



**CITY COUNCIL
REGULAR MEETING AGENDA**

**Tuesday, May 7, 2013
7:00 P.M.**

**Senior Center at Belle Haven, 110 Terminal Avenue,
Menlo Park, CA 94025**

7:00 P.M. REGULAR SESSION

ROLL CALL – Carlton, Cline, Keith, Ohtaki, Mueller

PLEDGE OF ALLEGIANCE

REPORT FROM CLOSED SESSION

ANNOUNCEMENTS

A. PRESENTATIONS AND PROCLAMATIONS

- A1.** Proclamation declaring Bike to Work Day May 9, 2013 *(Attachment)*
- A2.** Proclamation declaring Public Works Week May 19-25, 2013 *(Attachment)*
- A3.** Proclamation declaring Municipal Clerk's Week May 5-11, 2013 *(Attachment)*
- A4.** Proclamation recognizing National Mental Health Awareness Month *(Attachment)*
- A5.** Update on Belle Haven Neighborhood Vision process *(Staff report #13-082)*

B. COMMISSION/COMMITTEE VACANCIES, APPOINTMENTS AND REPORTS

- B1.** Consider applicants for appointment to fill four vacancies on the Bicycle Commission and One Vacancy on the Transportation Commission *(Staff report #13-079)*

C. PUBLIC COMMENT #1 (Limited to 30 minutes)

Under "Public Comment #1", the public may address the Council on any subject not listed on the agenda and items listed under the Consent Calendar. Each speaker may address the Council once under Public Comment for a limit of three minutes. Please clearly state your name and address or political jurisdiction in which you live. The Council cannot act on items not listed on the agenda and, therefore, the Council cannot respond to non-agenda issues brought up under Public Comment other than to provide general information.

D. CONSENT CALENDAR

- D1.** Award a contract for street sweeping services to Contract Sweeping Services, Inc. in the amount of \$638,512.70 and authorize the City Manager to extend the contract for up to an additional 4 years **(Staff report #13-073)**
- D2.** Award a contract for the Sidewalk Trip Hazard Removal Project to Precision Emprise, Inc. in the amount of \$80,000, and authorize a total budget of \$100,000 for construction, contingencies, material testing, inspection and construction administration **(Staff report #13-078)**
- D3.** Authorize the Public Works Director to accept the work performed by Golden Bay Construction, Inc., for the Safe Routes to Hillview Middle School Project **(Staff report #13-076)**
- D4.** Authorize the Public Works Director to accept the work performed by Amland Corporation, for the Middlefield Road at Linfield Drive Lighted Crosswalk Improvement Project **(Staff report #13-077)**

E. PUBLIC HEARINGS

- E1.** Conduct a Public Hearing and consider a request for Use Permit, Architectural Control, Tentative Map, Heritage Tree Removals and Below Market Rate (BMR) Housing in-lieu fee agreement for 6 detached dwelling units on two adjacent parcels at 1273 and 1281 Laurel Street **(Staff report #13-074)**

F. REGULAR BUSINESS

- F1.** Provide direction on the State Route 101/Willow Road Interchange Project alternative **(Staff report #13-075)**
- F2.** Consider a resolution authorizing preliminary conditional commitment of \$2.5 million from the Below Market Rate Fund for the CORE Affordable Housing Project at the Veteran's Administration facility in Menlo Park **(Staff report #13-081)**
- F3.** Council discussion and possible recommendation on various seats for determination at the next City Selection Committee meeting scheduled for May 17, 2013 **(Staff report #13-080)**
- F4.** Consider state and federal legislative items, including decisions to support or oppose any such legislation, and items listed under Written Communication or Information Item: **None**

G. CITY MANAGER'S REPORT – None

H. WRITTEN COMMUNICATION – None

I. INFORMATIONAL ITEMS – None

J. COUNCILMEMBER REPORTS

K. PUBLIC COMMENT #2 (Limited to 30 minutes)

Under “Public Comment #2”, the public if unable to address the Council on non-agenda items during Public Comment #1, may do so at this time. Each person is limited to three minutes. Please clearly state your name and address or jurisdiction in which you live.

L. ADJOURNMENT

Agendas are posted in accordance with Government Code Section 54954.2(a) or Section 54956. Members of the public can view electronic agendas and staff reports by accessing the City website at <http://www.menlopark.org> and can receive e-mail notification of agenda and staff report postings by subscribing to the “Home Delivery” service on the City’s homepage. Agendas and staff reports may also be obtained by contacting the City Clerk at (650) 330-6620. Copies of the entire packet are available at the library for viewing and copying. (Posted: 05/02/2013)

At every Regular Meeting of the City Council, in addition to the Public Comment period where the public shall have the right to address the City Council on the Consent Calendar and any matters of public interest not listed on the agenda, members of the public have the right to directly address the City Council on any item listed on the agenda at a time designated by the Mayor, either before or during the Council’s consideration of the item.

At every Special Meeting of the City Council, members of the public have the right to directly address the City Council on any item listed on the agenda at a time designated by the Mayor, either before or during consideration of the item.

Any writing that is distributed to a majority of the City Council by any person in connection with an agenda item is a public record (subject to any exemption under the Public Records Act) and is available for inspection at the Office of the City Clerk, Menlo Park City Hall, 701 Laurel Street, Menlo Park, CA 94025 during regular business hours. Members of the public may send communications to members of the City Council via the City Council’s e-mail address at city.council@menlopark.org. These communications are public records and can be viewed by any one by clicking on the following link: <http://ccin.menlopark.org>

City Council meetings are televised live on Government Access Television Cable TV Channel 26. Meetings are re-broadcast on Channel 26 on Thursdays and Saturdays at 11:00 a.m. A DVD of each meeting is available for check out at the Menlo Park Library. Live and archived video stream of Council meetings can be accessed at http://menlopark.granicus.com/ViewPublisher.php?view_id=2

Persons with disabilities, who require auxiliary aids or services in attending or participating in City Council meetings, may call the City Clerk’s Office at (650) 330-6620.

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Proclamation

BIKE-TO-WORK DAY THURSDAY, MAY 9, 2013

WHEREAS, breathing clean air is vital to healthy lungs and life and levels of air pollution in the City of Menlo Park may exceed health standards; and

WHEREAS, the City of Menlo Park acknowledges that cycling to work is a successful commute mode to alleviate traffic congestion, reduce air pollution and decrease fuel consumption; and

WHEREAS, bike-to-work days have proven effective in converting drivers into bicyclists and educating residents about the environmental importance of biking to work regularly; and

WHEREAS, the City of Menlo Park encourages its residents to bike to work, in order to improve air quality and promote the health benefits of cycling; and

WHEREAS, the Bay Area Bike to Work Day event is part of National Bike Month, an annual recognition of bicycling as a convenient and fun form of transportation that is growing more popular in the United States; and

WHEREAS, the Bay Area Bicycle Coalition will host the 19th annual Bike to Work Day event partnering with local bicycle coalitions, Bay Area corporate sponsors, government agencies, environmental and transportation organizations and non-profit agencies and hundreds of volunteers in promoting the message that bicycling is a fun, healthy, and environmentally viable form of transportation.

NOW, THEREFORE, I, Peter I. Ohtaki, Mayor of the City of Menlo Park and on behalf of the City Council, do hereby proclaim Thursday, May 9, 2013, as Bike-to-Work Day in the City of Menlo Park.



Peter I. Ohtaki, Mayor

May 9, 2013



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Proclamation

**RECOGNIZING MAY 19-25, 2013, AS
PUBLIC WORKS WEEK**

WHEREAS, Public Works programs and services provided to our community are an integral part of our residents' everyday lives; and

WHEREAS, the health, safety, and comfort of our community and neighborhoods greatly depend on these Public Works programs and systems such as public buildings, parks, playgrounds, sports fields, water supply, trees, refuse disposal and recycling, streets, street cleaning and lighting, storm drains and creeks, environmental conservation, and transportation; and

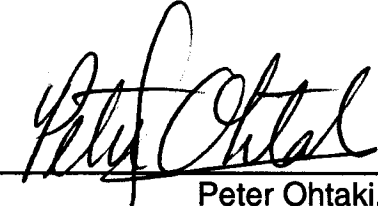
WHEREAS, the support and understanding of an informed citizenry are vital to the efficient operation of these programs and services; and

WHEREAS, the quality and effectiveness of these programs and systems depend upon the efforts and skills of Public Works personnel; and

WHEREAS, Public Works personnel are among the first responders during emergencies and disasters, often remaining after the departure of other first responders to ensure that services are restored and operations resume; and

WHEREAS, the infrastructure that Public Works personnel design, build, operate, and maintain forms an important part of the foundation upon which the City's prosperity and well-being are built.

Now, therefore, be it resolved that I, Peter Ohtaki, Mayor of Menlo Park, recognize May 19–25, 2013, as Public Works Week. During this week, I call upon residents to recognize the contributions that Public Works personnel make every day to residents' health, safety, and quality of life.


Peter Ohtaki,
Mayor of Menlo Park



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Municipal Clerks Week
May 5 - 11, 2013

Whereas, The Office of the Municipal Clerk, a time honored and vital part of local government exists throughout the world; and

Whereas, The Office of the Municipal Clerk is the oldest among public servants; and

Whereas, The Office of the Municipal Clerk provides the professional link between the citizens, the local governing bodies and agencies of government at other levels; and

Whereas, Municipal Clerks have pledged to be ever mindful of their neutrality and impartiality, rendering equal service to all; and

Whereas, The Municipal Clerk serves as the information center on functions of local government and community; and

Whereas, Municipal Clerks continually strive to improve the administration of the affairs of the Office of the Municipal Clerk through participation in education programs, seminars, workshops and the annual meetings of their state, province, county and international professional organizations; and

Whereas, it is most appropriate that we recognize the accomplishments of the Office of the Municipal Clerk.

Now, Therefore, I, Peter Ohtaki, Mayor of Menlo Park, do hereby proclaim the week of May 5 through May 11, 2013, as Municipal Clerks Week, and further extend appreciation to our City Clerk, Margaret Roberts and our Deputy City Clerk, Pamela Aguilar and to all Municipal Clerks for the vital services they perform and their exemplary dedication to the communities they represent.

Peter Ohtaki, Mayor

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Proclamation

National Mental Health Awareness Month May 2013

WHEREAS, mental health is fundamental to the overall health and well-being of all residents in San Mateo County; and

WHEREAS, severe mental health conditions such as schizophrenia, bipolar disorder, major depression, obsessive-compulsive disorder, and severe anxiety disorders are more common than cancer, diabetes and heart disease and affects one in four adults 18 and older in the United States; and

WHEREAS, the treatment success rates for mental health conditions compare favorably to the rate for chronic physical conditions such as heart disease, diabetes or cancer; and

WHEREAS, mental health conditions continue to remain shrouded in stigma and discrimination from societal misconceptions and prejudice creating barriers to full community integration and inclusion of people with mental health conditions and keeping those affected from seeking care; and

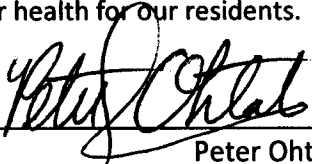
WHEREAS, increased public awareness of mental health issues may lead to reduced stigma and discrimination and earlier identification and treatment leading to lower health care and correctional costs, increased productivity, and improved ability to contribute to healthier families and communities; and

WHEREAS, each business, school, government agency, healthcare provider, organization and resident shares the responsibility of mental health challenges and has a responsibility to promote mental wellness and support prevention efforts; and

WHEREAS, Menlo Park takes pride in upholding the dignity and civil rights of all of its residents, including those with disabilities such as psychiatric disabilities; and

WHEREAS, the Mental Health Awareness Project, Turning Point LLC and the Menlo Park Library are presenting an educational forum on Saturday, May 11, 2013, at the Menlo Park City Council Chambers from 11 am – 2 pm to raise awareness about stigma and societal misconceptions about mental health conditions.

NOW, THEREFORE, BE IT RESOLVED that I, Peter Ohtaki, Mayor of Menlo Park, do hereby proclaim May 2013 as National Mental Health Awareness Month to increase public awareness of the importance of mental health, the impact of severe mental health conditions, the reduction of stigma and discrimination, and to promote greater understanding and hope for those who experience these challenges and pay tribute to all who devote their skill and expertise to the cause of better health for our residents.


Peter Ohtaki, Mayor



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COMMUNITY SERVICES DEPARTMENT

Council Meeting Date: May 7, 2013
Staff report # 13-082

Agenda Item #: A-5

PRESENTATION: Update on the Belle Haven Neighborhood Vision Process

RECOMMENDATION

Staff recommends that the City Council accept the report on the Belle Haven Neighborhood Vision Process results so far.

BACKGROUND

Given recent major land use changes adjacent to Belle Haven Neighborhood including Menlo Gateway and Facebook as well as the State's dissolution of Redevelopment Agencies and the need to invest city funds on the services that are of the highest priority to residents, the Council approved funding for a comprehensive community vision process in Belle Haven in September, 2012. This process is designed to engage residents and stakeholders in identifying the highest-priority services and programs for the Belle Haven community and create an action plan to guide future implementation.

The process, facilitated by consultants from MIG and City Community Services staff, began in January 2013 with the goal of providing City staff and the City Council with information in the spring of 2013 to use in supporting funding and program decisions. This process also seeks to organize and position the community to work effectively with the City and other groups to advocate for its needs and priorities now and in the future.

ANALYSIS

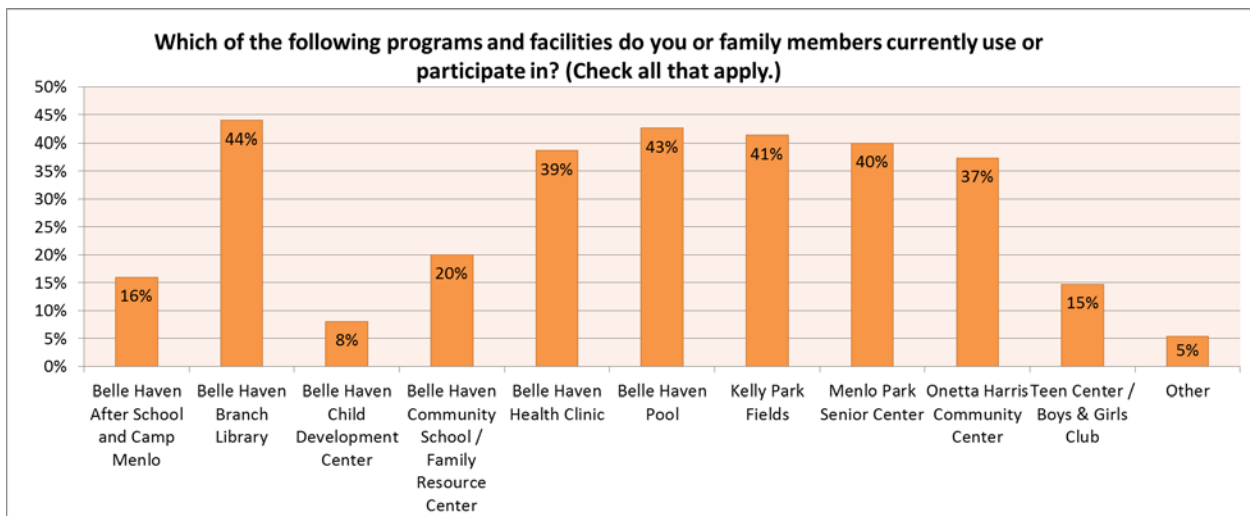
Highlights of the process so far include:

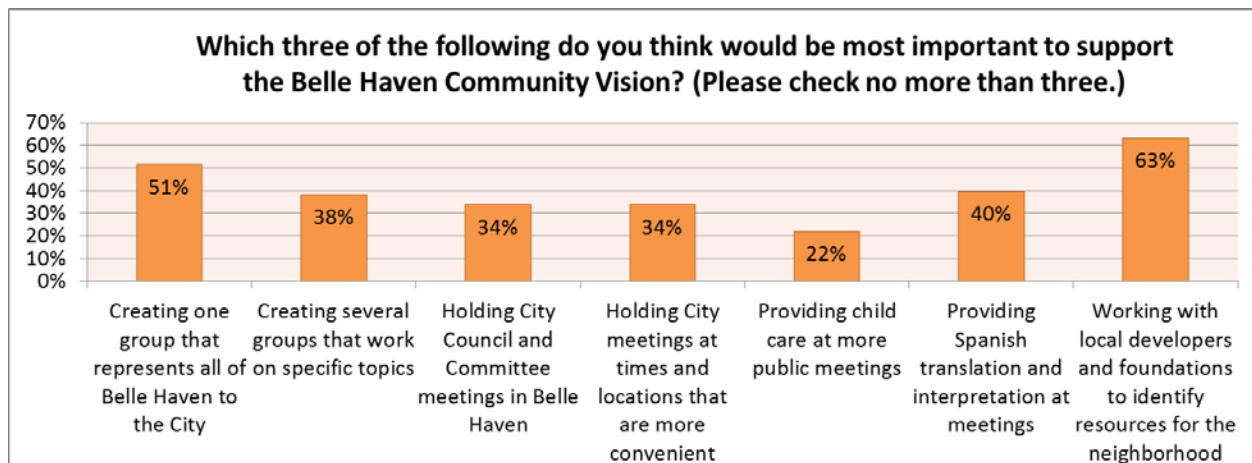
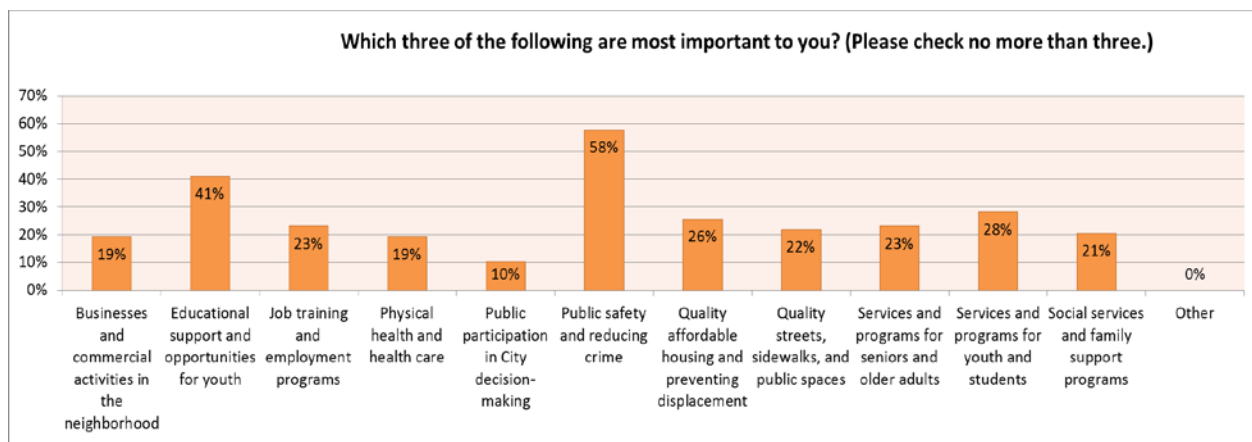
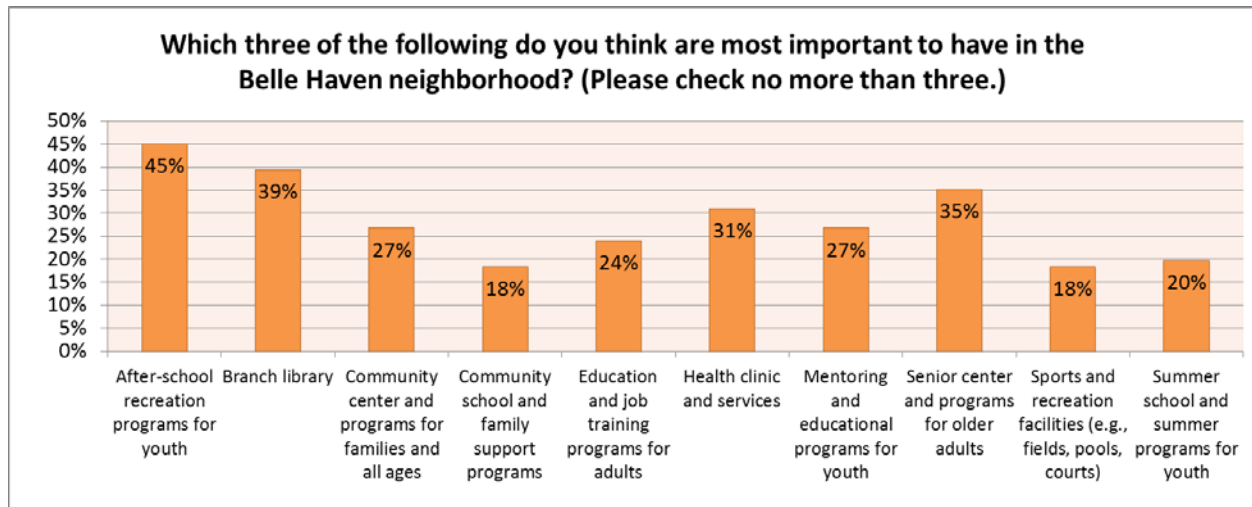
- Over 80 residents attended a Kick-Off meeting on January 29 that introduced the MIG team and solicited names to serve on the neighborhood Outreach Team
- February 19, the public recruitment for the Outreach Team Associates began
- March 21, The Outreach Team, composed of four Belle Haven residents, was introduced at a Community Meeting along with the new Police Chief (40 people attended). The Outreach Team includes 3 bi-lingual members.
- In March, the first neighborhood newsletter about the process was distributed in English and Spanish
- In March, the City launched a web page dedicated to the Visioning Process www.menlopark.org/bellehaven

- In April, on-line survey tools became available on the Visioning Process web site as well as hard copies for distribution
- April 2-9, Outreach Team received their training
- April 11-May 31, Outreach Team will canvass the neighborhood, meet with people at events and intercept activities, participate in community conversations, and conduct small group discussions
- April 20, nearly 300 people attended a Community Visioning Fair at the Senior Center that included interactive activities for resident input, 10 community organizations, children’s activities, lunch and free plant giveaway for Earth Day!
- As of April 25, 86 surveys, 2 small group discussions, and 3 intercept events, as well as informal outreach through networks and neighbors has been held.

Highlights of the 86 neighborhood surveys collected to date include:

- 55% were completed in Spanish and 45% in English
- 82% of respondents were residents of Belle Haven, 7% other Menlo Park neighborhoods, 6% work/volunteer in MP, 5% use Belle Haven services or attend church there
- Primary language is 55% Spanish, 42% English, 3% other
- Most respondents have lived in Belle Haven 10 years or less
- 60% Hispanic or Latino, 15% Caucasian, 12% African-American, 12% Asian/Pacific Islander, 1% other





Common themes are emerging in response to questions asking residents to describe their vision for an improved Belle Haven:

- Improved schools and opportunity for education, unified school district for Menlo Park

- Improved safety and security throughout the neighborhood
- More programs for youth and families, including job training
- Community beautification (cleanliness, green space, inviting mix of uses)
- More community involvement, working together

A neighborhood mapping exercise at the Community Fair asked three questions:

- What places are you most proud of in the community?
- Where do you feel unsafe and where are the problem areas?
- What things do you wish were in the neighborhood or would like to see more of?

Assets / Sources of Pride included:

- Senior Center and Onetta Harris Community Center
- Kelly Park
- Hamilton Park
- Homes and home ownership
- Family and neighborhood connections
- Belle Haven School and Library

Unsafe / Problem Areas included:

- Willow Road and Chilco Ave (traffic and speeding cars)
- Pedestrian bridge (at night)
- Areas where dumping and littering occurs including Ivy Drive
- Sites of shootings and gang activity, including EPA

Needs / Areas of improvement included:

- Safe routes to school
- Police patrol
- Parking options
- Expanded Library hours
- Police Substation – improved access and visibility
- More classes for adults and seniors
- Better streets, sidewalks and drainage
- Lighting for safety
- Bank or ATM
- Farmers Market or Community Garden

Comments cards were also collected at the Community Fair event and comments included:

- Concerns about traffic-related safety on Willow Road particularly for bicyclists and pedestrians.
- Interest in uniting with other Menlo Park schools to improve education opportunities for Belle Haven youth.
- Need for more stores, banks and related services.
- Support for more library hours and increased youth programs including athletics.

- Neighborhood clean-up and beautification needed.
- Improved after school programs for youth and more classes offered for youth and adults.

Community engagement will continue through the end of May including:

- Neighborhood canvassing
- Small group meetings
- Community conversations
- Intercept events
- Community surveying
- Community newsletter
- Community Visioning Workshop

The process will culminate in a community visioning workshop in June.

IMPACT ON CITY RESOURCES

The results of the neighborhood outreach will be documented in a Community Vision and Action Plan that will guide future work by the neighborhood and City. Since there is no dedicated City or consultant support for this project after July, it will be up to Belle Haven leaders, community organizations and residents to keep the momentum going. For that reason, the goals of the visioning process include developing a structure and framework for future community work with the City as well as a list of prioritized actions for the community to follow up on and resources to support that work.

POLICY ISSUES

Using community engagement processes to make long range decisions is consistent with Council policy.

ENVIRONMENTAL REVIEW

The Belle Haven Community Visioning process is not a project requiring environmental review.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed, at least 72 hours prior to the meeting.

ATTACHMENTS

None

Report prepared by:
Derek Schweigart
Assistant Community Services Director

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ADMINISTRATIVE SERVICES DEPARTMENT

Council Meeting Date: May 7, 2013
Staff Report #: 13-079

Agenda Item #: B-1

COMMISSION REPORT: Consider applicants for appointment to fill four vacancies on the Bicycle Commission and one vacancy on the Transportation Commission

RECOMMENDATION

Staff recommends appointing applicants to fill the four vacancies on the Bicycle Commission and one vacancy on the Transportation Commission.

BACKGROUND

Staff has been recruiting for the vacant positions by publishing press releases in the *Daily News* and posting notices on the City's website and City bulletin board.

There are four vacancies on the Bicycle Commission due to the expiring terms of Maynard Harding, Scott Lohmann, Jim Rowe and Robert Steel. Two applicants appointed will serve through April 30, 2016 and two through April 30, 2017.

Applicants for the Bicycle Commission vacancies:

- Drew Combs
- William Kirsch
- Nell Triplett
- Cindy Welton

There is one vacancy on the Transportation Commission due to appointment of Katherine Strehl to the Planning Commission. The applicant appointed will serve through the unexpired term of April 30, 2014.

