128

A1.1



ISSUED:

PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

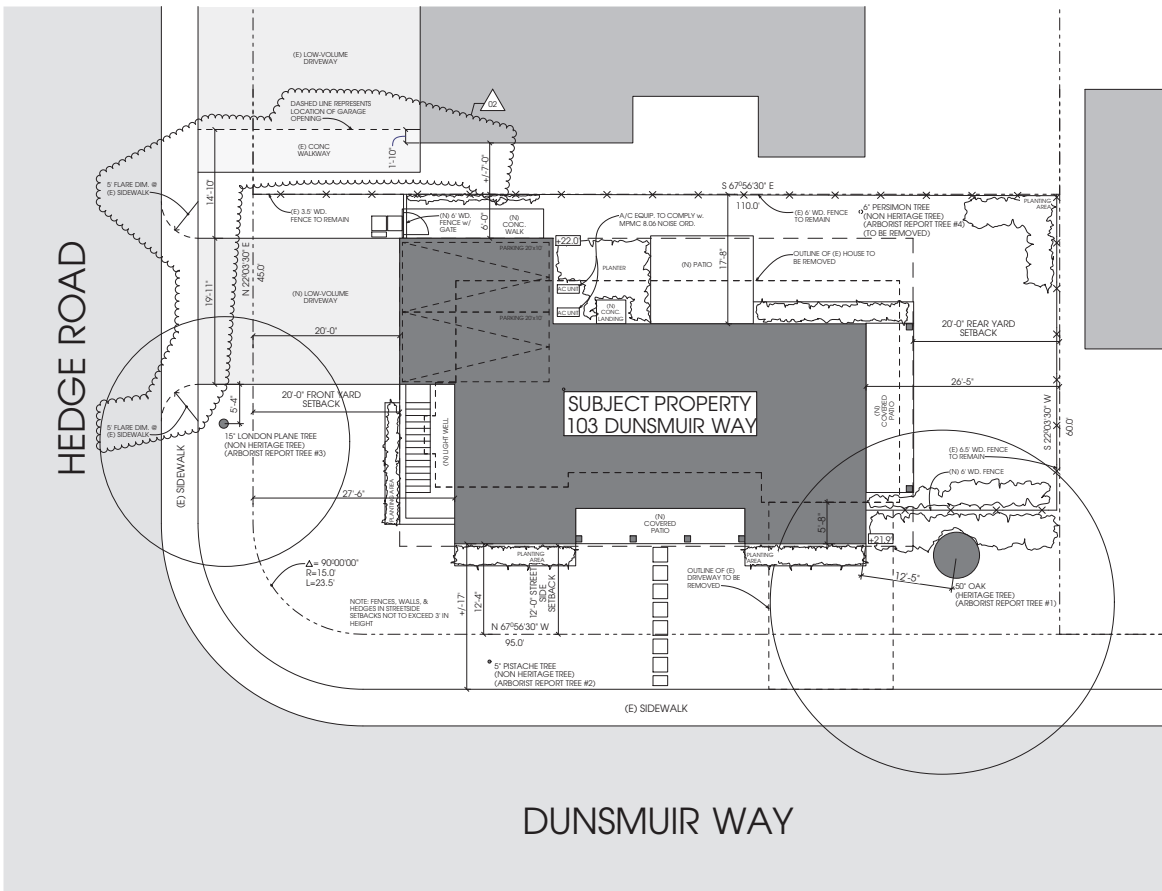
PROPOSED SITE PLAN

SCALE:	1/8" = 1'-0"
JOB NO.:	202128

- DEMOLISH (E) 1,737 S.F. SINGLE STORY HOUSE
- CONSTRUCT (N) 2,796 S.F. TWO-STORY HOME WITH 902 S.F. BASEMENT & 259 S.F. COVERED PORCHES

SITE ANALYSIS
ZONING : R-1-U

LOT AREA:	6,552 S.F.
ALLOWABLE FLOOR AREA: (PRESCRIBED BY CODE FOR LOTS BETWEEN 5,000 S.F. & 7,000 S.F.)	2,800 S.F.
PROPOSED 1 ST FLOOR AREA:	1,820 S.F.
PROPOSED 2 ND FLOOR AREA: (NO ATTIC SPACE > 5')	976 S.F.
TOTAL PROPOSED FLOOR AREA:	2,796 S.F.
LAND COVERED BY STRUCTURES:	32%
LANDSCAPING:	54%
PAVED AREAS:	14%
PARKING SPACES:	2 COVERED/2 UNCOVERED



1
A1.2 PROPOSED SITE PLAN
SCALE: 1/8" = 1'-0"



ISSUED:	
PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

TREE PROTECTION PLAN
SCALE: 1/8" = 1'-0"
JOB NO.: 202128

ARBORIST RECOMMENDATIONS (SEE ARBORIST REPORT FOR MORE DETAIL)

Preconstruction Phase

- Remove tree #4, upon approval from the City of Menlo Park (no permit appears to be needed).
- Install pre-demolition tree protection fencing approximately as shown in the Tree Map, below.
 - Minimum distances from trunk centers are given on the Tree Map. A larger area may be protected if desired.
 - Where existing barriers which will be retained impede access comparably to tree protection fencing, these barriers are an acceptable substitute for tree protection fencing.
 - Please be aware that tree protection fencing may differ from ideal tree protection zones, and from canopy sizes.
 - Tree protection fencing shall comprise 6' chain link fabric mounted on 1.5" diameter metal posts driven into the ground.
 - Place a 6" layer of wood chips inside tree protection fencing.
 - Tree protection fencing shall adhere to the requirements in the document titled "Tree Protection Specifications," available at <https://www.menlopark.org/DocumentCenter/View/90/Tree-Protection-Specifications>

Demolition Phase

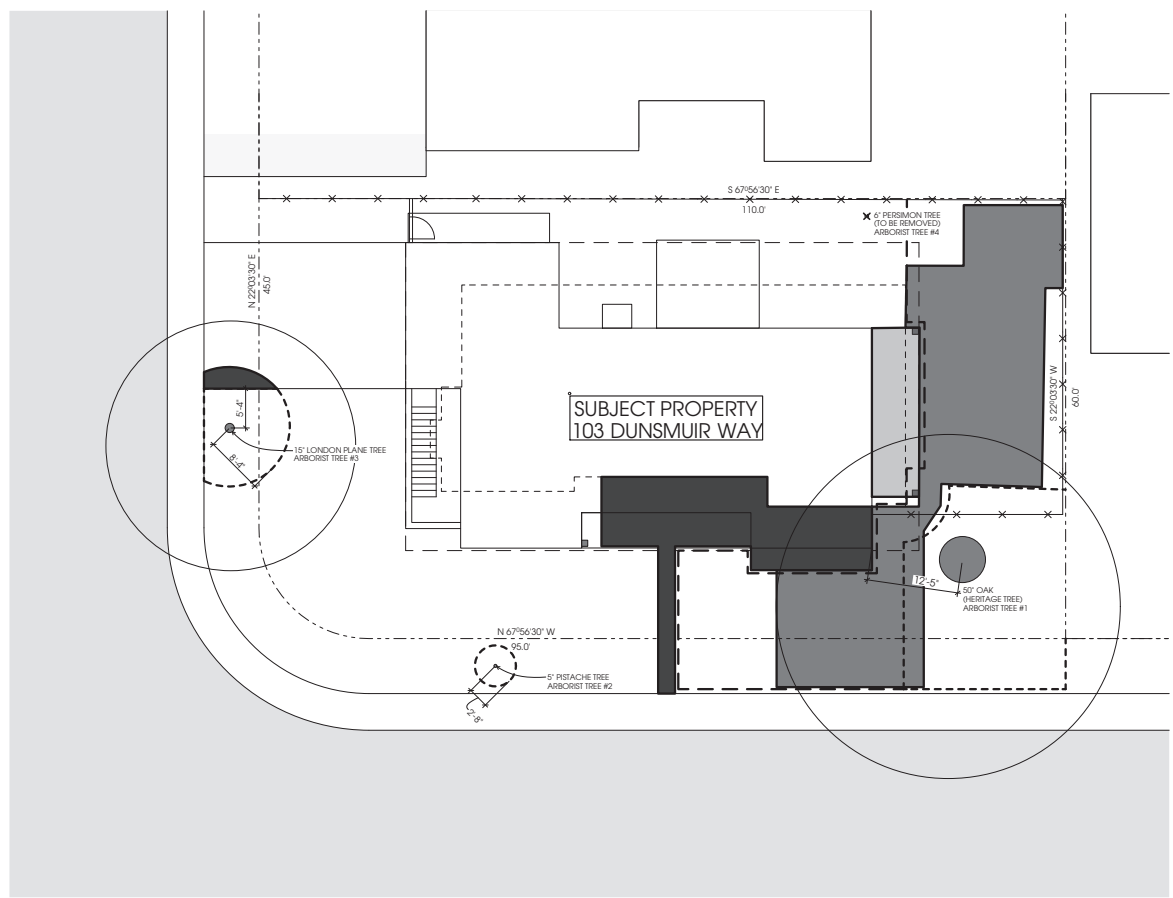
- When demolishing the existing hardscape within the TPZ and CRZ of tree #1, start work at the house and proceed backwards toward the street, limiting heavy equipment to still-paved areas.

Construction Phase

- Move tree protection fencing from the demolition locations to the post-demolition locations shown on the Tree Map. All specifications given above apply.
- Alert the project arborist if utility or other work becomes necessary within any tree TPZs.
- When excavating within TPZs for the front and side patios, front walkway, and driveway:
 - If footings will be used to support a raised surface, be as flexible as practical with footing placement to avoid any tree roots encountered.
 - Hand-excavate edge nearest trunk to the full depth of the feature being installed or to a depth of three feet, whichever is shallower.
 - Retain as many roots as practical.
 - If roots 1-2" in diameter must be cut, sever them cleanly with a sharp saw or bypass pruners.
 - If roots over 2" must be cut, stop work in that area and contact the project arborist for guidance.
 - Notify project arborist when excavation is complete. Project arborist shall inspect work to make sure all roots have been cut cleanly.
 - If excavation will be left open for more than 3 days:
 - Cover excavation wall nearest trunk with several layers of burlap or other absorbent fabric.
 - Install a timer and soaker hoses to irrigate with potable water twice per day, enough to wet fabric thoroughly.

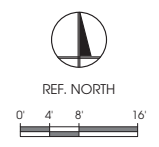
Post-Construction Phase

- Provide supplemental irrigation for tree #1 to aid in root regrowth for at least three years.



SUBJECT PROPERTY
103 DUNSMUIR WAY

LEGEND	
	AREA THAT MUST BE DEMOLISHED WHILE KEEPING VEHICLES ON (E) HARDSCAPE & WORKING BACKWARDS
	AREA WHERE EXCAVATION MUST BE DONE CAREFULLY, BY HAND - PRESERVE AS MANY ROOTS AS POSSIBLE
	TREE PROTECTION TO BE IN PLACE FOR DURATION OF PROJECT
	TREE PROTECTION TO BE IN PLACE DIRECTLY AFTER DEMOLITION FOR DURATION OF PROJECT



1
A1.3 TREE PROTECTION PLAN
SCALE: 1/8" = 1'-0"



SCHNEIDER
DESIGN
ASSOCIATES

514 23rd AVENUE
SAN FRANCISCO,
CALIFORNIA, 94121
415.845.5472

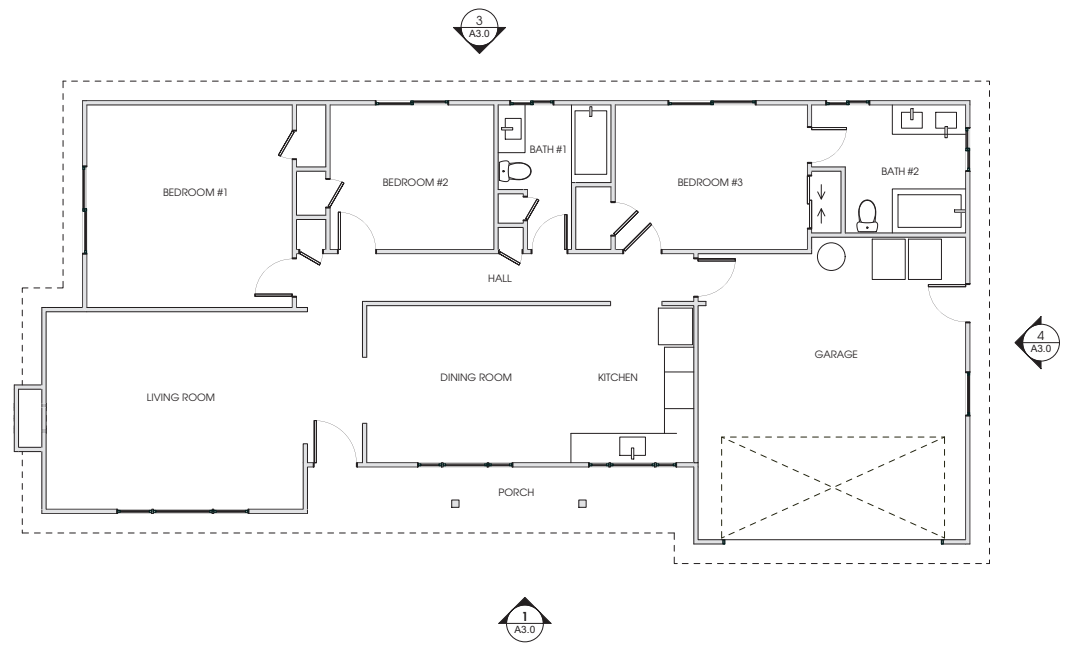
A.P.N. : 055-292-370
MCGARAGHAN RESIDENCE
103 DUNSMUIR WAY
MENLO PARK, CA 94025



ISSUED:	
PLANNING SUBMITTAL:	07.01.22
PLANNING RESPONSES:	09.22.22
PLANNING RESPONSES:	12.15.22

EXISTING FIRST FLOOR PLAN
SCALE: 1/4" = 1'-0"
JOB NO.: 202128

A2.0



GARAGE SQUARE FOOTAGE - 390 S.F.
 FIRST FLOOR SQUARE FOOTAGE - 1,347 S.F.
 TOTAL EXISTING SQUARE FOOTAGE - 1,737 S.F.



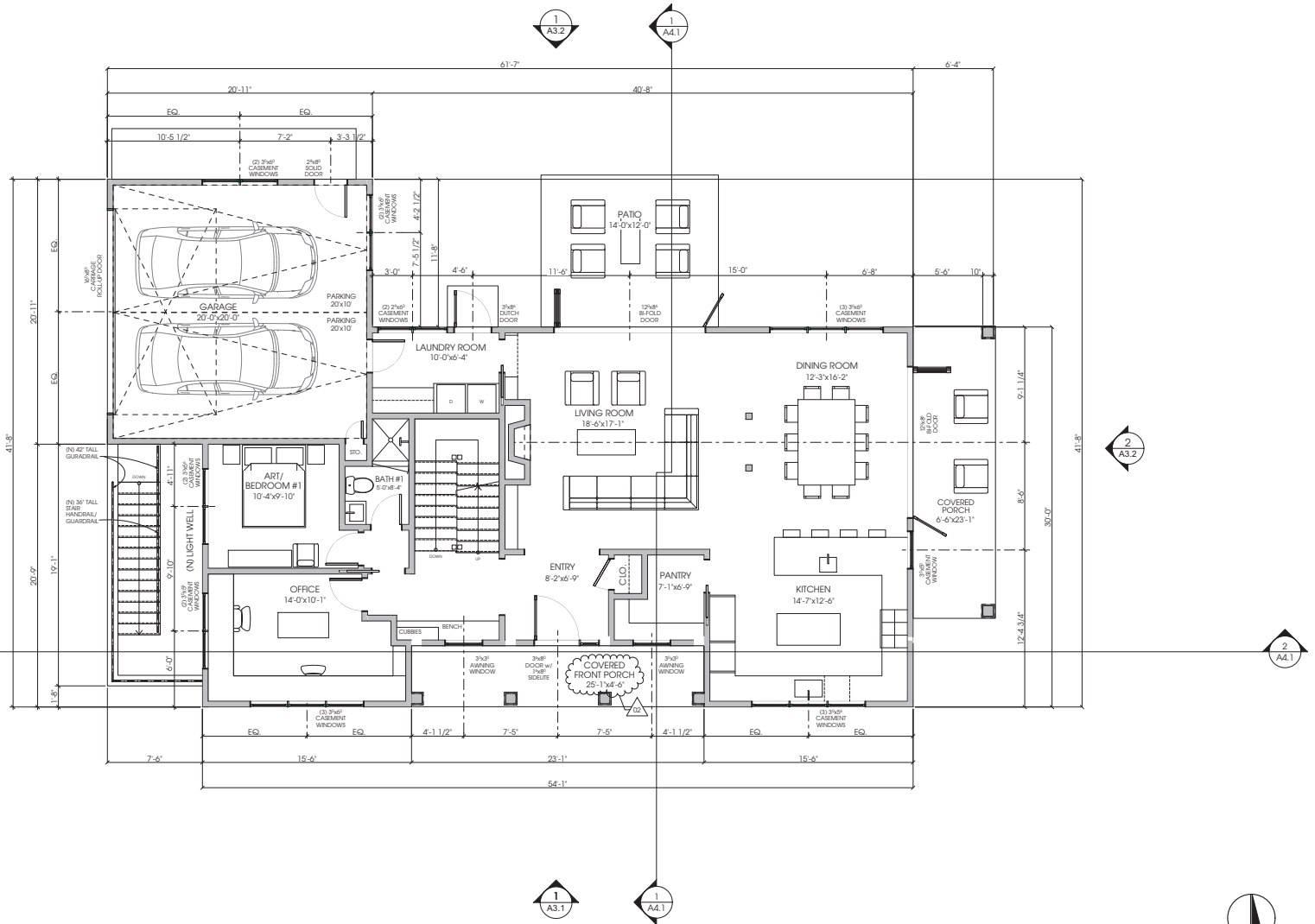
1
A2.0
EXISTING FIRST FLOOR PLAN (TO BE DEMOLISHED)
SCALE: 1/4" = 1'-0"



ISSUED:

PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

PROPOSED FIRST FLOOR PLAN
SCALE: 1/4" = 1'-0"
JOB NO: 202128



1 A2.1 PROPOSED FIRST FLOOR PLAN
SCALE: 1/4" = 1'-0"

LEGEND

(N) 2x6 CONSTRUCTION

NORTH

0 2 4 8'

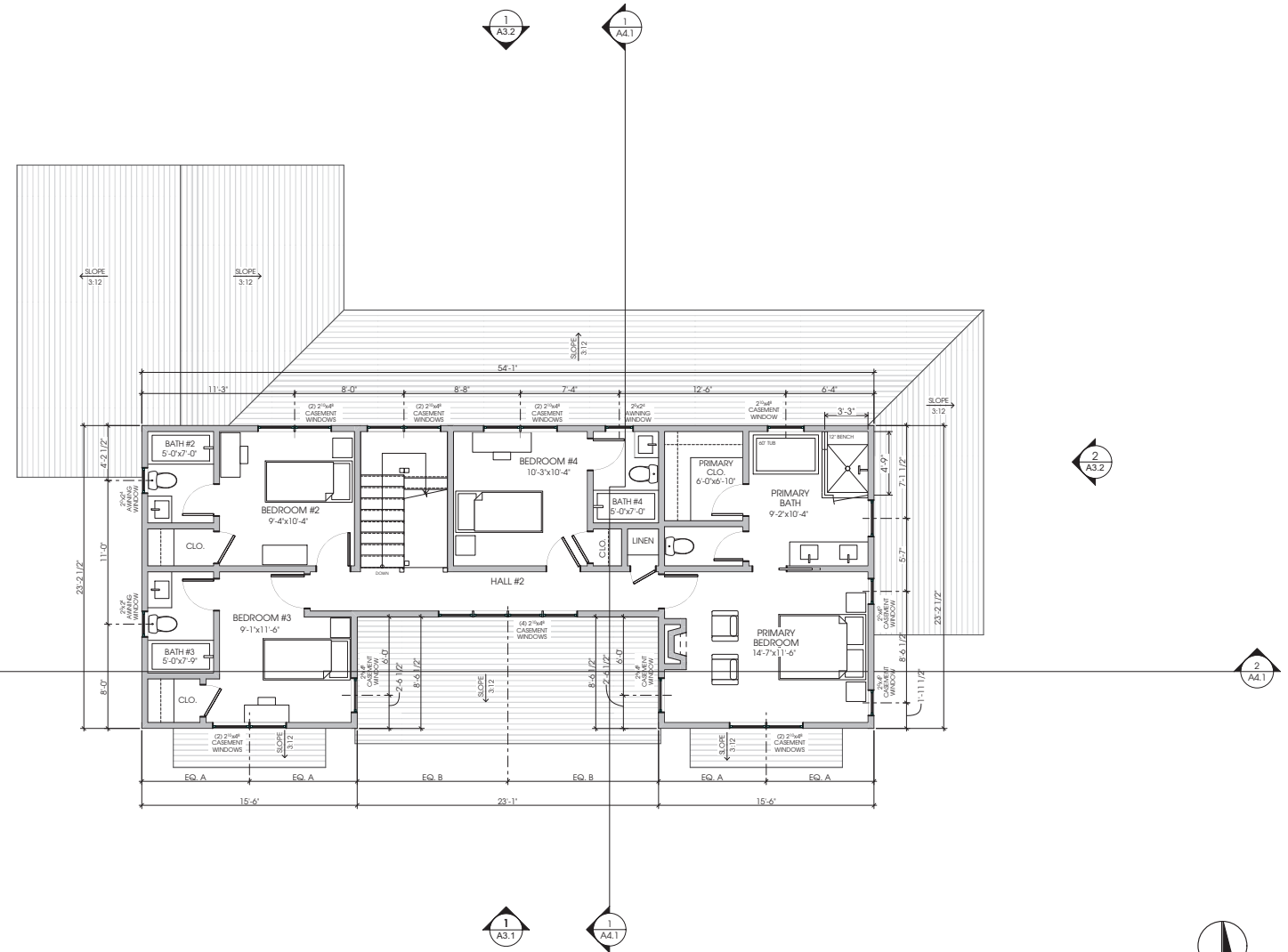


ISSUED:

PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

PROPOSED SECOND FLOOR PLAN
SCALE: 1/4" = 1'-0"
JOB NO.: 202128

A2.2



1 A2.2 PROPOSED SECOND FLOOR PLAN
SCALE: 1/4" = 1'-0"

LEGEND

(N) 2x6 CONSTRUCTION

NORTH

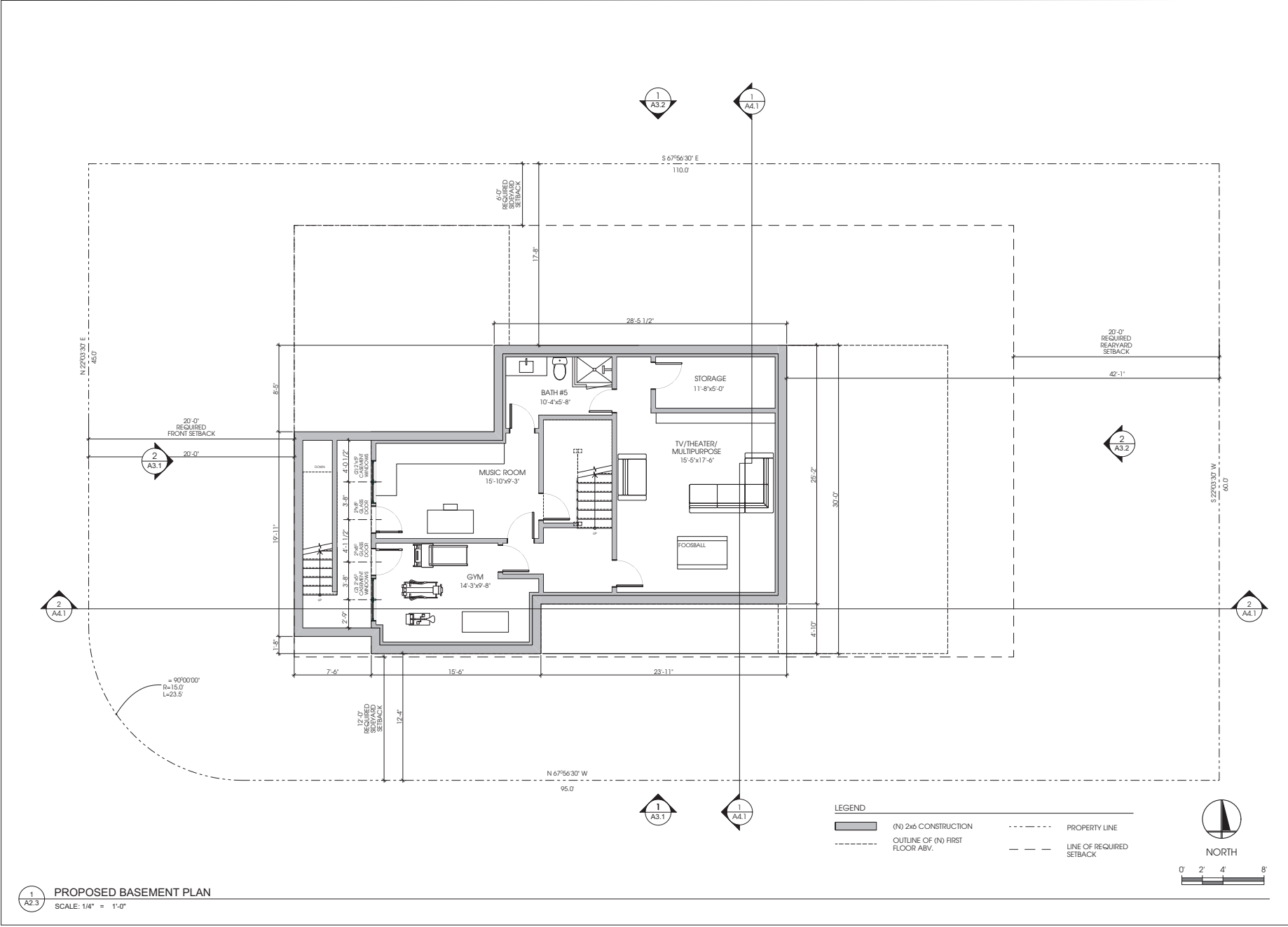


ISSUED:

PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

PROPOSED BASEMENT PLAN

SCALE:	1/4" = 1'-0"
JOB NO.:	202128



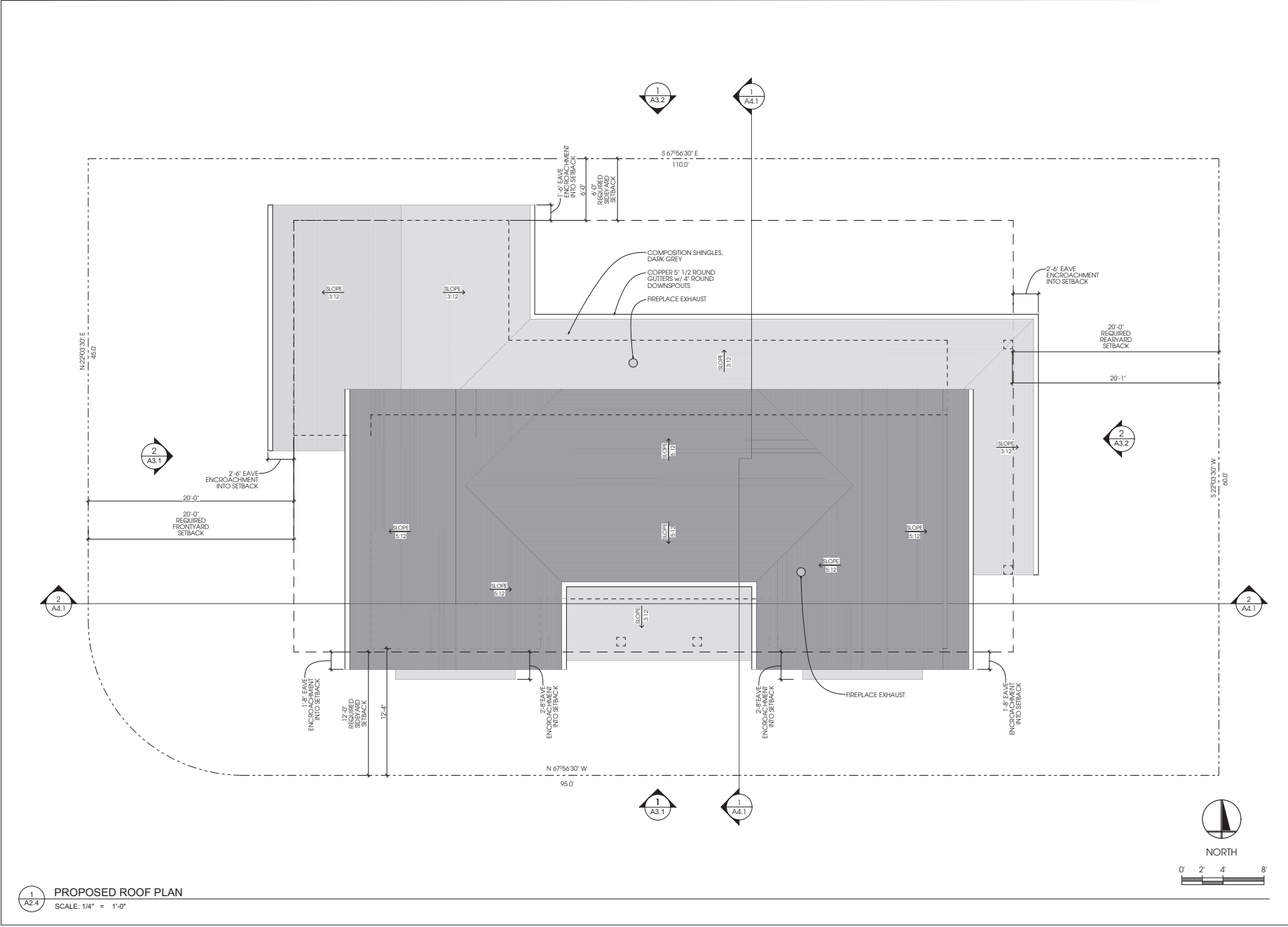


ISSUED:

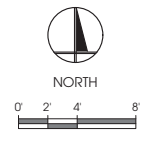
PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

ROOF PLAN
SCALE: 1/4" = 1'-0"
JOB NO.: 202128

A2.4



1
A2.4
PROPOSED ROOF PLAN
SCALE: 1/4" = 1'-0"





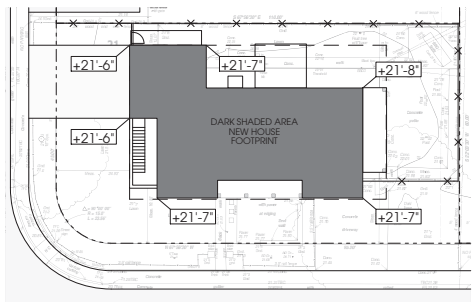
ISSUED:

PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

FLOOR AREA, BUILDING COVERAGE, & AVERAGE GRADE DIAGRAMS
SCALE: AS NOTED

JOB NO: 202128

A2.5



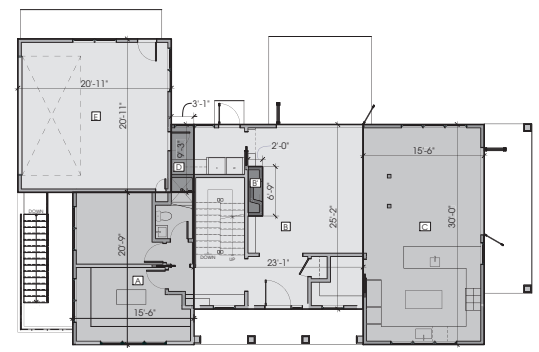
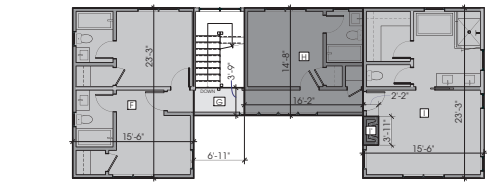
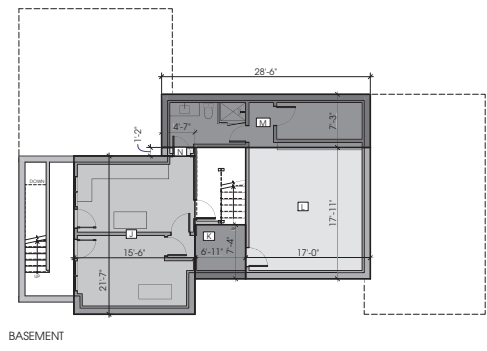
LEGEND

+21'-7"	GRADE @ BUILDING CORNER
---------	-------------------------

AVERAGE GRADE CALCULATION

LOWEST GRADE:	21.5
HIGHEST GRADE:	21.7
AVERAGE GRADE:	$\frac{21.5+21.7}{2} = 21.6$ (21'-7")

AVERAGE NATURAL GRADE DIAGRAM
SCALE: 1/16" = 1'-0"



BUILDING COVERAGE CALCULATIONS

TAG	SIZE	AREA
—	SEE 1/A2.5	1,819.77 S.F.
N	23'-1" x 4'-7"	106.87 S.F.
O	6'-4" x 23'-1"	152.35 S.F.
TOTAL		2,078.99 S.F.

FIRST FLOOR CALCULATIONS

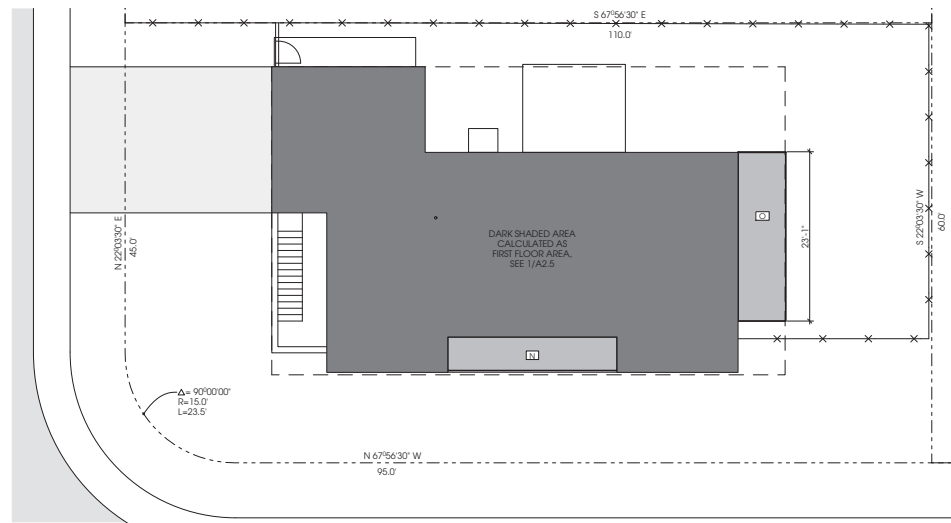
TAG	SIZE	AREA
A	15'-4" x 20'-0"	321.63 S.F.
B	23'-1" x 25'-2"	580.78 S.F.
B'	23'-1" x 25'-2"	-13.50 S.F. FIREPLACE SUBTRACTED
C	15'-4" x 30'-0"	465.00 S.F.
D	3'-1" x 9'-3"	28.49 S.F.
E	20'-11" x 20'-11"	437.37 S.F. GARAGE AREA
TOTAL		1,819.77 S.F.

SECOND FLOOR CALCULATIONS

TAG	SIZE	AREA
F	15'-4" x 23'-3"	350.38 S.F.
G	6'-11" x 3'-0"	25.91 S.F.
H	16'-2" x 14'-8"	237.00 S.F.
I	15'-4" x 23'-3"	351.15 S.F.
F'	2'-2" x 3'-11"	-8.45 S.F. FIREPLACE SUBTRACTED
TOTAL		975.99 S.F.

BASEMENT FLOOR CALCULATIONS

TAG	SIZE	AREA
J	15'-6" x 21'-7"	334.58 S.F.
K	6'-11" x 7'-4"	50.70 S.F.
L	17'-0" x 17'-11"	304.58 S.F.
M	28'-4" x 7'-3"	208.83 S.F.
N	4'-7" x 1'-2"	5.32 S.F.
TOTAL		901.77 S.F.



BUILDING COVERAGE DIAGRAM
SCALE: 1/8" = 1'-0"

FLOOR AREA DIAGRAMS
SCALE: 1/8" = 1'-0"

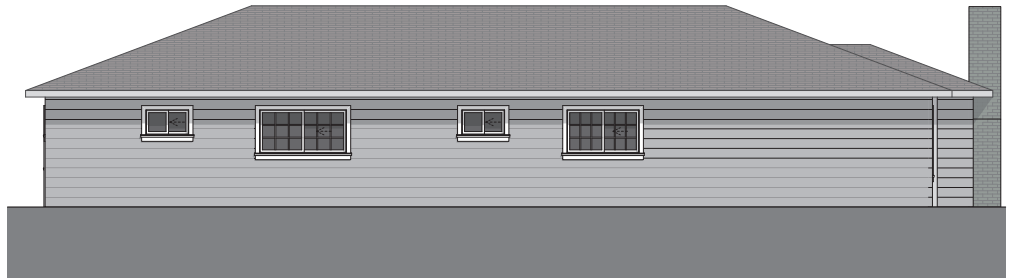


ISSUED:

PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

EXISTING EXTERIOR ELEVATIONS
SCALE: 1/4" = 1'-0"
JOB NO.: 202128

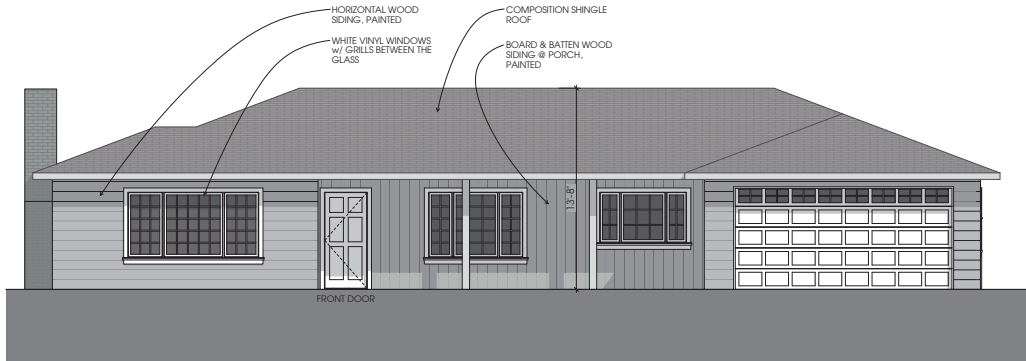
A3.0



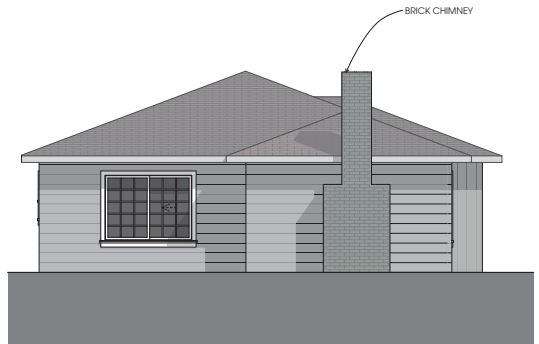
3
A3.0
EXISTING NORTH ELEVATION
SCALE: 1/4" = 1'-0"



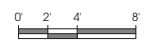
4
A3.0
EXISTING EAST ELEVATION
SCALE: 1/4" = 1'-0"



1
A3.0
EXISTING SOUTH ELEVATION
SCALE: 1/4" = 1'-0"



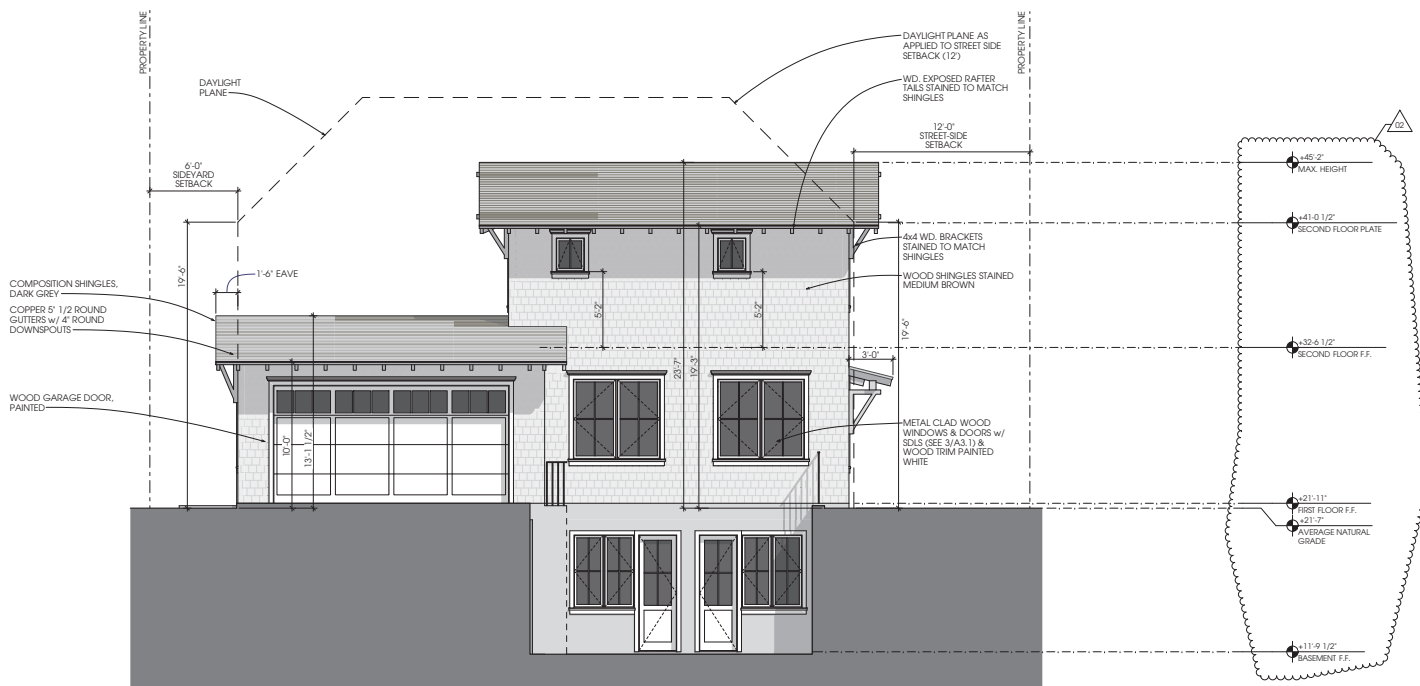
2
A3.0
EXISTING WEST ELEVATION
SCALE: 1/4" = 1'-0"



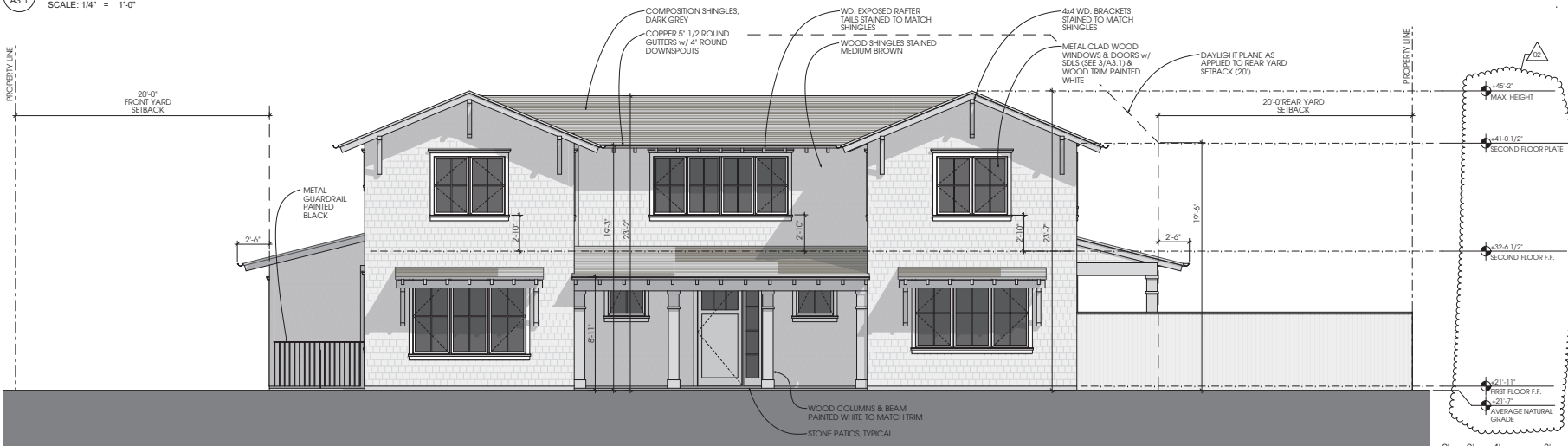


SIMULATED DIVIDED LITE WITH SPACER BAR (SDL)
 SDL bars are combined with spacer bars installed between the glass, creating the feel of Authentic Divided Lites.

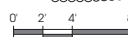
3 A3.1 **SIMULATED DIVIDED LITE**
 NOT TO SCALE



2 A3.1 **WEST ELEVATION**
 SCALE: 1/4" = 1'-0"



1 A3.1 **SOUTH ELEVATION**
 SCALE: 1/4" = 1'-0"



SCHNEIDER
 DESIGN
 ASSOCIATES
 514 23rd AVENUE
 SAN FRANCISCO,
 CALIFORNIA, 94121
 415.845.5472

A.P.N. : 055-292-370
MCGARAGHAN RESIDENCE
 103 DUNSMUIR WAY
 MENLO PARK, CA 94025



ISSUED:

PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

PROPOSED EXTERIOR
 ELEVATIONS
 SCALE: 1/4" = 1'-0"
 JOB NO: 202128

A3.1



SCHNEIDER
DESIGN
ASSOCIATES

514 23rd AVENUE
SAN FRANCISCO, CALIFORNIA, 94121
415.845.5472

A.P.N. : 055-292-370

MCGARAGHAN RESIDENCE

103 DUNSMUIR WAY
MENLO PARK, CA 94025



ISSUED:

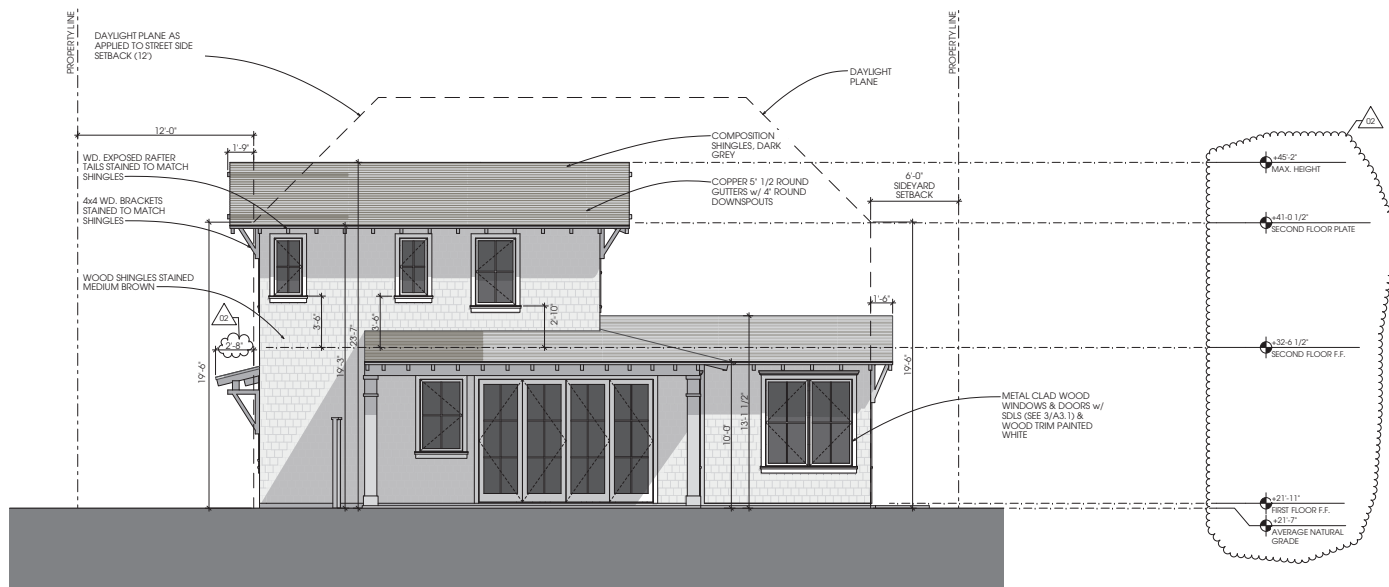
PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

PROPOSED EXTERIOR ELEVATIONS

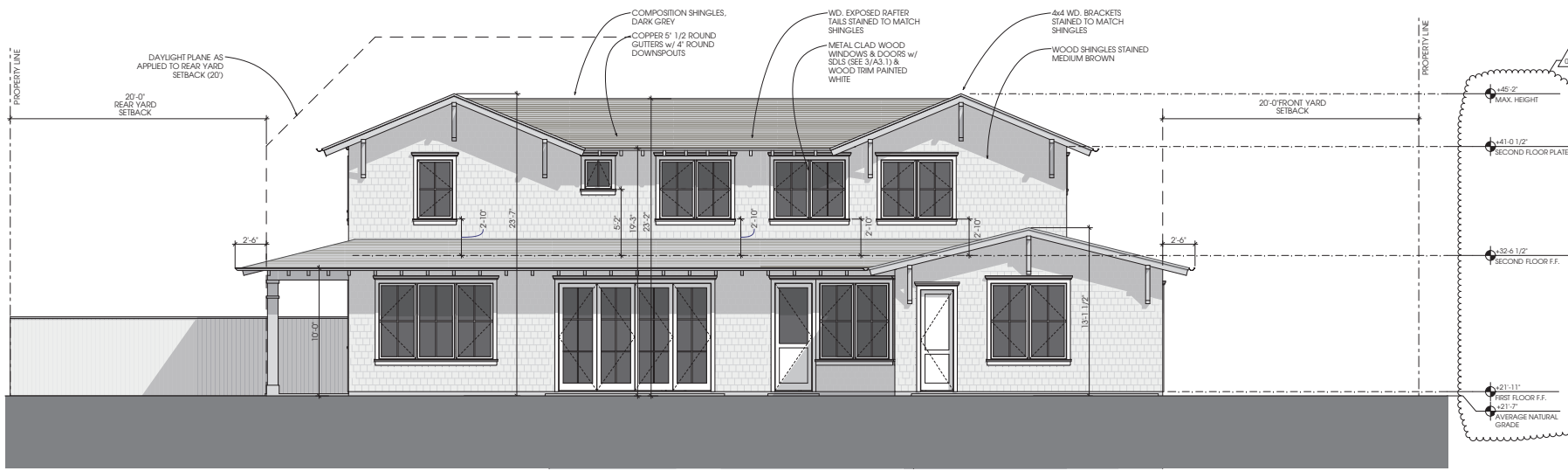
SCALE: 1/4" = 1'-0"

JOB NO.: 202128

A3.2



2 EAST ELEVATION
SCALE: 1/4" = 1'-0"



1 NORTH ELEVATION
SCALE: 1/4" = 1'-0"



SCHNEIDER
DESIGN
ASSOCIATES

514 23rd AVENUE
SAN FRANCISCO,
CALIFORNIA, 94121
415.845.5472

A.P.N. : 055-292-370

MCGARAGHAN RESIDENCE

103 DUNSMUIR WAY
MENLO PARK, CA 94025



ISSUED:

PLANNING SUBMITTAL	07.01.22
PLANNING RESPONSES	09.22.22
PLANNING RESPONSES	12.15.22

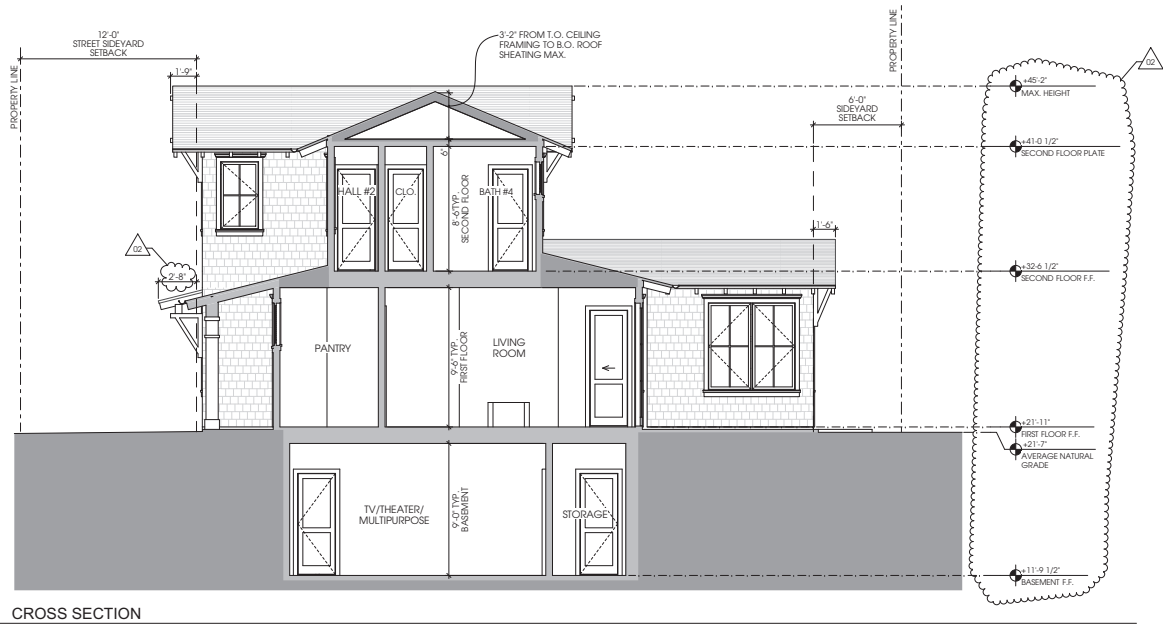
SCALE:

JOB NO.: 202128

A4.1



2
A4.1
LONGITUDINAL SECTION
SCALE: 1/4" = 1'-0"



1
A4.1
CROSS SECTION
SCALE: 1/4" = 1'-0"



1 FRONT RENDERING
A5.1 NOT TO SCALE



2 CORNER RENDERING
A5.1 NOT TO SCALE



SCHNEIDER
DESIGN
ASSOCIATES

514 23rd AVENUE
SAN FRANCISCO,
CALIFORNIA, 94121
415.845.5472

A.P.N. : 055-292-370
MCGARAGHAN RESIDENCE
103 DUNSMUIR WAY
MENLO PARK, CA 94025



ISSUED:

PLANNING SUBMITTAL	07.01.22
△ PLANNING RESPONSES	09.22.22
△ PLANNING RESPONSES	12.15.22

PERSPECTIVES
SCALE: N.T.S.
JOB NO: 202128

A5.1

PROJECT DESCRIPTION

103 DUNSMUIR WAY, MENLO PARK, CA

The current owners of this property have lived at 103 Dunsmuir for 14 years and love the neighborhood. The current home is a single story and their family of five has outgrown the home. Rather than leave their beloved neighborhood they have decided to build a new home closely suited to their family size with the intention of making this their "forever home".

The scope of work is to demolish an existing single story home and construct a new two-story home over a partial basement. The massing of the home has been carefully considered to enhance the neighborhood aesthetic by breaking the home into distinct smaller elements that keep the scale of the home in character with the neighborhood. The home is also designed to enhance the life of the street by incorporating a front porch as a focal point of the front facade. This outdoor space gives the owners a comfortable place to relax and enjoy their front yard while interacting with neighbors and activating the street. This element has been incorporated into the design because the current house has a front porch and the owners use their front yard and porch often and enjoy their interactions with neighbors.

The style of the home is California Craftsman with a shingled exterior and simple detailing to include window and door trim with crown molding and brackets to support roof overhangs. In keeping with this simple yet elegant architectural style the home incorporates exposed rafter tails and detailed front porch posts and beams.

The design process looked at the site layout and neighborhood pattern and interspersed lower, single story elements such as the front porch roof, wrap-around single story rear roof, covered side porch, and garage to bring the scale of the home down to the human scale on all sides. The second story of the home reads through on the front on either side of the front porch with articulated gable-end roofs that are in scale with the home and the neighborhood fabric.

The design seeks a balance between maintaining the setback from the street on the South facade and preserving the North neighbor's access to southern exposure. By keeping the majority of the rear of the home well away from the rear setback and stepping the second story away even more than the first floor we have been very careful to preserve the privacy and access to daylight for the North neighbor. On the South facade we have articulated the front of the home by stepping the center section of the home back to incorporate a protected front porch. The second story over this front porch is set back from the front of the home even further to articulate the front elevation and break it into smaller elements that fit with the character of the neighborhood.

The owners of the home have done extensive outreach to the neighbors, focusing their efforts on those that will be most effected by the project, the immediate neighbors. Please see attached the map showing the extent of the neighbor outreach done to date. Please also see the three letters of approval provided by three of the neighbors that are directly impacted by the project. To date the owners have not received any negative feedback regarding the project.

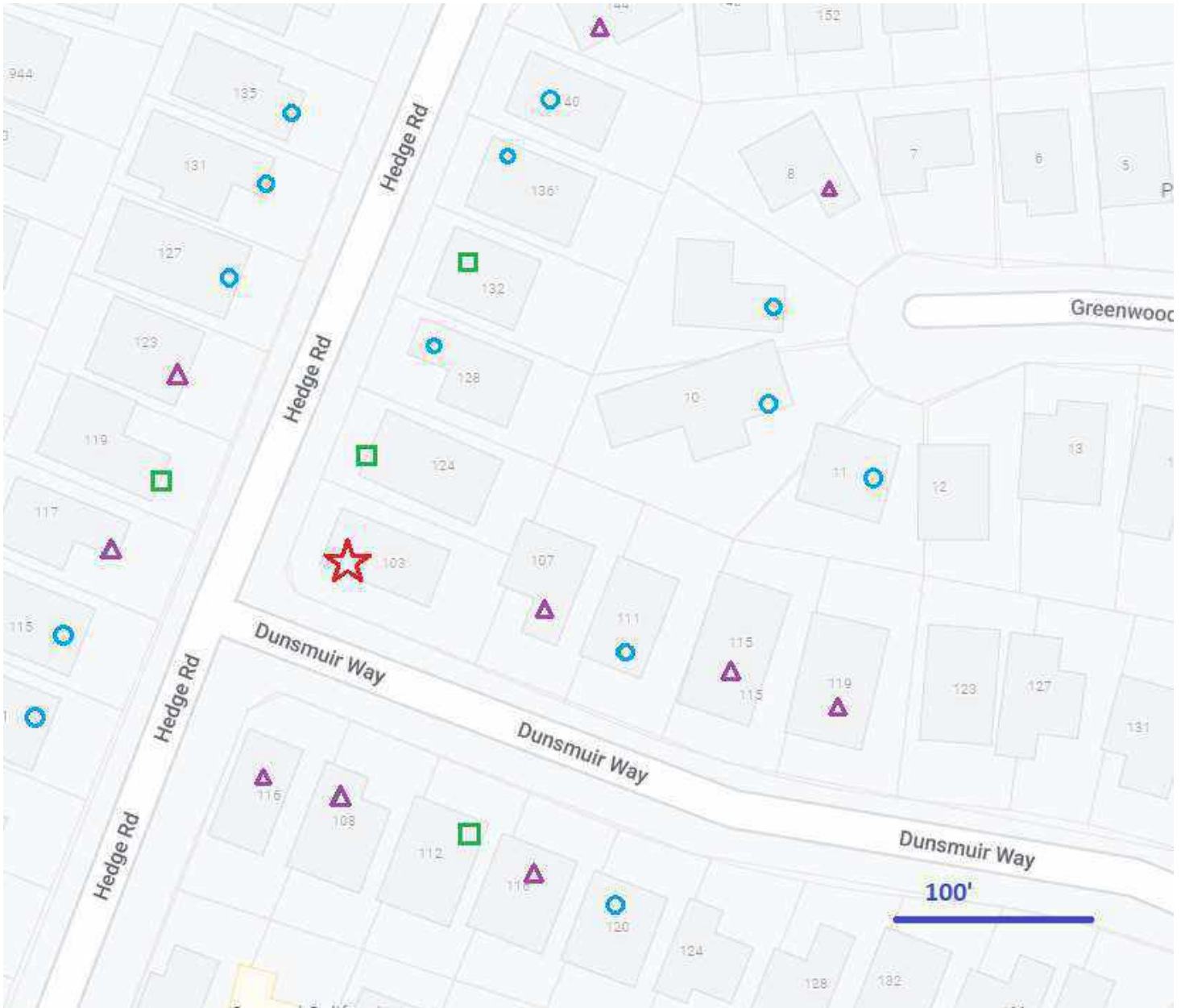
PROJECT DESCRIPTION (Continued)

103 DUNSMUIR WAY, MENLO PARK, CA

Overall we feel that the careful siting and design of this new home will make it feel like it belongs and fits seamlessly into the neighborhood. The intent of the design was, from the beginning, to be sensitive to the character of the neighborhood, respect the needs of the immediate neighbors in terms of their privacy and access to light, and closely fit the daily needs of its inhabitants.

The owners and architect feel strongly that this project, once complete, will blend so well with the fabric of the neighborhood that it will seem that it was "always there".

103 Dunsmuir
Project Plan Neighborhood Outreach Map*



-  **Subject Property**
-  **Delivered Flyer**
-  **Discussed Project In Person**
-  **Reviewed Detailed Plans**

* In addition to the outreach shown above, we have discussed our plans with many more of our neighbors on an informal basis, and have received positive feedback all around.

Dear Neighbors,

John and I moved to Suburban Park almost 14 years ago. Since we've lived here, our family has expanded from the two of us and a baby to the two of us and three tweens/teens(!). Thank you for being amazing neighbors to our family throughout these years. We all love this neighborhood and our neighbors, and we're excited to start on the adventure of expanding our house to accommodate our family.

We're planning to rebuild our home at 103 Dunsmuir Way and replace it with a two-story home in the California Shingle/Arts and Crafts style (you can see a rendering of the proposed design below). We are aware of the impact that constructing a new house can have on the neighborhood - both the benefits to home values in the long run but also the hassle of construction in the shorter term. We intend to optimize the first while minimizing the latter. If you have any questions, we would be happy to discuss them and share more details of our plans with you.

Kindest regards,

Amy, John, Paige, Fiona and PJ

amy@bricklily.com john@mcgaraghan.com

Mobile: 650-269-1472 (Amy) or 650-353-6128 (John)



September 3, 2022

City of Menlo Park Planning Commission

My wife and I, Carol and Jerry Marsh, live at 124 Hedge Rd. which is the adjoining property of 103 Dunsmuir Wy. We have lived in Suburban Park for fifty two years. We have known the family of Amy and John McGaraghan for approximately 14 years and consider the family to be outstanding neighbors.

We have had an opportunity to review and discuss the proposed remodel with Amy and John of their home located at 103 Dunsmuir Wy. It is my opinion the proposed remodel will be an asset to property value in Suburban Park and the surrounding community.

Carol and I approve of the proposed remodel and support moving the project forward as soon as possible.

Thank You,

Jerry & Carol Marsh

Juan Cueva
119 Hedge Road
Menlo Park, CA 94025
August 8, 2022

City of Menlo Park Planning Commission
701 Laurel Street
Menlo Park, CA 94025

Dear City of Menlo Park Planning Commission:

I am writing to voice my support of the McGaraghan rebuilding project at 103 Dunsmuir Way, Menlo Park.

I have lived at 119 Hedge Road, directly across the street from 103 Dunsmuir, since 2009. John and Amy McGaraghan reviewed their construction plans, the short- and long-term impacts, and the timeline for the project. I also had the opportunity to discuss the project with them. The McGaraghan rebuilding project has my full support. I can be reached at jgcueva@gmail.com or (650) 814-3559 if additional information is required.

Sincerely,

Juan Cueva

From: Jon Wright <wrightjon81@gmail.com>
Date: September 22, 2022 at 11:58:34 AM PDT
To: cchan@menlopark.org
Cc: "Annabel R. Chang" <annabel.chang@gmail.com>
Subject: In support of 103 Dunsmuir Way application

Hi Calvin -

We live at 112 Dunsmuir Way across from 103 Dunsmuir Way and are writing in strong support of their current application and plans.

The applicants are beloved members of the neighborhood who are gracious with both their time and space. Their current house is a welcome point when entering Suburban Park and has served as a natural convening point for neighbors, walkers and playing children.

It is clear they have taken this responsibility seriously with the current design as it compliments and enhances the character of the neighborhood. Notably, it will be a warm and beautiful home that welcomes people into our community as they turn on to Hedge from Bay Road.

We are aware that the applicant is applying for a variance on the Dunsmuir side for the daylight plane and are supportive. It will have no real impact whatsoever.

We are so happy for this family and eager to see their "forever home," be built.

Thank you for your consideration,

Jon Wright and Annabel Chang

Menlo Park Planning Department and Commissioners,

Amy and I have spoken at length with our architect about our variance application and options. Upon reflection, we feel strongly that the existing design is the best for the space, and that conforming designs we have considered will be more detrimental to the North neighbor's access to light and air. At this time, we would prefer to focus the Planning Commission meeting on our proposed design and the merits of our variance application. Because the conforming options we have explored would cast shadows on our neighbor's home we prefer not request a conforming option to the Planning Commission at this time. As much as Amy and I would like to expedite approval and move on to the next phase of the project, we think that it is more important to achieve the best outcome for the neighborhood.

To simplify our position, the language of MPMC 16.82.340(a) you shared with us expresses the spirit and intent of the variance process very clearly (i.e. to grant a variance where, under the specific circumstances "the literal enforcement of the requirements of [the] title would cause undue hardship unnecessary to carry out the spirit and purpose of [the] title"). Understanding that there is a high bar to achieve a variance, we believe that redesigning the project to conform would be unnecessarily burdensome, without any benefit of the type the daylight plane requirements are intended to address. In fact, the most readily available conforming designs would cause our structure to cast significant shade on our North neighbor's sun room, without any corresponding benefit to the street.

To help clarify and summarize the support for our variance we have put together the following outline of our responses to the Purpose and Findings for the application.

Thank you for your consideration.

John and Amy McGaraghan
103 Dunsmuir Way

Purpose. The purpose of the variance is to allow variation from the strict application of the terms of this title where, by reason of the exceptional narrowness, shallowness or unusual shape of a specific piece of property, or by reason of exceptional topographic conditions or other extraordinary situation or condition of such piece of property, or by reason of the use or development of property immediately adjoining the piece of property in question, the literal enforcement of the requirements of this title would cause undue hardship unnecessary to carry out the spirit and purpose of this title.

- The request addresses the specifics of the lot (i.e. the "side" in question is a south-facing street side) where the requested variance would not block neighbors' access to light, but a conforming design would.
- The immediately adjoining property on the affected side is the Dunsmuir Way right of way, not a neighboring lot, and in any event faces south (so the daylight will always come from the opposite direction of the structure).

- The immediately adjoining property on the North side is a single story residence, situated at the 6' setback, with a south facing, ground level sun room. The residents have lived there for over 50 years, are retired, and spend significant time at home.
- The spirit and purpose of the requirement is to protect access to light for lots, not street rights-of-way.
- The North neighbor's access to light from the South was not an afterthought but an integral part of the design intent of the house at 103 Dunsmuir. We made this very clear to our architect from the very start of our design process.
- We have considered a conforming design that meets both side daylight plane requirements, and conducted a "light study". As shown in our variance materials, implementing the conforming design will burden our neighbor with a meaningful reduction to light and sky on the south side of their house – literally casting shadows on their sun room.
- Alternatively, requiring a complete redesign that conforms and also adequately protects our North neighbor would be unnecessarily burdensome on us, and not provide any benefit of the type the daylight plane requirements are designed to protect.
- Having already put significant time and effort and money into this design and variance process (specifically for the purpose of protecting the North neighbor's access to light and sky), adding additional time, effort, and expense for an outcome that does not benefit neighbors, and would likely cause harm, is undue.

FINDING #1

That a hardship peculiar to the property and not created by any act of the owner exists. In this context, personal, family or financial difficulties, loss of prospective profits and neighboring violations are not hardships justifying a variance. Further, a previous variance can never have set a precedent, for each case must be considered only on its individual merits

- 103 Dunsmuir is a corner lot, situated with a neighbor to the North and a street to the South.
- Since the "side" of the lot faces south toward the street, there is no neighbor in that direction to protect, and since the sunlight comes from the south, the structure cannot block access to light on or across the street.
- This situation is rare - of +/-500 lots in Suburban Park/Lorelei Manor/Floor Park Triangle only eight (roughly 1.5%) have a similar situation where a corner lot has a South-facing street "side" and a lot to the North.
- 103 Dunsmuir is substandard width and narrower than several of the 8 similarly situated lots (at 60' rather than 65', the deficiency is greater than the amount of the requested variance).
- The neighbor directly to the North has a "sun room" situated at ground level on its south side, which we are trying to protect – a conforming design would cast significant shade on that room.

FINDING #2

That such variance is necessary for the preservation and enjoyment of substantial property rights possessed by other conforming property in the same vicinity and that a variance, if granted, would not constitute a special privilege of the recipient not enjoyed by his/her neighbors

- The additional street-side setbacks are intended to create a consistent street impression along the street side, but unlike the other lots that front on the street do not have the same daylight plane requirements on the fronts of their homes.
- In this way, our street side is treated like a “front”, whereas other homes’ fronts do not have daylight plane requirements, because the rationale of the daylight plane requirements is to protect the home on the adjacent lot. The application of the daylight plane to the street side prevents us from enjoying the same ability to choose how to mass and structure the front of our home that other neighbors have.
- An alternative conforming design would push the structure both North and East, and the variance preserves our ability to enjoy the same type of combination of back yard space and square footage as other similar properties that do not have a street-facing side.
- As applied in the traditional case (i.e. at the 6’ setback from the adjoining neighboring home), our design would get the benefit of the gable roof exceptions and would be permissible. The additional street side setback eliminates that right without any corresponding daylight benefit to the street.
- We have communicated extensively with neighbors throughout the process, including with regard to our approach to the design and the variance application. We have broad support from neighbors, many of whom have already reached out to the planning department directly. With regard to the variance, neighbors have generally expressed to us that they view it as a natural, rational adjustment of the rules to support their intent, rather than a special dispensation for us.
- 103 Dunsmuir is not gaining a privilege but is acting primarily to best protect their neighbors’ enjoyment of their lot, and prevent actual loss of light in their sun-room.