Applicants for the Transportation vacancy:

- Philip Mazzara
- Michael Meyer (Currently on the Bicycle Commission)

ANALYSIS

Pursuant to City Council Policy CC-01-0004 (Attachment A), commission members must be residents of the City of Menlo Park and serve for designated terms of four years, or through the completion of an unexpired term.

In addition, the Council's policy states that the selection/appointment process shall be conducted before the public at a regularly scheduled meeting of the City Council. Nominations will be made and a vote will be called for each nomination. Applicants receiving the highest number of affirmative votes from a majority of the Council present shall be appointed.

IMPACT ON CITY RESOURCES

Staff support for selection of commissioners is included in the FY 2012-13 Budget.

POLICY ISSUES

Council Policy CC-01-004 establishes the policies, procedures, roles and responsibilities for the City's appointed commissions and committees.

Currently the budget metrics set a goal of two applications for each appointment. Staff has not been unable to achieve this metric.

ENVIRONMENTAL REVIEW

The proposed action does not require environmental review.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed, at least 72 hours prior to the meeting.

ATTACHMENTS

- A. Excerpt from Council Policy CC-01-004, page 5
- B. Commission Applications

Report prepared by:
Margaret S. Roberts, MMC
City Clerk

Attachment B will not be available on-line, but is available for review at City Hall in the City Clerk's Office during standard City operating hours.

City of Menlo Park		City Council Policy
Department City Council Subject Commissions/Committees Policies and Procedures and Roles and Responsibilities	Page 5 of 10	Effective Date 3-13-01
	Approved by: Motion by the City Council on 03-13-2001; Amended 09-18-2001; Amended 04-05-2011	Procedure # CC-01-0004

Application/Selection Process

1. The application process begins when a vacancy occurs due to term expiration, resignation, removal or death of a member.
2. The application period will normally run for a period of four weeks from the date the vacancy occurs. If there is more than one concurrent vacancy in a Commission, the application period may be extended. Applications are available from the City Clerk’s office and on the City’s website.
3. The City Clerk shall notify members whose terms are about to expire whether or not they would be eligible for reappointment. If reappointment is sought, an updated application will be required.
4. Applicants are required to complete and return the application form for each Commission/Committee they desire to serve on, along with any additional information they would like to transmit, by the established deadline. Applications sent by fax, email or submitted on-line are accepted; however, the form submitted must be signed.
5. After the deadline of receipt of applications, the City Clerk shall schedule the matter at the next available regular Council meeting. All applications received will be submitted and made a part of the Council agenda packet for their review and consideration. If there are no applications received by the deadline, the City Clerk will extend the application period for an indefinite period of time until sufficient applications are received.
6. Upon review of the applications received, the Council reserves the right to schedule or waive interviews, or to extend the application process in the event insufficient applications are received. In either case, the City Clerk will provide notification to the applicants of the decision of the Council.
7. If an interview is requested, the date and time will be designated by the City Council. Interviews are open to the public.
8. The selection/appointment process by the Council shall be conducted open to the public. Nominations will be made and a vote will be called for each nomination. Applicants receiving the highest number of affirmative votes from a majority of the Council present shall be appointed.
9. Following a Council appointment, the City Clerk shall notify successful and unsuccessful applicants accordingly, in writing. Appointees will receive copies of the City’s Non-Discrimination and Sexual Harassment policies, and disclosure statements for those members who are required to file under State law as designated in the City’s Conflict of Interest Code. Copies of the notification will also be distributed to support staff and the Commission/Committee Chair.
10. An orientation will be scheduled by support staff following an appointment (but before taking office) and a copy of this policy document will be provided at that time.

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COMMISSION/COMMITTEE APPLICATION

OFFICE USE ONLY
RECEIVED

Received via email
08/22/2012
MSR

Please type or print clearly. You may attach additional pages, if necessary. This is a public document.

Date: August 20, 2012

Commission/Committee of Interest: Bicycle Commission

Name: Andrew "Drew" Combs

Education: Harvard Law School - Juris Doctor, 2002; Columbia University - Bachelor of Arts: Urban Studies, 1998.

Civic affiliations and community activities, including service on other commissions or committees:

Describe your understanding of the responsibilities of the commission that you are applying for and how your personal, community or professional experience relate to these responsibilities:

The city's bicycle commission is charged with assisting the city council with respect to bicycle related initiatives. This includes, but is not limited to, analyzing developments and their transportation impact with respect to bicycle traffic, overseeing informational/educational campaigns aimed at increasing awareness about bicycle safety and other related issues. I believe that my background as a avid recreational bicyclist, in addition to my educational background, which includes instruction in legal and planning issues, provides me a unique perspective to weigh-in on these issues.

Describe why you want to serve on this commission and what you hope to accomplish as a member:

I'm passionate about alternative modes of transportation, especially as this relates to both the bicycling option in low density (suburban) settings. It's clear that with a minimal amount of effort, settings that were built to serve the automobile are able to serve as an equally effective channel of transports for bike riders. Menlo Park, and other Peninsula communities, have made much progress in this area and, in my opinion, are a model for the rest of the country. My goal as a member of the commission would be to serve as the city council's eyes and ears with respect to not only new academic studies regarding bicycle transportation, but also new developments and planning initiatives that are being implemented in municipalities around the world and may serve as a template for how Menlo Park might build on the success it has already achieve in this area.

Terms

Terms for most commissions/committees are for a period of four years. Members are limited to two consecutive full terms*. If a person is appointed to fill an unexpired term and serves less than two years, that time will not be considered a full term. However, if a person is appointed to fill an unexpired term and serves two years or more, that time will be considered a full term.

Specific Information

Serving on a commission or committee may require one or two night meetings per month, with each meeting averaging three to four hours. You may also be asked to serve on additional subcommittees. Members are expected to attend all meetings. Attendance at less than two-thirds of scheduled meetings may result in removal by the Council. Commissioners are not paid for their volunteer service. General information related to the charge of the commissions and committees and their schedules are shown on the attachment. More specific information may be obtained by viewing the City’s website at http://www.menlopark.org/city_commissions.html and by contacting the staff liaison.

Information about the Appointment Process

The application process may take from six weeks to two months. Vacancies are advertised for approximately 30 days with a specific filing deadline. Deadlines may be extended. Please return your application, along with any attachments, to the City Clerk, at the address listed below. Applications are kept on file for one year. The City Council will review all applications, may contact you individually or may decide to hold interviews. All appointments will be made by nomination and vote of the City Council at a Council meeting. Questions about the application process should be directed to Margaret S. Roberts, City Clerk, at (650) 330-6620 or by e-mail at MSRoberts@menlopark.org.

Applicant’s Signature

**Return to the City Clerk, City of Menlo Park,
701 Laurel Street, Menlo Park, CA 94025
(Phone: (650) 330-6620 or e-mail at MSRoberts@menlopark.org)**

OFFICE USE ONLY	
Application Received: <u>August 22, 2012</u>	Address Verified in City Limits: <input checked="" type="checkbox"/> By: <u>MSR</u> Initials
Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
If Appointed Term ends: _____	

PERSONAL INFORMATION

Name: Andrew "Drew" Combs

Residence Address: 347 Cherry Ave. Menlo Park CA 94025

(Note: Residency within the City limits is required)

Telephone No: 310-985-1084 (cell) Number of years as a Menlo Park resident: New Resident

Occupation: Senior Reporter – The American Lawyer Magazine

Email address: combs.drew@gmail.com

Business Address/Telephone No: _____

Internet Posting

If I am appointed, the City is authorized to post the following information on the City's website:

	YES	NO
Home Address:	X <input type="checkbox"/>	<input type="checkbox"/>
Home Phone:	X <input type="checkbox"/>	<input type="checkbox"/>
Mailing Address (if not home address):	<input type="checkbox"/>	<input type="checkbox"/>
Business Address:	<input type="checkbox"/>	<input type="checkbox"/>
Business Phone:	<input type="checkbox"/>	<input type="checkbox"/>
E-mail:	X <input type="checkbox"/>	<input type="checkbox"/>

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COMMISSION/COMMITTEE APPLICATION

OFFICE USE ONLY RECEIVED RECEIVED APR 22 2013 City Clerk's Office City of Menlo Park

Please type or print clearly. You may attach additional pages, if necessary. This is a public document.

Date: 4/22/13 Commission/Committee of Interest: BICYCLE Name: WILLIAM KIRSCH (BILL) Education: BS ECONOMICS, LEHIGH UNIVERSITY '76 MBA ANDERSON SCHOOL, UCLA 1981

Civic affiliations and community activities, including service on other commissions or committees:

1) LATE 1970s FOUNDED & LEAD 'SHERMAN AVE WORKING GROUP' IN UNINCORP WEST MENLO. HELD COMMUNITY PLANNING & VISION MEETINGS WITH SM COUNTY OFFICIALS. DEVELOPED PLAN TO DISCOURAGE NEIGHBORHOOD CUT-THROUGH TRAFFIC AND

Describe your understanding of the responsibilities of the commission that you are applying for and how your personal, community or professional experience relate to these responsibilities:

IMPROVE PEDESTRIAN ACCESS. OUR WORK LEAD TO INSTALLATIONS OF NEIGHBORHOOD ENTRY GATES & NARROWER, REPAVED STREETS.

2) CO-FOUNDED & LEAD THE 'ALAMED STREETScape TASK FORCE' HELD COMMUNITY VISION WORKSHOPS FOR THE RE-ZONING AND

Describe why you want to serve on this commission and what you hope to accomplish as a member:

REDEVELOPMENT OF THIS WEST MENLO PARK BUSINESS DISTRICT. LEAD TO INFRASTRUCTURE RE-BUILD, INSTALLATION OF CONTIGUOUS SIDEWALKS, BIKE LANES, STREET TREES. REDUCED AUTO TRAVEL LANES FROM 4 TO 3. BULBED OUT STREET CORNERS TO REDUCE PEDESTRIAN CROSSING DISTANCES. TRANSFORMED ZONING TO NEIGHBORHOOD SERVING RETAIL.

COMMISSION RESPONSIBILITIES:

CREATE VISION FOR INTEGRATION OF CYCLING INTO OVERALL TRANSPORTATION PLAN FOR MENLO PARK, INCLUDING COMMUTING, RECREATION, SAFE ROUTES FOR SCHOOLS, BICYCLE INFRASTRUCTURE. GUIDE AND ADVISE CITY OFFICIALS ON ABOVE. EDUCATE PUBLIC ON SAFE CYCLING AND BUILD MUTUAL RESPECT BETWEEN ALL USERS OF ROADWAY INFRASTRUCTURE.

MY MANY YEARS OF CYCLING AND PEDESTRIAN ADVOCACY, PASSION FOR REDUCING AUTO USE AND PROMOTING CYCLING AS A VIABLE, SAFE TRANSPORT ALTERNATIVE, AND MY OWN LIFETIME EXPERIENCE AS A BICYCLE COMMUTER HAVE PREPARED ME FOR CHALLENGES AND GREAT OPPORTUNITIES TO HELP MAKE MENLO PARK A NATIONAL LEADER IN BICYCLE ACCESS AND SAFETY

MENLO PARK IS ALREADY A GREAT CYCLING CITY, BUT MUCH MORE CAN BE DONE TO TRULY INTEGRATE THE BICYCLE INTO THE CITY'S TRANSPORTATION SCHEME, THEREBY REDUCING AUTO ~~USE~~ USE, AND DEVELOPING A MORE HUMAN-SCALE, LIVABLE ENVIRONMENT WHERE A VARIETY OF TRANSPORT MODES ARE ACCOMMODATED AND ENCOURAGED.

Terms

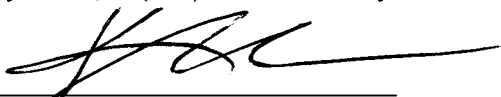
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Applicant's Signature

**Return to the City Clerk, City of Menlo Park,
701 Laurel Street, Menlo Park, CA 94025
(Phone: (650) 330-6620 or e-mail at MSRoberts@menlopark.org)**

OFFICE USE ONLY

Application Received: _____ **Address Verified in City Limits:** **By:** _____
Initials

Considered by City Council: _____ **Appointed:** Yes No

Considered by City Council: _____ **Appointed:** Yes No

Considered by City Council: _____ **Appointed:** Yes No

If Appointed Term ends: _____

PERSONAL INFORMATION

Name: WILLIAM E. KIRSCH

Residence Address: 872 PARTRIDGE AVE, MP 94025

(Note: Residency within the City limits is required)

Telephone No: _____ Number of years as a Menlo Park resident: 4, BUT

Occupation: VENTURE CAPITAL LIVED IN WEST MENLO PARK FOR 25 YEARS

Email address: BILL@COSTELLA KIRSCH.COM

Business Address/Telephone No: COSTELLA KIRSCH 3500 ALBUQUERQUE DE LAS PALMAS, MENLO PARK, CA 94025

Internet Posting

650-467-5790

If I am appointed, the City is authorized to post the following information on the City's website:

- | | YES | NO |
|--|-------------------------------------|--------------------------|
| Home Address: | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Home Phone: | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Mailing Address (if not home address): | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Business Address: | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| Business Phone: | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| E-mail: | <input checked="" type="checkbox"/> | <input type="checkbox"/> |



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COMMISSION/COMMITTEE APPLICATION

Please type or print clearly. You may attach additional pages, if necessary. This is a public document.

Date: August 8, 2012

Commission/Committee of Interest: Bicycle Commission

Name: Nell Triplett

Education: Master of Arts in International Environmental Policy from the Monterey Institute of International Studies (2006), Bachelor of Arts in International Development Studies from UCLA (2004), and high school diploma from Sacred Heart Preparatory (1999)

Civic affiliations and community activities, including service on other commissions or committees:

Silicon Valley Bicycle Association, testimony before Menlo Park Planning Commission and City Council on the Downtown Specific Plan, volunteerism with the Silicon Valley Turkey Trot among other fundraisers for charity.

Describe your understanding of the responsibilities of the commission that you are applying for and how your personal, community or professional experience relate to these responsibilities:

Through monthly meetings, I will work alongside other Bicycle Commissioners to promote a bicycle-friendly community, to provide the cyclist perspective for City decision-making processes, to support bike safety education, to suggest safe bike routes and connectivity, and to ensure maintained cyclist and resources. I can bring fresh perspective to the fulfillment of these Commission priorities as a relatively new resident of Menlo Park who is also a daily commute cyclist. I bring enthusiasm and passion for cycling as an important form of transportation with manifold health, environmental, fiscal, and community benefits as well as a form of recreation. I have been active for months in the local community with regard to the Downtown Specific Plan, I have a network of urban planners and smart growth advocates, and I have established ties with state officials, the Menlo Park Mayor, the City Council, and City Planning Commissioners. My professional work in the area of energy and environmental policies with the Silicon Valley Leadership Group means I am updated on regional issues related to transportation. Lastly, I have extensive experience living, cycling, and studying in Copenhagen, one of the bike capitals of the world where 35 percent of the population use a bicycle as their daily transportation method.

Describe why you want to serve on this commission and what you hope to accomplish as a member:

Based on city transit observations, personal experience cycling, and viewpoints expressed at Menlo Park Planning Commission and City Council meetings, I believe Menlo Park is in need of a strong Bicycle Commission that can boost a community perfectly poised to become a functional biking community. The nexus between my professional work and the Bicycle Commission as well as my own personal passion for cycling would be an asset for the Bicycle Commission's work. I also am looking for meaningful volunteer opportunities that can benefit my local community while also assisting in my own professional development.

Terms

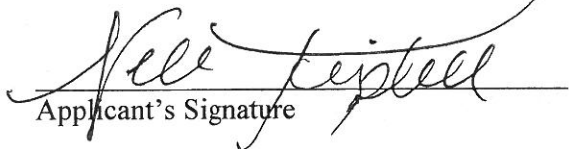
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Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
If Appointed Term ends: _____	

PERSONAL INFORMATION

Name: Nell Triplett

Residence Address: 603 Palmer Lane, Menlo Park, CA 94025

(Note: Residency within the City limits is required)

Telephone No: 202-731-0677 Number of years as a Menlo Park resident: 9 months

Occupation: Policy

Email address: nell.triplett@gmail.com

Business Address/Telephone No: Silicon Valley Leadership Group, 2001 Gateway Place Ste. 101E, San Jose, CA 95110

Internet Posting

If I am appointed, the City is authorized to post the following information on the City's website:

	YES	NO
Home Address:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Home Phone:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Mailing Address (if not home address):	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Business Address:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Business Phone:	<input checked="" type="checkbox"/>	<input type="checkbox"/>
E-mail:	<input checked="" type="checkbox"/>	<input type="checkbox"/>

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COMMISSION/COMMITTEE APPLICATION

Please type or print clearly. You may attach additional pages, if necessary. This is a public document.

Date: 04/21/2013

Commission/Committee of Interest: Bicycle Commission

Name: Cindy Welton

Education:

BS University of Michigan 1985, Majors: Biology and German

MBA University of Southern California 1989, Majors: Entrepreneurial Studies and Management Consulting

Civic affiliations and community activities, including service on other commissions or committees:

Co-founder and Coordinator of SVBC/Stanford Trauma Roadway Safety Solutions Team

Event Organizer of Stanford Trauma's Bike Safety Summit, November 2011

Co-founder of Canary Challenge benefiting Stanford Cancer Institute www.canarychallenge.com

Former volunteer at Bing Nursery School, Las Lomas Elementary School, La Entrada Middle School and Menlo Atherton High School

Describe your understanding of the responsibilities of the commission/committee that you are applying for and how your personal, community or professional experience relate to these responsibilities:

To advise the Council on matters related to cycling consistent w/ our City's goal to make MP a bicycle-friendly community where all roadway users thrive. For the past 2 yrs following my husband's near fatal cyclist v. motorist crash I have worked tirelessly to more fully understand and address the need for safety on our roadways for cyclists, pedestrians and motorists. As an active parent who has raised 3 children in MP, a consultant at SHC, wife of a Stanford surgeon, triathlete and kid's triathlon coach I have a broad perspective on the issues and complexities we face relating to our BPAC's mission.

Describe why you want to serve on this commission/committee and what you hope to accomplish as a member:

I would like to see MP become a safer and more pleasant place to raise families, work, and live active, healthy lifestyles. I would like to see us develop our cycling and pedestrian infrastructures and roadway culture so they become inviting, safe and respectful to support people having viable choices as to how to navigate our city. I believe they should support safe routes for all citizens including school children, recreational athletes, commuters, and walkers. I would like to see the east and west sides of MP become more connected and accessible for everyone. I would also like to see consistent and comprehensive bicycle education/safety courses offered regularly in all MP schools, both private and public, and through our community events and partners. MP has the potential to lead by example in terms of developing and supporting safe, sensible and respectful multi-modal transportation options.

Terms

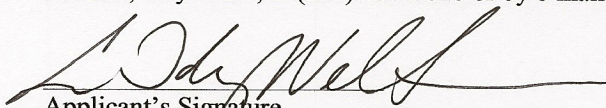
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Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
If Appointed Term ends: _____	

PERSONAL INFORMATION

Name: Cindy Welton

Residence Address: 1318 Bellair Way, Menlo Park, CA 94025-6615

(Note: Residency within the City limits is required)

Telephone No: 650.387.4376 cell Number of years as a Menlo Park resident: 19 years

Occupation: Event and Retail Business Consultant at Stanford Hospital & Clinics

Email address: clwelton@comcast.net

Business Address/Telephone No: 650.387.4376 cell

Internet Posting

If I am appointed, the City is authorized to post the following information on the City's website:

	YES	NO
Home Address:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Home Phone:	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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COMMISSION/COMMITTEE APPLICATION

Please type or print clearly. You may attach additional pages, if necessary. This is a public document.

Date: April 7, 2013

Commission/Committee of Interest: Transportation Commission

Name: Philip T. Mazzara

Education:

- B.S. in Political Science from the United States Naval Academy – 2000
- JD/MBA from Stanford Law School and the Stanford Graduate School of Business - 2015 (*expected*)

Civic affiliations and community activities, including service on other commissions or committees:

- Lieutenant Commander in the United States Naval Reserve (IRR)
- Pro bono legal assistance to low-income clients who had wage and hour claims or were victims of identity theft
- Volunteer Court Appointed Special Advocate (CASA) for children who were victims of abuse or neglect (2008 to 2011)
- Member of Trinity Episcopal Church in Menlo Park
- Member of the National Court Appointed Special Advocate Association
- Member of the Stanford Law Veterans Organization

Describe your understanding of the responsibilities of the commission that you are applying for and how your personal, community or professional experience relate to these responsibilities:

Members of the Transportation Commission serve in an advisory role to the City Council and provide a conduit for citizen input on “matters related to the adequacy and improvement of all types of transportation within and across the City.” The Transportation Commission is currently focused on three priorities: (1) a signage and branding project to assist downtown businesses; (2) review of the Neighborhood Traffic Management Program; and (3) review of the Street Light program.

As a former Navy pilot and current civilian private pilot, I have a good macro sense of how national, state, and local transportation systems fit together. Similarly, my military service took me across the United States and around the world, so I have an appreciation for different types of transportation and various approaches to transportation issues. From the train system in Japan and the bus system in Virginia Beach to the carpooling program at the Pentagon and the bicycling program at Stanford, I’ve seen it all and will be able to bring a new perspective to our local programs.

Describe why you want to serve on this commission and what you hope to accomplish as a member:

Menlo Park is a special place, and my family and I decided to make the city our home after I left military. But I still feel strongly about public service, and serving on the Transportation Commission is a small way I can get involved in local government and do that. Our 8-year-old daughter, Madalyn, attends public school here, and I want to ensure Menlo Park remains a safe community and great place to live.

In addition to working on the aforementioned Transportation Commission priorities, I would like to see if there are ways we can improve the walkability of the downtown area around Santa Cruz and El Camino. This would not only be helpful to residents and visitors of Menlo Park but could also spur economic

development in that area. In addition, I'd like to continue to update the Safe Routes to School projects for local schools. This is a fantastic program that can decrease traffic congestion, increase student safety, and promote physical activity. Once plans are finalized, I'd like to help the City Council and local schools develop ideas to increase public awareness about the routes and to incentivize participation in the program. Implementation is a continual process as families move into the community and young children become school age. Finally, I'd like to coordinate with the Bicycle Commission to see about the feasibility of having bike boxes at some of the city's busier intersections. These lane marking are a great way to help drivers and cyclists share the road, to improve safety, and to support a sustainable form of transportation.

Terms

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Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
If Appointed Term ends: _____	

PERSONAL INFORMATION

Name: Philip T. Mazzara

Residence Address: 40 Kent Place / Apt #1 / Menlo Park, CA 94025

Telephone No: 650-485-2715 **Number of years as a Menlo Park resident:** 1.5 years

Occupation: Graduate Student

Email address: ptmazzara@outlook.com

Business Address/Telephone No: Stanford University / 770-335-1156

Internet Posting

If I am appointed, the City is authorized to post the following information on the City's website:

	YES	NO
Home Address:	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Home Phone:	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Mailing Address (if not home address):	<input type="checkbox"/>	<input type="checkbox"/>
Business Address:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Business Phone:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
E-mail:	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Philip T. Mazzara

40 Kent Place / Apt 1 • Menlo Park, CA 94025 • (650) 485-2715 • ptmazzara@outlook.com

EDUCATION

Stanford Law School / Stanford Graduate School of Business **Palo Alto, CA**
Juris Doctor / Master of Business Administration (JD/MBA) Candidate **Expected 2015**

- Hilmer Oehlmann, Jr. Award for outstanding performance in Legal Research and Writing
- Stanford Law Veterans Organization leader for Yellow Ribbon program initiative

United States Naval Academy **Annapolis, MD**
Bachelor of Science, Political Science **2000**

- Regimental Officer (*leadership position awarded to only five percent of senior year midshipmen*)
- Pi Sigma Alpha National Political Science Honor Society

EXPERIENCE

Zynga Inc. **San Francisco, CA**
Summer Intern in the Office of the General Counsel **June - September 2012**

- Prepared contracts between Zynga.org, the company's philanthropic initiative, and partner charities enabling players to make charitable contributions through Zynga games to nonprofit organizations

Lieutenant Commander / Strike Fighter Squadron 106 **Virginia Beach, VA**
F/A-18 Instructor Pilot, Deputy for Future Operations, Quality Assurance Officer **2008 - 2011**

- Led a team of 11 schedulers and data analysts that managed the squadron's training schedule
- Streamlined scheduling inefficiencies resulting in a 15 percent increase in jet simulator usage within the framework of the existing contract and an estimated savings of more than \$1 million
- Led a 20-person team that oversaw the squadron's compliance with maintenance directives
- Developed and implemented an in-depth audit of all maintenance work centers in preparation for an annual maintenance inspection resulting in the department's highest inspection score in ten years

Honors:

- Ranked #1 of 33 junior officers during tour
- Awarded the Military Outstanding Volunteer Service Medal and the Navy Commendation Medal

Lieutenant / Strike Fighter Squadron 27 **Atsugi, Japan**
F/A-18 Mission Commander, Maintenance Division Officer, Strike Tactics Officer **2005 - 2007**

- Led a division of 50 sailors responsible for aircraft electronics and ordnance to a perfect score during an annual weapons inspection
- Mentored and tutored junior sailors resulting in the promotion of seven sailors and selection of one sailor to the Blue Angels enlisted team
- Handpicked by commanding officer as Officer-in-Charge (OIC) of an international detachment of aircraft and personnel to Guam – the only Lieutenant to serve in this capacity in three years

Honors:

- Ranked #1 of 8 junior officers during tour
- Awarded the Navy Commendation Medal and the Navy Achievement Medal

Lieutenant Junior Grade / Navy Flight School

Florida, Texas, California

Student Naval Aviator

2000 - 2004

- Designated as a naval aviator and qualified in the T-45A Goshawk and F/A-18E Super Hornet
- Selected to remain on staff as an instructor pilot immediately after graduation from flight school

Honors:

- Selected to the “Commodore’s List” for finishing in the top ten percent of flight school graduates
- Awarded Navy Achievement Medal

COMMUNITY SERVICE

Legal Pro Bono - Stanford, California

2012 - Present

- Provided legal assistance to low-income clients who had wage and hour claims or were victims of identity theft

Court Appointed Special Advocate - Virginia Beach CASA

2008 - 2011

- Volunteered more than 400 hours as a child advocate, attended court hearings, and provided judges with recommendations for safe, permanent homes for abused and neglected children
- Monitored court orders and ensured timely implementation of court-ordered services relating to the child's education, mental health, and physical well-being

Community Relations Projects - Navy Chaplain’s Office

2005 - 2007

- Organized volunteers and visited the Sydney Children’s Hospital in Australia during two ports of call to spend time with seriously ill children and their families

Midshipmen Action Group - Naval Academy Foundation

1996 - 1997

- Tutored elementary school children as part of the “Mids for Kids” program during weekly visits to a local school

ADDITIONAL INFORMATION

Military Awards (Listed)

- Military Outstanding Volunteer Service Medal for sustained, significant service (2008 - 2010)
- Navy Commendation Medal (gold star in lieu of second award) for meritorious service (2008 - 2010)
- Navy Commendation Medal for meritorious service (2005 - 2007)
- Navy Achievement Medal (gold star in lieu of second award) for professional achievement (2005)
- Navy Achievement Medal for professional achievement (2002 - 2004)

Hobbies

- Running - completed four half-marathons
- FAA private pilot’s license with a single-engine aircraft rating

Family

- Married to the former Misty Walker of Lilburn, Georgia for 12 years
- Father of eight-year-old Madalyn Mazzara



COMMISSION/COMMITTEE APPLICATION

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Date: 1/15/13

Commission/Committee of Interest: Transportation Commission

Name: Michael Meyer

Education:
3 years of college, did not finish degree.

Civic affiliations and community activities, including service on other commissions or committees:
Served for 4 years on transportation commission. Coached MA Little League for the past 2 years. Currently a Bicycle Commissioner.

Describe your understanding of the responsibilities of the commission/committee that you are applying for and how your personal, community or professional experience relate to these responsibilities:
My understanding of the responsibilities of the Transportation Commission is that it advises the City Council in matters related to all modes of Transportation throughout the City. I believe that my personal experience of multimodal transport throughout the city brings valuable data points to the decision making process.

Describe why you want to serve on this commission/committee and what you hope to accomplish as a member:
I want to bring a more multimodal point of view to the Commission as I believe it tends to be more car centric than it should be.

Terms

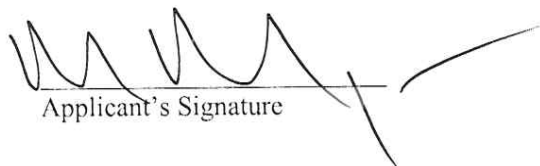
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Serving on a commission or committee may require one or two night meetings per month, with each meeting averaging three to four hours. You may also be asked to serve on additional subcommittees. Members are expected to attend all meetings. Attendance at less than two-thirds of scheduled meetings may result in removal by the Council. Commissioners are not paid for their volunteer service. General information related to the charge of the commissions and committees and their schedules are shown on the attachment. More specific information may be obtained by viewing the City's website at http://www.menlopark.org/city_commissions.html and by contacting the staff liaison.

Information about the Appointment Process

The application process may take from six weeks to two months. Vacancies are advertised for approximately 30 days with a specific filing deadline. Deadlines may be extended. Please return your application, along with any attachments, to the City Clerk, at the address listed below. Applications are kept on file for one year. The City Council will review all applications, may contact you individually or may decide to hold interviews. All appointments will be made by nomination and vote of the City Council at a Council meeting. Questions about the application process should be directed to Margaret S. Roberts, City Clerk, at (650) 330-6620 or by e-mail at MSRoberts@menlopark.org.


Applicant's Signature

Return to the City Clerk, City of Menlo Park,
701 Laurel Street, Menlo Park, CA 94025
(Phone: (650) 330-6620 or e-mail at MSRoberts@menlopark.org)

OFFICE USE ONLY	
Application Received: _____	Address Verified in City Limits: <input type="checkbox"/> By: _____ Initials
Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
Considered by City Council: _____	Appointed: Yes <input type="checkbox"/> No <input type="checkbox"/>
If Appointed Term ends: _____	

PERSONAL INFORMATION

Name: Michael Meyer

Residence Address: 222 Lawrie Ave

(Note: Residency within the City limits is required)

Telephone No: ⁶⁵⁰ 321 5691 Number of years as a Menlo Park resident: 10

Occupation: IT

Email address: wiredmeyer@gmail.com

Business Address/Telephone No: 2775 Sand Hill Rd Ste 100 ⁶⁵⁰ 233 8139

Internet Posting

If I am appointed, the City is authorized to post the following information on the City's website:

	YES	NO
Home Address:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Home Phone:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Mailing Address (if not home address):	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Business Address:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Business Phone:	<input type="checkbox"/>	<input checked="" type="checkbox"/>
E-mail:	<input checked="" type="checkbox"/>	<input type="checkbox"/>

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PUBLIC WORKS DEPARTMENT

Council Meeting Date: May 7, 2013
Staff Report #: 13-073

Agenda Item #: D-1

CONSENT CALENDAR: **Award of a Contract for Street Sweeping Services to Contract Sweeping Services, Inc. in the Amount of \$638,512.70 for Five Years and Authorize the City Manager to Extend the Contract for up to an Additional Four Years**

RECOMMENDATION

Staff recommends that the City Council award a contract for street sweeping services to Contract Sweeping Services, Inc. in the amount of \$ 638,512.70 (\$127,702.54 per year) for five years and authorize the City Manager to extend the contract for up to an additional four years, up to the yearly budgeted amount.

BACKGROUND

City street sweeping is a valued service provided to residents and businesses and required for compliance with the Storm Water Pollution Prevention Program.

This street sweeping schedule began in 2006 and has been very beneficial for residents and the City. During the winter when most of the trees drop their leaves the street sweeping occurs twice a week versus the previous schedule of once a week and has reduced the amount of ponding on streets due to leaves blocking inlets. In addition, this schedule has reduced staff's time in picking up leaves when the piles of leaves are too large for the sweepers.

The street sweeping schedule has also been coordinated with Recology's garbage and recycling material pick-up schedule.

ANALYSIS

On February 28, 2006, the City Council awarded an initial four-year contract that was extended based on the high quality of service provided by Universal Sweeping Services (name changed to Contract Sweeping Services, Inc.). The contract terminates in June 2013.

Staff advertised for bids for the street sweeping contract in March 2013. On April 11, 2013, five (5) bids were submitted and opened for the Street Sweeping Services. The

lowest bidder for the project, Contract Sweeping Services, Inc., submitted a bid in the amount of \$638,512.70 for five years or at an annual cost of \$127,702.54. The annual cost is approximately \$15,000 less than the current annual cost. Attachment A provides the bid summary. The City is currently using Contract Sweeping Services, Inc., for street sweeping services and is satisfied with their performance.

The proposed new contract will begin July 1, 2013, and end June 30, 2018, with the option for the City to extend it for four additional one-year terms. Any price increase for an additional one-year term will be according to the Consumer Price Index and may not exceed five percent in any year. Either party may terminate the contract with 90 days' notice.

IMPACT ON CITY RESOURCES

The City's street sweeping services are funded through a combination of the General Fund (\$55,000) which is reimbursed by Measure M (\$10 Vehicle Registration Fee), and the Landscaping/Tree Assessment Fund (\$95,000) for a total of \$150,000 to be funded in Fiscal Year 2013-14.

In future years, staff will budget for these services as part of the City's annual budget process.

POLICY ISSUES

The recommendation does not represent any change to existing City policy.

ENVIRONMENTAL REVIEW

Environmental review is not required.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed, at least 72 hours prior to the meeting.

ATTACHMENTS

- A. Bid Summary

Report prepared by:
Ruben Niño
Assistant Public Works Director



BID SUMMARY

STREET SWEEPING SERVICES

BID OPENING DATE: Thursday, April 11, 2013

Apparent Low Bidder

	CONTRACTOR	BID AMOUNT
1.	Contract Sweeping Services Inc.	\$638,512
2.	Flagship Facility Services	\$709,721
3.	CleanSweep Environmental	\$724,460
4.	Webco Sweeping	\$785,446
5.	CleanStreet	\$856,180

*** Pending City Council Approval**

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PUBLIC WORKS DEPARTMENT

Council Meeting Date: May 7, 2013
Staff Report #: 13-078

Agenda Item #: D-2

CONSENT CALENDAR: **Award a Contract for the Sidewalk Trip Hazard Removal Project to Precision Emprise, Inc., in the Amount of \$80,000, and Authorize a Total Budget of \$100,000 for Construction, Inspection and Construction Administration**

RECOMMENDATION

Staff recommends that the City Council award a contract for the Sidewalk Trip Hazard Removal Project to Precision Emprise, Inc., in the amount of \$80,000, and authorize a total budget of \$100,000 for Construction, Inspection and Construction Administration.

BACKGROUND

The Sidewalk Repair Program is conducted by the City to eliminate tripping hazards on sidewalks, parking strips and curbs/gutters. Typically trip hazards are created when the roots of City street trees extend underneath the sidewalk and uplift only a portion of the sidewalk or concrete and create a vertical offset between two adjacent segments of sidewalk. The Sidewalk Repair Program is implemented to eliminate the vertical offsets. The City Sidewalk Repair Program has two phases: the first phase is the Sidewalk Trip Hazard Removal Project and the second is the Sidewalk Repair Project.

The Sidewalk Trip Hazard Removal Project is conducted in two phases. The first phase, the contractor will address minor tripping hazards (that range in size between ¼" to 1¾") by implementing a horizontal saw cutting method that leaves a smooth, uniform surface that meets State and Federal accessibility requirements. In order to achieve this cut, the contractor utilizes a special concrete saw that ensures cut slopes are smooth and comply with accessibility requirements.

The second phase consists of removing and replacing sections of sidewalks, curbs, and parking strips in order to eliminate severe tripping hazards. Since the Sidewalk Trip Hazard Removal Project is less invasive than the Sidewalk Repair Program, this latter task is performed in areas where the structural integrity of the sidewalk has not been compromised and saw cutting would eliminate the trip hazard. Therefore, the second phase will be completed later this year. To guarantee that the trip hazards are addressed throughout the City, the Sidewalk Repair Program is cycled through the

City's five work zones. Each year, staff attempts to eliminate trip hazards in 1 or 2 of the work zones based on budget allocated.

ANALYSIS

This year, the Sidewalk Trip Hazard Removal Project will address trip hazards in Zone 5 (Belle Haven neighborhood), followed by Zone 1 (Downtown-West Menlo, Central Business District, Allied Arts), as funding allows. Precision Emprise, Inc. will inspect all sidewalk sections in the selected work zone and remove all trip hazards meeting the given tolerances. Where the vertical offset is too severe for horizontal saw cutting, the contractor will notify staff and these locations will be added to a future Sidewalk Repair Project.

On April 25, 2013, two (2) bids were submitted and opened for the Sidewalk Trip Hazard Removal Project. The lowest bidder for the project, Precision Emprise, Inc., submitted a unit price bid in the amount of \$22.70/in-ft. Attachment A provides the bid summary. Staff has checked the background and references of Precision Emprise, Inc., and is satisfied with its past performance. Precision Emprise, Inc. has previously performed work for the City and staff is satisfied with their performance.

The project was bid on a unit price basis, and the contractor will perform trip hazard removal work up to the budgeted amount of \$80,000.

Schedule

The Sidewalk Trip Hazard Removal Project is expected to begin in May 2013 with completion anticipated in May 2013.

IMPACT ON CITY RESOURCES

The construction budget for the Sidewalk Trip Hazard Removal Project consists of the following:

Construction contract amount	\$ 80,000
Construction Administration and Inspection Services	<u>\$ 20,000</u>
Total Construction Budget	\$ 100,000

Sufficient funds are available in the General CIP Fund and the Sidewalk Assessment Fund for the Sidewalk Repair Project. The project was budgeted in the FY 2012-13.

POLICY ISSUES

The recommendation does not represent any change to existing City policy.

ENVIRONMENTAL REVIEW

The project is categorically exempt under Class I of the current State of California Environmental Quality Act Guidelines, which allows minor alterations and replacement of existing facilities.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed, at least 72 hours prior to the meeting.

ATTACHMENTS

A. Bid Summary

Report prepared by:
Michel Jeremias
Senior Civil Engineer

Fernando Bravo
Engineering Services Manager

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BID SUMMARY

SIDEWALK TRIP HAZARD REMOVAL

BID OPENING DATE: Thursday, April 25, 2013

Apparent Low Bidder

	CONTRACTOR	Item Price Per Unit (IN-FT)
1.	Precision Emprise, Inc. d.b.a. Precision Concrete Cutting	\$22.70
2.	BPR, Inc.	\$29.50

*** Pending City Council Approval**

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PUBLIC WORKS DEPARTMENT

Council Meeting Date: May 7, 2013
Staff Report #: 13-076

Agenda Item #: D-3

CONSENT CALENDAR: Authorize the Public Works Director to Accept the Work performed by Golden Bay Construction Inc., for the Safe Routes to Hillview Middle School Project

RECOMMENDATION

Authorize the Public Works Director to accept the work performed by Golden Bay Construction Inc., for the Safe Routes to Hillview Middle School Project.

BACKGROUND

On August 28, 2012, the City Council awarded a contract for the Safe Routes to Hillview Middle School Project to Golden Bay Construction, Inc. The project consisted of the installation of three (3) lighted crosswalk systems and ADA (Americans with Disabilities Act) curb ramp improvements at the intersections of Santa Cruz Avenue and Olive Street, Cotton Street, and San Mateo Drive, in conjunction with establishing safe routes for bicycling and walking to Hillview Middle School students.

ANALYSIS

The project enhanced the pedestrian and bicycle safety at the marked crosswalks on Santa Cruz Avenue by installing in-pavement lighted crosswalk systems on Santa Cruz Avenue at its intersections with San Mateo Drive, Cotton Street and Olive Street, while complying with ADA requirements for intersection improvements. All the work was deemed complete and in accordance with the plans and specifications.

The project was completed within the approved project budget.

Contractor: Golden Bay Construction, Inc.
3826 Depot Road
Hayward, CA 94545

IMPACT ON CITY RESOURCES

Construction Budget

Construction Contract Amount	\$ 107,398
Contingency	\$ 16,110
Total Construction Budget	\$ 123,508

Construction Expenditures

Construction Contract Budget	\$ 123,508
Construction Contract expenditures	\$ 122,495
Balance remaining	\$ 1,013

POLICY ISSUES

The recommendation does not represent any change to existing City policy.

ENVIRONMENTAL REVIEW

The project is categorically exempt under Class I of the current State of California Environmental Quality Act Guidelines.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed at least 72 hours prior to the meeting.

ATTACHMENTS

None

Report prepared by:
René Baile
Transportation Engineer



PUBLIC WORKS DEPARTMENT

**Council Meeting Date: May 7, 2013
Staff Report #: 13-077**

Agenda Item #: D-4

CONSENT CALENDAR: **Authorize the Public Works Director to Accept the Work Performed by Amland Corporation for the Middlefield Road at Linfield Drive Lighted Crosswalk Improvement Project**

RECOMMENDATION

Authorize the Public Works Director to accept the work performed by Amland Corporation for the Middlefield Road at Linfield Drive Lighted Crosswalk Improvement Project.

BACKGROUND

On August 28, 2012, the City Council awarded a contract for the Middlefield Road at Linfield Drive Lighted Crosswalk Improvement Project to Amland Corporation. The project consisted of installation of one (1) lighted crosswalk system and ADA (Americans with Disabilities Act) curb ramp improvements at the intersections of Middlefield Road with Linfield Drive.

ANALYSIS

The project enhanced the pedestrian and bicycle safety at the intersection of Middlefield Road and Linfield Drive by installing an in-pavement lighted crosswalk system at this intersection, while complying with ADA requirements for intersection improvements. All the work was deemed complete and in accordance with the plans and specifications.

The project was completed within the approved project budget.

Contractor: Amland Corporation
1401 Felipe Avenue
San Jose, CA 95122

IMPACT ON CITY RESOURCES

Construction Budget

Construction contract amount	\$	45,239
Contingency	\$	6,786
Total Construction Budget	\$	52,025

Construction Expenditures

Construction contract budget	\$	52,025
Construction contract expenditures	\$	42,156
Balance remaining	\$	9,869

POLICY ISSUES

The recommendation does not represent any change to existing City policy.

ENVIRONMENTAL REVIEW

The project is categorically exempt under Class I of the current State of California Environmental Quality Act Guidelines.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed, at least 72 hours prior to the meeting.

ATTACHMENTS

None

Report prepared by:
René Baile
Transportation Engineer



COMMUNITY DEVELOPMENT DEPARTMENT

Council Meeting Date: May 7, 2013
Staff Report #: 13-074

Agenda Item #: E-1

PUBLIC HEARING: Conduct a Public Hearing and Consider a Request for Use Permit, Architectural Control, Tentative Map, Heritage Tree Removals and Below Market Rate (BMR) Housing In-Lieu Fee Agreement for Six Detached Dwelling Units on Two Adjacent Parcels at 1273 and 1281 Laurel Street

RECOMMENDATION

Staff recommends that the City Council conduct a public hearing and concur with the recommendation of the Planning Commission to approve the following actions associated with the proposed six-unit development at 1273 and 1281 Laurel Street in the R-3 (Apartment) zoning district:

1. **Environmental Review:** Make findings that the redevelopment of the site is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current State CEQA Guidelines;
2. **Use Permit:** Make findings and approve a Use Permit for construction of three or more units in the R-3 zoning district;
3. **Architectural Control:** Adopt findings and approve the Architectural Control for design review of the new buildings and site improvements;
4. **Tentative Map:** Make findings and approve the Tentative Map to create six condominium units on two legal lots;
5. **Heritage Trees:** Adopt a resolution approving the heritage tree removal permits for five trees (Attachment B); and
6. **Below Market Rate (BMR) Housing Agreement:** Adopt a resolution approving the BMR Housing Agreement for the payment of an in-lieu fee in accordance with the City's Below Market Rate Housing Program (Attachment C).

Since the development includes two legal parcels, a complete set of actions are required for each individual parcel, and are included in Attachment A.

BACKGROUND

The project site is located on Laurel Street between Oak Grove Avenue and Glenwood Avenue, across from Nativity School. A location map is provided in Attachment E. In addition to the school, parcels across the street are located within the Town of Atherton, and occupied by single-family residences. Parcels to the north, south, and west of the subject site, using Laurel Street in a north to south orientation, are also zoned R-3 and are occupied by a mixture of multi-family and single-family developments. The dominant land use pattern in the vicinity of the project is multi-family apartment development.

The applicant submitted an application, on September 28, 2012, for use permit and architectural control to demolish two existing, single-family homes on two adjacent legal parcels, and construct six detached dwelling units (three on each parcel) with a common driveway. The site is designed with six units, located in two rows on each side of a common 20-foot wide driveway. The project plans are included in Attachment H. In order to allow each unit to be owned individually, the applicant has applied for a tentative parcel map to create six condominium units, on two legal parcels. The proposed project would comply with all development regulations of the R-3 zoning district. The data summary table for the proposed project is included in Attachment F, and the project description is contained in the Planning Commission staff report (Attachment I).

The project site contains 18 trees (including five trees within the public right-of-way and two on a neighboring parcel), nine of which are of heritage size. The applicant has designed the proposed project around the heritage size live oak tree at the front of the parcel. In addition, two heritage size live oaks at the rear-right corner of the property are proposed to remain. The applicant is proposing to remove five heritage size trees, which are summarized in the table below:

Tree Number	Tree Type	Diameter	Location on Site	Condition	Basis for Removal Request
#2	Incense cedar	27 inches	Front middle	Poor	Health/Structure
#5	Mexican fan palm	22 inches	Back right-corner	Good	Construction
#6	Mexican fan palm	17 inches	Back right-corner	Good	Construction
#9	Coast live oak	42 inches	Middle-rear	Poor	Health/Possible hazardous
#12	Incense cedar	33 inches	Front-left side	Fair	Health/Structure

The City Arborist has reviewed the removal permits and given tentative approval to remove the trees, including the street tree. The applicant has submitted a landscape plan that shows the type, location, and size of existing and replacement trees on-site. The applicant is proposing to replace the five heritage trees with two Brisbane box trees

and three true green elm trees. The trees would be 24-inch box size plantings and would be planted along the side property lines, with two trees located along the left side property line of the site, and three located along the right side property line of the site.

The Below Market Rate (BMR) Housing Ordinance is applied to housing developments of five or more units. The applicant is proposing an in-lieu fee for the proposed project, due to the constraints of providing an on-site unit, which is discussed more in the Planning Commission staff report. Using the current BMR Guidelines, the in-lieu fee would be approximately 1.5 percent (1.5%) of the sale price per unit. In addition to the minimum in-lieu fee, the Developer is voluntarily proposing to increase the fee by one percent (1%) to an average of 2.5 percent (2.5%) per unit. The Housing Commission reviewed the applicant's proposed BMR in-lieu fee agreement on January 16, 2013, and recommended approval of the proposed BMR Agreement, 4-0, with Commissioner Dodick absent. The draft BMR in-lieu fee agreement is included in Attachment D.

The Planning Commission initially reviewed the proposed project (including the BMR in-lieu fee agreement) at its regular meeting of March 4, 2013. At the meeting, the Planning Commission heard testimony from the applicant and two members of the public in support of the project, considered six letters from members of the public regarding concerns about the heritage tree removals and the design of the project, and discussed the proposed project. At the meeting, the Planning Commission voted to continue the item with the following direction:

- Redesign Building One, Plan One to improve its orientation to the street and also deal with fenestration issues of the façade;
- Improve the massing of the roof structure for Plan One;
- Reconsider the use of applied materials, specifically the stone veneer, for the entire project; and
- Work with staff to reduce the driveway width and also utilize driveway materials to improve the design of the driveway.

Subsequently, the applicant modified the plans where possible, incorporating the input from the Planning Commission. The applicant's project description letter, which includes a response to the Planning Commission's direction, is contained in Attachment G, and explains the modifications to the project in more detail. The Planning Commission reviewed the modified project, including one additional item of correspondence identifying concerns about the heritage tree removals, at its regular meeting of April 8, 2013 and voted 6-0, with Commissioner O'Malley absent, to recommend that the City Council approve the proposed project, with the following modifications:

- The stone veneer should contain grout between the individual stones, instead of a dry stack design; and
- Add two screening trees between the units on the right side parcel (1281 Laurel Street).

The applicant has revised the project plans to include a stone veneer with grout between the individual stones, and has also revised the landscape plans to include two screening trees (24-inch box saucer magnolia trees) between the units on the right side parcel. Therefore, the conditions (Attachment A) have been updated to reference the project plans dated received April 17, 2013.

ANALYSIS

A complete discussion of the project proposal, requested land use entitlements, and other actions is included in the Planning Commission staff report dated April 8, 2013, which is included as Attachment I. The associated excerpt minutes are included as Attachment J.

IMPACT ON CITY RESOURCES

The project sponsor is required to pay planning permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

POLICY ISSUES

The Project does not require an amendment to the City's General Plan. The primary policy issues for the City Council to consider while reviewing the project are whether the required use permit, architectural control, and related findings can be made, along with the findings for the proposed heritage tree removal permits. In addition, the Council should consider the proposed BMR in-lieu fee agreement, including the voluntary increase in the in-lieu fee amount proposed by the developer.

ENVIRONMENTAL REVIEW

The proposed project is consistent with the General Plan land use designation for the site (Medium Density Residential) and would comply with the R-3 (Apartment) district land use regulations. The site is less than five acres in size and was previously developed, therefore would not result in additional impacts to endangered species. The proposed development would not result in noise impacts greater than typical residential development, and the project would meet all civil and hydrology requirements of the City Engineering Division. The Transportation Division reviewed the project and determined that the increase in four units would not result in a significant impact to the roadway network or the level of service (LOS) at the nearest intersection. As such, the proposed project is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current California Environmental Quality Act (CEQA) Guidelines. The trip generation analysis is available for public review at the Department of Community Development.

PUBLIC NOTICE

Public notification consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

ATTACHMENTS

- A. Draft Findings, Actions, and Conditions for Approval
- B. Draft Resolution for Heritage Tree Removal
- C. Draft Resolution for BMR Agreement
- D. Draft BMR In-Lieu Fee Agreement
- E. Location Map
- F. Project Data Table
- G. Project Description Letter
- H. Project Plans
- I. Planning Commission Staff Report, dated April 8, 2013, without attachments
- J. Planning Commission Meeting Draft Excerpt Minutes, dated April 8, 2013

Report prepared by:
Kyle Perata
Assistant Planner

Arlinda Heineck
Community Development Director

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**1273 and 1281 Laurel Street
Draft Findings, Actions, and Conditions for Approval
May 7, 2013**

1273 Laurel Street

1. Adopt a finding that the redevelopment of the site is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current State CEQA Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:
 - a. The general appearance of the structure is in keeping with the character of the neighborhood.
 - b. The development will not be detrimental to the harmonious and orderly growth of the City.
 - c. The development will not impair the desirability of investment or occupation in the neighborhood.
 - d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.
 - e. The property is not within any Specific Plan area, and as such no finding regarding consistency is required to be made.
4. Approve the use permit and architectural control requests subject to the following **standard** conditions:
 - a. Development of the project shall be substantially in conformance with the plans prepared by AJS Architecture and Planning, consisting of 29 plan sheets, dated received April 17, 2013, inclusive of the recommendations by the Planning Commission on April 8, 2013, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
 - b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.

- c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
 - d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. Landscaping shall properly screen all utility equipment that is installed outside of a building and that cannot be placed underground. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
 - e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for the review and approval of the Engineering Division.
 - f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to issuance of a grading, demolition or building permit.
 - g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.
 - h. Prior to building permit issuance, the applicant shall submit proposed landscape and irrigation documentation as required by Chapter 12.44 (Water-Efficient Landscaping) of the City of Menlo Park Municipal Code. If required, the applicant shall submit all parts of the landscape project application as listed in section 12.44.040 of the City of Menlo Park Municipal Code. This plan shall be subject to review and approval by the Planning and Engineering Divisions. The landscaping shall be installed and inspected prior to final inspection of the building.
5. Adopt findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.
 6. Adopt a Resolution approving the five heritage tree removal permits (Attachment B).
 7. Adopt a Resolution approving the Below Market Rate Housing In-Lieu Fee Agreement, recommended by the Housing Commission on January 16, 2013, and recommended by the Planning Commission on April 8, 2013. (Attachment C).