FINDING #3

That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property

- We have explored conforming designs – moving the structure back roughly 5 feet feet would put it inside both South and North side daylight planes – and the result would materially impair the North neighbor’s access to sun, as shown by our light study.
- The purpose of our variance request is to protect the North Neighbor’s supply of light and air, specifically to protect their south-facing sun room (which would be shaded by a conforming design).
- There is no detrimental effect on a neighbor to the South because there is no South neighbor - only a street right of way.
- There is no detrimental effect on the street because the street side setbacks and right of way already place the structure 20 feet back from the sidewalk, and the structure cannot cast a shadow to the south.
- The diminishing triangular shape of the incursions minimize any perceived impact from the street side (and, if on a neighbor-facing side, would be afforded the benefit of the gable roof exception).

FINDING #4

That the conditions upon which the requested variance is based would not be applicable, generally, to other property within the same zoning classification.

- Corner lots with a street-side facing south constitute only 1.5% of the homes in the Flood Triangle / Lorelei area.
- 103 Dunsmuir is substandard width (by more than the amount of the requested variance) and is narrower than some of the other corner lots with south facing street-sides
- Our North side neighbor that is at the 6' side setback, and that has a south facing, ground level sun room on that side of the house.

FINDING #5

That the condition upon which the requested variance is based is an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process.

- The situation where there is a street right of way to the South and a neighboring property to the North is unusual – it occurs in a very small portion of the lots in this area (see finding 1).
- The strict application of the zoning code as it relates to the daylight plane in this situation is detrimental to the North neighbors without protecting any other stakeholders (there is no immediate neighbor to the South)
- While the setback calculations in the code specifically address street-side lots (which address traffic sight lines around corners and consistency of building presentation along the street side), the daylight plane requirements only refer to setbacks, and it is not clear that the issue was considered or that this was the intended effect.
- While we are only asking for a variance in our case, we believe the zoning code could be improved by considering this situation and revising to more effectively enable thoughtful development and consistent application of the rules amongst neighbors, while still providing the protections for neighbors intended by the section.

VARIANCE FINDINGS

103 DUNSMUIR WAY, MENLO PARK, CA

The following findings must be made for the approval of a variance. Please see the findings and how we believe our project meets each one.

BACKGROUND

The property at 103 Dunsmuir Way is situated as shown in Figure 1. This configuration has a long street-side setback on its South side with a neighboring property directly to the North. This is important because the winter daylight comes from the South and is more readily blocked by the Southern neighbor. Because the street-side setback in the R-1-U zoning district is 12' while the interior-side setback is a minimum of 5' and is 6' in this particular case, the zoning code encourages the placement of the corner house closer to the neighboring property and further from the street. When the corner property is to the South of the neighboring property this sets up a situation where more daylight can potentially be blocked by the South neighbor than is desirable.

FINDING #1

That a hardship peculiar to the property and not created by any act of the owner exists. In this context, personal, family or financial difficulties, loss of prospective profits and neighboring violations are not hardships justifying a variance. Further, a previous variance can never have set a precedent, for each case must be considered only on its individual merits:

The property is one of +/-500 in the Suburban Park/Lorelei Manor/Floor Park Triangle. Within this same area there are (8) or 1.6% that have the situation described above and shown in Figure 1. This is shown in Figure 2. Because the particular situation of this lot is a minority case and the orientation of the lot and its relationship to its neighbors has not been created by any act of the owners Finding #1 is met.

FINDING #2

That such variance is necessary for the preservation and enjoyment of substantial property rights possessed by other conforming property in the same vicinity and that a variance, if granted, would not constitute a special privilege of the recipient not enjoyed by his/her neighbors;

Typically a side yard faces a neighboring property - this is the case in the vast majority of properties in this neighborhood. Because a majority of properties in this neighborhood have neighbors opposite their side yards it makes sense that the daylight plane be applied to these side yards. In the case of the South side of 103 Dunsmuir Way the "side yard" is actually the natural front of the house. The current house is oriented this way (see 1/A3.0 showing the existing front door on the South elevation). For the vast majority of properties in this neighborhood the front facade is not subject to a daylight plane limitation. Because the front of the vast majority of homes in the neighborhood do not have their front elevations

subject to a daylight plane restriction, tempering the side yard daylight plane on the South side of 103 Dunsmuir preserves the same property rights that other conforming properties enjoy.

To take this a step further it would make sense that the side-yard daylight plane be applied to what is the rear of 103 Dunsmuir as this is the second side of this property that abuts a neighbor and has a similar circumstance as a typical side-yard. Please see 1/A3.1 & 1/A3.2 showing the application of the daylight plane to the East side of 103 Dunsmuir way. This is the "rear" of the property but in actuality more closely aligns with the "side" orientation as there is a neighbor on this side. The current design keeps the new residence well clear of the daylight plane applied to this side of the property. Because of the special circumstances in the naming of the property lines for 103 Dunsmuir Way, allowing the daylight plane to be applied to what is the "rear" of the property instead of the "side" aligns perfectly with the interpretation of the zoning code for the vast majority of properties in the neighborhood therefore it does not constitute a special privilege. Because of this, Finding #2 is met.

FINDING #3

That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property;

By allowing what is effectively the "front" of the property to minimally penetrate the daylight plane restriction as described in Finding #2, we can place the proposed home on the property so that it is further to the South by +/-4'. This keeps the project out of the 12' side-yard setback per the zoning code with a minor incursion into the daylight plane on this side. By placing the home +/-4' further South, the shading that the proposed project does to the North neighbor is actually significantly reduced in the winter months due to the property's relative orientations. This is shown in Figure 3A-3D. The wintertime shading due to the proposed project is significantly better with the variance granted. Because of this the North neighbors' enjoyment of their property is actually better with the variance granted as they will receive more direct light in the winter months.

In an effort to minimize our incursion into the daylight plane the design of the home has been modified after initial submittal with the ceiling heights on each of the two floors reduced by 6" each for a total height reduction of 12". This allows the design to retain its craftsman style while limiting its protrusion into the daylight plane.

On the street side the placement of the house +/-4' closer to the side-yard setback does not have a meaningful impact on the supply of light and air as the right of way is 50' wide and the setback is a further 12'. As designed with the variance the front of the house is +/-17' from the back of the sidewalk. In addition to the wide right of way and setback is the fact that this "side" of the property faces South so there is never a time when the proposed project shades the right of way.

Because of the fact that granting the variance makes the neighbor's light and air situation materially better and does not materially effect the light and air at the right of way Finding #3 is met.

FINDING #4

That the conditions upon which the requested variance is based would not be applicable, generally, to other property within the same zoning classification.

Similar to the discussion for Finding #1 and referencing Figure #1 & Figure #2, this particular situation applies to only 1.6% of the +/-500 properties in the neighborhood. Because of this and the description given in the "Background" and 'Finding #1', Finding #4 is met.

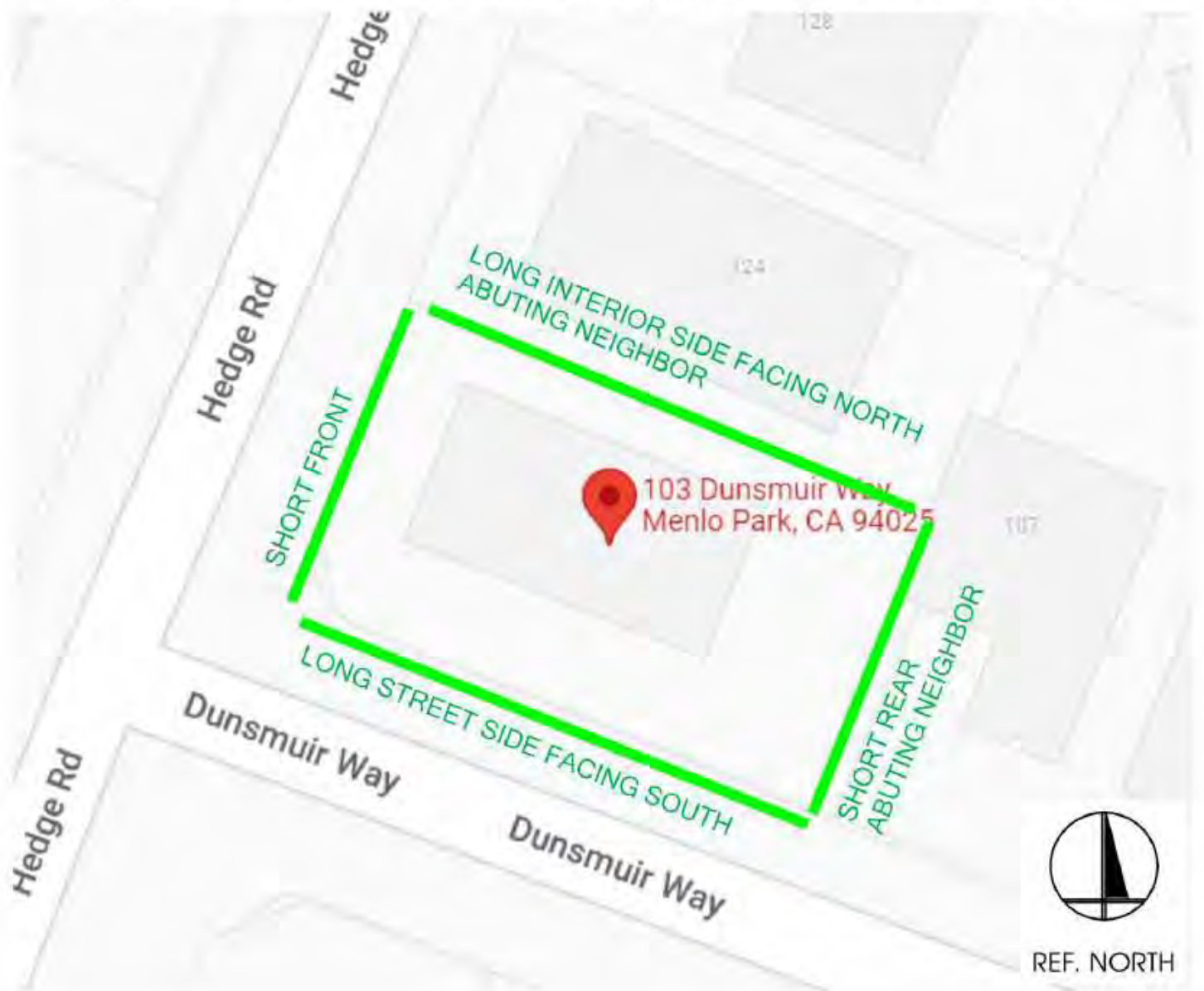
FINDING #5

That the condition upon which the requested variance is based is an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process.

Because this particular case as it relates to the property orientation relative to other properties and the orientation of the property in relation to the sun angles as described in the discussion of Finding #3, this specific situation is a very unusual factor that could not have been anticipated in detail. Because of this Finding #5 is met.

FIGURE 1

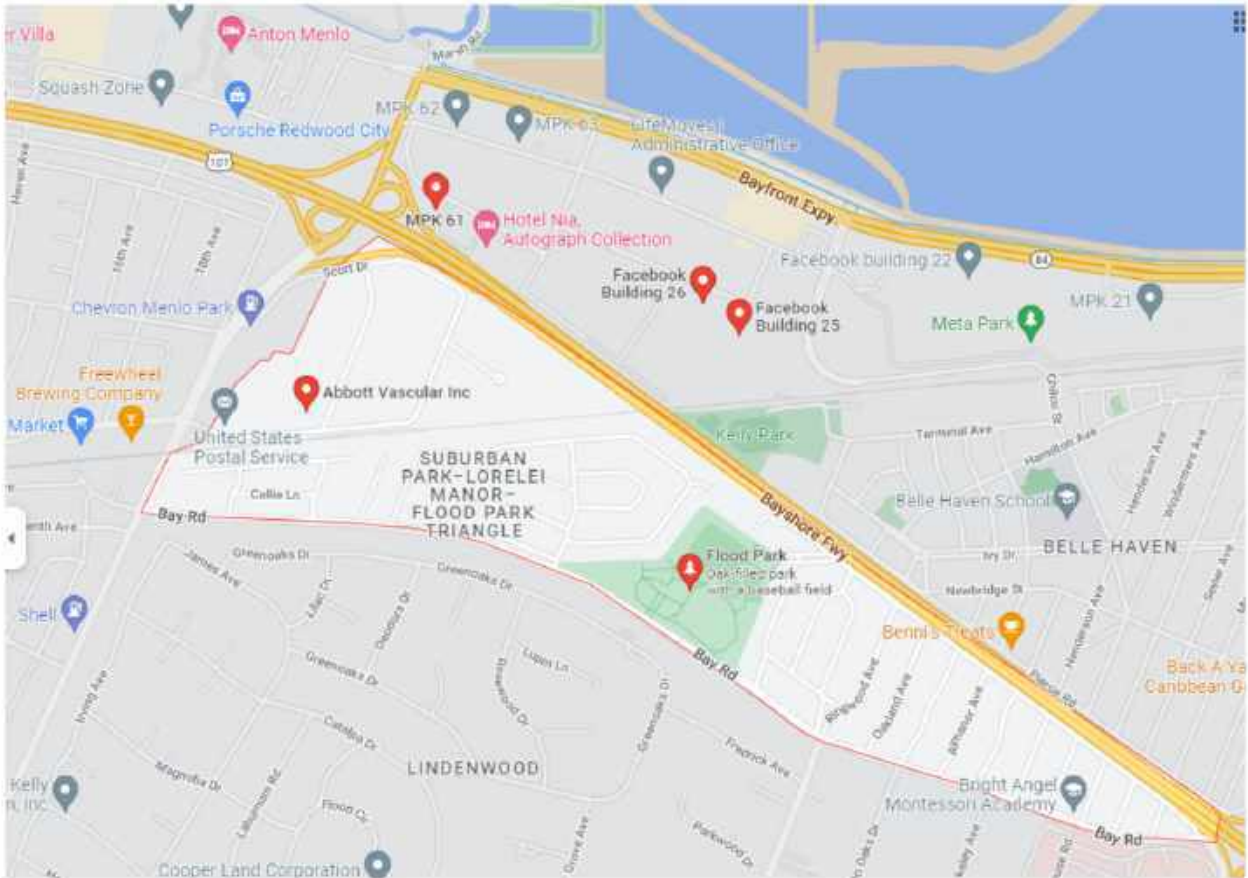
UNIQUE CHARACTERISTICS OF SUBJECT PROPERTY SITING - 103 DUNSMUIR WAY



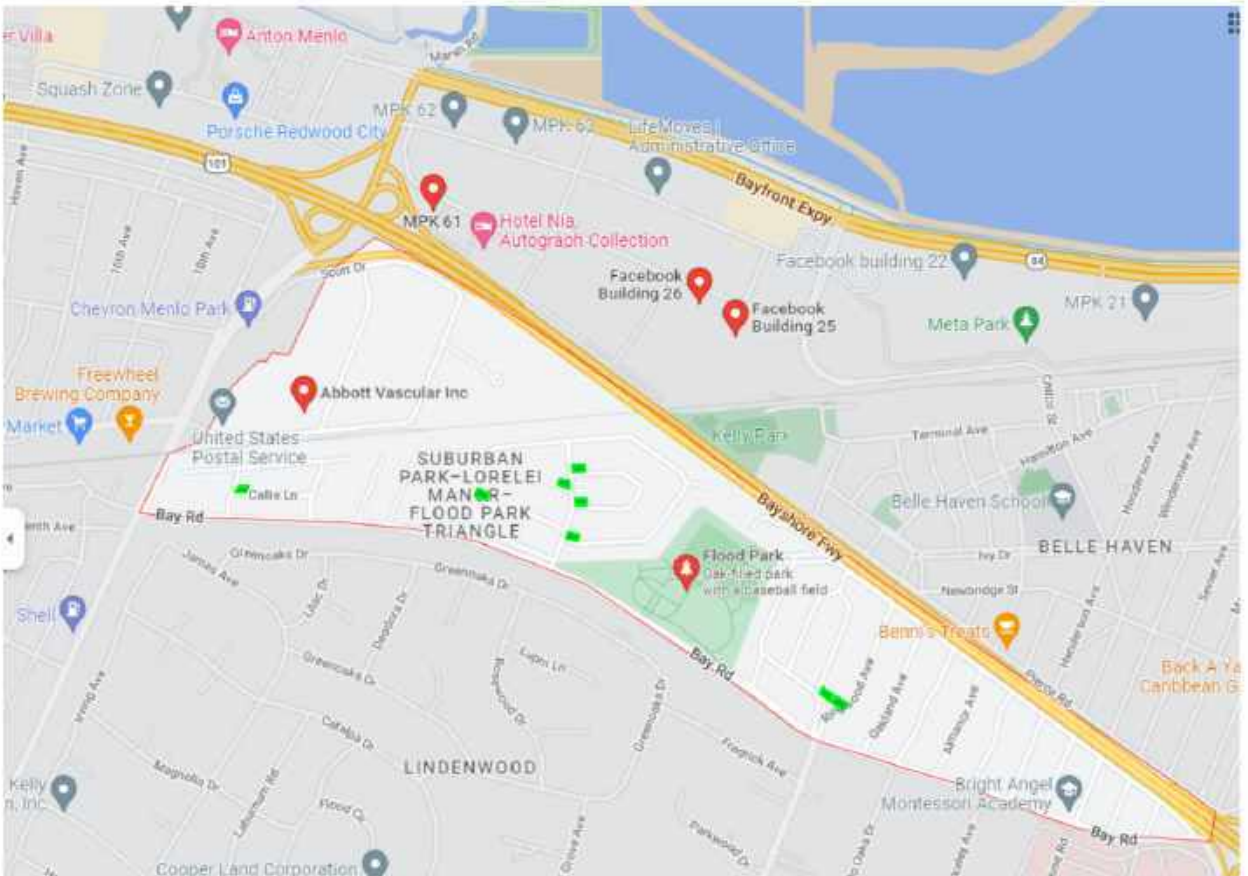
- THE SUBJECT PROPERTY IS SITUATED ON A CORNER WITH ITS FRONT YARD DETERMINED TO BE ITS SHORT SIDE
- THE SUBJECT PROPERTY HAS A LONG SIDE YARD FACING A STREET THAT IS TO THE SOUTH OF THE PROPERTY
- THE SUBJECT PROPERTY HAS A NEIGHBORING PROPERTY TO THE NORTH THAT SHARES A LONG INTERNAL SIDE YARD
- THE SUBJECT PROPERTY HAS WHAT IS DETERMINED TO BE ITS REAR YARD FACING A NEIGHBORING PROPERTY

FIGURE 2

AREA MAP SHOWING SMALL NUMBER AND % OF PROPERTIES WITH SIMILAR SITING



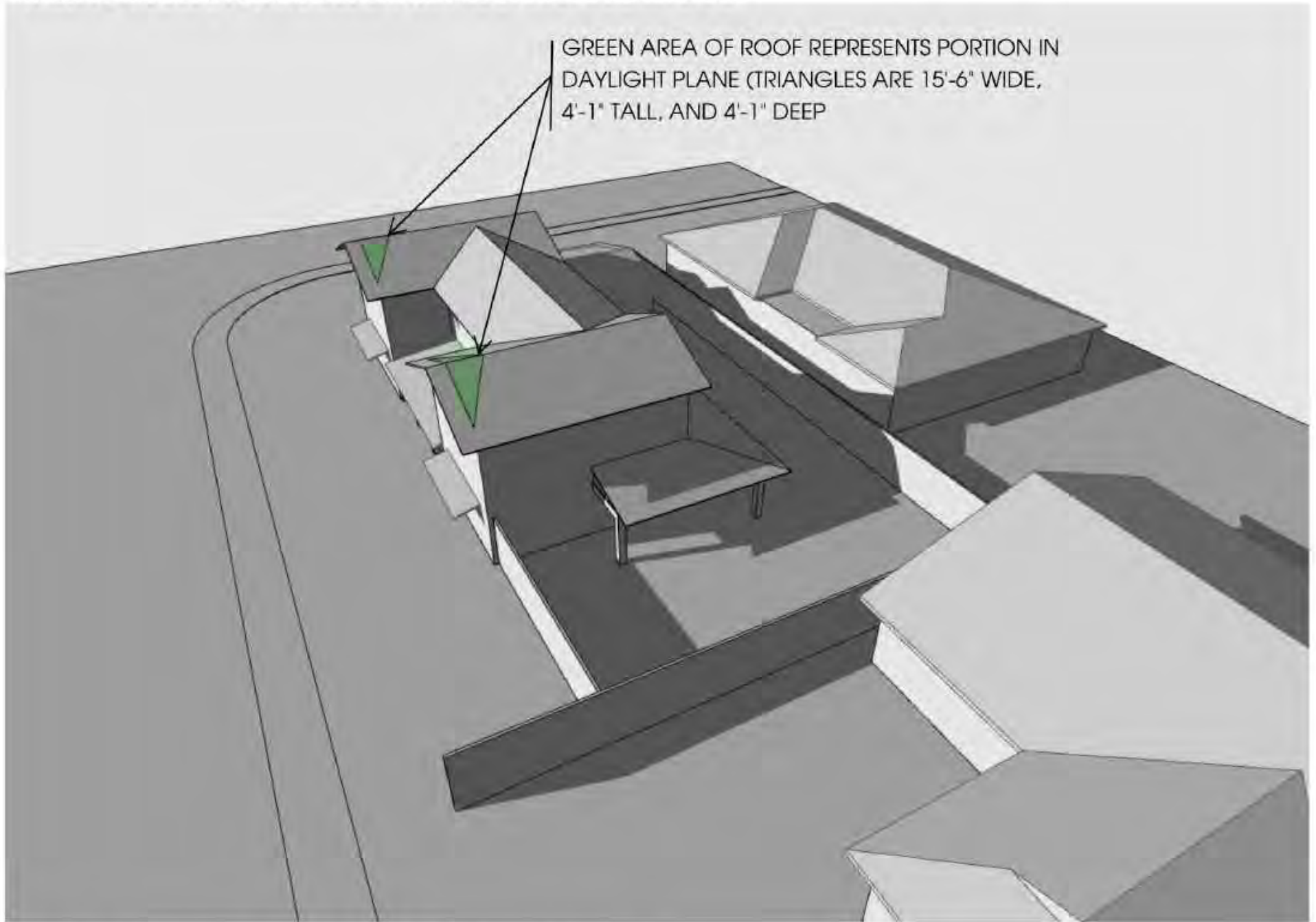
+/-500 RESIDENTIAL PROPERTIES IN SUBURBAN PARK/LORELEI MANOR/FLOOD PARK TRIANGLE



(8) PROPERTIES (1.6% INCLUDING SUBJECT PROPERTY) MARKED IN GREEN HAVE A LAYOUT SIMILAR TO THE SUBJECT PROPERTY AS SHOWN IN FIGURE 1. THESE ARE THE ONLY PROPERTIES WHERE LOW WINTER LIGHT IS MORE READILY BLOCKED FROM THE NORTH NEIGHBOR IF THE SIDE YARD DAYLIGHT PLANE IS APPLIED TO THE STREET SIDE.

FIGURE #3A

MASSING MODEL SHOWING DESIGN WITH ROOF IN DAYLIGHT PLANE



GREEN AREA OF ROOF REPRESENTS PORTION IN DAYLIGHT PLANE (TRIANGLES ARE 15'-6" WIDE, 4'-1" TALL, AND 4'-1" DEEP)

EXISTING DESIGN - WITH DAYLIGHT PLANE VARIANCE

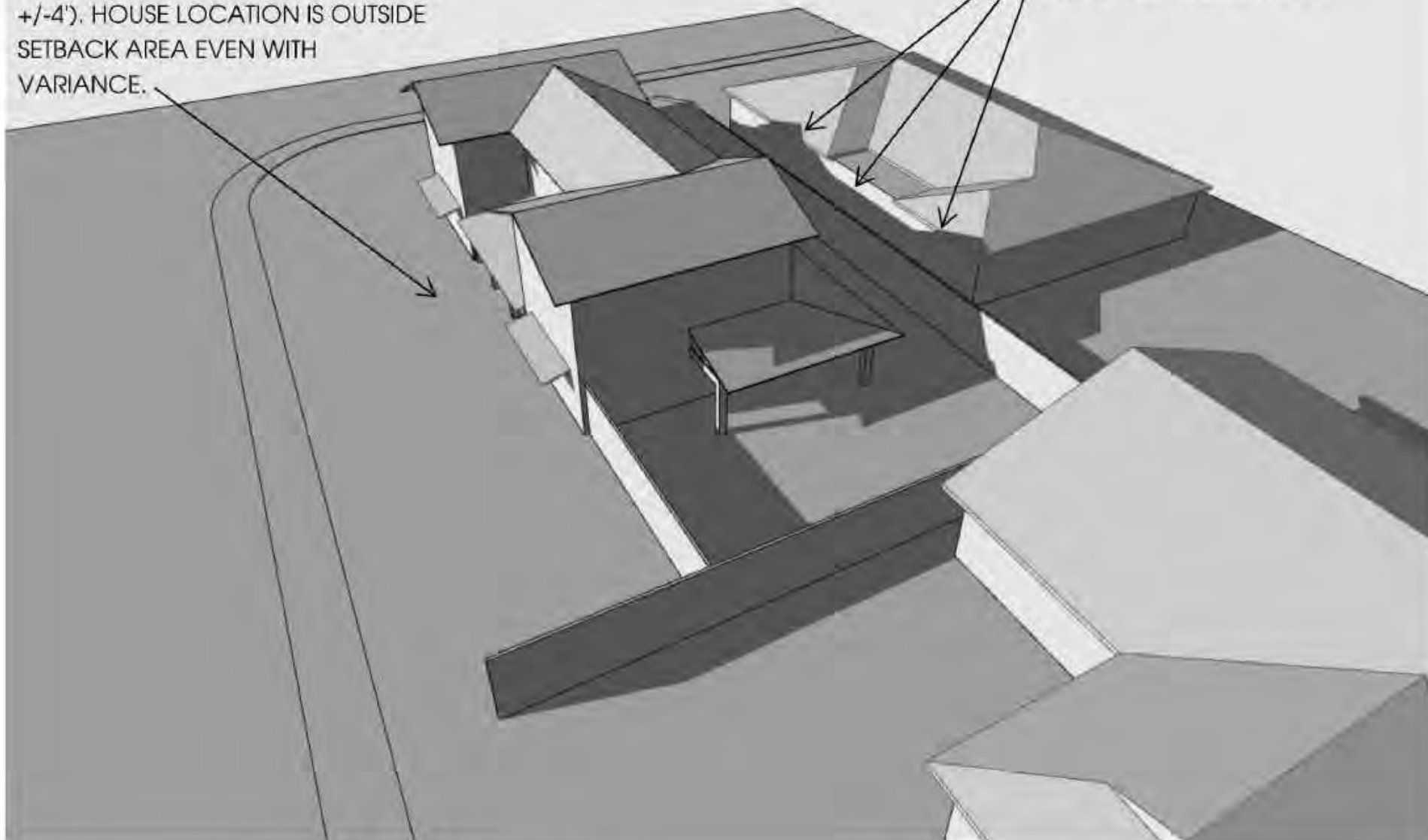
40 DECEMBER 1, 2:00pm

FIGURE #3B

MASSING MODEL SHOWING DESIGN WITH PROJECT PUSHED NORTH ON LOT TO AVOID STREETSIDE DAYLIGHT PLANE

NOTE NO MEANINGFUL CHANGE TO ACCESS TO LIGHT AND OPEN SPACE ON STREET SIDE (ROW IS 50' WIDE, SETBACK IS 12', & HOUSE LOCATION CHANGES BY +/-4'). HOUSE LOCATION IS OUTSIDE SETBACK AREA EVEN WITH VARIANCE.

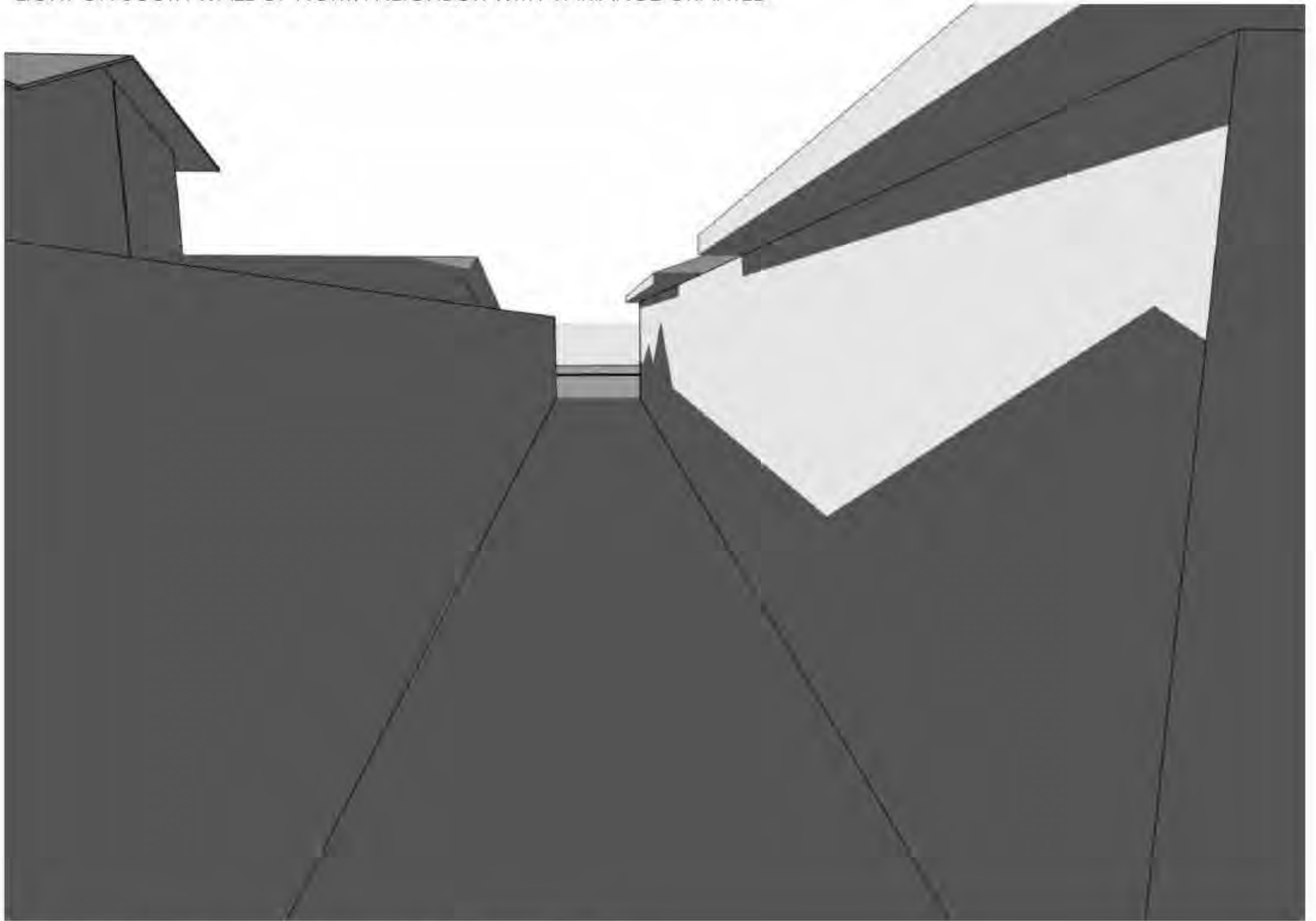
NOTE SIGNIFICANTLY MORE SHADING ON NORTH NEIGHBOR'S SOUTH SIDE



DESIGN WITHOUT DAYLIGHT PLANE VARIANCE
DECEMBER 1, 2:00pm

FIGURE #3C

LIGHT ON SOUTH WALL OF NORTH NEIGHBOR WITH VARIANCE GRANTED



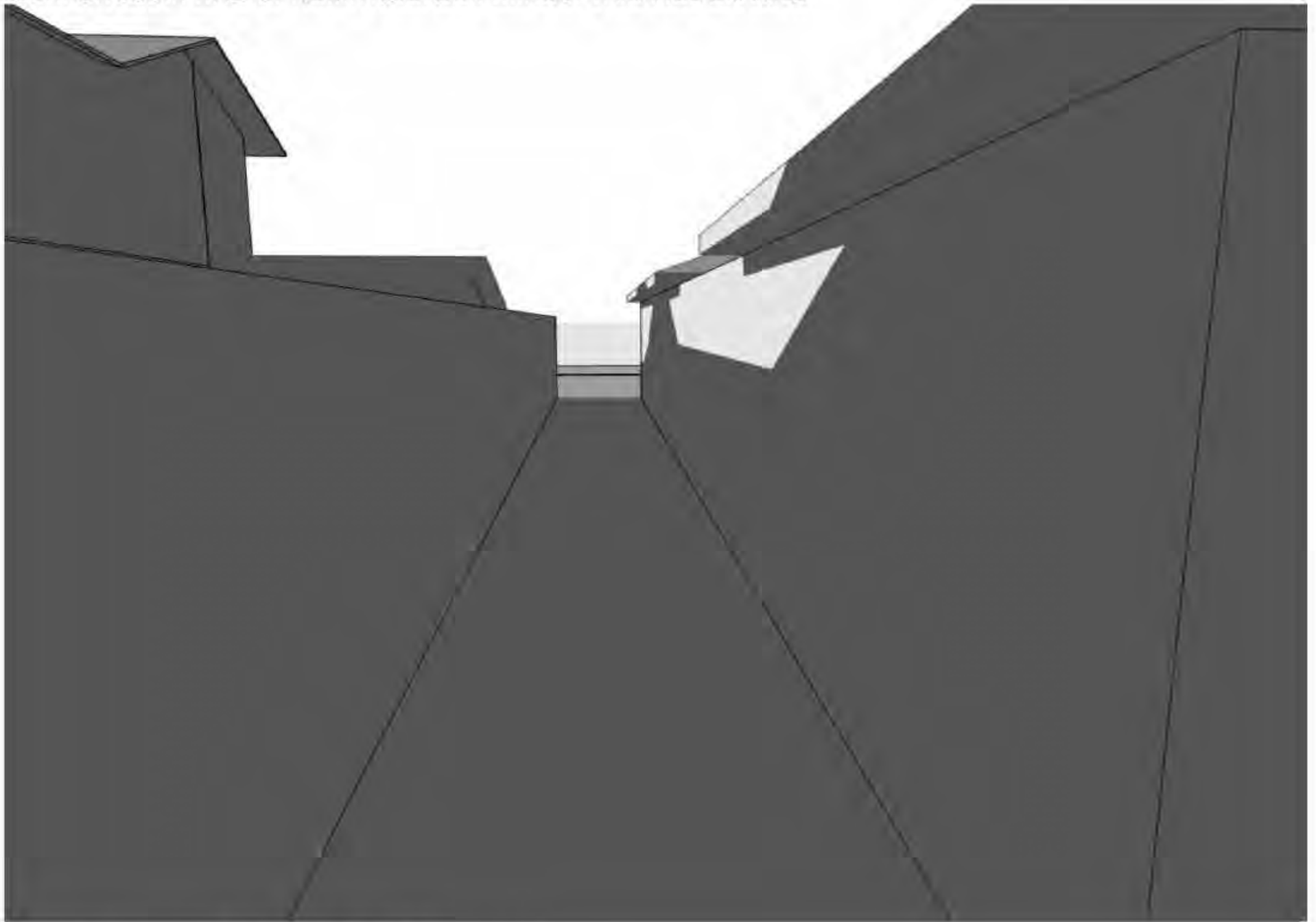
EXISTING DESIGN - WITH DAYLIGHT PLANE VARIANCE

DECEMBER 1, 2:00pm

A42

FIGURE #3D

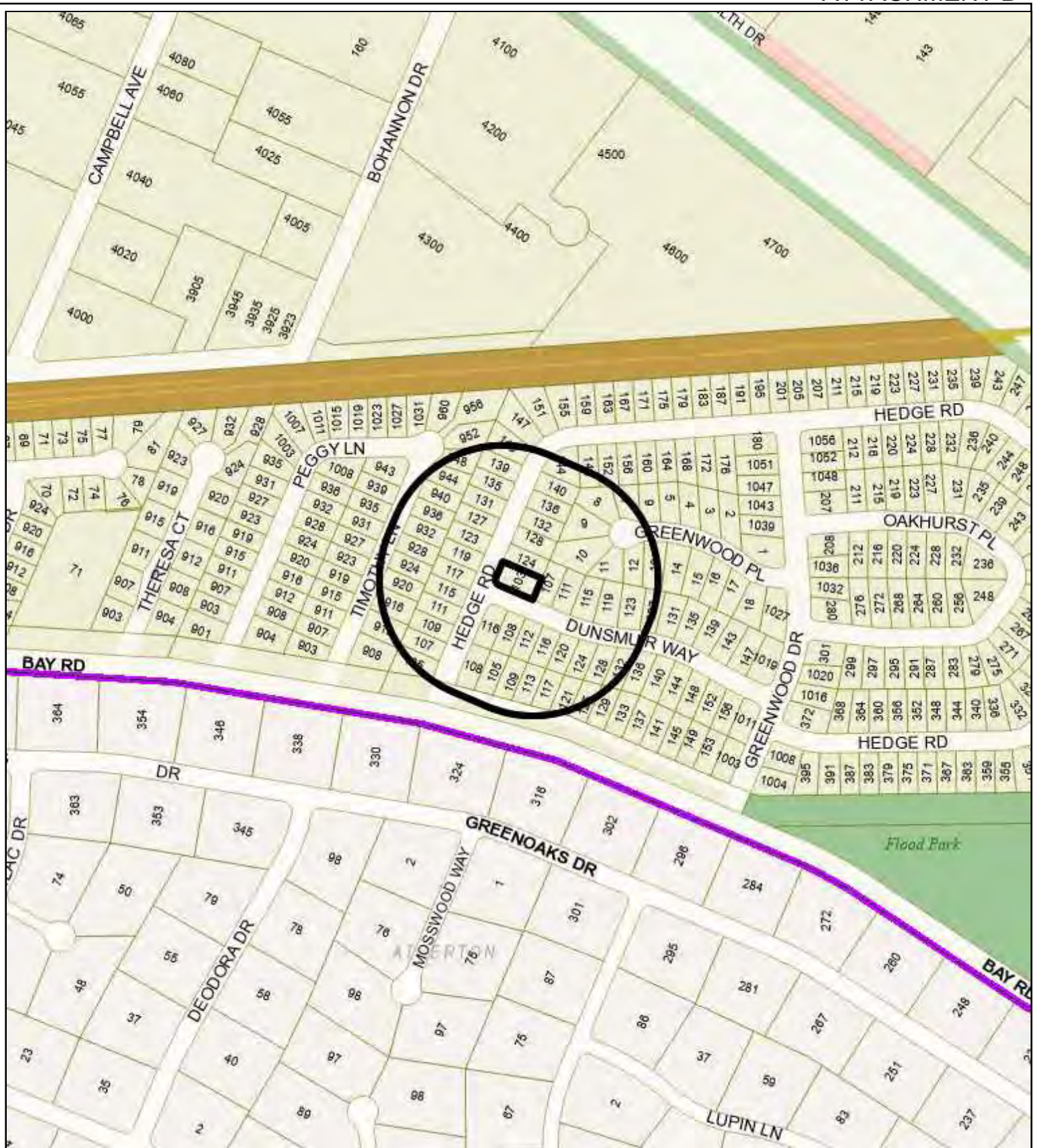
LIGHT ON SOUTH WALL OF NORTH NEIGHBOR WITHOUT VARIANCE GRANTED



DESIGN WITHOUT DAYLIGHT PLANE VARIANCE

DECEMBER 1, 2:00pm

A46



City of Menlo Park
 Location Map
 103 Dunsmuir Way



Scale: 1:4,000

Drawn By: CC

Checked By: CDS

Date: 1/23/2023

Sheet: 1

103 Dunsmuir Way (PLN2022-00039) – Data Table

	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	6,552.0 sf	6,552.0 sf	7,000 sf min
Lot width	60.0 ft	60.0 ft	65 ft min
Lot depth	110.0 ft	110.0 ft	100 ft min
Setbacks			
Front (west)	20.0 ft	23.3 ft	20 ft min
Rear (east)	20.0 ft	21.8 ft	20 ft min
Side (north-inside)	6.0 ft	11.8 ft	6 ft min
Side (south-street)	12.3 ft ¹	18.0 ft	12 ft min
Building coverage	2,079.0 sf 31.7 %	1,737.0 sf 26.5 %	2,293 sf max 35 % max
FAL (Floor Area Limit)	2,795.8 sf	1,737.0 sf	2,800 sf max
Square footage by floor	901.8 sf-basement 1,382.4 sf-1st 976.0 sf-2nd 437.4 sf-garage	0.0 sf-basement 1,347 sf-1st 0.0 sf-2nd 390.0 sf-garage	
Square footage of buildings	3,697.6 sf	1,737.0 sf	
Building height	23.6 ft	13.7 ft	28 ft max
Parking	2 covered spaces	2 covered spaces	1 covered space; 1 uncovered space
Note: Areas shown highlighted indicate a nonconforming or substandard situation			
Trees	Heritage trees ² 3	Non-Heritage trees 1	New trees 0
	Heritage trees proposed for removal 0	Non-Heritage trees proposed for removal 1	Total number of trees 3
Note 1: A variance is requested for building intrusion into the daylight plane at the south side. Note 2: Two heritage trees are street trees.			



10/3/22

John McGaraghan
103 Dunsmuir Way
Menlo Park, CA 94025
(650) 353-6128
john@mcgaraghan.com

Re: Tree protection for full teardown and rebuild of single-family home at 103 Dunsmuir Way, Menlo Park, CA 94025

Dear John,

At your request, we have visited the property referenced above to evaluate the trees present with respect to the proposed project. The report below contains our analysis.

Summary

There are three protected trees and one non-protected tree on this property, and none overhanging from neighboring properties. The non-protected tree is requested for removal by the client. All protected trees are in good condition and should be retained and protected as detailed in the Recommendations, below. With proper protection, all are expected to survive and thrive during and after construction.

Assignment and Limits of Report

We have been asked to write a report detailing impacts to trees from the proposed full teardown and rebuild of a single family home on this property. This report may be used by our client and other project members as needed to inform all stages of the project.

All observations were made from the ground with basic equipment. No root collar excavations or aerial inspections were performed. No project features had been staked at the time of our site visit.

Tree Regulations

In the City of Menlo Park, native oak trees are protected at 10 inches DBH (diameter at breast height, 4.5 feet above grade), and all other trees are protected at 15 inches DBH. Street trees are protected regardless of size.

According to the Heritage Tree Ordinance Administrative Guidelines, the dollar value of replacement trees is determined as follows:

- One (1) #5 container – \$100
- One (1) #15 container – \$200
- One (1) 24-inch tree box – \$400
- One (1) 36-inch tree box – \$1,200
- One (1) 48-inch tree box – \$5,000
- One (1) 60-inch tree box – \$7,000

Please be aware of the following documents guiding tree protection during construction in Menlo Park:

1. Heritage Tree Ordinance Administrative Guidelines - <https://www.menlopark.org/DocumentCenter/View/25577/Heritage-tree-ordinance-administrative-guidelines---draft>
2. Arborist Report Requirements: Large Projects - <https://www.menlopark.org/DocumentCenter/View/25468/Arborist-report-large-project-requirements#:~:text=The%20Arborist%20Report%20shall%20include,proposed%20for%20removal%20of%20heavy>
3. Tree Protection Specifications - <https://www.menlopark.org/DocumentCenter/View/90/Tree-Protection-Specifications>

Observations

Trees

There are four trees on and adjacent to this property (Images 1-4, below): a persimmon (*Diospyros kaki*), a Chinese pistache (*Pistacia chinensis*), a London plane (*Platanus x acerifolia*), and a valley oak (*Quercus lobata*).

Protected statuses - only tree #1 is a Heritage Tree. Trees #2 and 3 are street trees. Tree #4 is a non-protected private tree on this property.

Health - all four trees are in reasonably good condition, with no notable pest, disease, or structural issues. Valley oak #1 appears quite old and has a small amount of decay as typical for a mature individual of this species.

Current Site Conditions

A single-family home is currently present on the property. The driveway is near the southeast corner of the property. The grade appears flat.

There is a concrete patio in the back yard, and a concrete walkway against the house in the side yard on the north side of the property. A brick patio is present at the entryway, facing Dunsmuir Way.

Backyard fences are typical wood construction. A low fence is also present around the perimeter of the front yard on both streets.

There is a public sidewalk on both Dunsmuir Way and Hedge Road, with no park strip.

The gas meter is at the southeast corner of the house, and the electrical meter is at the northeast corner of the house. The sewer box and hose bib are on the north side of the property, near the house.

Project Features

A new single-family home is proposed, in approximately the same location as the existing home but with a different footprint. The proposed house foundation will be installed to a minimum depth of about 16 inches.

The proposed driveway is in the northwest corner of the property. A patio is proposed in the back yard on the north side of the house. Porches or covered patios are proposed at the front and east sides of the house. A new wooden fence segment with a gate is proposed at the northwest corner of the house.

No grading, drainage, or utility work is shown on the plans provided to me.

Potential Conflicts (Protected Trees Only)

Tree #1 - a substantial part of the existing house lies within this tree's TPZ,¹ as do the existing driveway and several patios and walkways. The existing gas meter is within this tree's CRZ.²

The proposed house lies mostly within the old house footprint, with a new area in the southeast corner. This new area is within the tree's TPZ and a small part of its CRZ. Ground penetrating radar (GPR) at and around the proposed edge of excavation revealed one root inside the new area, and one root just outside it to the south. Each is about 3±1 inches in diameter, with its top side about 21 inches below grade.

The proposed porch/covered patios at the south and east sides of the house are within this tree's TPZ, as is the proposed paved walkway leading to the front door.

Tree #2 - no project features lie within this tree's TPZ.

Tree #3 - the proposed driveway lies within this tree's TPZ, just outside its CRZ.

Tree #4 - this tree is not protected, so it has not been evaluated for potential conflicts.

Testing and Analysis

Tree DBHs were taken using a diameter tape measure if trunks were accessible. Multistemmed trees were measured below the point where the leaders diverge, if possible. The DBHs of trees with non-accessible trunks were estimated visually. All trees over four inches in DBH were inventoried, as well as street trees of all sizes.

Vigor ratings are based on tree appearance and experiential knowledge of each species.

¹ Tree protection zones. See Discussion, Tree Map, and Tree Table for more detail.

² Critical root zone. See Discussion, Tree Map, and Tree Table for more detail.

Tree location data was collected using a GPS smartphone application and processed in GIS software to create the maps included in this report. Due to the error inherent in GPS data collection, and due also to differences between GPS data and CAD drawings, tree locations shown on the map below are approximate except where matched to the survey.

We visited the site once, on 4/8/2022. All observations and photographs in this report were taken at that site visit.

The client reports that a ground penetrating radar scan was performed on 5/28/2022 by utility locating personnel from Exaro Technologies Corporation. The area around the proposed new house area at the southeast corner was scanned for roots 1" and larger in diameter, to a depth of 24 inches.

The tree protection analysis in this report is based on the plan set titled "MCGARAGHAN RESIDENCE: 103 DUNSMUIR WAY," dated 9/22/2022, provided to us electronically by the client.

Discussion

Tree Protection Zones (TPZs)

Tree roots grow where conditions are favorable, and their spatial arrangement is therefore unpredictable. Favorable conditions vary among species, but generally include the presence of moisture, and soft soil texture with low compaction.

Contrary to popular belief, roots of all tree species grow primarily in the top two feet of soil, with a small number of roots sometimes occurring at greater depths. Some species have taproots when young, but these almost universally disappear with age. At maturity, a tree's root system may extend out from the trunk farther than the tree is tall.

The optimal size of the area around a tree which should be protected from disturbance depends on the tree's size, species, and vigor, as shown in the following table (adapted from *Trees & Construction*, Matheny and Clark, 1998):

Species tolerance	Tree vitality³	Distance from trunk (feet per inch trunk diameter)
Good	High	0.5
	Moderate	0.75
	Low	1
Moderate	High	0.75
	Moderate	1
	Low	1.25
Poor	High	1
	Moderate	1.25
	Low	1.5

It is important to note that some roots will almost certainly be present outside the TPZ; however, root loss outside the TPZ is unlikely to cause tree decline.

Critical Root Zones (CRZs)

Although any root loss inside the TPZ may cause a short-term decline in tree condition, trees can often recover adequately from a small amount of root loss in the TPZ.

Tree stability is impacted at a shorter distance from the tree trunk. For linear cuts on one side of the tree, the minimum distance typically recommended is three times the DBH, measured from the edge of the trunk (*Best Management Practices: Root Management*, Costello, Watson, and Smiley, 2017). This is called the critical root zone (CRZ), as any distance shorter than this increases a tree's likelihood of failure.

Roots and Foundations

Tree roots do not generally grow under houses, as foundation installation requires these areas to be heavily compacted and dry. As discussed above, these conditions do not meet trees' needs for root colonization. Roots may grow under houses if foundations are poorly installed, or if trees are growing in contact with the foundation.

³ Matheny & Clark uses tree age, but we feel a tree's vitality more accurately reflects its ability to handle stress.

Tree Appraisal Methods

We use the trunk formula technique with discounting for condition and functional and external limitations, as detailed in the second printing of the 10th Edition of the *Guide for Plant Appraisal* (Council of Tree and Landscape Appraisers, 2019).

For palms, we use the approximate height of clear trunk (estimated visually) multiplied by the per-foot cost given in the regional plant appraisal committee species classification for California.

Conclusions

Tree #1 - minor impacts to this tree are likely overall.

Minor impacts are likely from demolition, if care is taken as detailed in the Recommendations below.

Minor impacts are likely from the proposed new part of the house, as only one significant root is present and it is below the proposed foundation depth. It will likely be impacted to some degree by soil compaction needed for foundation installation. The significant root just south of this area will likely also be impacted to some degree by construction access.

Minor impacts are likely from the front and side porches/covered patios, and from the front walkway.

Tree #2 - minimal impacts to this tree are likely from the project as proposed.

Tree #3 - minor to moderate impacts to this tree are likely from the proposed driveway.

Tree #4 - this tree is not protected and has not, therefore, been evaluated for construction impacts.

Recommendations

Preconstruction Phase

1. Remove tree #4, upon approval from the City of Menlo Park (no permit appears to be needed).
2. Install pre-demolition tree protection fencing approximately as shown in the Tree Map, below.
 - a. Minimum distances from trunk centers are given on the Tree Map. A larger area may be protected if desired.
 - b. Where existing barriers which will be retained impede access comparably to tree protection fencing, these barriers are an acceptable substitute for tree protection fencing.
 - a. Please be aware that tree protection fencing may differ from ideal tree protection zones, and from canopy sizes.
 - c. Tree protection fencing shall comprise 6' chain link fabric mounted on 1.5" diameter metal posts driven into the ground.
 - d. Place a 6" layer of wood chips inside tree protection fencing.
 - e. Tree protection fencing shall adhere to the requirements in the document titled "Tree Protection Specifications," available at <https://www.menlopark.org/DocumentCenter/View/90/Tree-Protection-Specifications>

Demolition Phase

1. When demolishing the existing hardscape within the TPZ and CRZ of tree #1, start work at the house and proceed backwards toward the street, limiting heavy equipment to still-paved areas.

Construction Phase

1. Move tree protection fencing from the demolition locations to the post-demolition locations shown on the Tree Map. All specifications given above apply.
2. Alert the project arborist if utility or other work becomes necessary within any tree TPZs.
3. When excavating within TPZs for the front and side patios, front walkway, and driveway:

- a. If footings will be used to support a raised surface, be as flexible as practical with footing placement to avoid any tree roots encountered.
- b. Hand-excavate edge nearest trunk to the full depth of the feature being installed or to a depth of three feet, whichever is shallower.
- c. Retain as many roots as practical.
- d. If roots 1-2" in diameter must be cut, sever them cleanly with a sharp saw or bypass pruners.
- e. If roots over 2" must be cut, stop work in that area and contact the project arborist for guidance.
- f. Notify project arborist when excavation is complete. Project arborist shall inspect work to make sure all roots have been cut cleanly.
- g. If excavation will be left open for more than 3 days:
 - i. Cover excavation wall nearest trunk with several layers of burlap or other absorbent fabric.
 - ii. Install a timer and soaker hoses to irrigate with potable water twice per day, enough to wet fabric thoroughly.

Post-Construction Phase

1. Provide supplemental irrigation for tree #1 to aid in root regrowth for at least three years.

Supporting Photographs

Image 1: valley oak #1



Image 2: Chinese pistache #2



Image 3: London plane #3



Image 4: persimmon #4



Respectfully submitted,



Katherine Naegele

She/Her

Consulting Arborist

Master of Forestry, UC Berkeley

International Society of Arboriculture Certified Arborist #WE-9658A

ISA Tree Risk Assessment Qualification Credentialed

American Society of Consulting Arborists, Member

katherine@aacarbor.com

(408) 201-9607 (direct cell)

(408) 675-1729 (main cell)

aacarbor.com

[Yelp](#)



Terms of Assignment

The following terms and conditions apply to all oral and written reports and correspondence pertaining to the consultations, inspections, and activities of Aesculus Arboricultural Consulting:

1. All property lines and ownership of property, trees, and landscape plants and fixtures are assumed to be accurate and reliable as presented and described to the consultant, either orally or in writing. The consultant assumes no responsibility for verification of ownership or locations of property lines, or for results of any actions or recommendations based on inaccurate information.
2. It is assumed that any property referred to in any report or in conjunction with any services performed by Aesculus Arboricultural Consulting is in accordance with any applicable codes, ordinances, statutes, or other governmental regulations, and that any titles and ownership to any property are assumed to be good and marketable. The existence of liens or encumbrances has not been determined, and any and all property is appraised and/or assessed as though free and clear, under responsible ownership and competent management.
3. All reports and other correspondence are confidential and are the property of Aesculus Arboricultural Consulting and its named clients and their assigns or agents. Possession of this report or a copy thereof does not imply any right of publication or use for any purpose, without the express permission of the consultant and the client to whom the report was issued. Loss, removal, or alteration of any part of a report invalidates the entire appraisal/evaluation.
4. The scope of any report or other correspondence is limited to the trees and conditions specifically mentioned in those reports and correspondence. Aesculus Arboricultural Consulting assumes no liability for the failure of trees or parts of trees, inspected or otherwise. The consultant assumes no responsibility to report on the condition of any tree or landscape feature not specifically requested by the named client.
5. All inspections are limited to visual examination of accessible parts, without dissection, excavation, probing, boring or other invasive procedures, unless otherwise noted in the report, and reflect the condition of those items and features at the time of inspection. No warranty or guarantee is made, expressed or implied, that problems or deficiencies of the plants or the property will not occur in the future, from any cause. The consultant shall not be responsible for damages caused by any tree defects, and assumes no responsibility for the correction of defects or tree related problems.
6. The consultant shall not be required to provide further documentation, give testimony, be deposed, or to attend court by reason of this appraisal/report unless subsequent contractual arrangements are made, including payment of additional fees for such services as set forth by the consultant or in the fee schedule or contract.
7. Aesculus Arboricultural Consulting makes no warranty, either expressed or implied, as to the suitability of the information contained in any reports or correspondence, either oral or written, for any purpose. It remains the responsibility of the client to determine applicability to his/her particular case.
8. Any report and the values, observations, and recommendations expressed therein represent the professional opinion of the consultant, and the fee for services is in no manner contingent upon the reporting of a specified value nor upon any particular finding.
9. Any photographs, diagrams, charts, sketches, or other graphic material included in any report are intended solely as visual aids, are not necessarily to scale, and should not be construed as engineering reports or surveys unless otherwise noted in the report. Any reproduction of graphic material or the work product of any other persons is intended solely for clarification and ease of reference. Inclusion of said information does not constitute a representation by Aesculus Arboricultural Consulting as to the sufficiency or accuracy of that information.

Tree #	Common Name	Species	DBH (in.)	Vitality (0-3)	Heritage Tree?	Street Tree?	Off-Site Tree?	Suitability for preservation (0-3)	Remove?	Appraised Value	Species Construction Tolerance (1 = poor, 3 = good)	TPZ radius (ideal; ft. from center of trunk)	CRZ radius (ideal; ft. from center of trunk)	Expected Impacts	Notes
1	Valley oak	Quercus lobata	50.2	2	X			3		\$96,000.00	2	50.2	14.6	Minor overall Minor from demolition Minor from proposed new part of house footprint Minor from proposed front and side porches/covered patios Minor from front walkway	Minor swelling and decay present. Two 3" roots found in proposed new house area with ground penetrating radar
2	Chinese pistache	Pistacia chinensis	5.5	3		X		3		\$1,320.00	3	2.8	1.6	Minimal	-
3	London plane	Platanus x acerifolia	16.6	3		X		3		\$6,600.00	3	8.3	4.8	Minor to moderate from proposed driveway	-
4	Persimmon	Diospyros kaki	7.1	3				3	X	-	-	-	-	Minimal	Requested for removal by client

Chan, Calvin

From: Jon Wright <wrightjon81@gmail.com>
Sent: Thursday, September 22, 2022 11:58 AM
To: Chan, Calvin
Cc: Annabel R. Chang
Subject: In support of 103 Dunsmuir Way application

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Hi Calvin –

We live at 112 Dunsmuir Way across from 103 Dunsmuir Way and are writing in strong support of their current application and plans.

The applicants are beloved members of the neighborhood who are gracious with both their time and space. Their current house is a welcome point when entering Suburban Park and has served as a natural convening point for neighbors, walkers and playing children.

It is clear they have taken this responsibility seriously with the current design as it compliments and enhances the character of the neighborhood. Notably, it will be a warm and beautiful home that welcomes people into our community as they turn on to Hedge from Bay Road.

We are aware that the applicant is applying for a variance on the Dunsmuir side for the daylight plane and are supportive. It will have no real impact whatsoever.

We are so happy for this family and eager to see their “forever home,” be built.

Thank you for your consideration,

Jon Wright and Annabel Chang

Chan, Calvin

From: Martin de Jong <dejong@fordham.edu>
Sent: Friday, September 23, 2022 4:08 PM
To: Chan, Calvin
Cc: Eleanor de Jong
Subject: 103 Dunsmuir Way

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Hi Calvin,

My wife Eleanor (cc'd) and I live at 116 Dunsmuir Way, diagonal from 103 Dunsmuir Way. We received the recent notice and have reviewed their building plans.

We are excited that they are planning to build such a beautiful home on our street and are fully supportive of their plans. We urge staff and the commission to support the project.

Many thanks,

Martin de Jong

City of Menlo Park Planning Commission

My Name is Jerry Marsh and my wife's name is Carol. We live at 124 Hedge Rd. which is the adjoining property to the north of 103 Dunsmuir Wy. We have previously written a letter of support for the proposed remodel of the property located at 103 Dunsmuir Wy.

We are aware and have reviewed the request for variance from the "daylight plane" requirements on the Dunsmuir side of the house. Carol and I fully support the required variance. The current remodel proposal has little or no impact on our "patio room" from being shaded. The proposed planning commission requirement would result negatively on our home.

It is my opinion that the original remodel proposal be confirmed and move forward as soon as possible.

Thank You,

Jerry & Carol Marsh

650 804-0814



STAFF REPORT

Planning Commission

Meeting Date:

1/23/2023

Staff Report Number:

23-008-PC

Public Hearing:

Consider and adopt a resolution to approve a use permit to remodel and construct first and second story additions to an existing nonconforming, one-story single-family residence on a substandard lot with regard to minimum lot width and area in the R-1-U (Single Family Urban Residential) zoning district, at 932 Peggy Lane.

Recommendation

Staff recommends that the Planning Commission approve a use permit to remodel and construct first and second story additions to an existing nonconforming, one-story single-family residence on a substandard lot with regard to minimum lot width and area in the R-1-U (Single Family Urban Residential) zoning district, at 932 Peggy Lane. The project would exceed 50 percent of the existing floor area and is considered equivalent to a new structure. The value of the proposed project would also exceed 50 percent of the existing replacement value in a 12-month period and requires approval of a use permit by the Planning Commission. The draft resolution, including the recommended actions and conditions of approval, is included as Attachment A.

Policy Issues

Each use permit request is considered individually. The Planning Commission should consider whether the required use permit findings can be made for the proposal.

Background

Site location

The subject property is located near the northern end of Peggy Lane, in the Suburban Park neighborhood. The surrounding homes to the north, south, and across Peggy Lane also have an R-1-U (Single Family Urban Residential) zoning designation. Properties located to the west of Theresa Court, a street located one block to the west of Peggy Lane, are in the R-1-U(LM) (Single Family Urban Residential [Lorelei Manor]) zoning district. Houses along Peggy Lane include both one- and two-story residences, developed in a variety of architectural styles, including ranch and craftsman. A location map is included as Attachment B.

Analysis

Project description

The subject property is developed with a one-story residence with an attached one-car garage. The

residence is considered to be a legal non-conforming structure, with a right-side setback of approximately 4.6 feet, where a minimum of five feet is required. The residence was originally built with only one required off-street parking space in the existing one-car garage. As a result, the building is considered legal non-conforming in terms of parking and the right side setback. The applicant is proposing to add first-floor and second-floor additions and conduct interior modifications.

With the proposed additions and interior modifications, the residence would include a total of three bedrooms and two bathrooms. The value of the proposed work would equal 235 percent of the replacement value of the existing non-conforming residence in a 12-month period, exceeding the 50 percent use permit threshold.

Apart from the existing nonconforming portion of the house and the nonconforming parking space count, the residence would meet all Zoning Ordinance requirements for setbacks, lot coverage, floor area limit (FAL), daylight plane, and height. Of particular note with regard to Zoning Ordinance requirements:

- The second floor would be limited in size relative to the development, with a floor area of 903.5 square feet, where 1,400 square feet is the maximum permitted.
- The proposed floor area for the residence is 2,786.5 square feet, where the maximum allowable is 2,800 square feet.
- The proposed project would be constructed at the maximum building coverage, with a total of 35.0 percent, where 35.0 percent is the maximum allowable building coverage for a two-story residence.
- The proposed residence would be 25.5 feet in height, where 28 feet is the maximum allowed.
- On the first floor, three areas of the residence would exceed 12 feet in floor to ceiling height. These include a portion of the stair landing area, near the rear, an area of the kitchen, and a portion of the private living area to the rear of the kitchen. These areas, which together constitute 73 square feet, have been counted at 200 percent within the floor area calculations.
- A portion of attic would exceed five feet in height above the first floor and adjacent to the second floor. This area, which totals 33 square feet, has been counted within the floor area calculations.
- A chimney located partially within the existing garage is being replaced in generally the same location. However, the nonconforming parking space within the garage would be made into a complaint, 20-foot by 10-foot sized space as a result of the change.

The existing residence is set back 24.8 feet from the front property line and 38.4 feet from the rear property line, and with the proposed additions, the residence would be set back 24.1 feet from the front property line and 30.4 feet from the rear property line. A 20-foot setback is required for both the front and rear setbacks within the R-1-U zoning district. The residence would maintain the nonconforming encroachment at the right side setback for the existing portion of the residence, but the proposed addition would be set back a minimum of 5.2 feet on the right side. The left side would remain the same, at 5.1 feet. In the R-1-U zoning district, side setbacks are 10 percent of the lot width, but no less than five feet and no greater than 10 feet. With a lot width of 50 feet, the required setback for each side of the property is five feet.

A data table summarizing parcel and project attributes is included as Attachment C. The project plans and the applicant's project description letter are included as Attachments D and E, respectively.

Design and materials

The applicant states in their project description letter that the existing residence is designed in a post-World War II ranch style and the applicant has stated in their project description letter that the addition is proposed to combine harmoniously with the existing style, incorporating a variety of contemporary design features. Along the front elevation, access to the residence would be possible through the main entry door, angled to the side. Stucco would be the predominant material along the façades, with vertical wood siding as accents along portions of all four façades. The new windows would contain aluminum framing and all roofing would be metal. Along the rear elevation, two sets of doors are proposed to provide access to a new uncovered deck and a partially covered porch.

Staff believes that the scale, materials, and style of the proposed residence would result in a consistent aesthetic approach and the proposed project would be generally consistent with the broader neighborhood, given the variety of architectural styles and sizes of structures in the area. Staff believes that the inclusion of contemporary materials and design modifications would be cohesive and well-proportioned.

Trees and landscaping

The applicant has submitted an arborist report (Attachment F), detailing the species, size, and conditions of the nearby heritage and non-heritage trees. The report discusses the impacts of the proposed improvements and provides recommendations for tree maintenance and protection. As part of the project review process, the arborist report was reviewed by the City Arborist.

Based on the arborist report, there are five existing trees located on or near the property. Of these trees, three trees are heritage size. The heritage trees consist of two Douglas fir trees (trees #85 and 86) located in rear of the property and one ginkgo biloba tree (tree #87) located along the right side of the property, midway onto the site.

A total of two trees assessed are non-heritage size, and both are street trees located in the public right-of-way in front of the subject property (trees #88 and 89).

To protect the heritage and non-heritage trees on site, the arborist report has identified such measures as prohibiting work actions in the tree protection zones, tree protection fencing, hand digging and limiting pier excavation, placing herbicides under paving materials that are safe for use, and designing irrigation systems to avoid trenching within the tree protection zones. All recommended tree protection measures identified in the arborist report would be implemented and ensured as part of condition 1h.

Correspondence

The applicant states in their project description letter that the property owner has completed some outreach efforts, which involved sharing project details with neighbors, and received positive verbal feedback.

As of the writing of this report, staff has received no direct correspondence.

Conclusion

Staff believes that the scale, materials, and style of the proposal are generally compatible with the surrounding neighborhood, and would result in a consistent aesthetic approach. The addition and remodeling work to the traditional ranch style would involve some more contemporary modifications, but would be generally attractive, well-proportioned and cohesive overall. Staff recommends that the Planning Commission approve the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 3 (Section 15303, "New Construction or Conversion of Small Structures") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

A. Draft Planning Commission Resolution

Exhibits to Attachment A

- A. Project Plans (See Attachment D to this (January 23, 2023) Planning Commission Staff Report)
- B. Project Description Letter (See Attachment E to this (January 23, 2023) Planning Commission Staff Report)
- C. Conditions of Approval

B. Location Map

C. Data Table

D. Project Plans

E. Project Description Letter

F. Arborist Report

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings, and exhibits are available for public

viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:
Matt Pruter, Associate Planner

Report reviewed by:
Corinna Sandmeier, Principal Planner

PLANNING COMMISSION RESOLUTION NO. 2023-XX**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF MENLO PARK APPROVING A USE PERMIT TO CONSTRUCT FIRST-FLOOR AND SECOND-FLOOR ADDITIONS AND INTERIOR MODIFICATIONS TO AN EXISTING NONCONFORMING SINGLE-FAMILY RESIDENCE IN THE R-1-U (SINGLE FAMILY URBAN RESIDENTIAL) ZONING DISTRICT**

WHEREAS, the City of Menlo Park (“City”) received an application requesting to construct first-floor and second-floor additions and interior modifications to an existing nonconforming one-story, single-family residence on a substandard lot with regard to minimum lot width and area in the Single Family Urban Residential (R-1-U) zoning district and the proposed work would exceed 50 percent of the replacement value of the existing nonconforming structure in a 12-month period; the proposal would also exceed 50 percent of the existing floor area and is considered equivalent to a new structure—(collectively, the “Project”) from Andrea Montalbano (“Applicant”), on behalf of the property owner Rorie and Jackeeline Overby (“Owner”), located at 932 Peggy Lane (APN 061-022-190) (“Property”). The Project use permit is depicted in and subject to the development plans and project description letter, which are attached hereto as Exhibit A and Exhibit B, respectively, and incorporated herein by this reference; and

WHEREAS, the Property is located in the Single Family Urban Residential (R-1-U) district. The R-1-U district supports single-family residential uses; and

WHEREAS, the proposed Project complies with all objective standards of the R-1-U district; and

WHEREAS, the proposed Project was reviewed by the Engineering Division and found to be in compliance with City standards; and

WHEREAS, the Applicant submitted an arborist report prepared by Bartlett Consulting, which was reviewed by the City Arborist and found to be in compliance with the Heritage Tree Ordinance and proposes mitigation measures to adequately protect heritage trees in the vicinity of the project; and

WHEREAS, the Project, requires discretionary actions by the City as summarized above, and therefore the California Environmental Quality Act (“CEQA,” Public Resources Code Section §21000 et seq.) and CEQA Guidelines (Cal. Code of Regulations, Title 14, §15000 et seq.) require analysis and a determination regarding the Project’s environmental impacts; and

WHEREAS, the City is the lead agency, as defined by CEQA and the CEQA Guidelines, and is therefore responsible for the preparation, consideration, certification, and approval of environmental documents for the Project; and

WHEREAS, the Project is categorically except from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures); and

WHEREAS, all required public notices and public hearings were duly given and held according to law; and

WHEREAS, at a duly and properly noticed public hearing held on January 23, 2023, the Planning Commission fully reviewed, considered, and evaluated the whole of the record including all public and written comments, pertinent information, documents and plans, prior to taking action regarding the Project.

NOW, THEREFORE, THE MENLO PARK PLANNING COMMISSION HEREBY RESOLVES AS FOLLOWS:

Section 1. Recitals. The Planning Commission has considered the full record before it, which may include but is not limited to such things as the staff report, public testimony, and other materials and evidence submitted or provided, and the Planning Commission finds the foregoing recitals are true and correct, and they are hereby incorporated by reference into this Resolution.