1281 Laurel Street

1. Adopt a finding that the redevelopment of the site is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current State CEQA Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:
 - a. The general appearance of the structure is in keeping with the character of the neighborhood.
 - b. The development will not be detrimental to the harmonious and orderly growth of the City.
 - c. The development will not impair the desirability of investment or occupation in the neighborhood.
 - d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.
 - e. The property is not within any Specific Plan area, and as such no finding regarding consistency is required to be made.
4. Approve the use permit and architectural control requests subject to the following **standard** conditions:
 - a. Development of the project shall be substantially in conformance with the plans prepared by AJS Architecture and Planning, consisting of 29 plan sheets, dated received April 17, 2013, inclusive of the recommendations by the Planning Commission on April 8, 2013, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
 - b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.
 - c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.

- d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. Landscaping shall properly screen all utility equipment that is installed outside of a building and that cannot be placed underground. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
 - e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for the review and approval of the Engineering Division.
 - f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to issuance of a grading, demolition or building permit.
 - g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.
 - h. Prior to building permit issuance, the applicant shall submit proposed landscape and irrigation documentation as required by Chapter 12.44 (Water-Efficient Landscaping) of the City of Menlo Park Municipal Code. If required, the applicant shall submit all parts of the landscape project application as listed in section 12.44.040 of the City of Menlo Park Municipal Code. This plan shall be subject to review and approval by the Planning and Engineering Divisions. The landscaping shall be installed and inspected prior to final inspection of the building.
5. Adopt findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.
 6. Adopt a Resolution approving the five heritage tree removal permits (Attachment B).
 7. Adopt a Resolution approving the Below Market Rate Housing In-Lieu Fee Agreement, recommended by the Housing Commission on January 16, 2013, and recommended by the Planning Commission on April 8, 2013. (Attachment C).

RESOLUTION NO.

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO
PARK APPROVING HERITAGE TREE REMOVAL PERMITS FOR THE
PROPERTY LOCATED AT 1273 AND 1281 LAUREL STREET**

WHEREAS, on October 3, 2012, the City of Menlo Park (“City”) received an application from Forrest Mozart of Laurel Oaks LP (“Project Sponsor”) for removal of 5 heritage trees at the property located at 1273 and 1281 Laurel Street (“Project Site”) as more particularly described and shown in “Exhibit A”; and

WHEREAS, the requested tree removals are necessary in order to redevelop the Project Site; and

WHEREAS, the removal of Heritage Trees within the City is subject to the requirements of Municipal Code Chapter 13.24, Heritage Trees; and

WHEREAS, the City Arborist reviewed the requested tree removals on October 25, 2012; and

WHEREAS, the City Arborist determined that the five Heritage Trees are impeding the redevelopment of the Project Site; and

WHEREAS, the City Arborist determined that two of the Heritage Trees proposed for removal were of inferior species and that the majority of the Heritage Trees are in fair to poor health; and

WHEREAS, the City Arborist determined that the proposed five 24-inch box replacement trees would be appropriate for the site; and

WHEREAS, all required public notices and public hearings were duly given and held according to law; and

WHEREAS, after notice having been lawfully given, public hearings were scheduled and held before the Planning Commission of the City of Menlo Park on March 4 and April 8, 2013 whereat all persons interested therein might appear and be heard; and

WHEREAS, the Planning Commission of the City of Menlo Park having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter voted affirmatively to recommend to the City Council of the City of Menlo Park to approve the Heritage Tree Removal Permits; and

WHEREAS, after notice having been lawfully given, a public hearing was scheduled and held before the City Council of the City of Menlo Park on May 7, 2013 whereat all persons interested therein might appear and be heard.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Menlo Park hereby approves the Heritage Tree Removal Permits, which shall be valid for six months from the date of approval.

I, Margaret S. Roberts, City Clerk of Menlo Park, do hereby certify that the above and foregoing Council Resolution was duly and regularly passed and adopted at a meeting by said Council on the seventh day of May, 2013, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this seventh day of May, 2013.

Margaret S. Roberts, MMC
City Clerk

LEGAL DESCRIPTION

Real property in the City of Menlo Park, County of San Mateo, State of California, described as follows:

PARCEL ONE:

BEGINNING AT A POINT ON THE SOUTHERLY LINE OF LAUREL AVENUE DISTANT FOUR HUNDRED AND SEVENTY-FIVE (475) FEET EASTERLY FROM THE INTERSECTION OF SAID LINE OF LAUREL AVENUE WITH THE EASTERLY LINE OF GLENWOOD AVENUE, AS SAID AVENUES ARE SHOWN AND DELINEATED ON THE MAP HEREINAFTER MENTIONED; THENCE FROM SAID POINT OR BEGINNING EASTERLY ALONG THE SAID LINE OF LAUREL AVENUE SEVENTY-FIVE (75) FEET TO THE EASTERLY LINE OF THE LOT FOURTEEN (14); THENCE AT RIGHT ANGLES SOUTHERLY AND ALONG THE SAID EASTERLY LINE OF LOT FOURTEEN (14) ONE HUNDRED AND SEVENTY-FIVE (175) FEET ONE AND THREE-FOURTHS (1-3/4) INCHES, MORE OR LESS, TO THE SOUTHERLY LINE OF SAID LOT; THENCE WESTERLY ALONG THE SOUTHERLY BOUNDARY LINE OF LOTS FOURTEEN (14) AND FIFTEEN (15) TO A POINT WHICH IS DISTANT AT RIGHT ANGLES ONE HUNDRED AND SEVENTY-TWO (172) FEET AND THREE (3) INCHES, MORE OR LESS, FROM THE POINT OF BEGINNING; THENCE NORTHERLY AND PARALLEL TO THE EASTERLY BOUNDARY OF SAID LOT FIFTEEN (15) ONE HUNDRED AND SEVENTY-TWO (172) FEET AND THREE (3) INCHES, MORE OR LESS, TO THE POINT OF BEGINNING.

BEING ALL OF LOT FOURTEEN (14) AND THE ADJOINING PORTION OF LOT FIFTEEN (15) IN BLOCK B AS SAID LOTS AND BLOCK ARE SHOWN AND DELINEATED ON THAT CERTAIN MAP ENTITLED "MAP OF EDGAR MILLS TRACT, MENLO PARK, SAN MATEO COUNTY", FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN MATEO COUNTY, STATE OF CALIFORNIA ON SEPTEMBER 14, 1887 IN BOOK "A" OF ORIGINAL MAPS AT PAGE 51 AND COPIED INTO BOOK 1 OF MAPS AT PAGE NINETY-FOUR (94).

PARCEL TWO:

BEGINNING AT A POINT ON THE SOUTHERLY LINE OF LAUREL AVENUE DISTANT THEREON EASTERLY FOUR HUNDRED (400) FEET FROM THE INTERSECTION OF THE SOUTHERLY LINE OF LAUREL AVENUE WITH THE EASTERLY LINE OF GLENWOOD AVENUE; THENCE SOUTHERLY ALONG THE DIVIDING LINE BETWEEN LOTS 16 AND 17, 169 FEET 4 1/4 INCHES; THENCE EASTERLY ALONG THE SOUTHERLY LINE OF LOTS 16 AND 15, 75 FEET; THENCE NORTHERLY TO A POINT ON THE SOUTHERLY LINE OF LAUREL AVENUE WHICH IS DISTANT 75 FEET FROM THE POINT OF BEGINNING; THENCE AT RIGHT ANGLES WESTERLY ALONG THE SOUTHERLY LINE OF LAUREL AVENUE 75 FEET TO THE POINT OF BEGINNING. BEING ALL OF LOT 16 AND THE ADJOINING 25 FEET FRONTING ON LAUREL AVENUE, OF LOT 15 IN BLOCK "B" AS SHOWN ON THE MAP OF THE EDGAR MILLS TRACT, AT MENLO PARK, WHICH SAID MAP WAS FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN MATEO COUNTY, STATE OF CALIFORNIA, SEPTEMBER 14, 1887 IN BOOK 1 OF MAPS AT PAGE 94.

PARCEL ONE

APN: 061-401-080; JPN: 061-040-401-08a

PARCEL TWO

APN: 061-401-070 JPN: 061-040-401-07A

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RESOLUTION NO.

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO
PARK APPROVING THE BELOW MARKET RATE HOUSING
AGREEMENT BETWEEN THE CITY OF MENLO PARK AND LAUREL
OAKS LP**

WHEREAS, the City Council of the City of Menlo Park ("City") has read and considered that certain Below Market Rate Housing Agreement ("BMR Agreement") between the City and Laurel Oaks LP ("Developer") that satisfies the requirement that Developer comply with Chapter 16.96 of the City's Municipal Code and with the Below Market Rate Housing Program Guidelines.

NOW, THEREFORE, BE IT RESOLVED, that the public interest and convenience require the City to enter into the Below Market Rate Housing Agreement; and

BE IT FURTHER RESOLVED that the City Council hereby approved the agreement and authorizes the City Manager to execute said agreement.

I, Margaret S. Roberts, City Clerk of Menlo Park, do hereby certify that the above and foregoing Council Resolution was duly and regularly passed and adopted at a meeting by said Council on the seventh day of May, 2013, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this seventh day of May, 2013.

Margaret S. Roberts, MMC
City Clerk

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**BELOW MARKET RATE
IN LIEU FEE AGREEMENT**

This Below Market Rate In Lieu Fee Agreement ("Agreement") is made as of this ____ day of _____, 2013 by and between the City of Menlo Park, a California municipal corporation ("City") and Laurel Oaks LP, a limited partnership ("Developer"), with respect to the following:

RECITALS

A. Developer owns that certain real property located in the City of Menlo Park, County of San Mateo, State of California and more particularly described in Exhibit A, attached hereto ("Property"). The Property is commonly known as 1273 and 1281 Laurel Street, Menlo Park and consists of Assessor's Parcel Numbers 061-401-080 and 061-401-070.

B. Pursuant to City Municipal Code Chapter 16.96, the City's BMR Housing Ordinance ("BMR Ordinance"), and the BMR Housing Program Guidelines ("Guidelines"), Developer is required to enter into this Agreement for the benefit of the City to ensure compliance with the City's BMR Ordinance and Guidelines, which is a prerequisite to obtaining final development approvals and "Final Inspection" of the units from the Building Division.

C. Developer plans to develop the Property with and to sell a total of six market rate units ("Project").

D. The BMR Ordinance and Guidelines require the Developer to provide ten percent (10%) of the six market rate units or (rounding up) one Below Market Rate ("BMR") unit; however, if one BMR unit were provided for this project, the development would potentially be eligible for one bonus market rate unit for a total of seven units.

E. Given the existing zoning ordinance regulations, the size of the parcels, and the existing heritage trees on-site, if the bonus market rate unit were developed, it would place a constraint on the feasibility of the Project, and therefore, Developer seeks to take advantage of Section 4.3.2 of the Guidelines where it states that a developer may pay a residential in lieu fee if the City determines it is infeasible to provide an on-site BMR Unit.

F. The Developer has proposed to increase the average in-lieu fee from 1.5 percent per unit, to 2.5 percent per unit, which would increase the in-lieu fee from \$108,000 to approximately \$180,000.

G. Given the constraints of this Project and the proposed increase in the total in-lieu fee amount, the City will allow Developer to take advantage of the payment of residential in lieu fees, as described in this Agreement.

NOW, THEREFORE, the parties agree as follows:

1. Developer will pay an average in-lieu fee of 2.5 percent per unit, which is greater than the BMR Ordinance requirements that result in an average of 1.5 percent per unit (one percent for units 1, 2, and 3, and two percent for units 4, 5, and 6). Therefore, the Developer agrees to pay an average in-lieu fee of 2.5 percent per unit, for a projected total in-lieu fee of \$180,000, which represents an approximately \$72,000 increase from the required fee of the BMR Ordinance.

2. Upon the close of escrow for each of the six market rate units, Developer shall pay the City the applicable in lieu fee (2.5 percent of sale price) for the unit sold to be deposited into the City of Menlo Park BMR Housing Fund. The obligation to pay such in lieu fees shall constitute a lien against the title to such unit, enforceable against the units for which such fees are due together with interest at the rate of ten percent (10%) per annum accruing from the close of escrow until paid, if not paid at the closing. As part of the payment of the in lieu fee for a unit, the City shall execute any document reasonably required by a title insurance company to provide marketable title to the unit free of the lien imposed by this Agreement.

3. This Agreement shall be binding on and inure to the benefit of the parties hereto and any respective assigns and or owners of the property. Either party may freely assign this Agreement without the consent of the other. However, to be valid, an assignment of this Agreement must be in writing.

4. This Agreement is a covenant running with the land for the benefit of the City and all lands owned by the City within the limits of the City.

5. If any legal action is commenced to interpret or enforce this Agreement or to collect damages as a result of any breach of this Agreement, the party prevailing shall be entitled to recover all reasonable attorneys' fees and costs incurred in such action from the other party.

6. Developer shall record this Agreement in the Office of the County Recorder of San Mateo County prior to the recording of a final subdivision map for any portion of the Property and shall provide a copy of such recorded agreement to the City.

7. This Agreement shall be governed by and construed in accordance with the laws of the State of California.

8. The terms of this Agreement may not be modified or amended except by an instrument in writing executed by each of the parties hereto.

9. The exhibits attached hereto are hereby incorporated herein by this reference for all purposes.

10. This Agreement supersedes any prior agreements, negotiations and communications, oral or written, and contains the entire agreement between the

parties as to the subject matter hereof.

11. If any portion of this Agreement as applied to either party or to any circumstances shall be adjudged by a court to be void or unenforceable, such portion shall be deemed severed from this Agreement and shall in no way affect the validity or enforceability of the remaining portions of this Agreement.

12. Any and all obligations or responsibilities of Developer under this Agreement shall terminate upon the recording of the final grant deeds conveying the last of the six (6) market rate units to third party purchasers in accordance with the terms and provisions of this Agreement and the payment of all the requisite in lieu fees are paid through escrow, as set forth in Section 4.3 of the Guidelines.

13. To the extent of any conflict between the terms and provisions of the Guidelines and the terms and provisions of this Agreement, the terms and provisions of this Agreement shall prevail.

IN WITNESS WHEREOF, the parties hereto have executed this Agreement as of the day and year first written above.

City of Menlo Park

Developer:

By: _____
Alex McIntyre
City Manager

By: _____
Forrest Mozart
Laurel Oaks, LP

List of Exhibits:

Exhibit A: Property Description

EXHIBIT A
LEGAL DESCRIPTION

Order Number: 4102-3983117
Page Number: 6

LEGAL DESCRIPTION

Real property in the City of Menlo Park, County of San Mateo, State of California, described as follows:

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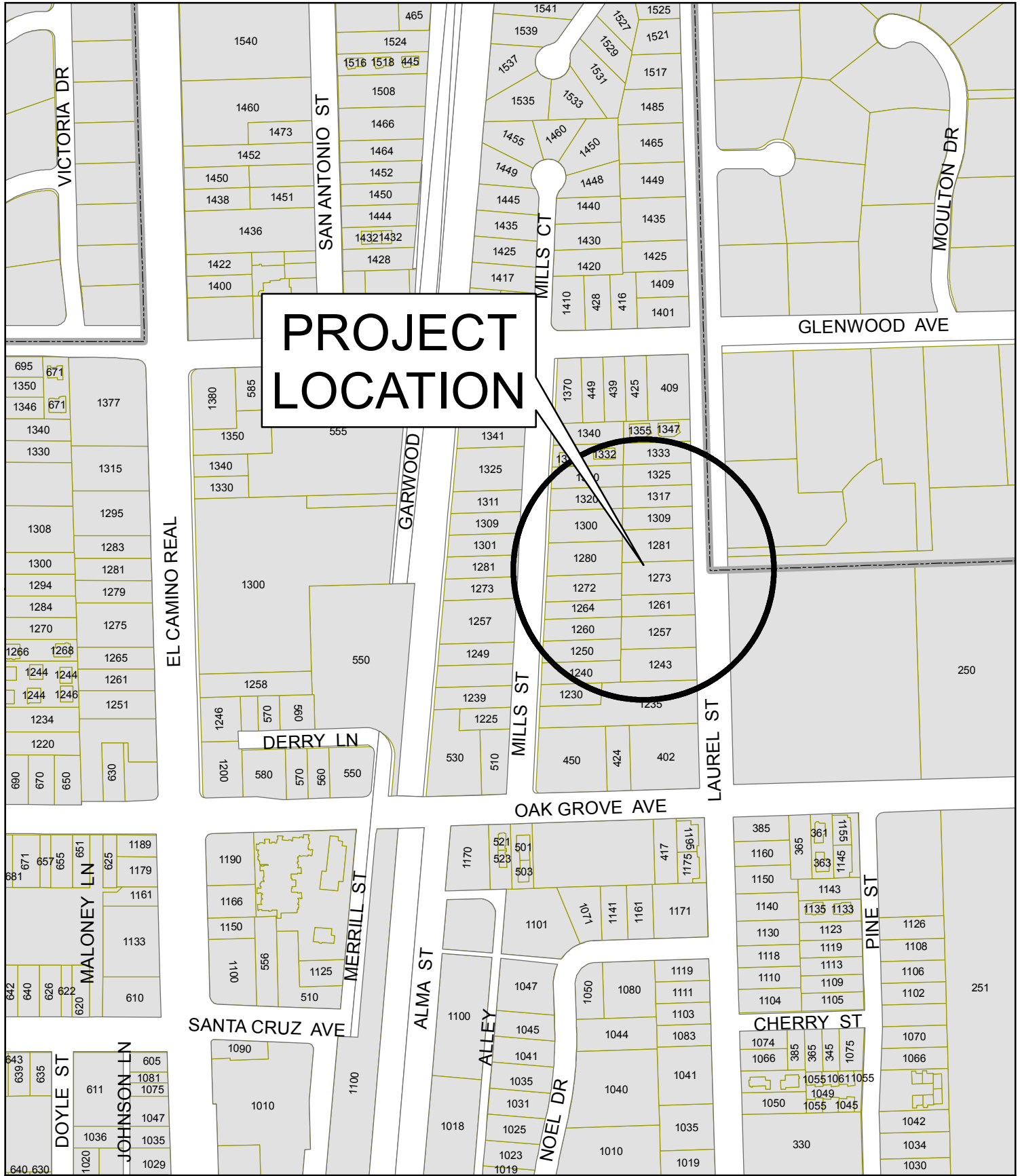
PARCEL ONE

APN: 061-401-080; JPN: 061-040-401-08a

PARCEL TWO

APN: 061-401-070 JPN: 061-040-401-07A

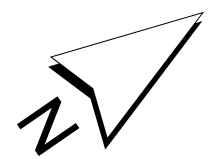
First American Title



**PROJECT
LOCATION**

CITY OF MENLO PARK
 LOCATION MAP
 1273 & 1281 LAUREL STREET

DRAWN: KTP CHECKED: KTP DATE: 05/07/13 SCALE: 1" = 300' SHEET: 1



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Project Data Summary Table 1273 and 1281 Laurel Street

	PROPOSED PROJECT		EXISTING COMBINED SITE (1273 & 1281 LAUREL STREET)	ZONING ORDINANCE*
	1273 LAUREL STREET	1281 LAUREL STREET		
Lot area	13,024 sf	12,808 sf	25,832 sf	7,000 sf min.
Lot width	75.0 ft.	75.0 ft.	150 ft.	70 ft. min.
Lot depth	173.7 ft.	170.8 ft.	172.3 ft.	100 ft. min.
Setbacks				
Front	20.2 ft.	39.1 ft.	37 ft.	20 ft. min.
Rear	15.0 ft.	15.3 ft.	19 ft.	15 ft. min.
Side (left)	18.0 ft.	15.0 ft.	23 ft.	10 ft. min.
Side (right)	14.0 ft.	12.0 ft.	32 ft.	10 ft. min.
Distance Between Bldgs				
Main Buildings on Site	20 ft. min	20 ft. min	N/A ft. min	20 ft. min
Main Buildings on Adjacent Properties	28 ft. Right 25 ft. Left 16 ft. Rear	20 ft. Right 28 ft. Left 33 ft. Rear	40 ft. Right 30 ft. Left 108 ft. Rear	20 ft. Right 20 ft. Left 20 ft. Rear
Building coverage	30 % 3,905.9 sf	28.3 % 3,620.0 sf	12.3 % 3,171 sf	30 % max. 7,749.6 sf max.
FAR (Gross Floor Area)	44.9 % 5,853.5 sf	44.9 % 5,756.4 sf	9.4 % 2,433 sf	45 % 11,624.4 sf
Landscaping	50.5 % 6,585.4 sf	52.7 % 6,753.5 sf	74.8 % 19,325 sf	50 % min. 12,916.0 sf
Paving	19.5 % 2,532.7 sf	19.0 % 2,434.5 sf	12.9 % 3,336 sf	20 % 5,166.4 sf
Square footage by floor, per unit	722.8 sf/1 st floor 1,228.4 sf/2 nd floor 440.4 sf/garage	851.0 sf/1 st floor 1,068.0 sf/2 nd floor 242.7 sf/garage	2,289 sf/1 st floor 738 sf/garage 144 sf/accessory	
Square footage of individual units**	2,391.6 sf	2,161.7 sf	3,171 sf	
Building height maximum	23.0 ft.	24.8 ft	12 ft.	35 ft. max.
Parking	9 spaces	6 spaces	4 Spaces	12 spaces
Note: Areas shown highlighted indicate a nonconforming or substandard situation.				
Trees	# of existing Heritage trees 12***	# of existing non-Heritage trees 6****	# new trees	5
	# of Heritage trees to be removed 5	# of non-Heritage trees to be removed 3	Total # of trees	15

*The zoning ordinance calculation is for the entire site, inclusive of both parcels.

**Square footage of individual units does not calculate covered porches as the porches differ by unit. Please see site area calculations on Sheet A1.3 of the plan set.

***One heritage tree is a street tree and three heritage size trees are located on adjacent properties.

****Four non-heritage trees are City street trees and one non-heritage size street tree is proposed to be removed as part of the project.

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March 25, 2013

Response to Planning Commission direction regarding design of 1273 & 1281 Laurel Street

1. Building One, Plan One has been redesigned to improve its orientation to Laurel Street. The porch size has been slightly adjusted to be able to change the roof to two identical feature gables perpendicular to each other, intersecting at the corner. This is a traditional Craftsman style treatment to a corner porch, and addresses Laurel Street while managing to remain different from the Plan Two porch. Additionally, the fenestration (windows) facing Laurel Street have been completely rethought, creating a picture window flanked by operable sidelights in the living room, topped by an architectural (wood framed) decorative canopy. Also, the kitchen has been redesigned to accommodate a window facing the street, and the bedroom and bathroom windows upstairs have been enlarged. A foundation-like wainscot base of stone veneer has been added along this entire side of the main body of the building, complementing the porch post bases and the column-like pilasters anchoring the corners of the garage on the other side. The small pop-outs at the two ends of the wrap-around porch are now differentiated from the principal massing forms of the house with the use of a contrasting siding material, board and battens, vs. the primary stucco material used elsewhere.
2. The massing of the roof structure for Plan One has been improved in three ways. The primary change, which only applies to house number One (the one closest to Laurel Street) is the porch roof change discussed above. The formerly plain look of the shed roof sloping down toward the street has been replaced with the new gable, which intersects at the corner with the previously existing perpendicular gable. This creates a 'dynamic' wrap-around corner effect which relates to the first and second floor corner windows above and below. The second change is that the hipped corner at the front upper level has been changed to a gable (due to P.C. input). The third roof massing change is the one on the left side of the houses, where the one-foot jog in the wall plane now continues up through the roof above. Note that it is not possible to create any side-facing gables along these facades facing each other within the project site due to the lack of room on the site to separate the buildings more than 20 feet, as would be required by the zoning code. This significantly limits options for roof massing.
3. Stone and brick masonry are traditionally and very commonly used as accent materials on Craftsman style houses on the West Coast. Traditional uses are as porch columns, porch post bases, site walls, foundations and wainscots, and chimneys. In all cases the material is at the base of the structure, starting at the ground and going up. Never used on upper levels above other materials. That is the idea here with both plans One and Two. Significant additional stone veneer has been added to the street-facing side of house number One, which ties the main body of the building visually into the porch post bases and garage-corner columns previously featuring stone.
4. The MPFPD standard for private roads and driveways serving 3 or more residential occupancies (section 4) requires "all-weather roads with a minimum width of 20 feet" per a memo provided by Karl Schneider of the district. That being the case, the intent is to make that 20 feet as aesthetically pleasing as possible (as well as environmentally friendly) with significant use of interlocking permeable pavers, alternating with sections of concrete as necessary for long term durability. See the site plans for layout. See photo below for an example of the type of pavers intended for this project, pending verification that they can meet fire dept. requirements.



City of Menlo Park
701 Laurel Street
Menlo Park, CA 94025

Project Description

The Laurel Oaks development is a proposed new community to be located on approximately 25,832 sq.ft parcel on Laurel Street in Menlo Park. The property consists of two parcels, 1273 and 1281 Laurel. Each parcel contains 1 single-family home on an oversized lot. Both of these homes are in bad condition and redevelopment would suit the community. The current property is an extreme “eye sore” to the community and is not in good health. Both 1273 and 1281 Laurel Street are zoned R-3. The property is bordered to the North and South by R3 zoning and also to the west by R3 apartments. Across the street is R1 single family homes and the School of Nativity.

The Laurel Oaks property is well suited for a single-family development. We are proposing 6 homes with 2 story high quality wood construction. The square footages of the proposed six houses are an average of 1,869sq.ft.per the F.A.R. 1273 and 1281 Laurel Street will both have 3 units with a shared driveway easement running between the two parcels. In order to prevent on-street parking, the 3 homes on 1273 Laurel will each have a two-car garage (of less than 450 square feet) with no tandem parking. The 3 homes on 1281 Laurel street will have 1 covered parking stall and

adjacent to the covered spot will have an uncovered stall for their exclusive use. This will be stated in the CC&R's along with on-site signage. We are also proposing three onsite guest parking stalls that are located on the 1273 parcel. We find this an attractive selling point and a convenience for future buyers since the City of Menlo Park doesn't allow overnight street parking.

The first level of each unit would be comprised of the garage, the entry, kitchen, and the family's living space. The 2nd level will be comprised of the master bedroom and secondary bedrooms. There are two different floor plans. Floor plan 1, which is located on the 1281 parcel, consists of the 1 car covered parking along with 4 beds and 3 baths. Floor plan 2, which is located on the 1273 parcel, consists of the 2 car covered parking along with 4 beds and 2.5 baths.

By proposing six units we are not fully "maximizing" the yield in the R3 zoning. But, given the extremely restrictive setbacks, amount of trees we are saving, and designing the entire project with these trees in mind the project could not support another unit. Developing anything less than six units would make the project financially infeasible. The layout of six units being oriented around the driveway creates a sense of community. The architectural style of the proposed houses is designed to fit well within the eclectic mix of buildings in the surrounding neighborhood. It could be described as "contemporary craftsman", in that the massing forms, low pitched roofs with wide overhangs, groups of vertically proportioned windows, and combination of stucco with siding on the walls are all characteristic of the Craftsman style, while the trims and details are streamlined in a more contemporary way. We've incorporated porches and

variations in roof forms and building massing as much as possible within the strict regulations of the zoning code. With 3 different color schemes it will look unique but all tie together.

The landscaping of this project is something I take very seriously. Having grown up in Menlo Park and currently reside less than a mile away, I understand the importance of the trees on this site and have become a tree advocate. I have developed a few projects in Palo Alto where the trees are a focal point of the project and they have turned out very successful. I have also strayed away from many projects because I didn't feel comfortable developing around trees. This is why early in the process before I acquired 1273 and 1281 Laurel; I met with the city arborist, Brian Henry, along with my arborist to discuss these trees. We discussed in great detail the Heritage Oak trees and how to work around them and preserve them to make sure they would thrive long after this project is completed. These trees will truly make this project a staple of Menlo Park living. My main concern was Oak tree (9) and how this could be preserved. This tree is in poor condition with extensive cavities and moth larva damage. We came to the conclusion that this tree along with cedars (2 and 12), Orange tree (3), Loquat (8), and two miniature Palm trees (5 and 6) that are insignificant in value could be replaced with 3 true green elm trees and 2 brisbane box trees. We are also proposing the removal of a non-heritage size London plane tree (street tree) for the driveway curb cut.

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LAUREL OAKS

California Communities

1273 & 1281 Laurel Street

Menlo Park, California

Location Map



Vicinity Map



Project Data (SQ. FT. U.O.N.)

	1273 LAUREL	1281 LAUREL	WHOLE SITE
TOTAL SITE AREA:	13,024	12,808	25,832
45% MAXIMUM FLOOR AREA: 45 x SITE AREA =	5,860.8	5,763.6	11,624.4
PROPOSED FLOOR AREA:	5,853.5	5,756.4	11,609.9
30% MAXIMUM BUILDING FOOTPRINT: 3 x SITE AREA =	3,907.2	3,842.4	7,749.6
PROPOSED BUILDING FOOTPRINT:	3,905.9	3,620.0	7,525.9
20% MAXIMUM DRIVEWAY + PARKING: 2 x SITE AREA =	2,604.8	2,561.6	5,166.4
PROPOSED DRIVEWAY + PARKING:	2,537.7	2,434.5	4,967.2
50% MINIMUM LANDSCAPE AREA: 5 x SITE AREA =	6,512.0	6,404.0	12,916.0
PROPOSED LANDSCAPE AREA:	6,585.4	6,753.5	13,338.9
HOUSE AREA PER F.A.R. (SQ. FT.)			
	PLAN 1	1,918.78	
	PLAN 2	1,951.16	
FOOTPRINT INC. GARAGE & PORCH (SQ. FT.)			
	PLAN 1, #1	1,267.67	
	PLAN 1, #2,3	1,181.67	
	PLAN 2, #4,5	1,281.86	
	PLAN 2, #6	1,342.17	

Index

- C5 COVER SHEET
- A1.1 AREA PLAN
- A1.2 ARCHITECTURAL SITE PLAN
- A1.3 SITE SOUND FOOTCAGE CALCULATIONS & STREETSCAPE
- A2.1 FLOOR PLAN 1
- A2.2 ROOF PLAN 1 & SQ.FT. CALCULATIONS 1
- A2.3 ELEVATIONS 1
- A2.4 ELEVATIONS 1
- A2.5 BRIDGING SECTIONS 1
- A3.1 FLOOR PLAN 2
- A3.2 ROOF PLAN 2 & SQ.FT. CALCULATIONS 2
- A3.3 ELEVATIONS 2
- A3.4 ELEVATIONS 2
- A3.5 BRIDGING SECTIONS 2
- A4.1 EXISTING PLAN & ELEVATIONS, 1273 LAUREL
- A4.2 EXISTING PLAN & ELEVATIONS, 1281 LAUREL
- CIVIL PLANS
- C1.1 TITLE SHEET
- C2.2 SITE PLAN
- C3.3 PRELIMINARY GRADING & DRAINAGE PLAN
- C4.4 LANDSCAPE AREA EXHIBIT
- C5.5 IMPERVIOUS AREA EXHIBIT
- C6.6 GRADING SPECIFICATIONS
- D1.1 DE: A/E
- C7.8 DE: A/E
- SURV TOPOGRAPHIC SURVEY
- LANDSCAPE PLANS
- L1.1 PRELIMINARY LANDSCAPE PLAN - SITE
- L1.2 PRELIMINARY LANDSCAPE PLAN - PLANTING
- L1.3 PRELIMINARY PLANT LIST
- L1.4 PRELIMINARY LANDSCAPE DE: A/E

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RECEIVED

REQUIRED PARKING: 2 PER UNIT, AT LEAST 1 COVERED
PROVIDED PARKING: PLAN 1: 1 COVERED, 1 UNCOVERED
PLAN 2: 2 COVERED
GUEST: 3 UNCOVERED

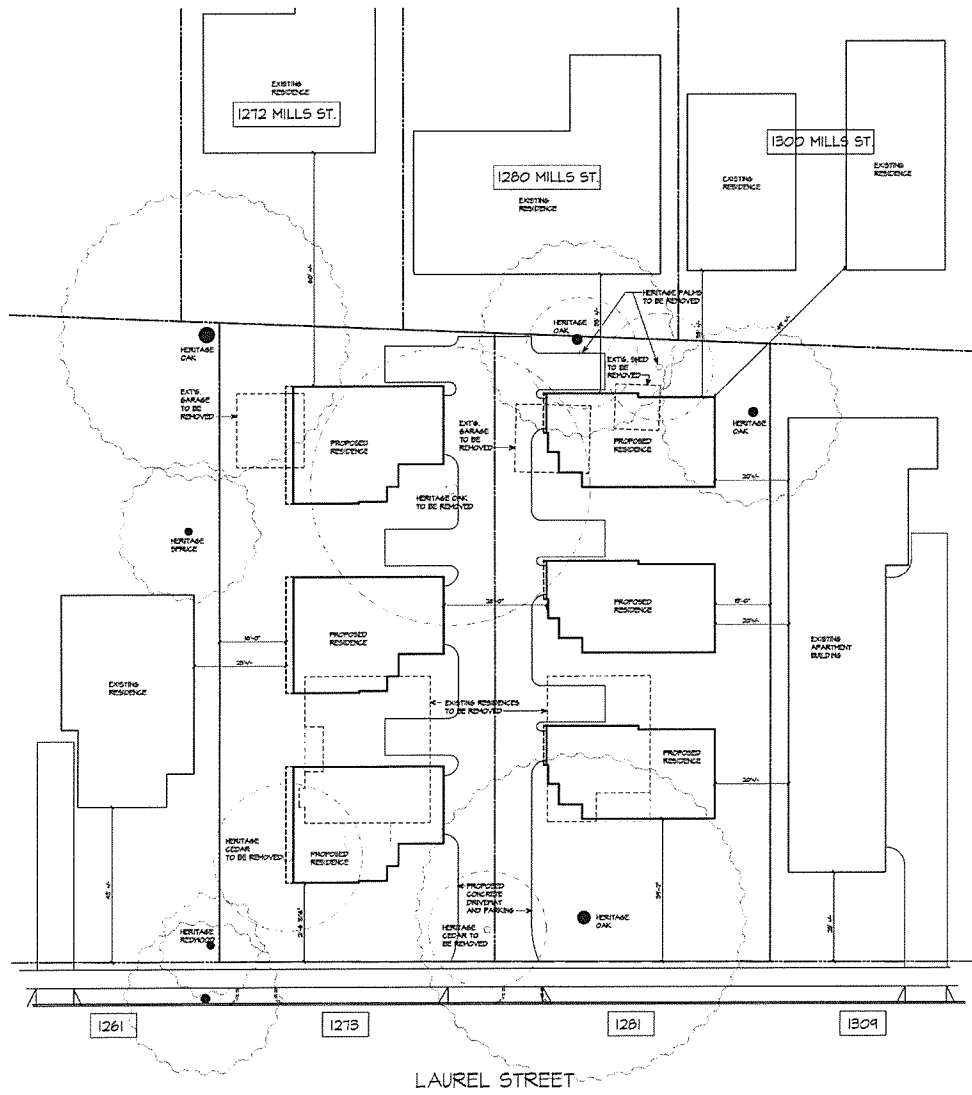
APR 17 2013

CITY OF MENLO PARK
PLANNING

MARCH 22 2013
Project No. 109.009

CS

COVER SHEET



AREA PLAN
 0 4 8 12 16 20 24 SCALE IN FEET

LAUREL OAKS



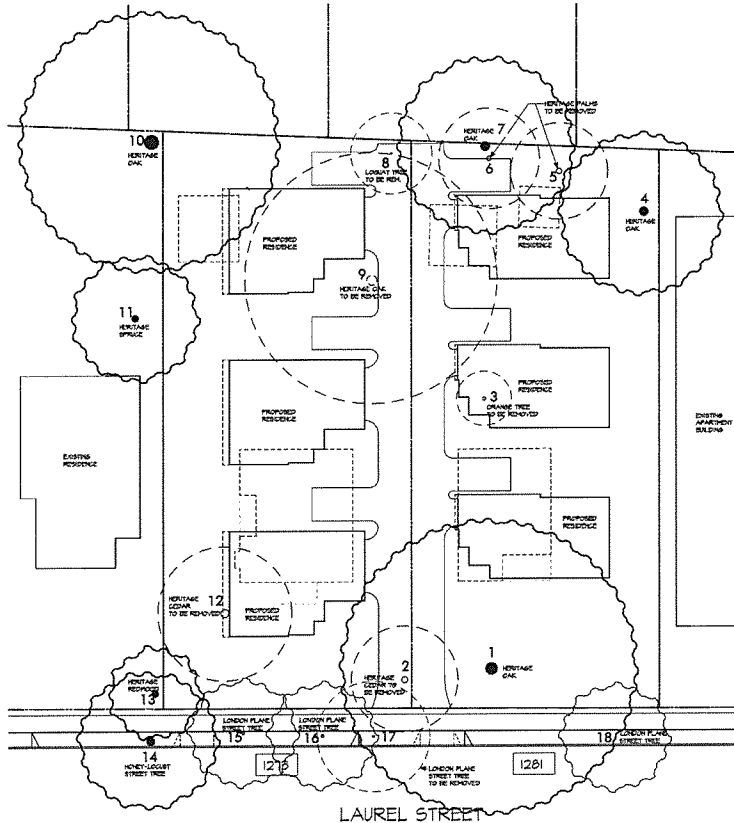
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AREA PLAN

CALIFORNIA COMMUNITIES - 1273 & 1281 LAUREL STREET - MENLO PARK CALIFORNIA

MARCH 22, 2013
 Project No. 129.009

A11



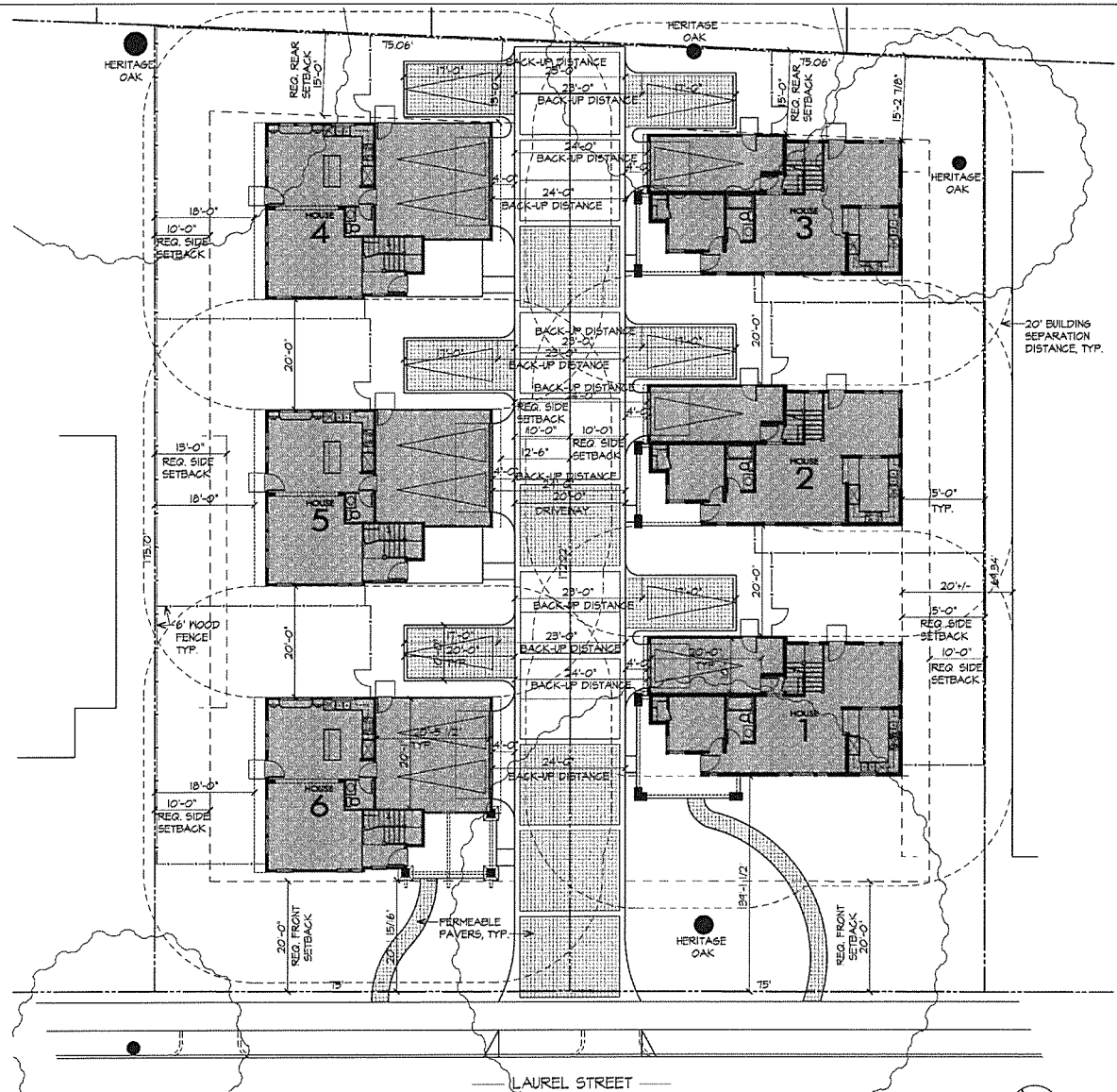
TREE DISPOSITION PLAN
NOT TO SCALE

Ray Morneau, Arborist ISA Certif. #WE-0132A 650.964.7664

TREE DISPOSITION / INVENTORY SUMMARY

TREE #	COMMON NAME	OVERALL CONDITION	HERITAGE TREE?	KEEP OR REMOVE?
1	Oak, Coast Live	fair	Yes	keep
2	Cedar, Incense	poor	Yes	remove
3	Orange	very poor	No	remove
4	Oak, Coast Live	fair	Yes	keep
5	Palm, Mes. Fan	good	Yes	remove
6	Palm, Mes. Fan	fair	Yes	remove
7	Oak, Coast Live	poor	Yes	keep
8	Logquat	poor	No	remove
9	Oak, Coast Live	poor	Yes	remove
10	Oak, Coast Live	fair	Yes, O/H	keep
11	Spruce	fair	Yes, O/H	keep
12	Cedar, Incense	poor	Yes	remove
13	Redwood, Coast	good	Yes, O/H	keep
14	Honey-Locust	poor	Yes, ST	keep
15	London Plane	fair	No, ST	keep
16	London Plane	fair	No, ST	keep
17	London Plane	fair	No, ST	keep
18	London Plane	very poor	No, ST	keep

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925.980.4103
Anthony Sarberovic, AIA



ARCHITECTURAL SITE PLAN
SEE CIVIL ENGINEERING PLANS AND LANDSCAPE PLANS FOR ADDITIONAL INFORMATION
SCALE IN FEET
0 4 8 12 16 20 24

LAUREL OAKS

ARCHITECTURAL SITE PLAN

SITE AREA CALCULATIONS - 1273 LAUREL ST. (SQ.FT.)

PLAN 2, #6	DRIVEWAY AND PARKING
4A: 17'-11 1/2" x 32'-0" = 574.61	A: 10'-0" x 17'-3 1/2" = 1,712.92 x1= 1,712.92
4B: 6'-0 1/2" x 31'-6" = 190.32	B: 20'-0" x 10'-0" = 200.00 x3= 600.00
4C: 2'-8" x 33'-6" = 89.34	C: 4'-0" x 16'-0" = 64.00 x3= 192.00
4D: 12'-4" x 33'-2" = 422.88	E: (4'-0" x 4'-0") - (π(4'-0") ² / 4) = 3.43 x5= 17.15
4E: 1'-7" x 33'-2" = 53.05	F: (2'-6" x 2'-6") - (π(2'-6") ² / 4) = 1.34 x5= 6.70
4F: 1'-1" x 2'-8" = 2.89	G: (2'-6" x 3'-8") - (π(1'-10") ² / 2) = 3.89 x1= 3.89
4G: 9" x 8'-2" = 6.13	
4H: 1'-1" x 2'-8" = 2.89	
TOTAL: 1,342.17 x 1 = 1,342.17	TOTAL DRIVEWAY AND PARKING: 2,532.66

PLAN 2, #4,5	LANDSCAPE AREA
2A: 17'-11 1/2" x 32'-0" = 574.61	TOTAL SITE AREA: 13,024
2B: 21'-2 1/2" x 31'-6" = 668.07	TOTAL BUILDING COVERAGE + TOTAL DRIVEWAY & PARKING: - 6,430.55
2C: 1'-10" x 21'-4" = 39.12	LANDSCAPE AREA: 6,585.45
TOTAL: 1,281.86 x 2 = 2,563.72	
TOTAL BUILDING COVERAGE: 3,905.89	

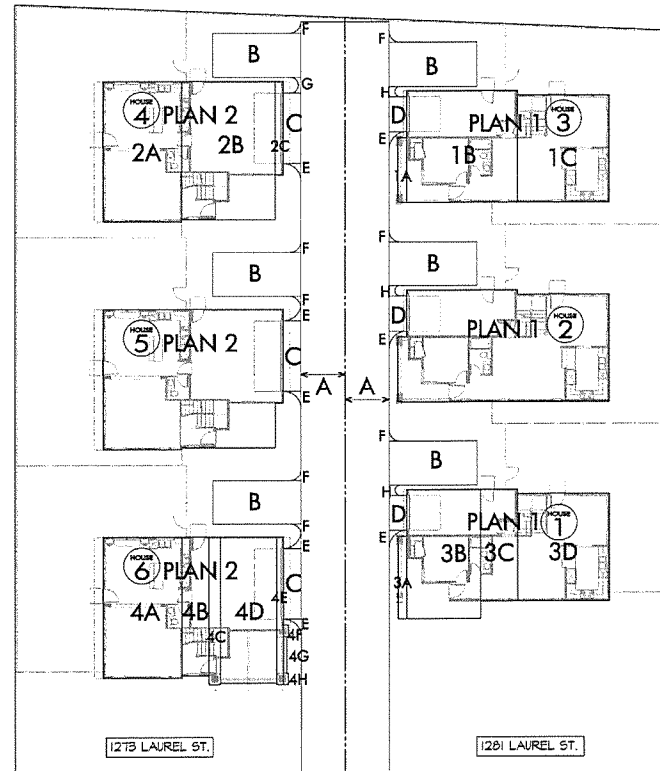
3,905.89 / 13,024 = 0.299899 29.99% BUILDING COVERAGE
 2,532.66 / 13,024 = 0.194461 19.45% DRIVEWAY AND PARKING
 6,585.45 / 13,024 = 0.50564 50.56% LANDSCAPE AREA

SITE AREA CALCULATIONS - 1281 LAUREL ST. (SQ.FT.)

PLAN 1, #1	DRIVEWAY AND PARKING
3A: 2'-0" x 18'-9" = 37.50	A: 10'-0" x 17'-3 1/2" = 1,712.92 x1= 1,712.92
3B: 16'-9" x 29'-6" = 494.13	B: 20'-0" x 10'-0" = 200.00 x3= 600.00
3C: 8'-5" x 25'-6" = 214.62	D: 4'-0" x 8'-0" = 32.00 x3= 96.00
3D: 20'-10" x 24'-6" = 510.42	E: (4'-0" x 4'-0") - (π(4'-0") ² / 4) = 3.43 x3= 10.29
TOTAL: 1,256.67 x 1 = 1,256.67	F: (2'-6" x 2'-6") - (π(2'-6") ² / 4) = 1.34 x3= 4.02
	H: (2'-6" x 2'-4 1/2") - (π(1'-2 1/4") ² / 2) = 3.73 x3= 11.19
	TOTAL DRIVEWAY AND PARKING: 2,434.42

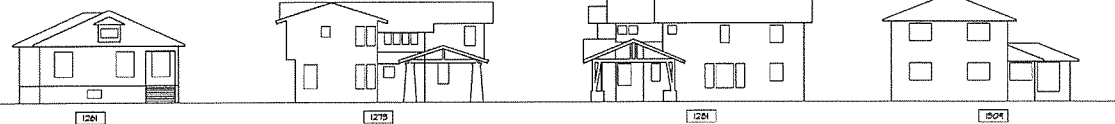
PLAN 1, #2,3	LANDSCAPE AREA
1A: 2'-0" x 14'-9" = 29.50	TOTAL SITE AREA: 12,808
1B: 25'-2" x 25'-6" = 641.75	TOTAL BUILDING COVERAGE + TOTAL DRIVEWAY & PARKING: - 6,054.43
1C: 20'-10" x 24'-6" = 510.42	LANDSCAPE AREA: 6,753.57
TOTAL: 1,181.67 x 2 = 2,363.34	
TOTAL BUILDING COVERAGE: 3,620.01	

3,620.01 / 12,808 = 0.282637 28.27% BUILDING COVERAGE
 2,434.42 / 12,808 = 0.19007 19.01% DRIVEWAY AND PARKING
 6,753.57 / 12,808 = 0.527293 52.72% LANDSCAPE AREA



SITE AREA DIAGRAM

0 4 8 16 24 SCALE IN FEET



STREETSCAPE DIAGRAM

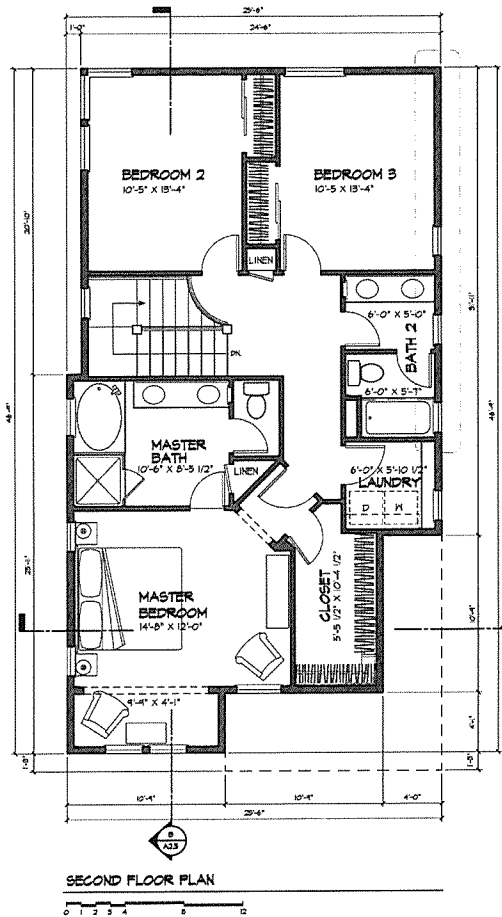
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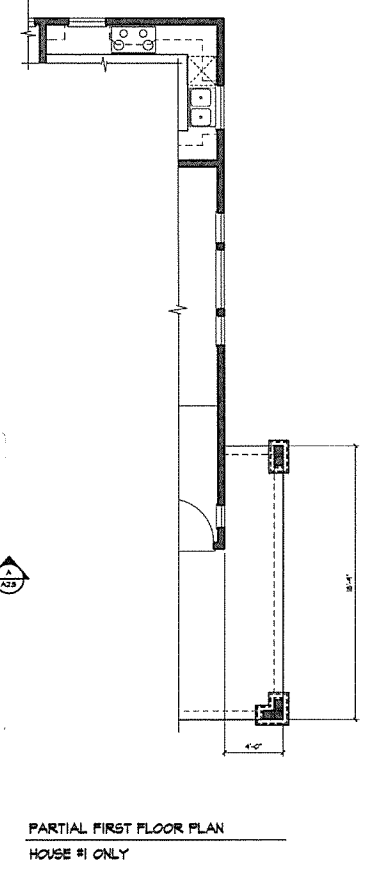
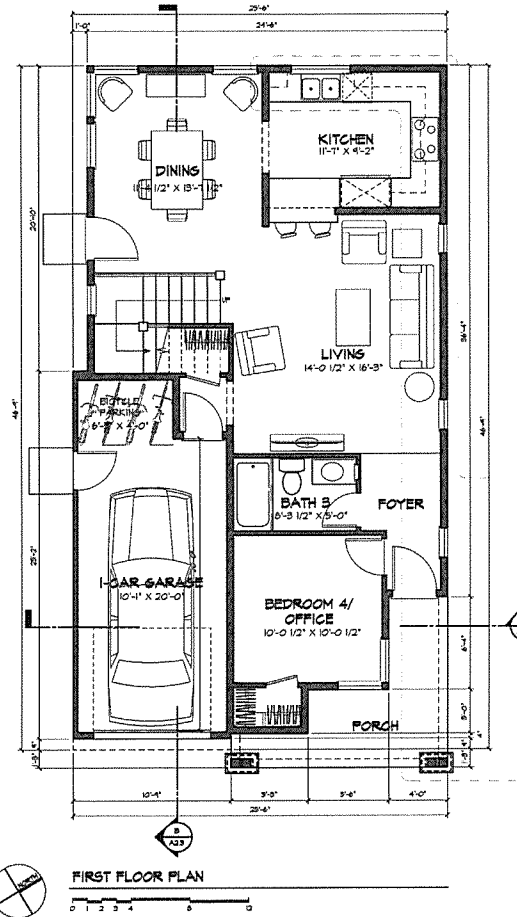
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 925.980.4103
 Anthony Sarberaria, AIA

LAUREL OAKS

SITE
 SQUARE FOOTAGE
 CALCULATIONS



PARTIAL SECOND FLOOR PLAN
HOUSE #1 ONLY



PLAN AREAS

FIRST FLOOR:	880.72 SQ. FT.
SECOND FLOOR:	1203.31 SQ. FT.
TOTAL:	1884.03 SQ. FT.
GARAGE:	242.74 SQ. FT.