Section 2. Conditional Use Permit Findings. The Planning Commission of the City of Menlo Park does hereby make the following Findings:

The approval of the use permit for the proposed first-floor and second-floor additions and interior modifications is granted based on the following findings which are made pursuant to Menlo Park Municipal Code Section 16.82.030:

1. That the establishment, maintenance, or operation of the use applied for will, under the circumstance of the particular case, not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing in the neighborhood of such proposed use, or injurious or detrimental to property and improvements in the neighborhood or the general welfare of the city because:
 - a. Consideration and due regard were given to the nature and condition of all adjacent uses and structures, and to general plans for the area in question and surrounding areas, and impact of the application hereon; in that, the proposed use permit is consistent with the R-1-U zoning district and the General Plan because the construction of first-floor and second-floor additions and interior modifications to an existing nonconforming one-story, single-family residence are allowed to be constructed on substandard lots subject to granting of a use permit and provided that the proposed residence conforms to applicable zoning standards, including, but not limited to, minimum setbacks (note: only the new portions of the residence would

comply with setbacks), maximum floor area limit, and maximum building coverage.

- b. The residence includes one off-street parking space, which is nonconforming because two covered parking spaces are required; however, this nonconformity may remain.
- c. The proposed Project is designed to meet all the applicable codes and ordinances of the City of Menlo Park Municipal Code and the Commission concludes that the Project would not be detrimental to the health, safety, and welfare of the surrounding community as the new residence would be located in a single-family neighborhood and designed such that privacy concerns would be addressed through second story setbacks greater than the minimum required setbacks in the R-1-U district.

Section 3. Conditional Use Permit. The Planning Commission approves Use Permit No. PLN2022-00005, which use permit is depicted in and subject to the development plans and project description letter, which are attached hereto and incorporated herein by this reference as Exhibit A and Exhibit B, respectively. The Use Permit is conditioned in conformance with the conditions attached hereto and incorporated herein by this reference as Exhibit C.

Section 4. ENVIRONMENTAL REVIEW. The Planning Commission makes the following findings, based on its independent judgment after considering the Project, and having reviewed and taken into consideration all written and oral information submitted in this matter:

- A. The Project is categorically except from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Construction of Small Structures)

Section 5. Severability.

If any term, provision, or portion of these findings or the application of these findings to a particular situation is held by a court to be invalid, void or unenforceable, the remaining provisions of these findings, or their application to other actions related to the Project, shall continue in full force and effect unless amended or modified by the City.

I, Corinna Sandmeier, Principal Planner and Planning Commission Liaison of the City of Menlo Park, do hereby certify that the above and foregoing Planning Commission Resolution was duly and regularly passed and adopted at a meeting by said Planning Commission on January 23, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS THEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this 23rd day of January, 2023

Corinna Sandmeier
Principal Planner and Planning Commission Liaison
City of Menlo Park

Exhibits

- A. Project Plans
- B. Project Description Letter
- C. Conditions of Approval

LOCATION: 932 Peggy Lane	PROJECT NUMBER: PLN2022-00005	APPLICANT: Andrea Montalbano	OWNER: Rorie and Jackeeline Overby
<p>PROJECT CONDITIONS:</p> <ol style="list-style-type: none"> 1. The use permit shall be subject to the following standard conditions: <ol style="list-style-type: none"> a. The applicant shall be required to apply for a building permit within one year from the date of approval (by January 23, 2024) for the use permit to remain in effect. b. Development of the project shall be substantially in conformance with the plans prepared by Dorman Associates, consisting of 13 plan sheets, dated received November 29, 2022 and approved by the Planning Commission on January 23, 2023, except as modified by the conditions contained herein, subject to review and approval of the Planning Division. c. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project. d. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project. e. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes. f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division. g. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits. h. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the arborist report prepared by Bartlett Consulting, dated received September 22, 2022. i. Prior to building permit issuance, the applicant shall pay all fees incurred through staff time spent reviewing the application. j. The applicant or permittee shall defend, indemnify, and hold harmless the City of Menlo Park or its agents, officers, and employees from any claim, action, or proceeding against the City of Menlo Park or its agents, officers, or employees to attack, set aside, void, or annul an approval of the Planning Commission, City Council, Community Development Director, or any other department, committee, or agency of the City concerning a development, variance, permit, or land use approval which action is brought within the time period provided for in any applicable statute; provided, however, that the applicant's or permittee's duty to so defend, indemnify, and hold harmless shall be subject to the City's promptly notifying the applicant or permittee of any said claim, action, or proceeding and the City's full cooperation in the applicant's or permittee's defense of said claims, actions, or proceedings. 			



City of Menlo Park
 Location Map
 932 Peggy Lane



	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	5,491.0 sf	5,491.0 sf	7,000 sf min.
Lot width	50.0 ft.	50.0 ft.	65 ft. min.
Lot depth	109.8 ft.	109.8 ft.	100 ft. min.
Setbacks			
Front	24.1 ft.	24.8 ft.	20 ft. min.
Rear	30.4 ft.	38.4 ft.	20 ft. min.
Side (left)	5.1 ft.	5.1 ft.	5 ft. min.
Side (right)	4.6 ft.	4.6 ft.	5 ft. min.
Building coverage	1,922.0 sf	1,699.6 sf	1,922 sf max.
	35.0 %	31.0 %	35 % max.
FAL (Floor Area Limit)	2,786.5 sf	1,377.0 sf	2,800 sf max.
Square footage by floor	1,626.0 sf/1st	1,074.5 sf/1st	
	797.5 sf/2nd	254.5 sf/garage	
	257.0 sf/garage	48.0 sf/acc.	
	33.0 sf/attic	buildings	
	73.0 sf/greater than 12 feet	313.8 sf/porches	
	33.0 sf/porches	8.8 sf/chimneys	
	6.0 sf/fireplaces		
Square footage of buildings	2,824.3 sf	1,699.6 sf	
Building height	25.5 ft.	16.5 ft.	28 ft. max.
Parking	1 covered	1 covered	1 covered/1 uncovered
Note: Areas shown highlighted indicate a nonconforming or substandard situation.			

Trees	Heritage trees*	3	Non-Heritage trees**	2	New Trees	0
	Heritage trees proposed for removal	0	Non-Heritage trees proposed for removal	0	Total Number of Trees	5

* All three heritage trees are located on the subject property.

** Both non-heritage trees are street trees located in the public right-of-way in front of the subject property.

Dorman Associates
CHRIS DORMAN, AIA
229 FLAMINGO ROAD
MILL VALLEY, CA 94641
415.380.7914
415.380.7915 FAX
CDOR@DORMANASSOCIATES.COM

OVERBY RESIDENCE
932 PEGGY LANE
MENLO PARK, CA 94025
APN061-022-190

PLANNING SUBMITTAL

COVER SHEET & AREA PLAN

REVISIONS

DATE: 11/29/22
SHEET

A1

ABBREVIATIONS

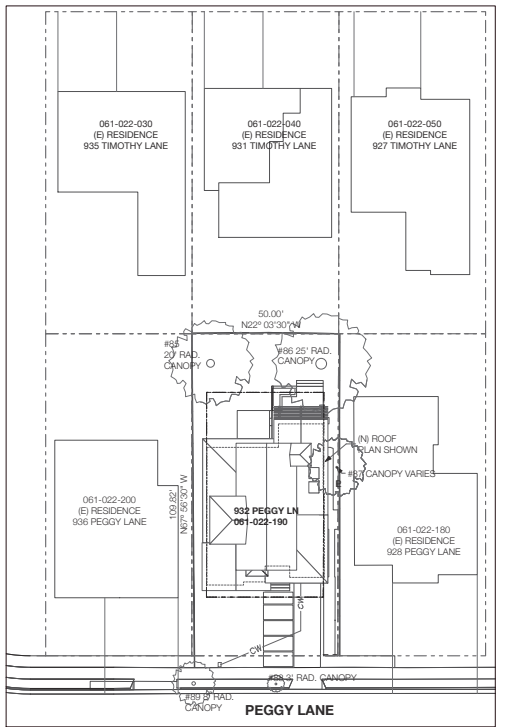
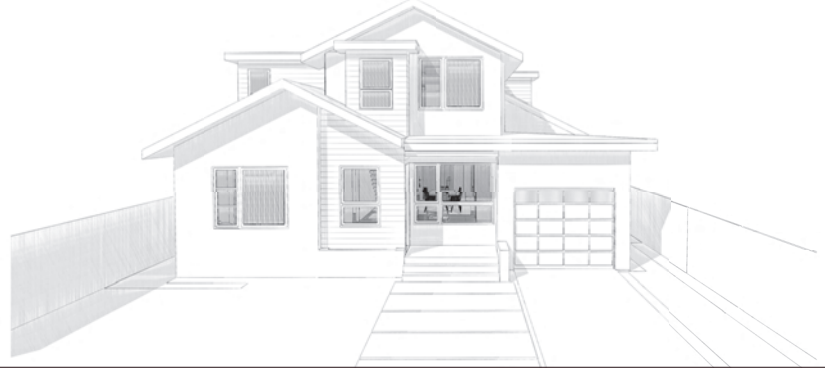
A.B.	Anchor Bolt	JAN.	Janitor
A.C.	Air Conditioning	JT.	Joint
A.O.	Acoustic	KIT.	Kitchen
A.D.	Area Drain	LAB.	Laboratory
A.D.O.	Added/Additional	LAM.	Laminar
ADJ.	Adjustable or Adjacent	LAV.	Lavatory
AF	Arc Fault	LVR.	Locker
AFD	Arc Fault Device	LWT.	Light
ALUM.	Aluminum	MAX.	Maximum
ARCH.	Architectural	M.S.	Machine Bolt
ASPH.	Asphalt	M.C.	Medicine Cabinet
B.B.	Bread Board	MECH.	Mechanics
BD.	Board	MEMB.	Membrane
BLDG.	Building	MET.	Metal
BLC.	Block	MFR.	Manufacturer
BLKG.	Blocking	MH.	Manhole
BM.	Beam	MIN.	Minimum
BOL.	Bottom	MIR.	Mirror
BRKT.	Bracket	MISC.	Miscellaneous
B.U.	Built-Up	M.O.	Masonry Opening
CAB.	Cabinet	M.R.	Moisture Resistant
C.B.	Catch Basin	MTD.	Mounted
CEM.	Cement	MUL.	Mulch
CER.	Ceramic	N.	New
C.I.	Cast Iron	N.	North
C.G.	Corner Guard	N.I.C.	Not In Contract
CLG.	Ceiling	N.O.	Number
CLGK.	Caulking	NGM.	Nominal
CL.	Closet	N.T.S.	Not To Scale
CLF.	Clear	O.A.	Overall
CNTR.	Counter	OBS.	Obscure
C.O.	Cased Openings	O.C.	On Center
CO.	Column	O.D.	Outside Diameter (Dim)
CONC.	Concrete	OFF.	Office
CONN.	Connection	OPNG.	Opening
CONST.	Construction	OPP.	Opposite
CONT.	Continuous	OSB.	Oriented Strand Board
CORR.	Corridor	O.V.	Over
CTR.	Center	P.	Plate/Plantry
CTS.K.	Countersunk	P.	Plate
D.	Dryer	P.W.D.	Plywood
DBL.	Double	PAR.	Pair
DEPT.	Department	PRCST.	Pre-Cast
DET.	Detail	PT.	Point
D.F.	Drinking fountain or	PTN.	Partition
	Douglas Fir	Q.T.	Quarry Tile
DI.	Diameter	R.	Riser or right
DIM.	Dimension	RAD.	Radius
DISP.	Dispenser	R.A.	Return Air Grill
DN.	Down	R.D.	Roof Drain
DR.	Door	REF.	Reference
DS.	Downspout	REFR.	Refrigerator
DW.	Dishwasher	REG.	Register
DWG.	Drawing	REIN.	Reinforced
DWR.	Drawer	REQ.	Required
E.	East	RESIL.	Resilient
EA.	Each	RTNG.	Retaining Wall
E.A.	Existing to Remain(U.O.N.)	R.O.	Rough Opening
EXPJ.T.	Expansion Joint	RSN.	Resin
ELEC.	Electrical	R.W.L.	Rain Water Leader
ELEV.	Elevator	S.	South or Shell
EMER.	Emergency	S.C.	Solid Core
ENCL.	Enclosure	SCHED.	Schedule
E.P.B.	Electrical Panelboard	S.D.	Smoke Detector
EQ.	Equal	SECT.	Section
EQUIP.	Equipment	SHR.	Shower
EXP.	Expansion or Exposed	SHT.	Sheet
EXST.	Existing	SHTG.	Sheathing
EXT.	Exterior	SL.	Sliding
FA.	Fire Alarm	SPEC.	Specification
F.A.U.	Flat Air Unit	SQ.	Square
F.B.	Flat Bar	S.S.	Stainless Steel
F.C.	Face of Finish	S.C.D.	See Civil Drawing
F.D.	Floor Drain	S.E.D.	See Electrical Dwg
FDN.	Foundation	S.M.D.	See Mechanical Dwg
F.E.	Fire Extinguisher	S.S.D.	See Structural Dwg
F.E.C.	Fire Extinguisher Cab.	STA.	Station
FF.	Finish Floor	STD.	Standard
F.H.C.	Fire Hose Cabinet	STL.	Steel
F.H.W.S.	Flat Head Wood Screw	STOR.	Storage
FIN.	Finish	STR.	Structural
FL.	Floor	SUSP.	Suspended
FLASH.	Flashing	SYM.	Symmetrical
FLUOR.	Fluorescent	T.	Tile, Top or Tread
F.O.C.	Face of Concrete	T.&G.	Tongue and Groove
F.O.F.	Face of Finish	T.B.	Top of Bar
F.O.S.	Face of Stud	T.C.	Top of Curb
FP.	Fireplace	TEL.	Telephone
FPR.	Fireproof	TER.	Terrazzo
FT.	Foot or Feet	THR.	Threshold
FT.	Footing	THK.	Thick
FURR.	Furring	T.	Thin
FGUL.	Fixed Glass	T.O.	Top of
G.	Gas	T.O.P.	Top of Plate
GALV.	Galvanized	T.P.	Top of Pavement
G.B.	Grid Bar	T.P.H.	Toilet Paper Holder
G.D.	Garbage Disposal	T.V.	Television
G.F.I.	Ground Fault Interrupter	T.O.W.	Top of Wall
G.L.	Glass	T.U.P.	Touch-Up & Paint Wall
GND.	Ground	TYP.	Typical
GR.	Grain	UNE.	Unfinished
G.S.M.	Galvanized Sheet Metal	U.O.N.	Unless Otherwise Noted
GYP.	Gypsum	UR.	Urinal
H.	Hole	VERT.	Vertical
H.C.	Hollow Core/Handicap	VEST.	Vestibule
HDR.	Header	W.D.P.	Wet Drain
HDR.	Header	W.	West, Washer or Water
H.D.W.D.	Hardwood	W	With
H.W.E.	Hardware	W.C.	Water Closet
H.M.	Hollow Metal	WD.	Wood
HORIZ.	Horizontal	WIND.	Window
H.R.	Hand Rail	WP.	Waterproof
HT.	Height	WSCST.	Wanscot
I.D.	Inside Diameter (Dim)	WT.	Weight
INSUL.	Insulation	W.W.F.	Welded Wire Fabric
INT.	Interior		

SYMBOLS

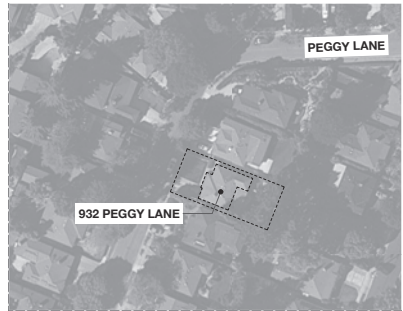
DETAIL / TITLE	DOOR ID
—DETAIL/TITLE NUMBER	SEE DOOR SCHED.
— SHEET NUMBER	WINDOW ID
EXTERIOR ELEVATIONS	SEE WINDOW SCHED.
— ELEVATION NUMBER	NOTE: SEE ELEC. DWGS FOR ADD. SYMBOLS
— SHEET NUMBER	REVISION (NO.)
INTERIOR ELEVATIONS	— CENTERLINE
— ELEVATION NUMBER	— DIAMETER
— SHEET NUMBER	— PROPERTY LINE OR PLATE

OVERBY RESIDENCE

932 PEGGY LANE, MENLO PARK, CA 94025



③ AREA PLAN (SITE CONTEXT)
SCALE: 1" = 30'



② SITE CONTEXT
NOT TO SCALE



① VICINITY MAP
NOT TO SCALE

PROJECT DESCRIPTION 37°28'39.6"N 122°10'44.3"W
ADDRESS: 932 PEGGY LANE
MENLO PARK, CA 94025
APN061-022-190
ZONING: R-1-U
CONSTRUCTION TYPE: TYPE-VB - SPRINKLERED
OCCUPANCY GROUP: R-3/U
BUILDING CODE: STRUCTURAL: 2019 CBC
NON-STRUCTURAL: 2019 CRC
OTHER APPLICABLE CODES: 2019 CMC, 2019 OFC, 2019 CEC, 2019 CEES, 2019 CGBS, 2019 OFC, 2019 SFBC
SCOPE OF WORK:
FIRST FLOOR AREA ADDITION & INTERIOR REMODEL,
SECTION FLOOR ADDITION, EXTERIOR SIDING OVERALL
REWORK, RE-ROOF ALL AROUND

ARCHITECTURAL SHEETS
SHEET # DRAWING
A1 COVER SHEET & AREA PLAN
A1.1 EROSION CONTROL PLAN & PERVIOUS AREA CALCULATIONS
A2 ENLARGED SITE PLANS (E & N)
A3 FAL & LOT CALCULATIONS
A4 (E) ELEVATIONS & SITE DEMO PLAN
A5 SECTION PLANS
A6 (N) MAIN FLOOR PLAN
A7 (N) UPPER FLOOR & ROOF PLANS
A8 (N) ELEVATIONS - WEST & EAST
A9 (N) ELEVATIONS - SOUTH & NORTH
A10 (N) BUILDING SECTIONS
A11 (N) MATERIALS & FINISH SELECTIONS

SURVEYOR SHEETS
SHEET # DRAWING
BOUNDARY AND TOPOGRAPHIC SURVEY

PROJECT DIRECTORY
OWNER: RORIE & JACKELINE OVERBY
932 PEGGY LANE
MENLO PARK, CA 94025
ARCHITECT: DORMAN ASSOCIATES, INC.
CHRIS DORMAN, AIA
229 FLAMINGO ROAD
MILL VALLEY, CA 94641
P: 415.380.7914
F: 415.380.7915
ENERGY CONSULTANT: TIMOTHY CARSTAIRS
CARSTAIRS ENERGY INC.
2238 BAYVIEW HEIGHTS DRIVE, SUITE E
LOS OSOS, CA 93402
TITLE2@MWHARD.COM
P: 805.804.9349
CIVIL ENGINEER: ROMIG ENGINEERS
1390 EL CAMINO REAL
SECOND FLOOR
SAN CARLOS, CA 94070
P: 650.591.5224
SURVEY: L. WADE HAMMOND
LAND SURVEYING
26660 NEWARK BLVD, SUITE C
NEWARK, CA 94560
P: 510.579.6112

NOTE: AN ENCROACHMENT PERMIT FROM THE ENGINEERING DIVISION IS REQUIRED PRIOR TO ANY CONSTRUCTION ACTIVITIES, INCLUDING UTILITY LATERALS IN THE PUBLIC RIGHT OF WAY.
NOTE: THIS PROJECT COMPLIES WITH THE CRITERIA OF THE WATER CONSERVATION IN LANDSCAPE ORDINANCE AND HAVE APPLIED THEM FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE AND IRRIGATION DESIGN PLAN.
A 3" LAYER OF MULCH SHALL BE APPLIED TO ALL EXPOSED PLANTING SURFACES W/ THE EXCEPTION OF TURF.

COMMUNITY DEVELOPMENT DEPARTMENT
PLANNING DIVISION
791 Laurel Street
Menlo Park, CA 94025
phone: (650) 338-6702
fax: (650) 327-1653
planning@menlopark.org
http://www.menlopark.ca.gov

DATA SHEET
Please provide the appropriate information pertaining to your application. It is important to complete the existing and proposed development items even if the existing structure is being demolished or if there is no specific zoning ordinance requirement.

LOCATION:	932 Peggy Lane					
EXISTING USE:	Single Family Residential					
APPLICANT:	Andrea Montalbano					
PROPOSED USE:	Single Family Residential					
PROPERTY OWNER(S):	Jackeline and Rorie Overby					
ZONING:	R-1-U					
APPLICATION(S):	Use Permit					
DEVELOPMENT STANDARDS	PROPOSED PROJECT	EXISTING DEVELOPMENT	ZONING ORDINANCE			
Lot area	6,491 sf	6,491 sf	7,000 sf min.			
Lot width	80 ft	80 ft	60 ft min.			
Lot depth	109.92 ft	109.92 ft	100 ft min.			
Setbacks						
Front	24'-1" ft	24'-10" ft	20'-0" ft min.			
Rear	30'-4" ft	30'-4" ft	20'-0" ft min.			
Side (left)	6'-3" ft	6'-3" ft	5'-0" ft min.			
Side (right)	6'-7" ft	6'-7" ft	5'-0" ft min.			
Building coverage	110.00 %	152.72 %	1922 SF max.			
FAR (Floor Area Ratio)	35 %	28.67 %	30 % max.			
FAL (Floor Area Limit)	2786.5 SF	1377 SF	2,800 SF			
Square footage by floor						
below grade	0 SF	0 SF	0 SF			
1st	1,160 SF	1,128 SF	1,128 SF			
2nd	630 (SACTUAL) 603 (SPLAN)	0 SF	0 SF			
above grade	257 SF	254.5 SF	254.5 SF			
above ground (total)	887 SF	1,382.5 SF	1,382.5 SF			
total	887 SF	1,382.5 SF	1,382.5 SF			
Structure coverage of building	97.9 (SACTUAL) 2786 (SPLAN)	1077 SF	2800 SF max.			
Building height	25'-6" ft	18'-6" ft	28'-0" ft max.			
Landscaping**	0	0	0			
Paving***	0	0	0			
Parking	0	0	0			
Vehicle spaces for parking	1 COVERED SPACES	1 COVERED SPACES	1 COVERED SPACES			
(Example: 1 covered/1 uncovered per residential unit or 2 of 5000 Square Feet)						
Trees	# of existing Heritage trees	3	# of non-Heritage trees	2	# of new trees	0
	# of existing Heritage trees to be removed	0	# of non-Heritage trees to be removed	0	Total # of trees	0

* Commercial and Multiple-residential properties | ** Single Family residential and R-2 zoned properties | *** Commercial, Multiple-residential, and R-2 zoned properties
V:\M\00173\Approved\CDP\Draw.dwg
11/29/22

Construction Best Management Practices (BMPs)

Construction projects are required to implement the stormwater best management practices (BMP) on this page, as they apply to your project, all year long.

Materials & Waste Management

- Non-Hazardous Materials**
- Store and cover stockpiles of sand, dirt or other construction material with tarps when rain is forecast or if not actively being used within 14 days.
 - Use dust control measures to prevent wind-blown dust.

- Hazardous Materials**
- Label all hazardous materials and hazardous wastes (such as petroleum, paints, thinners, solvents, fuel, oil, and antifreeze) in accordance with city, county, state and federal regulations.
 - Store hazardous materials and wastes in water-tight containers, use appropriate secondary containment, and cover them at the end of every work day or during wet weather or when rain is forecast.
 - Follow manufacturer's application instructions for hazardous materials and be careful not to use more than necessary. Do not apply chemicals unless when rain is forecast within 24 hours.
 - Arrange for appropriate disposal of all hazardous wastes.

- Waste Management**
- Cover waste disposal containers securely with tarps at the end of every work day and during wet weather.
 - Check waste disposal containers frequently for leaks and to make sure they are not overflowing. Never hose down a dumpster on the construction site.
 - Clean or replace portable toilets, and inspect them frequently for leaks and spills.
 - Dispose of all waste and debris properly. Recycle materials and wastes that can be recycled (such as asphalt, concrete, aggregate base materials, wood, dry board, etc.).
 - Dispose of liquid wastes from paints, thinners, solvents, glues, and cleaning fluids as hazardous waste.

- Construction Entrances and Perimeter**
- Establish and maintain effective perimeter controls and stabilize all construction entrances and exits to effectively control erosion and sediment discharge from site and tracking off site.
 - Stump or vacuum any snow tracking immediately and vacuum sediment control to prevent further tracking. Never hose down drums to clean up tracking.

Equipment Management & Spill Control

- Maintenance and Parking**
- Disrupt as few areas as possible with equipment. BMPs for vehicle and equipment parking and storage.
 - Perform major maintenance, repair jobs and vehicle and equipment washing off site.
 - If vehicles or vehicle maintenance areas do leak fluids, wash in a hatched area away from storm drains and cover the spill with a tarp that is big enough to contain fluids. Recycle or dispose of fluids as hazardous waste.
 - If fluids or equipment cleaning must be done on-site, clean with water only in a hatched area that will not affect flow water to any site (pipes, ditches, storm drains, or surface waters).
 - Do not clean vehicles or equipment areas using soap, solvents, degreasers, or other cleaning equipment.

- Spill Prevention and Control**
- Keep spill cleanup materials (e.g., rags, absorbents and catch basins) available at the construction site at all times.
 - Inspect vehicles and equipment frequently for fuel and coolant leaks promptly. Use drip pans to catch leaks until repairs are made.
 - Clean up spills or leaks immediately and dispose of cleanup materials properly.
 - Do not hose down surfaces where fluids have spilled. Use dry cleanup materials (absorbent materials, cat litter, sand or sawdust).
 - Store up spilled dry materials immediately. Do not try to wash them away with water or into drains.
 - Clean up spills on dirt areas by digging up and properly disposing of contaminated soil.
 - Report significant spills immediately. You are required by law to report all significant releases of hazardous materials, including oil. To report a spill, 1) Dial 911 or your local emergency response number, 2) Call the Governor's Office of Emergency Services/Vacation Cares, (800) 852-7350 (24 hours).

Earthmoving



- Schedule grading and excavation work during dry weather.
- Stabilize all denuded areas, install and maintain temporary erosion controls (such as erosion control fabric, or hatched areas) until permanent erosion controls (such as native seed) vegetation is established.
- Remove existing vegetation only when absolutely necessary and seed or plant vegetation for erosion control on slopes or where construction is not immediately planned.
- Prevent sediment from migrating offsite and prevent storm drain inlets, gutters, ditches, and drainage courses by installing and maintaining appropriate BMPs, such as filter socks, silt fences, sediment basins, or other erosion control measures.
- Keep excavated soil on site and transfer it to dump trucks on site, not in the streets.

- Contaminated Soils**
- At any of the following conditions are observed, test for contamination and contact the Regional Water Quality Control Board:
 - Unusual soil conditions, discoloration, or odor.
 - Abandoned underground tanks.
 - Abandoned wells.
 - Barrel barrels, debris, or trash.
 - Protect recognized landscaping materials from theft and rain by storing them under tarps all year-round.
 - Stack bagged material on pallets and under cover.
 - Document applications of any available, suitable material within 2 days before a forecast rain event or during wet weather.

Paving/Asphalt Work



- Avoid paving and seal coating to wet weather or when rain is forecast to prevent materials that have not cured from contacting stormwater runoff.
- Cover storm drain inlets and manholes when applying seal coat, tack coat, slurry seal, fog seal, etc.
- Collect and recycle or appropriately dispose of excess slurry seal material or seal coat.
- Do NOT spray or wash seal coats.
- Do not use water to wash down fresh asphalt concrete pavement.

- Sealing and Asphalt Concrete Removal**
- Prevent nearby street dust when seal coating. Use dry fabric, catch basin clean sheets, or ground logs to keep slurry away from the storm drain system.
 - Shovel, absorb, or vacuum wet seal slurry and dispose of all waste in a curb or area not included in the location or at the end of each work day (whichever is sooner).
 - If excess slurry enters a catch basin, clean it up immediately.

Concrete, Grout & Mortar Application



- Store concrete, grout, and mortar away from storm drains or waterways, and do not allow under cover to prevent flows from rain, runoff, and wind.
- Work on concrete equipment/trucks offsite or in a designated washout area, where the water will flow into a temporary water pit and a drainage that will prevent leaching into the underlying soil or into surrounding areas. Let concrete harden and dispose of as garbage.
- When washing exposed aggregate, prevent wastewater from entering storm drains. Back any culms and vacuum gutters, hose wastewater into dirt area, or drain into a hatched surface to be pumped and disposed of properly.

Landscaping



- Protect recognized landscaping materials from theft and rain by storing them under tarps all year-round.
- Stack bagged material on pallets and under cover.
- Document applications of any available, suitable material within 2 days before a forecast rain event or during wet weather.

Painting & Paint Removal



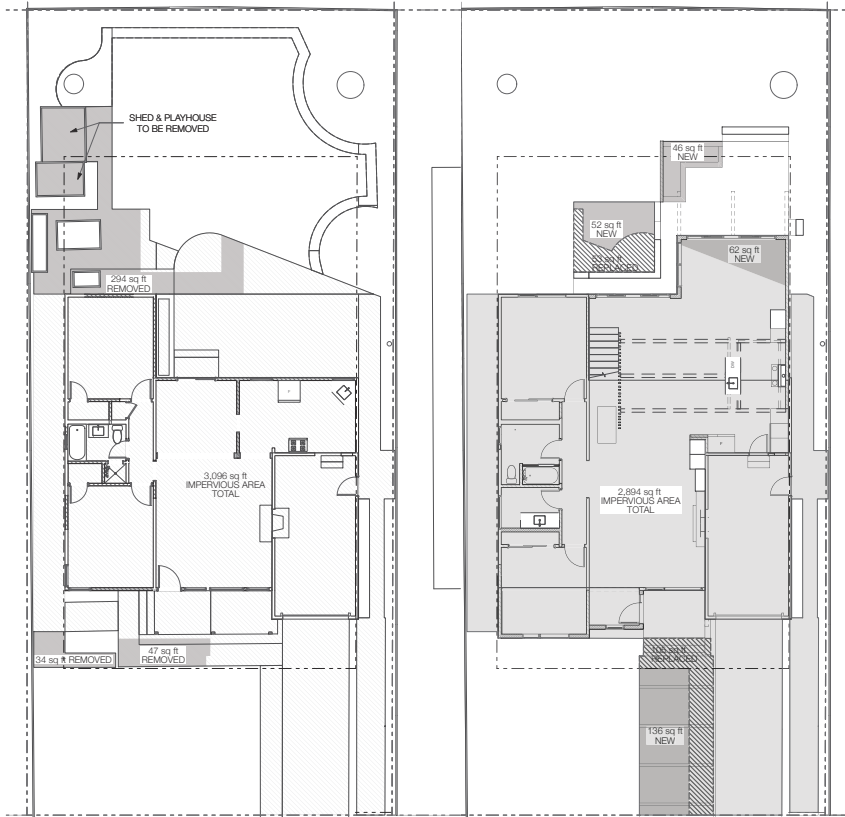
Painting Cleanup and Removal

- Never clean brushes or reuse paint containers into a storm, gutter, storm drain, or stream.
- For water-based paints, paint out brushes to the extent possible, and reuse into a drum that goes to the sanitary sewer. Never pour paint down a storm drain.
- For oil-based paints, paint out brushes to the extent possible and clean with thinner or solvent in a temporary water pit and a drainage that will prevent leaching into the underlying soil or into surrounding areas. Let concrete harden and dispose of as garbage.
- Paint chips and dust from non-hazardous dry stripping and sandblasting may be swept up or collected in plastic drop cloth and disposed of as hazardous waste.
- Clean paint trays, rollers and chips and dust from motor paint or pump containing lead, mercury, or other toxic materials by sweeping or collecting in plastic drop cloth and disposed of as hazardous waste. Lead-based paint removal requires a specialized contractor.

Dewatering



- Discharge of groundwater or captured runoff from dewatering operations must be properly managed and disposed. When pumped, the dewatering discharge by landlocked area or sanitary sewer. If discharging to the sanitary sewer, call your local wastewater treatment plant.
- Cover any area from which runoff flows from disturbed areas.
- When dewatering, notify and obtain approval from the local municipality before discharging water to a storm drain or storm drain. Filtration or diversion through a filter sock, or similar trap may be required.
- In case of known or suspected contamination, call your local agency to determine whether the ground water must be tested. Pumped groundwater may need to be collected and held off site for treatment and proper disposal.



2 (E) IMPERVIOUS AREA
 SCALE 1/8" = 1'-0"

(E) IMPERVIOUS AREA TO BE REMOVED
 (N) IMPERVIOUS AREA
 (Hatched) IMPERVIOUS AREA (EXISTING)

1 (P) IMPERVIOUS AREA
 SCALE 1/8" = 1'-0"

(N) IMPERVIOUS AREA
 (Hatched) IMPERVIOUS AREA (REPLACED)
 (Dotted) IMPERVIOUS AREA TO REMAIN

Storm drain polluters may be liable for fines of up to \$10,000 per day!

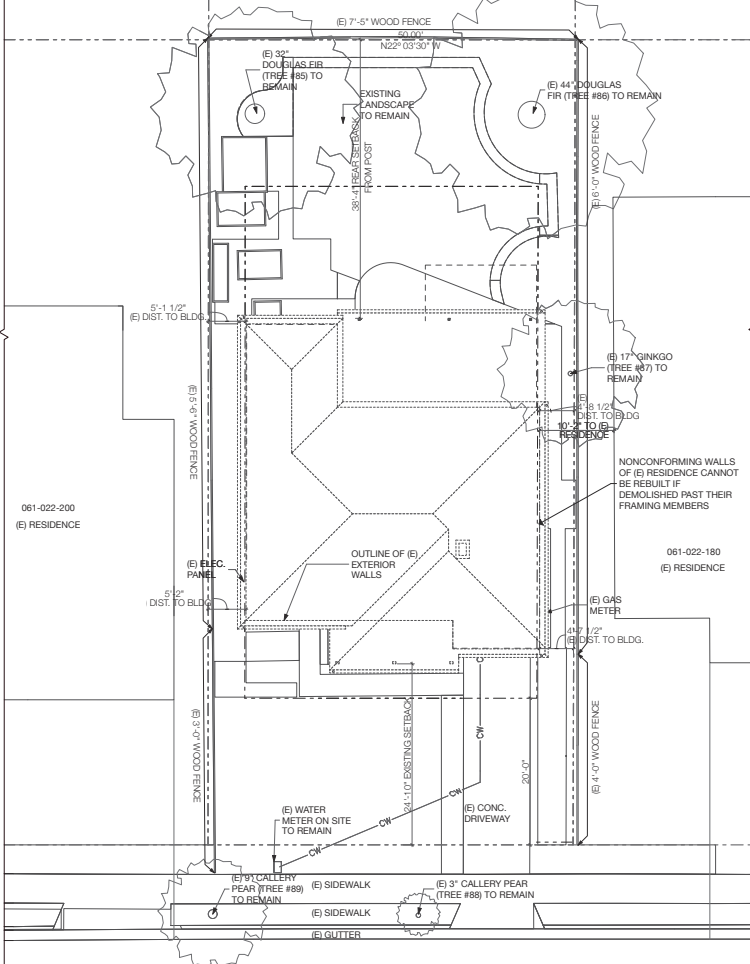
IMPERVIOUS AREA TABLE

Total Area of Parcel	A	5,491	ft ²
Existing Pervious Area	B	2,395	ft ²
Existing Impervious Area	C	3,096	ft ²
Existing % Impervious	D	56.4	%
Existing Impervious Area To Be Replaced W/ New Impervious Area	E	158	ft ²
Existing Pervious Area To Be Replaced W/ New Impervious Area	F	296	ft ²
New Impervious Area (Creating and/or Replacing)* *If greater than 10,000sqft, a hydrology report must be submitted	G	454	ft ²
Existing Impervious Area To Be Replaced W/ New Pervious Area	H	375	ft ²
Net Change In Impervious Area ¹	I	-79	ft ²
Proposed Pervious Area	J	2,474	ft ²
Proposed Impervious Area* *Verify that J + K = A	K	3,017	ft ²
Proposed % Impervious	L	55	%

REVISIONS

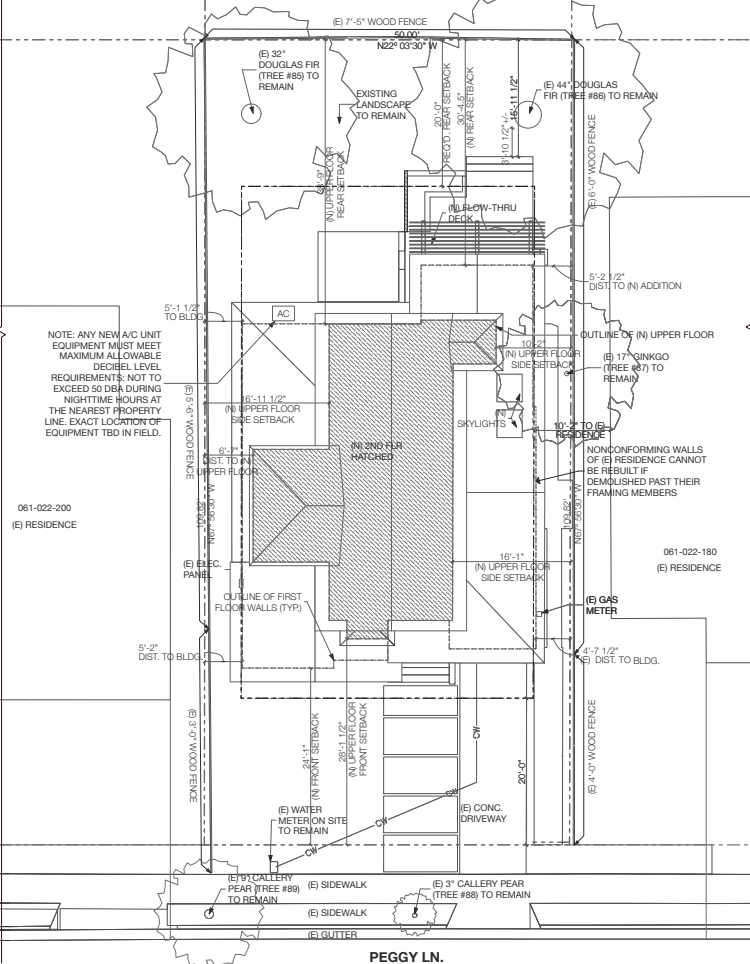
NOTE: UNDER NO CIRCUMSTANCE SHALL DRAINAGE RESULTING FROM THIS PROJECT, DURING OR POST CONSTRUCTION, DIRECTLY SHEETFLOW ACROSS AN ADJOINING PROPERTY. RUNOFF SHALL BE CONTAINED ON-SITE UP TO THE 10-YEAR STORM.

MAIN LEVEL EXISTING FLOOR AREAS	
495 + 281.5 + 298 + 48 (SHED) + 254.5 SF (GARAGE)	
(EXISTING) MAIN LEVEL TOTAL FAL	1,377 SF



2 (E) SITE PLAN
 SCALE: 1/8" = 1'-0"
 N

NOTE: UNDER NO CIRCUMSTANCE SHALL DRAINAGE RESULTING FROM THIS PROJECT, DURING OR POST CONSTRUCTION, DIRECTLY SHEETFLOW ACROSS AN ADJOINING PROPERTY. RUNOFF SHALL BE CONTAINED ON-SITE UP TO THE 10-YEAR STORM.



1 (N) SITE PLAN
 SCALE: 1/8" = 1'-0"
 N

OVERBY RESIDENCE
 932 PEGGY LANE
 MENLO PARK, CA 94025
 APN061-022-190

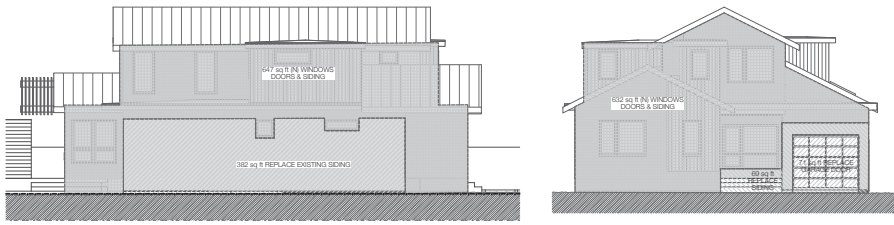
DESIGN DEVELOPMENT

ENLARGED SITE PLANS (E) & (N)

REVISIONS

DATE: 1/13/23
 SHEET

A2

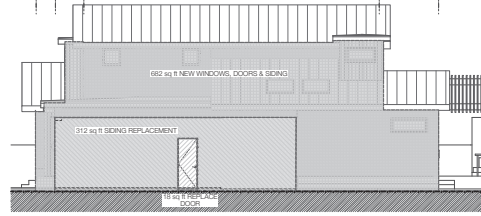


8 NORTH
SCALE: 1/8" = 1'-0"

6 WEST
SCALE: 1/8" = 1'-0"



7 EAST
SCALE: 1/8" = 1'-0"



5 SOUTH
SCALE: 1/8" = 1'-0"

932 Peggy Lane, Menlo Park, CA 94025

NONCONFORMING STRUCTURE - NEW WORK VALUE CALCULATION
 Address: 932 Peggy Lane, Menlo Park, CA 94025
 Case No.:
 50% of Existing Value \$116,690.00
 75% of Existing Value \$175,035.00
 Value of Proposed Project \$548,750.00 235%

Existing Development				
Non-Conforming Structure Type	Square Footage	Construction Cost	Existing Value	
Existing 1st floor (M1-M2+M3)	1074.5	X \$200/Sq.Ft	\$214,900.00	
Existing 2nd floor	0	X \$200/Sq.Ft	\$0.00	
Existing Basement	0	X \$200/Sq.Ft	\$0.00	
Existing Garage (M4)	254	X \$70/Sq.Ft	\$18,480.00	
Total	1338.5		\$233,380.00	
Proposed Development				
Proposed Development Type	Square Footage	Construction Cost	Development Value	
Category 1: New square footage (areas of new foundation and/or wall framing)				
1st Flr Addition (M5+M6+M7+M8)	548	X \$200/Sq.Ft	\$109,600.00	
2nd Flr Addition (sum of U1 thru U9 +M10 thru M12)	903.5	X \$200/Sq.Ft	\$180,700.00	
Basement Floor (Wine Cellar) Addition	25	X \$200/Sq.Ft	\$5,000.00	
Garage Addition	0	X \$70/Sq.Ft	\$0.00	
Category 2: Remodel of existing square footage, foundation and wall framing are both retained)				
Remodel of Kitchen (10'-10"X15'-11")	173	X \$130/Sq.Ft	\$22,490.00	
Remodel of Bathrooms (7'-11" X 8'-3 1/2")	65	X \$130/Sq.Ft	\$8,450.00	
Remodel of Other Living Areas (1883-65-173-257)	1388	X \$100/Sq.Ft	\$138,800.00	
Remodel of Garage	0	X \$35/Sq.Ft	\$0.00	
Category 3: Exterior modifications to existing structure				
New Roof Structure Over Existing Sq. Ft. (BLDG COVERAGE - NEW 2ND FLR)	1019	X \$50/Sq.Ft	\$50,950.00	
Replacement of Existing Windows/Exterior Doors	89	X \$35/Sq.Ft	\$3,115.00	
Replacement of Existing Siding	847	X \$35/Sq.Ft	\$29,645.00	
Total	5957.5		\$548,750.00	

4 NONCONFORMING WORKSHEET

(PROPOSED) UPPER LEVEL FLOOR AREAS (& AREA >12' HT)

U1	(5.96' X 5.79')	34.5 SF
U2	(22.38' X 12.13')	271 SF
U3	(16.2' X 17.9')	290 SF
U4	(15.2' X 10.83')	165 SF
U5	(4.08' X 4.25')	17 SF
U6	(5.42' X 2.5)	14 SF
U7	(1.96' X 16.66')	33 SF
U8	(7.96' X .46')	4 SF
U9	(.5' X 4.13')	2 SF
M10	(4.5' X 4.54')	20 SF
M11	(3.79' X 9')	34 SF
M12	(2.5' X 7.5')	19 SF
UPPER LEVEL TOTAL FAL		903.5 SF

(PROPOSED) MAIN LEVEL FLOOR AREAS

M1	(27.58' X 10.28')	284 SF
M2	(12.29' X 40.29')	495 SF
M3	(15.92' X 18.7')	298 SF
M4	(22.58' X 11.66'-6)	257 SF
M5	(15.13' X 8')	121 SF
M6	(27.2' X 11.38')	310 SF
M7	(6.46' X 12.29')	79 SF
M8	(7.375' X 5.29')	39 SF
(PROPOSED) MAIN LEVEL TOTAL FAL		1883 SF

FLOOR AREA LIMIT CALCULATION

UPPER LEVEL TOTAL FAL	903.5 SF
MAIN LEVEL TOTAL	1,883 SF
TOTAL FAL	2,786.5 SF
MAX. ALLOWABLE = 2,800 sf	

(PROPOSED) MAIN LEVEL FLOOR ADD'L AREAS

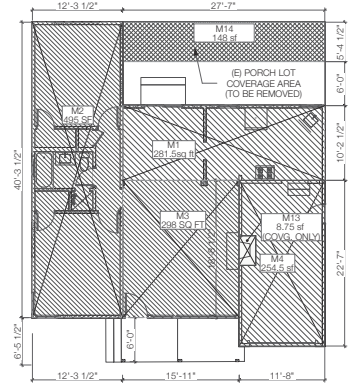
M9 (COV'G ONLY)	(8.46' X 3.88')	33 SF
M13 (COV'G ONLY)	(4' x 1.42')	6 SF
WINE CELLAR (NOT COUNTED)	(5' x 5')	25 SF

PROPOSED BUILDING COVERAGE AREAS

MAIN LEVEL BLDG. COVERAGE	1,883 SF
+M9 & +M13	39 SF
TOTAL PROPOSED COVERAGE	1,922 SF
MAX. ALLOWABLE = (35% of 5,491) = 1,922 sf	

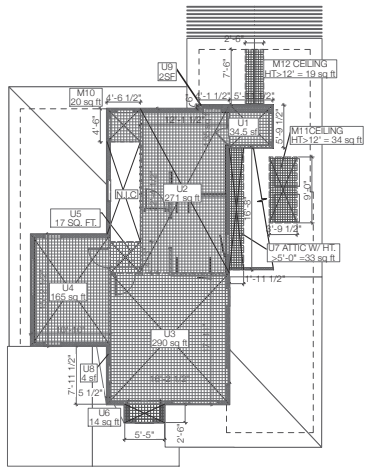
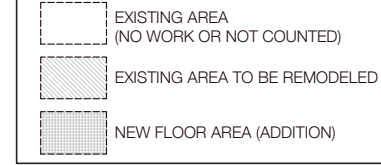
MAIN LEVEL EXISTING FLOOR AREAS

495 + 281.5 + 298 + 48 (SHED) + 254.5 SF (GARAGE)	
(EXISTING) MAIN LEVEL TOTAL FAL	1,377 SF

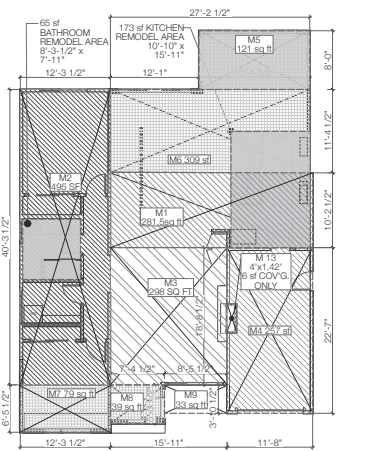


3 FLOOR AREA @ EXISTING PLAN
SCALE: 1/8" = 1'-0"

DIAGRAM LEGEND



2 FLOOR AREA PROPOSED UPPER LEVEL
SCALE: 1/8" = 1'-0"



1 FLOOR AREA PROPOSED MAIN LEVEL
SCALE: 1/8" = 1'-0"

Dorman Associates
 CHRIS DORMAN, AIA
 229 FLAMINGO ROAD
 MILL VALLEY, CA 94541
 415.380.7914
 415.380.7915 FAX
 CD@DORMANASSOCIATES.COM

OVERBY RESIDENCE
 932 PEGGY LANE
 MENLO PARK, CA 94025
 APP#061-022-190

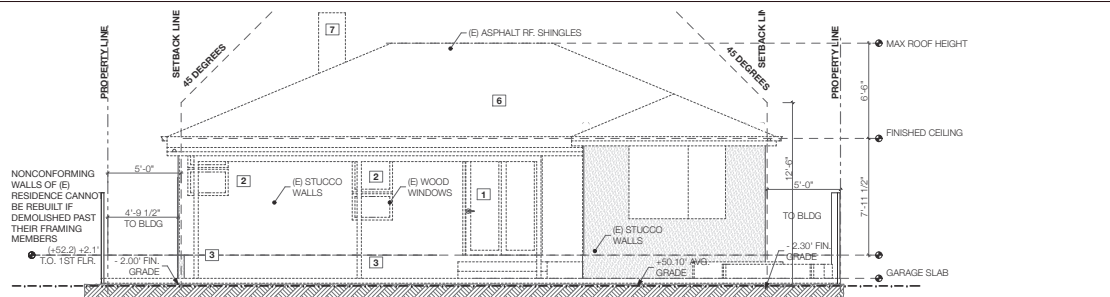
DESIGN DEVELOPMENT

FAL & LOT CALCULATIONS

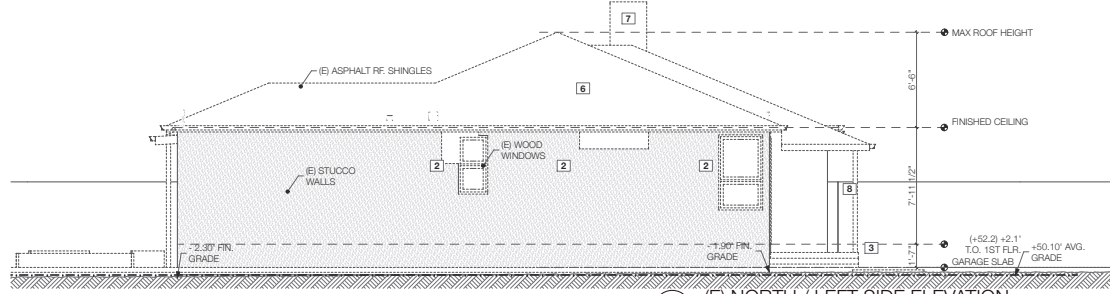
REVISIONS

DATE: 11/29/22
 SHEET

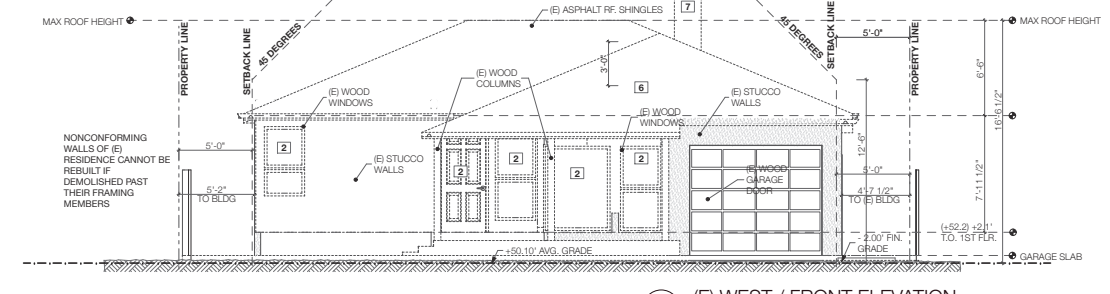
A3



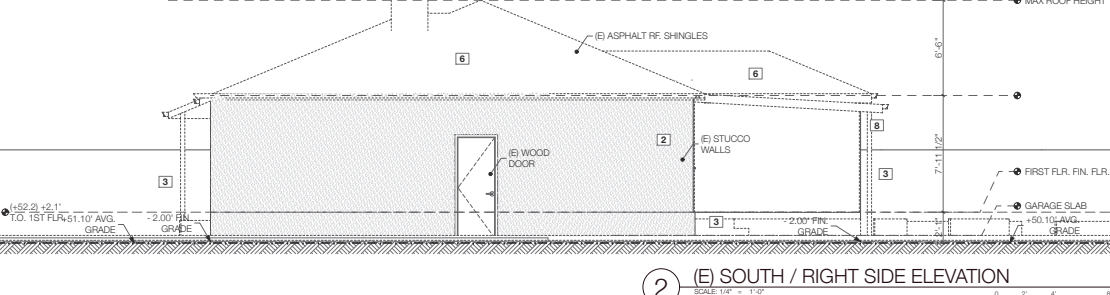
5 (E) EAST / REAR ELEVATION
 SCALE 1/4" = 1'-0"



4 (E) NORTH / LEFT SIDE ELEVATION
 SCALE 1/4" = 1'-0"



3 (E) WEST / FRONT ELEVATION
 SCALE 1/4" = 1'-0"



2 (E) SOUTH / RIGHT SIDE ELEVATION
 SCALE 1/4" = 1'-0"

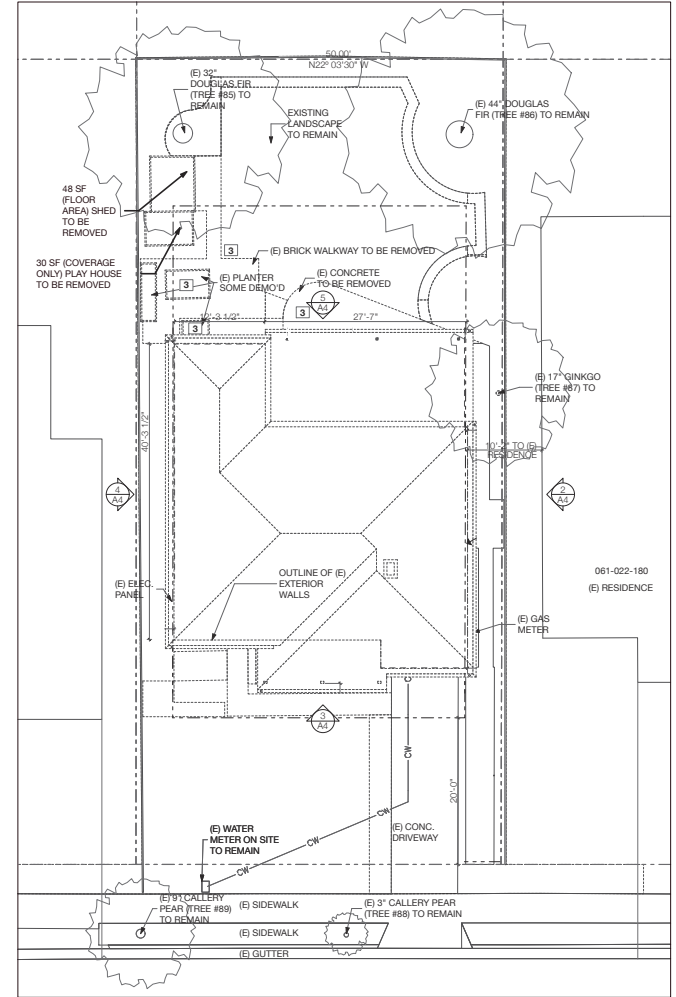
- GENERAL DEMO NOTES**
1. REMOVE INTERIOR WALLS WHERE SHOWN DASHED.
 2. PREPARE NEW WALLS FOR NEW WINDOWS.
 3. REMOVE DOORS WHERE SHOWN DASHED.
 4. ALL DEMOLITION IS TO COMPLY WITH CITY OF MENLO PARK CONSTRUCTION & DEMOLITION PRACTICES FOR REMOVAL AND RECYCLING OF MATERIALS.
 5. TAKE CARE TO AVOID REMOVING ANY STRUCTURAL MEMBERS.
 6. NONCONFORMING WALLS OF (E) RESIDENCE CANNOT BE REBUILT IF DEMOLISHED PAST THEIR FRAMING MEMBERS.
- DEMOLITION SCOPE**
1. REMOVE EXISTING WALL FINISHES, PATCH & REPAIR SHEETROCK AS NECESSARY.
 2. REMOVE EXISTING PARTITIONS, DOORS, CASEWORK, AND WINDOWS WHERE SHOWN DASHED.
 3. REMOVE EXISTING EXTERIOR STAIRS, POSTS, DECK & SITE WORK AS SHOWN.
 4. REMOVE EXISTING PLUMBING FIXTURES, CAP PLUMBING AS NEEDED.
 5. REMOVE (E) ELECTRICAL OUTLETS, CAP AS NEEDED.
 6. REMOVE (E) ROOF WHERE SHOWN DASHED.
 7. REMOVE (E) CHIMNEY & FIREBOX WHERE SHOWN DASHED.
 8. REMOVE (E) WOOD POSTS

WALL LEGEND

	EXISTING WALL TO REMAIN
	EXISTING WALL TO BE REMOVED

MAIN LEVEL EXISTING FLOOR AREAS

500 + 283 + 297 + 48 (SHED) = 1,128 SF
256 SF = GARAGE
MAIN LEVEL TOTAL FAL 1,384 SF



1 (E) SITE DEMO PLAN
 SCALE 1/8" = 1'-0"

OVERBY RESIDENCE
 932 PEGGY LANE
 MENLO PARK, CA 94025
 APN091-022-190

PLANNING SUBMITTAL

(E) ELEVATIONS & SITE DEMO PLAN

REVISIONS

DATE: 11/29/22
 SHEET

A4

DEMOLITION SCOPE

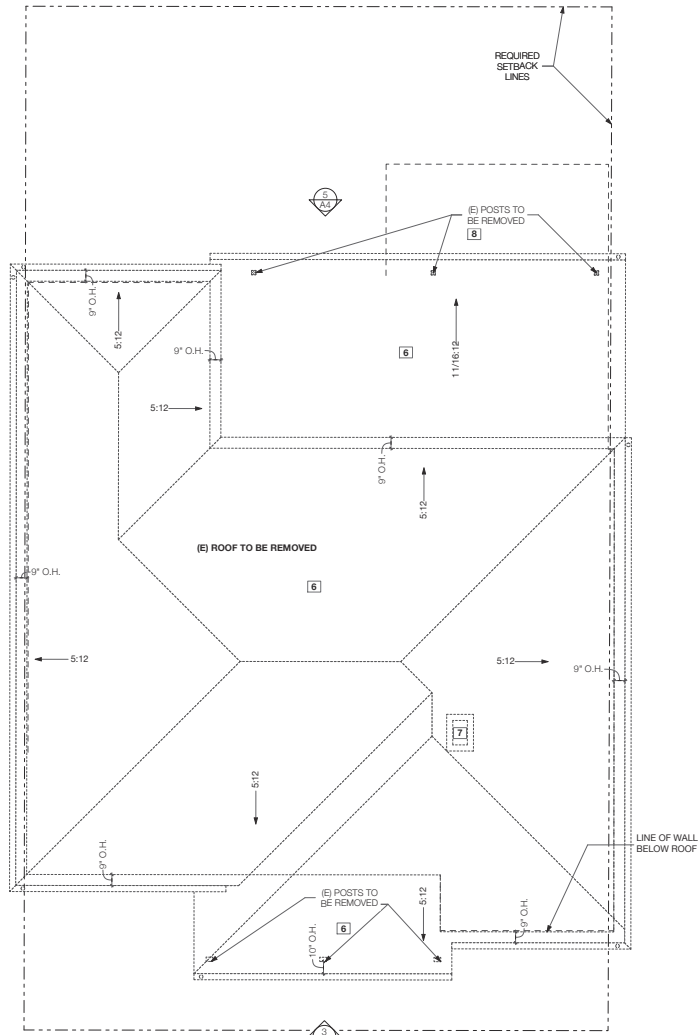
- 1 REMOVE EXISTING WALL FINISHES. PATCH & REPAIR SHEETROCK AS NECESSARY.
- 2 REMOVE EXISTING PARTITIONS, DOORS, CASEWORK, AND WINDOWS WHERE SHOWN DASHED.
- 3 REMOVE EXISTING EXTERIOR STAIRS, POSTS, DECK & SITE WORK AS SHOWN.
- 4 REMOVE EXISTING PLUMBING FIXTURES, CAP PLUMBING AS NEEDED.
- 5 REMOVE (E) ELECTRICAL OUTLETS, CAP AS NEEDED.
- 6 REMOVE (E) ROOF WHERE SHOWN DASHED.
- 7 REMOVE (E) CHIMNEY & FIREBOX WHERE SHOWN DASHED.
- 8 REMOVE (E) WOOD POSTS

GENERAL DEMO NOTES

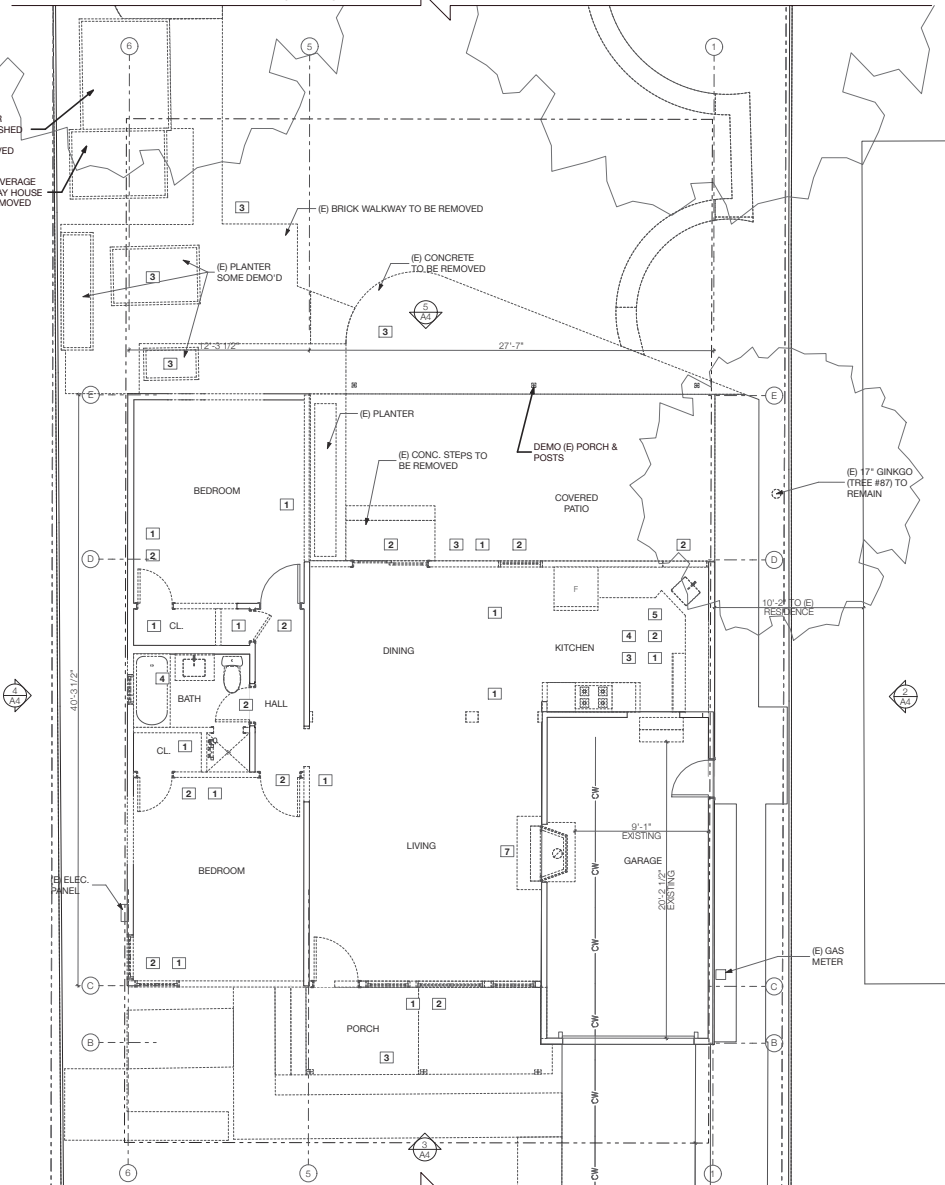
1. REMOVE INTERIOR WALLS WHERE SHOWN DASHED.
2. PREPARE NEW WALLS FOR NEW WINDOWS.
3. REMOVE DOORS WHERE SHOWN DASHED.
4. ALL DEMOLITION IS TO COMPLY WITH CITY OF MENLO PARK CONSTRUCTION & DEMOLITION PRACTICES FOR REMOVAL AND RECYCLING OF MATERIALS.
5. TAKE CARE TO AVOID REMOVING ANY STRUCTURAL MEMBERS.
6. NONCONFORMING WALLS OF (E) RESIDENCE CANNOT BE REBUILT IF DEMOLISHED PAST THEIR FRAMING MEMBERS.

WALL LEGEND

- EXISTING WALL TO REMAIN
- EXISTING WALL TO BE REMOVED



2 ROOF DEMO PLAN
 SCALE 1/8\"/>



1 MAIN FLOOR DEMO PLAN
 SCALE 1/8\"/>

EXTERIOR FINISH SCHEDULE		
ID	DESCRIPTION	LOCATION
D-1	EXT. GARAGE DOOR - PAINTED METAL & FROSTED GLASS	AS SHOWN
D-2	ALUMINUM & GLASS EXTERIOR DOORS	TYPICAL UNLESS OTHERWISE NOTED
D-3	WOOD, FIBERGLASS OR STEEL EXT. DOOR	AS SHOWN
F-1	WD. COMP. WD. OR CEMENT BD. FASCIA	TYPICAL
G-1	RAMMED EARTH HARDSCAPE WALLS / C.I.P. BOARD FORMED CONCRETE	AS SHOWN
R-1	METAL ROOF	ALL ROOFS
S-1	STUCCO	EXTERIOR WALLS WHERE SHOWN
S-2	CHARRED CYPRESS VERTICAL SIDING	EXTERIOR WALLS WHERE SHOWN
SK-1	SKYLIGHT	ROOFS WHERE SHOWN
TR-1	WOOD TRELLIS TO MATCH CHARRED CYPRESS	AS SHOWN
TR-2	STAINED CEDAR OR MAHOGANY TRIM	AS SHOWN
W-1	ALUMINUM WINDOWS	AS SHOWN

Dorman Associates
 CHRIS DORMAN, AIA
 223 FLAMINGO ROAD
 MILL VALLEY, CA 94541
 415-380-7914
 415-380-7915 FAX
 CD@DORMANASSOCIATES.COM

OVERBY RESIDENCE
 932 PEGGY LANE
 MENLO PARK, CA 94025
 APN061-022-190

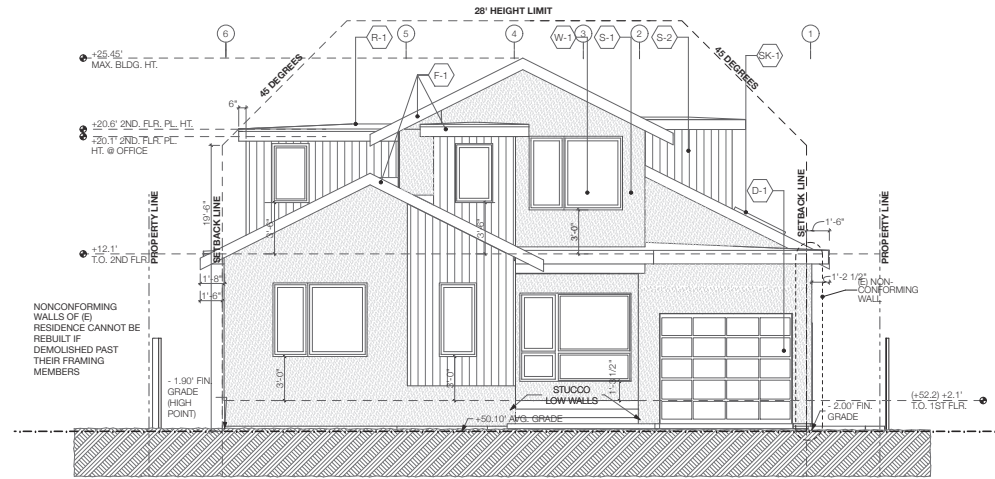
PLANNING SUBMITTAL

(N) ELEVATIONS - WEST & EAST

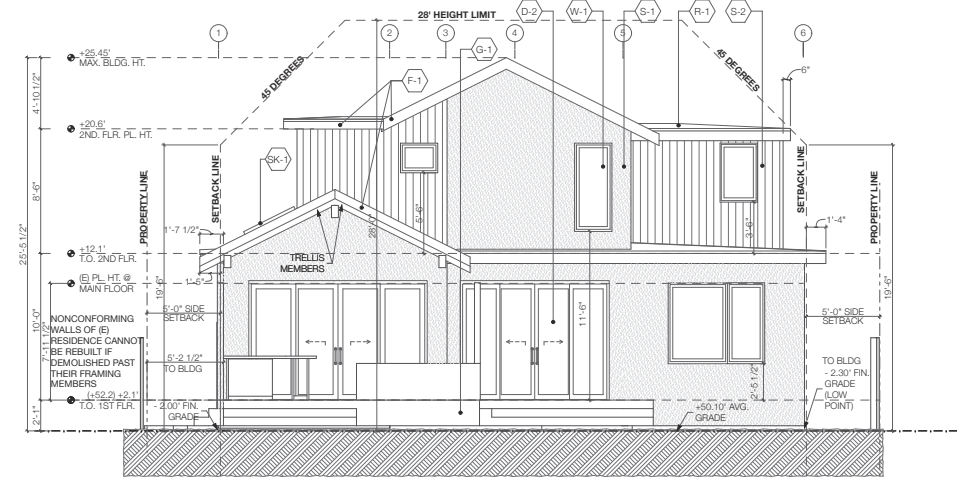
REVISIONS

DATE: 11/29/22
 SHEET

A8

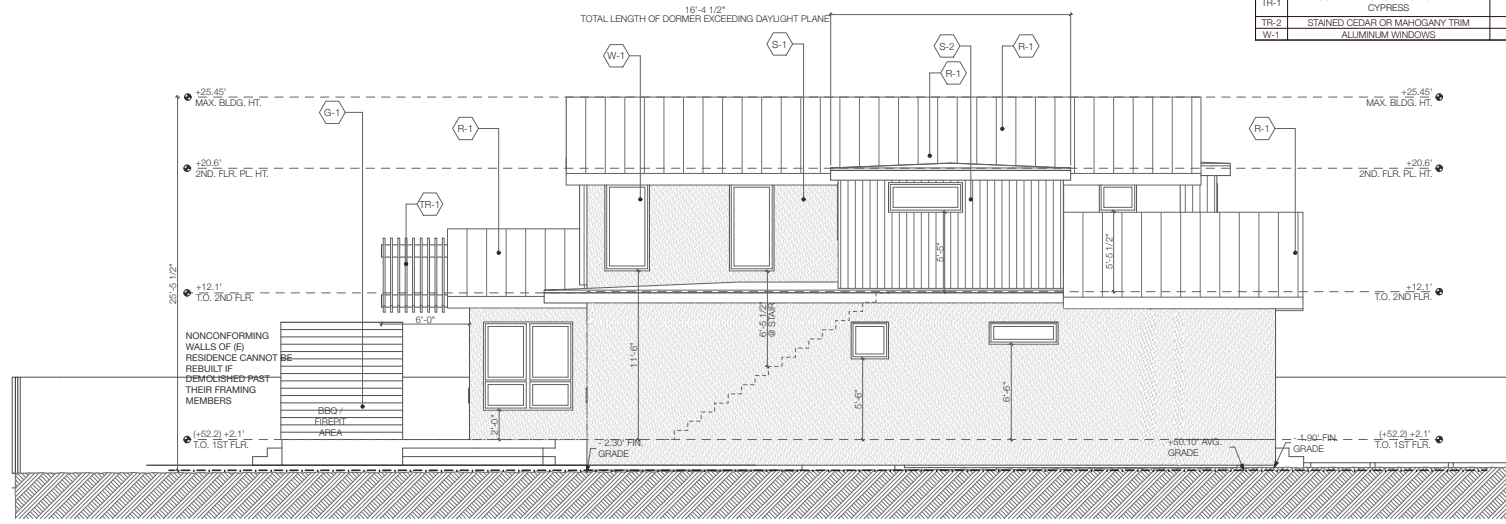


② (N) WEST / FRONT ELEVATION
 SCALE 1/4" = 1'-0"

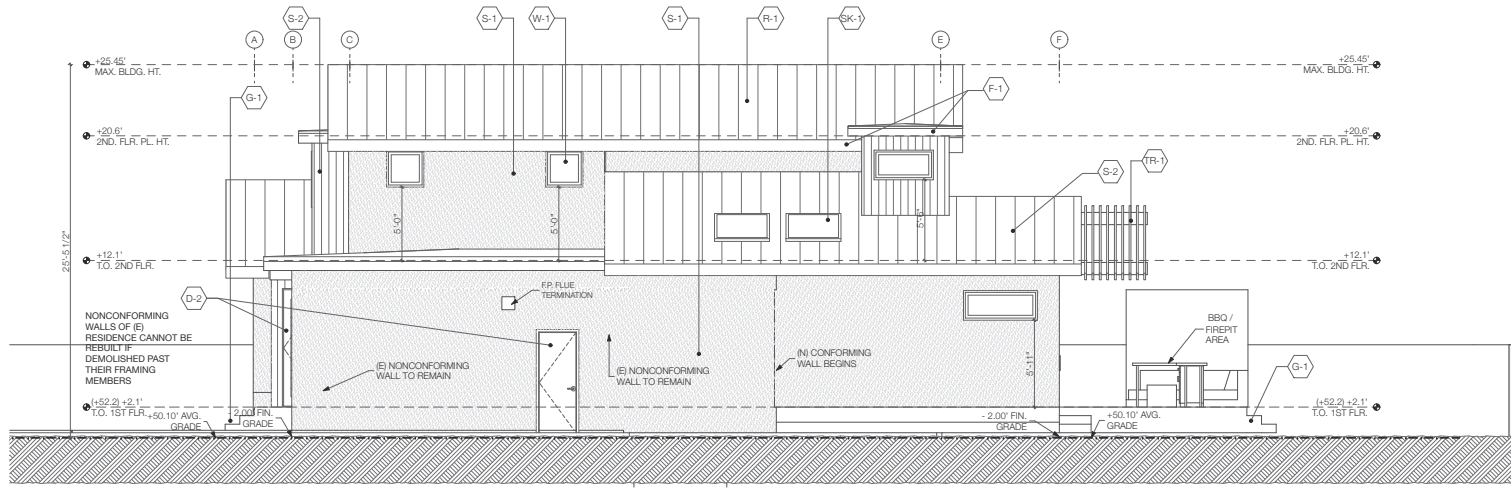


① (N) EAST / REAR ELEVATION
 SCALE 1/4" = 1'-0"

EXTERIOR FINISH SCHEDULE		
ID	DESCRIPTION	LOCATION
D-1	EXT. GARAGE DOOR - PAINTED METAL & FROSTED GLASS	AS SHOWN
D-2	ALUMINUM & GLASS EXTERIOR DOORS	TYPICAL UNLESS OTHERWISE NOTED
D-3	WOOD, FIBERGLASS OR STEEL EXT. DOOR	AS SHOWN
F-1	WD, COMP. WD. OR CEMENT BD. FASCIA	TYPICAL
G-1	FRAMED EARTH HARDSCAPE WALLS / C.I.P. BOARD FORMED CONCRETE	AS SHOWN
R-1	METAL ROOF	ALL ROOFS
S-1	STUCCO	EXTERIOR WALLS WHERE SHOWN
S-2	CHARRED CYPRESS VERTICAL SIDING	EXTERIOR WALLS WHERE SHOWN
SK-1	SKYLIGHT	ROOFS WHERE SHOWN
TR-1	WOOD TRELLIS TO MATCH CHARRED CYPRESS	AS SHOWN
TR-2	STAINED CEDAR OR MAHOOGANY TRIM	AS SHOWN
W-1	ALUMINUM WINDOWS	AS SHOWN



2 (N) NORTH / LEFT SIDE ELEVATION
SCALE: 1/4" = 1'-0"



1 (N) SOUTH / RIGHT SIDE ELEVATION
SCALE: 1/4" = 1'-0"

Dorman Associates
CHRIS DORMAN, AIA
225 FLAMINGO ROAD
MILL VALLEY, CA 94541
415-380-7914
415-380-7915 FAX
CDADORMANASSOCIATES.COM

OVERBY RESIDENCE
932 PEGGY LANE
MENDOCINO PARK, CA 94025
APP091-022-190

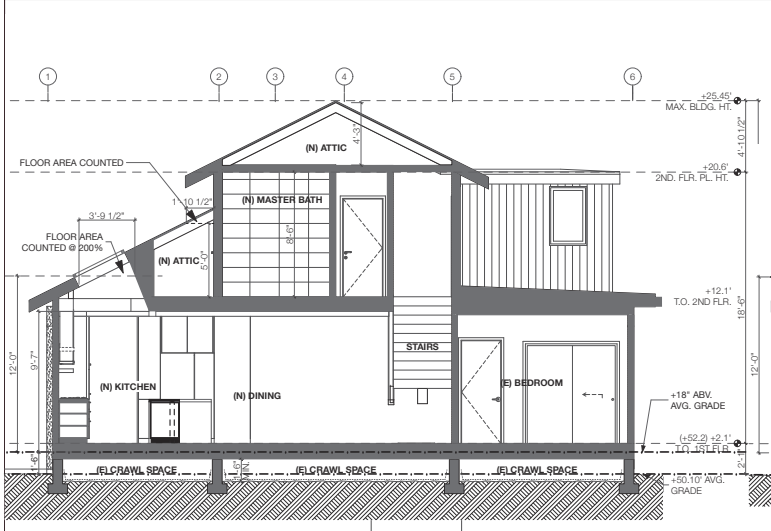
PLANNING SUBMITTAL

(N) ELEVATIONS - SOUTH & NORTH

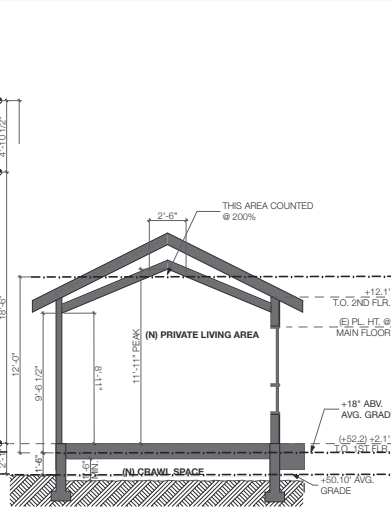
REVISIONS

DATE: 11/29/22
SHEET

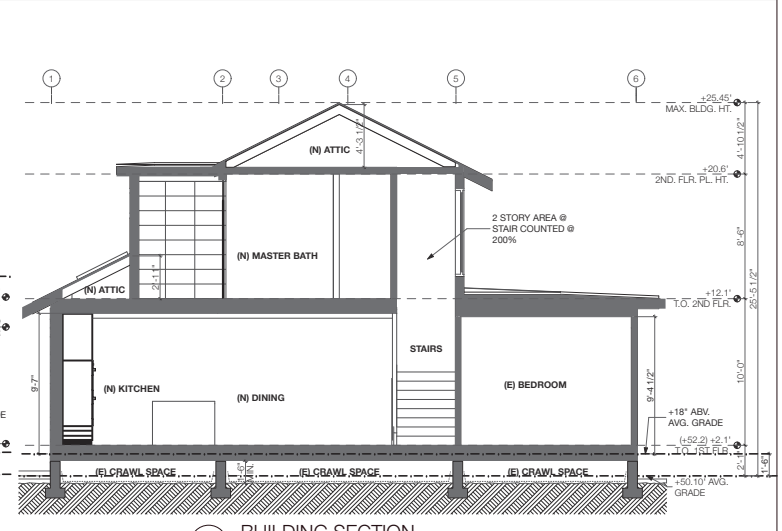
A9



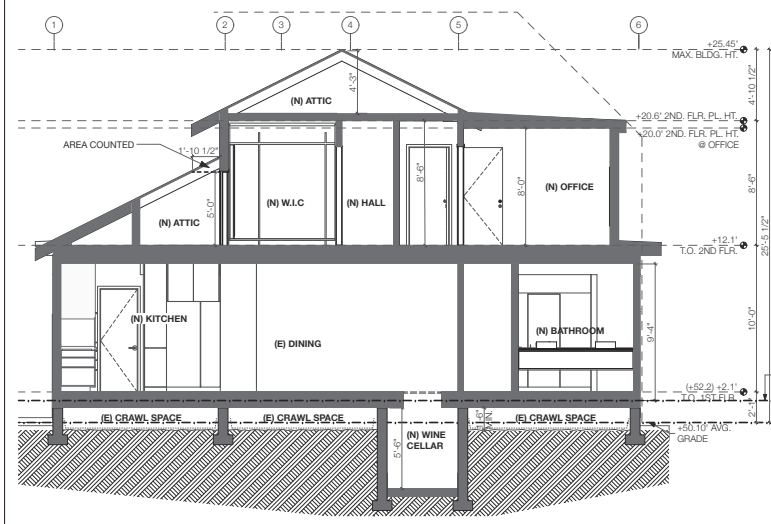
5 BUILDING SECTION
 SCALE: 1/4" = 1'-0"



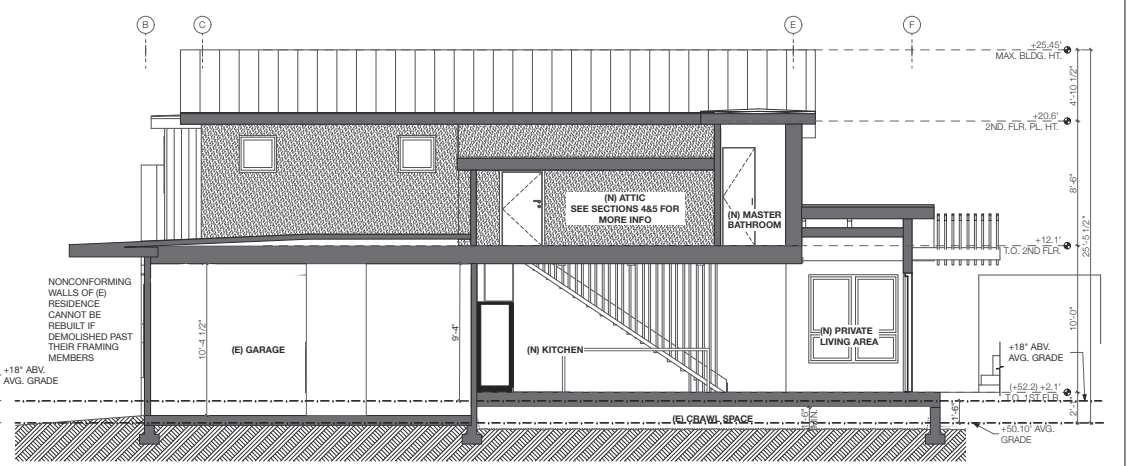
3 BUILDING SECTION
 SCALE: 1/4" = 1'-0"



2 BUILDING SECTION
 SCALE: 1/4" = 1'-0"



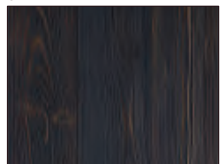









4 BUILDING SECTION
 SCALE: 1/4" = 1'-0"



1 BUILDING SECTION
 SCALE: 1/4" = 1'-0"

NOTES
 EXTERIOR DOWNLIGHTING TO BE RECESSED LED CANS
 EXTERIOR WALL LIGHTING TO BE LED SCONCES
 (DARK SKY COMPLIANT)

EXTERIOR FINISHES

<p>S-2</p>  <p>CHARRED CYPRESS VERTICAL SIDING, OR SIM.</p>	<p>TR-2</p>  <p>WHITE OAK HORIZONTAL BEAM, OR SIM.</p>
<p>R-1</p>  <p>METAL ROOF AND GUTTERS</p>	<p>F-1</p>  <p>METAL, COMP. WOOD OR PNTD. WOOD FASCIA</p>
<p>S-1</p>  <p>STUCCO - BEIGE, OR SIM. COLOR</p>	<p>G-1</p>  <p>RAMMED EARTH: HARDSCAPE BRICK WALLS - BEIGE, OR SIM. COLOR</p>
<p>Soffit - Solid Cem. Bd. Pntd., color to match fascia</p> 	<p>Soffit Inspiration - color to match fascia</p> 
<p>D-2/ D-3/ W-1</p>  <p>DOORS & WINDOWS - BLACK, OR SIM. COLOR (ALUMINUM OR FIBERGLASS)</p>	<p>D-1</p>  <p>GARAGE DOOR - PTD. STEEL & FROSTED GLASS</p>



OVERBY RESIDENCE
 932 PEGGY LANE
 MENLO PARK, CA 94025
 APN:081-022-190

PLANNING SUBMITTAL

(N) MATERIALS & FINISH SELECTIONS

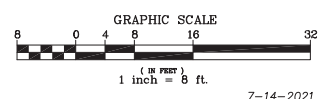
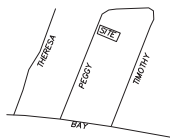
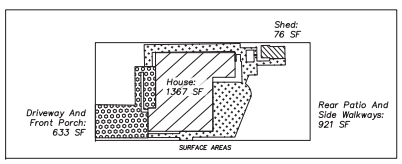
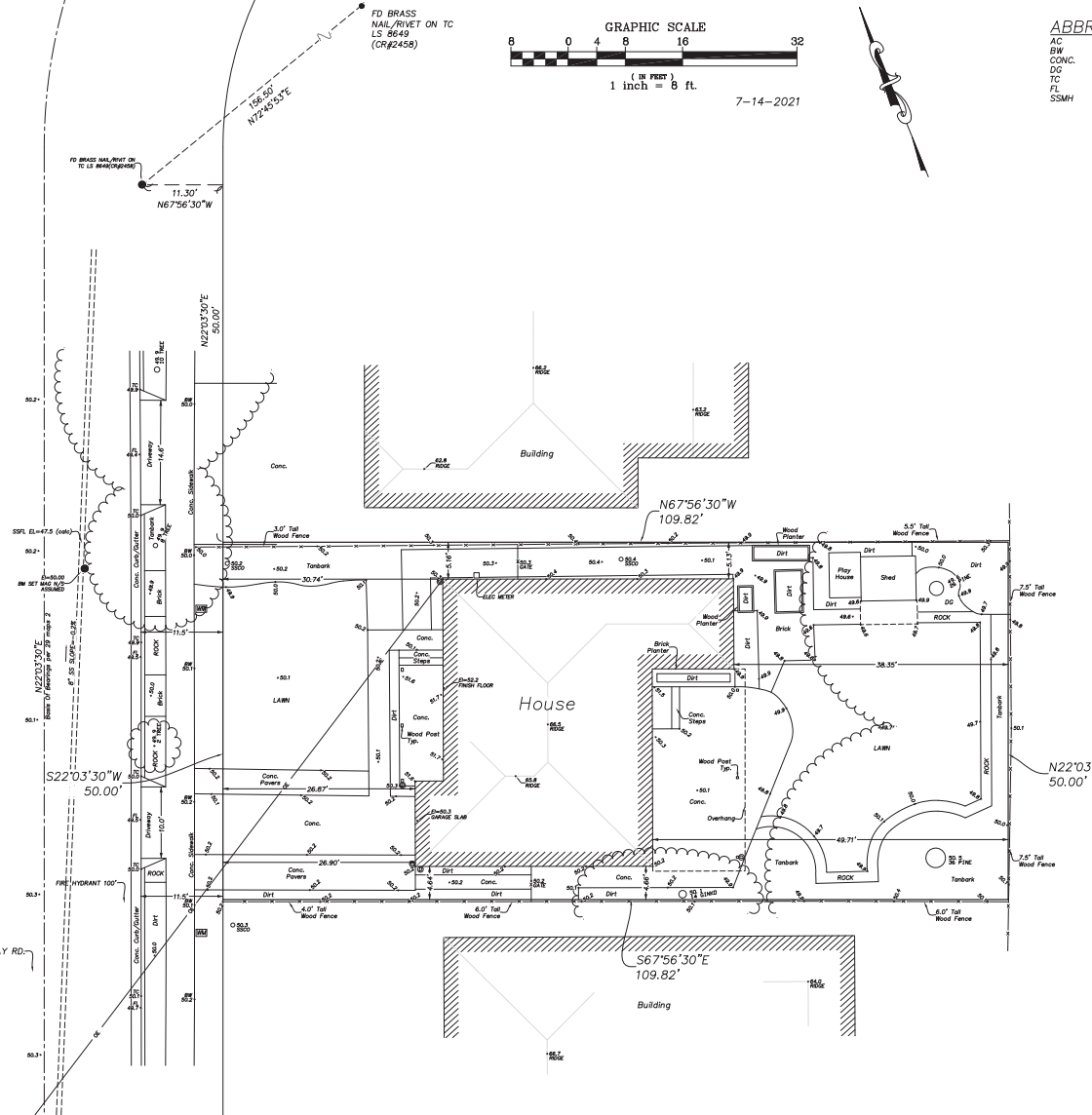
REVISIONS

DATE: 11/29/22
 SHEET

A11

PEGGY LANE
(50' WIDE)

BAY RD.



ABBREVIATIONS

AC	ASPHALT
BW	BACK OF WALK
CONC.	CONCRETE
DC	DECOMPOSED GRANITE
TC	TOP OF CURB
FL	FLOW LINE
SSMH	SANITARY SEWER MANHOLE

NOTES

ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS.

UNDERGROUND UTILITY - LOCATION IS BASED ON SURFACE EVIDENCE.

BUILDING LOCATION DIMENSIONS ARE MEASURED PERPENDICULAR TO THE PROPERTY LINES.

DIMENSIONS TO THE BUILDING ARE TAKEN AT THE EXTERIOR FINISHED SURFACE. THE BUILDING EXTERIOR FINISHED SURFACE IS STUCCO AND VARIES APPROXIMATELY 0.04"-0.08" IN THICKNESS.

FINISH FLOOR ELEVATION TAKEN AT DOOR THRESHOLD (EXTERIOR).

BENCHMARK: ASSUMED DATUM, POINT AS SHOWN

A BOUNDARY SURVEY WAS PERFORMED TO ACCURATELY LOCATE THE LEGAL PROPERTY LINES IN RELATION TO THE EXISTING IMPROVEMENTS (BUILDING)

A CURRENT TITLE REPORT FOR THE SUBJECT PROPERTY HAS NOT BEEN EXAMINED BY L. WADE HAMMOND LAND SURVEYOR. EASEMENTS OF RECORD MAY EXIST THAT ARE NOT SHOWN ON THIS MAP.

TREE SPECIES IDENTIFICATION: BEST EFFORT, WE ARE NOT ARBORISTS OR DENDROLOGISTS.

TREES SHOWN ARE 6" TRUNK DIAMETER OR LARGER, MEASURED 5' ABOVE GRADE

LEGEND

●	FOUND POINT AS NOTED
()	RECORD DATA / REFERENCE
⊠	WATER METER OR WATER VALVE BOX
⊞	FIRE HYDRANT
○ 16 12 8 OAK	TREE - TRUNK DIAMETER IN INCHES TREE SPECIES IDENTIFICATION: BEST EFFORT, WE ARE NOT ARBORISTS OR DENDROLOGISTS
○ 16 12 8 OAK	TREE WITH MULTIPLE TRUNKS
~	TREE DRIP LINE POINTS TOWARDS TREE TRUNKS. TREE DRIP LINES ABOVE PROPERTY LOCATED AS SHOWN.
+50.34	TOP OF CURB
— — —	FENCE
— — —	OVERHEAD WIRES
○ 12.34	POWER POLE
+ 12.34	SPOT ELEVATION
⊞ 16 14	SANITARY SEWER CLEAN OUT
⊞	DOWN SPOUT
⊠	IRRIGATION VALVE BOX

I CERTIFY THAT THIS PARCEL'S BOUNDARY WAS ESTABLISHED BY ME OR UNDER MY SUPERVISION AND IS BASED ON A FIELD SURVEY IN CONFORMANCE WITH THE LAND SURVEYOR'S ACT. ALL MONUMENTS ARE OF THE CHARACTER AND OCCUPY THE POSITIONS INDICATED AND ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.



BOUNDARY AND TOPOGRAPHIC SURVEY
 932 PEGGY LN.
 MENLO PARK
 APN: 061-022-190
 LOT 7, 29 MAPS 2
 LOT AREA: 5,491 SQ. FT.

L. Wade Hammond
 Land Surveying
 Civil Engineering
 36660 Newark Blvd. Suite C
 Newark, California 94560
 Tel: (510) 579-6112
 wade@whlandsurveyor.com www.wadehammondpls.com

Attachment 1: Project Description 932 Peggy Lane

The existing home was one of many thousands built during the post-WWII housing boom, constructed by “merchant builders”, the first group of builders to employ mass-production and assembly line techniques to the building process. Houses of this type are by-definition devoid of artistic craft. The existing building does not have any of the notable characteristics of architecture of the period. It appears to be a copy of one of the ranch house designs popular at the time, constructed with low quality materials and unremarkable character traits.

Please see the Historic Evaluation for a more in-depth analysis and photographs of the existing residence.

The enclosed proposed project consists of modifications and additions to an existing single-family residence for a growing family. The owners are a couple with two children that are entering the teen years and the current residence is only two bedrooms with a small overall footprint. With this in mind, the proposed design adds area to the existing main floor and the addition of a second level for the primary bedroom.

The proposed design centers around harmoniously combining materials currently found on the residence with a clean palette of new roofing, siding, windows and doors. The massing of the additional areas created are centered on the form of the existing structure and uses wood framed construction methods with the goal of re-using as much of the existing structure as possible (including anticipated re-use of framing lumber, etc.)

Andrea Montalbano
Dorman Associates, Inc.
am@dormanassociates.com
415.380.7914

Attachment 2: Neighbor Outreach Description:

The homeowners have been in contact with several neighbors and have received only positive verbal feedback to date, but no written feedback, therefore no revisions to proposed design are anticipated.

The date (or dates) that neighbor outreach was attempted or performed: November of 2021.

The addresses of neighbors the owners have successfully contacted (or tried to reach but were unable to connect.)

Succeeded to contact:

936 Peggy

931 Peggy (This home owner also texted to say “very nice” on 2/5/22, after the application notice was submitted – see screen shot below)

935 Peggy

Failed to contact:

924 Peggy

931 Timothy



September 9, 2022

Rorie and Jackeeline Overby
932 Peggy Lane
Menlo Park, CA 94025



Subject: **Revised Arborist Report**

Dear Rorie and Jackeeline:

Dorman Associates is preparing plans to renovate your property located at 932 Peggy Lane in Menlo Park. HortScience | Bartlett Consulting, Divisions of The F. A. Bartlett Tree Expert Company, was asked to prepare an **Arborist Report** for the trees within the project area as part of the application to the City of Menlo Park. This report responds to that request.

Description of Trees

I visited the site on March 2, 2022. Five (5) trees were assessed, tagged as #85 - 89. Two street trees were included in the assessment. Descriptions of trees are provided in the **Tree Assessment Form** and locations are shown on the **Tree Assessment Plan** (see Exhibits)

The site consists of a single-family residence on a flat lot. Trees on the property were located in the back yard. Following are brief descriptions of the trees:

- Trees #85 and 86 were mature Douglas-fir (*Pseudotsuga menziesii*). Both were growing in rear corner planting beds. Both were in fair condition. Fir #85 had a diameter of 32 inches and had a high raised crown. It was located approximately 1 ft. from a play structure/shed (Photo 1). Fir #86 was 44 inches and had a similarly high crown and history of limb removal.
- Ginkgo #87 (*Ginkgo biloba*) was in fair condition. It was growing in a narrow 2-ft. bed along the south fence, with its base pillowing over a concrete patio. The ginkgo had been topped and crown was extensively pruned back on the north and south sides (Photos 2 and 3, next page).



Photo 1. Douglas-fir #85 was growing in a corner of the back yard. Branches of nearby fir #86 are visible at upper right.



Photo 2 (left). Ginkgo #87 was growing between a patio and fence. It had been heavily pruned (red arrows). View is from the east.

Photo 3 (above). The ginkgo was being girdled by the patio to north (red arrow) and restricted by fence to south. View is from the west.

- Street trees #88 and 89 were Callery pears (*Pyrus calleryana*). Pear #88 was a young tree of 3 in. diameter and was in good condition. Pear #89 was semi-mature and in fair condition. It had a 9 in. diameter and multiple attachments at 6 ft. Both were growing in 3-ft. square planting beds along Peggy Lane.

The City of Menlo Park Municipal Code (Chapter 13.24.020, Heritage Trees) defines a Heritage tree as any tree with a diameter of 15 inches or greater or any *Quercus* which is native to California and is 10 inches or larger. Based on these criteria, all 3 on-site trees are considered Heritage trees. Street trees #88 and 89 are Protected by City ordinance. None of the trees can be removed without a permit.

Suitability for Preservation

Before evaluating the impacts that will occur during development, it is important to consider the quality of the tree resource itself, and the potential for individual trees to function well over an extended length of time. Trees that are preserved on development sites must be carefully selected to make sure that they may survive development impacts, adapt to a new environment and perform well in the landscape.

Evaluation of suitability for preservation considers several factors:

- **Tree health**
Healthy, vigorous trees are better able to tolerate impacts such as root injury, demolition of existing structures, changes in soil grade and moisture, and soil compaction than are non-vigorous trees.
- **Structural integrity**
Trees with significant amounts of wood decay and other structural defects that cannot be corrected are likely to fail. Such trees should not be preserved in areas where damage to people or property is likely.
- **Species response**
There is a wide variation in the response of individual species to construction impacts and changes in the environment. For example, Douglas-fir is moderately tolerant of site disturbance, but intolerant of fill within root zones and poor drainage. Ginkgo is tolerant of root pruning and other construction impacts.
- **Tree age and longevity**
Old trees, while having significant emotional and aesthetic appeal, have limited physiological capacity to adjust to an altered environment. Young trees are better able to generate new tissue and respond to change.
- **Invasiveness**
Species which spread across a site and displace desired vegetation are not always appropriate for retention. This is particularly true when indigenous species are displaced. The California Invasive Plant Inventory Database (<http://www.cal-ipc.org/paf/>) lists species identified as being invasive. Menlo Park is part of the Central West Floristic Province. Callery pear is on the watch list for invasive potential.

Each tree was rated for suitability for preservation based upon its age, health, structural condition and ability to safely coexist within a development environment. Suitability ratings are provided for each tree in the **Tree Assessment Form** (see Exhibits). A summary is provided in Table 1 (next page).

**Table 1: Tree Suitability for Preservation
932 Peggy Lane, Menlo Park.**

High	These are trees with good health and structural stability that have the potential for longevity at the site. Callery pear #88 had high suitability for preservation.
Moderate	Trees in this category have fair health and/or structural defects that may be abated with treatment. Trees in this category require more intense management and monitoring, and may have shorter life-spans than those in the “high” category. Douglas-firs #85 and 86 and Callery pear #89 had moderate suitability for preservation.
Low	Trees in this category are in poor health or have significant defects in structure that cannot be abated with treatment. These trees can be expected to decline regardless of management. The species or individual tree may possess either characteristics that are undesirable in landscape settings or be unsuited for use areas. Ginkgo #87 had low suitability for preservation.

We consider trees with high suitability for preservation to be the best candidates for preservation. We do not recommend retention of trees with low suitability for preservation in areas where people or property will be present. Retention of trees with moderate suitability for preservation depends upon the intensity of proposed site changes.

Evaluation of Impacts

Appropriate tree retention develops a practical match between the location and intensity of construction activities and the quality and health of trees. The ***Tree Assessment*** was the reference point for tree condition and quality. I used the Overby Residence Planning Submittal set (Dorman Associates, 1/5/2022) to determine the project area and evaluate impacts to trees.

The plans propose a first floor area addition and a new second floor. Exterior siding will be re-worked and the entire structure will have a new roof. An exterior deck will be constructed within the dripline of douglas-fir #86. Excavation for post footings will be as close as 3'-10" to the tree. I expect impacts to its root zone to be moderate if monitoring recommendations are followed. Construction of the unit will have significant impacts on ginkgo #87 due to its proximity to the existing building and the proposed expansion. I recommend preservation for all five trees, provided recommendations included in the ***Tree Preservation Guidelines*** (following page) can be followed. Recommended actions for each tree are provided in Table 2 (page 5).

**Table 2. Recommendations for Action
932 Peggy Lane, Menlo Park.**

Tree No.	Common Name	Trunk Diameter (in.)	Heritage Tree?	Recommendation for Action
85	Douglas-fir	32	Yes	Preserve , protect from impacts
86	Douglas-fir	44	Yes	Preserve , protect from impacts
87	Ginkgo	17	Yes	Preserve , protect from impacts
88	Callery pear	3	No	Preserve , protect from impacts
89	Callery pear	9	No	Preserve , outside impacts

Estimate of Value

The City of Menlo Park requires establishing the value of all assessed trees. To accomplish this, I used the standard methods found in Guide for Plant Appraisal, 10th edition (published in 2018 by the International Society of Arboriculture, Champaign IL). In addition, I referred to Species Classification and Group Assignment (2004), a publication of the Western Chapter of the International Society of Arboriculture. These two documents outline the methods employed in estimating tree value.

I estimated the reproduction cost of each tree based upon four factors: size, condition, functional limitations, and external limitations. Size is measured as trunk diameter, normally 54" above grade. Condition reflects the health and structural integrity of the individual tree, as noted in the Tree Assessment. Functional limitations consider the interaction of the tree with its planting site currently and for the potential for future development. I did not identify any external limitations at this site.

The estimate of value for the five trees assessed is \$24,500 (see the **Tree Assessment Form** for estimated value of each tree).

Tree Preservation Guidelines

The goal of tree preservation is not merely tree survival during development but maintenance of tree health and beauty for many years. Retained trees that are either subject to extensive injury during construction or are inadequately maintained become a liability rather than an asset. The response of individual trees will depend on the amount of excavation and grading and the construction methods.

The following recommendations will help reduce impacts to trees from development and maintain and improve their health and vitality through the clearing, grading and construction phases.

Design recommendations

1. Any changes to the plans affecting the trees shall be reviewed by the Consulting Arborist with regard to tree impacts. These include, but are not limited to, demolition plans, site plans, improvement plans, utility and drainage plans, grading plans, and landscape and irrigation plans.
2. A **TREE PROTECTION ZONE** shall be established around each tree to be preserved. No grading, excavation, construction or storage of materials shall occur within that zone. The **TREE PROTECTION ZONES (TPZ)** for Douglas-fir #85 shall be the dripline at west and south sides, connecting to existing property fences to completely enclose the tree.
3. Douglas-fir #86 and ginkgo #87 will require trunk protection (see below). Existing paving around tree #87 may be left in place as temporary root buffer, but the tree will benefit from removal of pavement after construction to a minimum 2 ft. radius from the trunk. Tree #86 will require protection from root zone compaction. During excavation, special construction techniques such as hand digging or tunneling under roots shall be employed where necessary to minimize root injury. For pier excavation, locations of post foundations must be adjusted to avoid cutting buttress support roots over 2 inches in diameter.
4. Fencing for Street Tree #88 shall be installed at the edges of the tree well. All tree protection fences shall be 6-ft. high chain link fencing mounted on 8 ft. tall, 2 in. diameter galvanized posts, driven 24 in. into the ground, or equivalent as required by the City. Street Tree #89 will not require fencing.
5. **Tree Preservation Notes**, prepared by the Consulting Arborist, should be included on all plans.
6. Any herbicides placed under paving materials must be safe for use around trees and labeled for that use.
7. Irrigation systems must be designed so that trenching will not occur within the **TREE PROTECTION ZONE**. Design irrigation to be placed as far from trees as possible, ideally immediately behind any new curbs defining the planter areas.

Pre-construction treatments and recommendations

1. The demolition contractor and construction superintendent shall meet with the Consulting Arborist before beginning work to discuss work procedures and tree protection.
2. Fence all trees to be retained to completely enclose the **TREE PROTECTION ZONE** prior to demolition, grubbing or grading. Fences are to remain until all grading, construction and landscaping is completed. Place weather proof signs, 2ft. x 2 ft.,

on the fencing that read "**TREE PROTECTION ZONE** Keep Out" (eg. one sign for each of the four compass points).



3. Where demolition, building and deck construction must occur close to Trees #86 and 87, install temporary trunk protection using a minimum of 4 layers of orange plastic snow fencing, then a layer of wood planks set on end, edge-to-edge and wrapped with a minimum of 2 additional layers of orange plastic snow fencing. The trunks must be wrapped to a height at least one foot above adjacent construction. Any low branches that are within the work zone should also be protected. Trunk protection is to remain in place during construction and carefully removed when construction is completed.
4. Limit foot and equipment traffic around the root zone of Tree #86 where demolition and construction are planned. Install 3/4-in. thick plywood boards over a 4-in. mulch layer within a 3-foot radius of the trunk where equipment will be used and/or foot traffic will occur.
5. Apply and maintain 4-6 in. of wood chip mulch within the **TREE PROTECTION ZONE**. Keep the mulch 2 ft. from the base of tree trunks.
6. Prune the trees to provide demolition and construction clearances. Pruning should focus on clearance and avoid removal of live material. All pruning shall be completed by a Certified Arborist or Tree Worker and adhere to the latest edition of the ANSI Z133 and A300 standards as well as the *Best Management Practices -- Tree Pruning* published by the International Society of Arboriculture.
7. All tree work shall comply with the Migratory Bird Treaty Act as well as California Fish and Wildlife code 3503-3513 to not disturb nesting birds. To the extent feasible tree pruning and removal should be scheduled outside of the breeding season. Breeding bird surveys should be conducted prior to tree work. Qualified biologists should be involved in establishing work buffers for active nests.

Recommendations for tree protection during construction

1. Prior to beginning work, the contractors working in the vicinity of trees to be preserved are required to meet with the Consulting Arborist at the site to review all work procedures, access routes, storage areas and tree protection measures.
2. Any root pruning required for construction purposes shall receive the prior approval of and be supervised by the Consulting Arborist. Roots should be cut with a saw to provide a flat and smooth cut. Removal of roots larger than 2 in. diameter should be avoided.
3. If roots 2 in. and greater in diameter are encountered during site work and must be cut to complete the construction, the Consulting Arborist must be consulted to evaluate effects on the health and stability of the tree and recommend treatment.
4. No grading, construction, demolition or other work shall occur within the **TREE PROTECTION ZONE**. Any modifications must be approved and monitored by the Consulting Arborist.

5. If injury should occur to any tree during construction, it should be evaluated as soon as possible by the Consulting Arborist so that appropriate treatments can be applied.
6. No excess soil, chemicals, debris, equipment or other materials shall be dumped or stored within the **TREE PROTECTION ZONE**.
7. Any tree pruning needed for clearance during construction must be performed by a Certified Arborist and not by construction personnel.

If you have any questions regarding my observations or recommendations, please feel free to contact me.

Sincerely,



Pam Nagle
Consulting Arborist and Urban Forester
Certified Arborist #WE-9617A
ISA Tree Risk Assessment Qualified

Attached: *Tree Assessment Form*

Tree Assessment Plan

Tree Assessment

932 Peggy Lane
Menlo Park, CA
March 2022



Tree No.	Species	Trunk Diameter (in.)	Approx. dripline radius (ft.)	Heritage Tree?	Condition 1=poor 5=excellent	Suitability for Preservation	Estimate of Value	Comments
85	Douglas-fir	32	20	Yes	3	Moderate	\$6,750	1' from shed; high raised crown; history of limb removals.
86	Douglas-fir	44	25	Yes	3	Moderate	\$12,600	High raised crown; leans S.; weeping sap S. side trunk; history of limb removals; swing on branch.
87	Ginkgo	17	8 - N. 8 - S. 15 - E. 15 - W.	Yes	3	Low	\$2,800	Measured at 3.5' below attachments; in 2' bed between concrete patio and fence; base pillowing over patio; multiple narrow attachments at 5 and 6'; history of limb removals; upright crown; topped and headed back N./S. sides.
88	Callery pear	3	3	No	4	High	\$450	Street tree. 3' planting bed; suckers at base; multiple attachments at 5'; good young tree.
89	Callery pear	9	8	No	3	Moderate	\$1,900	Street tree. 3' planting bed; on property line; multiple attachments at 6', otherwise good form and structure.
Total							\$24,500	



STAFF REPORT

Planning Commission

Meeting Date:

1/23/2023

Staff Report Number:

23-009-PC

Public Hearing:

Consider and adopt a resolution to approve a use permit to demolish an existing one-story, single-family residence and construct a new two-story residence on a substandard lot with regard to minimum lot depth and area in the R-1-U (Single Family Urban Residential) zoning district, at 225 Lexington Drive

Recommendation

Staff recommends that the Planning Commission adopt a resolution approving a use permit to demolish an existing one-story, single-family residence and construct a new two-story residence on a substandard lot with regard to minimum lot depth and area in the R-1-U (Single Family Urban Residential) zoning district. The proposal includes an attached accessory dwelling unit (ADU) which is not subject to discretionary review. The draft resolution, including the recommended actions and conditions of approval, is included as Attachment A.

Policy Issues

Each use permit request is considered individually. The Planning Commission should consider whether the required use permit findings can be made for the proposal.

Background

Site location

The subject property is a corner lot located at the southwestern corner of the intersection of Lexington Drive and Robin Way in The Willows neighborhood. All neighboring properties are also located in the R-1-U zoning district. A location map is included as Attachment B. Lexington Drive features mostly one-story ranch-style homes and several properties have been redeveloped with newer two-story homes with varying architectural styles (e.g., 255 Robin Way across the street to the north).

Analysis

Project description

The applicant is proposing to demolish the existing one-story, single-family residence, and construct a new two-story, single-family residence with an attached ADU. A data table summarizing parcel and project characteristics is included as Attachment C. The project plans and project description letter are included as Attachment A, Exhibits A and B, respectively.

The proposed residence would be a three-bedroom, three-bathroom home. The first floor would be shared

living space, including the kitchen, dining room, family/living room, and a covered rear patio. The attached ADU would also be located on the first floor. The three bedrooms and a laundry room would be located on the second floor. The required parking for the primary dwelling would be provided by an attached, front-loading, two-car garage. The required parking for the ADU would be provided by a tandem parking space in front of the garage.

The proposed residence would meet all Zoning Ordinance requirements for setbacks, lot coverage, floor area limit (FAL), daylight plane, parking, and height. Of particular note, the project would have the following characteristics with regard to the Zoning Ordinance:

- The proposed floor area would be at the maximum with 3,575.6 square feet proposed where 2,800 square feet is the maximum permitted. The primary residence would be 2,782.6 square feet and the attached ADU would be 793.0 square feet and would exceed the maximum floor area limit, however, the maximum FAL is permitted to be exceeded by up to 800 square feet in order to accommodate the ADU.
- The proposed residence and ADU would be at the maximum building coverage with 44.4 percent proposed where 40 percent is the maximum. The primary residence would be 31.4 percent, and the attached ADU would exceed the maximum building coverage by 271.5 square feet, however, the maximum building coverage is permitted to be exceeded by up to 800 square feet in order to accommodate the ADU.
- The proposed second floor would be below the second floor limit with 1,178.4 square feet proposed where the maximum allowable second-story floor area is 1,400 square feet.
- The proposed residence would be below the maximum height, with 24 feet, six inches proposed where 28 feet is the maximum permitted height.

The proposed residence, with the main entry facing Lexington Drive, would have a front-east setback of 20 feet and a rear-west setback of approximately 32 feet, where 20 feet is required in either case. The residence is proposed to be built to the minimum six-foot-nine-inch interior side-south setback. The attached ADU has independent entry facing Robin Way and would be setback approximately nine feet, 10 inches, where four feet is required. The proposed second story would be stepped back from the first story on all sides. The second story would be stepped back 23 feet, seven inches on the front; 20 feet on the rear; 10 feet on the south side; and 12 feet, eight inches on the north side.

Design and materials

Within the project description letter (Attachment A, Exhibit B), the applicant indicates the design of the residence as “modern farm style” but minimized for a more simplified aesthetic. The residence exterior includes horizontal wood siding at the first level and vertical wood siding at the second level. The roof is proposed to be a combination of standing seam metal roofing and composition asphalt shingles. Solar panels are proposed at the second level. The proposed windows are wood frame aluminum clad double or casement windows with true simulated divided lights.

Second-story windows along the front facing Lexington Way would have sill heights with a minimum of two-feet, five inches. Second-story windows along the north side facing Robin Way would have sill heights between two feet and four feet, five inches. Second story windows along the interior south side would have sill heights between two feet, five inches and four feet, six inches. Second story windows at the rear would have a sill height of four feet, six inches. With the proposed step back of the second story from the first story on all sides, staff does not believe the proposed windows would cause an impact to privacy. Staff believes that the scale, materials, and style of the proposed residence would result in a consistent

aesthetic approach and are generally consistent with the broader neighborhood, given the similar architectural styles and sizes of structures in the area.

Trees and landscaping

The applicant has submitted an arborist report (Attachment D), detailing the species, size, and conditions of on-site and nearby heritage and non-heritage trees. The arborist report lists a total of seven trees on and around the subject property of which five are heritage street trees; Trees #1-2 are Southern Magnolia along Lexington Drive and Trees #3-5 are Camphor Trees along Robin Way. No heritage street trees are proposed for removal. On the subject property, there are two non-heritage trees (Trees #6-7). Tree #6 is a Holly Tree designated for removal. Tree #7 is a Lemon Tree to remain.

The project includes a new landscaping plan with the following statement of design intent: *The planting design is to be simple and elegant to match the modern farmhouse architecture of the home, with a colorful palette of greens, purples, pinks & white. Native or adaptive climate appropriate plant material has been selected based on the solar positioning of the new residence. At least 75% of the plant material will be native or adaptive, and low water use. A native garden has been designed in the front yard. A lawn area in the back yard will serve as an active space for the occupant. (1) Lemon Tree and (5) large street trees will remain & be protected during construction.*

Staff notes that along the side-south and rear-west property lines, Pacific Wax Myrtle and Carolina Cherry Laurel shrub/hedge privacy screening is proposed. Shrubs are also proposed along the front-east and side-north property lines along Lexington Drive and Robin Way, respectively. Condition 1h is included within Attachment A, Exhibit C to ensure the height of fences, walls, hedges or similar structures on the subject property shall comply with the maximum height requirements as listed in Menlo Park Municipal Code section 16.64.020.

The arborist report includes tree protection recommendations for the pre-construction, construction, and post-construction phases of the project. As part of the project review process, the arborist report was reviewed by the City Arborist and accepted. The City Arborist has provided Condition 1i to require a tree protection verification letter from the Project Arborist prior to the start of demolition/construction. It is required that the Project Arborist provide periodic construction monitoring/tree protection inspections during construction to monitor the effectiveness of the approved Tree Protection Plan and to provide recommendations for any additional care or treatment. Implementation of all recommendations to mitigate impacts to the heritage trees identified in the arborist report would also be ensured as part of Condition 1j.

Correspondence

Within the project description letter (Attachment A, Exhibit B), the applicant indicates that the property owners have conducted outreach to adjacent neighbors and all responses to the project have been positive. As of the publication of this report, staff has not received any direct correspondence regarding the project.

Conclusion

Staff believes that the design, scale, and materials of the proposed residence are generally compatible with the surrounding neighborhood. The modern farm style would be generally attractive and well-proportioned, and the inset of the second floor would support increased privacy while reducing the perception of mass. Staff recommends that the Planning Commission approve the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's

Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 3 (Section 15303, "New Construction or Conversion of Small Structures") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Draft Planning Commission Resolution Adopting Findings of Approval for project Use Permit, including project Conditions of Approval
Exhibits to Attachment A
 - A. Project Plans
 - B. Project Description Letter
 - C. Conditions of Approval
- B. Location Map
- C. Data Table
- D. Arborist Report

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings, and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:
Calvin Chan, Senior Planner

Report reviewed by:
Corinna Sandmeier, Principal Planner

PLANNING COMMISSION RESOLUTION NO. 2023-XX**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF MENLO PARK APPROVING A USE PERMIT FOR THE DEMOLITION OF AN EXISTING ONE-STORY, SINGLE-FAMILY RESIDENCE AND CONSTRUCTION OF A NEW TWO-STORY, SINGLE-FAMILY RESIDENCE ON A SUBSTANDARD LOT WITH REGARD TO MINIMUM LOT DEPTH AND AREA IN THE R-1-U (SINGLE FAMILY URBAN RESIDENTIAL) ZONING DISTRICT**

WHEREAS, the City of Menlo Park (“City”) received an application requesting to demolish an existing one-story, single-family residence, and construct a new two-story, single-family residence on a substandard lot with regard to minimum lot depth and area in the Single Family Urban Residential (R-1-U) zoning district; the project includes an attached accessory dwelling unit (collectively, the “Project”) from Andrew Young (“Applicant”), on behalf of the owners Sid and Ruchi Murlidhar (“Owners”) located at 225 Lexington Drive (APN 062-305-120) (“Property”). The Project use permit is depicted in and subject to the development plans and project description letter which are attached hereto as Exhibit A and Exhibit B, respectively, and incorporated herein by this reference; and

WHEREAS, the Property is located in the Single Family Urban Residential (R-1-U) district. The R-1-U district supports single-family residential uses; and

WHEREAS, the proposed Project complies with all objective standards of the R-1-U district; and

WHEREAS, the proposed Project was reviewed by the Engineering Division and found to be in compliance with City standards; and

WHEREAS, the Applicant submitted an arborist report prepared by Ned Patchett Consulting which was reviewed by the City Arborist and found to be in compliance with the Heritage Tree Ordinance and proposes mitigation measures to adequately protect heritage trees in the vicinity of the project; and

WHEREAS, the Project, requires discretionary actions by the City as summarized above, and therefore the California Environmental Quality Act (“CEQA,” Public Resources Code Section §21000 et seq.) and CEQA Guidelines (Cal. Code of Regulations, Title 14, §15000 et seq.) require analysis and a determination regarding the Project’s environmental impacts; and

WHEREAS, the City is the lead agency, as defined by CEQA and the CEQA Guidelines, and is therefore responsible for the preparation, consideration, certification, and approval of environmental documents for the Project; and

WHEREAS, the Project is categorically exempt from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures); and

WHEREAS, all required public notices and public hearings were duly given and held according to law; and

WHEREAS, at a duly and properly noticed public hearing held on January 23, 2023, the Planning Commission fully reviewed, considered, and evaluated the whole of the record including all public and written comments, pertinent information, documents and plans, prior to taking action regarding the Project Revisions.

NOW, THEREFORE, THE MENLO PARK PLANNING COMMISSION HEREBY RESOLVES AS FOLLOWS:

Section 1. Recitals. The Planning Commission has considered the full record before it, which may include but is not limited to such things as the staff report, public testimony, and other materials and evidence submitted or provided, and the Planning Commission finds the foregoing recitals are true and correct, and they are hereby incorporated by reference into this Resolution.

Section 2. Conditional Use Permit Findings. The Planning Commission of the City of Menlo Park does hereby make the following Findings:

The approval of the use permit for the construction of new two-story residence with attached accessory dwelling unit on a substandard lot is granted based on the following findings which are made pursuant to Menlo Park Municipal Code Section 16.82.030:

1. That the establishment, maintenance, or operation of the use applied for will, under the circumstance of the particular case, not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing in the neighborhood of such proposed use, or injurious or detrimental to property and improvements in the neighborhood or the general welfare of the city because:
 - a. Consideration and due regard were given to the nature and condition of all adjacent uses and structures, and to general plans for the area in question and surrounding areas, and impact of the application hereon; in that, the proposed use permit is consistent with the R-1-U zoning district and the General Plan because two-story residences are allowed to be constructed on substandard lots subject to granting of a use permit provided that the proposed residence conforms to applicable zoning standards, including, but not limited to, minimum setbacks, maximum floor area limit, and maximum building coverage.
 - b. The proposed residence would include the required number of off-street parking spaces because one covered and one uncovered parking space

would be required at a minimum, and two covered parking spaces are provided.

Section 3. Conditional Use Permit. The Planning Commission approves Use Permit No. PLN2021-00058, which use permit is depicted in and subject to the development plans and project description letter, which are attached hereto and incorporated herein by this reference as Exhibit A and Exhibit B, respectively. The Use Permit is conditioned in conformance with the conditions attached hereto and incorporated herein by this reference as Exhibit C.

Section 4. ENVIRONMENTAL REVIEW. The Planning Commission makes the following findings, based on its independent judgment after considering the Project, and having reviewed and taken into consideration all written and oral information submitted in this matter:

- A. The Project is categorically exempt from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures).

Section 5. SEVERABILITY

If any term, provision, or portion of these findings or the application of these findings to a particular situation is held by a court to be invalid, void or unenforceable, the remaining provisions of these findings, or their application to other actions related to the Project, shall continue in full force and effect unless amended or modified by the City.

I, Corinna Sandmeier, Principal Planner and Planning Commission Liaison of the City of Menlo Park, do hereby certify that the above and foregoing Planning Commission Resolution was duly and regularly passed and adopted at a meeting by said Planning Commission on January 23, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS THEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this 23rd day of January, 2023.

Corinna Sandmeier
Principal Planner and Planning Commission Liaison
City of Menlo Park

Exhibits

- A. Project Plans
- B. Project Description Letter
- C. Conditions of Approval

MURLIDHAR RESIDENCE MENLO PARK, CALIFORNIA

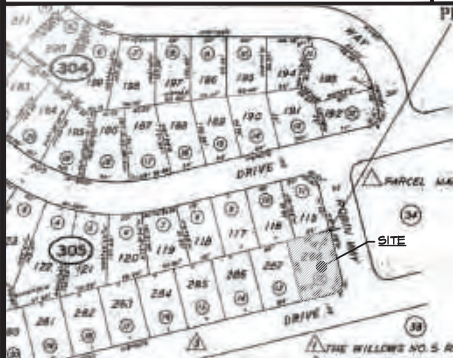


BASE FLOOD ELEVATION: 47.3' (NAVDS8 DATUM)

PROPOSED STREET CORNER PERSPECTIVE VIEW (FOR REFERENCE ONLY)



VICINITY MAP



PARCEL MAP

CIVIL SURVEYOR
LEA & BRAZE ENGINEERING INC.
2405 INDUSTRIAL PARKWAY WEST
HAYWARD, CA 94545
TEL: (510) 887-4086
TEL: (510) 887-3019
ATTN: PETER CARLINO
pcarlino@leabraze.com

LANDSCAPE ARCHITECT
KVOLA LANDSCAPE ARCHITECTURE
224 WEST 71ST STREET, #4
NEW YORK, NY 10023
TEL: 800-905-0289
ATTN: KARLA VAN DUYN
WWW.KVOLA.COM
karlavanduynd@gmail.com

STRUCTURAL ENGINEERING & TITLE-24
ATA ENGINEERING, INC.
1202 MAIN ST.
REDWOOD CITY, CA 94063
TEL: (650) 363-2338
FAX: (650) 363-2301
ATTN: ALI ADIB
ata@ataeng.net

SOILS ENGINEER
P.G. SOILS, INC.
901 ROSE COURT,
BURLINGAME, CA 94010
TEL: (650) 347-3934
ATTN: PAUL GRISHABER
pgsoils.inc@gmail.com

PROJECT DESIGN DATA:

2019 CALIFORNIA BUILDING CODE - VOL. 1&2
2019 CALIFORNIA RESIDENTIAL CODE
2019 CALIFORNIA MECHANICAL CODE
2019 CALIFORNIA PLUMBING CODE
2019 CALIFORNIA ELECTRIC CODE
2019 CALIFORNIA FIRE CODE
2019 CALIFORNIA GREEN BUILDING CODE (CalGreen)
2019 CALIFORNIA ENERGY CODE
2019 CALIFORNIA BUILDING ENERGY EFFICIENCY STANDARDS
ALONG WITH ALL OTHER LOCAL AND STATE LAWS AND REGULATIONS.

THE DOCUMENTS PREPARED BY THESE CONSULTANTS ARE AN INTEGRAL PART OF THE ARCHITECTURAL CONSTRUCTION DOCUMENTS AND SHALL BE INCORPORATED INTO THIS SET BY REFERENCE, I.E. SOILS REPORT, TITLE-24, STRUCTURAL CALCULATIONS, ETC. THE MOST STRINGENT REQUIREMENTS SHALL BE FOLLOWED. THE CONTRACTOR SHALL OBTAIN CURRENT COPIES OF ALL DOCUMENTS, READ, UNDERSTAND AND CONFIRM ANY CONFLICTS OR DISCREPANCIES OR QUESTIONS WITH APPROPRIATE CONSULTANTS.

ARCHITECT
YOUNG AND BORLIK ARCHITECTS, INC.
4962 EL CAMINO REAL, SUITE 218
LOS ALTOS, CA 94022
TEL: (650) 688-1950
ATTN: ANDREW YOUNG
AYOUNG@YBARCHITECTS.COM
TWINKAL@YBARCHITECTS.COM

ARBORIST
RED PATCHETT CONSULTING
841 OLD COUNTY RD.
SAN CARLOS, CA 94070
TEL: (650) 728-8309
FAX: (650) 897-8025
ATTN: NED PATCHETT
ned@redpatchettconsulting.com

CIVIL ENGINEER
NNR ENGINEERING
636 WEYBRIDGE DR
SAN JOSE, CA 95123
TEL: (408) 348-7813
FAX: (408) 225-3967
ATTN: NADIM RAFFOUL
nnrengineering@yahoo.com

- ARCHITECTURAL**
- A0.1 COVER SHEET, VICINITY MAP, CONSULTANTS, SHEET INDEX, PROJECT SUMMARY
 - A0.1.1 3D RENDERING (FOR REFERENCE ONLY)
 - A0.3.1 NEIGHBORHOOD CONTEXT - SITE PLAN
 - A0.4 EXISTING SITE PLAN
 - A0.4.1 LOT AND WIDTH CALCULATION DIAGRAM
 - A0.5 PROPOSED SITE PLAN
 - A0.6 AREA CALCULATIONS
 - A2.1.1 PROPOSED FIRST FLOOR PLAN
 - A2.2.1 PROPOSED SECOND FLOOR PLAN
 - A2.3 ROOF PLAN
 - A3.1 EXISTING & PROPOSED FRONT ELEVATIONS
 - A3.2 EXISTING & PROPOSED LEFT SIDE ELEVATIONS
 - A3.3 EXISTING & PROPOSED REAR ELEVATIONS
 - A3.4 EXISTING & PROPOSED RIGHT SIDE ELEVATIONS
 - A4.1 EXISTING & PROPOSED SECTIONS
 - A4.2 EXISTING & PROPOSED SECTIONS
 - A4.3 EXISTING & PROPOSED SECTIONS

- CIVIL SURVEY**
- SU-1 TOPOGRAPHIC SURVEY PLAN
- CIVIL**
- C-1 GRADING AND DRAINAGE PLAN
 - C-2 MISC. DETAILS
 - C-3 EROSION CONTROL PLAN
 - C-4 CONSTRUCTION BEST MANAGEMENT PRACTICES BMP'S SHEET

- LANDSCAPE**
- L1.0 LANDSCAPE SITE PLAN
 - L1.1 LANDSCAPE DETAILS
- ARBORIST REPORT**
- AR-1 ARBORIST REPORT
 - AR-2 ARBORIST REPORT

CONSULTANTS

4

SHEET INDEX

3

SCOPE:

NEW 2,782.7 SF TWO STORY HOUSE WITH 793 SF ATTACHED ADU

APN#: 062-305-130
OWNER: RUCHI AND SID MURLIDHAR
PROJECT ADDRESS: 225 LEXINGTON DRIVE, MENLO PARK, CA 94025

PROJECT OCCUPANCY: R-3 U
TYPE OF CONSTRUCTION: V-B
ZONING: R-1U
LOT SIZE: 6,170 sf
HISTORIC STATUS: NO
FLOOD ZONE: AE
STORIES: 2
ACCESSORY STRUCTURE: NO
FIRE SPRINKLERS: YES
ALLOWABLE LOT COVERAGE: 2,468 sf (40% OF LOT AREA)
ALLOWABLE F.A.R.: 2,800 sf

FRONT SETBACK: 20'
SIDE SETBACK: 6'-9 5/8" INTERIOR, 12' STREET SIDE
REAR SETBACK: 20'
HEIGHT LIMIT: 28'

AREA CALCULATION:

EXISTING CONDITIONED FIRST FLOOR LEVEL:	1,840.5 sf
EXISTING UNCONDITIONED SPACE (GARAGE):	419.4 sf
TOTAL EXISTING FLOOR AREA:	2,259.9 sf
EXISTING COVERED PORCHES:	164.8 sf
EXISTING LOT COVERAGE:	2,443.2 sf < 2,468 SF (ADDED FP 18.5 SF)
PROPOSED CONDITIONED FIRST FLOOR LEVEL:	1,167.3 sf
PROPOSED CONDITIONED SECOND FLOOR LEVEL:	1,178.4 sf
PROPOSED CONDITIONED FLOOR AREA:	2,345.7 sf
PROPOSED UNCONDITIONED FLOOR AREA:	437.1 sf
PROPOSED PORCHES:	335.9sf (COUNTS TOWARDS LOT COVERAGE)
TOTAL PROPOSED FLOOR AREA:	2,782.7 sf (3,575.7 gross, minus 793 for ADU)
PROPOSED ADU FLOOR AREA:	793 sf < 800 sf
PROPOSED LOT COVERAGE:	1,948.6 sf (2,741.6 gross, minus 793 for ADU) < 2,468 SF

SEE SHEET A0.6 FOR DETAILED AREA CALCULATIONS

PROJECT SUMMARY

1

ISSUE LOG

PLANNING SUBMITTAL	DEC 02, 2021
PLANNING REV 1	JULY 18, 2022
PLANNING REV 2	SEPT 08, 2022
PLANNING REV 3	NOV 01, 2022
NEIGHBORS REV 4	JAN 18, 2023

YOUNG AND BORLIK ARCHITECTS, INCORPORATED
4962 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
TEL: (650) 688-1950 FAX: (650) 323-1112 www.ybarchitects.com

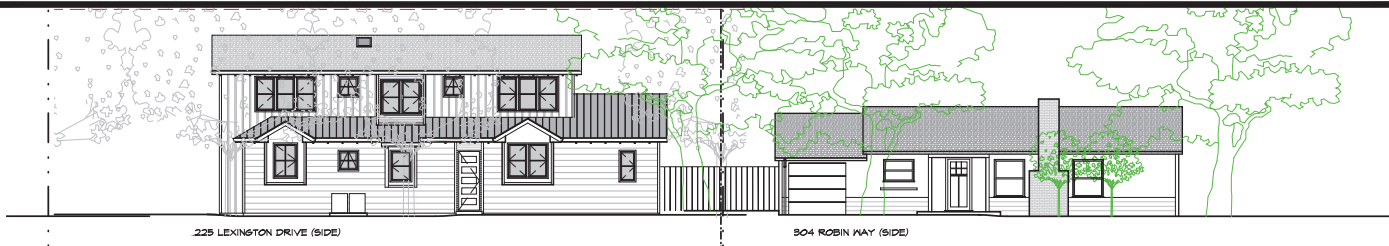


NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
225 LEXINGTON DRIVE
MENLO PARK, CA 94025

A.P.N. 062-305-130

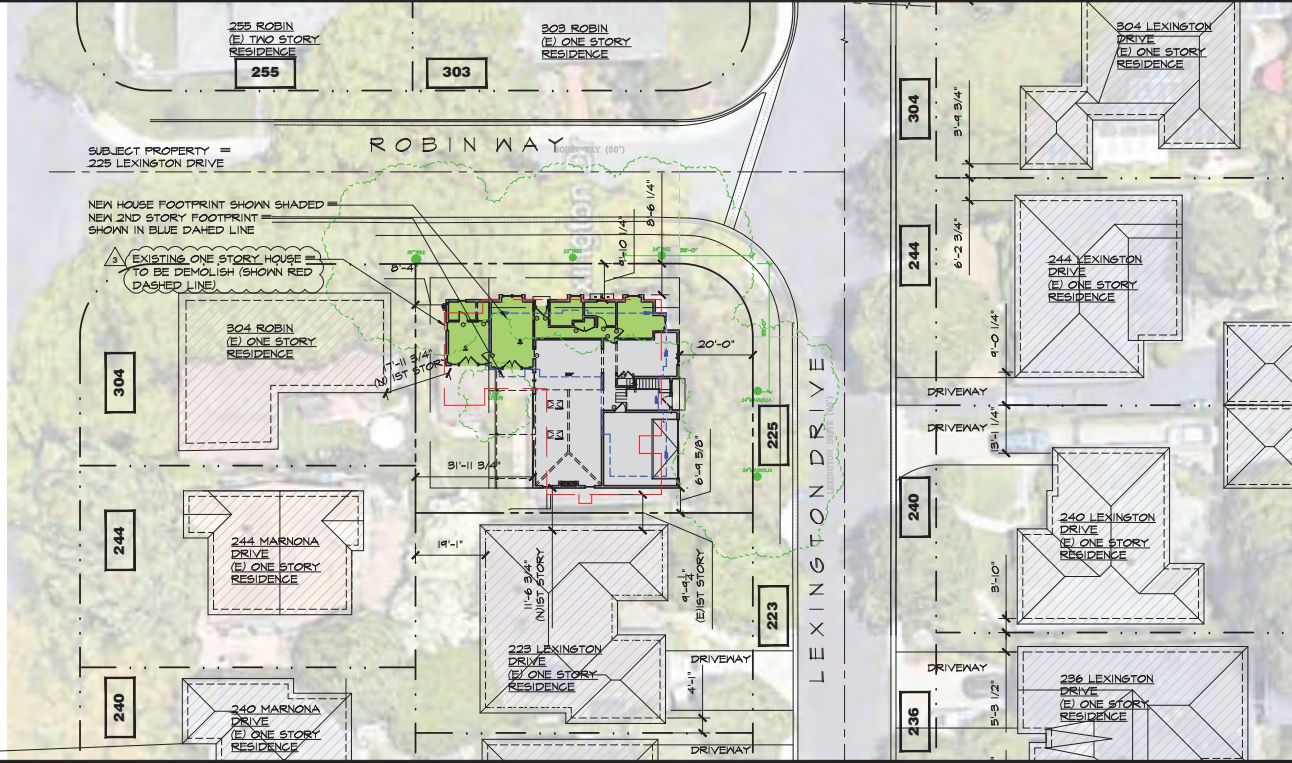
CHECKED BY	DATE
AYE	JULY 28, 2021
DATE	
BY	MURLIDHAR

A0.1



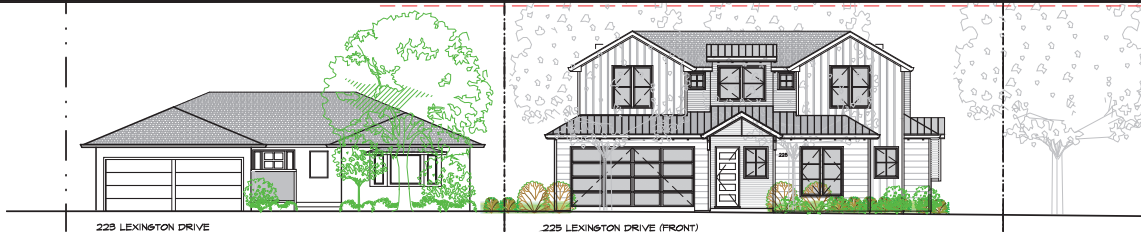
STREETSCAPE ELEVATION - ROBIN WAY

1/8" = 1'-0" 3



PROPOSED AREA PLAN w/ CONTEXTUAL AERIAL VIEW AND NON CONFIRMING NEIGHBORHOOD SETBACKS

1/16" = 1'-0" 2



STREETSCAPE ELEVATION - LEXINGTON DRIVE

1/8" = 1'-0" 1

ISSUE LOG	
PLANNING SUBMITTAL	DEC 22, 2017
PLANNING REV 1	JAN 11, 2018
PLANNING REV 2	JULY 18, 2018
SEPT 2018 REV 3	
PLANNING REV 4	
NEIGHBORS REV 4	JAN 18, 2023

YOUNG AND BORLIK
 ARCHITECTS, INCORPORATED
 4862 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
 TEL: (650) 688-1950 FAX: (650) 323-1112 www.yabarchitects.com



NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
 225 LEXINGTON DRIVE
 MENLO PARK, CA 94025

A.P.N. 062-305-120	
CHECKED AEV	DRAWN TP, DT
DATE JULY 28, 2021	
BY MURLIDHAR	

A0.3.1

CITY OF MENLO PARK FLOOD NOTES:

THE PROJECT WILL BE DESIGNED TO COMPLY WITH THE CITY'S FLOOD DAMAGE PREVENTION ORDINANCE, CHAPTER 12, SECTION 42.

NON-HABITABLE ENCLOSURES USED SOLELY FOR STORAGE OR PARKING, (SUCH AS A GRAYSPACE OR GARAGE), ARE ALLOWED BELOW THE DFE PROVIDED THAT THE ENCLOSURE IS ADEQUATELY VENT-FLOODED PROTECTED TO ALLOW FOR THE AUTOMATIC ENTRY AND EXIT OF FLOODWATER.

FLOOD VENTS OR OPENINGS SHALL HAVE A TOTAL NET AREA OF NOT LESS THAN ONE SQUARE INCH FOR EVERY SQUARE FOOT OF ENCLOSED SPACE, AT LEAST ONE FLOOD VENT SHALL BE LOCATED ON EACH EXTERIOR SIDE OF THE ENCLOSURE TO ALLOW THE AUTOMATIC ENTRY AND EXIT OF FLOODWATER. PLEASE CONFIRM THAT THERE ARE ADEQUATE AMOUNT OF FLOOD VENTS.

ELEVATIONS OF BOTTOM OF P64E GAS METER, AC UNIT, OR OTHER APPLIANCES SERVING THE BUILDING, IF ANY, NO UTILITIES (E.G. GAS, METERS, AC UNITS, ELECTRICAL CONDUITS) ARE PERMITTED BELOW THE DFE (BFE+). WATER AND SEWER PIPES, SEALED TO PREVENT FLOOD WATER INTRUSION, ARE ALLOWED.

A TREE PROTECTION VERIFICATION LETTER IS REQUIRED FROM THE PROJECT ARBORIST PRIOR TO THE START OF DEMOLITION/CONSTRUCTION. THE LETTER SHALL INCLUDE PHOTOS OF TREE PROTECTION INSTALLED TO SPECIFICATION. PLEASE ALSO INCLUDE LANGUAGE ABOUT THE REQUIREMENT FOR REGULAR INSPECTIONS IN THE LETTER. IT IS REQUIRED THAT THE PROJECT ARBORIST PROVIDE PERIODIC CONSTRUCTION MONITORING/TREE PROTECTION INSPECTIONS DURING CONSTRUCTION. FOUR-WEEK INTERVALS WOULD BE SUFFICIENT TO ASSESS AND MONITOR THE EFFECTIVENESS OF THE APPROVED TREE PROTECTION PLAN AND TO PROVIDE RECOMMENDATIONS FOR ANY ADDITIONAL CARE OR TREATMENT.

TREE SCHEDULE			
#	TYPE	DIA.	STATUS
1	SOUTHERN MAGNOLIA	24"	REMAIN
2	SOUTHERN MAGNOLIA	24"	REMAIN
3	CAMPOR TREE	24"	REMAIN
4	CAMPOR TREE	22"	REMAIN
5	CAMPOR TREE	30"	REMAIN
6	HOLLY	6"	REMOVE
7	FRUIT TREE (LEMON)	6"	REMAIN

CITY OF MENLO PARK FLOOD NOTES:

THE OWNER IS ENCOURAGED TO FILE FOR A LETTER OF MAP REVISION (LOMR) / LETTER OF MAP AMENDMENT (LOMA) WITH FEMA GIVEN THAT BOTH THE STRUCTURES LOWEST FLOOR AND LOWEST ADJACENT GRADE IS AT OR ABOVE THE BFE. IF GRANTED A LOMR / LOMA WILL PLACE THE PROPERTY IN FLOODZONE X AND MAY RESULT IN SUBSIDIZED FLOOD INSURANCE RATES.