PLAN AREAS PER BLDG. PERMITS FOR CALIF.

FIRST FLOOR:	880.72 SQ. FT.
SECOND FLOOR:	1203.31 SQ. FT.
TOTAL:	1884.03 SQ. FT.
GARAGE:	242.74 SQ. FT.

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LAUREL OAKS

PLAN ONE
FLOOR PLANS

CALIFORNIA COMMUNITIES - 1273 & 1281 LAUREL STREET - MENLO PARK CALIFORNIA

MARCH 22, 2018
Project No. 109-009

A2.1

FLOOR AREA CALCULATIONS

FIRST FLOOR

- A: 24'-6" x 21'-5 1/2" = 525.73 SF
- B: 18'-6 1/2" x 4'-0" = 74.84 SF
- C: 15'-0 1/2" x 10'-10 1/2" = 163.58 SF
- D: 11'-0 1/2" x 6'-4" = 69.94 SF
- E: 5'-6 1/2" x 3'-0" = 16.63 SF

TOTAL: 850.72 SF

SECOND FLOOR

- F: 24'-6" x 14'-2" = 347.04 SF
- G: 14'-7" x 6'-10" = 99.66 SF
- H: 6'-8 1/2" x 6'-8 1/2" = 46.29 SF
- J: 0'-5 1/2" x 6'-10" = 31.4 SF
- K: 12' x 2" = 0.17 SF
- L: 25'-6" x 10'-11" = 278.38 SF
- M: 21'-6" x 10'-9" = 231.13 SF
- N: 10'-9" x 4'-1" = 43.90 SF

TOTAL: 1,010.31 SF

TOP FLOOR STAIRWELL VOLUME

P: (9'-5 1/2" x 6'-10") ÷ 2 = 57.75 SF

TOTAL: 57.75 SF

GARAGE

- G1: 10'-5 1/2" x 20'-6 1/2" = 214.84 SF
- G2: 6'-9 1/2" x 4'-0" = 27.17 SF
- G3: 12" x 7 1/2" = 0.83 SF
- G4: 3 1/2" x 4" = 0.10 SF

TOTAL: 242.74 SF

F.A.R. SQUARE FOOTAGE

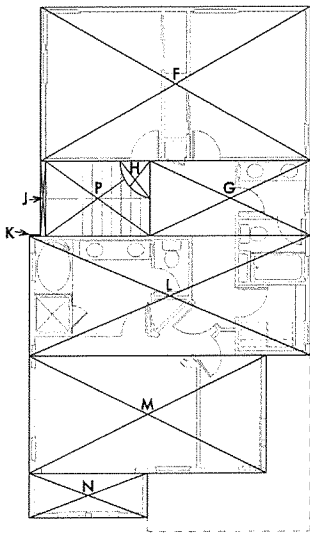
- FIRST FLOOR: 850.72 SF
- SECOND FLOOR: 1,010.31 SF
- STAIRWELL VOL: 57.75 SF

TOTAL: 1,918.78 SF → 1,919 SF

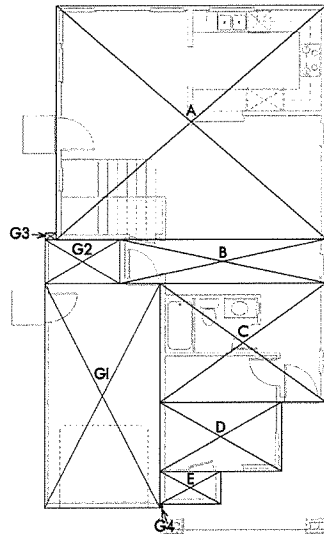
LIVEABLE SQUARE FOOTAGE

- FIRST FLOOR: 850.72 SF
- SECOND FLOOR: 1,010.31 SF

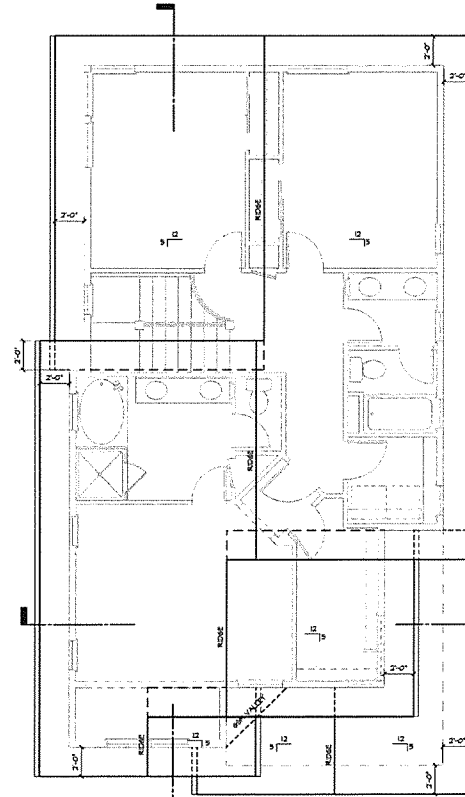
TOTAL: 1,861.03 SF → 1,861 SF



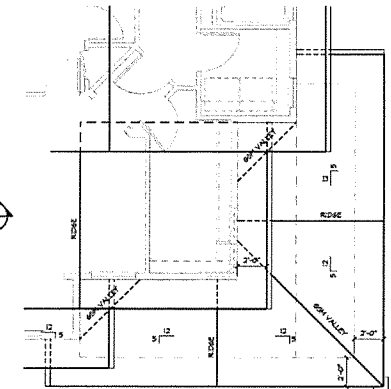
SECOND FLOOR AREA DIAGRAM



FIRST FLOOR AREA DIAGRAM



ROOF PLAN



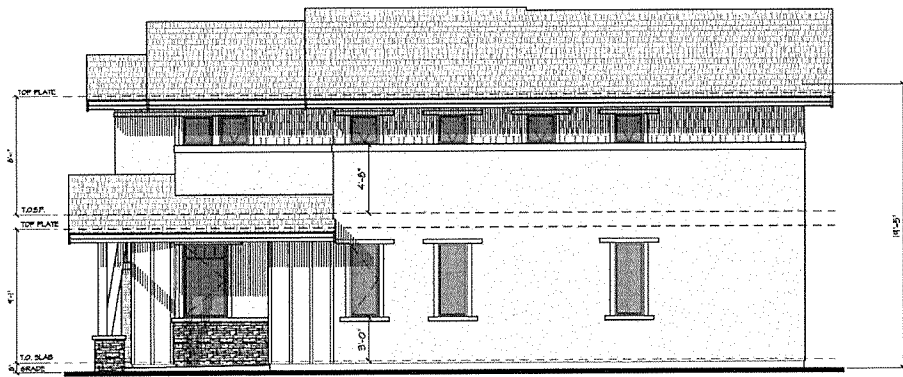
PARTIAL ROOF PLAN - HOUSE #1 PORCH

AJS Architecture | Planning

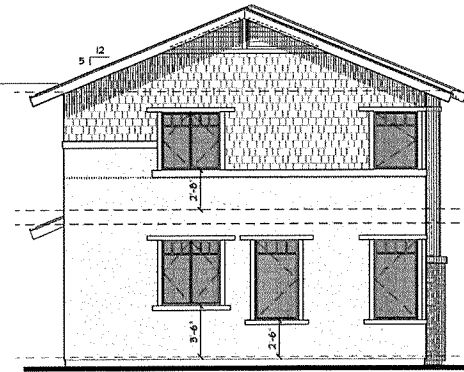
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LAUREL OAKS

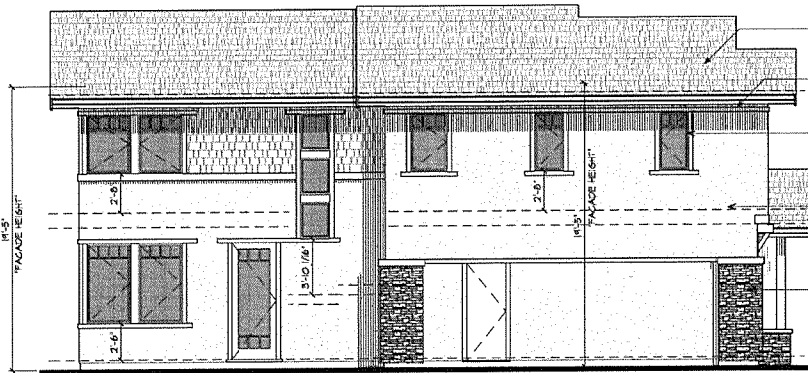
PLAN ONE
ROOF PLAN
SQ.FT. CALCS.



RIGHT SIDE / NORTH-EAST ELEVATION



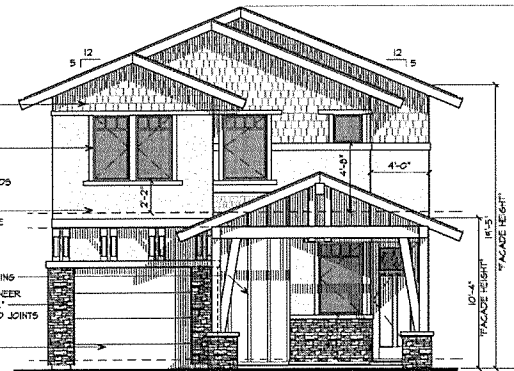
REAR / NORTH-WEST ELEVATION



LEFT SIDE / SOUTH-WEST ELEVATION



- COMPOSITION SHINGLE ROOFING
- HORIZONTAL SIDING OR SHINGLES (PER HOUSE)
- CLAD WOOD WINDOW FRAMES 1/4" TRICE SIMULATED DIVIDED LITES (W/ OUTSIDE & INSIDE GRIDS & SPACE BAR)
- STUCCO MEDIUM DASH TEXTURE
- BOARD & BATTEN SIDING
- ELPORADO STONE VENEER
- PROFILE: "CLIFFSTONE" INSTALL WITH GROUVED JOINTS
- SECTIONAL GARAGE DOOR



FRONT / SOUTH-EAST ELEVATION

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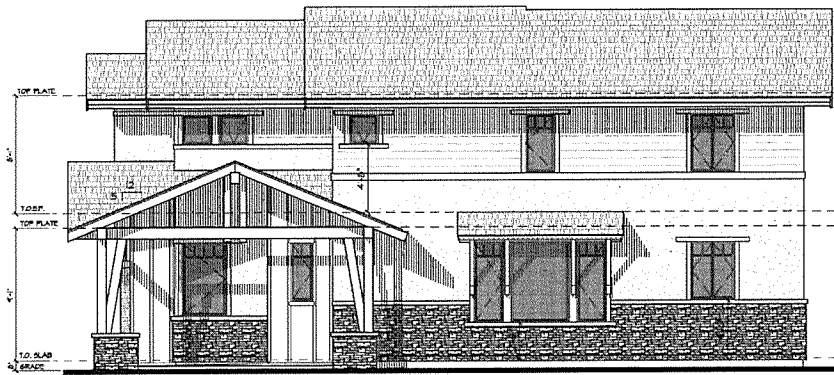
LAUREL OAKS

PLAN ONE
ELEVATIONS
HOUSE No. 2.3

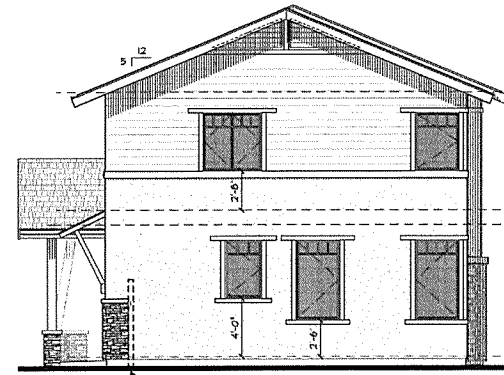
CALIFORNIA COMMUNITIES · 1273 & 1281 LAUREL STREET · MENLO PARK CALIFORNIA

APRIL 19, 2018
Project No. 109.003

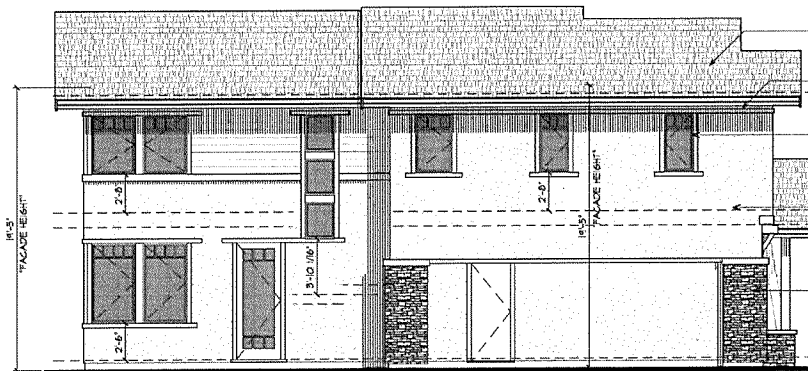
A2.3



RIGHT SIDE / NORTH-EAST ELEVATION



REAR / NORTH-WEST ELEVATION

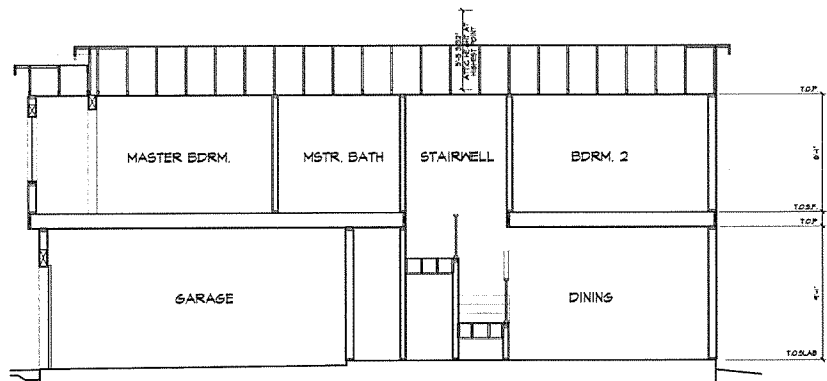


LEFT SIDE / SOUTH-WEST ELEVATION

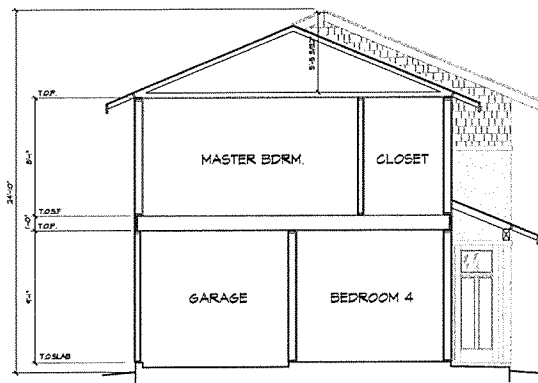


FRONT / SOUTH-EAST ELEVATION

- COMPOSITION SHINGLE ROOFING
- HORIZONTAL SIDING OR SHINGLES (PER HOUSE)
- CLAD WOOD WINDOW FRAMES w/ "TRIE SIMULATED DIVIDED LITES" (w/ OUTSIDE & INSIDE GRIDS & SPACER BAR)
- STUCCO MEDIUM DASH TEXTURE
- BOARD & BATTEN SIDING
- ELDORADO STONE VENEER PROFILE: "CLIFFSTONE" INSTALL WITH GROUTED JOINTS
- SECTIONAL GARAGE DOOR

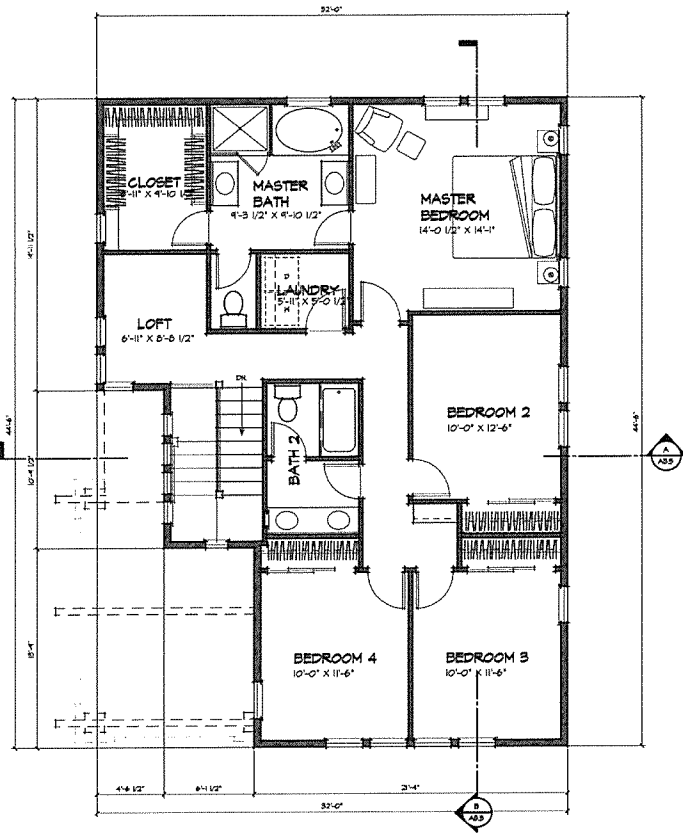


SECTION "B"



SECTION "A"





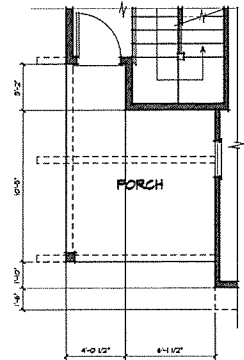
PLAN AREAS

FIRST FLOOR	1228.81	Sq. Ft.
SECOND FLOOR	1393.02	Sq. Ft.
TOTAL	2621.83	Sq. Ft.
GARAGE	440.25	Sq. Ft.

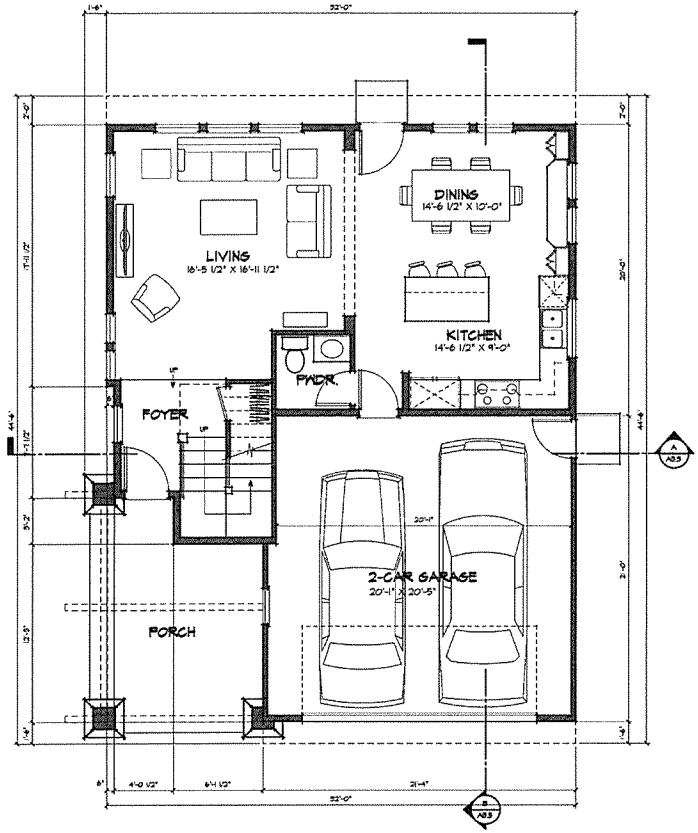
PLAN AREAS
FOX HENLO PARK F.A.R. CALIF.

FIRST FLOOR	1228.81	Sq. Ft.
SECOND FLOOR	1393.02	Sq. Ft.
TOTAL	2621.83	Sq. Ft.
GARAGE	440.25	Sq. Ft.

SECOND FLOOR PLAN



PARTIAL FIRST FLOOR PLAN
AT HOUSE #4 & 5 PORCH



FIRST FLOOR PLAN



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Livermore, California
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Anthony Sarberaria, AIA

LAUREL OAKS

PLAN TWO
FLOOR PLANS

CALIFORNIA COMMUNITIES - 1273 & 1281 LAUREL STREET - MENLO PARK CALIFORNIA

MARCH 22, 2018
Project No. 102.000

A3.1

FLOOR AREA CALCULATIONS

FIRST FLOOR

A: 11'-6 1/2" x 17'-11 1/2" = 207.27 SF
 B: 20'-5 1/2" x 20'-0" = 409.17 SF
 C: 4'-0 1/2" x 7'-7 1/2" = 30.82 SF
 D: 7'-0" x 10'-4 1/2" = 75.55 SF

TOTAL: 122.81 SF

SECOND FLOOR

F: 32'-0" x 14'-4 1/2" = 633.34 SF
 G: 20'-4" x 10'-6" = 217.88 SF
 H: 21'-4" x 14'-2 1/2" = 303.12 SF
 J: 4'-6 1/2" x 2" = 0.76 SF

TOTAL: 1,155.10 SF

TOP FLOOR STAIRWELL VOLUME

K: 6'-8 1/2" x 10'-6" = 70.44 SF
 L: 6'-1 1/2" x 5 1/2" = 2.81 SF

TOTAL: 73.25 SF

GARAGE

G1: 20'-5 1/2" x 21'-0" = 429.63 SF
 G2: 10 1/2" x 12'-3" = 10.72 SF

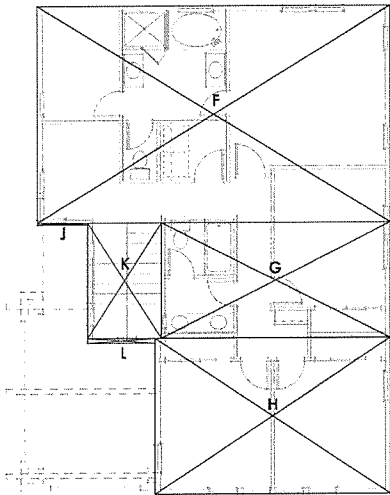
TOTAL: 440.35 SF

F.A.R. SQUARE FOOTAGE

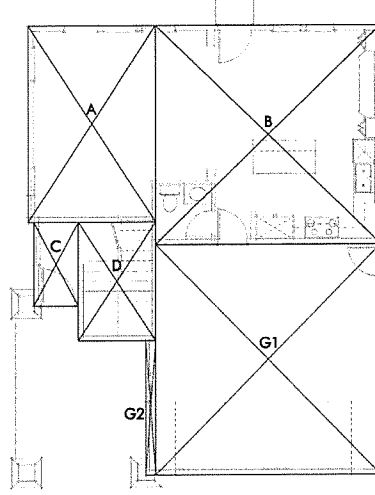
FIRST FLOOR: 122.81 SF
 SECOND FLOOR: 1,155.10 SF
 STAIRWELL VOL.: 73.25 SF
 TOTAL: 1,951.16 SF

LIVEABLE SQUARE FOOTAGE

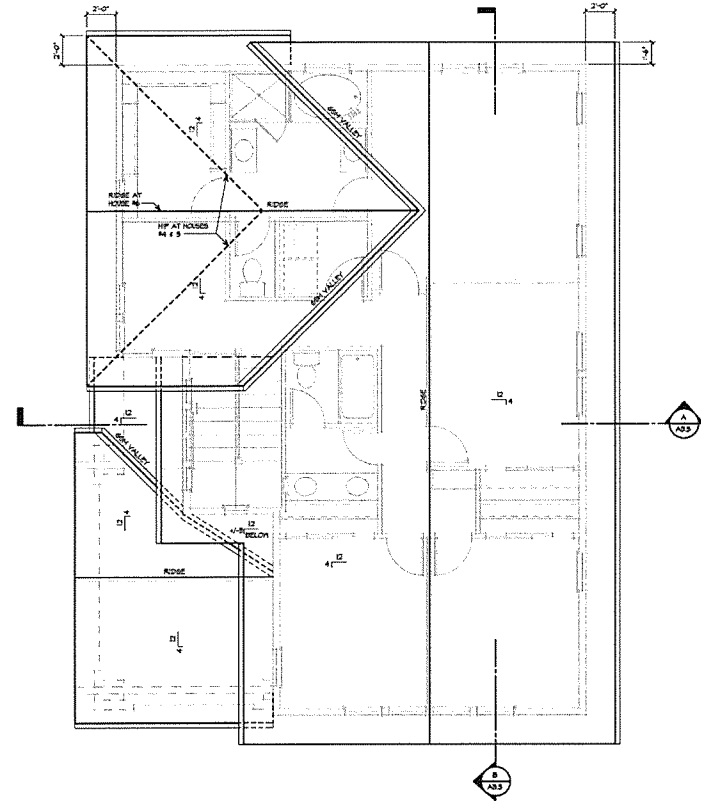
FIRST FLOOR: 122.81 SF
 SECOND FLOOR: 1,155.10 SF
 TOTAL: 1,877.91 SF



SECOND FLOOR AREA DIAGRAM



FIRST FLOOR AREA DIAGRAM



ROOF PLAN



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 Anthony Sarbarino, AIA

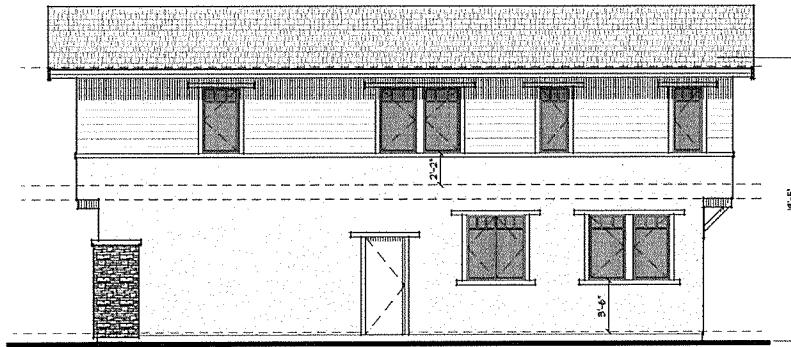
LAUREL OAKS

PLAN TWO
 ROOF PLAN
 SQ.FT. CALCS.

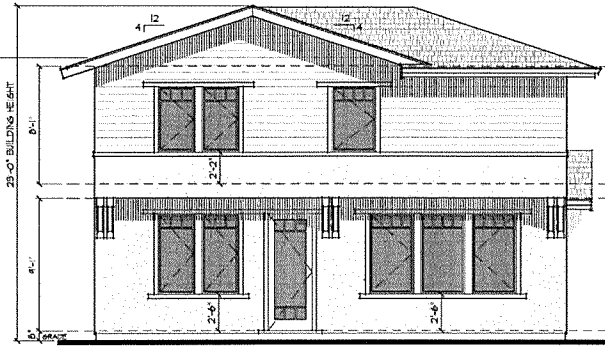
CALIFORNIA COMMUNITIES · 1273 & 1281 LAUREL STREET · MENLO PARK CALIFORNIA

MARCH 22, 2018
 Project No. 105.005

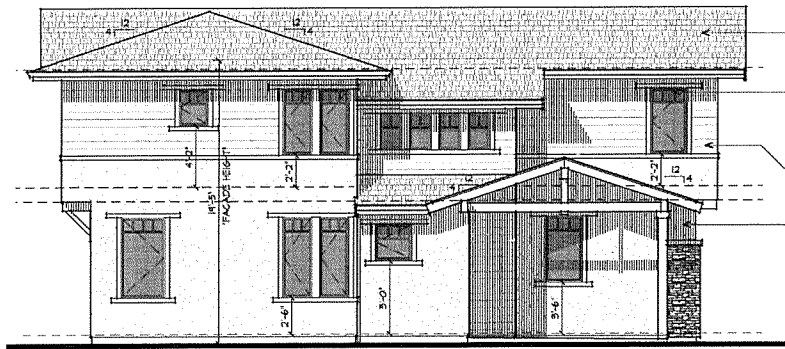
A3.2



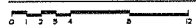
RIGHT SIDE / SOUTH-WEST ELEVATION



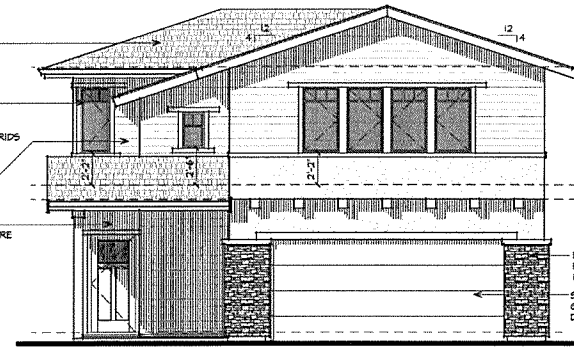
REAR / SOUTH-EAST ELEVATION



LEFT SIDE / NORTH-EAST ELEVATION

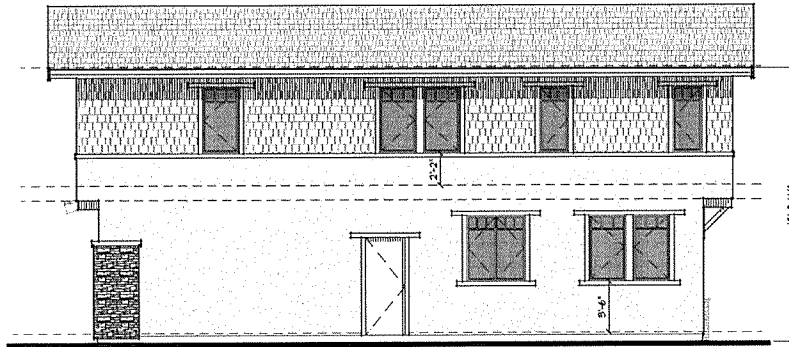


- COMPOSITION SHINGLE ROOFING
- GLAD WOOD WINDOW FRAMES w/ TYPICAL SIMULATED DIVIDED LITES (w/ OUTSIDE & INSIDE GRIDS & SPAGE BAR)
- SHINGLES OR HORIZONTAL SIDING (PER HOUSE)
- STUCCO MEDIUM DASH TEXTURE

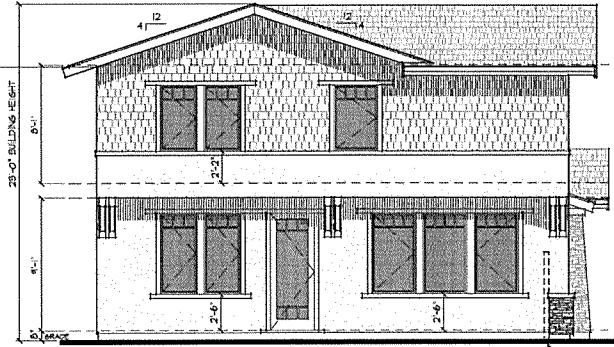


FRONT / NORTH-WEST ELEVATION

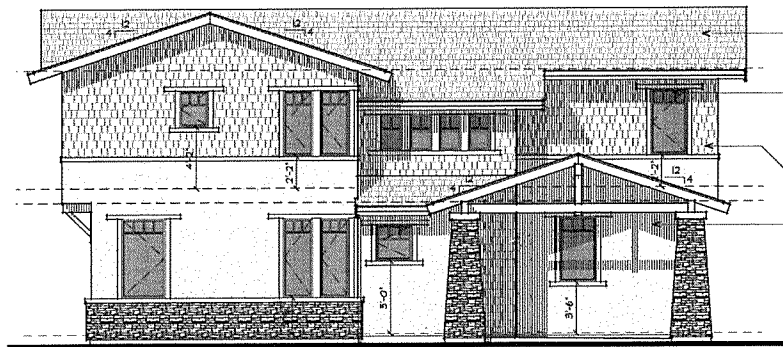
- ELDORADO STONE VENEER PROFILE: "CLIFFSTONE" INSTALL WITH GROUTED JOINTS
- SECTIONAL GARAGE DOOR



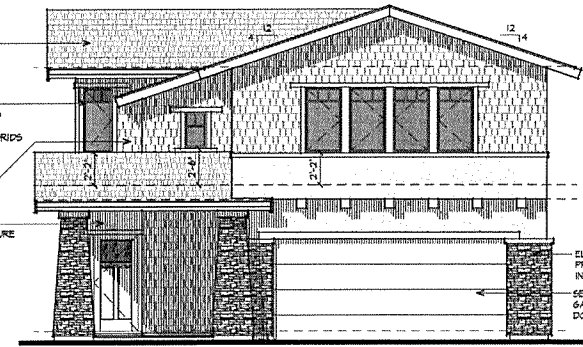
RIGHT SIDE / SOUTH-WEST ELEVATION



REAR / SOUTH-EAST ELEVATION



LEFT SIDE / NORTH-EAST ELEVATION



FRONT / NORTH-WEST ELEVATION

- COMPOSITION SHINGLE ROOFING
- GLAD WOOD WINDOW FRAMES w/ "TREE SIMULATED DIVIDED LITES" (w/ OUTSIDE & INSIDE GRIDS & SPACE BAR)
- SHINGLES OR HORIZONTAL SIDING (PER HOUSE)
- STUCCO MEDIUM DASH TEXTURE
- ELDORADO STONE VENEER PROFILE, "CLIFFSTONE" INSTALL WITH GRouted JOINTS
- SECTIONAL GARAGE DOOR

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Livermore, California
925.980.4103
Anthony Sarbarbera, AIA

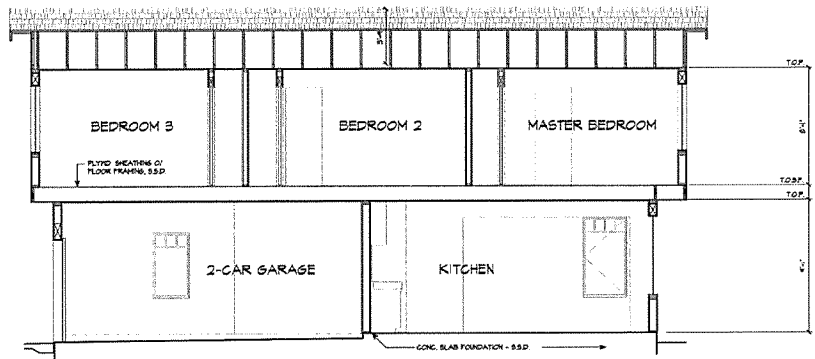
LAUREL OAKS

PLAN TWO
ELEVATIONS
HOUSE No. 6

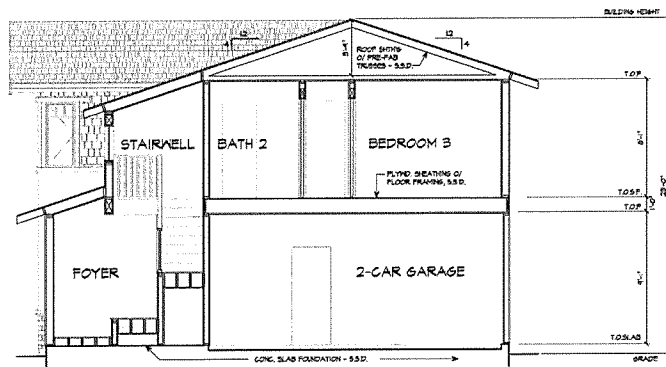
CALIFORNIA COMMUNITIES · 1273 & 1281 LAUREL STREET · MENLO PARK CALIFORNIA

APRIL 15, 2015
Project No. 108.005

A3.4

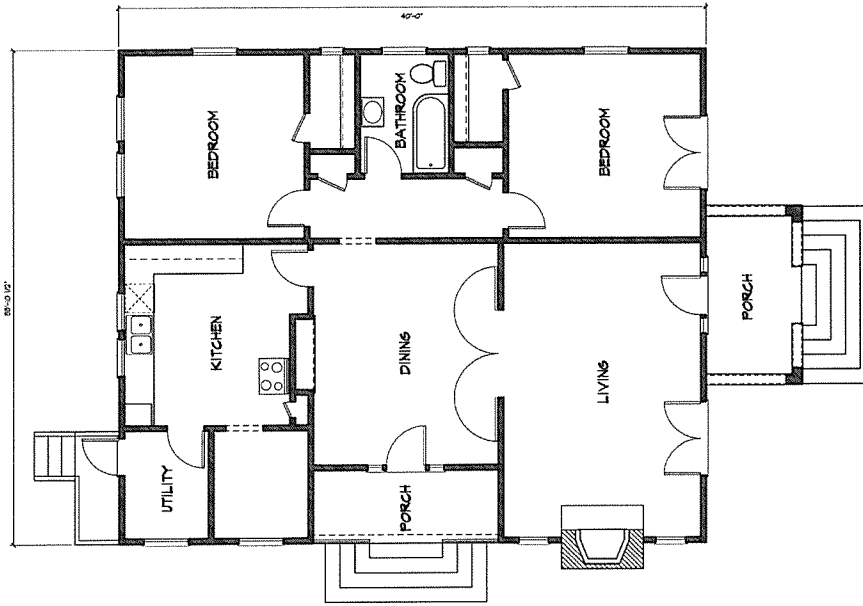


SECTION "B"

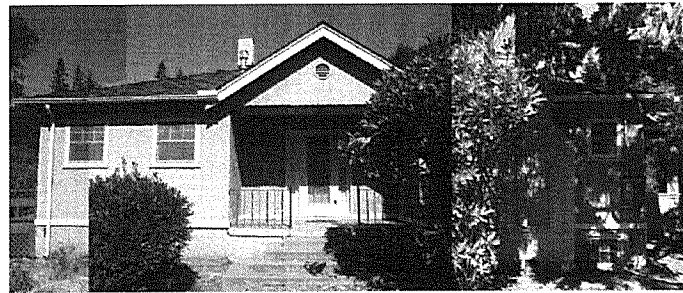


SECTION "A"





EXISTING FLOOR PLAN - 1273 LAUREL STREET



LAUREL OAKS

AJS Architecture | Planning

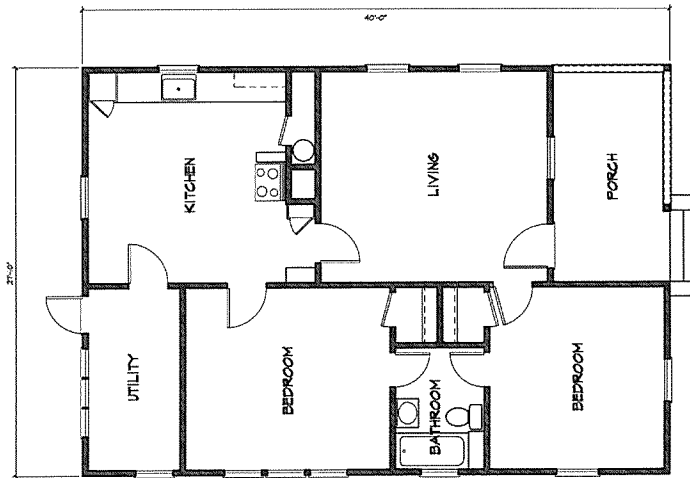
Livermore, California
925.980.4103
Anthony Sarberardo, AIA

EXISTING PLAN
1273 LAUREL STREET

CALIFORNIA COMMUNITIES · 1273 & 1281 LAUREL STREET · MENLO PARK CALIFORNIA

MARCH 22, 2018
Project No. 109.008

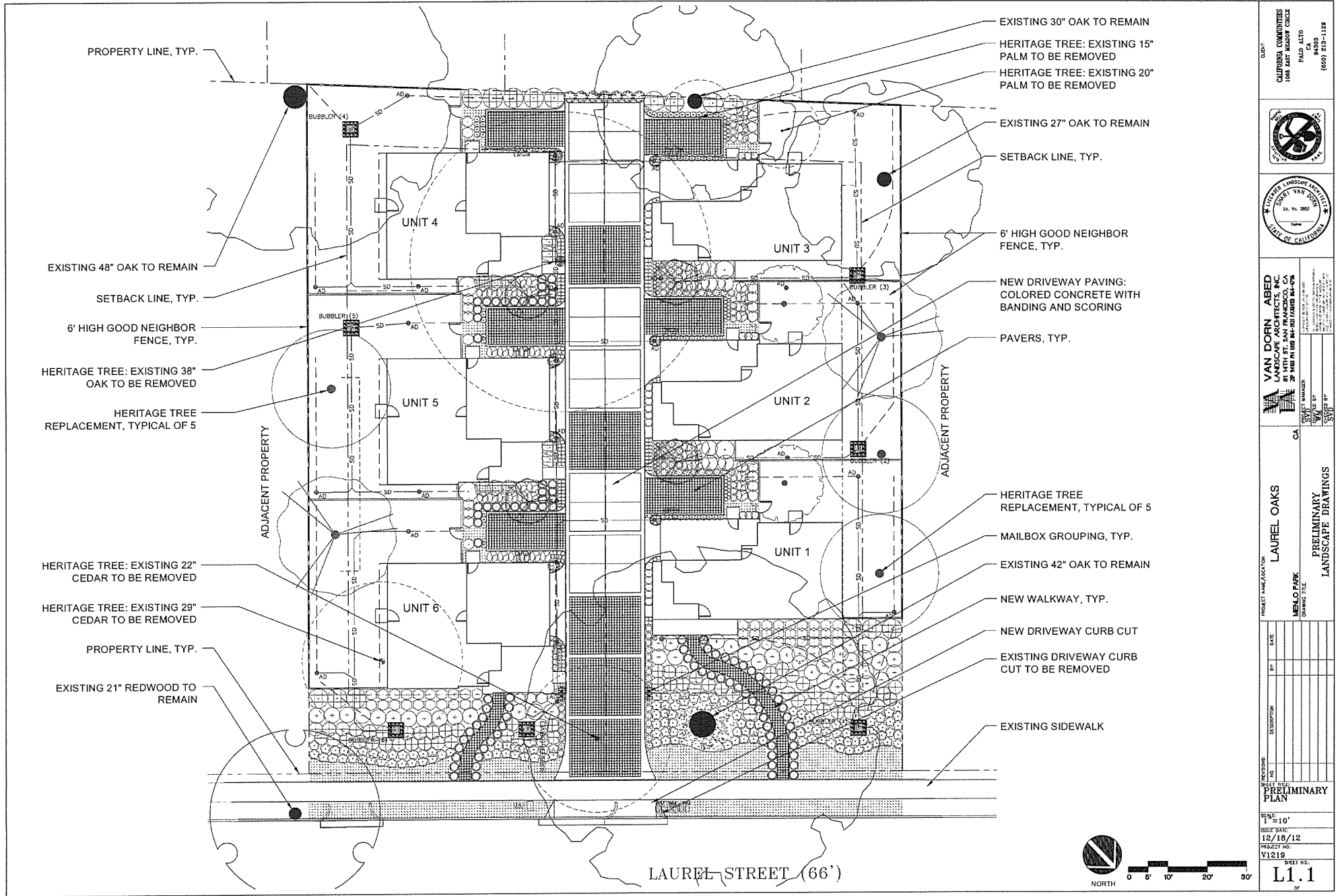
A4.1



EXISTING FLOOR PLAN - 1281 LAUREL STREET



LAUREL OAKS



CLIENT:
 CULTURAL COMMUNITIES
 1000 EAST WALTON STREET
 PALO ALTO
 CA 94303
 (650) 213-1159

PROJECT NAME/LOCATION:
 LAUREL OAKS

DRAWING TITLE:
 PRELIMINARY LANDSCAPE DRAWINGS

PROJECT MANAGER:
 MELLO PARK

DRAWING NO.:
 V1219

SHEET NO.:
 L1.1

SCALE:
 1" = 10'
 DATE:
 12/16/12
 PROJECT NO.:
 V1219

ARCHITECT:
 VAN DORN ABED
 LANDSCAPE ARCHITECTS, INC.
 814TH ST., SAN FRANCISCO, CA
 94118
 TEL: 415.774.8474 FAX: 415.774.8474

LICENSE NO.: 2002
 STATE OF CALIFORNIA

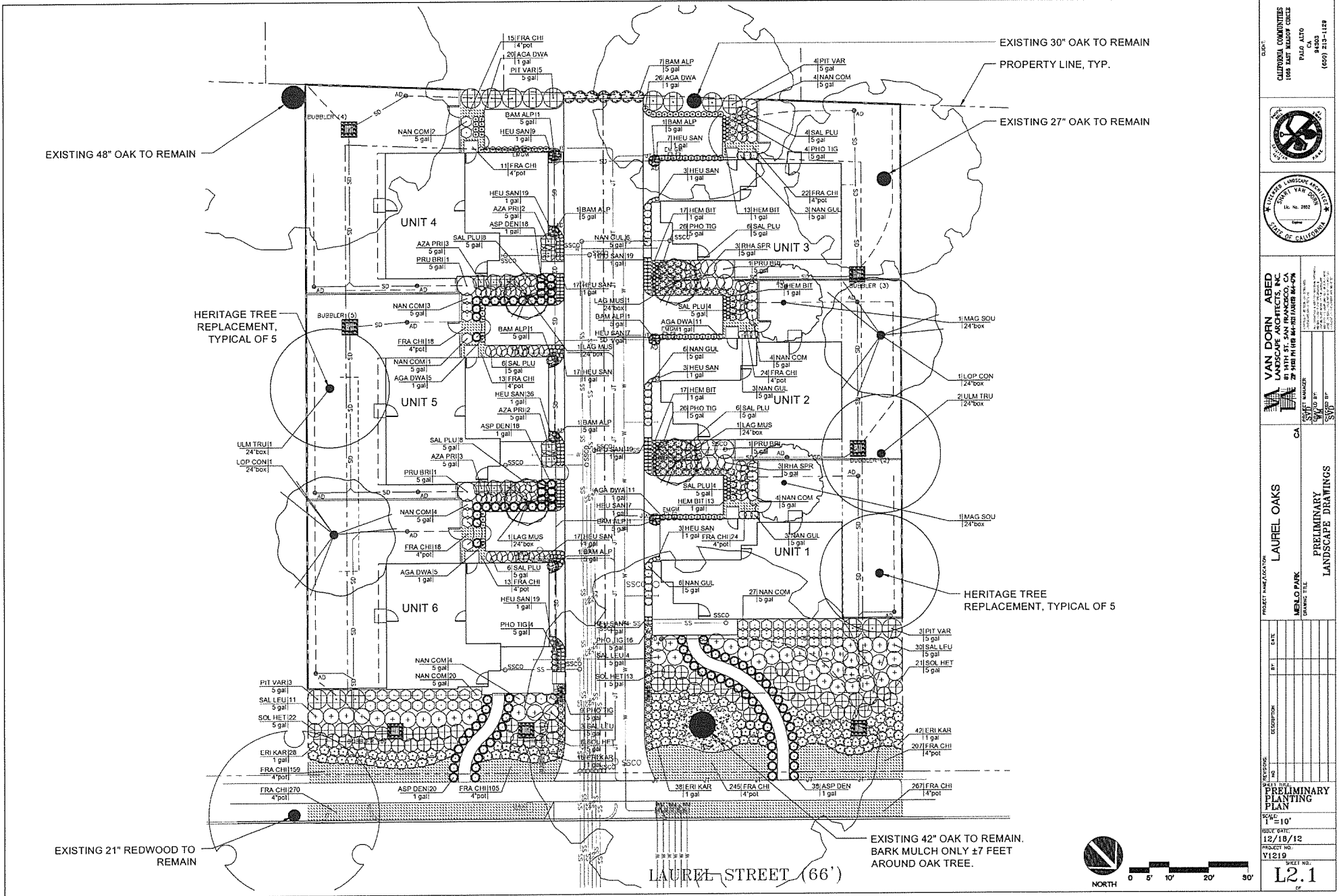
PROJECT NO.: 1219

DATE:

BY:

CHECKED BY:

APPROVED BY:



COUNTY OF CALIFORNIA
 CLAYTON COMMUNITIES
 1000 EAST WALTON CIRCLE
 PALO ALTO
 CA 94301
 (650) 210-1129

VAN DORN ABED
 LANDSCAPE ARCHITECTS, INC.
 81 N. ST. SAN FRANCISCO, CA
 415.775.1111
 22 FIELD ST. SAN FRANCISCO, CA
 415.398.0000

PROJECT NAME/LOCATION: LAUREL OAKS
 DRAWING TITLE: PRELIMINARY LANDSCAPE DRAWINGS
 DATE: 12/18/12
 SCALE: 1" = 10'
 SHEET NO.: L2.1

PLANT IMAGES



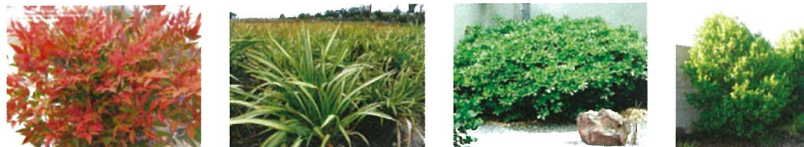
LAGERSTROEMIA X 'MUSKOGEE' LOPHOSTEMON CONFERTUS MAGNOLIA X SOULANGIANA ULMUS PARVIFOLIA 'TRUE GREEN'



AGAPANTHUS AFRICANUS 'PETER PAN' ASPARAGUS DENSIFLORUS 'MYERS' AZALEA SOUTHERN INDICA HYBRID 'PRIDE OF DORKING' BAMBUSA MULTIPLEX 'ALPHONSE KARR'



ERIGERON KARVINSKIANUS HEMEROCALLIS X 'BITSY' HEUCHERA MAXIMA 'SANTA ANA CARDINAL' NANDINA DOMESTICA 'COMPACTA'



NANDINA DOMESTICA 'GULF STREAM' PHORMIUM TENAX 'TONEY TIGER' PITTOSPORUM TOBIRA 'VARIEGATA' PRUNUS CAROLINIANA 'BRIGHT 'N TIGHT'



RAPHIOLEPIS INDICA 'SPRING RAPTURE' SALVIA LEUCANTHA SALVIA MICROPHYLLA 'PLUM WINE' SOLLYA HETEROPHYLLA

PLANT SCHEDULE

TREES	CODE	BOTANICAL NAME	COMMON NAME	CONT	QTY	REMARKS
	LAG MUS	LAGERSTROEMIA X 'MUSKOGEE'	LAVENDER CRAPE MYRTLE	24"BOX	4	
	LOP CON	LOPHOSTEMON CONFERTUS	BRISBANE BOX	24"BOX	2	HERITAGE TREE REPLACEMENT
	MAG SOU	MAGNOLIA X SOULANGIANA	SAUCER MAGNOLIA	24"BOX	2	
	ULM TRU	ULMUS PARVIFOLIA 'TRUE GREEN'	TRUE GREEN ELM	24"BOX	3	HERITAGE TREE REPLACEMENT
SHRUBS	CODE	BOTANICAL NAME	COMMON NAME	CONT	QTY	REMARKS
	AGA DWA	AGAPANTHUS AFRICANUS 'PETER PAN'	DWARF BLUE LILY OF THE NILE	1 GAL	78	
	ASP DEN	ASPARAGUS DENSIFLORUS 'MYERS'	MYERS ASPARAGUS	1 GAL	84	
	AZA PRI	AZALEA SOUTHERN INDICA HYBRID 'PRIDE OF DORKING'	PRIDE OF DORKING AZALEA	5 GAL	10	
	BAM ALP	BAMBUSA MULTIPLEX 'ALPHONSE KARR'	ALPHONSE KARR BAMBOO	5 GAL	15	
	ERI KAR	ERIGERON KARVINSKIANUS	FLEABANE	1 GAL	123	
	HEM BIT	HEMEROCALLIS X 'BITSY'	YELLOW DAYLILY	1 GAL	73	12" TALL EVERGREEN
	HEU SAN	HEUCHERA MAXIMA 'SANTA ANA CARDINAL'	ISLAND ALUM ROOT	1 GAL	206	GREEN LEAVES ONLY
	NAN COM	NANDINA DOMESTICA 'COMPACTA'	DWARF HEAVENLY BAMBOO	5 GAL	73	
	NAN GUL	NANDINA DOMESTICA 'GULF STREAM' TM	HEAVENLY BAMBOO	5 GAL	27	
	PHO TIG	PHORMIUM TENAX 'TONEY TIGER'	DWARF FLAX	5 GAL	85	
	PIT VAR	PITTOSPORUM TOBIRA 'VARIEGATA'	VARIEGATED MOCK ORANGE	5 GAL	15	
	PRU BRI	PRUNUS CAROLINIANA 'BRIGHT 'N TIGHT' TM	BRIGHT 'N TIGHT CAROLINA LAUREL	5 GAL	4	
	RHA SPR	RHAPHIOLEPIS INDICA 'SPRING RAPTURE' TM	SPRING RAPTURE INDIAN HAWTHORNE	5 GAL	6	
	SAL LEU	SALVIA LEUCANTHA	MEXICAN BUSH SAGE	5 GAL	48	
	SAL PLU	SALVIA MICROPHYLLA 'PLUM WINE'	PLUM WINE SAGE	5 GAL	52	
	SOL HET	SOLLYA HETEROPHYLLA	AUSTRALIAN BLUEBELL	5 GAL	64	
GROUND COVERS	CODE	BOTANICAL NAME	COMMON NAME	CONT	QTY	REMARKS
	FRA CHI	FRAGARIA CHILOENSIS	BEACH STRAWBERRY	4"POT@ 12" OC	1,411	

PLANTING AND WATER USE DESIGN INTENT STATEMENT

The planting design utilizes primarily drought tolerant, low and medium water use plant materials located in separate hydrozones. Shade tolerant, medium water use plants will be utilized on the North facing sides of the project. Low and medium water use plants will be used in all other perimeter plant areas and in the pases. The plants will be selected utilizing the State of California's Model Water Efficient Landscape Ordinance plant list and ET Calc water management computer software. A minimum of 2" of bark mulch will be placed in all shrub and groundcover areas.