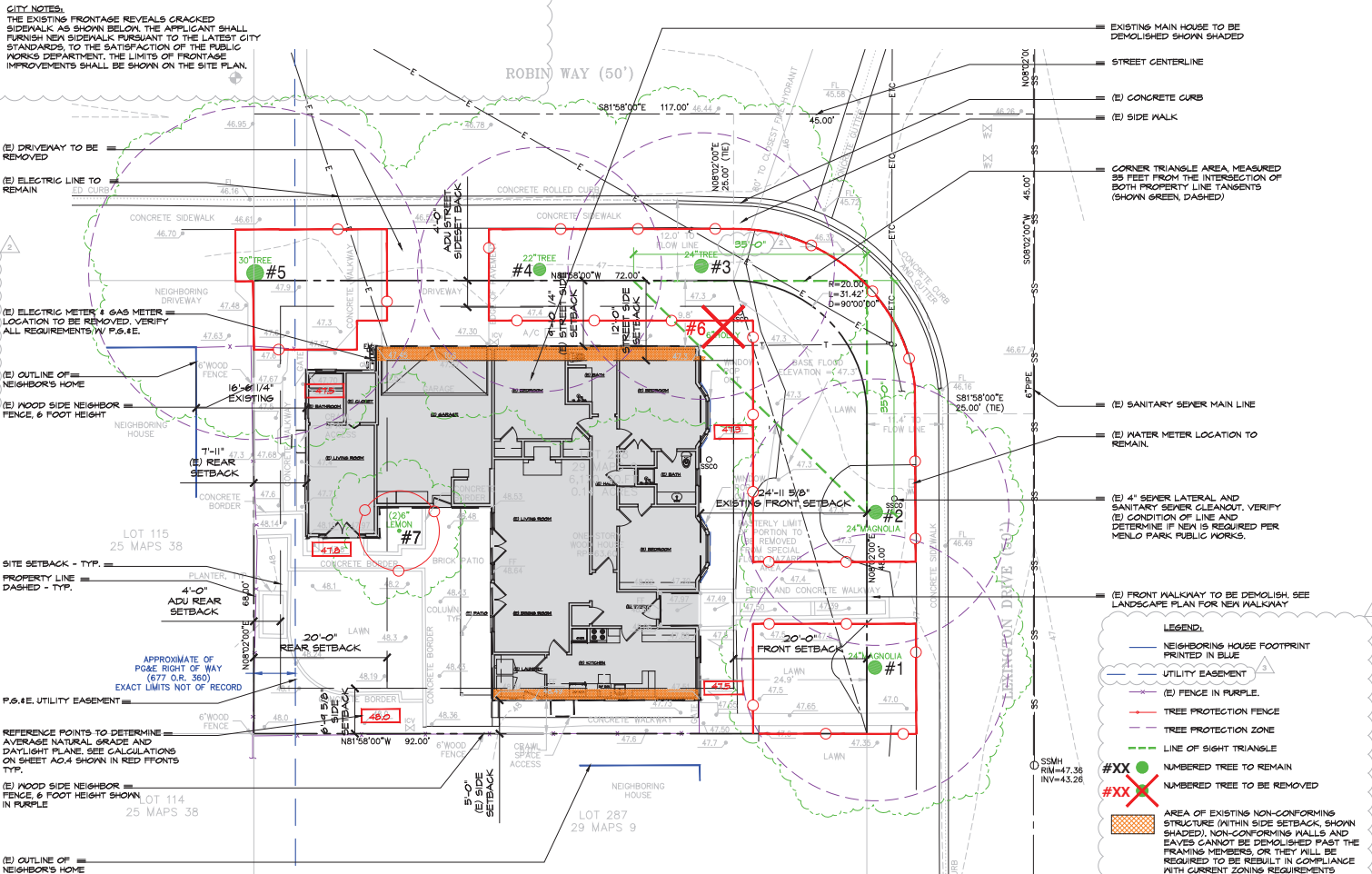
NOTE THAT THIS MEASURE IS NOT A CONDITION OF PERMIT ISSUANCE AND APPROVAL IS SOLELY GOVERNED BY FEMA AT THE OWNER'S DISCRETION. PLEASE REFER TO THE LINK BELOW FOR ADDITIONAL INFORMATION:
https://www.fema.gov/media-library-data/14010187612-642837250CD74516B085D81C7151E/LOMA-LOMR_FACT_SHEET.PDF

CITY NOTES:

THE EXISTING FRONTAGE REVEALS CRACKED SIDEWALK AS SHOWN BELOW. THE APPLICANT SHALL FURNISH NEW SIDEWALK PURSUANT TO THE LATEST CITY STANDARDS, TO THE SATISFACTION OF THE PUBLIC WORKS DEPARTMENT. THE LIMITS OF FRONTAGE IMPROVEMENTS SHALL BE SHOWN ON THE SITE PLAN.

AVG. NATURAL GRADE CALCULATIONS TO ESTABLISH DAYLIGHT PLANE SEE GRADE POINT ON THIS SHEET AND SURVEY S.U. FOR NUMBERS

$47.9 + 47.5 + 47.8 + 48.0 + 47.5 = 47.6$



LEGEND:

- NEIGHBORING HOUSE FOOTPRINT PRINTED IN BLUE
- UTILITY EASEMENT
- (E) FENCE IN PURPLE
- TREE PROTECTION FENCE
- TREE PROTECTION ZONE
- LINE OF SIGHT TRIANGLE
- #XX NUMBERED TREE TO REMAIN
- #XX NUMBERED TREE TO BE REMOVED
- AREA OF EXISTING NON-CONFORMING STRUCTURE (WITHIN SIDE SETBACK, SHOWN SHADED). NON-CONFORMING WALLS AND EAVES CANNOT BE DEMOLISHED PAST THE FRAMING MEMBERS, OR THEY WILL BE REQUIRED TO BE RESULT IN COMPLIANCE WITH CURRENT ZONING REQUIREMENTS

ISSUE LOG

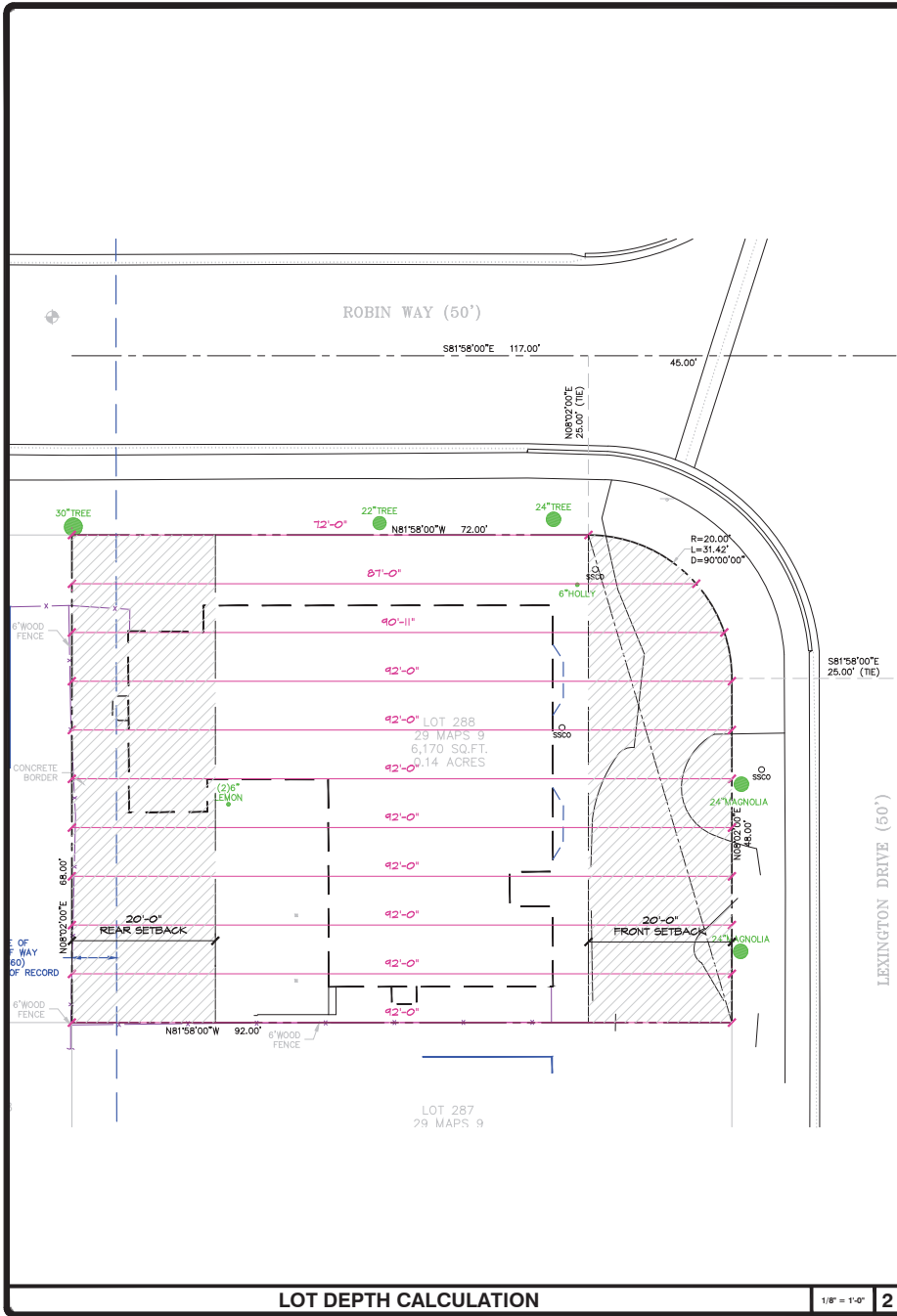
PLANNING SUBMITTAL	DEC 20, 2021
PLANNING REV 1	JAN 18, 2022
SEPT 28, 2021	
PLANNING REV 2	
NEIGHBORS REV 4	JAN 18, 2022

YOUNG AND BORLIK ARCHITECTS, INCORPORATED
 4962 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
 TEL: (650) 688-1956 FAX: (650) 323-1112 www.ybarchitects.com

NEW RESUBMIT FOR:
RUCHI & SID MURLIDHAR
 225 LEXINGTON DRIVE
 MENLO PARK, CA 94025

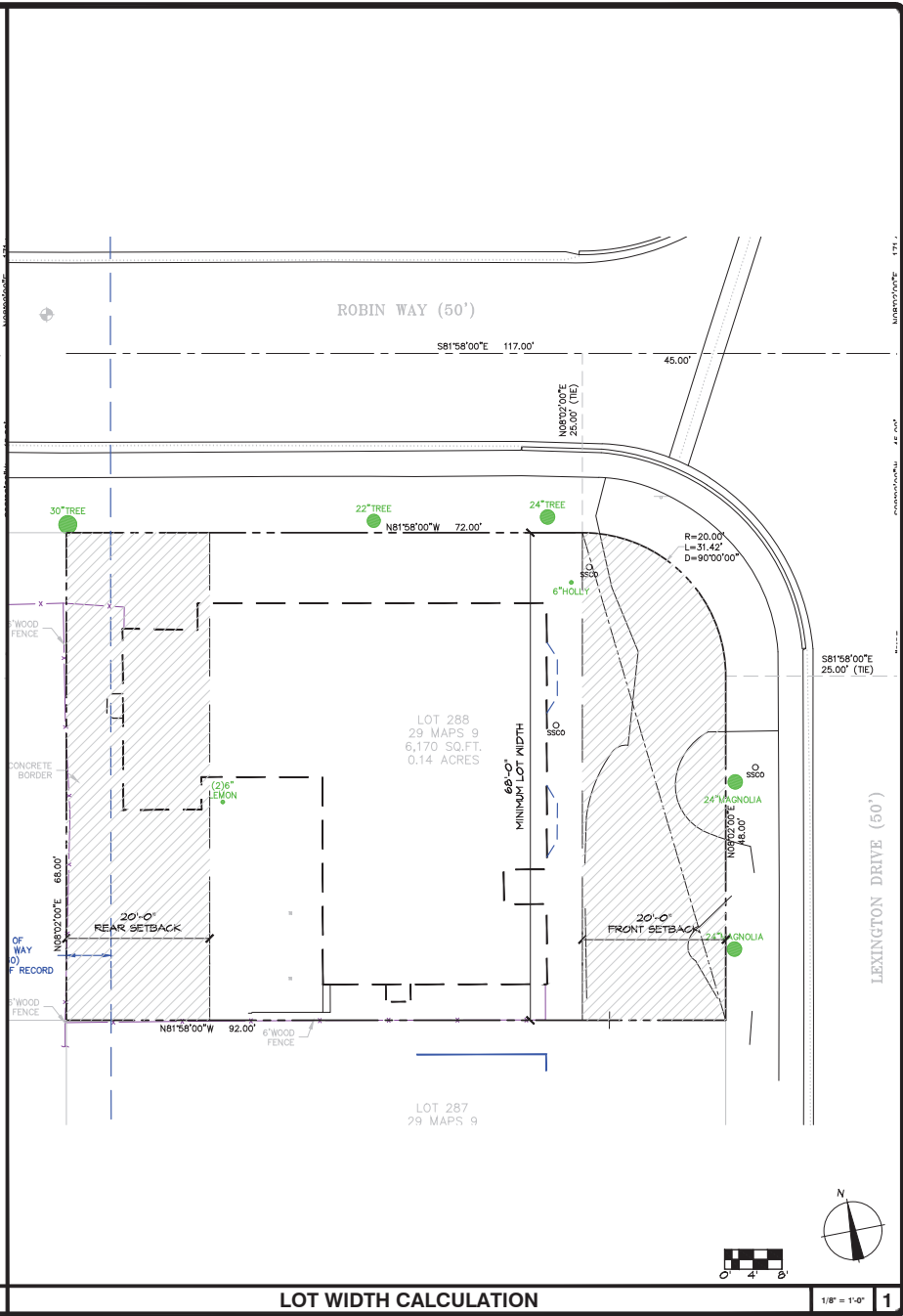
A.P.N. 062-308-120
 CHECKED BY: [Signature] DRAWN BY: DM
 DATE: JULY 28, 2021
 BY: MURLIDHAR

A0.4



LOT DEPTH CALCULATION

1/8" = 1'-0" 2



LOT WIDTH CALCULATION

1/8" = 1'-0" 1

ISSUE LOG

PLANNING SUBMITTAL	DEC 02, 2017
PLANNING REV 1	JULY 18, 2022
PLANNING REV 2	OCT 18, 2022
PLANNING REV 3	OCT 18, 2022
NEIGHBORS REV 4	JAN 18, 2023

YOUNG AND BORLIK ARCHITECTS, INCORPORATED
4962 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
TEL: (650) 688-1950 FAX: (650) 323-1112 www.ybarchitects.com

NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
225 LEXINGTON DRIVE
MENLO PARK, CA 94025

A.P.N. 062-308-120

CHECKED	DATE
AEV	JULY 28, 2021
DATE	DATE
TP	TP
DT	DT

MURLIDHAR

A0.4.1

GENERAL NOTES:
 ALL GRADING, EARTHWORK, FOUNDATION PREPARATION AND DRAINAGE SUBJECT TO RECOMMENDATIONS IN THE SOILS REPORT BY P.S. SOILS, INC. (REPORT DATE SEP. 2021)

SEE GEOTECHNICAL INVESTIGATION REPORT BY "P.S. SOILS," DATED SEPTEMBER 2021, FOR SOILS CONDITIONS & ANALYSIS IN ADDITION TO RECOMMENDATIONS FOR SUBSURFACE PREPARATION, STRUCTURAL DESIGN, & DRAINAGE.

SOILS ENGINEER SHALL OBSERVE AND TEST GRADINGS INCLUDING SUB GRADE PREPARATION TO VERIFY THAT THE CONTRACTOR MEETS THE RECOMMENDED MATERIAL QUALITY, MOISTURE CONDITIONING, AND COMPACTION REQUIREMENTS. SOILS ENGINEER SHALL OBSERVE THE FOOTING EXCAVATIONS PRIOR TO THE PLACEMENT OF REINFORCING STEEL TO CONFIRM THAT THE FOUNDATIONS ARE FOUNDED IN UNDISTURBED, FIRM NATURAL SOILS AND AT THE MINIMUM DEPTH OR DEEPER.

MAINTAIN MIN. 5% SLOPE AWAY FROM FOUNDATION AT LANDSCAPE AREAS, MIN. 2% SLOPE AWAY AT PAVED AREAS, WITHIN 5' OF STRUCTURE.

SETBACK VERIFICATION WILL BE REQUIRED BY A LICENSED SURVEYOR OR CIVIL ENGINEER TO VERIFY THE LOCATION OF STRUCTURES ON THE PROPERTY AND DOCUMENTATION SHALL BE SUBMITTED TO THE CITY BUILDING DEPARTMENT PRIOR TO FOUNDATION INSPECTION.

ALL NEW UTILITY CONNECTIONS PER CITY APPROVAL.

VERIFY DAILY WORK HOURS FOR CONSTRUCTION ALLOWED BY THE CITY OF MENLO PARK.

ADJACENT PROPERTIES, CONTRACTOR AND SUBCONTRACTORS TO RESPECT NEIGHBOR CONCERNS FOR NOISE, PARKING AND MATERIAL/EQUIPMENT STORAGE.

TREE PROTECTION NOTES:
 IT IS UNLAWFUL FOR ANY PERSON TO DAMAGE OR HARM A HERITAGE TREE BY ANY MEANS WHATSOEVER, INCLUDING WITHOUT LIMITATION, VEHICLES, MACHINERY OR BUILDING SUPPLIES, OR MATERIAL (INCLUDING FLUIDS) DURING ANY CONSTRUCTION OR RENOVATION OF STRUCTURES ON THE PARCEL.

TREE PROTECTION MEASURES TO BE IN PLACE PRIOR TO DEMOLITION AND THROUGHOUT CONSTRUCTION ACTIVITIES. IF NECESSARY, A CONSULTING ARBORIST SHALL BE RETAINED TO MONITOR THE CONDITION OF ANY HERITAGE TREES, AND CONDUCT ANY ROOT OR CROWN PRUNING DURING THE CONSTRUCTION PROCESS.

PROVIDE 6" LAYER OF MULCH ON GRADE OVER ROOT ZONES WITHIN TREE CANOPIES OF PROTECTED TREES. PROVIDE TEMPORARY CONSTRUCTION FENCING AS SHOWN TO PROTECT ENTIRE ROOT ZONE TO OUTSIDE OF THE TREE CANOPY PERIMETER. FENCE SHOULD BE IN PLACE PRIOR TO ARRIVAL OF ANY MATERIALS OR EQUIPMENT AND SHOULD REMAIN IN PLACE UNTIL ALL CONSTRUCTION IS COMPLETED AND GIVEN FINAL APPROVAL. PROTECTIVE FENCING MUST NOT BE TEMPORARILY MOVED DURING CONSTRUCTION.

TREE PROTECTION NOTES (CONT'D):
 PROVIDE TEMPORARY CONSTRUCTION FENCING AS SHOWN TO PROTECT ENTIRE ROOT ZONE TO OUTSIDE OF THE TREE CANOPY PERIMETER. FENCE SHOULD BE IN PLACE PRIOR TO ARRIVAL OF ANY MATERIALS OR EQUIPMENT AND SHOULD REMAIN IN PLACE UNTIL ALL CONSTRUCTION IS COMPLETED AND GIVEN FINAL APPROVAL. PROTECTIVE FENCING MUST NOT BE TEMPORARILY MOVED DURING CONSTRUCTION.

NO GRADING, TRENCHING, OR SURFACE SCRAPING INSIDE THE CANOPY PERIMETER OF RETAINED TREES, UNLESS SPECIFICALLY INDICATED ON THE ENGLOUED PLANS. EXCAVATED SOIL OR EQUIPMENT MAY NOT BE STORED, TEMPORARILY OR EXTENDED, UNDER THE CANOPIES OF TREES.

ANY PRUNING MUST BE DONE BY I.S.A. CERTIFIED ARBORIST AND ACCORDING TO I.S.A. WESTERN CHAPTER STANDARDS, 1988

TRENCHES SHALL BE OUTSIDE THE DRIP LINES OF THE TREES IN ORDER TO MINIMIZE NEGATIVE IMPACTS. ANY TRENCHING WITHIN A DISTANCE SIX TIMES THE DIAMETER OF THE TREE SHALL BE HAND EXCAVATED, AND ANY ROOTS ENCOUNTERED SHALL BE REVIEWED ON SITE BY THE PROJECT ARBORIST.

TREE PROTECTION NOTES (CONT'D):
 NO STORAGE OF MATERIALS, SOIL, VEHICLES, OR DEBRIS WITHIN THE TREE DRIP LINES SHALL BE PERMITTED DURING ANY TIME DURING DEMOLITION OR CONSTRUCTION ACTIVITIES.

LANDSCAPE MATERIALS (COBBLE, DECORATIVE BARK, STONES, FENCING, ETC.) MUST NOT BE INSTALLED DIRECTLY IN CONTACT WITH THE BARK OF TREES TO PREVENT RISK OF SERIOUS DISEASE OF INFECTION. LANDSCAPE PATHWAYS OR OTHER ADVENTIES (IF ANY) CONSTRUCTED UNDER TREE CANOPIES MUST BE COMPLETELY ON GRADE WITHOUT EXCAVATION.

SEE ALSO A0.4 EXISTING PLAN & PROPOSED SITE PLAN FOR ADDITIONAL REQUIREMENTS

SEE LANDSCAPE SHEETS FOR LANDSCAPE PLANS

AVG. NATURAL GRADE CALCULATIONS SEE SURVEY SU 1 AND A0.4 FOR NUMBERS

41.3, 41.5, 41.6, 41.9, 41.13, 41.16

(N) OUTDOOR CONDENSERS & PAD LOCATION. A/C UNIT SOUND SHALL NOT EXCEED 60 DBA DURING THE DAYTIME HOURS OR 50 DBA DURING THE NIGHTTIME HOURS AT THE NEAREST RESIDENTIAL PROPERTY LINE.

(E) CONCRETE CURB
 (E) SIDE WALK
 CORNER TRIANGLE AREA MEASURED 55 FEET FROM THE INTERSECTION OF BOTH PROPERTY LINE TANGENTS (SHOWN GREEN DASHED)

(N) 18" 9" ADU SHOWN SHADED
 (N) ADU PATH OF TRAVEL

(E) SANITARY SEWER MAIN LINE
 OUTLINE OF PROPOSED SECOND STORY (SHOWN DASHED BLUE LINE)
 (E) WATER METER LOCATION TO REMAIN.
 (E) 4" SEWER LATERAL AND SANITARY SEWER CLEANOUT. VERIFY (E) CONDITION OF LINE AND DETERMINE IF NEW IS REQUIRED PER MENLO PARK PUBLIC WORKS.

PROVIDE ADDRESS SIGNAGE ON THE FRONT OF RESIDENCE PLACED IN A POSITION THAT IS PLAINLY LEGIBLE, CONTRAST TO ITS BACKGROUND AND VISIBLE FROM STREET OR ROAD FRONTING THE PROPERTY. NUMBERS SHALL BE 4" HIGH WITH 1/2" STROKE. VERIFY ADDRESS LOCATIONS W/ FIRE INSPECTOR FOR CONFORMANCE WITH REGM.T.

(N) DRIVEWAY. SEE LANDSCAPE PLAN

(N) 20' X 30' INTERIOR CLEAR COVERED GARAGE PARKING

LEGEND:
 - NEIGHBORING HOUSE FOOTPRINT PRINTED IN BLUE
 - UTILITY EASEMENT
 - SECOND FLOOR FOOTPRINT
 - (E) FENCE IN PURPLE
 - TREE PROTECTION ZONE
 - LINE OF SIGHT TRIANGLE
 - #XX NUMBERED TREE TO REMAIN
 - PATH OF TRAVEL
 - PROPOSED ADU SHADED
 - PVIOUS STEPPING STONES

TREE SCHEDULE

#	TYPE	DIA.	STATUS
1	SOUTHERN MAGNOLIA	24"	REMAIN
2	SOUTHERN MAGNOLIA	24"	REMAIN
3	GAMPHOR TREE	24"	REMAIN
4	GAMPHOR TREE	22"	REMAIN
5	GAMPHOR TREE	30"	REMAIN
6	HOLLY	6"	REMOVE
7	FRUIT TREE (LEMON)	6"	REMAIN

NEW LANDSCAPE SCREENING

#	TYPE	SIZE
8	PACIFIC MAX MYRTLE	24" BOX
9	PACIFIC MAX MYRTLE	24" BOX
10	PACIFIC MAX MYRTLE	24" BOX
11	PACIFIC MAX MYRTLE	24" BOX
12	PACIFIC MAX MYRTLE	24" BOX
13	PACIFIC MAX MYRTLE	24" BOX
14	PACIFIC MAX MYRTLE	24" BOX
15	PACIFIC MAX MYRTLE	24" BOX
16	PACIFIC MAX MYRTLE	24" BOX
17	PACIFIC MAX MYRTLE	24" BOX
18	CAROLINA CHERRY LAUREL	24" BOX
19	CAROLINA CHERRY LAUREL	24" BOX
20	CAROLINA CHERRY LAUREL	24" BOX
21	CAROLINA CHERRY LAUREL	24" BOX
22	CAROLINA CHERRY LAUREL	24" BOX
23	CAROLINA CHERRY LAUREL	24" BOX
24	CAROLINA CHERRY LAUREL	24" BOX
25	CAROLINA CHERRY LAUREL	24" BOX
26	CAROLINA CHERRY LAUREL	24" BOX
27	CAROLINA CHERRY LAUREL	24" BOX
28	CAROLINA CHERRY LAUREL	24" BOX
29	CAROLINA CHERRY LAUREL	24" BOX
30	CAROLINA CHERRY LAUREL	24" BOX
31	CAROLINA CHERRY LAUREL	24" BOX
32	CAROLINA CHERRY LAUREL	24" BOX
33	CAROLINA CHERRY LAUREL	24" BOX
34	CAROLINA CHERRY LAUREL	24" BOX
35	CAROLINA CHERRY LAUREL	24" BOX
36	CAROLINA CHERRY LAUREL	24" BOX
37	CAROLINA CHERRY LAUREL	24" BOX
38	PACIFIC MAX MYRTLE	24" BOX
39	PACIFIC MAX MYRTLE	24" BOX
40	PACIFIC MAX MYRTLE	24" BOX
41	PACIFIC MAX MYRTLE	24" BOX
42	PACIFIC MAX MYRTLE	24" BOX

(E) ELECTRIC LINE TO REMAIN

SEPARATE ENTRANCE TO NEW ADU. PROVIDE DOOR WHICH IS LOCKABLE FROM THE INSIDE AND OUTSIDE

(N) 400 AMP ELECTRIC METER. VERIFY ALL REQUIREMENTS WITH P.S.E. COUNTY INSPECTOR AND OWNER IN FIELD PRIOR TO WORK. NO UTILITIES ARE PERMITTED BELOW THE DFE (BPEH). WATER AND SEWER PIPES, SEALED TO PREVENT FLOOD WATER INTRUSION, ARE ALLOWED.

(E) OUTLINE OF NEIGHBOR'S HOME

(N) 20'-0" (N) 2ND STORY

(N) CRAWL SPACE ACCESS LOCATION. VERIFY EXACT LOCATION IN FIELD

(E) REAR SETBACK

(E) WOOD SIDE NEIGHBOR FENCE, 6 FOOT HEIGHT

LAWN AREA - SEE LANDSCAPE PLANS FOR ADDITIONAL INFORMATION

SITE SETBACK - TYP.

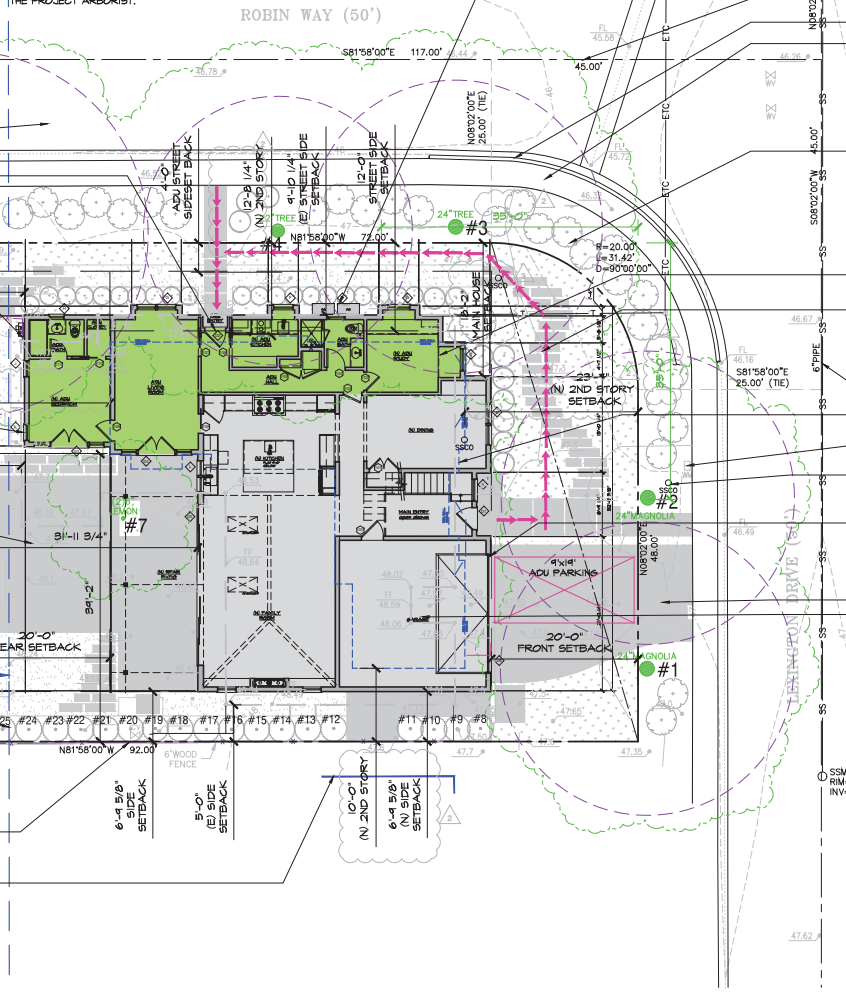
PROPERTY LINE DASHED - TYP.

P.S.E. UTILITY EASEMENT

APPROXIMATE OF PG&E RIGHT OF WAY (677 O.R. 360) EXACT LIMITS NOT OF RECORD

(E) WOOD SIDE NEIGHBOR FENCE, 6 FOOT HEIGHT SHOWN IN PURPLE

(E) OUTLINE OF NEIGHBOR'S HOME



PROPOSED SITE PLAN



1/8" = 1'-0" 1

ISSUE LOG	
PLANNING SUBMITTAL	DEC 20, 2021
PLANNING REV 1	JULY 14, 2021
PLANNING REV 2	SEP 15, 2021
PLANNING REV 3	SEP 15, 2021
PLANNING REV 4	JAN 18, 2023

NEIGHBORS REV 4
 JAN 18, 2023

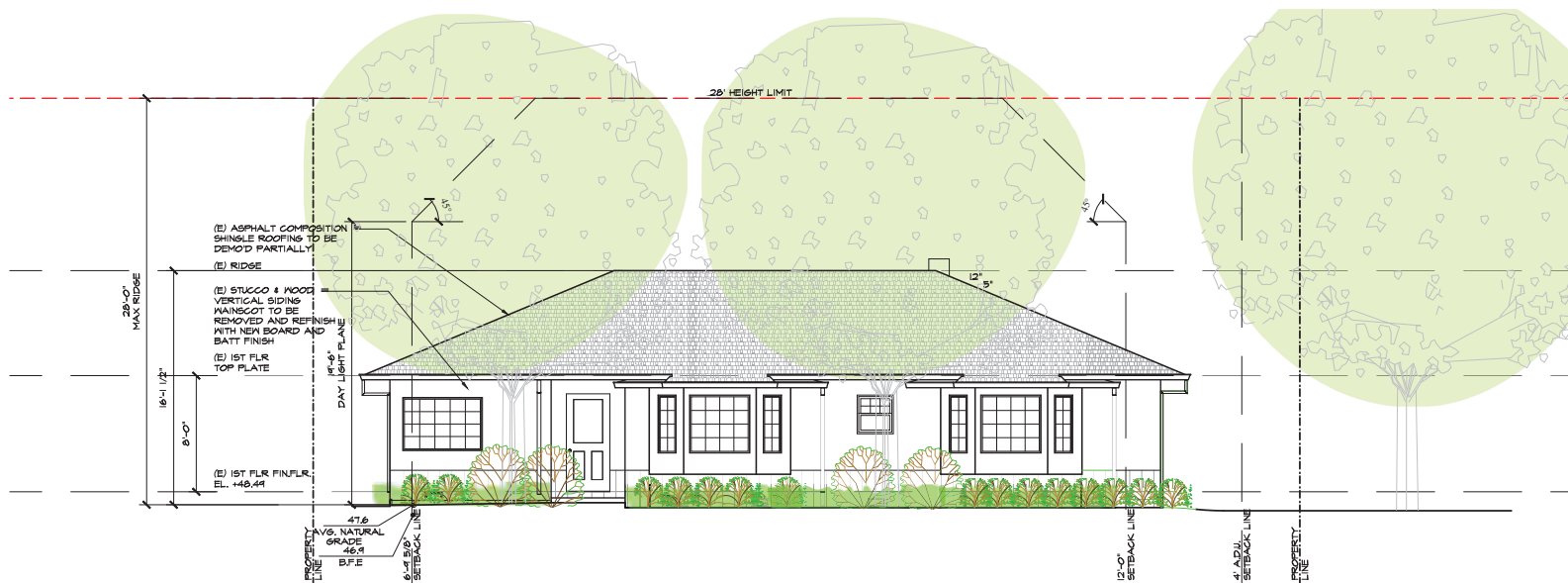
YOUNG AND BORLIK
 ARCHITECTS, INCORPORATED
 4962 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
 TEL: (650) 688-1950 FAX: (650) 323-1112 www.ybarchitects.com



NEW REFERENCE FOR:
RUCHI & SID MURLIDHAR
 225 LEXINGTON DRIVE
 MENLO PARK, CA 94025

A.P.N. 062-308-120	
CHECKED BY	DRAWN BY
DATE	DATE
JULY 28, 2021	
BY	
MURLIDHAR	

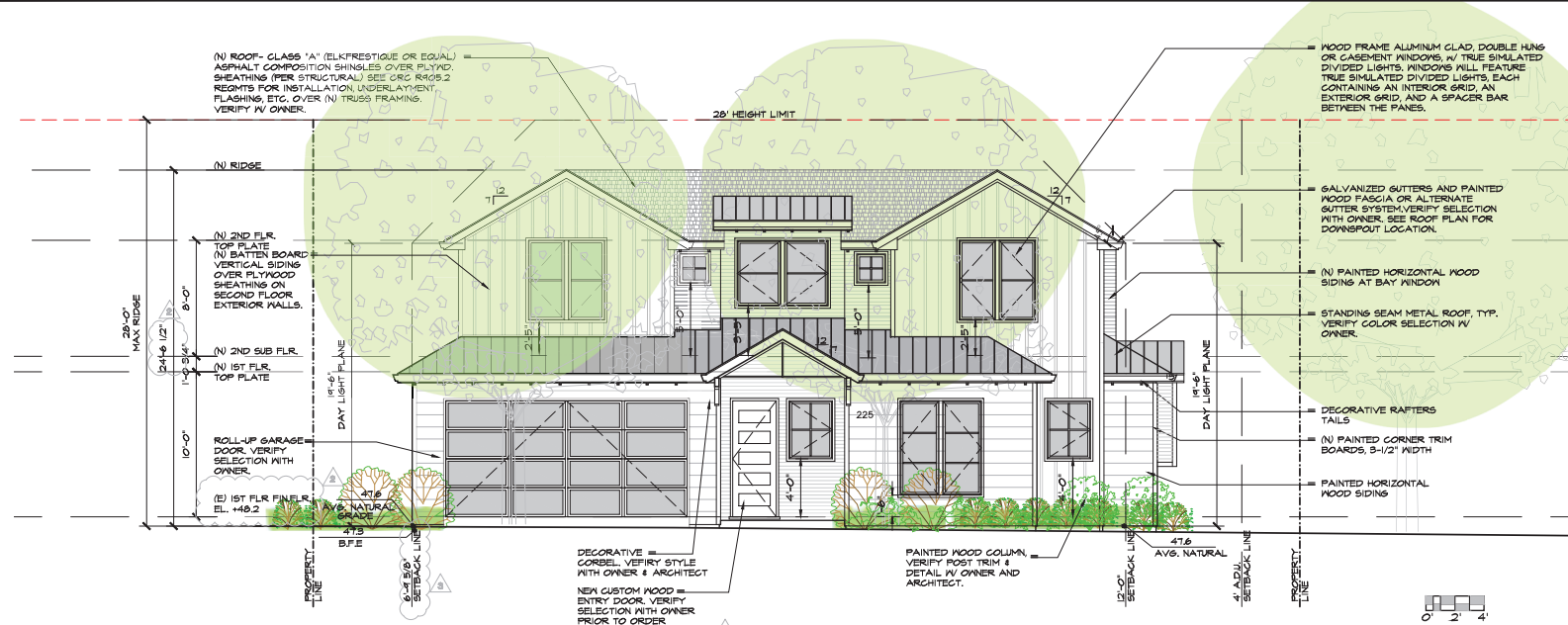
A0.5



EXISTING FRONT (EAST) - LEXINGTON DRIVE FRONTAGE ELEVATION

1/4" = 1'-0"

2



PROPOSED FRONT (EAST) - LEXINGTON DRIVE FRONTAGE ELEVATION

1/4" = 1'-0"

1

ISSUE LOG	
PLANNING SUBMITTAL	DEC 02, 2021
PLANNING REV 1	FEB 04, 2022
PLANNING REV 2	MAY 13, 2022
PLANNING REV 3	SEP 08, 2022
PLANNING REV 4	NOV 01, 2022
NEIGHBORS REV 1	JAN 18, 2023

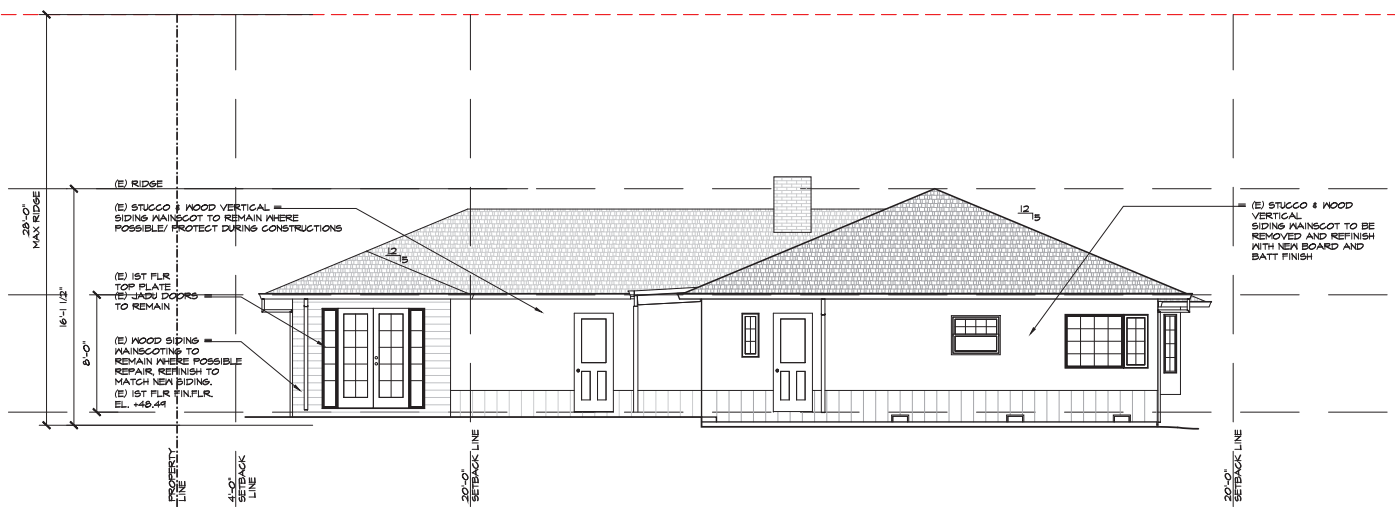
YOUNG AND BORLIK
ARCHITECTS, INCORPORATED
4862 EL CAMINO REAL, SUITE 225 LOS ALTOS, CA 94022
TEL: (650) 688-8598 FAX: (650) 323-1112 www.yabarchitects.com

ARCHITECT
REGISTERED
C-21879
STATE OF CALIFORNIA

NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
225 LEXINGTON DRIVE
MENLO PARK, CA 94025

A.P.N. 062-305-120
CHECKED BY: [Signature] DATE: JULY 26, 2021
DATE: [Signature] MURLIDHAR

A3.1



EXISTING LEFT (SOUTH) SIDE ELEVATION

1/4" = 1'-0" 2



PROPOSED LEFT (SOUTH) SIDE ELEVATION

1/4" = 1'-0" 1

ISSUE LOG

PLANNING SUBMITTAL	NOV 02, 2017
PLANNING REV 1	JULY 18, 2022
PLANNING REV 2	SEP 28, 2022
PLANNING REV 3	OCT 18, 2022
NEIGHBORS REV 4	JAN 18, 2023

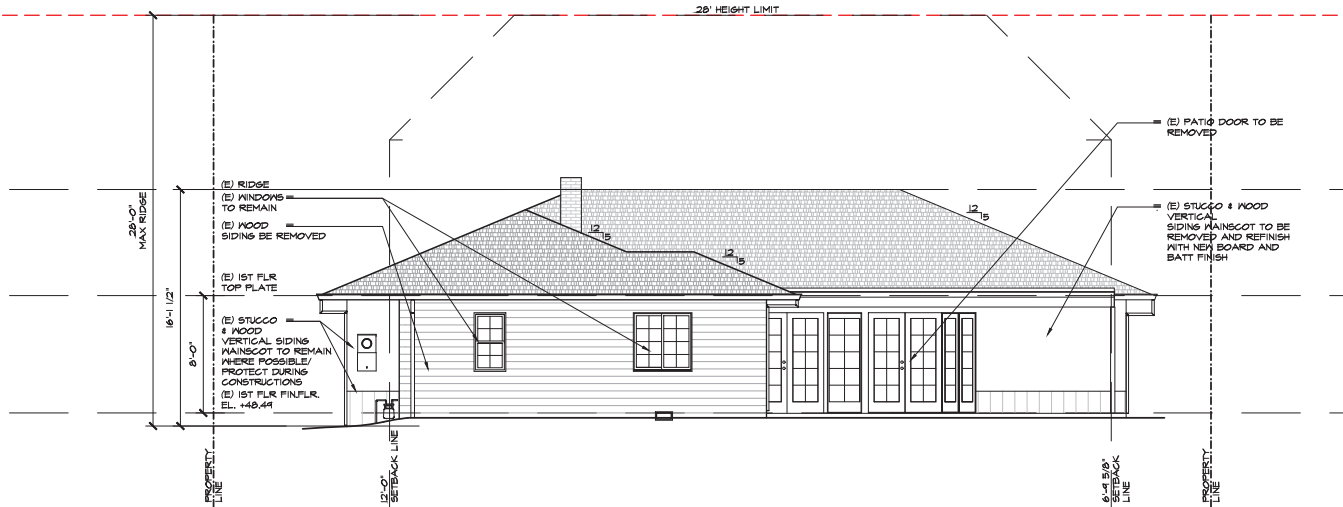
YOUNG AND BORLIK
ARCHITECTS, INCORPORATED
4862 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
TEL: (650) 688-1950 FAX: (650) 324-1112 www.ybarchitects.com



NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
225 LEXINGTON DRIVE
MENLO PARK, CA 94025

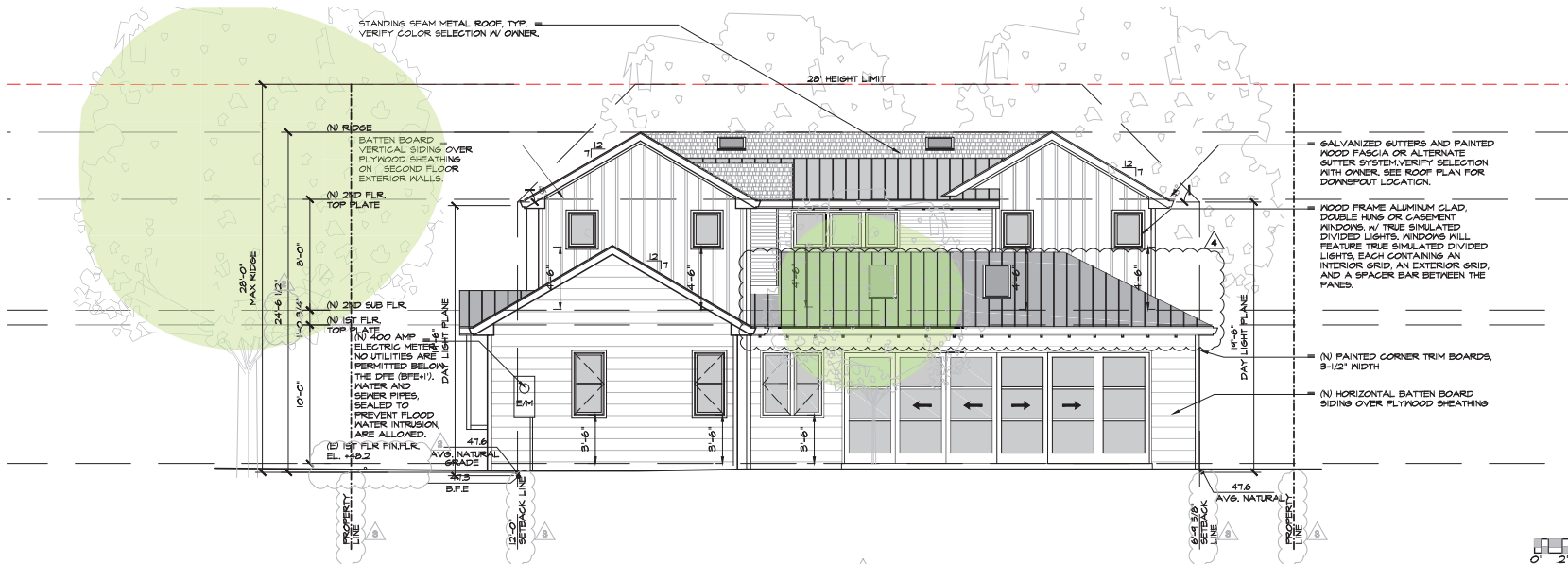
A.P.N. 062-305-120
CHECKED BY: MUR
DATE: JULY 28, 2021
BY: MURLIDHAR

A3.2



EXISTING REAR (WEST) ELEVATION

1/4" = 1'-0" 2



PROPOSED REAR (WEST) ELEVATION

1/4" = 1'-0" 1

ISSUE LOG

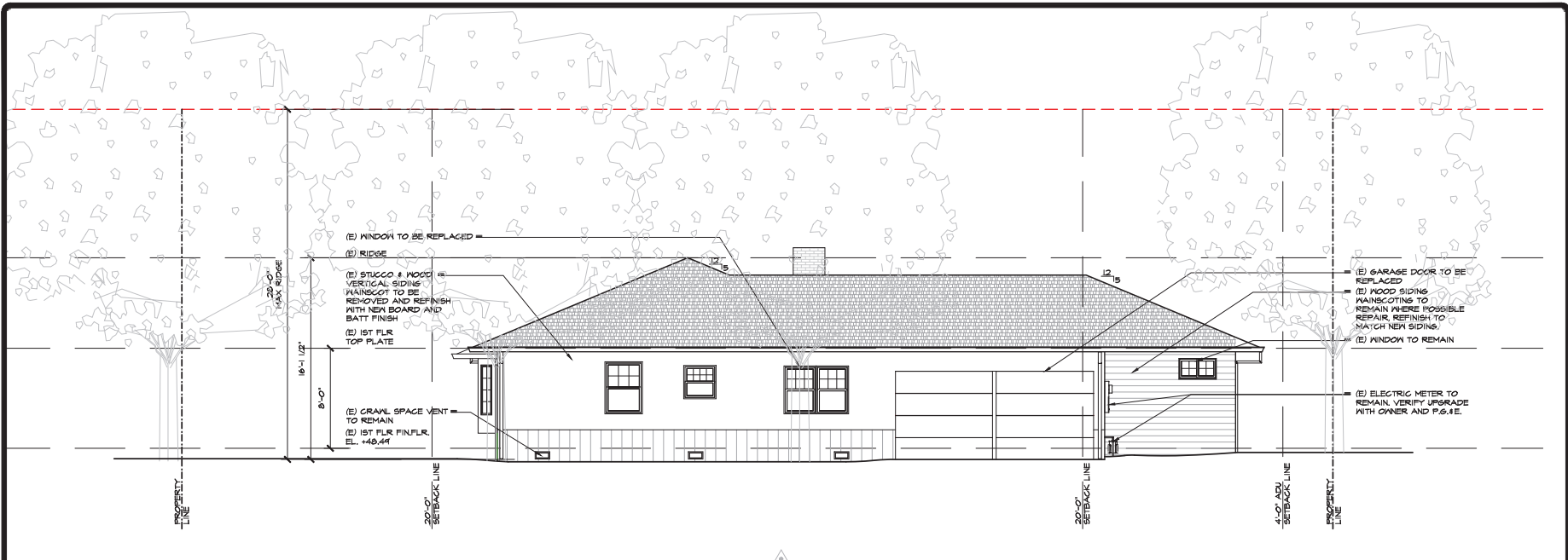
PLANNING SUBMITTAL	DEC 02, 2017
PLANNING REV 1	JULY 18, 2021
PLANNING REV 2	SEP 14, 2021
PLANNING REV 3	OCT 14, 2021
PLANNING REV 4	JAN 18, 2023

YOUNG AND BORLIK ARCHITECTS, INCORPORATED
 4802 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
 TEL: (650) 688-1950 FAX: (650) 323-1112 www.ybarchitects.com

NEW RESIDENCE FOR: RUCHI & SID MURLIDHAR
 225 LEXINGTON DRIVE
 MENLO PARK, CA 94025

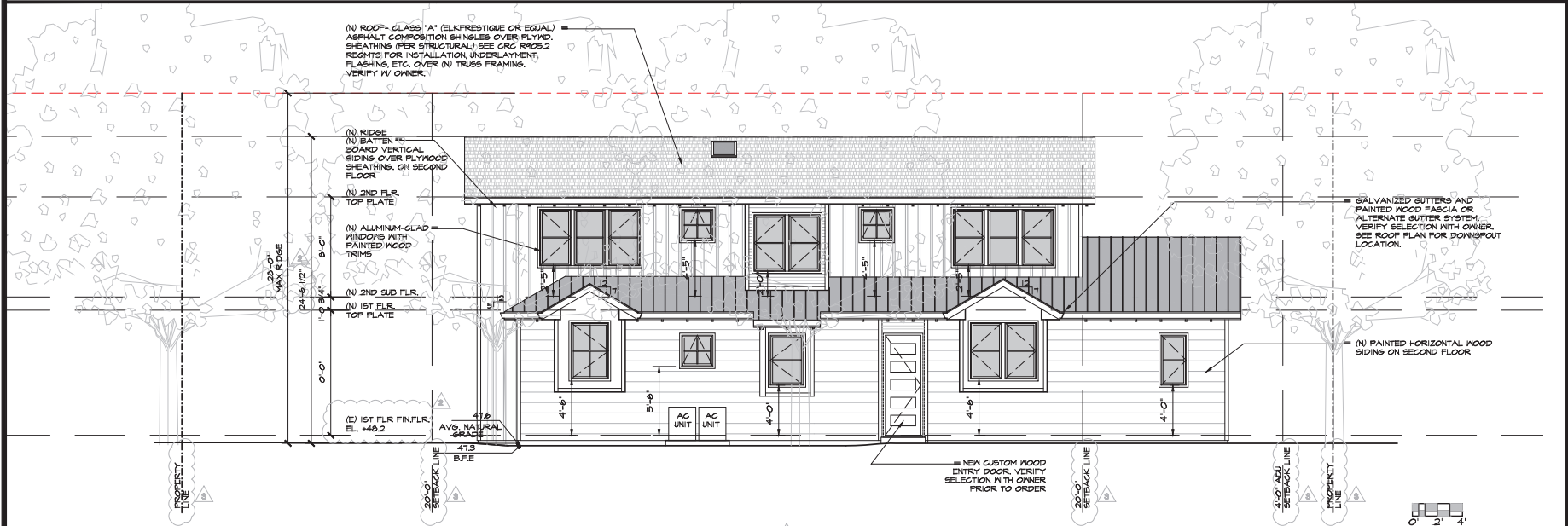
A.P.N. 062-305-120
 CHECKED BY: AVEY DATE: JULY 28, 2021
 DRAWN BY: TP, DT
 ARCHITECT: MURLIDHAR

A3.3



EXISTING RIGHT (NORTH) SIDE - ROBIN WAY FRONTAGE ELEVATION

1/4" = 1'-0" 2



PROPOSED RIGHT (NORTH) SIDE - ROBIN WAY FRONTAGE ELEVATION

1/4" = 1'-0" 1

ISSUE LOG	
PLANNING SUBMITTAL	DEC 02, 2017
PLANNING REV 1	JULY 18, 2018
PLANNING REV 2	SEP 24, 2018
PLANNING REV 3	NOV 14, 2018
NEIGHBORS REV 4	JAN 18, 2023

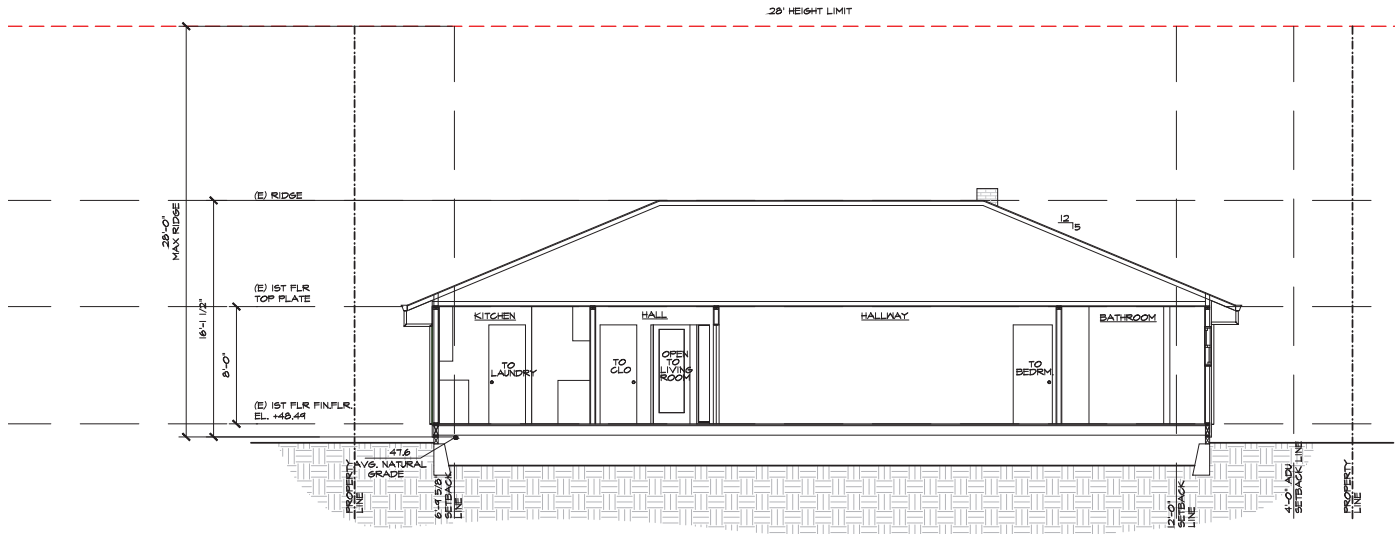
YOUNG AND BORLIK
 ARCHITECTS, INCORPORATED
 482 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
 TEL: (650) 688-1950 FAX: (650) 323-1112 www.ybarchitects.com



NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
 225 LEXINGTON DRIVE
 MENLO PARK, CA 94025

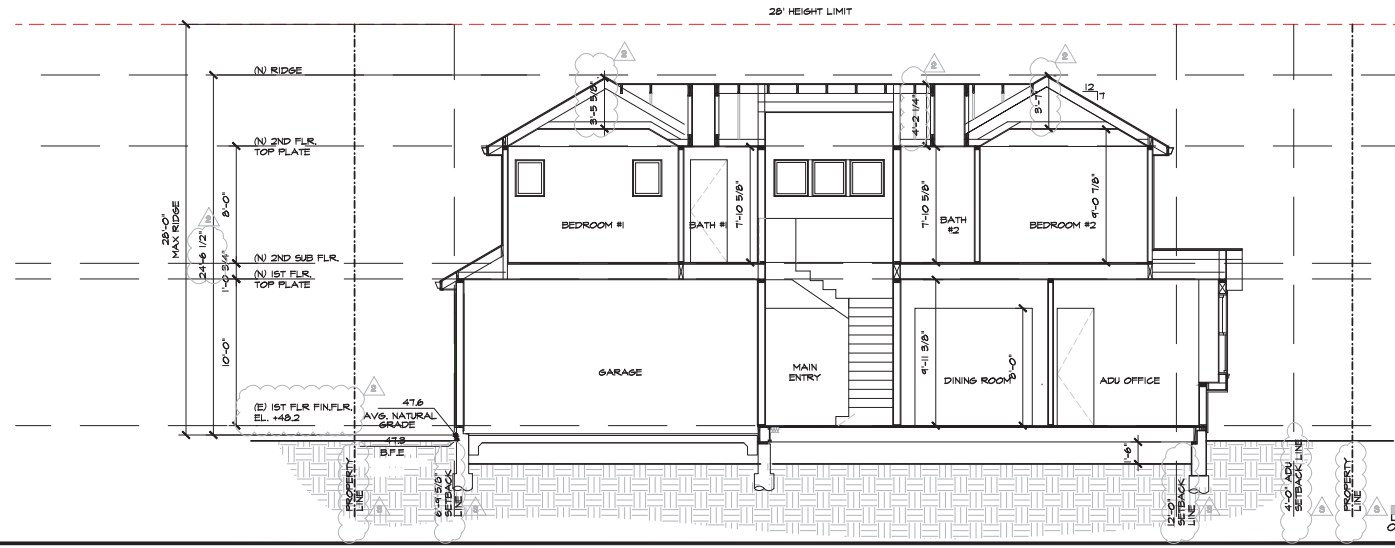
A.P.N. 062-305-120	
CHECKED BY: AVE	DRAWN BY: DT
DATE: JULY 28, 2021	
BY: MURLIDHAR	

A3.4



EXISTING SECTION

1/4" = 1'-0" 2



PROPOSED SECTION

1/4" = 1'-0" 1

ISSUE LOG	
△	PLANNING SUBMITTAL DEC 02, 2017
△	PLANNING REV 1 JULY 13, 2018
△	PLANNING REV 2 OCT 14, 2018
△	PLANNING REV 3 OCT 14, 2018
△	NEIGHBORS REV 4 JAN 18, 2023

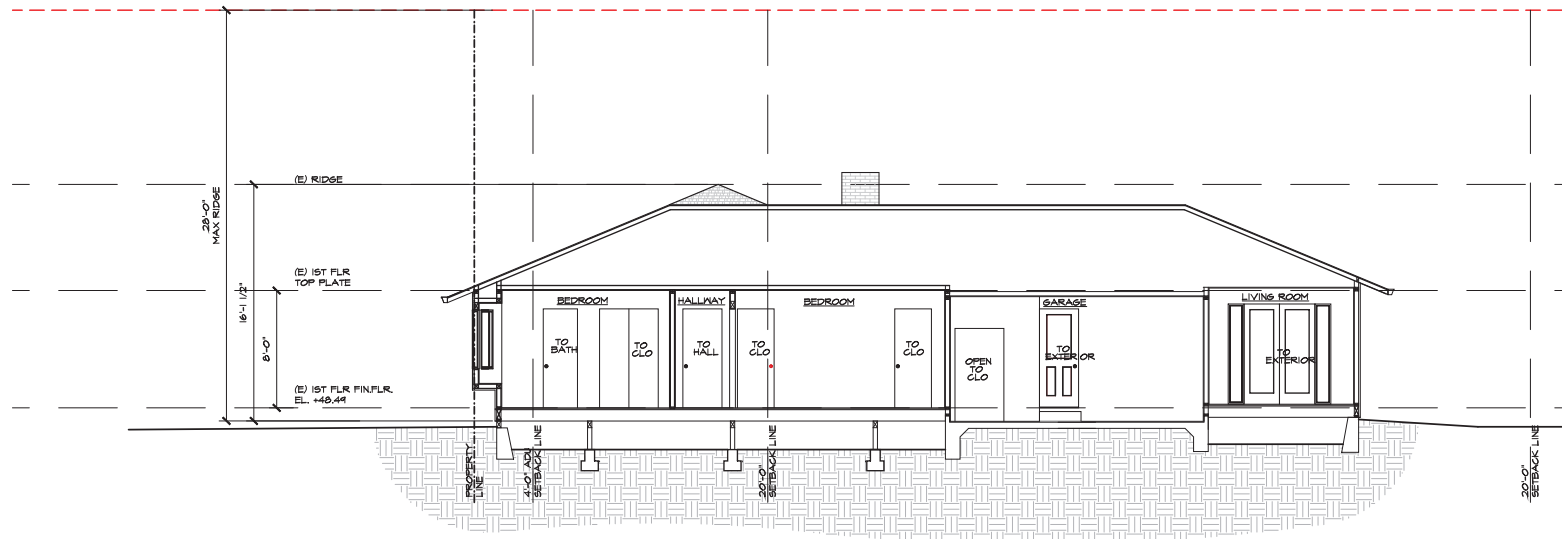
YOUNG AND BORLIK
 ARCHITECTS, INCORPORATED
 482 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
 TEL: (650) 688-1950 FAX: (650) 323-1112 www.ybarchitects.com



NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
 225 LEXINGTON DRIVE
 MENLO PARK, CA 94025

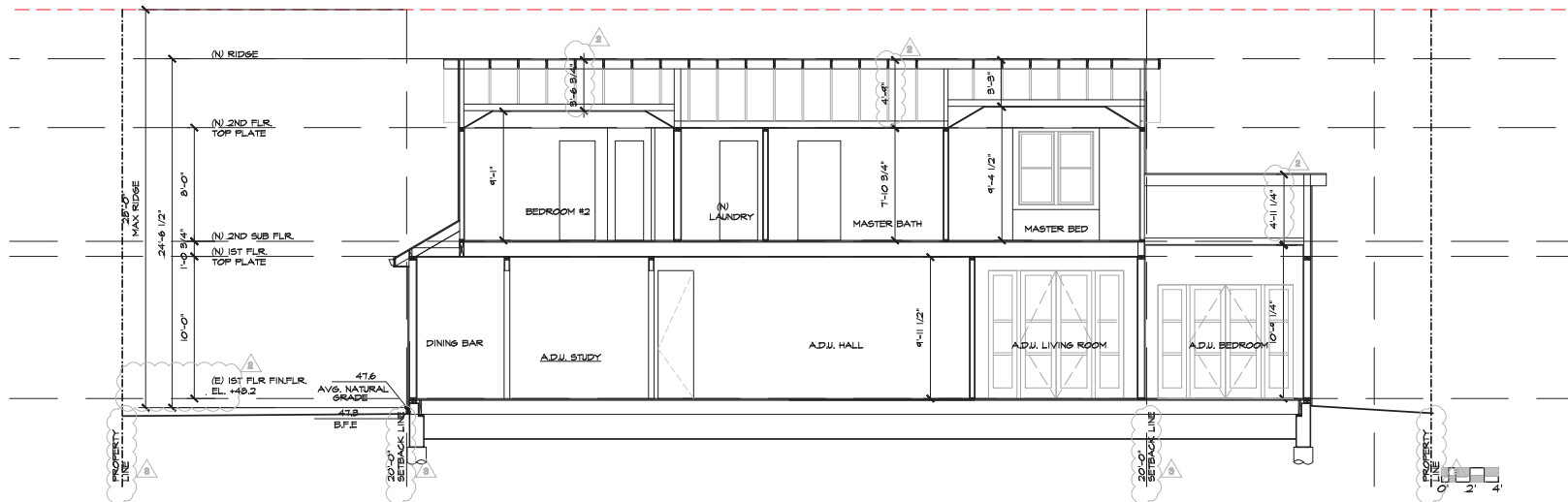
A.P.N. 062-305-120	
CHECKED BY: AVE	DRAWN BY: TP, DT
DATE: JULY 28, 2021	DATE: 07/28/21
BY: MURLIDHAR	

A4.1



EXISTING SECTION

1/4" = 1'-0" 2



PROPOSED SECTION

1/4" = 1'-0" 1

ISSUE LOG	
△	PLANNING SUBMITTAL DEC 02, 2017
△	PLANNING REV 1 JULY 13, 2018
△	PLANNING REV 2 OCT 14, 2018
△	PLANNING REV 3 OCT 14, 2018
△	NEIGHBORS REV 4 JAN 18, 2023

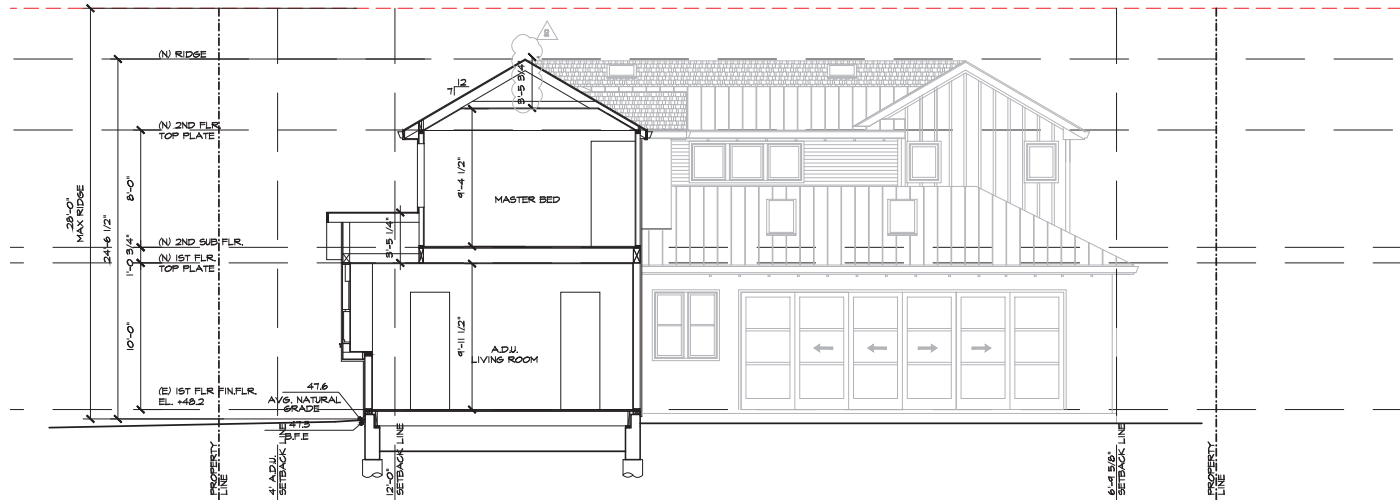
YOUNG AND BORLIK
 ARCHITECTS, INCORPORATED
 4962 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
 TEL: (650) 688-1950 FAX: (650) 323-1112 www.ybarchitects.com



NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
 225 LEXINGTON DRIVE
 MENLO PARK, CA 94025

A.P.N. 062-305-120
 CHECKED BY: [Signature] DATE: JULY 28, 2021
 DRAWN BY: [Signature] DATE: [Blank]
 PROJECT NO.: MURLIDHAR

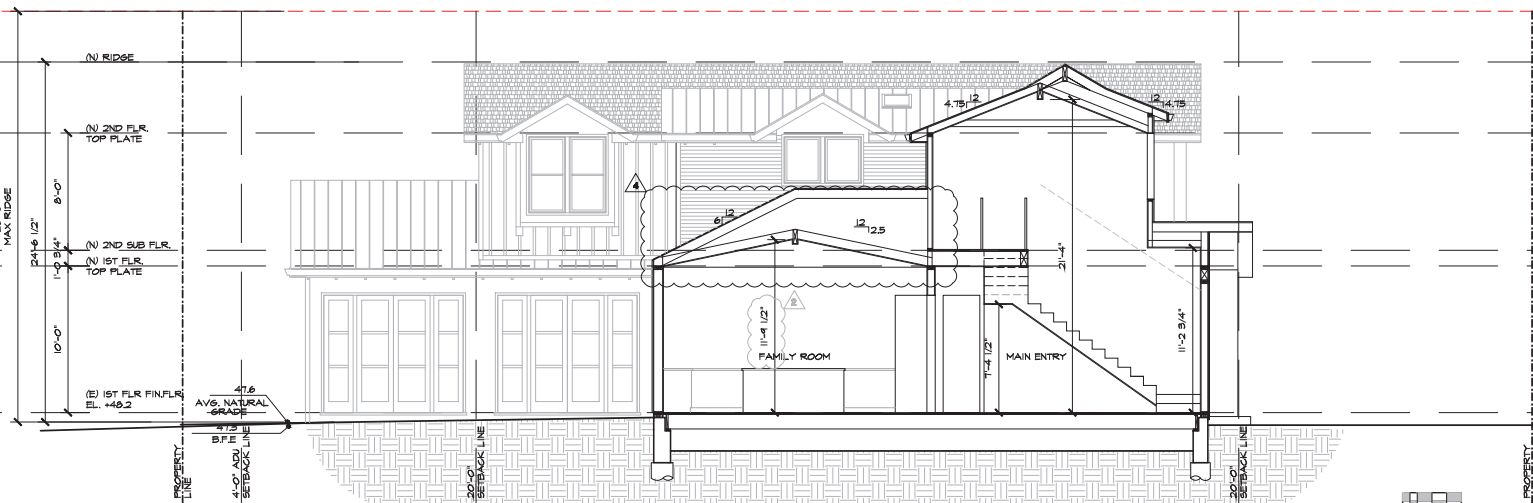
A4.2



SECTION THROUGH MAIN HOUSE MASTER BEDROOM & A.D.U. LIVING ROOM

1/4" = 1'-0"

2



SECTION THROUGH NEW FAMILY ROOM & ENTRY HALLWAY

1/4" = 1'-0"

1

ISSUE LOG	
△	PLANNING SUBMITTAL
△	PLANNING REV 1
△	PLANNING REV 2
△	PLANNING REV 3
△	PLANNING REV 4
△	NEIGHBORS REV 1
△	NEIGHBORS REV 2
△	NEIGHBORS REV 3
△	NEIGHBORS REV 4
△	NEIGHBORS REV 5

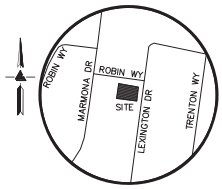
YOUNG AND BORLIK
 ARCHITECTS, INCORPORATED
 482 EL CAMINO REAL, SUITE 218 LOS ALTOS, CA 94022
 TEL: (650) 688-1950 FAX: (650) 323-1112 www.yabarchitects.com



NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
 225 LEXINGTON DRIVE
 MENLO PARK, CA 94025

A.P.N. 062-305-120	
CHECKED BY	DATE
AY	JULY 28, 2021
BY	DATE
MURLIDHAR	

A4.3



VICINITY MAP
NO SCALE

LEGEND AND NOTES

- BOUNDARY LINE
- - - FEMA BASE FLOOD LINE
- - - BUILDING OVERHANG LINE
- ETC ELECTRICAL/TELEPHONE/CABLE TV OVERHEAD LINE
- E ELECTRICAL OVERHEAD LINE
- T TELEPHONE/ OVERHEAD LINE
- - - EASEMENT
- x FENCE LINE
- FLOW LINE
- SS SANITARY SEWER LINE
- A/C AIR CONDITIONING UNIT
- ⊕ BENCHMARK
- EM ELECTRICAL METER
- FF FINISH FLOOR
- ⊕ FIRE HYDRANT
- FL FLOW LINE
- GM GAS METER
- INV INVERT
- ICV IRRIGATION CONTROL VALVE
- ⊕ JOINT POLE
- ⊕ PILLAR, OR SIMILAR
- RP ROOF PEAK
- SSCO SANITARY SEWER CLEAN-OUT
- SSMH SANITARY SEWER MAINTENANCE HOLE
- TS STREET SIGN
- TC TOP OF CURB
- TOS TOP OF SLAB
- WM WATER METER
- WV WATER VALVE
- XXX SPOTGRADE
- ASPHALT
- BRICK
- CONCRETE
- LAWN
- RIVER ROCK
- WOOD

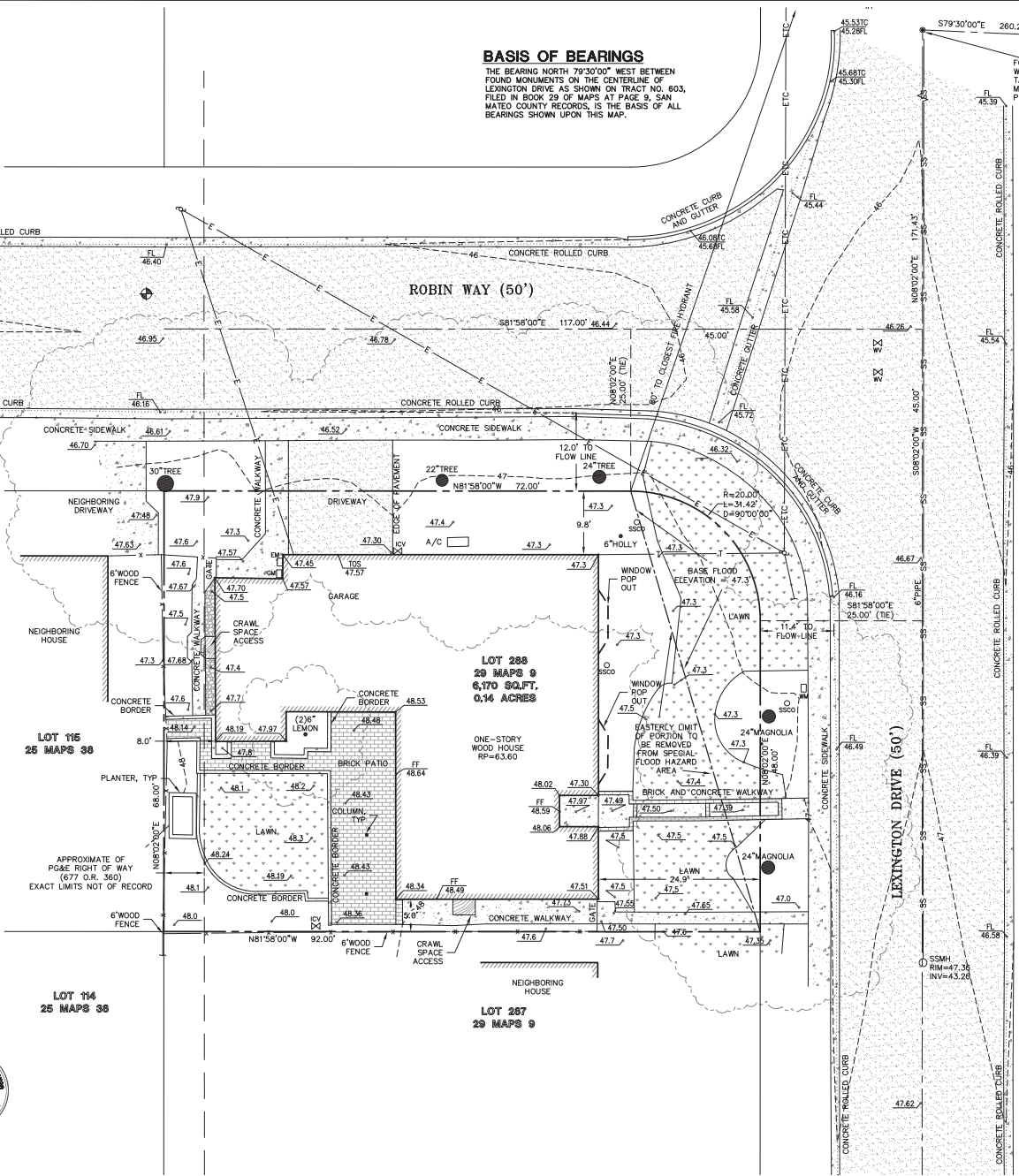
BASIS OF BEARINGS

THE BEARING NORTH 79°30'00" WEST BETWEEN FOUND MONUMENTS ON THE CENTERLINE OF LEXINGTON DRIVE AS SHOWN ON TRACT NO. 603, FILED IN BOOK 29 OF MAPS AT PAGE 9, SAN MATEO COUNTY RECORDS, IS THE BASIS OF ALL BEARINGS SHOWN UPON THIS MAP.

SURVEYOR'S STATEMENT

I CERTIFY THAT THIS PARCEL'S BOUNDARY WAS ESTABLISHED BY ME OR UNDER MY SUPERVISION AND IS BASED ON A FIELD SURVEY IN CONFORMANCE WITH THE LAND SURVEYOR'S ACT. ALL MONUMENTS ARE OF THE CHARACTER AND OCCUPY THE POSITIONS INDICATED AND ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.

GREGORY F. BRAZE DATE
L.S. NO. 7623



NOTES

- ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS.
- BUILDING FOOTPRINTS ARE SHOWN TO FINISHED MATERIAL (STUCCO/SIDING) AT GROUND LEVEL.
- FINISH FLOOR ELEVATIONS ARE TAKEN AT DOOR THRESHOLD (EXTERIOR).
- THE AREA OF THE SURVEYED LOT IS 6,170± SQUARE FEET / 0.14± ACRES

EASEMENT NOTE

EASEMENTS ARE SHOWN PER PRELIMINARY TITLE REPORT ISSUED BY CHICAGO TITLE COMPANY, ORDER NO. 12-94723695-MA, DATED DECEMBER 13, 2011.

BENCHMARK

SANTA CLARA VALLEY WATER DISTRICT 8M455 BRASS DISK ON TOP OF CONCRETE ISLAND AT THE SOUTHWESTERLY CORNER OF UNIVERSITY AVENUE AND WOODLAND AVENUE. ELEVATION = 37.42' (NAVD88)

SITE BENCHMARK

SURVEY CONTROL POINT MAG AND SHINER SET IN ASPHALT ELEVATION = 46.90' (NAVD 88 DATUM)

FEMA FLOOD NOTE

PORTIONS OF SUBJECT PROPERTY ARE LOCATED IN FLOOD ZONE: AE
100-YEAR BASE FLOOD ELEVATION (BFE): 47.3' (NAVD88 DATUM)
PER CURRENT FLOOD INSURANCE RATE MAP (FIRM)
FEMA FLOOD INSURANCE RATE MAP NO. 06080C0308
EFFECTIVE DATE: OCTOBER 18, 2012

UTILITY NOTE

ALL UNDERGROUND PIPE TYPES, SIZES AND LOCATION SHOWN ON THIS SURVEY ARE BASED ON VISUAL OBSERVATION. ANY USE OF THIS INFORMATION SHOULD BE VERIFIED, BEFORE ITS USE, WITH THE CONTROLLING MUNICIPALITY OR UTILITY PROVIDER. THIS SURVEY MAKES NO GUARANTEE OF THE INSTALLED ACTUAL LOCATION, DEPTHS OR SIZE.

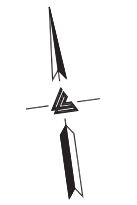
TREE NOTE

TREE SIZE, TYPE AND DRUPLINES ARE BASED ON A VISUAL OBSERVATION. FINAL DETERMINATION SHOULD BE MADE BY THE PROJECT ARBORIST.



225 LEXINGTON DRIVE
MENLO PARK
CALIFORNIA

BOUNDARY AND
TOPOGRAPHIC SURVEY



ADD	REVISIONS	PCW
DATE	BY	
JOB NO:	2210071	
DATE:	3-10-21	
SCALE:	1"=8'	
FIELD BY:	KR	
DRAWN BY:	ZB	
SHEET NO:		



GRADING AND DRAINAGE CONSTRUCTION NOTES:

1. DIRECT ROOF DOWNSPOUT LEADERS TO APPROVED SPLASH BLOCKS (2' LENGTH MIN.), DIRECT AWAY FROM BUILDING FOR POSITIVE FLOW, & TOWARDS PERVIOUS AREA OF THE SITE - TYP. (SEE DETAIL).
2. DIRECT SURFACE FLOW DRAINAGE AWAY FROM BUILDING AT 2% SLOPE FOR PAVED AREAS AND SLOPE 5% FOR AT LEAST 10 FEET, FOR NON-PAVED (DIRT & LANDSCAPE) AREAS.
3. 4" SDR-26 SS. L.A.T. @ S=2%.
4. N-WATER SERVICE LINE AND UPGRADE WATER METER. (DESIGN BY OTHERS)
5. 6" PVC (SDR-35) @ S=1% MIN.
6. GRAVEL BASIN (6'X10'X4.5 DEEP)-SEE CITY STD. DETAIL DR-18, OPTION 2. ON SHEET C-2.
7. (N) INFILTRATION DEVICE (CHRISTY V-24) WITHOUT BOTTOM. SEE INFILTRATION DEVICE, SEE DETAIL ON SHEET C-2.
8. PROVIDE AN ON-SITE DOUBLE CHECK VALVE FOR BACKFLOW PROTECTION PER CITY STD. DETAIL, WA-21. (COVER PER CITY STD. DETAIL, WA-21 IS RECOMMENDED TO PREVENT THEFT, BUT IS NOT REQUIRED)
9. THE EXISTING FRONTAGE REVEALS CRACKED SIDEWALK AS SHOWN BELOW. THE APPLICANT SHALL FURNISH NEW SIDEWALK PURSUANT TO THE LATEST CITY STANDARDS, TO THE SATISFACTION OF THE PUBLIC WORKS DEPARTMENT.
10. REMOVE AND REPLACE EXISTING DRIVEWAY, SIDEWALK, AND ROLLED CURB. SEE CITY STD. DETAIL ON SHEET C-2.

GENERAL NOTES:

1. ELEVATIONS AND LOCATIONS OF ALL EXISTING UTILITY CROSSINGS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO START OF ANY CONSTRUCTION AND LINES. CONTACT USA AT (800) 642-2444 AT LEAST TWO WORKING DAYS PRIOR TO EXACTION.
2. ALL APPLICABLE WORK AND MATERIALS SHALL BE DONE IN ACCORDANCE WITH THE CITY OF MENLO PARK STANDARD DETAILS, SPECIFICATIONS AND ORDINANCES.
3. THE CONTRACTOR SHALL RESTORE ALL DAMAGED, REMOVED OR OTHERWISE DISTURBED WALLS, FENCES, SERVICES, UTILITIES, IMPROVEMENTS OR FEATURES OF WHATEVER NATURE, DUE TO CONTRACTOR'S WORK.
4. THE CONTRACTOR SHALL COORDINATE HIS/HER WORK WITH ALL UTILITY COMPANIES, PG&E, AT&T, WEST BAY SANITARY, CAL WATER OR MENLO PARK WATER, VALVE BOXES AND MANHOLES, AND STRUCTURES TO BE SET TO GRADE IN CONCRETE AFTER PAVING.
5. ALL STREET MONUMENTS AND OTHER PERMANENT MONUMENTS DISTURBED DURING THE PROCESS OF CONSTRUCTION SHALL BE REPLACED BEFORE ACCEPTANCE OF THE IMPROVEMENTS BY THE PUBLIC WORKS DIRECTOR.
6. THE CONTRACTOR SHALL GIVE THE CITY INSPECTOR TWO WORKING DAYS ADVANCE NOTICE FOR INSPECTION.
7. REMOVAL OF HERITAGE TREES REQUIRES HERITAGE TREE REMOVAL PERMIT.
8. FOR LANE CLOSURES, THE CONTRACTOR SHALL PREPARE A TRAFFIC CONTROL PLAN AND OBTAIN APPROVAL OF THE CITY ENGINEER BEFORE COMMENCING WORK. THE CONTRACTOR SHALL PROVIDE FLAGMEN, CONES OR BARRICADES, AS NECESSARY TO CONTROL TRAFFIC AND PREVENT HAZARDOUS CONDITIONS PER THE CALIFORNIA STANDARD PLANS, SPECIFICATIONS, AND MANUAL ON TRAFFIC CONTROL DEVICES, LATEST EDITION.
9. PEDESTRIAN, PUBLIC ACCESSSES, WHEELCHAIR ACCESSSES SHALL BE MAINTAINED DURING THE CONSTRUCTION TO THE SATISFACTION OF THE PUBLIC WORKS DIRECTOR.
10. NO TRENCHES OR HOLES SHALL BE LEFT OPEN OVERNIGHT; USE STEEL PLATING OR HOT-MIX ASPHALT AS REQUIRED TO PROTECT OPEN TRENCHES OVERNIGHT.
11. THE CONTRACTOR SHALL CONTROL DUST AT ALL TIMES AND SWEEP STREETS AS OFTEN AS NECESSARY DURING CONSTRUCTION AS REQUIRED BY THE PUBLIC WORKS DIRECTOR.
12. ALL REVISIONS TO THIS PLAN MUST BE REVIEWED AND APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION AND SHALL BE ACCURATELY SHOWN ON REVISED LINES STAMPED AND SIGNED BY CITY ENGINEER PRIOR TO THE INSTALLATION OF THE IMPROVEMENTS.
13. ALL CONSTRUCTION STAKING FOR CURB, GUTTER, SIDEWALK, SANITARY SEWERS, STORM DRAINS, WATER LINES, FIRE HYDRANTS, ELECTROLES, ETC., SHALL BE DONE BY A REGISTERED CIVIL ENGINEER OR LICENSED LAND SURVEYOR.
14. ALL EXISTING FRONTAGE IMPROVEMENTS THAT ARE DAMAGED, CRACKED, UPLIFTED OR DEPRESSED DURING THE COURSE OF CONSTRUCTION, OR THAT WERE DAMAGED PRIOR TO CONSTRUCTION, SHALL BE REMOVED, REPLACED AND/OR REPAIRED. THE PROPOSED PLAN SECTIONS SHALL MEET CITY STANDARDS ALONG THE ENTIRE PROPERTY FRONTAGE. CITY WILL NOT BEAR THE COSTS OF RECONSTRUCTION.
15. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARDS DETAILS.
16. A SEPARATE ENCROACHMENT PERMIT IS REQUIRED FOR ANY WORK WITHIN THE PUBLIC RIGHT OF WAY. THE APPLICANT/CONTRACTOR SHALL OBTAIN THE PERMIT FROM THE CITY ENGINEERING DIVISION PRIOR TO START OF ANY WORK WITHIN THE CITY'S RIGHT-OF-WAY OR PUBLIC EASEMENT AREAS. THE APPLICANT SHALL OBTAIN PERMITS FROM UTILITY COMPANIES PRIOR TO APPLYING FOR CITY ENCROACHMENT PERMIT. TO VIEW ENCROACHMENT PERMIT REQUIREMENTS PLEASE VISIT THE CITY'S WEBSITE AT: [HTTP://WWW.MENLOPARK.ORG/202/ENCROACHMENT-PERMITS](http://www.menlopark.org/202/ENCROACHMENT-PERMITS)
17. PROVIDE MINIMUM 5 FT HORIZONTAL CLEARANCE BETWEEN SANITARY SEWER LINE AND STORM DRAIN LINE.

CITY NOTES:

- I. ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPLACED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.
- II. AN ENCROACHMENT PERMIT FROM THE ENGINEERING DIVISION IS REQUIRED PRIOR TO ANY CONSTRUCTION ACTIVITIES, INCLUDING UTILITY LATERALS, IN THE PUBLIC RIGHT OF WAY.
- III. THE EXISTING FRONTAGE REVEALS DAMAGES AT SELECT LOCATIONS AND THE APPLICANT SHALL FURNISH NEW SIDEWALK WHERE APPLICABLE, PURSUANT TO THE LATEST CITY STANDARDS SET BY THE PUBLIC WORKS DEPARTMENT. THE LIMITS OF REPAIR ARE DEFINED IN THE SCREENSHOTS BELOW AND SHALL BE DOCUMENTED ON THE PROPOSED SITE PLAN PRIOR TO PERMIT ISSUANCE.

STANDARD NOTES:

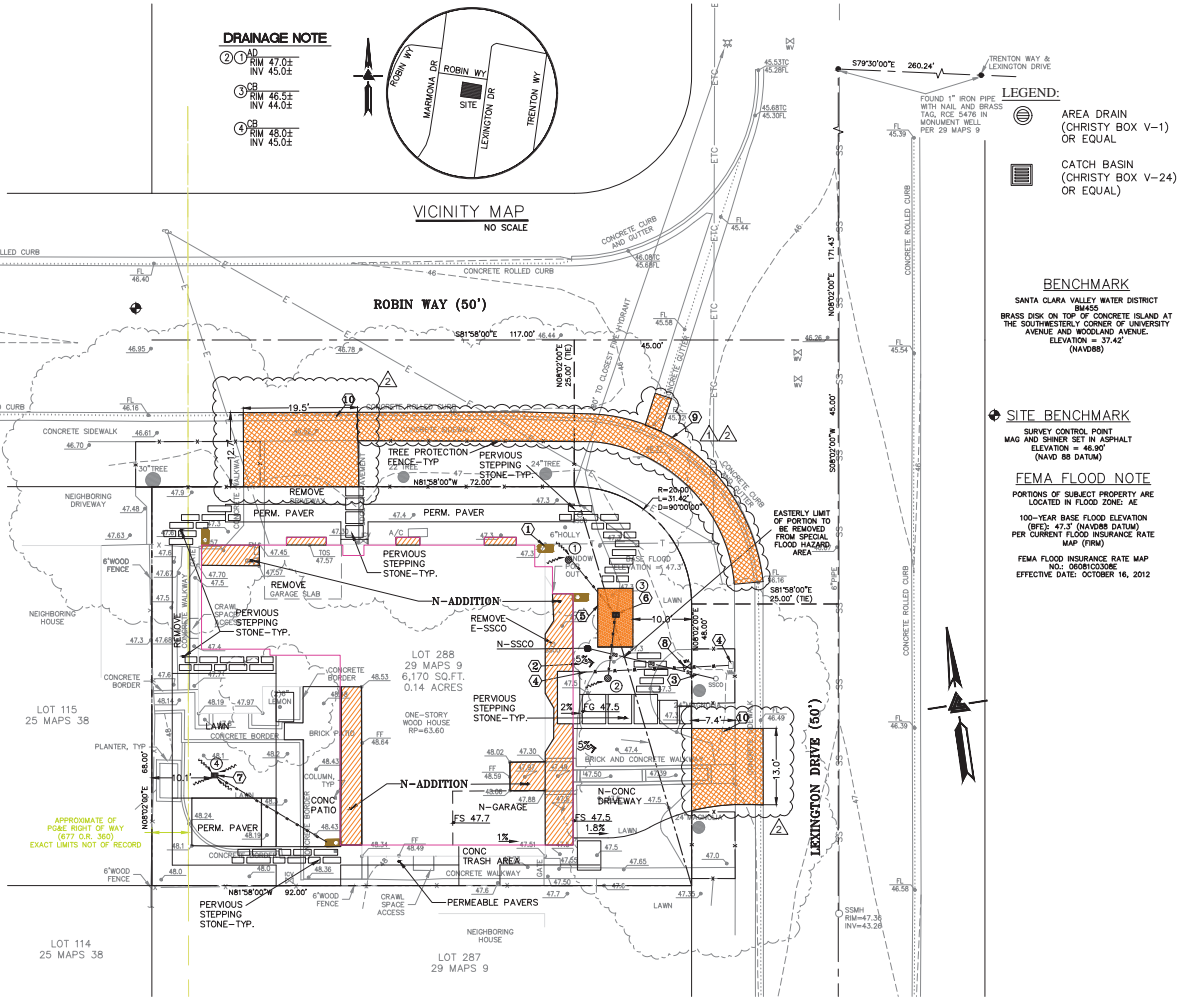
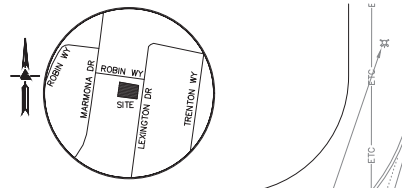
- A. "UNDER NO CIRCUMSTANCE SHALL THE GRADING AND DRAINAGE ACTIVITIES ASSOCIATED WITH THIS PROJECT DIRECTLY SHEET FLOW ONTO THE NEIGHBORING PROPERTY."
- B. ALL HERITAGE TREES MUST BE PROTECTED DURING THE COURSE OF CONSTRUCTION.
- C. "UNDER NO CIRCUMSTANCE SHALL DRAINAGE RESULTING FROM THIS PROJECT, DURING OR POST CONSTRUCTION, DIRECTLY SHEET FLOW ACROSS AN ADJOINING PROPERTY. RUNOFF SHALL BE CONTAINED ON-SITE UP TO THE 10 YEAR STORM."
- D. "GRADES WITHIN THE FIRST 10 FEET ADJACENT TO A STRUCTURE MUST HAVE A 5% SLOPE ON PERVIOUS SURFACES, AND A 2% SLOPE ON IMPERVIOUS SURFACES PER 18014.4.3 OF THE CALIFORNIA BUILDING CODE (CBC)."

DRAINAGE NOTE

1. RM 47.02 INV 45.02
2. RM 46.52 INV 44.02
3. RM 46.02 INV 43.02



VICINITY MAP
NO SCALE



LEGEND:

- AREA DRAIN (CHRISTY BOX V-1) OR EQUAL
- CATCH BASIN (CHRISTY BOX V-24) OR EQUAL

BENCHMARK

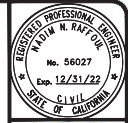
SANTA CLARA VALLEY WATER DISTRICT BRASS DISK ON TOP OF CONCRETE ISLAND AT THE SOUTHWEST CORNER OF UNDERSTY AVENUE AND WOODLAND AVENUE. ELEVATION = 57.42 (NAVD88)

SITE BENCHMARK

SURVEY CONTROL POINT MAG AND SHIRNER SET IN ASPHALT ELEVATION = 46.00 (NAVD 88 DATUM)

FEMA FLOOD NOTE

PORTIONS OF SUBJECT PROPERTY ARE LOCATED IN FLOOD ZONE AE 100-YEAR BASE FLOOD ELEVATION (BFE): 47.3 (NAVD88 DATUM) PER CURRENT FLOOD INSURANCE RATE MAP (FIRM) FEMA FLOOD INSURANCE RATE MAP NO. 080103003E EFFECTIVE DATE: OCTOBER 16, 2012



NFR ENGINEERING
INCORPORATED
1000 METROPOLITAN DRIVE
SAN MATEO, CA 94403
415-948-7900

2255 LEXINGTON DRIVE
MENLO PARK
CALIFORNIA

MINOR GRADING AND DRAINAGE PLAN

PLANS REVIEW COMMENTS	DATE
	6/7/22
	8/15/22

REVISIONS	DATE

JOB NO: DATE: 11-18-2021 SCALE: 1" = 10'

DRAWN BY: NR SHEET NO: C-1 OF 4 SHEETS

SHEET INDEX

GRADING AND DRAINAGE PLAN	C-1
MISC. DETAILS	C-2
EROSION CONTROL PLAN	C-3
CONSTRUCTION BEST MANAGEMENT PRACTICES "BMPs" SHEET	C-4

EARTHWORK QUANTITY

APPROXIMATE CUT REQUIRED	20± CY
FILL REQUIRED	0± CY

EARTH WORK NOTE:

THE CONTRACTOR SHALL STRICTLY ADHERE TO THE SOILS ENGINEER'S RECOMMENDATIONS ON STRIPPING AND SITE PREPARATION FOR ALL PERTINENT GRADING, PAVING AND TRENCH BACKFILL ON THIS SITE.



NIR ENGINEERING
 CIVIL ENGINEER
 600 WILSON AVENUE
 SUITE 100
 MENLO PARK, CA 94025
 (650) 346-7800

1051 MENLO OAKS AVENUE
 MENLO PARK
 A.P.N. 082-042-090

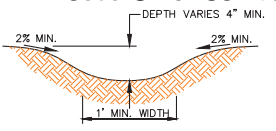
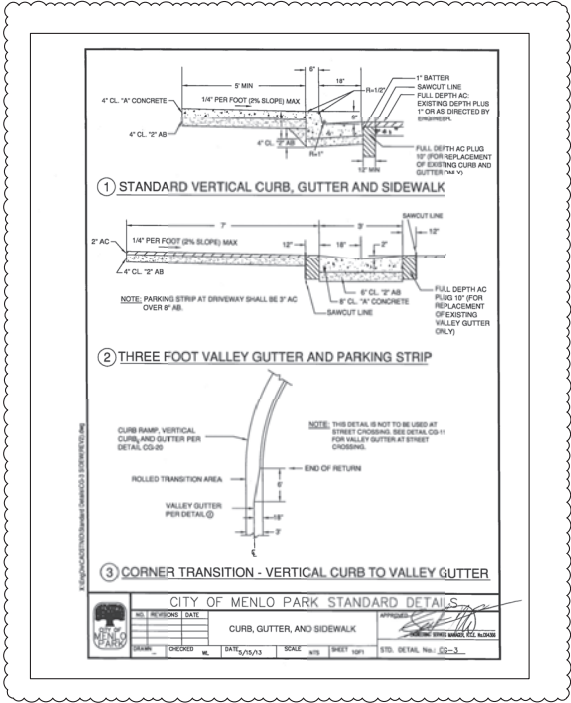
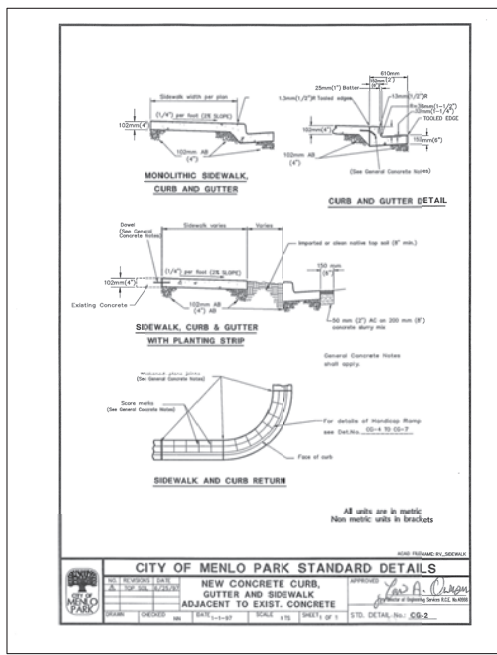
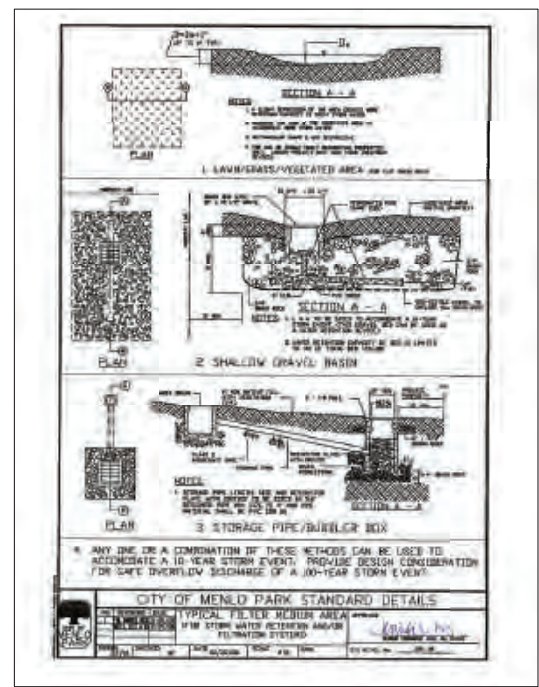
CALIFORNIA
 SAN MATEO COUNTY

DETAILS
 MISC.

PLANS	1/28/20
REVIEW	3/22/21
COMMENTS	8/15/22
REVISIONS	DATE
JOB NO.	
DATE:	11-26-2019
SCALE:	N.T.S.
DRAWN BY:	NR
SHEET NO.:	

C-2

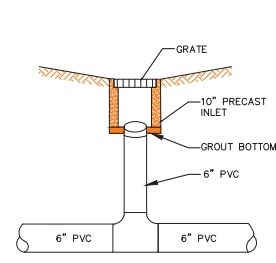
OF 4 SHEETS



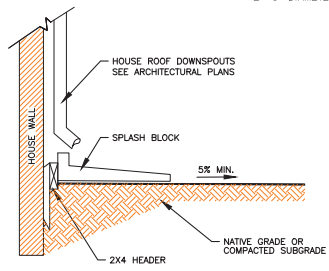
- NOTES:
 1. LONGITUDINAL SLOPE = 2% MIN.
 2. SEE LANDSCAPE PLANS FOR SURFACING

EARTH SWALE DETAIL

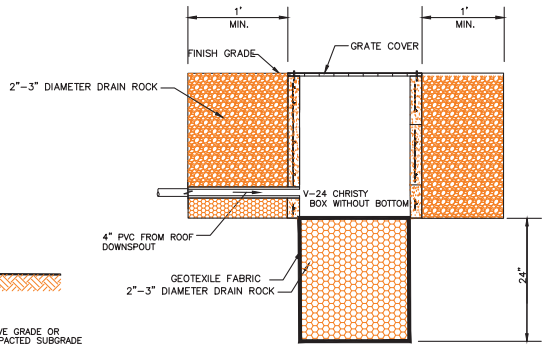
- MAINTENANCE NOTES** N.T.S.
 1. OWNER IS RESPONSIBLE FOR MAINTAINING ALL INLETS, RETENTION SYSTEM AND INFILTRATION DEVICE FROM TRASH, DEBRIS & SEDIMENTS.
 2. THE REGULAR CLEARING OF SILT AND DEBRIS IS ESPECIALLY IMPORTANT PRIOR TO EACH RAINY SEASON.



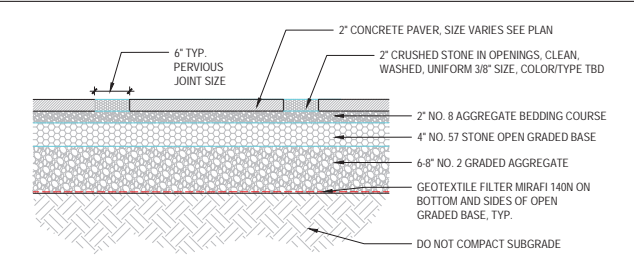
AREA DRAIN DETAIL
 N.T.S.



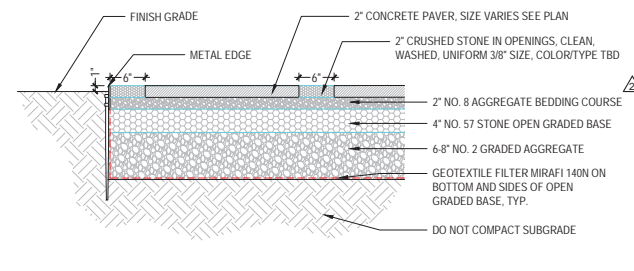
ROOF DOWNSPOUT/SPLASH BLOCK
 N.T.S.



INFILTRATION DEVICE
 N.T.S.



PERVIOUS STEPPING STONE WALKWAY
 N.T.S.



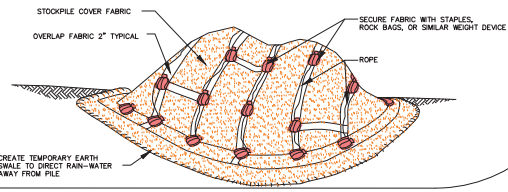
PERVIOUS STEPPING STONE WALKWAY
 N.T.S.

EROSION AND SEDIMENT CONTROL NOTES:

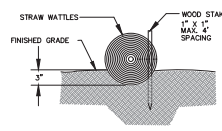
1. ALL CONSTRUCTION ACTIVITIES SHALL BE PERFORMED IN CONFORMANCE WITH THE STORM WATER POLLUTION PREVENTION PLAN FOR THIS PROJECT AND AS REQUIRED BY THE STATE OF CALIFORNIA WATER RESOURCES CONTROL BOARD ORDER RW-2003-0001 AND NPDES PERMIT NO. CAS 0029831.
2. THE DEVELOPER IS RESPONSIBLE FOR ENSURING THAT ALL CONTRACTORS AND SUBCONTRACTORS ARE AWARE OF ALL STORM WATER QUALITY MEASURES AND IMPLEMENT SUCH MEASURES. FAILURE TO COMPLY WITH THE APPROVED CONSTRUCTION BEST MANAGEMENT PRACTICES WILL RESULT IN THE ISSUANCE OF CORRECTION NOTICES, CITATIONS, AND/OR STOP ORDERS.
3. ANY VEHICLE OR EQUIPMENT WASHING/STEAM CLEANING MUST BE DONE AT AN APPROPRIATELY EQUIPPED FACILITY WHICH DRAINS TO THE SANITARY SEWER. OUTDOOR WASHING MUST BE MANAGED IN SUCH A WAY THAT THERE IS NO DISCHARGE OF SOAPS, SOLVENTS, CLEANING AGENTS OR OTHER POLLUTANTS TO THE STORM DRAINS. WASH WATER SHALL DISCHARGE TO THE SANITARY SEWER, SUBJECT TO REVIEW AND APPROVAL OF THE CITY ENGINEER.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LITTER CONTROL AND SWEEPING OF ALL PAVED SURFACES DURING CONSTRUCTION.
5. THE FACILITIES SHOWN ON THIS PLAN ARE DESIGNED TO CONTROL EROSION AND SEDIMENT DURING THE RAINY SEASON, OCTOBER 1 TO APRIL 30. EROSION CONTROL MEASURES ARE TO BE FUNCTIONAL PRIOR TO OCTOBER 1ST OF ANY YEAR GRADING OPERATIONS HAVE LEFT AREAS UNPROTECTED FROM EROSION.
6. ALL ON-SITE STORM DRAINS SHALL BE CLEANED IMMEDIATELY BEFORE THE START OF THE RAINY SEASON BEGINNING ON OCTOBER 1ST EACH YEAR, SUBJECT TO THE REVIEW OF THE BUILDING/ENGINEERING INSPECTOR.
7. IF RAINY WEATHER BECOMES IMMINENT, GRADING OPERATIONS SHALL BE STOPPED AND EROSION CONTROL MEASURES SHALL BE IMPLEMENTED TO PROTECT DISTURBED AREAS.
8. DURING THE RAINY SEASON, ALL PAVED AREAS SHALL BE KEPT CLEAR OF EARTH MATERIAL AND DEBRIS. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT LOAD RUNOFF TO ANY STORM DRAIN SYSTEM.
9. CONSTRUCTION ENTRANCES SHALL CONSIST OF A MINIMUM 6" THICK LAYER OF 3"-4" FRACTURED STONE AGGREGATE UNLINED WITH GEOTEXTILE LINER FOR A MINIMUM DISTANCE OF 50 FEET, AND IS TO BE PROVIDED AT EACH VEHICLE ACCESS POINT TO EXISTING PAVED STREETS. THE DEPTH AND LENGTH OF AGGREGATE MAY NEED TO BE ADJUSTED IN THE FIELD TO ENSURE NO TRACKING OF SEDIMENT ONTO EXISTING PAVED STREETS. CONSTRUCTION ENTRANCES SHALL SLOPE AWAY FROM EXISTING PAVED STREETS.
10. INLETS NOT USED IN CONJUNCTION WITH EROSION CONTROL MEASURES ARE TO BE BLOCKED UNLESS THE AREA DRAINED IS UNDISTURBED OR STABILIZED.
11. BORROW AREAS AND TEMPORARY STOCKPILES SHALL BE PROTECTED WITH APPROPRIATE EROSION CONTROL MEASURES TO THE SATISFACTION OF THE CITY ENGINEER.
12. NO STRAW BALES OR SILT FENCES SHALL BE USED AS EROSION CONTROL MEASURES. SILT FENCES MAY ONLY BE USED AS A PHYSICAL BARRIER TO PREVENT VEHICULAR AND PEDESTRIAN TRAFFIC FROM USING NON-APPROVED ACCESS POINTS (E.G. - ALONG RIGHT-OF-WAY).
13. ALL DISTURBED AREAS INCLUDING FLAT PADS ARE TO BE TREATED WITH STRAW AND TAGGER AT A RATE OF 2 TONS PER ACRE APPROXIMATELY 3 INCHES THICK.

NOTES:

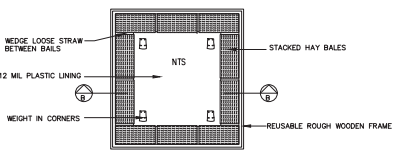
1. PLACE FIBER ROLLS AROUND THE INLET - CONSISTENT WITH BASIN SEDIMENT BARRIER DETAIL ON THIS SHEET. FIBER ROLLS ARE TUBES MADE FROM STRAW BOUND BY PLASTIC NETTING. THEY ARE APPROX. 6" DIA. AND 20 - 30 FT. LONG.
2. FIBER ROLL INSTALLATION REQUIRES THE PLACEMENT AND SECURE STAKING OF THE FIBER ROLL IN A TRENCH, 2" DEEP, ON CONTOUR. RUNOFF MUST NOT BE ALLOWED TO RUN UNDER OR AROUND FIBER ROLL.
3. THE TOP OF THE STRUCTURE (PONING HEIGHT) MUST BE WELL BELOW THE GROUND ELEVATION DOWNSLOPE TO PREVENT RUNOFF FROM BY-PASSING THE INLET. EXCAVATION OF A BASIN ADJACENT TO THE PROP INLET OR A TEMPORARY DIKE ON THE DOWNSLOPE OF THE STRUCTURE MAY BE NECESSARY.



TEMPORARY COVER ON STOCK PILE
NTS

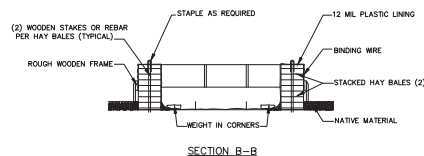


**ENTRENCHMENT
DETAIL
IN FLAT AREA
STRAW WATTLES**
NTS

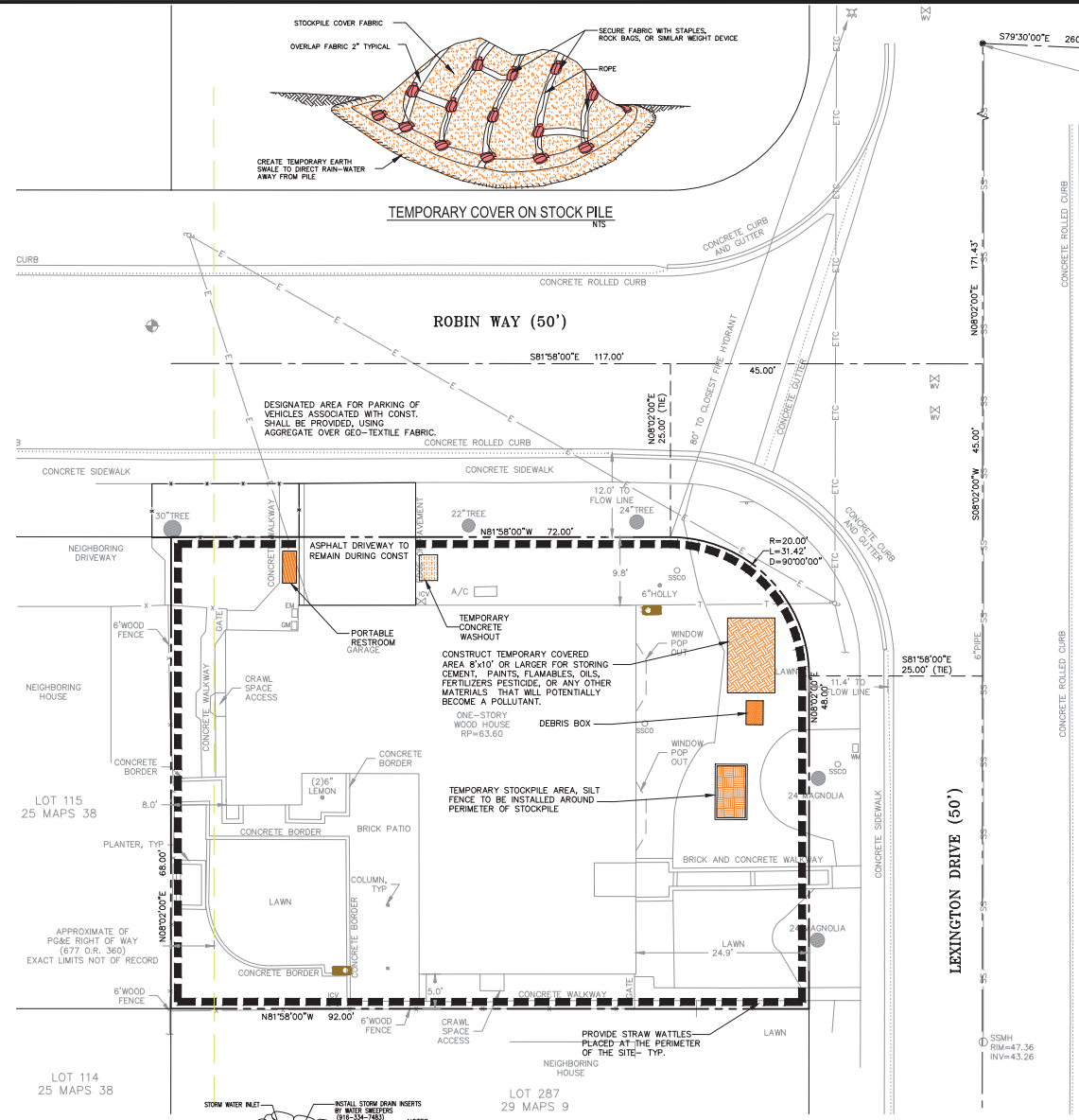


TEMPORARY CONCRETE WASHOUT FACILITY (ABOVE GRADE)
NTS

EROSION CONTROL MEASURES MUST BE IN PLACE THROUGHOUT THE RAINY SEASON (OCT. 1-APR. 30).



**BURLAP SACK DRAIN INLET (D.I.)
SEDIMENT FILTER DETAIL**
NTS



NNR ENGINEERING
INCORPORATED
200 WINTERGARD DRIVE
SANTA ANA, CALIFORNIA 92705
714.944.7820

**225 LEXINGTON DRIVE
MENLO PARK
CALIFORNIA**

EROSION CONTROL PLAN

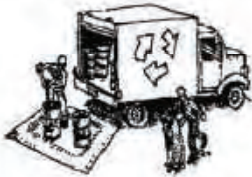
REVISIONS	DATE

JOB NO:
DATE: 11-18-2021
SCALE: 1"=10'
DRAWN BY: NR
SHEET NO:

Construction Best Management Practices (BMPs)

Construction projects are required to implement the stormwater best management practices (BMP) on this page, as they apply to your project, all year long.

Materials & Waste Management



Non-Hazardous Materials

- Store and cover stockpiles of sand, dirt or other construction material with tarps when rain is forecast or if not actively being used within 14 days.
- Cap (or lock) overflows reclaimed water for dust control.

Hazardous Materials

- Label all hazardous materials and hazardous wastes (such as pesticides, paints, thinners, solvents, fuel, oil, and antifreeze) in accordance with city, county, state and federal regulations.
- Store hazardous materials and wastes in water-tight containers, away from inappropriate environmental conditions, and cover them at the end of every work day or during wet weather or when rain is forecast.
- Follow manufacturer's application instructions for hazardous materials and be careful not to use more than necessary. Do not apply chemicals outdoors when rain is forecast within 24 hours.
- Arrange for appropriate disposal of all hazardous wastes.

Waste Management

- Cover waste disposal containers securely with caps at the end of every work day and during wet weather.
- Check waste disposal containers frequently for leaks and to make sure they are not overfilled. Never hose down a dumpster on the construction site.
- Clean or replace portable toilets, and inspect them frequently for leaks and spills.
- Dispose of all wastes and debris properly. Recycle materials and wastes that can be recycled (such as asphalt, concrete, aggregate base materials, wood, gypsum board, pipe, etc.)
- Dispose of liquid residues from paints, thinners, solvents, glues, and cleaning fluids as hazardous waste.

Construction Entrances and Perimeters

- Establish and maintain effective perimeter controls and stabilize all construction entrances and exits to sufficiently control erosion and sediment discharge from site and tracking off site.
- Sweep or vacuum any street tracking immediately and remove sediment sources to prevent further tracking. Never hose down streets to clean up tracking.

Equipment Management & Spill Control



Maintenance and Parking

- Designate an area, fitted with appropriate BMPs, for vehicle and equipment parking and storage.
- Perform major maintenance, repair jobs, and vehicle and equipment washing off site.
- If refueling or vehicle maintenance must be done onsite, work in a bermed area away from storm drains and over a drip pan or deep curbs big enough to collect fluids. Recycle or dispose of fluids as hazardous waste.
- If vehicle or equipment cleaning must be done onsite, clean with water only in a bermed area that will not allow rinse water to run into gutters, streets, storm drains, or surface waters.
- Do not clean vehicle or equipment onsite using soaps, solvents, degreasers, or steam cleaning equipment.

Spill Prevention and Control

- Keep spill cleanup materials (e.g., rags, absorbents and ear litter) available at the construction site at all times.
- Inspect vehicles and equipment frequently for and repair leaks promptly. Use drip pans to catch leaks until repairs are made.
- Clean up spills or leaks immediately and dispose of cleanup materials properly.
- Do not hose down surfaces where fluids have spilled. Use dry cleanup methods (absorbent materials, air filter, and/or rags).
- Sweep up spilled dry materials immediately. Do not try to wash them away with water, or spray them.
- Clean up spills on dirt areas by digging up and properly disposing of contaminated soil.
- Report significant spills immediately. You are required by law to report all significant releases of hazardous materials, including oil. To report a spill: 1) Call 911 or your local emergency response number; 2) Call the Governor's Office of Emergency Services Warning Center: (800) 852-7330 (24 hours).

Earthmoving



- Schedule grading and excavation work during dry weather.
- Stabilize all denuded areas, roads, and maintain temporary erosion controls (such as erosion control fabric or bonded fiber mats) until vegetation is established.
- Remove existing vegetation only when absolutely necessary, and seed or plant vegetation for erosion control on slopes or where construction is not immediately planned.
- Prevent sediment from migrating offsite and protect storm drain inlets, gutters, ditches, and drainage courses by installing and maintaining appropriate BMPs, such as fiber mats, silt fences, sediment basins, gravel bags, berms, etc.
- Keep excavated soil on site and monitor to dump trucks or site, not in the streets.

Contaminated Soils

- If any of the following conditions are observed, test for contamination and contact the Regional Water Quality Control Board:
 - Unsatified soil conditions, discoloration, or odor
 - Abandoned underground tanks
 - Abandoned wells
 - Buried barrels, debris, or tanks

Paving/Asphalt Work



- Avoid paving and seal coating in wet weather or when rain is forecast to prevent materials that have not cured from contacting stormwater runoff.
- Cover storm drain inlets and manholes when applying seal coat, tack coat, slurry seal, big seal, etc.
- Collect and recycle or appropriately dispose of excess adhesive gravel or sand. Do NOT sweep or wash it into gutters.
- Do not use water to wash down fresh asphalt concrete pavement.

Sawcutting & Asphalt/Concrete Removal

- Protect nearby storm drain inlets before sawcutting. Use fiber fabric, catch basins, silt filters, or gravel bags to keep slurry out of the storm drain system.
- Shovel, blow, or vacuum sawcut slurry, and dispose of all waste as early as you are finished to free location or at the end of each work day (whichever is sooner).
- If slurry enters a catch basin, clean it up immediately.

Concrete, Grout & Mortar Application



- Store concrete, grout, and mortar away from storm drains or waterways, and or pallets under cover to protect them from rain, runoff, and wind.
- Wash out concrete equipment tracks offsite or in a designated washout area, where the water will flow into a temporary water pit, and in a manner that will prevent leaching into the underlying soil or into surrounding areas. Let concrete harden and dispose of as garbage.
- When washing exposed aggregate, prevent washwater from entering storm drains. Block any inlets and vacuum gutters, hose washwater onto dirt areas, or drain onto a bermed surface to be pumped and disposed of properly.

Landscaping



- Protect stockpiled landscaping materials from wind and rain by storing them under tarps all year-round.
- Stack bagged material on pallets and under covers.
- Discontinue application of any erodible landscape material within 2 days before a forecast rain event or during wet weather.

Painting & Paint Removal



Painting Cleanup and Removal

- Never clean brushes or rinse paint containers into a street, gutter, storm drain, or stream.
- For water-based paints, paint and brushes to the extent possible, and rinse into a drain that goes to the sanitary sewer. Never pour paint down a storm drain.
- For oil-based paints, paint and brushes to the extent possible and clean with diluter or solvent in a proper container. Filter and reuse thinners and solvents. Dispose of excess liquids as hazardous waste.
- Paint chips and dust from non-hazardous dry stripping and sand blasting may be swept up or collected in plastic shop clothes and disposed of as trash.
- Chemical paint stripping residue and chips and dust from impure paints or paints containing lead, mercury, or tributyltin must be disposed of as hazardous waste. Lead-based paint removal requires a state-certified contractor.

Dewatering



- Discharges of ground water or captured runoff from dewatering operations must be properly managed and disposed. When possible, seal dewatering discharge to landscaped area or sanitary sewer. If discharging to the sanitary sewer call your local wastewater treatment plant.
- Divert run-on water from ditches away from all disturbed areas.
- When dewatering, notify and obtain approval from the local municipality before discharging water to a street gutter or storm drain. Filtration or diversion through a basin, tank, or sediment trap may be required.
- In areas of known or suspected contamination, and your local agency is determining whether the ground water must be tested. Potential groundwater may need to be collected and treated off-site for treatment and proper disposal.

Storm drain polluters may be liable for fines of up to \$10,000 per day!



NRI ENGINEERING

1000 W. 10TH ST., SUITE 100
SAN MATEO, CA 94401
TEL: 650-992-7800

225 LEXINGTON DRIVE
MENLO PARK
CALIFORNIA

CONSTRUCTION BEST
MANAGEMENT
PRACTICES (BMPs)

SAN MATEO COUNTY APR 062-005-120 CALIFORNIA

REVISIONS	DATE
JOB NO.	11-18-2021
DATE:	N.T.S.
SCALE:	NR
DRAWN BY:	
SHEET NO.:	

C-4

OF 4 SHEETS

Summary

MARLIDAR examined my services to assess Heritage trees at 225 Lexington Drive in Menlo Park, CA 94025. The purpose of my examination was to identify which trees are considered Heritage Trees as defined by the Menlo Park Tree Ordinance...

Introduction

Assignment

- Marlboro Residence retained my services to perform the following tasks:
1. Assess tree health, condition, and potential impacts for any Heritage Trees located within the site of the proposed construction at 225 Lexington Drive in Menlo Park, CA.
2. Identify which trees are considered to be Heritage Trees as defined in the Menlo Park Tree Ordinance...

Limits of Assignment

I did not perform an aerial inspection of the appraisals or a detailed root zone inspection on the subject trees.

Tree Assessment Methods

- 1. Identify tree species.
2. Measure tree diameter at 4.5 feet above natural ground level (DBH).
3. Measure tree height to the highest live branch.
4. Assess tree health and condition.
5. Assess tree stability.
6. Assess tree root zone.

Health and Structure Rating System

Table with 4 columns: Health, Structure, Stability, and Comments. It provides a scale for rating tree health and structural integrity.

Menlo Park Heritage Tree Definition

The following is the definition of a Heritage Tree in Menlo Park as defined in the Menlo Park Tree Ordinance.

Definition of a heritage tree

- 1. Any tree other than oaks has a trunk with a circumference of 41.3 inches (diameter of 13 inches) or more, measured at 4.5 inches above natural grade.
2. Any oak tree native to California has a trunk with a circumference of 34.1 inches (diameter of 10 inches) or more measured at 4.5 inches above natural grade.
3. A tree or group of trees specifically designated by the City Council for protection because of its historical significance, special character or community benefit.

Any tree with more than one trunk that falls under (1) and (2) shall be measured at the diameter before the most serious of all such trunk lines. If the tree has more than one trunk and the oaks in a hollow grade, each stem shall be measured as a standalone tree. Multi-trunk trees under 12 feet in height shall not be considered a heritage tree.

Suitability for Preservation

The goal of tree preservation is for the existing trees to remain assets to the site for years to come. Trees that are in poor condition and cannot tolerate construction impacts will become a liability and therefore should be removed.

- 1. Tree Health - A healthy tree can tolerate construction impacts better than a tree in poor health and is more likely to adapt to new site conditions after development.
2. Tree Structure - Trees with structural defects such as decayed trunk, weak branch attachments and combination stems are a liability and therefore should be removed.
3. Tree Age - Mature and over-mature trees are less able to tolerate construction impacts while younger trees have more tolerance for construction impacts.
4. Species Tolerance - All trees require protection to avoid injury. However, certain tree species can tolerate construction impacts better than others.

Observations

Site Description

The site is located at 225 Lexington Drive in Menlo Park, CA 94025. A single family residential house is currently located on the site. The proposed construction consists of a remodel and addition to the existing house, a new 'ADU' and associated landscape and site work.

Glossary of Terms

- Aerial inspection - An inspection of the upper canopy of the tree that requires climbing.
Crown - Parts of the tree above the trunk, including leaves, branches and seedling limbs.
Diameter at standard height (DBH) - The diameter of a tree's trunk as measured at 4.5 feet from the ground.
Windthrow - Tree failure due to uprooting caused by wind.
Root crown - Area where the main roots join the plant stem, usually at or near ground level.
Root crown inspection - Process of removing soil to expose and assess the root crown of a tree.
Visual Tree Assessment (VTA) - A method of visual assessing the condition of a tree that does not include a root crown inspection or an aerial inspection.

Tree Protection Recommendations

Anticipated Construction Impacts and Specific Tree Protection Recommendations

Portions of the proposed construction are located within the Tree Protection Zone (TPZ) of the heritage trees in this report. Therefore, this work has the potential to impact these trees and cause decline. The following outlines anticipated construction impacts and recommendations to reduce the potential for these impacts to a less than significant level.

- 1. The existing driveway should be removed by breaking up the pavement with a jackhammer and removing the debris with wheelbarrows.
2. Any roots encountered during the installation of the new walkway and flagstone that are 1 inch in diameter and smaller can be cleanly cut at the edge of the excavation area.
3. No plantings or irrigation should be located within 3-3 feet of the trunk of these trees.
4. No stumps should be located within 2-3 feet of the trunk of these trees.
5. It is my opinion that this work is within the threshold for these trees' tolerance to construction impacts.

1. Any grading that is required for clearance purposes should be performed by a certified arborist or tree care professional following best management practices and not construction practices.

Bibliography

- Mahony, M.P. and J.B. Clark. A Photographic Guide to the Evaluation of Natural Trees in Urban Areas (2nd Edition). Phoenix, CA: Horticulture Inc., 1994.
Mahony, M.P. and J.B. Clark. Trees and Development: A Technical Guide to Preservation of Trees During Land Development. Champaign, IL: International Society of Arboriculture, 1998.
Harris, R. Arboriculture: Integrated Management of Landscape Trees, Shrubs, and Climbs. Englewood Cliffs, NJ: Prentice Hall, Inc., 1992.
International Society of Arboriculture. Glossary of Arboriculture Terms. Champaign, IL: Champaign, 2007.

Proactive Tree Care for Heritage Trees or Other Trees

Proactive tree care is essential for overall tree health and to ensure that trees are able to tolerate construction impacts.

- 1. Any improvements to the existing foundation should be performed in a manner that is sensitive to the roots of these trees.
2. No plantings or irrigation should be located within 3-3 feet of the trunk of these trees.
3. It is my opinion that this work is within the threshold for these trees' tolerance to construction impacts.

Tree Protection Zones

Each Heritage Tree or significant tree requires a Tree Protection Zone (TPZ) to be established around the tree to protect it from construction impacts.

- 1. Any grading that is required for clearance purposes should be performed by a certified arborist or tree care professional following best management practices and not construction practices.
2. Any improvements to the existing foundation should be performed in a manner that is sensitive to the roots of these trees.
3. No plantings or irrigation should be located within 3-3 feet of the trunk of these trees.
4. No stumps should be located within 2-3 feet of the trunk of these trees.
5. It is my opinion that this work is within the threshold for these trees' tolerance to construction impacts.

Special Activities within the Tree Protection Zone

Work to be done within the TPZ requires the best arborist practices to be followed.

Tree Pruning Recommendations

A crown cleaning is removal of all dead branches 2 inches in diameter and larger, removal of all broken branches and selective limb removal or root weight reduction to reduce the chance of limb failure.

Mulching Recommendations

Recommended that wood chips be spread within the TPZ to a 3-3 inch depth, leaving the trunk clear of mulch.

Continued Maintenance Of Heritage Trees During and After Construction Recommendations

- 1. Monthly arborist inspections for the duration of the construction activities.
2. Quarterly arborist inspections for the first year after the completion of the construction project.

ISSUE LOG
PLANNING SUBMITTAL
PLANNING REVIEW
JULY 13, 2022
PLANNING REVIEW
SEPT 06, 2022
PLANNING REVIEW
OCT 21, 2022

YOUNG AND BORLIK ARCHITECTS, INCORPORATED
4962 EL CAMINO REAL, SUITE 2218 LOS ALAMOS, CA 94022
FAX: (650) 688-1950 www.yborliks.com



NEW RESIDENCE FOR:
RUCHI & SID MURLIDHAR
225 LEXINGTON DRIVE
MENLO PARK, CA 94025

A.P.N. 062-306-120
OWNER: AEY
DATE: JULY 28, 2021
JOB # MURLIDHAR

AR-1



Young & Borlik Architects

4962 El Camino Real, Suite 218
Los Altos, California 94022

650-688-1950 | YBarchitects.com

November 21, 2022

Planning Department
City of Menlo Park, Planning Division
701 Laurel Street
Menlo Park, CA 94025

Re: Conditional Use Permit - Project description letter for:
Ruchi and Sid Murlidhar
225 Lexington Drive
Menlo Park, CA 94025

The purpose of this letter is to describe the proposed new two-story home with attached ADU project at 225 Lexington Drive, to accompany our submittal of plans and application for the Conditional Use Permit approval for “substandard lot” dimensions. The overall project includes the demolition of the existing one-story 2,259.9 s.f. residence and the construction of a new two-story home with attached ADU. The floor area of the new home will be 2,782.6 s.f. with attached ADU 793 sf.

The parcel is 6,170 sf, zoned as R-1/U. Based on lot dimensions, the parcel is considered substandard with respect to the minimum size for the district. The site area of 6,170 is less than the minimum lot size of 7,000 s.f. The average depth of the property is 89.6 feet where 100 feet is the minimum required depth.

The existing one-story home structure is “non-conforming” with regards to the interior side setback (5’-0-1/2” where 6’-9.5” is required), street side setback (9’-10” where 12’-0” is required) and rear setback (8’-0” where 20’-0” is required). The attached two-car garage is located approximately 9’-10” from the side yard property line, where 20 feet is required, however there is a 17’-6” driveway depth to the sidewalk. The existing architecture of the home is traditional one story “Ranch style” typical of most of the original homes in the neighborhood and in Menlo Park.

The proposed design incorporates “Modern Farm style” influences but minimized for a more simplified aesthetic. The design will feature a new covered front entry for the main house, to provide a welcoming presence and emphasize the pedestrian scale of the streetscape. The main house front door and relocated two car garage will face Lexington Avenue and ADU main entry will face at Robin Way with high visibility.

Exterior wall materials will be “horizontal siding” for the first story, with “board and batten” vertical treatment on the second floor. The roof will have combination of standing seam metal roof and composition asphalt shingles, with solar panels on the two rear south & west planes on second floor. The exterior materials will all be fire rated. A few decorative bay windows and shed roofs break up the scale of the elevations. The windows will be aluminum clad with wood trim, predominantly casement style. Trim, casing, and moldings will be painted.

The second floor is centered, from side to side, within the footprint of the first floor below, which maintains a comfortable margin to fit within the daylight plane envelope. Due to the sub-standard depth of the property the second floor is pulled forward on the first floor to allow for more light and open sky to the small rear yard. This also enables a one-story vaulted ceiling in the main family room and kitchen space on the first floor opening to the rear yard. The new attached two-car garage location will have a new driveway which can accommodate uncovered off street parking spaces.

The surrounding neighborhood is all single-family dwellings. The immediate vicinity seems to be mostly one-story homes with several two-story developments intermixed. Most residences have an attached two-car garage facing the front yard with a double wide driveway connecting to the street for the additional off-street parking. There are 5 heritage size street trees: two Magnolias along the Lexington Drive frontage and three Camphor's along the Robin Way side street frontage. All these street trees remain and will be protected.

As part of the outreach efforts for this project, the owners have reached out to the adjacent neighbors to the side and rear, as well as a few others, to provide awareness of the proposed improvements and to solicit feedback and support. The owners have met with several of their immediate neighbors to review and discuss their projects, and all the responses have been positive.

Thank you for your time in review of this project. We are proud to present this design for your consideration and look forward to any feedback the city may have, and the opportunity to see this new design compliment the neighborhood.

Sincerely,



Andrew E. Young,
Young and Borlik Architects Inc.

225 Lexington Drive – Exhibit C: Conditions of Approval

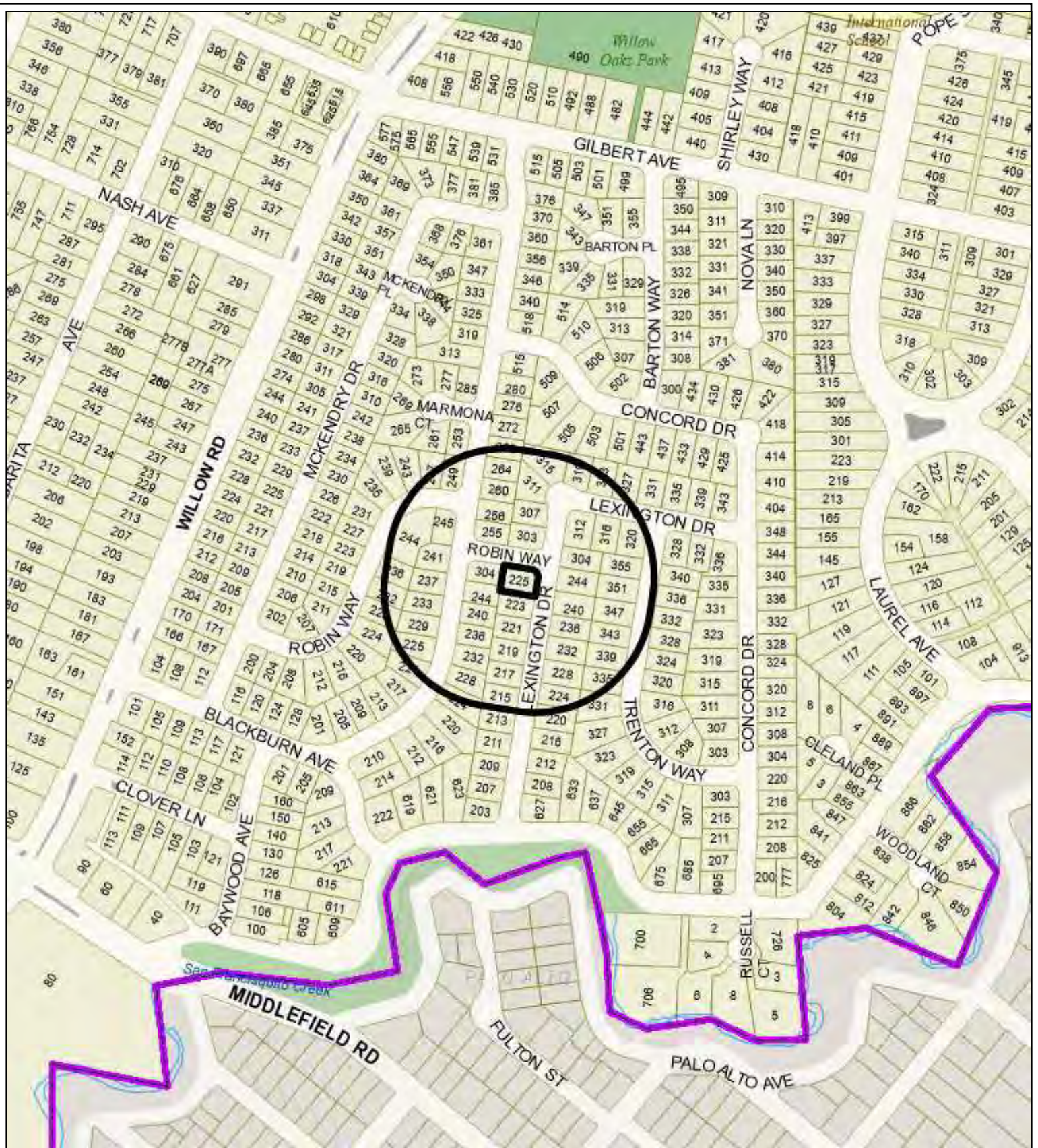
LOCATION: 225 Lexington Drive	PROJECT NUMBER: PLN2021-00058	APPLICANT: Andrew Young	OWNER: Sid and Ruchi Murlidhar
---	---	-----------------------------------	--

PROJECT CONDITIONS:

1. The use permit shall be subject to the following standard conditions:
 - a. The applicant shall be required to apply for a building permit within one year from the date of approval (by January 23, 2024) for the use permit to remain in effect.
 - b. Development of the project shall be substantially in conformance with the plans prepared by Young and Borlik Architects, Inc. consisting of 26 plan sheets, dated received November 21, 2022 and approved by the Planning Commission on January 23, 2023, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
 - c. Prior to building permit issuance, the applicant shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies’ regulations that are directly applicable to the project.
 - d. Prior to building permit issuance, the applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
 - e. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
 - f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division.
 - g. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits.
 - h. The height of fences, walls, hedges or similar structures on the subject property shall comply with the maximum height requirements as listed in Menlo Park Municipal Code section 16.64.020.
 - i. A tree protection verification letter is required from the Project Arborist prior to the start of demolition/construction. The letter shall include photos of tree protection installed to specification. Please also include language about the requirement for regular inspections in the letter. It is required that the Project Arborist provide periodic construction monitoring/tree protection inspections during construction. Four-week intervals would be sufficient to assess and monitor the effectiveness of the approved Tree Protection Plan and to provide recommendations for any additional care or treatment.
 - j. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the arborist report prepared by Ned Patchett Consulting dated December 2, 2021 and revised October 27, 2022.
 - k. As part of the building permit submission, the project plans shall include a note indicating that no skylights shall exceed an interior height of 12 feet from finished floor to ceiling. In the event that the interior height is greater than 12 feet, a clear lens flush with the ceiling under the skylight will be included and maintained.

225 Lexington Drive – Exhibit C: Conditions of Approval

LOCATION: 225 Lexington Drive	PROJECT NUMBER: PLN2021-00058	APPLICANT: Andrew Young	OWNER: Sid and Ruchi Murlidhar
<p>i. Prior to building permit issuance, the applicant shall pay all fees incurred through staff time spent reviewing the application.</p> <p>m. The applicant or permittee shall defend, indemnify, and hold harmless the City of Menlo Park or its agents, officers, and employees from any claim, action, or proceeding against the City of Menlo Park or its agents, officers, or employees to attack, set aside, void, or annul an approval of the Planning Commission, City Council, Community Development Director, or any other department, committee, or agency of the City concerning a development, variance, permit, or land use approval which action is brought within the time period provided for in any applicable statute; provided, however, that the applicant's or permittee's duty to so defend, indemnify, and hold harmless shall be subject to the City's promptly notifying the applicant or permittee of any said claim, action, or proceeding and the City's full cooperation in the applicant's or permittee's defense of said claims, actions, or proceedings.</p>			



City of Menlo Park
 Location Map
 225 Lexington Drive



225 Lexington Drive (PLN2021-00058) – Data Table

	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	6,170.0 sf	6,170.0 sf	7,000.0 sf min
Lot width	68.0 ft	68.0 ft	65.0 ft min
Lot depth	89.6 ft	89.6 ft	100.0 ft min
Setbacks			
Front (East-Lexington Drive)	20.0 ft	24.9 ft	20.0 ft min
Rear (West)	32.0 ft to house 8.3 ft to ADU	8.0 ft to house	20.0 ft min to house 4.0 ft min to ADU
Side-left (South)	6.8 ft	4.9 ft	6.8 ft min
Side-right (North-Robin Way)	18.2 ft to house 9.8 ft to ADU	9.8 ft to house	12.0 ft min to house 4.0 ft min to ADU
Building coverage ¹	2,741.6 sf 44.4 %	2,443.2 sf 39.6 %	2,468.0 sf max 40.0 % max
FAL (Floor Area Limit) ¹	3,575.6 sf	2,259.9 sf	2,800 sf max
Square footage by floor	1,167.1 sf-1st-primary 793.0 sf-1st-ADU 1,178.4 sf-2nd 437.1 sf-garage	1,840.5 sf-1st 419.4 sf-garage	
Square footage of buildings	3,575.6 sf	2,259.9 sf	
Building height	24.5 ft	16.1 ft	28.0 ft max
Parking	2 covered spaces; 1 ADU space	2 covered spaces	2 spaces (1 covered min); 1 ADU space
Note: Areas shown highlighted indicate a nonconforming or substandard situation			

Trees	Heritage trees ²	5	Non-Heritage trees	2	New trees	0
	Heritage trees proposed for removal	0	Non-Heritage trees proposed for removal	1	Total number of trees	6

Note 1: The building coverage and FAL are permitted to be exceeded by the ADU.

Note 2: The five heritage trees are street trees.



**NED PATCHETT
CONSULTING**

**Tree Inventory and Protection Report
For
Murlidhar Residence
225 Lexington Drive in Menlo Park, CA 94025**

Submitted by
Ned Patchett
Certified Arborist WE-4597A
Date: December 2, 2021
Revised: October 27, 2022



Ned Patchett Consulting
830 Buena Vista Street in Moss Beach, CA 94038
Office 650 728-8308
ned@nedpatchettconsulting.com
www.nedpatchettconsulting.com

© 2022 Ned Patchett Consulting, Inc.

All rights reserved. No part of this publication may be reproduced, stored in a retrieval system, or transmitted in any form or by any means (electronic, mechanical, photocopy, recording or otherwise) without written permission from Ned Patchett Consulting, Inc.

Table of Contents

<i>Summary</i>	1
<i>Introduction</i>	1
Assignment	1
Limits of Assignment.....	1
<i>Tree Assessment Methods</i>	2
<i>Health and Structure Rating System</i>	2
<i>Menlo Park-Heritage Tree Definition</i>	3
<i>Suitability for Preservation</i>	3
<i>Observations</i>	3
Site Description	3
Subject Trees	4
<i>Conclusion</i>	4
<i>Tree Protection Recommendations</i>	5
Anticipated Construction Impacts and Specific Tree Protection Recommendations	5
Protective Tree Fencing for Heritage Trees or Street Trees.....	7
Tree Protection Zones	7
Activities prohibited within the TPZ include	7
Special Activities within the Tree Protection Zone.....	7
Tree Pruning Recommendations	8
Mulching Recommendations	8
Continued Maintenance Of Heritage Trees During and After Construction Recommendations	8
<i>Glossary of Terms</i>	9
<i>Bibliography</i>	10
Appendix A – Tree Inventory	11
Appendix B – Tree Inventory Map	13
Appendix C – Tree Appraisal Calculations.....	14
Appendix D – Arborist Disclosure Statement	15
Appendix E – Certification of Performance.....	16

Summary

Sid Murlidhar retained my services to assess Heritage trees at 225 Lexington Drive in Menlo Park, CA 94025. The purpose of my examination was to identify which trees are considered Heritage Trees as defined in the Menlo Park Tree Ordinance, to assess the health and condition of the Heritage Trees, determine their potential for preservation during the proposed construction, and provide an appraised value for each Heritage Tree and to provide recommendations to reduce the impacts of the proposed construction to a less than significant level.

A total of (7) trees, of which (5) are considered Heritage trees, are included in this report. I reviewed Sheet A0.5, dated October 21, 2022, to prepare this report. I have provided recommendations to reduce the potential for construction impacts on the trees included within this report to a less than significant level. I have also provided an appraised value for the (5) Heritage Trees located on the site. Any tree on-site protected by the City's Municipal Code will require replacement according to its appraised value if it is damaged beyond repair due to construction.

Introduction

Assignment

Murlidhar Residence retained my services to perform the following tasks:

1. Assess tree health, condition, and potential impacts for any Heritage Trees located within the zone of the proposed construction at 225 Lexington Drive in Menlo Park, CA.
2. Identify which trees are considered to be Heritage Trees as defined in the Menlo Park Tree Ordinance.
3. Provide an appraised value for each Heritage Tree
4. Provide construction guidelines to be followed throughout all phases of a construction project
5. Document this information in a written report.

Limits of Assignment

I did not perform an **aerial inspection** of the upper crown or a detailed **root crown inspection** on the subject trees.

Tree Report for Murlidhar Residence
Ned Patchett Certified Arborist WE-4597A

10/27/2022

Page 1

Tree Assessment Methods

On November 11, 2021, I visited the site to collect information for this report. A **Level 1 Visual Tree Assessment (VTA)** was performed on the subject trees. The tree numbers in this report correspond to the tree numbers on the included Tree Map ([see Tree Map in Appendix C](#)). The following outlines the procedure for collecting information for this report:

1. Identify tree species
2. Measure the diameter of the trunk at 54 inches above grade **Diameter at Standard Height (DSH)**
3. Identify if the tree is a Heritage Tree, as defined in the Menlo Park Tree Ordinance:
4. Assess the health and condition of each tree
5. Assess the structural stability of each tree
6. Inspect the trees for pest or disease.

Health and Structure Rating System

The following table provides an overview of the rating system used when visually assessing the health and structure of the subject trees within this report.

Rating	Health	Structure
1=Poor	Dead, diseased or dying	Hazardous
2=Poor to Fair	Declining with significant signs of dieback	Structural weakness or flaws that could lead to failure
3=Fair	Minor dead branches, early stages of decline	Corrective measures such as pruning or structural support systems may be needed
4=Fair to Good	Tree is in good health	No major structural issues
5=Good	Excellent health	No structural issues

Tree Report for Murlidhar Residence
Ned Patchett Certified Arborist WE-4597A

10/27/2022

Page 2

Menlo Park-Heritage Tree Definition

The following is the definition of a Heritage Tree in Menlo Park as defined in the Menlo Park Tree Ordinance:

Definition of a heritage tree

1. Any tree other than oaks has a trunk with a circumference of 47.1 inches (diameter of 15 inches) or more, measured at 54 inches above natural grade
2. Any oak tree native to California has a trunk with a circumference of 31.4 inches (diameter of 10 inches) or more measured at 54 inches above natural grade
3. A tree or group of trees specifically designated by the City Council for protection because of its historical significance, special character or community benefit

Any tree with more than one trunk that falls under (1) and (2) shall be measured at the diameter below the main union of all multi-trunk trees. If the tree has more than one trunk and the union is below grade, each stem shall be measured as a standalone tree. Multi-trunk trees under 12 feet in height shall not be considered a heritage tree.

Suitability for Preservation

The goal of tree preservation is for the existing trees to remain assets to the site for years to come. Trees that are in poor condition and cannot tolerate construction impacts will become a liability and therefore should be removed. An assessment of a tree's suitability for preservation includes the following:

1. **Tree Health**-A healthy tree can tolerate construction impacts better than a tree in poor health and is more likely to adapt to new site conditions after development.
2. **Tree Structure**-Trees with structural defects such as decayed wood, weak branch attachments and codominant stems are a liability and therefore should be removed.
3. **Tree Age**-Mature and over-mature trees are less able to tolerate construction impacts while younger trees have more tolerance for construction impacts.
4. **Species Tolerance**-All trees require protection to avoid injury. However, certain tree species can tolerate construction impacts better than others.

Observations

Site Description

The site is located at 225 Lexington Drive in Menlo Park, CA 94025. A single family residential home is currently located on the site. The proposed construction consists of a remodel and addition to the existing house, a new ADU and associated landscape and

Tree Report for Murlidhar Residence
Ned Patchett Certified Arborist WE-4597A

10/27/2022

Page 3

civil improvements which should have minimal impact on the trees (see [Tree Maps in Appendix B](#)).

Subject Trees

I have prepared a tree inventory with all the necessary information that is required by the city on Menlo Park (see [Tree Inventory in Appendix A](#)).

Conclusion

Protection of Heritage Trees during construction is a mandatory part of the construction process in Menlo Park. Arborist inspections can be a required part of the construction process per the Menlo Park Tree Ordinance.

In addition, proposed construction within Tree Protection Zones requires the direct onsite supervision of a Project Arborist and can include specialized construction designs and methods to reduce tree impacts.

Tree Protection Fencing must be erected around these trees prior to the commencement of any construction activities on the site. I have provided recommendations to protect all Heritage Trees during the proposed construction process.

Tree Protection Recommendations

Anticipated Construction Impacts and Specific Tree Protection Recommendations

Portions of the proposed construction are located within the Tree Protection Zone (TPZ) of the heritage trees in this report. Therefore, this work has the potential to impact these trees and cause decline. The following outlines anticipated construction impacts and recommendations to reduce the potential for these impacts to a less than significant level. Tree protection fencing should be erected around the trees prior to any construction activities occurring on the site.

- Tree 1 & 2-The construction activities located within the TPZ of these trees consists of a new walkway to the front door, landscape walking paths and landscape plantings. No other significant construction activities are located within the TPZ of these trees and therefore, I do not anticipate any significant construction impacts to these trees. The following outlines my recommendations for these trees-
 1. The existing pathways should be removed by breaking up the pathways with a jackhammer and removing the debris with wheelbarrows.
 2. Any roots encountered during the installation of the new walkways and plantings that are 1 inch in diameter and smaller can be cleanly cut at the edge of the excavation zone. If a root larger than 1 inch in diameter is encountered it should be retained and wrapped in burlap that is kept moist on a daily basis until the project arborist can inspect the root to determine an appropriate course of action.
 3. No plantings or irrigation should be located within 3-5 feet of the trunk of these trees.
 4. No mulch should be located within 2-3 feet of the trunk of these trees.
 5. It is my opinion that this work is within the threshold for these trees' tolerance to construction impacts.
- Tree 3 & 4-The construction activities located within the TPZ of these trees consists of a second floor addition and improvements to the existing foundation. This work has the potential to impact these trees and cause decline. The following are my recommendations to help reduce the potential of these impacts.
 1. Any pruning that is required for clearance purposes should be performed by a certified arborist or tree care professional following best management practices and not construction personnel.

2. Any improvements to the existing foundation should be performed in a manner that is sensitive to the roots of these trees. Locate this work as far from the trunk of these trees as possible. Any roots encountered during this work that are 1 inch in diameter and smaller can be cleanly cut at the edge of the excavation zone. If a root larger than 1 inch in diameter is encountered it should be retained and wrapped in burlap that is kept moist on a daily basis until the project arborist can inspect the root to determine an appropriate course of action.
 3. No plantings or irrigation should be located within 3-5 feet of the trunk of these trees.
 4. No mulch should be located within 2-3 feet of the trunk of these trees.
 5. It is my opinion that this work is within the threshold for these trees' tolerance to construction impacts.
- Tree 5-The construction activities located within the TPZ of these trees consists of a second floor addition, improvements to the existing foundation and portion of the installation of the new ADU. This work has the potential to impact these trees and cause decline. The following are my recommendations to help reduce the potential of these impacts.
 1. Any pruning that is required for clearance purposes should be performed by a certified arborist or tree care professional following best management practices and not construction personnel.
 2. Any improvements to the existing foundation should be performed in a manner that is sensitive to the roots of these trees. Locate this work as far from the trunk of these trees as possible. Any roots encountered during this work that are 1 inch in diameter and smaller can be cleanly cut at the edge of the excavation zone. If a root larger than 1 inch in diameter is encountered it should be retained and wrapped in burlap that is kept moist on a daily basis until the project arborist can inspect the root to determine an appropriate course of action.
 3. No plantings or irrigation should be located within 3-5 feet of the trunk of these trees.
 4. No mulch should be located within 2-3 feet of the trunk of these trees.
 5. It is my opinion that this work is within the threshold for these trees' tolerance to construction impacts.
-

Protective Tree Fencing for Heritage Trees or Street Trees

Fenced enclosures shall be erected around trees to be protected to establish the **TPZ** in which no soil disturbance is permitted and activities are restricted.

Size and type of fence

All trees to be preserved shall be protected with 6-foot high, minimum 12-gauge chain link fence. Fences are to be mounted on 2-inch diameter galvanized iron posts, driven into the ground to a depth of at least 2-feet at no more than 10-foot spacing. This detail shall appear on grading, demolition and building permit plans.

Duration

Tree fencing shall be erected before any demolition, grading or construction begins and remain in place until the completion of the project.

Tree Protection Zones

Each Heritage Tree to be protected, including those on neighboring properties, shall have a designated **TPZ** identifying the area sufficiently large enough to protect the tree and roots from disturbance. **The TPZ area can be determined by the formula: 10 inches per inch of diameter.** For example a 20" diameter tree shall have a 16' radius from the perimeter of the trunk or a 16-foot **TPZ**.

I have calculated the optimal **TPZ** for each that is going to be retained. This information can be found in the Tree Inventory ([See Tree Inventory in Appendix A](#)).

Activities prohibited within the TPZ include

1. Storage or parking vehicles, building materials, refuse, excavated spoils or dumping of poisonous materials, including but not limited to, paint, petroleum products, concrete, stucco mix or dirty water.
2. The use of tree trunks as a winch support, anchorage, as a temporary power pole, signposts or other similar function.
3. Cutting of tree roots by utility trenching, foundation digging, placement of curbs and trenches and other miscellaneous excavation.
4. Soil Disturbance, Soil Compaction or grade changes.
5. Drainage changes.

Special Activities within the Tree Protection Zone

Work in this area (TPZ) requires the direct onsite supervision of the Project Arborist.

Tree Pruning Recommendations

A **crown cleaning** is removal of all dead branches 2 inches in diameter and larger, removal of all broken branches and selective limb removal or end weight reduction to reduce the chances of limb failure.

I have indicated which trees require a crown cleaning within the Tree Inventory.

Mulching Recommendations

I recommended that wood chips be spread within the **TPZ** to a 3-to 5-inch depth, leaving the trunk clear of mulch.

Continued Maintenance Of Heritage Trees During and After Construction Recommendations

I recommend the following for continued maintenance after the completion of the construction process.

1. Monthly arborist inspections for the duration of the construction activities.
2. Quarterly arborist inspections for the first year after the completion of the construction project.

Glossary of Terms

Aerial inspection	An inspection of the upper crown of the tree that requires climbing.
Crown	Parts of the tree above the trunk, including leaves, branches and scaffold limbs. (Matheny and Clark, 1994)
Diameter at standard height (DSH)	The diameter of a tree's trunk as measured at 4.5 feet from the ground. (Matheny and Clark, 1994)
Windthrow	Tree Failure due to uprooting caused by wind. (Glossary of Arboriculture Terms, 2007)
Root crown	Area where the main roots join the plant stem, usually at or near ground level. Root Collar. (Glossary of Arboriculture Terms, 2007)
Root crown inspection	Process of removing soil to expose and assess the root crown of a tree. (Glossary of Arboriculture Terms, 2007)
Visual Tree Assessment (VTA)	A method of visual assessing the condition of a tree that does not include a root crown inspection or an aerial inspection.

Bibliography

Matheny, N.P. and J.R. Clark. *A Photographic Guide to the Evaluation of Hazard Trees in Urban Areas* (2nd Edition). Pleasanton, CA. HortScience Inc., 1994.

Matheny, N.P. and J.R. Clark. *Trees and Development A Technical Guide to Preservation of Trees During Land Development*. Champaign, IL. International Society of Arboriculture, 1998

Harris, R. *Arboriculture Integrated Management of Landscape Trees, Shrubs, and Vines*. Englewood Cliffs, NJ: Prentice-Hall, Inc., 1992

International Society of Arboriculture. *Glossary of Arboriculture Terms*. Champaign, IL Dixon Graphics, 2007

Appendix A – Tree Inventory

Tree #	Species	Botanical Name	DSH (inches)	Heritage Tree	Health Rating	Structural Condition	Observation	Recommendations	10 X Tree Protection Zone
1	Southern Magnolia	<i>Magnolia grandiflora</i>	24	Yes	3	3	There is abnormal growth pattern on the main trunk that may be from a past disease. The upper canopy show signs of decline with twiggy dieback and has been topped in the past for line clearance. The nearby sidewalk has been replaced.	I recommend a crown cleaning and fertilization in fall of 2022.	20
2	Southern Magnolia	<i>Magnolia grandiflora</i>	24	Yes	3	3	There is abnormal growth pattern on the main trunk that may be from a past disease. The upper canopy show signs of decline with twiggy dieback and has been topped in the past for line clearance. The nearby sidewalk has been replaced.	I recommend a crown cleaning and fertilization in fall of 2022.	20
3	Camphor Tree	<i>Cinnamomum camphora</i>	24	Yes	2	2	The upper canopy shows signs of stress/decline and is sparse with large dead branches. There is evidence of past branch removal and I suspect these branches were dead and therefore required removal. This tree has a substantial amount of surface roots. The nearby concrete sidewalk appears to have been replaced in the recent past and is showing signs of uplifting and cracking that I suspect is from the roots of this tree. I suspect that root pruning for this work has contributed to the decline of this tree.	Consider removal and replacement of this tree. If this tree is retained then I recommend a crown cleaning and fertilization in fall of 2022.	20
4	Camphor Tree	<i>Cinnamomum camphora</i>	22	Yes	3	3	There are minor dead branches in the upper canopy. This tree has a large root crown with several surface roots. It appears that roots from this tree are starting to damage the nearby existing driveway surface.	I recommend a crown cleaning and fertilization in fall of 2022.	18
5	Camphor Tree	<i>Cinnamomum camphora</i>	30	Yes	2	2	The upper canopy shows signs of stress/decline and is sparse with dead branches. The upper canopy is one sided due to growing in proximity to other trees. The surface of the neighbors nearby driveway looks like it was replaced in the past from root damage and I suspect this tree was exposed to root loss during this work.	Consider removal and replacement of this tree. If this tree is retained then I recommend a crown cleaning and fertilization in fall of 2022.	25

Tree Report for Murlidhar Residence
Ned Patchett Certified Arborist WE-4597A

10/27/2022

Page 11

Tree #	Species	Botanical Name	DSH (inches)	Heritage Tree	Health Rating	Structural Condition	Observation	Recommendations	10 X Tree Protection Zone
6	Holly Tree	<i>Ilex aquifolium</i>	6	No	3	3	This tree is growing against the foundation of the house and has minor internal dead branches.	This tree is designated for removal.	5
7	Lemon Tree	<i>Citrus limon</i>	6-6	No	3	3	This tree is growing close to the foundation of the existing house. Minor internal dead branches and yellowing of leaves.	Consider removal and replacement of this tree. If this tree is retained then I recommend a crown cleaning and fertilization in fall of 2022. Supplemental irrigation.	7.5

Appendix C – Tree Appraisal Calculations

Tree #	Species	Diameter	Cross Sectional Area of Subject Tree	Condition	Functional Limitations	External Limitations	Replacement Tree Diameter	Cross Sectional Area of Replacement Tree	Replacement Tree Cost (36 inch box)	Unit Tree Cost	Basic Reproduction Cost	Depreciated Reproduction Cost	Cleanup-Tree Removal and Stump Removal	Replacement Tree Installation Cost	Attire-1 Year of PHC and Fertilization (4) Visits	Total Additional Cost	Total Reproduction Cost	Final Appraise Value (Rounded)
1	Magnolia grandiflora	24	452	55%	50%	80%	3.0	7	\$660.00	\$91.96	\$41,600.00	\$9,152.00	\$4,000.00	\$850.00	\$800.00	\$5,650.00	\$14,802.00	\$14,800.00
2	Magnolia grandiflora	24	452	55%	50%	80%	3.0	7	\$660.00	\$91.96	\$41,600.00	\$9,152.00	\$4,000.00	\$850.00	\$800.00	\$5,650.00	\$14,802.00	\$14,800.00
3	Cinnamomum camphora	24	452	40%	60%	80%	3.0	7	\$660.00	\$91.96	\$41,600.00	\$7,987.20	\$4,000.00	\$850.00	\$800.00	\$5,650.00	\$13,637.20	\$13,600.00
4	Cinnamomum camphora	22	380	60%	60%	80%	3.0	7	\$660.00	\$91.96	\$34,953.56	\$10,027.20	\$4,000.00	\$850.00	\$800.00	\$5,650.00	\$15,717.20	\$15,700.00
5	Cinnamomum camphora	30	707	55%	55%	80%	3.0	7	\$660.00	\$91.96	\$65,000.00	\$15,730.00	\$5,000.00	\$850.00	\$800.00	\$6,650.00	\$22,380.00	\$22,400.00
Total																		
\$61,300.00																		

Tree Report for Murlidhar Residence
Ned Patchett Certified Arborist WE-4597A

10/27/2022

Page 14

Appendix D – Arborist Disclosure Statement

Arborists are tree specialists who use their education, knowledge, training, and experience to examine trees. They recommend measures to enhance the beauty and health of trees and attempt to reduce the risk of living near trees. Clients may choose to accept or disregard the recommendations of the arborist or to seek additional advice.

Arborists cannot detect every condition that could possibly lead to the structural failure of a tree. Trees are living organisms that fail in ways we do not fully understand. Conditions are often hidden within trees and below the ground. Arborists cannot guarantee that a tree will be healthy or safe under all circumstances or for a specified period of time. Likewise, remedial treatments like any medicine cannot be guaranteed.

Treatment, pruning, and removal of trees may involve considerations beyond the scope of the arborist's services such as property boundaries, property ownership, site lines, disputes between neighbors, and other issues. Arborists cannot take such considerations into account unless complete and accurate information is disclosed to the arborist. An arborist should then be expected to reasonably rely upon the completeness and accuracy of the information provided.

Trees can be managed, but they cannot be controlled. To live near trees is to accept some degree of risk. The only way to eliminate all risk associated with trees is to eliminate all trees.

Ned Patchett

Ned Patchett
Certified Arborist WE-4597A

Tree Report for Murlidhar Residence
Ned Patchett Certified Arborist WE-4597A

10/27/2022

Page 15

Appendix E – Certification of Performance

I, Ned Patchett, certify;

- That I have personally inspected the tree and the property referred to in this report. I have stated my findings accurately. The extent of the evaluation and appraisal is stated in the attached report and the Terms of Assignment;
- That I have no current or prospective interest in the vegetation or the property that is the subject of this report and have no personal interest or bias with the parties involved;
- That the analysis, opinions and conclusions within this report are my own;
- That my analysis, opinions and conclusions were developed and this report has been prepared accordingly to commonly accepted arboricultural practices;
- That no one provided significant professional assistance to the consultant, except as indicated within the report;
- That my compensation is not contingent upon the reporting of a predetermined conclusion that favors the cause of the client or any other party.

I further certify that I am an International Society of Arboriculture Certified Arborist, and have been involved in the practice of arboriculture and the study of trees for over 27 years.

Signed: Ned Patchett

Date: 10/27/22

Tree Report for Murlidhar Residence
Ned Patchett Certified Arborist WE-4597A

10/27/2022

Page 16



STAFF REPORT

Planning Commission

Meeting Date:

~~4/2/2022~~ 1/23/2023

Staff Report Number:

22-073-PC

Public Hearing and Study Session:

Public hearing for the environmental impact report (EIR) scoping session and study session for the proposed Parkline masterplan project to redevelop SRI International's research and development (R&D) campus with a new office/R&D campus with no net increase in commercial square footage, up to 550 new multi-family dwelling units and 25 acres of publicly accessible open space at 333 Ravenswood Avenue

Recommendation

Staff recommends that the Planning Commission conduct the following items for the proposed project to redevelop SRI International's research campus with a new office/R&D, transit-oriented campus with no net increase in commercial square footage, up to 550 new dwelling units at a range of affordability levels, new bicycle and pedestrian connections, and 25 acres of publicly accessible open space:

- EIR scoping session to receive public testimony and provide comments on the scope and content of a EIR for the proposed project; and
- Study session to receive public comments and provide feedback on the proposed project.

The December 12th meeting will not include any project actions. The proposal will be subject to additional review at future Planning Commission and City Council meetings.

Staff recommends the following meeting procedure to effectively and efficiently move through the two items, allowing the public and the Planning Commission to focus comments on the specific project components.

EIR scoping session

- Introduction by Staff
- Presentation by Applicant on Project Proposal
- Presentation by City's EIR Consultant
- Public Comments on EIR scope
- Commissioner Questions on EIR scope
- Commissioner Comments on EIR scope
- Close of Public Hearing

Project proposal study session

- Introduction by Staff
- Public Comments on Project
- Commissioner Questions on Project

- Commissioner Comments on Project

Staff believes that it would be beneficial for the Planning Commission and members of the public to receive the applicant's presentation during the EIR scoping session portion of the public hearing to provide a more robust understanding of the proposed project that will be studied in the EIR. Accordingly, staff recommends that the Planning Commission allow the applicant to present the overall project, followed by a presentation from the City's EIR consultant (ICF) outlining the California Environmental Quality Act (CEQA) process.

Policy Issues

Scoping sessions on the EIR provide an opportunity early in the environmental review process for Planning Commissioners, public agencies, and community members to comment on specific topics that they believe should be addressed in the environmental analysis. Study sessions provide an opportunity for Planning Commissioners and interested community members to provide more general feedback on a proposed project, with comments used to inform future review and consideration of the proposal. The EIR scoping session public hearing and study session should be considered as separate items, as part of the same hearing.

A masterplan project provides a vision and framework for growth and development of the site. The applicant is requesting general plan and zoning ordinance amendments to enable the proposed masterplan development. The new general plan land use designation would allow for residential dwelling units, public and quasi-public uses, office, R&D, and supporting uses. As currently proposed, the designation would apply to the entire site and establish a maximum residential density at 45 dwelling units per acre and a maximum commercial floor area ratio (FAR) of 0.6, based on the amount of existing square footage on-site, to allow up to approximately 1.38 million square feet of non-residential uses. The non-residential square footage would not exceed the current square footage of all buildings on the project site.

The proposed project is anticipated to require the following entitlements and/or City permits:

1. **Environmental Review** to analyze potential environmental impacts of the proposed project through a full EIR, pursuant to CEQA;
2. **General Plan and Zoning Ordinance Amendments** to enable the proposed masterplan development;
3. **Rezoning** to apply the new zoning district(s) to the project site;
4. **Conditional Development Permit (CDP)** to enable comprehensive planning of the project;
5. **Development Agreement (DA)** for vested rights in exchange for community benefits;
6. **Architectural Control** to review the design of the future new buildings and associated site improvements;
7. **Vesting Tentative Map** to merge the existing lots and re-subdivide in a manner consistent with the proposed improvements;
8. **Heritage Tree Removal Permits** to remove heritage trees to enable the proposed project and plant heritage tree replacements per the City's municipal code requirements; and
9. **Below Market Rate (BMR) Housing Agreement** to provide on-site BMR units in accordance with the City's BMR Ordinance.

In addition, a Fiscal Impact Analysis (FIA), Water Supply Assessment (WSA), and Housing Needs Assessment (HNA) will be prepared. Additional actions and entitlements may be required as the project plans are refined.

The City Council would be the decision-making body for the EIR, general plan amendment, zoning ordinance amendment, and rezoning to allow the proposed mix of uses and densities/intensities requested by the applicant, and the CDP to enable comprehensive planning of the project. The City Council would also be the acting body on the development agreement, which would provide vested rights in exchange for community benefits, the vesting tentative map to merge the existing lots and re-subdivide in a manner consistent with the proposed improvements, and the BMR Housing Agreement. The Planning Commission would be the acting body for any future architectural control permits for the proposed new buildings and the recommending body on all other entitlements, and the City Arborist would issue the Heritage Tree Removal Permits.

Background

SRI International (formerly known as the Stanford Research Institute) is an independent, nonprofit research institute located on an approximately 63-acre campus at 333 Ravenswood Avenue. The existing development on the SRI campus is regulated through a CDP, which was first approved in 1975. The most recent amendment to the CDP occurred in 2004 for the construction of Building T, at the southernmost portion of the campus. The CDP establishes standards for the use and development of the campus.

The site of SRI's campus has been reduced over time. In 1978, an amendment to the CDP was approved to remove approximately 10.3 acres from the site for the development of the McCandless office complex on Middlefield Road, and in 1997, the size of the campus was further reduced when part of the property was sold to Classic Communities for the development of 33 single family residential units in the Burgess Classics development.

The existing CDP allows a maximum employee count of 3,308. The applicant indicates approximately 1,100 people are currently employed at the project site, although SRI's headcount has fluctuated between approximately 1,400 and 2,000 workers since 2003.

Lane Partners has been working with staff on this proposal and submitted a pre-application package in April 2021. On June 22, 2021, the applicant gave an introductory presentation on the project to the City Council. Although the Council didn't provide specific feedback, public comment was received. In October 2021, the applicant submitted a formal application package, with a resubmittal package submitted in January 2022. The Planning Commission reviewed the proposed project at a study session during its meeting on March 28, 2022, received public comments and asked clarifying questions. Planning Commissioners discussed the following at the study session:

- Interest in increasing residential densities and inquiries regarding an acre of land being used for a fully affordable project, in addition to the required 15% below market rate (BMR) housing units;
- Interest in reducing proposed parking and/or placing parking underground;
- Questions about the programming for the sports field and potential conflict with the adjacent church;
- Questions about the pre-pandemic parking needs and number of employees at SRI;
- Concerns about traffic congestion and interest in increased transit use for future site occupants; and
- Interest in the potential realignment of Ravenswood Avenue and Ringwood Avenue.

Excerpt minutes from the March 28th Planning Commission meeting are included as Attachment I.

The City Council held a study session on May 10, 2022, reviewed the proposed project, received public comments and asked clarifying questions. City Council members discussed the following general topics at

the study session:

- Options to mitigate noise;
- Security and bike and pedestrian safety;
- Potential shuttle programs, parking ratios, and transit passes;
- BMR requirements and other affordable housing;
- Onsite amenities/community amenities;
- Site density and intensity; and
- Water usage of the proposed project.

Site location

For purposes of this staff report, Ravenswood Avenue is used in an east to west geographic orientation. The project site is located at 333 Ravenswood Avenue and generally bound by Laurel Street to the west, Ravenswood Avenue to the north, Middlefield Road to the east and the Burgess Drive ROW to the south. The site contains 38 existing buildings, totaling approximately 1.38 million gross square feet, which include a mix of office, R&D, and support uses. The surrounding zoning and land uses are provided in Table 1 and a location map is included as Attachment A.

Table 1:Surrounding Land Uses and Zoning		
Item	Existing Land Uses	Zoning
North	Single-Family & Multi-Family Residential/Church	R-1-S/R-2/R-3/City of Atherton
South	City Corp. Yard/USGS/Multi-Family Residential	PF/R-3(A)
East	Menlo Atherton High School/Office (McCandless office complex)	City of Atherton/C-1-X
West	Civic Center/Burgess Park/Single-Family Residential (Classic Communities)	PF/R-3(X)

Most nearby buildings are one to three stories in height. Ravenswood Avenue and Middlefield Road are major city through streets (classified as “Avenues – Mixed Use” in the City’s Circulation Element). The Park Station (Caltrain) and the developing Downtown/El Camino Real area are within walking and biking distance.

Project overview

The applicant is proposing to comprehensively redevelop the SRI campus with a residential, office, R&D, and retail mixed-use project. The proposed project would be divided into an approximately 53-acre office/R&D campus covering most of the existing project site and a 10-acre residential area along the Laurel Street edge of the project site extending slightly east along Ravenswood Avenue. Site circulation, open space, and landscaping (other than retained trees) would be redesigned and rebuilt per a new comprehensive campus plan, including new bicycle and pedestrian connections. There would be no net increase of non-residential square footage. Primary program elements include:

- Approximately 287,000 square feet of existing office/R&D (retained in Buildings P, S, and T);
- Demolition of 35 structures comprising approximately 1.1 million square feet, to be replaced with new office/R&D space in five main structures, three to five stories in height, along with a smaller amenity building;
- Three new parking structures for the non-residential uses;
- 450 multifamily residential dwelling units (19 townhomes at two stories) and (431 apartments at three to

- six stories) in approximately 500,000 square feet of floor area;
- An approximately one-acre portion of land, proposed to be dedicated to an affordable housing developer for the future construction of a 100 percent affordable housing or special needs project of up to 100 dwelling units;
- Approximately 25 acres of landscaped, publicly-accessible open space, including a large central open space between the office/R&D buildings; and
- A sports field and one-story community building adjacent to the Ravenswood Avenue/Middlefield Road intersection.

As part of the proposed project, the existing 6-megawatt natural gas power plant that generates power and steam energy for the existing SRI International campus would be demolished and the entire project site would be converted to all-electric energy usage, with the exception of two of the existing buildings that would remain (Buildings P and T) and potential backup diesel generators, in compliance with the city Reach Code. (It is possible that limited exceptions may be requested to accommodate additional life science uses.)

The project plans are included in Attachments B, C and D and the applicant's project description letter is included in Attachment E.

CEQA review

An EIR is an informational document that the City must prepare and consider before any discretionary action is taken by the City on the proposed project. The purpose of an EIR is to provide decision makers and the public with detailed information about the effect that the proposed project may have on the environment, list ways in which the significant effects of the proposed project might be minimized and identify alternatives to the proposed project. The main substantive components of an EIR are as follows:

- The project description, which discloses the activities that are proposed for approval;
- Discussion and analysis of the potentially significant environmental effects of the proposed project, including cumulative impacts and growth-inducing impacts;
- Discussion of ways to mitigate or avoid the proposed project's potentially significant environmental impacts; and
- Discussion of alternatives to the project as proposed.

The EIR process begins with the City's decision to prepare an EIR. The City determined that an EIR was required for the proposed project and issued a Notice of Preparation (NOP). The proposed project requires a full EIR instead of a focused EIR as has been prepared for some projects in the Specific Plan and Bayfront areas. Because this will be a full EIR, an initial study was not prepared as has been done for projects that utilize a focused EIR; this is because a full analysis will be done in the EIR of the proposed project's potential impacts. The City released the NOP (Attachment F) on December 2, 2022.

The draft EIR will be prepared and processed in accordance with CEQA and the State CEQA Guidelines in effect at the time of the release of the NOP.

CEQA topic areas included in EIR

The EIR will analyze whether the proposed project would have significant environmental effects in the following topic areas:

- Aesthetics¹
- Air Quality
- Biological Resources
- Cultural Resources
- Energy
- Geology and Soils
- Greenhouse Gas Emissions
- Hazards and Hazardous Materials
- Hydrology and Water Quality
- Land Use and Planning
- Noise
- Population and Housing
- Public Services and Recreation
- Utilities
- Transportation and Traffic
- Tribal Cultural Resources

To help prepare several of these sections and analyze the potential impacts, a transportation impact analysis (TIA) will be prepared in accordance with the City's TIA Guidelines. The EIR will use vehicle miles traveled (VMT) as the CEQA threshold of significance for transportation and traffic. In addition, while not required by CEQA, a housing needs assessment (HNA) will be prepared to inform the population and housing analysis.

CEQA topic areas not requiring further analysis

The proposed project is not anticipated to result in environmental impacts in the following topic areas:

- Agricultural or Forestry Resources
- Mineral Resources
- Wildfire

The project site is fully developed in an urbanized area and within a transit priority area. As such, agricultural and mineral resources do not exist on the site and wildfires are not considered a concern. A detailed analysis of these topics will not be included in the EIR. Therefore, these topic areas are currently scoped out of the EIR. This, however, does not limit the public or Planning Commission's ability to comment on the scope and content of the EIR relative to these topic areas.

Analysis

EIR Scoping Session

The City released a Notice of Preparation (NOP) (Attachment F) for the proposed project on December 2, 2022, beginning an extended review and comment period ending on January 9, 2023 to account for the City Hall closure from December 26, 2022 through January 2, 2023. Hard copies are also available for review at the Menlo Park Main Library and Belle Haven Branch Library. Interested persons should inquire at the library reference desk.

A NOP signals the City plans to prepare an EIR for the proposed project and begins the EIR process. The NOP and scoping process is designed as an early opportunity to seek guidance from interested parties,

¹ The project site is located within a "transit priority area", as defined, and thus pursuant to Public Resources Code Section 21099, aesthetic and parking impacts are not considered significant impacts on the environment. Accordingly, the analysis in the EIR will reflect this statutory directive. Nevertheless, the City still retains authority to consider aesthetic impacts pursuant to its design review authority.

agencies and members of the public on the scope and content of the EIR. The EIR is an informational document the purpose of which is to provide decision makers and the public with detailed information about the potential impacts that the proposed project may have on the environment, list ways in which the potentially significant impacts of the proposed project might be minimized, and identify alternatives to the proposed project.

The December 12, 2022 Planning Commission meeting falls within the required minimum 30-day comment period, and serves as a scoping session for the proposed project. Comments can be made on the scope, content, and focus of the analyses in any of the CEQA topic areas, including the topics proposed to be scoped out of the EIR. Examples of comments include, but are not limited to, suggested mitigation measures, suggested alternatives (e.g. increase or decrease in housing units, commercial square footage, other uses etc.), or areas of study that should not be scoped out. These topics are only examples to help provide context to the Commission, interested agencies, and members of the public on the types of comments that could be provided on the EIR scope and are not intended to limit the scope of comments.

Verbal comments received during the scoping session and written comments received during the NOP comment period on the scope and content of the environmental review will be considered while preparing the draft EIR. NOP comments will not be responded to individually; however, all written comments on the NOP will be included in an appendix of the draft EIR, and a summary of all comments received (both written and verbal) on the NOP will be included in the body of the draft EIR.

Analysis of proposed office and R&D

The Office/R&D District buildings would be flexibly designed to accommodate office or R&D tenants, including life science uses, depending on future tenant and market needs. Likely the proposed project buildout would contain a mixture of these uses. Because future tenants have not been identified, the EIR will evaluate two scenarios: a 100 percent office scenario and a 100 percent R&D scenario. Each section in the EIR will evaluate the most intense scenario for the resource area being analyzed. This will ensure that the EIR evaluates the proposed project's maximum potential impact, and that any future tenant mix is within the scope of the EIR. The applicant indicates anticipated tenant occupancy levels within the Office/R&D District would be consistent with current market demands:

- Office: Approximately one occupant per 250 square feet
- R&D: Approximately one occupant per 350 square feet to 425 square feet for life sciences

Variants

Variants are variations of a project at the same project site, with the same objectives, background, and development controls but with additions and changes from a project, whose inclusion may or may not reduce environmental impacts. Thus, variants are distinct from "alternatives" (discussed below) insofar as CEQA requires the consideration of alternatives to avoid or lessen significant effects of a project. The EIR will include variants proposed by the Project Sponsor or the City and the description and analysis of the variants will be equal in detail to those of the proposed project. The EIR will describe and analyze the following variants:

- Emergency Reservoir Variant: This variant would be similar to the proposed project except it would also include an approximately 2 million gallon below grade concrete water reservoir and associated facilities (including a pump station building, surge tank, and well head) that would be aboveground and surrounded by a fence or screen. The area for the emergency reservoir and associated facilities would be leased by the City. The specific location of the emergency water reservoir and associated facilities within the project site has not yet been determined, but would likely be located on the northeastern portion of the project site.

- **Increased Residential Variant:** This variant would be similar to the proposed project except it would include up to 600 multi-family residential units, 50 more dwelling units than under the Proposed Project. The additional residential dwelling units would be located along Laurel Street within the Residential District. As a result, the proposed building height along Laurel Street would increase and additional subterranean parking may be required.

Alternatives

If there are significant impacts, the alternatives analysis will focus on those alternatives that would reduce identified impacts. If the impacts are less than significant with mitigation, the alternatives analysis is anticipated to focus on those alternatives that would further reduce those impacts or provide policy focused alternatives considering allowable development under the Zoning Ordinance. Section 15126.6(e) of the State CEQA Guidelines requires the evaluation of a No Project Alternative. Other alternatives may be considered during preparation of the EIR and will comply with the State CEQA Guidelines, which call for a “range of reasonable alternatives to the project, or to the location of the project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project.” The City is currently considering analysis of the following alternatives, and is seeking input on these alternatives and any other potential alternative that should be evaluated as part of the EIR:

- CEQA-Required No Project Alternative (maintaining the existing buildings with no new construction);
- Project Alternative that would reduce any environmental impacts; and
- Policy focused project alternative.

Next steps

Following the close of the comment period on the scope and content of the EIR, City staff and the consultant will consider all comments in the development of the draft EIR. The draft EIR is tentatively planned to be released in the summer of 2023 with a minimum 45-day public review and comment period. During the 45-day review and comment period on the draft EIR, the Planning Commission would hold a public hearing to discuss the draft EIR at which interested persons would be able to provide comments. Once the draft EIR comment period is completed, the environmental consultant will review and respond to all comments received in what is referred to as a “Response to Comments” document or final EIR.

Study Session

Planning Commission considerations

The study session portion of this report highlights a variety of topic areas and discussion items for consideration. As the Planning Commission reviews the proposal, staff recommends that the Commission consider the following topics and use these as a guide to ask clarifying questions:

- Proposed land uses and site density and intensity;
- Site access, including vehicular, pedestrian and bicycle;
- Architectural styles;
- Design and layout of open space;
- Parking locations and ratios; and
- Proposed sustainability measures.

Project updates

The addition of a separate parcel of land to be leased to an affordable housing developer and the increase from 400 to 450 proposed dwelling units, not including the separate parcel, are the main revisions to the proposed project since the previous Planning Commission study session on March 28, 2022. Smaller revisions, including orienting proposed buildings to make the main publicly accessible open spaces visible

from the public streets and some square footage updates have also been made and are discussed below.

Office/R&D district

The office/R&D district would be located in the middle of the site and extend to the eastern and southern property lines as well as to a portion of the northern property line. The applicant proposes that the new office/R&D buildings would be designed for established and emerging businesses. The project would consist of approximately 1.1 million square feet of office/R&D in five new buildings, an office amenity building, and a new community building. The project site currently contains approximately 1.38 million square feet of R&D/office uses. Existing Buildings S and T, located to the west of the USGS site, and Building P, located to the east of the proposed new residential buildings along Laurel Street, total approximately 283,826 square feet and would be retained for SRI’s continued operations. Table 2 provides additional information on the buildings that would comprise the non-residential uses and minor square footage adjustments made since the previous submittal.

Table 2: Non-residential buildings		
Building	Gross floor area (March 2022)	Gross floor area (December 2022)
Bldg. 1 (3 stories)	165,000 sf	184,000 sf
Bldg. 2 (5 stories)	244,000 sf	227,300 sf
Bldg. 3 (5 stories)	244,000 sf	227,300 sf
Bldg. 4 (4 stories)	198,000 sf	229,000 sf
Bldg. 5 (4 stories)	198,000 sf	184,000 sf
Office Amenity Bldg. (2 stories)	44,719 sf	40,000 sf
Community Bldg. (1 story)	2,000 sf	2,002 sf
Sub-Total (new)	1,095,719 sf	1,093,602 sf
Bldg. P (existing to remain)	180,519 sf	183,423 sf*
Bldg. S (existing to remain)	21,241 sf	21,241 sf**
Bldg. T (existing to remain)	82,066 sf	82,066 sf**
Sub-total (existing to remain)	283,826 sf	286,730 sf
TOTAL	1,379,545 sf	1,380,332 sf

*This number is a correction of the existing square footage for Building P

** These square footages represent the existing square footages and do not reflect any changes associated with SRI’s separately proposed tenant improvements. The applicant indicates the tenant improvements are estimated to yield approximately 3,000 additional square feet within Building P and a reduction of approximately 6,000 square feet within Building S.

The applicant proposes that the non-residential portion of the project would be accessible to vehicles from two entrances along Ravenswood Avenue and two entrances along Middlefield Road. The applicant indicates the proposed office/R&D buildings would be arranged to form a central aggregated, publicly-accessible open space, and the proposed architectural character of the buildings would be modern, with

building masses defined by main entrances, first floor articulations such as loggias, elevated exterior balconies, and the use of natural materials and integrated indoor/outdoor workspaces. As shown in the preliminary architectural plans exterior cladding systems under consideration include terracotta rainscreen, glass fiber reinforced concrete, metal panel, stone, and other natural materials.

An approximately 40,000-square-foot, two-story office/R&D amenity building is proposed directly adjacent to Parking Garage 3. The building would contain a full-service café and other amenities, including a possible fitness center, for SRI and the site’s commercial tenants.

The project plans identify that parking for the non-residential uses would be provided in three parking garages, three to four stories tall, and surface parking areas located throughout the site. Parking Garages 1 and 2 would be located along the eastern property line and Parking Garage 3 would be located more centrally near the southwest of the project site, just south of the office amenity building. Buildings 1 and 5 would each have some underground parking spaces as well.

Community building

An approximately 2,000-square-foot, one-story, community-serving building is proposed to be located on the northeast corner of the site, across Middlefield Road from Menlo Atherton High School. The applicant indicates this building would include community-serving retail uses, which may include a bicycle repair shop and juice bar, and publicly-accessible restrooms. As project review continues, the uses within this building would be further refined by the applicant. This building is proposed to be adjacent to a publicly-accessible open space, which could provide community functions, such as a recreational field, public parking, and a children’s play area. The public parking would be available to users of the publically-accessible open space and community building, and the neighboring church would use some spaces, as they currently use some SRI parking spaces per parking agreements. The applicant indicates specific programming functions for the community building and surrounding facilities would be determined in coordination with the City and community.

Residential district

The proposed 450 housing units would consist of approximately 431 apartments and 19 townhomes, with 15 percent of units proposed to be affordable units pursuant to the City’s BMR housing program. Table 3 below indicates the proposed unit types and totals. As currently proposed, the totals include BMR units but the specific numbers of BMR units for each unit type and income level have not been determined.

Table 3: Residential Units		
Unit Type	Unit total (March 2022)	Unit total (Dec. 2022)
Studio	70	75
1 bedroom/1 bath	175	198
2 bedroom/2 bath	125	144
3 bedroom/ 2 bath	11	14

3 bedroom/2bath (townhouse)	19	19
Total	400	450

The residential district would extend from the Burgess Classics neighborhood along Laurel Street north to Ravenswood Avenue and east, partially along Ravenswood Avenue. Approximately 19 rental townhouses would be located between the apartment buildings and the Burgess Classics neighborhood to further diversify the housing mix and provide a scaled transition from the multi-family buildings to the single-family residences. The 431 apartments would be distributed between the three buildings, three to six stories in height, and a total of approximately 500,000 square feet of gross floor area. All residential units are noted in the project description to have some type of exterior deck or patio.

Since the previous study session, the proposed project has been revised to include a separate approximately one-acre portion of land, proposed to be dedicated to an affordable housing developer for the future construction of a 100 percent affordable housing or special needs project which would be separately rezoned as part of the proposed project for up to 100 units. The exact location of this dedicated land area has not been determined.

Parking for the apartments is proposed to be above-grade, in one-story garages, creating a podium on the second floor for common open space for each apartment building. Residential buildings 1 and 2 are also now designed to include one level of subterranean parking. The majority of the garages would not be visible as apartments would partially wrap the sides of the parking structures on the first level. There would also be some surface parking along the private street adjacent to apartment buildings for short-term and visitor parking. The townhome portion of the project would be organized around its vehicle access, with the parking spaces for the townhomes in attached garages.

The project description indicates the buildings would be Mission Style (i.e., Spanish derivative) with white stucco walls, heavy timber brackets and detailing, and clay tile roofs. Building massing would include peaked/sloped rooflines. Additionally, the applicant indicates main building entrances would be highlighted along the street with landscaping, human-scaled plazas, lighting, and trellis structures.

The proposed residential units would be rental units. The applicant indicates that a ground lease for the residential units, and the rest of the project site, is anticipated and this would limit the ability to include for-sale units.

Vehicular access and site circulation

The proposal includes separate vehicular circulation for the residential and office/R&D uses although paths for pedestrian and bicycle access would provide connections between the two elements. A loop road, with access off of Ravenswood Avenue and Middlefield Road, would provide access to the office/R&D buildings and the community building. The apartment buildings are proposed to have their own access road with entry points at Laurel Street and Ravenswood Avenue. The townhomes are proposed to have a separate access directly from Laurel Street, which would not connect to the road between the apartment buildings or the loop road. As shown on the master plan project plans (Attachment B), there would also be emergency vehicle access from the apartment buildings to the loop road and from the loop road to Laurel Street. There would also be emergency vehicle access to the loop road from Burgess Drive. The applicant indicates a security

gate for emergency access and limited service vehicles would likely be located where Burgess Drive intersects the loop road, however, the gate would not impede bicycle or pedestrian circulation.

The applicant's proposal states the circulation design would achieve the following objectives:

- Establishment of private internal streets and roads;
- Separation of office/R&D from residential access and circulation;
- Creation of on-site roads to manage internal vehicular circulation and access to office/R&D and residential buildings;
- Minimization of additional vehicular circulation to and from Laurel Street;
- Three access points to the residential portion of the site (one along Ravenswood Avenue, toward the west side of the site, one along Laurel Street for the multi-family residential buildings, and a separate driveway entrance along Laurel Street for the townhouses);
- An internal road to the three main residential buildings and vehicular access to parking garages and loading areas;
- Four access points to the office/R&D portion of the site (two along Ravenswood Avenue and two along Middlefield Road, with one at Ringwood Avenue and one at Seminary Drive); and
- An internal loop road to provide access to all of the office/R&D buildings, office amenity building, community building, parking garages, surface parking areas, loading areas, as well as emergency vehicle access.

The applicant indicates the project would develop a project-specific TDM (Transportation Demand Management) plan for both the residential and non-residential uses to reduce the total number of single-occupancy vehicle trips affiliated with the project by 20 percent, with a TDM plan that would complement the mixed-use campus' proximity to downtown and the Menlo Park Caltrain station. The applicant indicates that they anticipate the Project would provide electric-powered shuttles for use by employees and residents for access to and from the Caltrain station.

Pedestrian and bicycle circulation

A Class I multiuse bicycle and pedestrian path would be located on the north side of the site along Ravenswood Avenue. This on-site path would create a protected alternative option for bicyclists currently using the bike lane on Ravenswood Avenue. The Class I path would loop southward into the project site toward the east and provide a crossing at Ringwood Avenue and Middlefield Road. This would provide safe access to Menlo Atherton High School and would connect to the existing bicycle path on Middlefield Road. A Class I multi-use bicycle and pedestrian path would extend from Laurel Street at Burgess Drive along Burges and the south side of the project site to connect to Middlefield Road at Seminary Drive. On the west, this path would be situated at Laurel Street to connect to the City's proposed Caltrain undercrossing at El Camino Real.

The proposed bicycle and pedestrian connections through the site would link with a broader network of existing and planned infrastructure, as can be seen on the map included as Attachment G from the City's Transportation Master Plan. The proposed Middle Avenue undercrossing would connect bicycle/pedestrian infrastructure to the west of El Camino Real with the bicycle/pedestrian path along the southern edge of the project site. At Middlefield Road, bicyclists would be able to travel east along Ringwood Avenue to the US 101 bicycle and pedestrian bridge, through the Belle Haven neighborhood and access the Bay Trail through

the recently opened bicycle/pedestrian bridge over Bayfront Expressway at the Meta West Campus.

Parking

Overall, the parking rate for the non-residential uses would be approximately two spaces per 1,000 square feet. According to City records, the current parking rate for the project site is approximately 2.3 spaces per 1,000 square feet. For comparison, the LS (Life Sciences) district in the Bayfront area requires a maximum of 2.5 parking spaces per 1,000 square feet and a minimum of 1.5 parking spaces per 1,000 square feet for R&D uses and the O (Office) district requires a maximum of three spaces and a minimum of two spaces per 1,000 square feet of office space.

The parking rate for the residential dwelling units would be approximately one space per apartment and two spaces per townhome. The applicant indicates in their project description letter that shared parking would be available for residential visitors on evening and weekends at the office/R&D surface lots and parking structures. While parking rates vary throughout the zoning districts, the R-MU (Residential Mixed Use) zoning district has an emphasis on residential and requires a minimum of one parking space per unit. This district also limits permitted parking to a maximum of 1.5 spaces per unit.

Trees, landscaping, and open space

The applicant indicates their landscape concept is to create a network of publicly-accessible pedestrian and bicycle trails, parks, open spaces, and active/passive recreational areas, incorporating many existing and new trees. Additionally, the applicant indicates open space would also be utilized to create welcoming edges along Ravenswood Avenue, Laurel Street and Middlefield Road. The three main open space areas are described below.

Ravenswood Avenue Parklet

The Ravenswood Avenue parklet would be approximately six acres located on the northerly edge of the site along Ravenswood Avenue and would protect the existing heritage trees and provide a landscaped and screened frontage. A shared use path would weave through the existing trees in the setback area to connect with and support pedestrian and bicycle circulation throughout the site. Small scale public spaces, such as picnic areas and exercise stations would be connected to the shared-use path. The parklet would lead to a large multi-use plaza which would provide a visual connection to the Parkline Central Commons.

Parkline Recreational Area

The Parkline Recreational Area would provide a community recreational sports area of approximately two acres, located on the northeast corner of the site at the intersection of Ravenswood Avenue and Middlefield Road. This area would be connected to the Ravenswood shared-use path. This open space area would provide publicly accessible community functions, such as a recreational field, public parking, a children's play area, and other activity areas. In addition, the approximately 2,000-square-foot community amenities building would contain publicly accessible restrooms, and potentially small retail spaces. The City is exploring a possible partnership with Parkline to evaluate opportunities for emergency water supply and/or storage facilities in this area, and this concept is included as a project variant and described under the EIR Scoping Session portion of this report.

Parkline Central Commons

The Parkline Central Commons would provide an approximately 9-acre central open space area located between the Office/R&D buildings and the office amenities building. This space would offer a variety of programmed open space, such as flexible-use lawn areas and a multi-use plaza that can accommodate gatherings. The Parkline Central Commons may also include an event pavilion and landscaped areas. Additionally, smaller landscaped spaces for tenant use would be located adjacent to the buildings, which would provide outdoor seating and shaded tree groves. Primary pedestrian circulation paths would connect all the edges of the site to the Parkline Central Commons.

Since the previous study session, the applicant has revised the plans to increase the visibility of the central publically accessible open space by relocating the office amenity building to be directly adjacent to Parking Garage 3 and reorienting Office Building 5. These revisions to the site plan would allow the Central Commons to be visible from Laurel Street, and to lesser extent from Ravenwood Avenue.

Heritage trees

The site currently contains 565 heritage trees, of which 351 would be retained and 214 would be removed. Including non-heritage trees, approximately 615 trees would be retained on site and an additional 912 trees are proposed to be planted. The size/age of the trees to be planted has not yet been determined but as the plans develop, staff will work with the applicant to determine appropriate tree sizes/ages. A complete tree survey and disposition plan is included as hyperlink Attachment H. The applicant indicates their tree management and retention plan is based on the following:

- The preservation of healthy heritage trees that are of a desirable tree species;
- Special effort to preserve coastal live oaks, valley oaks, and coast redwoods based on their native habitat and ecological significance; and
- Incorporation of existing heritage trees into the overall design.

As the project review continues, the Planning Division and City Arborist team will review and evaluate the arborist report, the tree disposition and removal plans, and determine whether the requested heritage tree removals are supportable based on the information to be provided with heritage tree removal permit applications. If the City Arborist approves some or all of the removals, his or her decision is appealable to the Environmental Quality Commission. Further, as part of that review, the City will evaluate the potential impacts of the project on the heritage trees proposed to remain and work with the applicant team to identify preservation measures. The heritage tree replacement plan would be subject to the City's valuation requirements for replacement trees. The replacement plan will be incorporated into subsequent reviews of the proposed project.

Sustainability

The applicant indicates the project would incorporate the following sustainability measures:

- Pursue certification by the state as an Environmental Leadership Development Project (under SB 7). As part of that certification, the proposed project would need to demonstrate that it would result in no net additional GHG emissions compared to existing conditions.
- Source-separating and tracking waste throughout construction to divert waste away from landfills.
- Demolition of most existing buildings onsite, including the cogeneration plant, and replacement with more energy efficient buildings.
- Incorporation of a range of LEED certification strategies or equivalent standards across the Office/R&D

- and Residential Districts, including minimum LEED Gold certification by the USGBC or equivalency verified through the City of Menlo Park's LEED Performance Program, and related certifications;
- LEED New Construction certification or equivalent standards for multifamily residential buildings;
 - LEED for Homes certification or equivalent standards for residential.

Correspondence

As of the writing of this report, staff has received one item of correspondence regarding the project since the previous study session. The email, included as Attachment J, discusses concerns regarding the sports field.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the proposed project. The project sponsor is also required to fully cover the cost of work by consultants performing environmental review and additional analyses to evaluate potential impacts of the project.

Environmental Review

An EIR will be prepared for the proposed project. On October 18, 2022 the City Council authorized the City Manager to enter into a revised contract with ICF to complete the environmental review and prepare an EIR for the proposed project. The Planning Commission would provide a recommendation to the City Council on the project entitlements including the certification of the EIR, after the completion of the environmental review.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 1,320-foot radius of the subject property.

Attachments

- A. Location Map
- B. Masterplan Project Plan: – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/20221031-parkline-masterplan-plan-set.pdf>
- C. Non-residential Project Plans – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/20221031-non-residential-architectural-plans.pdf>
- D. Residential Project Plans – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/20221031-residential-architectural-control-plans.pdf>
- E. Project Description letter – hyperlink: <https://menlopark.gov/files/sharedassets/public/services/community-development/documents/projects/under-review/parkline/20221205-parkline-project-description.pdf>
- F. Notice of Preparation – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/parkline-notice-of-preparation.pdf>
- G. Existing and Proposed Bike Paths from Transportation Master Plans

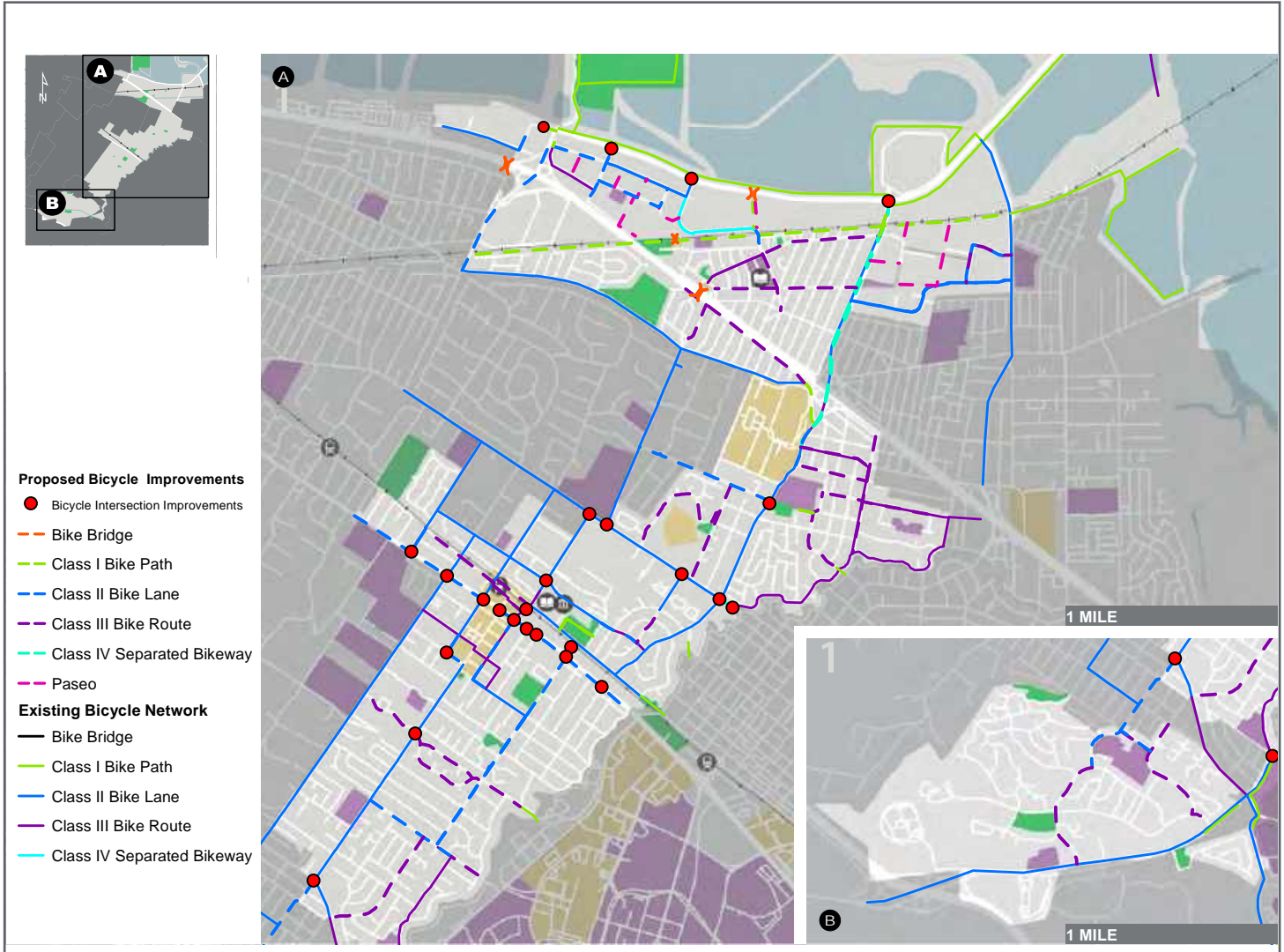
- H. Tree disposition Plan – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/parkline-tree-disposition-plan.pdf>
- I. Excerpt minutes of Planning Commission meeting on March 28, 2022
- J. Correspondence

Report prepared by:
Corinna Sandmeier, Acting Principal Planner

Report reviewed by:
Kyle Perata, Planning Manager
Michael Biddle, Special Counsel

Recommended TMP Projects

EXISTING BIKE NETWORK AND RECOMMENDATIONS



Recommendations for people biking at various locations throughout Menlo Park will help improve safety and connections to the existing bicycle network.

Planning Commission



REGULAR MEETING MINUTES - EXCERPT

Date: 3/28/2022
Time: 7:00 p.m.
Location: Zoom

Regular Meeting

A. Call To Order

Chair Michael Doran called the meeting to order at 7:01 p.m. At Chair Doran's request, Associate Planner Matt Pruter explained how applicants and the public would be able to participate in the virtual meeting.

B. Roll Call

Present: Andrew Barnes, Chris DeCardy (Vice Chair), Michael Doran (Chair), Camille Gonzalez Kennedy, Cynthia Harris, Henry Riggs, Michele Tate

Staff: Payal Bhagat; Contract Planner; Nira Doherty, City Attorney; Fahteen Khan, Assistant Planner; Eric Phillips, Special Counsel; Matt Pruter; Associate Planner; Corinna Sandmeier, Acting Principal Planner

G. Study Session

- G1. Study Session/Nick Menchel/333 Ravenswood Avenue (Parkline):
 Request for a study session on a master plan development to comprehensively redevelop the SRI campus with a residential, office, research and development, and retail mixed-use project. The proposed project includes requests for a general plan amendment, zoning ordinance amendment, rezoning, conditional development permit (CDP), development agreement (DA), architectural control, vesting tentative map, and below market rate (BMR) housing agreement. The project would necessitate the preparation of an environmental impact report (EIR) in compliance with the California Environmental Quality Act (CEQA). (Staff Report #22-018-PC)

Staff Comment: Acting Principal Planner Sandmeier said 23 new emails had been received on the proposed project, and that many were in favor of additional housing and additional BMR housing; and some expressed concerns about the impacts to neighbors and to the church located at 201 Ravenswood Avenue. She said the existing SRI campus was an approximately 63-acre site with 38 buildings and 1.38 million square feet of gross floor area. She said the proposed project had no net increase of nonresidential square footage and that approximately 284,000 square feet would be retained for SRI's use in Buildings P, S and T. She said approximately 1.1 million new square feet of office and research and development uses were proposed in five main structures from three to five stories, a new office amenity building, and three parking structures for nonresidential use. She said the proposal included 400 residential rental units. She said that included 15% Below Market Rate (BMR) units, 19 two-story townhomes with attached two-car garages, 391 apartments in three buildings, three to five stories tall, and approximately one parking space per unit and one-story parking garages with podiums at the second level for private open space for the apartments. She

said the proposal also included a sports field and a one-story community building adjacent to the Ravenswood Avenue and Middlefield Road intersection, 25 acres of landscaped publicly accessible open space, and new pedestrian and bicycle paths and connections through the site.

Ms. Sandmeier highlighted that the proposed circulation was private internal streets, an internal road to the three main residential buildings and parking garages, and an internal loop road to provide access to all nonresidential buildings, parking garages, surface parking areas, loading areas and for emergency vehicle access. She described the entry points for each of the building types. She said the requested entitlements included a General Plan Amendment, Zoning Ordinance Amendment, Rezoning, Development Agreement, Conditional Development Permit, architectural control for the new buildings, and a vesting tentative map to merge existing walks and create new parcels.

Ms. Sandmeier said topics for the Commission's consideration were the proposed land uses including site density and intensity, the site layout including building orientation and site access, conceptual architectural styles, design and layout of open space, parking locations and ratios, and proposed sustainability measures.

Questions of Staff: Commissioner DeCardy asked if this project would be reviewed standardly or whether it would have unique review.

Ms. Sandmeier said it would require a number of public hearings both at the City Council and Planning Commission as the environmental review progressed but it would be similar to what they saw with other projects.

Applicant Presentation: John McIntire, SRI, said that they were collaborating with a local firm Lane Partners to reimagine the site to serve both SRI's and the community's needs.

Mark Murray, Lane Partners, said their firm was Menlo Park based with an office about a half mile from the SRI campus. He said they had met with City staff and the Fire District, with community groups and had one on ones with dozens of residents. He said they held a series of open houses last summer before making their initial submittal in the fall. He said three of those were open to the general public and then they held a fourth specifically for the Burgess Classics neighborhood. He said those 32 homes shared a property line with the SRI site. He said that meeting was focused on the design particularly regarding the buffer zone between those properties and SRI. He said they received constructive feedback and were able to implement changes that responded to that.

Mr. Murray said one of their goals was to open up what currently was kind of a void in the center of town. He said the existing campus was large and for the most part had had security fencing around it. He said they envisioned as the Parkline name implied a new district characterized by open space, noting they planned to have 25 acres of publicly accessible green space. He said the site contained numerous mature heritage trees with some species over 100 years old that many community members had never seen. He said the goal was to preserve many of those heritage trees. He said another goal was to improve pedestrian and bicycle transportation through the area. He said regarding the commercial development component they were doing a one-to-one replacement for the existing 38 buildings. He said SRI would consolidate into three of the existing buildings and the other older 35 ones would be demolished and that same square footage would be consolidated into five new state of the art R&D buildings that were much more efficient and sustainable. He said another goal shared with the community was housing and that was proposed on 10 acres closest to

the downtown and amenities. He said they were proposing 400 units at variable affordability and were open to community feedback on what the appropriate amount and types of housing were.

Thomas Yee, principal architect, Studios Architecture, referred to the site analysis and noted in addition to Mr. Murray's comments that there was an electrical substation near the corner of Ravenswood and Laurel. He said the three buildings, P, S and T that SRI was planning to retain were intended to be included in the master plan effort. He said the existing parking made up about 50% of the entire site area with the building footprint another 23% so 70% of the existing site was hard surface. He said their goal was to convert that into a more amenable resource for the community. He said onsite there were about 1,370 existing trees, a great percentage of which were heritage trees, and that it had been important to incorporate the trees into the plan. He described how in removing the fence the site would be opened up and how it might connect with other parts of the city. He described the pedestrian circulation plan and how the City's bicycle path plan might be extended through the redeveloped campus. He said regarding vehicular circulation they were purposely trying to separate residential from the office R&D and to not have any office R&D traffic go onto Laurel. He described elements of the residential portion of the development that would provide separation and enhanced open space for neighboring residential areas. He said for the residential design they took cues from the Allied Arts neighborhood and the Davis Polk building and were proposing sort of the Mission style. He provided visual imagery of the proposed design starting with Laurel Street and then from the corner of Ravenswood and Laurel toward the east with an alternative pathway that was pedestrian oriented and an alternative bicycle pathway. He showed a view if walking down Ravenswood toward one of the entrances to the office R&D side with entrances clearly defined. He said they would create signals for the public to clearly show that this was a public trail and people were welcome into the site. He showed the proposed commons area of the office R&D site and existing heritage trees and the introduction of both passive and active uses that might be utilized both by tenants and the public. He showed lastly a view to the upper right of the playing field at Ravenswood and Middlefield.

Chair Doran opened for public comment.

Public Comment:

- Sue Connelly said she saw three potential problem areas noting she was a resident of the Burgess Classics community. She said her community's chief concern was the size of the proposed project. She said the elevations shown were only of the lower story and the apartment buildings would be five, five-story buildings and three five-story buildings plus the 20 townhomes. She referred to the office noting those were also five story buildings. She said the project meant the introduction of a great number of people who had not been there before and that would put pressure on the infrastructure and on water. She said safety was another chief concern as having the area fenced for many years had protected her community on one side. She noted they were having problems with the shared gate area with unhoused people. She said they had been trying for three years to resolve this humanely to obtain services and help and had been steadily rejected. She said she and her neighbors proposed that the number and the height of the office buildings be reduced. She said having fewer office buildings meant less of an impact on housing.
- Kalisha Webster, Housing Advocate for Housing Choices, said they were a nonprofit service provider helping people with developmental and other disabilities find and retain affordable housing throughout San Mateo County. She said she was calling in support of the proposed

project but noted the City's draft Housing Element and the development need of around 1700 affordable units, nearly half of which were for very low-income level. She urged the applicants to do more with the project to serve people of all income levels and abilities. She said the site was ideally situated near transit and the downtown that supported a walkable and more sustainable community. She encouraged the city and developer to take advantage of the opportunities at the site to increase heights and densities and to include more affordable homes at all income levels and abilities. She said they supported the request for a one-acre parcel to be donated to an affordable housing developer that could develop more affordable housing at deeper levels of affordability than that under the inclusionary housing ordinance. She said a nonprofit developer was better able to serve the needs of lower income residents for the provision of more onsite support services. She said as of December 2021, 77% of Menlo Park adults with developmental disabilities still lived in the family homes, not by choice, but due to the lack of deeply affordable housing available.

- Kelly Vavor said she was a former public high school teacher and now a community volunteer engaged. She said she felt optimistic about this proposed development and grateful for the thought that had gone into it. She said she was the mother of four children and the public open space and better bicycle and pedestrian routes really resonated with her. She said the project would generate significant new tax revenue that would benefit their elementary and high school districts. She said she supported the project.
- Michal Bortnik, Allied Arts, expressed appreciation for SRI and Lane Partners for bringing a great opportunity and being open to the community's feedback. He said he liked all the open and green space, the trees, the bicycle and walking paths, and the thoughtful layout to work with the surroundings. He said it was great that hundreds of housing units were within easy walking distance of so many things. He said his only request was that more housing be provided. He noted the unfortunate reality of homelessness in the community. He said he made more specific comments in his written letter to the Planning Commission. He said at the last Commission meeting a presentation was made on development in the Bayshore area and how much new development was happening there and how quickly. He said he hoped that a double standard would not be applied here as to what was acceptable versus what was acceptable in other parts of town.
- Anna Zara, Linfield Oaks, said she supported the Parkline project as it was an ideal location due to its proximity to transportation, shopping, entertainment and recreation. She said she also supported higher density apartment buildings as part of the project so that one of those buildings might be made available to people with intellectual, developmental and physical challenges. She said many in this vulnerable population in Menlo Park were forced to relocate away from family, friends and familiar surroundings due to the lack of affordable housing.
- Verle Aebi, Linfield Oaks, said for those who lived on Laurel Street the traffic impact of the proposed project in conjunction with the projects that would be occupied in the near future on El Camino Real, the Stanford project and the other project further north on El Camino Real could put quite a few additional cars on Laurel Street as it was commonly used to cut through. He said when they got to the environmental impact analysis the traffic needed to be analyzed in conjunction with the future grade separation project, which he was sure would happen someday. He said one of those options involved cutting off Alma Street, which would put quite an increase in traffic pressure on Laurel Street. He said he thought it was discussed last summer that there should be no car access from the project even from the residential portion onto Laurel Street and

the access should all be onto Ravenswood Avenue and Middlefield Road as those were much larger streets. He said years ago SRI maintained a “black house” with very toxic gases and chemicals that were used for some of the semiconductor work on campus and if that was the case today that was inconsistent with the density housing proposed.

- Bob MacDonald, Chair, ad hoc Church Committee for the Parkline project for the Menlo Park Christian Science Church on Ravenswood, and a Menlo Park resident said on behalf of his fellow church members attending this evening, that their church had been a neighbor and partner with SRI for over 60 years. He said in the late 1950s their church did a land swap with SRI that led to their current location surrounded by SRI on three sides. He said at that time a perpetual parking agreement was made that provided parking on SRI property for services, meetings and events at their church as well as some mutual traffic flow easements that ensured traffic flow and emergency vehicle access around the perimeter of their property and the ability to exit onto Middlefield Road. He said they had identified a significant issue for their church with the proposed plan, and were requesting that the playing field be moved so it was not adjacent to them to ensure the sanctity and serenity of their religious services, meetings and events. He said they were comfortable with continuing to have parking lots, parking structures, and office buildings adjacent to their property as that would create a buffer similar to what they had enjoyed for over 60 years. He said two of the three existing mutual traffic flow easements, Ravenswood 1 and Ravenswood 2, needed to remain in place to ensure that emergency vehicles were able to get to any location around the periphery of their property. He said they would also like to reach a mutually acceptable agreement regarding the Middlefield Road connection.
- Alex Ho, said he lived near the site. He said it was great that SRI was planning to redevelop the property and help solve the City’s housing shortfall. He said Lane Partners had incorporated much input from the neighbors. He said there were two issues he hoped might be addressed. He noted the egress from Burgess Drive and that it was specified during the presentation as a locked gate but he wondered about assurances that it would remain so in the future. He said the entry would drive additional commute traffic through the Linfield Oaks residential neighborhood and more importantly along Laurel Street, which was the Peninsula Bicycle Corridor and used by numerous children going back and forth to Encinal School. He said it was really important to look at traffic flows along Laurel Street. He asked what could be done to ensure that unhoused people did not start camping along the bicycle path and behind the Burgess Classics adjoining homes. He said currently people were sleeping on the sidewalks back there. He said also there was a history of shopping cars and garbage being left in the neighborhood, and the SRI back fence served as a homeless laundry every weekend. He asked that this be addressed through the project development.
- Emily Simonson, Laurel Street resident, said she supported the proposed project. She noted the thoughtful planning, additional housing, and the addition of better and safer ways to commute by bike and walking. She said as a mother of three young children that was lacking in this area. She said she appreciated the addition of green space as it was a rare opportunity to create more green space while creating more housing.
- Ken Chan said he was an organizer with the nonprofit Housing Leadership Council of San Mateo County. He said they worked with communities and their leaders to produce and preserve quality affordable homes. He expressed appreciation for SRI and their partners for the proposal. He said while the 400 proposed housing units would address the housing and jobs imbalance there was much more that could be done. He said they would like the project proponents to partner with an

affordable housing developer to provide the highest number of affordable homes at the deepest affordability levels that would include services and support for residents such as after school care, computer lab, playgrounds and other amenities.

- Adina Levin, resident, said she served on the Complete Streets Commission but was speaking for herself. She said the proposed development was near amenities and offered paths and green space for people to enjoy and go to and from without really having to use cars for numerous short local needs and potentially near jobs. She said a letter recently sent to the City Council observed that southern California cities were ahead of Menlo Park in developing draft Housing Elements and had had their Housing Elements rejected due to unviable sites and lack of affirmatively furthering fair housing. She said it was pointed out that Menlo Park was at risk of a similar situation. She said she agreed with others to have additional homes particularly deeply affordable homes to accommodate housing needs.
- Karen Grove, Housing Commission, said she was speaking for herself. She said she supported the project noting the bike and walking paths, preservation of the beautiful trees, and the housing. She agreed that the site could be used for more housing and highlighted the comments made by Housing Choices noting the relationship of homelessness to low income. She said she supported the property owner donating land to an affordable housing provider to partner with to provide homes and support services. She said the Housing Element was dependent upon affordable housing. She said she canvassed nearby residents of the project over the weekend and found that many were supportive of more housing, more affordable housing, more extremely low-income homes through the dedication of land and partnership with a nonprofit provider for this proposed project.
- Brittani Baxter, District 3 resident, said she lived within walking distance of the proposed project and loved the idea of opening up the site. She said her neighborhood was walkable and fantastic and she would love for more people to have that opportunity. She said she shared the enthusiasm for the future of this project and what this once in a generation opportunity meant for the city. She said concerns were expressed about traffic and parking and the site was perfectly located wherein a person would not actually need a car to get around. She suggested the site be set up with things in place to encourage people to choose more sustainable, ecofriendly, and congestion-reducing transit. She said using space for homes and people was preferable to using it for car storage. She said given the scale of the site there was a great opportunity to think about everybody in the community and help create that much needed difficult to create affordable housing especially for populations with specific needs. She noted the density of Bayshore projects with 100 units per acre and 40 units here per acre and suggested more could be done.
- Lynne Bramlett, District 3 resident on Mills Court, said she was speaking for herself noting she also led the disaster preparedness organization MPC Ready, which focused on Menlo Park and the unincorporated county islands within or adjacent to Menlo Park. She said their focus was disaster prepared neighborhoods as research showed in a disaster the most immediate source of help was the neighbors living closest. She said there were serious gaps in the local government's disaster preparedness. She said development projects represented opportunities to significantly improve disaster preparedness through the community amenity process. She said she agreed with another speaker's suggestion about the idea of putting underground water cisterns in new development. She said the city had less than one day's worth of stored water for emergency medical drinking and water was also essential for firefighting. She said fires were

secondary consequences of earthquakes and pointed to the Hetch Hetchy water delivery's vulnerability to disruption from an earthquake. She said the local fire district had very little water stored and she thought water storage was much more important than a juice bar or a playing field.

- Rob Willington, Menlo Park resident, said he and his family supported the project. He said the SRI campus land was currently underutilized and it was a great idea to redevelop it into a new neighborhood with open space and new housing.
- Steve Pang, Burgess Classics, said he was opposed to the open space concept of the proposal as it would lead to unhoused people using for encampments. He said over the past three years they had tried to work with SRI to handle the unhoused problem with their back gate and nothing had been done. He said their children used to be able to bike and walk around the neighborhood but it did not feel safe anymore. He said he opposed the bicycle path from Middlefield to Laurel for substantially the same reason. He said he opposed the number and location of housing units proposed as there was potential for a lot of traffic on Laurel Street. He said he had submitted additional comments in a written comment letter.
- Frank Contreras, Menlo Park resident, said he and his family had lived in the area for 40 years and he supported the project proposal. He said he would like his family to be able to stay in the area and affordable housing was needed. He said he agreed about the homelessness and encampments that those needed to be addressed. He said he agreed with housing being provided to special needs population as he thought everybody should have the opportunity to live in Menlo Park as it was such a great area.
- Will Connors, Willows resident, said he strongly supported the project particularly the bicycle and pedestrian access to schools and the downtown. He said his only critique was about the townhomes on Laurel Street as he would like to see more density in that area similar to the other residential units proposed at three to five stories as that was a better use of space near transit.
- Susan Stimson, Linfield Oaks, said she had attended some of the community input sessions and was pleased to see that some of what was recommended by residents had been incorporated. She said she would appreciate consideration of a closed wall for the parking structure to preserve privacy and block headlights at night as well as noise. She said she would like information on how security would be maintained throughout the green space so that the space might be utilized at night. She said that other large mixed-use projects in this area and their impacts on traffic and resources should be determined before adding another large development.
- Kenneth Mah, Burgess Classics, said they generally supported the proposal particularly the bicycle and pedestrian paths. He said they asked that the impact of the development and specifically the housing density be thoroughly considered. He said he and his wife used to bike to Stanford for five years and there was a safety issue at Laurel Street and Ravenswood Avenue as there was no dedicated bike lane. He said this project would worsen that safety issue. He said traffic in general would be increased on Laurel by the project. He said the current proposed designs might decrease the safety of both residents in his neighborhood and the Parkline residents trying to cross Laurel Street to get to Burgess Park. He asked the Commission to mitigate impact to Laurel Street by considering ingress and egress exclusively onto Ravenswood Avenue and Middlefield Road. He said they supported other issues needing attention including

gate access on Burgess Drive, ensuring the intended use of the green space and insuring provision of safety and security of that space.

- Katie Behroozi, Menlo Park, said she served on the Complete Streets Commission, but was speaking for herself. She said this project addressed three big needs. She said one was a direct response to increased density as they needed better connectivity for bikes and pedestrians between Middlefield Road and Laurel Street. She said the project also offered open space noting recent conversations in the city on how to save parks. She said with the housing crisis there were homeless people. She said for several years there had been discussion to have a shelter in the area which was not supported. She said in general this was a great area for denser housing. She supported keeping the proposed openness and ensuring safe crossings at Middlefield Road noting the Vintage Oaks intersection. She said she was a member of the Trinity Church and they had a shared parking agreement with SRI but were also joyfully anticipating the idea of new potential parishioners and members of the community.
- Peter (no last name given) said he met with Mark Murray and Lane Partners and they had listened to the community's opinions. He said he lived in the Classics and loved the quiet nature and the streets. He said his one concern was traffic as although the plan was to replace existing square footage one to one those were primarily currently unoccupied buildings with lower employee density. He said he understood the vision for open space but that had consequences. He said the connectivity to the ingress and egress made sense but did not really address safety issues of the ingress and egress along Burgess. He said there were dedicated bicycle lanes already along Linfield Drive and Ravenswood Avenue so they disagreed with having ingress and egress along Burgess. He said he wanted to make sure that they did not provide programming activities directly behind his and his neighbors' back yards between his community and the parking structure as that would encourage homeless encampments. He referred to comments on safety and unhoused people in the vicinity.
- Gail Gorton, Burgess Classics, said in general she supported the proposed project. She asked that the Commission be sensitive to a huge residential development dropped into a mixed residential area ranging from single family homes to apartment buildings, the tallest of which were only two-story. She said traffic impacts would be huge. She said Laurel Street, Ravenswood Avenue and Middlefield Road were two-lane roads already heavily congested. She said traffic was heavy on Laurel Street with Burgess Park there and she had seen near misses with bicycles from cars exiting the parking lots. She said they had to consider how the schools would absorb additional population and the impacts to natural resources. She said she appreciated the inclusion of a playing field as the fields at Burgess were at maximum usage. She asked how the Parkline playing field would be operated. She said she would prefer to see affordable homes for purchase on the site. She said it was important to provide affordable rental housing too. She said she would like the number of affordable units to remain the same as proposed but for the overall number housing units to be reduced.

Chair Doran closed public comment

Commission Comment: Chair Doran noted the time was 10:24 p.m. and that they would need to stop at 11 p.m. unless they voted to extend beyond that time.

Chair Doran said the first topic staff requested input on was land use. He said overall he thought the project was great and very thoughtful, and the land use was appropriate. He said he liked the

residential uses closest to the train station, the playing field close to Menlo Atherton (MA) because there was not a lot of parkland around MA. He said the application included a request for a zoning ordinance amendment and rezoning so everything was on the table. He said it was a very large site and a great opportunity close to transit. He said he would encourage more housing and was amenable to higher density for housing. He said the proposed site layout seemed respectful to neighbors and he liked the townhouses as a bridge to existing residential neighbors. He said he liked the three stories nearer the front edge of the property and the five stories further behind. He said he would support higher densities especially if they were behind the five stories so height was gradual. He said also he would support more land being used for residential than for office. He said he appreciated the preservation of the heritage trees. He said access seemed well thought out. He said he heard the objections to residential access on Laurel Street but they needed residential development and the applicants had done a good job of keeping at least the commercial access off Laurel Street. He said regarding conceptual architectural styles that he believed it was very appropriate noting it was in early stages but he thought Mission style seemed appropriate. He said the design layout of the open space looked good. He said regarding parking locations and ratios that it was better than what was there now. He said regarding proposed sustainability measures it was still early in the design but he appreciated the LEED gold goal. He said he was generally supportive and would like to see more housing.

Commissioner Kennedy said generally she was supportive of the proposed project. She said she agreed with Chair Doran's comments on increased density and that significantly increasing density would be appropriate for this project. She said they had seen a number of letters contemplating what it would look like to take an acre and partner with an affordable housing developer to provide meaningful affordable housing. She said that might help them to embrace what was starting to happen across both Santa Clara and San Mateo Counties to move toward the attempted zero of homelessness. She said this site was their hope for putting the right amount of housing at the right densities downtown where it belonged.

Commissioner Andrew Barnes asked if the project site was outside of the Downtown / El Camino Real Specific Plan area (Specific Plan) and if so, what community amenities program applied to it. Planner Sandmeier said the site was outside of the Specific Plan and there was no specific community amenities program. She said the applicant was requesting a new general plan designation and new zoning ordinance amendment that the property would be rezoned to. She said as part of those there could be an exchange for some type of community amenity that was negotiated.

Commissioner Barnes said he liked the idea of a sports field but that was not a community amenity in the formal sense. He asked if they were considering have Parks and Recreation program the use of the field. Mr. Murray said they were open to how the field would be programmed. He said in a sense it was a community amenity as that sports field with an adjacent park area and a community building was really a community use rather than an amenity base for their office occupants or residents. He said it was meant for AYSO or other recreational leagues. He said hopefully it could allow for office occupants use as well but they intended it to be truly a community sports field. He said that it was early on and they were open to ideas on management of it.

Commissioner Riggs asked what the approximate occupied density of SRI was currently. Planner Sandmeier said she did not have that information. Mr. Murray said they did not either as occupancy had been significantly disrupted by the pandemic. He said SRI's intent was to consolidate into those three existing buildings totaling about 280,000 square feet but he thought currently employees were

spread out in much more space. Commissioner Riggs said they would have to look at something historic then like a 2019 Google map or something like that. He said his question related to traffic and noted the Meta campus with three office clusters of roughly 500,000 square feet each and the amount of traffic going in and out of those clusters. He said over the 20 years he had regularly traveled down Ringwood and Ravenswood he had never seen even a fraction of that traffic in the SRI parking lots. He asked if that was accurate. Mr. Murray said the last Conditional Use Permit, approved around 2004, showed a headcount cap of about 3,200 people but that had declined significantly due to Covid.

Commissioner Riggs said the public they heard from were supportive of the project because of housing, BMR units and opportunities to create more affordable housing and for special needs populations. He said however the project would have approximately 1.4 million square feet of office space and 400 residential units. He said by comparison Willow Village had over 1700 proposed housing units for roughly the same amount of office use, and that project was providing significantly less housing than the additional workers generated by it. He said the proposed Parkline project was not a housing development project. He said that did not mean he was opposed to it unless it was a housing project – he just thought it should be clear what the project was. He said one letter from the public asked how many workers were expected and how that related to housing / jobs imbalance and traffic. He said five story office buildings here would indicate a higher density. He said there were three parking structures proposed so he expected there was some concept of what kind of density was expected. He said information on that would be expected at the next session.

Commissioner Riggs said regarding the proposed land use, intensity and density, that the most notable thing was this was not a jobs and housing imbalance correcting project. He said the question would be how much it would contribute to the imbalance. He said that this might not be the project that needed to address the imbalance, just that it was something to be noted. He said since the project was predominantly an office space project, he thought it made sense to put the office space as close to the train station as possible. He suggested that office space users might take advantage of transportation much better and more immediately than residents. He said that he did not really have any comments on the site access, design, layout of open space, parking locations or ratios as theoretically those would be rethought to place office closer to transit. He said regarding conceptual architectural styles that they were taking the correct approach, and when that style was done well, it was really exciting.

Commissioner Harris said this was a unique opportunity for the City to transform an aging property with limited use to an open and mixed-use neighborhood. She said with so much community interest there were of course different ideas about what was wanted. She complimented the applicants on the 25 acres of publicly available green space, the retention of heritage trees and locating buildings around them, only the residential entrance on Laurel Street, listening to the community, and the pedestrian / bicycle paths and connectivity. She agreed they could not go wrong with the attractive Mission style architecture and was supportive that the five stories were set back from the three stories, and the 50-foot setback between the site buildings and Burgess Classics. She said her areas of concern included traffic impacts and mitigation. She said regarding a Transportation Demand Management plan (TDM) they had indicated a shuttle to Caltrain and suggested that might be extended to go downtown, maybe circle around to Safeway and then back again. She said she would like the TDM to go even further than that. She said they had had success on other projects with trip caps so she would like to see that. She said she would like Menlo Park to eliminate minimum parking requirements entirely toward significantly reducing the number of people driving and parking on this site as it was close to Caltrain and El Camino Real buses, and close to

downtown amenities. She said the proposed three large parking structures took up too much land that could be used for housing. She asked if they had considered putting the parking underground, which would allow room for additional residences and reduce parking.

Mr. Murray said they considered it and a big drawback was the digging as that increased construction timing by nearly two times, and involved environmental impact and construction noise impact with trucks hauling dirt away. He said while the end result made the parking sort of disappear, it obviously was very costly. He said with this site and being able to provide 25 acres of open space they did not think it was necessary to do underground parking.

Commissioner Harris said the difference between the number of office workers for R&D versus regular office use was a pretty big delta, which might mean a greater parking need. She said when the project came back, she would like information on employee count, to see the parking reduced or ideas of how they might do that. She said to let the Commission know if the city would need to help them with parking reduction. She said they might consider charging for parking both the residents with unbundled parking and also the office workers or give rebates to those who did not drive to work. She said her second suggestion was to increase the number of housing units noting if parking was reduced that they would have more space. She referred to the idea of dedicating an acre to a nonprofit housing group to get more density and housing for people of all abilities and deeply affordable housing, and noted that deeply affordable housing residents were less likely to need cars and that would help the parking. She said as they got closer to a project submittal that she would like to review the recreation site to understand what made the most sense, whether it was really for the community, whether it was truly a recreational field and if so what type.

Chair Doran noted it was 10:59 p.m. and two Commissioners were requesting to speak. He proposed taking a vote on extending the meeting time in a finite amount, and suggested 20 minutes acknowledging that some Commissioners had severe time constraints.

ACTION: M/S (Harris/Doran) to extend the meeting to 11:20 p.m.; passes 7-0.

Commissioner Tate said her biggest concern was the project would not provide enough housing. She said she liked the idea of donating not just one but a couple of acres to a nonprofit or low-income housing developer for affordable housing development. She said additionally she was concerned about the field near the existing church, as she thought the church needed quiet for their activities. She suggested the project team as a good neighbor might consider moving the field or to come to a compromise with the church. She said her assumption was there would be some sort of security to ensure the grounds were safe, but she had not heard that addressed in response to community comments.

Mr. Murray said the 25 acres would be privately owned. He said it was something they were trying to create as an amenity and not to burden the neighbors or the city. He said he envisioned that they would privately develop and maintain the space and there would be some kind of public access license or easement to use it as a park during certain hours. He said they were open to ideas. He said in terms of safety late at night and early morning, as this was private property, they would be responsible for securing it. He said they would have every incentive to secure it as the property owner for the benefit of the residents who lived there. He said that was something they were very confident they could manage.

Commissioner Tate asked if they had given consideration to donating some of the land. Mr. Murray said they were speaking with different groups and others about how to generate more affordable housing. He said the idea had been discussed and they were open to it.

Commissioner Barnes said a couple of areas could use more thought. He said as he conceptualized the 25 acres of green space, he saw that was good for the site and for instance the office users and residents. He said the common area in the middle was underutilizing the site. He noted the dearth of playing fields in the area and suggested two fields on the site that were neither a park or a tenant feature amenity. He said he had no use for in lieu fees but a use for an accretive, material and tangible community benefit. He said he supported parceling out some of the property, an acre or so, for a deeply affordable housing project. He referred to traffic impacts from the project notably to the Willow Road, Middlefield Road and Woodland intersection. He said moving forward he would want discussion on what impacts the project would have transportation and transit infrastructure.

Commissioner Tate said for the record that her request was for one or two acres donated to a low-income housing group but that it was not in lieu of the BMR units the project was providing. She said that integrated housing was better than when it was just in one building but she understood the need for the latter, and they had the property size to make it happen.

Commissioner DeCardy said he appreciated community interest in the project. He said what the applicants were trying to do and the direction they were going could work very well and there were challenging things to sort out. He said two things were not working and those needed to work in a fundamentally different way. He said one was affordable housing. He said with 400 units that 15% BMR would be about 60 units of affordable housing. He said that was one unit of affordable housing per acre on this property. He said the simplest thing would be to set a goal for affordable housing and then they could sort out what that required but the goal needed to be significantly higher than 60 units. He said the second was the congestion that would come with attracting so many people to this area and what to do about that. He said a parking garage would not get them out of the congestion problem. He said the project team proposed shuttles. He said he had the opportunity to have a walkthrough with Mr. Murray and that was helpful. He said an electric shuttle that went from the site down to Caltrain was a beginning point. He said working with City Council they could open this up and as Commissioner Harris had commented, take the opportunity to look across the community and finally get connectivity from Bayfront to the downtown that would get people out of cars, work for this development and act as a catalyst to make that work for the rest of the community. He said the city had major developments from the Bayfront, along Willow Road and downtown not to mention what might come out of the Life Sciences District and the USGS site. He said now was the time as a community to address connecting all that with something other than single occupancy vehicles. He said it was not this project's responsibility to own this but it was their responsibility to catalyze it to help make their project work.

Commissioner Riggs said he supported Commissioner DeCardy's call for action for transit from Bayfront, past SRI and to the Caltrain station and that would require the City Council to do something more locally. He said he was surprised the challenging Ravenswood and Ringwood intersection had not been mentioned as here was an opportunity to bring Ravenwood around the church property and align with Ringwood. He said the current intersection was dangerous for the many pedestrians coming from the high school, particularly dangerous for bicyclists going southbound on Middlefield Road and crossing that loop connector. He said it was an annoyance to everyone who had to navigate those double traffic lights and it was time to fix it.

I. Adjournment

Chair Doran adjourned the meeting at 11:20 p.m.

Staff Liaison: Corinna Sandmeier, Acting Principal Planner

Recording Secretary: Brenda Bennett

Approved by the Planning Commission on August 29, 2022

From: [David Fencil](#)
To: [Sandmeier, Corinna D](#)
Subject: Parkline
Date: Tuesday, December 6, 2022 2:24:23 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

I was looking at the map of the proposed Parkline development...the map was very small but there was green at the corner of Ravenswood and Middlefield...between the church and Middlefield...if that is a park, my experience with the police department would predict a big problem with kids hanging around even during school days and other kids hanging out waiting for the HS kids...
Dominick (650) 269-6279

Sent from [Mail](#) for Windows

Additional Comments Received after Staff Report Publication

From: [Marlene Santoyo](#)
To: [Planning Commission](#)
Subject: Agenda G1
Date: Monday, January 23, 2023 4:57:53 PM
Attachments: [M2G Letter - Agenda G1.pdf](#)

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Members of the Menlo Park Planning Commission,

38 members of your community have signed the following letter about the SRI proposal you will be studying tonight. In addition, twelve neighbors have written a personal note, which I encourage you to read. You will find the full letter and notes attached below.

Please consider the input from these residents who support the increased number of homes and increased affordability of the current proposal and ask you to go even further towards planning for housing equity and sustainability in Menlo Park.

Thank you for your consideration,
Marlene Santoyo

--

Marlene Santoyo | Organizer | (she/hers)
Menlo Together
510-945-7490
<https://menlotogether.org>



January 23, 2023

Members of the Menlo Park Planning Commission,

38 members of your community have signed the following letter about the SRI proposal you will be studying tonight. In addition, twelve neighbors have written a personal note, which I encourage you to read. You will find the notes beneath the letter.

Please consider the input from these residents who support the increased number of homes and increased affordability of the current proposal and ask you to go even further towards planning for housing equity and sustainability in Menlo Park.

Regards,

Marlene Santoyo and The Menlo Together Team

Members of the Menlo Park Planning Commission,

We, and the residents listed below, believe that our city can and must build more homes across all levels of affordability, especially near transit and downtown services, for a variety of household sizes and for people of all abilities.

We are glad to see that the Parkline proposal has increased the number of homes to 550, including a much needed and appreciated 100 deeply affordable homes for people of all abilities. We are pleased that the EIR will study up to 600 homes, and hope that the plans will grow to include that number of homes. Thank you for these important changes to the proposal.

We encourage the city and the developer to do even more.

A sufficient and diverse housing supply is required for a sustainable, welcoming and thriving community. Additionally, state law requires that we meet our fair share of and affirmatively further fair housing by planning for affordable homes in high resource areas. The State will make sure that we achieve our goals - willingly and through our own planning, or unwillingly through by-right development.

To that end, we:

- Celebrate the plan to dedicate an acre of land within the development to be donated to a non-profit housing developer and developed to meet our most pressing needs: deeply affordable housing for families and people of all abilities.

- Support increasing the number of homes beyond 550, and increasing the inclusionary Below Market Rate (BMR) units from 15% to 20%. We encourage reimagining the proposal to produce 100s more homes on this once-in-many-generations opportunity site that is walking distance from downtown services, transit, recreation and schools.
- Support reducing the amount of parking to attract non-driving residents and reduce local traffic, and to leave more space for community-enhancing amenities.

No matter where you begin, success in life starts at home for all ages and all people. When we have safe, secure places to live, parents earn more, kids learn better, health and well being improve, and our community is strengthened because it now has the building blocks needed to thrive.

Let's take full advantage of the Parkline project to build a strong community of people and families of all incomes and abilities who thrive.

Thank you.

Sincerely,

1. Anna Zara (Menlo Park)

I would also like to add that lately most of the new large housing developments in Menlo Park have been clustered in the Belle Haven and Linfield Oaks neighborhoods. It is time to look at adding housing to other Menlo Park neighborhoods as well and to even out the new housing units between the Menlo Park City School District and the Las Lomas School District.

Thank you for your coordination.

Anna

2. Michal Bortnik (Menlo Park)

3. Bridgit Louie (Menlo Park)

4. Beanie Zollweg (Menlo Park)

5. Caroline Beckman (Palo Alto)

6. Caroline Kory (Menlo Park)

7. Connor Gilbert (Menlo Park)

8. Dayna Schocke (Menlo Park)

9. Dennis Irwin (Menlo Park)

I want the benefits of living in a more diverse community. The more affordable housing there is in the Parkline project, the more we'll be going in that direction!

10. Hannah Gilbert (Menlo Park)

11. Julian Cortella (Menlo Park)

More housing near downtown is great! Please support the Parkline proposal with the increased number of homes.

12. Jessica Clark (Menlo Park)

13. Jennifer Johnson (Menlo Park)

14. Joseph Grass (Menlo Park)

15. JP Garcia (Menlo Park)

16. Julie Shanson (Menlo Park)

More housing at all income levels near transit and schools helps the whole town.

17. Karen Grove (Menlo Park)

I got interested in local housing issues as a way to "act locally" to achieve racial justice. I know others are interested in housing as a way to minimize our climate impact by reducing local traffic and emissions from people commuting to work in Menlo Park or nearby, because they cannot afford to live here (or near).

I support the increase in number of units, and the dedication of land to a partner who will develop 100 units of homes for those most impacted by housing insecurity.

But this proposal could be SO much more and go a lot further towards achieving fair housing and climate action in our city.

We should be looking at Willow Village - a 59 acre site (as compared to this 64 acre site) as a model. Willow Village is going to produce over 1700 homes including extremely low income affordable senior homes through a partnership similar to the one being contemplated for the SRI site.

What's good near Belle Haven would be even better at the SRI site, which is an easy walk from Caltrain, El Camino busses, downtown, parks, schools and restaurants.

This is a once in more than a generation opportunity to share a vibrant, equitable and sustainable future for Menlo Park.

18. Katie Behroozi (Menlo Park)

I'm enthusiastic about the redevelopment of this centrally located under-utilized land – but I'd like to see less parking, less office space, more housing at all income levels, well-integrated bike-ped

facilities and open space, and public access to all on-site amenities so that adjacent neighbors can use not only the open spaces but also whatever cafes and fitness facilities are developed (I don't think cities benefit from the Google/Meta in-house private amenities that have become the norm.)

19. Katherine Dumont (Menlo Park)

I live just one-half mile from the Parkline site, so I'm very interested in this project. In several meetings with the developer, I've been very impressed by their willingness to build housing for a range of needs and abilities. We should jump at this chance to provide more diverse and affordable housing in this location, which is so close to transit, Burgess Park, the community center, and to downtown shops and services.

This is a great opportunity to reverse the trend of pushing people to live further and further away from their jobs. It's hard on individuals and families, and it's hard on the environment. It's going to cost us all a lot more in the future if we don't take bold steps now.

Thank you for considering more housing on the Parkline site so we can move forward in a more sustainable and equitable way.

20. Lesley Feldman (Menlo Park)

21. Lorri Holzberg (Menlo Park)

22. Mary Kelly (Menlo Park)

I believe in increasing density and affordability!

We all benefit from the diversity!

23. Michael Arruza (Menlo Park)

24. Marijane Leonard (Menlo Park)

25. Margarita Mendez (Menlo Park)

26. Marlene Santoyo (Newark)

27. Nathan Rolander (Menlo Park)

I support this petition to build new homes

28. Nina Wouk (Menlo Park)

29. Jennifer Michel (Menlo Park)

Dear Chair, Vice Chair, Commissioners, Staff, Neighbors,

Thank you for listening to us and granting us much needed vibrancy in our City! I support the Parkline project and applaud the applicant for increasing housing.

Further I support:

Housing at all income levels keeps our community resilient, inclusive, and thriving. Do you have children? Where are they going to live in a few years? Have you been housing unstable? Do you have issues obtaining and retaining labor?

There is a cool recent batch of data from Arlington VA who saw a net decrease in traffic despite adding more units to the city, because of how the units are smartly clustered around transit. We will not meet our Climate Action Plan goals without reducing the number of miles people commute to work in or near Menlo Park, simply because they cannot afford to live here. New York Times also came out with a map of your carbon use mapped by neighborhoods showing that those of us working near where we live, who live within their means, generate climate stability. It's no longer cool or something to boast if you are an empty nester in an SFR on a 10,000 SF lot. Parkline gives us much needed dignity to get out of the car and use much less carbon.

I support local businesses and want them to have a robust, local workforce who are able to thrive and contribute to the community in which they work. Parkline would give a much needed infusion of new mouths to feed and serve. Our local businesses will see a much needed economic lift. Because Parkline is walking distance to downtown and major transportation infrastructure, the residents will also thrive! The current neighbors will feel welcome to walk and get out of their vehicles! What a win win win!!

I value equity and welcome people who have been discriminated against into all neighborhoods, parks and our schools.

Dedicating land in this prime location to a non-profit affordable housing developer is a great way to meet hard-to-meet housing needs: seniors, large families, single-women headed households, people with developmental and physical disabilities. I've mentioned before that I can't get labor to service my buildings because of the overly burdensome commute, but this project would help bridge that gap!

This site will be a strong applicant for federal, state, and county funds because of its proximity to transit and services.

The developer has shown that they are willing and open to building more housing for people of all incomes and abilities. We should take advantage of this opportunity and work with them.

Additionally, we are sending a message to all parties and stakeholders that our residents, workforce, families, and retirees all are incredibly valued and we stand with them, us, to meet the moment with our various housing needs. I'm proud to call Menlo my home and the City where we raise our son.

With all my love,

Jenny Michel from the Coleman Place Neighborhood Block

30. Frances Kieschnick (Menlo Park)

31. Sandy Sloan (Menlo Park)

We need more affordable homes west of Middlefield.

Thank you!

32. Sara Matlin (Redwood City)

33. Sarah Zollweg (Menlo Park)

34. Sharika Thiranagama (Menlo Park)

35. Sarah Brophy (Menlo Park)

This is the type of project that Menlo Park City council should encourage.

36. Tim Clark (Portola Valley)

37. Tom Kabat (Menlo Park)

38. Vikas Maturi (San Mateo)

From: [M. ADHAM](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review
Date: Monday, January 23, 2023 1:39:45 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning commission members:

We have been residents of Linfield Oaks for 30 years, and raised our family here. Please do not approve the proposed changes in the density and size of the SRI/Parkline development as it is unfair for our neighborhood to disproportionately bear the impact of the initial 400 units, not to mention increasing it to 600 units. It's also not fair as we have taken on the additional housing of the Morgan Lane Development that was completed in 2008.

Taking the already extremely large total housing number from 400 units of the SRI Development to 600 jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and increased traffic congestion in this area. Further:

- The apartment complex and townhome driveway should be removed from residential streets.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Residential streets leading to the new development are not used. The office traffic can be significantly reduced if Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.
- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage.
- Include the emergency water storage tank , because 1) there

is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development.

Omar and Mary Adham
157 Linfield Dr
Menlo Park, CA. 94025

Sent from my iPhone

From: [larry anderson](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]
Date: Monday, January 23, 2023 1:08:49 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area.

Larry Anderson
321 Linfield Place

From: [Anna Hall](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan
Date: Monday, January 23, 2023 3:12:24 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning Commission Members

Adding 400 housing unit in Menlo Park was intended by the State for more housing for people who need to live and work in Menlo Park. On the other hand, adding 200 additional units is questionable, especially if many of those units are earmarked for Office Space. Most people living near SRI know that 400 new units will seriously impact traffic, parking, infrastructure, and quality of life. It will have deleterious effects on students, teachers, and staff who work at Menlo-Atherton high school. Thus, plans to build numerous units so close to M-A should include input by school administration.

Most important, the Planning Commission must not ignore or minimize the impact that tens of thousands of recent job cuts in the Computer Sector in this area will create less need, if any, for more Office Space. Looking around Downtown Palo Alto, or El Camino Blvd., one sees countless signs for empty Office Space.

A responsible Planning Commission will need to go back to the drawing board and re-evaluate the SRI/Parkline Plan before proceeding any further. Failure to do so would indicate that members of the Planning Commission are not beholden to the residents of Menlo Park, but to Real Estate Developers.

Anna Hall
212 Gilbert Avenue
Menlo Park, CA 94025

From: [Judith Asher](#)
To: [Planning Commission](#)
Subject: SRI/Parkline Plan Review - requested changes
Date: Monday, January 23, 2023 7:56:03 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my next door neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

- The **project should net out to provide the state-mandated housing number of 400**, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on the Ravenswood driveways if

Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Quoting from my next door neighbor, Sue Connelly:

" SRI /ParkLine will have highly profitable housing and office revenue annually, but the costs will be borne by the taxpayers.

Based on current Menlo Park office rates, the office project stands to generate \$50M per year. This doesn't include ANY of the apartment rentals, for which most will be at very high rents (see the current rents for the new SpringLine apartments!). There will be some city revenue, but since SRI is a non-profit, this massive development will not offset many of the costs residents must pay for infrastructure (schools, police, fire, water and roads). Yet it will create a significant reduction in our quality of life (and possibly home values), bike/pedestrian safety for school children and residents, and increasing the state-mandated affordable housing units even more.

We need to require that any new office development provides/includes the affordable housing that the office spaces

and employee densities will be required to be built in Menlo Park."

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development .

Judith Saltzman Asher
530 Barron Street
Menlo Park, CA 94025

From: [Christopher Baldwin](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]Planning commission meeting Jan 23, 2023 for the SRI/ParkLane Plan Study Session
Date: Monday, January 23, 2023 3:14:09 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear commission,

As a resident of Menlo Park, I am providing my comments regarding the **SRI/ParkLine Plan Study Session** which is being held tonight to be captured in the public record.

1. **The SRI/ParkLine project should net out to provide the state-mandated housing.**
2. **Reduce the amount of office to comply with the current C1 zoning.**
3. **Remove the apartment complex driveway on Laurel Street to protect bike safety for school children and pedestrians.**
4. **Use the (currently gated) SRI driveway onto Middlefield.**
5. **Increase parking for renters and employees.**
6. **Provide underground parking for the housing units and for the offices.**
7. **Include the emergency water storage tank.**

Thank you.

Christopher Baldwin
345 Claremont Way, Menlo Park, Ca 94025

From: [Susan Bryan](#)
To: [Planning Commission](#)
Subject: Parkline Study session Jan 23, 2023
Date: Monday, January 23, 2023 10:58:10 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning Commissioners: I am writing to remind you that members of Trinity Church, Menlo Park are neighbors of the new Parkline Development. Last year, we submitted the signature of some 30 church members asking for the maximum amount of affordable market rate housing to be included in the developer's plans. That means we would be in favor of the extra 50 units being proposed at the study session tonight.

Thank you - Susan Bryan, church member, Trinity Church, 330 Ravenswood Avenue, Menlo Park

From: [Daryl Camarillo](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/ParkLine project request
Date: Monday, January 23, 2023 7:48:54 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Corrina and Planning Commission,

As a resident of The Classics at Burgess, we are requesting a third level in the EIR scope to review a lower-impact, smaller development option -- especially since the proposed plan **INCREASES the affordable housing deficit.**

In this smaller-scope project, we request the EIR to measure the following:

1. The SRI/ParkLine project should net out to provide the state-mandated housing that the amount of office planned will require Menlo Park to build.
 - o Reduce the amount of office to comply with the current C1 zoning. The planned office use will actually **NEGATIVELY** impact the affordable housing deficit and result in increasing the deficit due to the proposed office use. The risk of the projected lab use FAR being changed to higher employee densities per 1000 square feet will further increase the affordable housing deficit. In short, the office size and density is creating a bigger housing problem.
 - o Keep the housing at 400 apartments, but have 25% of them be BMR (Below Market Rate) units, so the separate one-acre donation being considered for an affordable housing development will not be required.
2. Study the option of removing the apartment complex driveway onto Laurel to preserve bike safety for school children and pedestrians and to reduce the existing gridlock on Laurel Street. The smaller driveway for the townhome residents can remain as indicated in the current plan.
3. Measure the use of the (currently gated) SRI driveway onto Middlefield to redirect traffic flow as a viable alternative to the removal of the Laurel Street for the apartment buildings. The office traffic can be significantly reduced on the Ravenswood driveways if the Middlefield driveway opens (it will reduce Ravenswood gridlock to/from Middlefield and El Camino) and direct commuter traffic closer to Willow and Highway 101.
4. Increase parking for renters and employees since inadequate parking forces apartment renters, visitors and employees to clog residential streets with traffic while looking for parking and for taking up limited residential parking
(Note: In the 12/12 Planning Commission meeting on the SRI EIR, some commissioners wanted to reduce the proposed parking to force renters/employees to use public transit. But the representative from the firm that will conduct the EIR said that studies showed that reducing parking spaces did NOT reduce cars or numbers of car trips. It just pushed drivers to surrounding residential areas to take street parking, which added traffic as well. There were no reductions in Greenhouse Emissions or in number of car trips.)
5. Provide underground parking for the housing units and for the offices to reduce the overall height of the project (notably to reduce the height of the 3-story parking garage behind the Barron Street homes) and the potential of five six-story apartment

buildings if the project is approved for the 600 total housing unit option being reviewed.

6. Include the emergency water storage tank since there is no emergency water for residents and workers west of El Camino (per the latest water report) which said the emergency well in the city yard is not online yet. The risk of toxic contamination of the city yard emergency well makes it a problem since the city's gas tanks and city yard with other toxic substances (oil, pesticides, etc.) are above it could leak into the groundwater, especially in the expected large earthquake event at some point in the future.

Thank you for your help in getting this lower-impact option included in the EIR so we have a solid comparative analysis of the other two scenarios, especially the much larger scope option, that are being proposed in the EIR scope.

Daryl Camarillo/ Yolanda Font
525 Barron Street
Menlo Park, CA 94025
650-269-1493

From: [Angel Chen](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/ParkLine Building Project - Impact on Classics of Burgess Neighborhood
Date: Monday, January 23, 2023 1:01:26 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Attention:
Corrina Sandmeier -- Acting Principal Planner
and the Menlo Park Planning Commission

Dear Corrina and Planning Commission,

As a resident of The Classics at Burgess, we are requesting a **lower-impact, smaller development** -- especially since the proposed plan actually **INCREASES the affordable housing deficit**.

In this smaller-scale project, we request the following:

1. **The SRI/ParkLine project should net out to provide the state-mandated housing** that the amount of office planned will require Menlo Park to build.
 - o **Reduce the amount of office to comply with the current C1 zoning.**The planned office use will actually **NEGATIVELY** impact the affordable housing deficit and result in increasing the deficit due to the proposed office use. The risk of the projected lab use FAR being changed to higher employee densities per 1000 square feet will further increase the affordable housing deficit. In short, the office size and density is creating a bigger housing problem.
 - o **Keep the housing at 400 apartments**, but have 25% of them be BMR (Below Market Rate) units, so the separate one-acre donation being considered for an affordable housing development will not be required.
2. **Remove the apartment complex driveway onto Laurel** to preserve bike safety for school children and pedestrians and to reduce the existing gridlock on Laurel Street. The smaller driveway for the townhome residents can remain as indicated in the current plan.
3. Instead of the Laurel Street driveway, **use the (currently gated) SRI driveway onto Middlefield to redirect traffic flow** as a viable alternative to the removal of the Laurel Street for the apartment buildings. The office traffic can be significantly reduced on the Ravenswood driveways if the Middlefield driveway opens (it will reduce Ravenswood gridlock to/from Middlefield and El Camino) and direct commuter traffic closer to Willow and Highway 101.
4. **Increase parking for renters and employees** since inadequate parking forces apartment renters, visitors and employees to clog residential streets with traffic while looking for parking and for taking up limited residential parking.
(Note: In the 12/12 Planning Commission meeting on the SRI EIR, some commissioners wanted to reduce the proposed parking to force renters/employees to use public transit. But the representative from the firm that will conduct the EIR said that studies showed that reducing parking spaces did NOT reduce cars or numbers of car trips. It just pushed drivers to surrounding residential areas to take street parking, which added traffic as well. There were no

reductions in Greenhouse Emissions or in number of car trips.)

5. **Provide underground parking for the housing units and for the offices** to reduce the overall height of the project (notably to reduce the height of the 3-story parking garage behind the Barron Street homes) and the potential of five six-story apartment buildings if the project is approved for the 600 total housing unit option being considered.

6. **Include the emergency water storage tank** since there is no emergency water for residents and workers west of El Camino (per the latest water report) which stated that the emergency well in the City Yard is not online yet. The risk of toxic contamination of the City Yard emergency well makes it a problem since the city's gas tanks and city yard with other toxic substances (oil, pesticides, etc.) are above it and risk leaking into the groundwater, especially in the expected large earthquake event at some point in the future.

Thank you for your help in seriously considering this lower-impact development solution.

Best,
Angel Chen

From: [Sue Connelly](#)
To: [Planning Commission](#); [PlanningDept](#); [Sandmeier, Corinna D](#)
Subject: [Sent to Planning]Request to reduce the office and housing for SRI/ParkLine
Date: Monday, January 23, 2023 4:45:58 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Ms. Sandmeier and Planning Commissioners,

I'm a Board Member and resident of The Classics at Burgess HOA. I would like to reiterate the requests I submitted for the EIR scoping deadline on January 9th regarding concerns about the massive size of the SRI/ParkLine development.

We are requesting a smaller development that reduces the negative impact of a development of this large scale -- especially since the plan **INCREASES the affordable housing deficit with the quantity of office space and density proposed.**

In this smaller-scale project, the following is requested:

1. **The SRI/ParkLine project should net out** to provide the affordable housing that the amount of offices and workers that the State mandates Menlo Park to build to accommodate the number of new workers.
 - o **Reduce the amount of office space** to comply with the current C1 zoning since the planned office use will actually **NEGATIVELY impact the affordable housing shortage** and result in increasing the number of affordable housing units that will need to be met by yet another development project. The risk of the projected lab use FAR being changed to higher employee densities per 1000 square feet will further increase the affordable housing deficit. Currently, it appears SRI has 1,000 employees on the Menlo Park campus. Even at the lab and biotech use of 4 employees per 1,000 square feet raises the number of workers on the site to 4,000. In short, the office size and density is creating a bigger housing problem. If the office FAR changes to even denser use for start ups and high tech companies, the density of workers per 1,000 square feet will go up significantly, and drive the deficit even deeper.
 - o **Keep the housing at 400 units**, but have 25% of them be BMR (Below Market Rate) units, so the separate one-acre donation considered for an affordable housing development will not be required and the community open space for a soccer field or other public use will be preserved. Also, with a reduction in office space, the housing can be reduced in height and density and spread out more on the SRI campus. With the possibility of five 6-story apartment buildings, in addition to the five 3-story buildings, this height will be 300% higher than any of the surrounding apartments and homes. Also, the apartment complex does not currently have a play area or community area, or pool. Burgess Park across the street is already overbooked and unavailable to soccer and baseball teams. How will we accommodate so many new residents who are in high-density housing without an open space?
2. **Remove the apartment complex driveway onto Laurel** to preserve bike safety for school children and pedestrians and to reduce the existing gridlock on Laurel Street. The smaller driveway for the townhome residents can remain as indicated in the current plan.

3. **Use the currently gated SRI driveway onto Middlefield** to redirect traffic flow as a viable alternative to the removal of the Laurel Street for the apartment buildings. The office traffic can be significantly reduced on the SRI/ParkLine office and apartment driveways on Ravenswood if the Middlefield driveway opens. It will reduce Ravenswood gridlock to/from Middlefield and El Camino and direct commuter traffic more efficiently to Willow Road and Highway 101.

4. **Increase parking for renters and employees** since inadequate parking forces apartment renters, visitors and employees to clog residential streets with traffic while looking for parking and for taking up limited residential parking.

(Note: In the 12/12 Planning Commission meeting on the SRI EIR, some commissioners wanted to reduce the proposed parking to force renters/employees to use public transit. But the representative from the firm that will conduct the EIR said that studies showed that reducing parking spaces did NOT reduce cars or numbers of car trips. It just pushed drivers to surrounding residential areas to take street parking, which added traffic as well. There were no reductions in Greenhouse Emissions or in number of car trips.)

5. **Provide underground parking** for the apartment buildings and for the offices to reduce the overall height of the project (especially to reduce the height of the 3-story parking garage behind the Barron Street homes facing bedrooms and private living spaces on both floors of the homes) and the potential of five six-story apartment buildings if the project is approved for the 600 total housing unit option being considered. Although developers say underground parking is costly, based on current Menlo Park office rental pricing, the one million square feet of office can command an estimated \$50M per year. Considering the negative impact on the surrounding areas of this project, the cost of underground parking for the benefit of the community will be offset by the profits from just the office space alone. The apartment rental income will be another large annual revenue generator since most of the units will be at high market-rate pricing (e.g. SpringLine's rental pricing).

6. **Include an emergency water storage tank** since there is no emergency water for residents and workers west of El Camino (per the latest Menlo Park Municipal Water Report that was mailed to residents) which stated that the emergency well in the City Yard is not online yet. The risk of toxic contamination of the City Yard emergency well makes it a problem since the city's gas tanks and city yard with other toxic substances (oil, pesticides, etc.) are above it and risk leaking into the groundwater, especially in the expected large earthquake event at some point in the future.

Thank you for your serious consideration of a lower-impact development solution,

Sue Connelly

From: [Dr. Harvey Fishman](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]New development comments
Date: Monday, January 23, 2023 4:52:11 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

- The project should net out to provide the state-mandated housing number of 400, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on the Ravenswood driveways if

Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Sent from my iPhone.

Best Harvey
650-387-8481 cell

From: [Patti Fry](#)
To: [Planning Commission](#)
Cc: [CCIN](#)
Subject: SRI Parkline project
Date: Monday, January 23, 2023 8:22:39 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning Commissioners --

Please be sure that the project is modified so it improves rather than worsens the jobs/housing imbalance in Menlo Park.

There are decades of precedent with SRI to manage the number of allowed workers on site, well-documented by a submission in the public record by former Council Member Paul Collacchi, The current proposed project blows out prior precedent, including when land was spun off for housing. Managing the number of workers continues to be an important lever.

The proposed EIR scope continues to include worker density metrics that likely would greatly underestimate the potential number of workers and related negative impacts. The staff report describes office worker density assumptions of 250 SF/worker whereas tech companies have allocated 50-150 SF/worker, 66% to 400% more. Be sure that the metrics used will measure realistic impacts. Fix the metrics to be used in the analysis.

Patti Fry, former Menlo Park Planning Commissioner

From: [JoAnne Goldberg](#)
To: [PlanningDept](#)
Cc: [CCIN](#)
Subject: [Sent to Planning]Planning commission meeting January 23: Item G1, Parkline Study Session
Date: Monday, January 23, 2023 11:55:13 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning Commissioners and Staff:

Thank you for accepting comments on this important project.

First, I want to endorse the information and analysis that former council member Paul Collacchi sent the Council and Planning Commission two weeks ago, asking for a big picture EIR analysis of the entire project, including the longer-term impact on housing requirements. His analysis points out that the overall project will increase the new housing obligation by over 2,000 units. Long-term consequences always need to be a consideration.

Meanwhile, tonight's study session focuses on the addition of 400-600 housing units in high-rise apartment buildings with few (if any) amenities offered to those new residents, or to current residents of the city. Burgess Park is across the street, but as the only city park with diverse facilities designed to meet the needs of a large segment of the population, it is already fully utilized (until this year, I scheduled practices and games for our local non-profit, all-volunteer youth soccer organization, AYSO. Space all over town is severely limited, especially at Burgess. We don't have enough room for our kids to play as is).

Next, proposals for this housing project specify that it be massively underparked, with (paid) housing advocates suggesting even less housing, holding up visions of a utopian community in which everyone -- no matter their age, physical health, or work/family obligations -- can bike or walk everywhere. In reality, the residents are going to have cars, which will either have to be parked at Burgess or in adjacent neighborhoods.

In the past, the city Planning Commission has rejected projects that did not meet parking requirements. I urge you to continue that tradition with this project.

Although most people in Menlo Park seem unaware of the Parkline project, it will impact almost all neighborhoods and have a deleterious effect on east-west connectivity. I second's Paul's request to expand the EIR to encompass most of the city, with particular note to the fact that Ravenswood and Laurel Street are heavily used by children bicycling to school.

I ask that you consider the needs of all residents and take a long-term approach to this proposal. Once the project has been approved, the change will be irrevocable.

JoAnne Goldberg

From: [Kathy Goodell](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Springline Project Requests
Date: Monday, January 23, 2023 6:30:08 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

For the SRI/Springline project I respectfully request that you not exceed the 400-residential unit plan and keep office at the current C1 level, have the apartment complex not exit onto Laurel, and provide additional (not less) parking --including underground parking for offices and renters.

For those wishing to go west on Ravenswood (to connect to downtown and El Camino) our only street exit from Linfield Oaks is at the Laurel/Ravenswood intersection and in case of emergency and everyday travel (and for vehicles coming from the police station on Laurel) it's important to not have huge traffic bottlenecks at the Laurel/Ravenswood intersection. Opening up the Middlefield gate for the SRI/Springline folks would seem a logical alternative to reroute and help alleviate traffic pressure at Laurel/Ravenswood.

Thank you for your consideration of my requests.

Sincerely,

KATHY

Katherine L. "Kathy" Goodell
21 Willow Road
Menlo Park

From: [Tom Hall](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI Property
Date: Monday, January 23, 2023 8:49:28 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area.

Tom Hall
212 Gilbert Ave.
Menlo Park

From: [Betsy Henze](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline
Date: Monday, January 23, 2023 10:46:57 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

- The **project should net out to provide the state-mandated housing number of 400**, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on the Ravenswood driveways if

Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Quoting from my neighbor, Sue Connelly, who says it far better than I :

" SRI /ParkLine will have highly profitable housing and office revenue annually, but the costs will be borne by the taxpayers.

Based on current Menlo Park office rates, the office project stands to generate \$50M per year. This doesn't include ANY of the apartment rentals, for which most will be at very high rents (see the current rents for the new SpringLine apartments!). There will be some city revenue, but since SRI is a non-profit, this massive development will not offset many of the costs residents must pay for infrastructure (schools, police, fire, water and roads). Yet it will create a significant reduction in our quality of life (and possibly home values), bike/pedestrian safety for school children and residents, and increasing the state-mandated affordable housing units even more.

We need to require that any new office development provides/includes the affordable housing that the office spaces

and employee densities will be required to be built in Menlo Park."

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development .

Betsy Henze
320 Sherwood Way
Menlo Park

From: [Nancy Hosay](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review - requested changes
Date: Sunday, January 22, 2023 11:22:12 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

- The **project should net out to provide the state-mandated housing number of 400**, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on the Ravenswood driveways if

Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Quoting from my neighbor, Sue Connelly, who says it far better than I :

" SRI /ParkLine will have highly profitable housing and office revenue annually, but the costs will be borne by the taxpayers.

Based on current Menlo Park office rates, the office project stands to generate \$50M per year. This doesn't include ANY of the apartment rentals, for which most will be at very high rents (see the current rents for the new SpringLine apartments!). There will be some city revenue, but since SRI is a non-profit, this massive development will not offset many of the costs residents must pay for infrastructure (schools, police, fire, water and roads). Yet it will create a significant reduction in our quality of life (and possibly home values), bike/pedestrian safety for school children and residents, and increasing the state-mandated affordable housing units even more.

We need to require that any new office development provides/includes the affordable housing that the office spaces

and employee densities will be required to be built in Menlo Park."

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development .

Nancy Hosay
325 Linfield Place
Menlo Park

From: [John Henze](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review - Requested Changes
Date: Monday, January 23, 2023 3:11:59 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my neighbors regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

- The **project should net out to provide the state-mandated housing number of 400**, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be

significantly reduced on the Ravenswood driveways if Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Quoting from my neighbor, Sue Connelly, who says it far better than I :

" SRI/ParkLine will have highly profitable housing and office revenue annually, but the costs will be borne by the taxpayers.

Based on current Menlo Park office rates, the office project stands to generate \$50M per year. This doesn't include ANY of the apartment rentals, for which most will be at very high rents (see the current rents for the new SpringLine apartments!). There will be some city revenue, but since SRI is a non-profit, this massive development will not offset many of the costs residents must pay for infrastructure (schools, police, fire, water and roads). Yet it will create a significant reduction in our quality of life (and possibly home values), bike/pedestrian safety for school children and residents, and increasing the state-mandated affordable housing units even more.

We need to require that any new office development provides/includes the affordable housing that the office spaces and employee densities will be required to be built in Menlo Park."

Please don't forget about all of the long-time Menlo Park residents that value the quality of life that Menlo Park has long afforded. Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development.

Thanks,

John Henze

31 year Menlo Park resident
320 Sherwood Way

Confidentiality notice: This message may contain confidential information. It is intended only for the person to whom it is addressed. If you are not that person, you should not use this message. We request that you notify us by replying to this message, and then delete all copies including any contained in your reply. Thank you.

From: [Lauren John](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]
Date: Monday, January 23, 2023 9:50:19 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

- The **project should net out to provide the state-mandated housing number of 400**, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on

the Ravenswood driveways if Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Quoting from my neighbor, Sue Connelly, who says it far better than I :

" SRI /ParkLine will have highly profitable housing and office revenue annually, but the costs will be borne by the taxpayers.

Based on current Menlo Park office rates, the office project stands to generate \$50M per year. This doesn't include ANY of the apartment rentals, for which most will be at very high rents (see the current rents for the new SpringLine apartments!). There will be some city revenue, but since SRI is a non-profit, this massive development will not offset many of the costs residents must pay for infrastructure (schools, police, fire, water and roads). Yet it will create a significant reduction in our quality of life (and possibly home values), bike/pedestrian safety for school children and residents, and increasing the state-mandated affordable housing units even more.

We need to require that any new office development

provides/includes the affordable housing that the office spaces and employee densities will be required to be built in Menlo Park."

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development .

George and Lauren John
331 Laurel Street
Menlo Park 94025

From: [John Kadwany](#)
To: [Planning Commission](#)
Cc: [CCIN](#)
Subject: Parkline/SRI proposal comments
Date: Monday, January 23, 2023 11:11:08 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning Commissioners:

Following are comments on the land use policies implied by the Parkline/SRI redevelopment proposal, followed by recommendations.

– This project presents as a large office park with some housing included. The parking including three multi-story parking garages is significantly out of scale for a transit-oriented proposal. There is a commercial-to-housing ratio of about 2:1 or 3:1 (including old buildings) by square footage. Given that the Specific Plan major developments (Stanford, 1300 ECR) are about 50:50 residential compared to office + retail, for square footage, that amount of commercial space is out of step with recent transit-oriented development.

– Given the scarcity of housing in the Bay Area, this proposed office-residential ratio should not be encouraged by the PC or the city. A better use of this site would be to include more housing and less commercial and parking space. I do not know of city policy or resident preferences for this projected level of commercial space, especially given over-built office capacity today.

– The current proposal is not that of a 'neighborhood' or 'mixed-use' as stated in the Master Plan. This is principally an office park. While pedestrian and bicycle circulation through the project is good, the site space is dominated by the commercial and parking buildings. The two amenity buildings do not create a mixed-use plan. (That's not to suggest significant retail should be included, so the 'mixed-use' goal needs clarification. Certainly the office + residential design is not 'mixed-use'.) The 'open space' is numerically generous, and the designated use areas are good, but the overall layout is not that of an inviting public space. The plan does provide desirable benefits including the planned affordable housing area and the playing field.

- The current configuration of commercial buildings and parking garages, while apparently (and gratefully) not designed as 'secure' areas, are not oriented to encourage interaction with the community, or even the planned residences. The busy scenes full of pedestrians or office workers shown enjoying walkways in the project slides will not likely materialize.

– The rezoning and General Plan amendments options are open-ended. I do not agree with changes which would allow the development as proposed. It's a poor use of this site, more appropriate to urban planning now several decades past. I would not want amendments or zoning allowing new or existing buildings to be sold off to others, at least for significant periods of time. Plans for existing buildings including 'P', 'T' and 'S', and options for the affordable housing plan area, should be clarified.

- I understand the applicant is assuming that existing commercial entitlements, based on square footage, justify the proposed commercial space and parking. Instead, the applicant should acknowledge the very low intensity uses SRI has enjoyed in Menlo Park for decades. The applicant, PC and CC should use past site use intensities as a point of comparison for overall benefit-cost comparisons. A smaller total commercial use target should be considered.

RECOMMENDATIONS:

- The plan needs a different balance of residential-commercial use of the site, and reduction of multi-story parking. For that, the site perimeter and large site size are sufficient to accommodate higher buildings for the site interior, keeping in mind existing streets and neighborhoods. For comparison, San Mateo and Palo Alto have several higher and older residential buildings mixed in smaller scale neighborhoods or downtowns. Consideration should be given where relevant to additional height for residential and commercial buildings to add floor area. Affordable housing plans could be integrated with these changes.

- Given fewer and possibly taller buildings, the remaining open space can be consolidated into a larger space shared by commercial and residence buildings. Such an approach could create a genuine shared open space, and a distinctive neighborhood less isolated from the adjoining residences, streets and neighborhoods.

Sincerely,
John Kadvany / College Avenue

From: [Kenneth Everett Mah](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/ParkLine Study Session with Planning Commission public comment
Date: Monday, January 23, 2023 4:33:26 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Menlo Park Planning Commission and Planning Department,

My wife and I are writing to express our concerns about the project overall and EIR, and request additional items be added to the scope and be studied/changed. We, along with our 4.5yo daughter and 7mo son, bought our home in the Burgess Classic neighborhood ~1 year ago (November 2021) and live directly on Laurel St across from Burgess Pool. We have lived on Laurel St for now 6+ years total.

Generally, we are concerned about the impact of the size of the residential and commercial development on local safety and resources. Specifically, traffic on Laurel St, safety of biking and walking on Laurel street especially for children since it's a safe route to school, and utilization of Burgess Park amenities.

- Entrances/exits on Laurel St
 - These should all be removed. All traffic, both residential and commercial, should be routed to Middlefield and Ravenswood. There is an opportunity to create an additional network of roads within SRI to either offload current traffic or at a minimum keep new traffic that will be added by this project off Laurel St, which is residential. We requested this in writing and verbally to both the City Council/Planning Commission and Lane Partners, but continue to be ignored and have not received any explanations on why they want to direct the new residential traffic onto Laurel as opposed to the internal SRI roads or Ravenswood. Furthermore, not having driveways onto Laurel would encourage new residents to use alternative modes of transportation rather than drive.
 - Request: Please remove all entrances/exits on Laurel St, or study the impact on traffic on Laurel St and demonstrate there will be no difference from the current state. Also, study the impact at the different variations of housing density.
- Safety on Laurel St
 - Laurel St is a residential street that is designated a safe route to school. Any increase in car traffic or driveway use (the current SRI driveways on Laurel have minimal traffic to no traffic) will compromise the safety of children. Walking and biking will be more dangerous due to traffic and more intersections. We have verbally requested Lane Partners extend truly protected (by physical barriers such as curb, and not just paint) bike lanes in both directions on Laurel from Ravenswood to Burgess, and they verbally agreed, but we don't see it on the proposal.
 - Request: Please remove all entrances/exits on Laurel St, or study the impact on traffic on Laurel St and demonstrate that traffic accidents (car vs car, car vs bike, car vs pedestrian) will not increase, and the impact of at the different variations of housing density.
 - Request: Install truly protected (by physical barrier such as curb or

- immobile ballard) bike lanes in both directions on Laurel St from Ravenswood to Burgess.
 - Request: Install truly protected (by physical barrier such as curb or immobile ballard) bike lanes in both directions on Burgess Drive from Laurel St to SRI/Menlo Park Corporation Yard (since this will be open to bike/pedestrian traffic).
 - Also, would like protected bike lanes the full length of Burgess between Alma and SRI whether as part of this Parkline Project or the Middle Tunnel.
- Utilization of Burgess Park amenities
 - Adding 400+ units and commercial space will severely overcrowd the amenities at Burgess Park, and decrease how current residents can use them. These include the pool, tennis courts, playground, library, gymnastics center, etc. and the associated classes with them, such as gymnastic and dance classes, swim lessons, etc.
 - Request: Study the impact on Burgess amenities by specific amenities, not generally, and class/course offerings at each amenity, and demonstrate there will be no difference than current state. Also, study the impact at the different variations of housing density.
 - Request: Give Burgess Classics residents priority and discounted/free access to Burgess Park amenities if the Parkline development will impact access in any way.
- Menlo Park Corporation Yard Parking lot
 - This parking lot is primarily used by MP staff during the day, and Burgess Classics residents at night. We are currently not allowed to get annual overnight parking passes despite our limited street parking, but we can use the lot and tennis court. We are concerned that Parkline residents and workers will use the lot, as will other people who come to use the public space and amenities in Parkline as it is the closest parking lot to SRI/Parkline.
 - Request: Study the impact of the development on use of the Corporation Yard parking lot during the day, evening, and overnight, and demonstrate there will be no impact.
 - Request: If there is an impact, make lot not accessible to Parkline residents or workers nor the public, and give Burgess Classic residents access to overnight annual parking permits for free so we can park on the streets of Burgess Classics (Thurlow, Hopkins, and Barron) and the Corporation Yard parking lot.

Please let me know if you have questions or need clarification about these concerns or requests.

Thank you for your time and consideration,
Kenneth Mah

From: [Rob McCool](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]please reconsider SRI/ParkLine site specifics
Date: Monday, January 23, 2023 4:24:44 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Menlo Park City Council,

Reducing housing to office space imbalance by increasing housing should be a priority for us all. Increasing the housing to 600 units at this site, from 400, while still allowing 4000 more employees into the site, does nothing to relieve this imbalance.

I am also disappointed to see that parking is being reduced in an attempt to reduce car traffic. Our peninsula cities are simply not correctly set up for this to be realistic at this time, meaning that anyone living in these new properties will absolutely have a car, as will many of the employees commuting into the site each day. I urge the council to be realistic as to how people will get around our city from this new development, which is going to remain car-based due to the last mile problem associated with caltrain.

Finally I would also urge the council to consider Laurel Street, and not include a driveway onto Laurel from this complex. Middlefield is far more well set up to handle this increased traffic, and would be the more appropriate way to direct traffic. Our police frequently use Laurel Street to get to and from various parts of town and introducing more traffic blockage on Laurel is not going to be positive.

Thanks, Rob McCool 360 Sherwood Way

From: [Peter C](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]Traffic at SRI
Date: Sunday, January 22, 2023 5:07:35 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning Department,

It is apparent that the Planning Department and City Council are acting counter to the concerns of the Burgess Classics neighbors. Yes, we have a housing deficit in the Bay Area, but replacing it with this project does not solve the area's housing problem.

My concerns are as follows:

- 1) Major traffic along Laurel, Ravenswood and Middlefield. We need to make sure the trip caps are low enough to manage this large project.
- 2) This project will create an imbalance to jobs to housing units, further exacerbating the region's housing crisis. Let's not use tax receipt collections as a smoke screen to endorse the project. We need to ensure it does not impact schools and our local infrastructure.
- 3) 600-unit mid-rises don't conform to the area's existing uses.

I'm generally supportive, but let's go back to 400 units the original proposal by the developer.

Thank you

Peter C (District 3 resident)

From: [Susan Stimson](#)
To: [PlanningDept](#); [_CCIN](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review - requested changes
Date: Monday, January 23, 2023 3:50:22 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

City Council and Planning Commission Members,

As a 14 year resident of Menlo Park, I urge you to curtail the scope of the Parkline housing project to protect safety and accessibility in Menlo Park.

As you know from past examination of the railroad crossings, the crossing at Ravenswood is especially tenuous during high traffic hours which surround both business hours AND very importantly school hours.

In addition, the accessibility to and from Highway 101 via Willow road has deteriorated. Of course, there was respite amidst the pandemic, however, the existing two lane road is insufficient to accommodate future growth.

The city has expressed interest in forward and future thinking which I think is apt. Preparing for additional housing is an important part of that for certain.

That said, the plans must be coupled with forward thinking and planning regarding infrastructure to accommodate additional neighbors such as above/below grade railroad crossings and additional routes to access highways 101 and 280. Not doing so puts current and future neighbors at risk and lacks prudence.

The Parkline project is scoped to add over twice as many units as the 2 large developments yet to be inhabited (Springline is open but not at capacity and the Stanford project is still under construction). Despite how the city chooses to draw district lines, all properties are adjacent to downtown. While convenience to public transit is a benefit, it is not realistic or fair to assume that new residents will give up their freedom of owning and using an automobile. People have lives off of El Camino... kids sports activities, jobs off highways vs downtown, jobs like sales or construction that require daily driving, hiking in the hills, volunteering on the coast for example.

While I understand that speculative models have been generated regarding the potential effects to traffic and safety, I urge the city to "digest" the new additions from other downtown adjacent developments before adding extensively to them.

I am fully supportive of adding new housing on the SRI campus and the campus development overall. I also support stipulating that a higher percentage become affordable housing.

My asks:

- Perform a traffic and safety assessment subsequent to the large developments on El Camino being inhabited. That will be possible very soon if the need for housing near downtown is dire.
- Perform a survey of those new neighbors to see how they in fact are commuting and using / not using public transit.
- Ensure city of the future planning includes near term investments in infrastructure to improve access to highways 101 and 280 and also above or below grade RR track crossings

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development.

Susan Stimson

From: [Karen Wang](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review - requested changes
Date: Monday, January 23, 2023 4:29:25 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning Commission and City Council members:

I object to proposed changes in the density and size of the SRI/Parkline development for the following reasons.

- At even 400 housing units, never mind 600, the density of this development far outstrips anything in the adjoining neighborhoods and will negatively impact basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area.
- We should not increase the jobs-housing imbalance by adding any more office space to this proposal. We need to stop big office development until we meet the affordable housing deficit for the offices already built and others already approved in the pipeline. We need to bring jobs and housing in balance, not keep widening the gap between them.
- It is fantasy to believe workers and residents will exclusively use public transit and not have cars. The traffic and parking impact on the surrounding neighborhoods will be terrible.

I hope you reject this enlarged and negatively impactful proposal for this development. Thank you for your consideration.

Karen Wang
29 Willow Road
Menlo Park