IRRIGATION SYSTEM WATER USE DESIGN INTENT STATEMENT

The irrigation system will be a fully automatic, low gallon system with matched precipitation rate heads or emitters on each circuit. This system will design with head to head coverage and minimum overspray. The low, medium and high water use hydrozones will be on separate valve circuits. All new trees will have separate drip or bubbler circuits. The remote control valves will have integral pressure regulators to prevent fluctuations and ensure constant application rates to minimize over or under watering. The electronic irrigation controller will be weather based and make automatic adjustments based on current climate along with multiple programs and application cycles/start times. A rain switch will be installed to prevent irrigation during rainy periods. A flow sensor and master valve will be connected to the controller to allow automatic shut off of any valve circuit or main line in the event of a pipe brake to prevent water waste.

CULPIN COUNTY COMMUNITIES
1008 WEST MADISON CIRCLE
PALO ALTO
94303
(650) 215-1129



VAN DORN ABED
1000 CALIFORNIA ST. SAN FRANCISCO, CA
47 HATH ST. SAN FRANCISCO, CA
27 HED PT HGB 84-RT FARMED 84-476
STP. 10/12/12
CA
MENLO PARK
PRELIMINARY
LANDSCAPE DRAWINGS

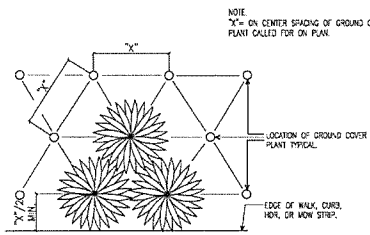
LAUREL OAKS
PRELIMINARY
LANDSCAPE DRAWINGS

NO.	DATE	REVISION

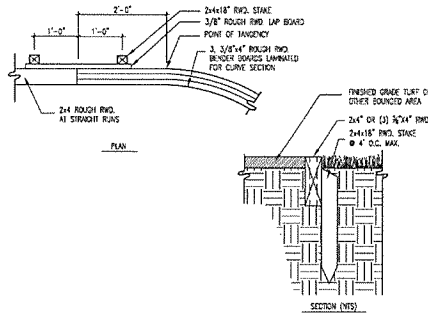
PRELIMINARY
PLANT LIST
& IMAGES

SCALE: 1" = 10'
DATE: 12/18/12
PROJECT: V1219

SHEET NO:
L2.2

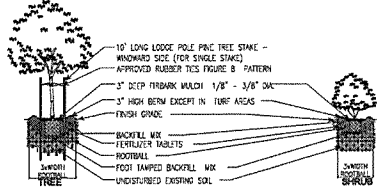


A GROUND COVER PLANTING
N.T.S.

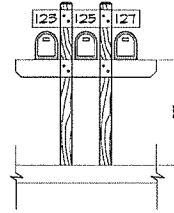


D HEADER BOARD
N.T.S.

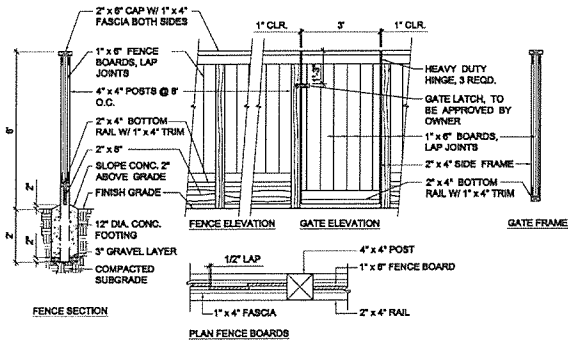
- NOTES:
1. PLANT CROWN SHALL BE 1" ABOVE FINISH GRADE FOR SHRUBS AND 1-1/2" ABOVE FINISH GRADE FOR TREES AFTER WATERING AND SETTLING.
 2. ONE STAKE FOR 5 GAL. TREES AND TWO STAKES FOR 15 GAL. AND LARGER TREES.
 3. LOCATE STAKES AT OUTSIDE EDGE OF ROOTBALL.
 4. SCARIFY SIDES OF PIT.



B TREE AND SHRUB PLANTING
N.T.S.



E MAILBOX
N.T.S.



- NOTES:
1. ALL POST SHALL BE PRESSURE TREATED CEDARTONE, ALL OTHER WOOD SHALL BE CON. HEART CEDAR.
 2. ALL METAL HARDWARE SHALL BE GALVANIZED STEEL.
 3. ALL GATES INSTALLED SHALL BE SUPPORTED BY A DIAGONAL TENSION CABLE AS SHOWN, LOCATE CABLE ON INSIDE OF GATE.

C GOOD NEIGHBOR FENCE & GATE
N.T.S.

CURTIS CONCRETE
1065 EAST MARSH CIRCLE
PALMDALE, CA 94303
(805) 213-1124

VAN DORN ABED
LANDSCAPE ARCHITECTS, INC.
4114 ST. SAN FRANCISCO, CA
27 HILL STREET #100 PALMDALE CA 93550

PROJECT NAME/LOCATION: LAUREL OAKS
DRAWING TITLE: PRELIMINARY LANDSCAPE DRAWINGS
SCALE: 1" = 10'
DATE: 12/19/12
PROJECT NO: V1219
SHEET NO: 2 OF 23

NO.	DESCRIPTION	BY	DATE

TENTATIVE MAP LAUREL OAKS SUBDIVISION 1273 & 1281 LAUREL STREET MENLO PARK, CALIFORNIA

LEGEND

SYMBOL	DESCRIPTION
---	BOUNDARY
- - - -	PROPERTY LINE
=====	RETAINING WALL
-----	LANDSCAPE RETAINING WALL
- SUB -	SUBDRAIN LINE
- RW - RW -	TIGHTLINE
- S-D -	STORM DRAIN LINE
- S-S -	SANITARY SEWER LINE
- W -	WATER LINE
- G -	GAS LINE
- P -	PRESSURE LINE
- JT -	JOINT TRENCH
=====	SET BACK LINE
--->--->	CONCRETE VALLEY GUTTER
--->---	GRASS LANED SWALE
CB	CATCH BASIN
JB	JUNCTION BOX
AD	AREA DRAIN
CIL	CURB INLET
SCMH	STORM DRAIN MANHOLE
FWH	FIRE HYDRANT
SSMH	SANITARY SEWER MANHOLE
SS	STREET SIGN
▲	SPOT ELEVATION
→	FLOW DIRECTION
□	DEMOLISH/REMOVE
□	BENCHMARK
---	CONTOURS
○	TREE TO BE REMOVED

ABBREVIATIONS

AB	AGGREGATE BASE	MAX	MAXIMUM
AC	ASPHALT CONCRETE	MH	MANHOLE
ACC	ACCESSIBLE	MIN	MINIMUM
AD	AREA DRAIN	MON.	MONUMENT
BC	BEGINNING OF CURVE	(N)	NUMBER
B & D	BEARING & DISTANCE	N.D.	NOT TO SCALE
BW	BENCHMARK	NTS	NOT TO SCALE
BW/FG	BOTTOM OF WALL/FINISH GRADE	O.C.	OVER CENTER
CB	CATCH BASIN	P	PLANTING AREA
C & G	CURB AND GUTTER	PA	PEDESTRIAN
C	CENTER LINE	PVI	POST INDICATOR VALVE
CPP	CORRUGATED PLASTIC PIPE	PSS	PUBLIC SERVICES EASEMENT
CO	CLEANOUT	P	PROPERTY LINE
COTD	CLEANOUT TO GRADE	PP	POWER POLE
CONC	CONCRETE	PUE	PUBLIC UTILITY EASEMENT
CONCT	CONSTRUCT OR -TION	PVC	POLYVINYL CHLORIDE
CONC COR	CONCRETE CORNER	R	RADIUS
CT	CUBIC YARD	RCP	REINFORCED CONCRETE PIPE
D	DIAMETER	RM	RISE ELEVATION
DIP	DROP INLET	R/W	RAINWATER
DI	DUCTILE IRON PIPE	R/W	RIGHT OF WAY
EA	EACH	S	SLOPE
EO	END OF CURVE	S.A.D.	SEE ARCHITECTURAL DRAWINGS
EG	EXISTING GRADE	SAH	SANITARY
EL	ELEVATIONS	SD	STORM DRAIN
EP	EDGE OF PAVEMENT	SDMH	STORM DRAIN MANHOLE
EQ	EQUIPMENT	SHT	SHEET
EW	EACH WAY	S.L.O.	SEE LANDSCAPE DRAWINGS
FD	FACE OF CURB	SPC	SPECIAL
FF	FINISHED FLOOR	SS	SANITARY SEWER
FG	FINISHED GRADE	SSOD	SANITARY SEWER CLEANOUT
FN	FIRE HYDRANT	SSMH	SANITARY SEWER MANHOLE
FL	FLOW LINE	ST	STREET
FS	FINISHED SURFACE	STA	STATION
GA	GAUGE OR GAUGE	STD	STANDARD
GB	GRADE BREAK	STRUC	STRUCTURAL
HPCP	HIGH DENSITY CORRUGATED	T	TELEPHONE
H	HORIZONTAL	TC	TOP OF CURB
H PT	HIGH POINT	TEMP	TEMPORARY
H&T	HUB & TACK	TP	TOP OF PAVEMENT
HW	HIGH WATER	TV/FG	TOP OF WALL/FINISH GRADE
INV	INVERT ELEVATION	VC	VERTICAL CURVE
ID	INSIDE DIAMETER	VP	VERTICAL POINT
IB	JUNCTION BOX	VERT	VERTICAL
JT	JOINT TRENCH	W	WATER LINE
JF	JOINT UTILITY POLE	W, WL	WATER LINE
L	LENGTH	WF	WELDED WIRE FABRIC
LAND	LANDING	W	WATER METER
LF	LINEAL FEET	WF	WELDED WIRE FABRIC



VICINITY MAP
NO SCALE

OWNER INFORMATION
OWNER:
FORREST MOZART - CALIFORNIA COMMUNITIES
1063 EAST MEADOW CIRCLE
PALO ALTO, CA 94303
APN: 081-401-070 & 081-401-050

REFERENCES
THIS TENTATIVE MAP IS SUPPLEMENTAL TO:
1. TOPOGRAPHIC SURVEY BY LEA & BRAZE ENGINEERING, INC., ENTITLED: "TOPOGRAPHIC SURVEY" 1273 & 1281 LAUREL STREET MENLO PARK, CA DATED: 8-10-12 JOB#: 2120288
2. SITE PLAN BY AJS ARCHITECTURE, ENTITLED: "LAUREL OAKS" 1273 & 1281 LAUREL STREET MENLO PARK, CA DATED: 7-24-12 JOB#: 103.003

THE CONTRACTOR SHALL REFER TO THE ABOVE NOTED SURVEY AND PLAN, AND SHALL VERIFY BOTH EXISTING AND PROPOSED ITEMS ACCORDING TO THEM.

UTILITY SERVICES

GAS & ELECTRIC:	PACIFIC GAS & ELECTRIC COMPANY
TELEPHONE:	AMERICAN TELEPHONE & TELEGRAPH COMPANY
WATER:	CALIFORNIA WATER SERVICE COMPANY
SANITARY SEWER:	NORTH BAY SANITARY DISTRICT
STORM DRAINAGE:	CITY OF MENLO PARK
FIRE PROTECTION:	MENLO PARK FIRE PROTECTION DISTRICT

SITE-BENCHMARK

SURVEY CONTROL
SET MAG NAIL AND SHINER
ELEVATION = 59.29'

BENCHMARK NOTE

BENCHMARK U0110
DISK SET IN MASSIVE STRUCTURE
LOCATED IN MENLO PARK 0.1 MILE SOUTHWEST OF THE SOUTHERN PACIFIC COMPANY RAILROAD STATION, AT THE INTERSECTION OF SANTA CRUZ AVENUE AND EL CAMINO REAL AT THE ELIOT BUILDING, IN THE TOP PROJECTION OF THE GRANITE BLOCK FOUNDATION, BETWEEN TWO GRANITE COLUMNS, 15.9 FEET SOUTHWEST OF THE SOUTHWEST CURB OF THE HIGHWAY, 0.3 FEET SOUTHWEST OF THE SOUTHWEST BRICK WALL AND 2.0 FEET ABOVE THE SIDEWALK
ELEVATION = 71.13'

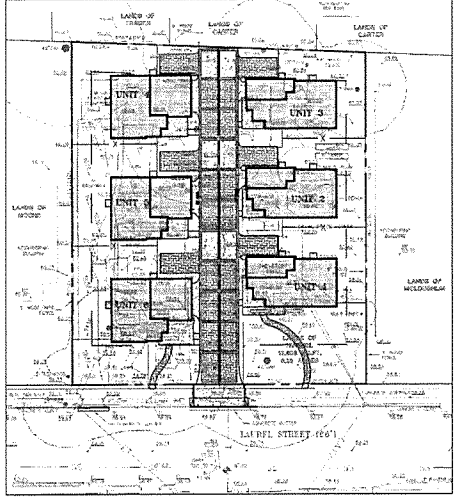
EASEMENT NOTE

1281 LAUREL STREET
THERE ARE NO EASEMENTS LISTED IN TITLE REPORT PREPARED BY FIRST AMERICAN TITLE COMPANY, NO. 4102-3083117, DATED FEBRUARY 3, 2012.

1273 LAUREL STREET
THERE ARE NO EASEMENTS LISTED IN TITLE REPORT PREPARED BY FIRST AMERICAN TITLE COMPANY, NO. 4102-3089732, DATED JANUARY 23, 2012.

NOTE:
ADDITIONAL CONSTRUCTION STAKING SCHEDULING OR QUOTATIONS PLEASE CONTACT GREG BRAZE AT LEA & BRAZE ENGINEERING (651)887-4686 EXT 103. gbraze@leabrazee.com

* BUILDING PAD NOTE:
ADJUST PAD LEVEL AS REQUIRED. REFER TO STRUCTURAL PLANS FOR S-18 SECTION OR CRAWL SPACE DEPTH TO ESTABLISH PAD LEVEL.



KEY MAP
1" = 30'

ESTIMATED EARTHWORK QUANTITIES

DEVELOPMENT AREA SUMMARY

PRE-DEVELOPMENT (SqFT)	
BUILDING	3,224
DRIVEWAY & PARKING	3,358
PATIOS, WALKWAYS & PADS	1,054
WOOD DECKS	633
TOTAL	8,247
POST-DEVELOPMENT (SqFT)	
BUILDINGS	6,785
DRIVEWAY & PARKING	1,714
PATIOS, WALKWAYS & PADS	952
WOOD DECKS	0
PERVIOUS DRIVEWAY/WALKWAY (2977 SF)	0
TOTAL	9,454
DIFFERENCE (INCREASE)	1,187

CUT 110 C.Y.
FILL 400 C.Y.
IMPORT 290 C.Y.

NOTE: GRADING QUANTITIES REPRESENT BANK YARDAGE. IT DOES NOT INCLUDE ANY SWELLING OR SHRINKAGE FACTORS AND IS INTENDED TO REPRESENT IN-SITU CONDITIONS. QUANTITIES DO NOT INCLUDE OVER-EXCAVATION, TRENCHING, STRUCTURAL FOUNDATIONS OR PIERS, OR POOL EXCAVATION (IF ANY). NOTE ADDITIONAL EARTHWORKS, SUCH AS KEYWAYS OR BENCHING MAY BE REQUIRED BY THE GEOTECHNICAL ENGINEER IN THE FIELD AT TIME OF CONSTRUCTION. CONTRACTOR TO VERIFY QUANTITIES.

EXISTING USE:	RESIDENTIAL
PROPOSED USE:	RESIDENTIAL
EXISTING NUMBER OF UNITS:	2
PROPOSED NUMBER OF UNITS:	6

PUBLIC WORKS NOTE:

THE STORM RUNOFF GENERATED BY THE NEW DEVELOPMENT SHALL NOT DRAIN ONTO ADJACENT PROPERTIES. THE EXISTING STORM DRAINAGE FROM THE ADJACENT PROPERTIES SHALL NOT BE BLOCKED BY THE NEW DEVELOPMENT.

THE APPLICANT/CONTRACTOR SHALL OBTAIN AN ENCROACHMENT PERMIT FROM THE CITY'S ENGINEERING DIVISION PRIOR TO START OF ANY WORK WITHIN THE CITY'S RIGHT-OF-WAY OR PUBLIC EASEMENT AREAS. THE APPLICANT SHALL OBTAIN PERMITS FROM UTILITY COMPANIES PRIOR TO APPLYING FOR CITY ENCROACHMENT PERMIT.

ALL TRENCHES IN THE CITY'S RIGHT-OF-WAY SHALL COMPLY WITH CITY STANDARD DETAILS ST-9A, ST-9B, AND ST-16.

ALL CONCRETE WORK IN THE CITY'S RIGHT-OF-WAY SHALL COMPLY WITH CITY STANDARD DETAIL G-3.

ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY STANDARDS AND TO THE SATISFACTION OF THE CITY ENGINEER.

EXISTING FRONTAGE IMPROVEMENTS (A.C., PARKING STRIPS, DRIVEWAY, AND VALLEY GUTTER) THAT ARE CRACKED, DAMAGED, ELEVATED, OR DERESSED OR THAT CAUSE SURFACE WATER PONDING SHALL BE REMOVED AND REPLACED BY THE APPLICANT PER CITY STANDARDS.

INSTALL STABILIZED CONSTRUCTION ENTRANCE (AS APPLICABLE) PER CITY STANDARD DETAIL G-18.

NOTES

CONTRACTOR SHALL OBTAIN THE PROPER PERMITS PRIOR TO ANY GRADING.

A SEPARATE PERMIT IS REQUIRED FOR ANY & ALL WORK WITHIN THE CITY'S RIGHT-OF-WAY. THE CONTRACTOR(S) SHALL OBTAIN AN APPROVED STREET WORK (ENCROACHMENT PERMIT) PERMIT FROM THE PUBLIC WORKS DEPARTMENT PRIOR TO THE COMMENCEMENT OF THIS WORK WITHIN THE CITY RIGHT-OF-WAY.

ALL GRADED SLOPES SHALL BE PLANTED WITH FAST GROWING, DEEP ROOTED GRASS COVER TO REDUCE THE EROSION DURING HEAVY RAINS.

REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION, INCLUDING BUT NOT LIMITED TO: ADDITIONAL UTILITY SERVICES, DIMENSION CONTROL, DEMOLITION, DETAILS, TREE REMOVAL MEASURES, AND LANDSCAPING.

FINISHED GRADE ELEVATIONS NOTED AS [F0 (MAX.)] ARE THE MAXIMUM ALLOWABLE GRADE AT THE BUILDING PERIMETER TO PROVIDE 6" MIN. CLEAR TO GRADE PER C.B.C. SECTION 2317.8. THESE GRADES MAY BE LOWER PROVIDED PROPER FLOW AWAY FROM THE FOUNDATION IS ACHIEVED. REFER TO ARCHITECTURAL & STRUCTURAL DRAWINGS FOR SPECIAL DETAILS AS REQUIRED.

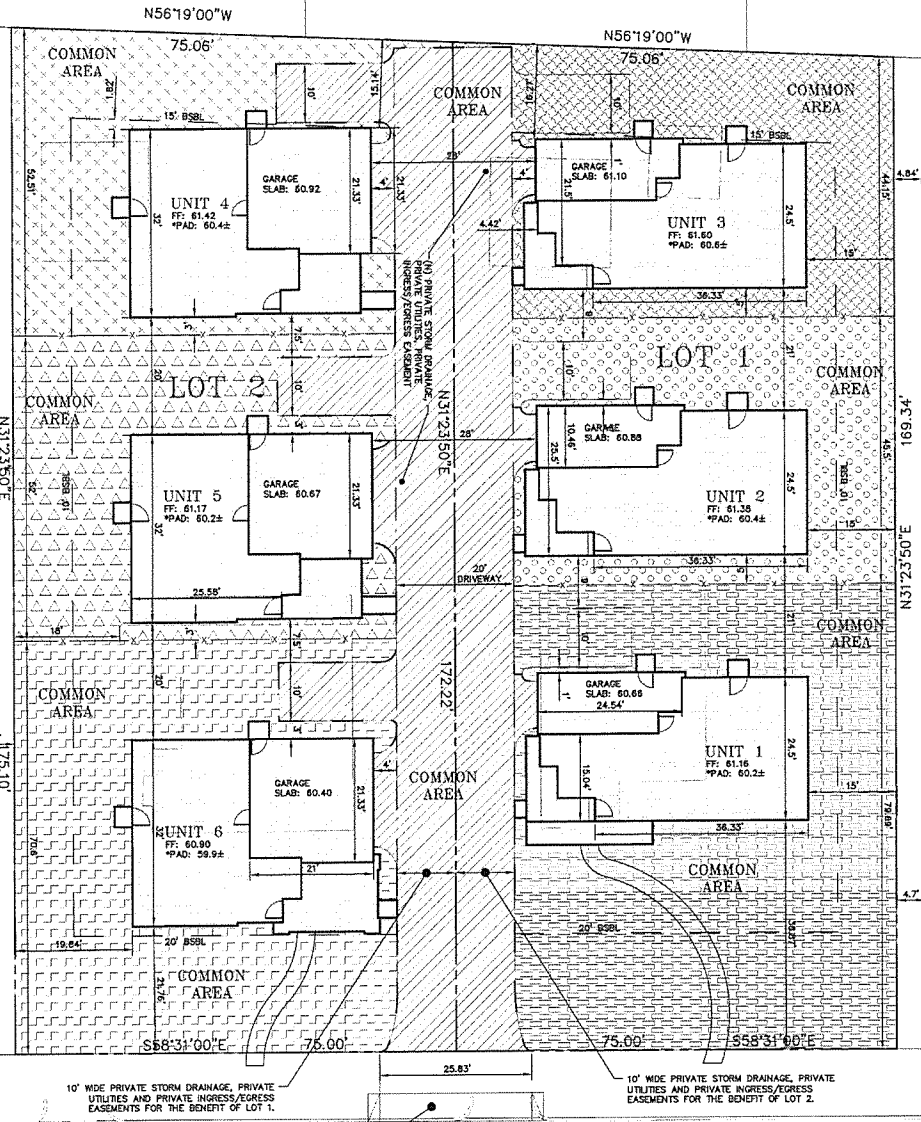
CONTRACTOR SHALL NOTIFY THE OWNER AND/OR MAINTENANCE STAFF IN WRITING OF THE NEED OF PERIODIC MAINTENANCE OF THE DRAINAGE SYSTEM AND STRUCTURES.



LAUREL OAKS SUBDIVISION
1273 & 1281 LAUREL STREET
MENLO PARK, CALIFORNIA
SAN MATEO COUNTY
APN: 081-401-070 & 081-401-050
JOB#: 2120288
WWW.LEA&BRAZE.COM

TITLE SHEET

REVISONS	BY



[Pattern]	EXCLUSIVE USE COMMON AREA OF UNIT 1 3,659.0 SF
[Pattern]	EXCLUSIVE USE COMMON AREA OF UNIT 2 1,712.4 SF
[Pattern]	EXCLUSIVE USE COMMON AREA OF UNIT 3 1,716.8 SF
[Pattern]	EXCLUSIVE USE COMMON AREA OF UNIT 4 1,747.1 SF
[Pattern]	EXCLUSIVE USE COMMON AREA OF UNIT 5 1,787.2 SF
[Pattern]	EXCLUSIVE USE COMMON AREA OF UNIT 6 2,940.1 SF
[Pattern]	COMMON AREA 4,419.0 SF
[Pattern]	BUILDING AREA FOR SPECIFIC UNIT

EASEMENT NOTE
 1281 LAUREL STREET
 THERE ARE NO EASEMENTS LISTED IN TITLE REPORT PREPARED BY FIRST AMERICAN TITLE COMPANY, NO. 4102-3983117, DATED FEBRUARY 3, 2012.

 1273 LAUREL STREET
 THERE ARE NO EASEMENTS LISTED IN TITLE REPORT PREPARED BY FIRST AMERICAN TITLE COMPANY, NO. 4102-398752, DATED JANUARY 23, 2012.

LEA & BRAZE ENGINEERING, INC.
 CIVIL ENGINEERS - LAND SURVEYORS
 BAY AREA REGION, PO BOX 451
 SACRAMENTO REGION, 300
 FAIRWAY, CALIFORNIA 94554
 (P) (916) 938-4006
 (F) (916) 938-4006
 (T) (916) 938-4006
 WWW.LEA&BRAZE.COM
 APN: 061-401-020
 APN: 061-401-020

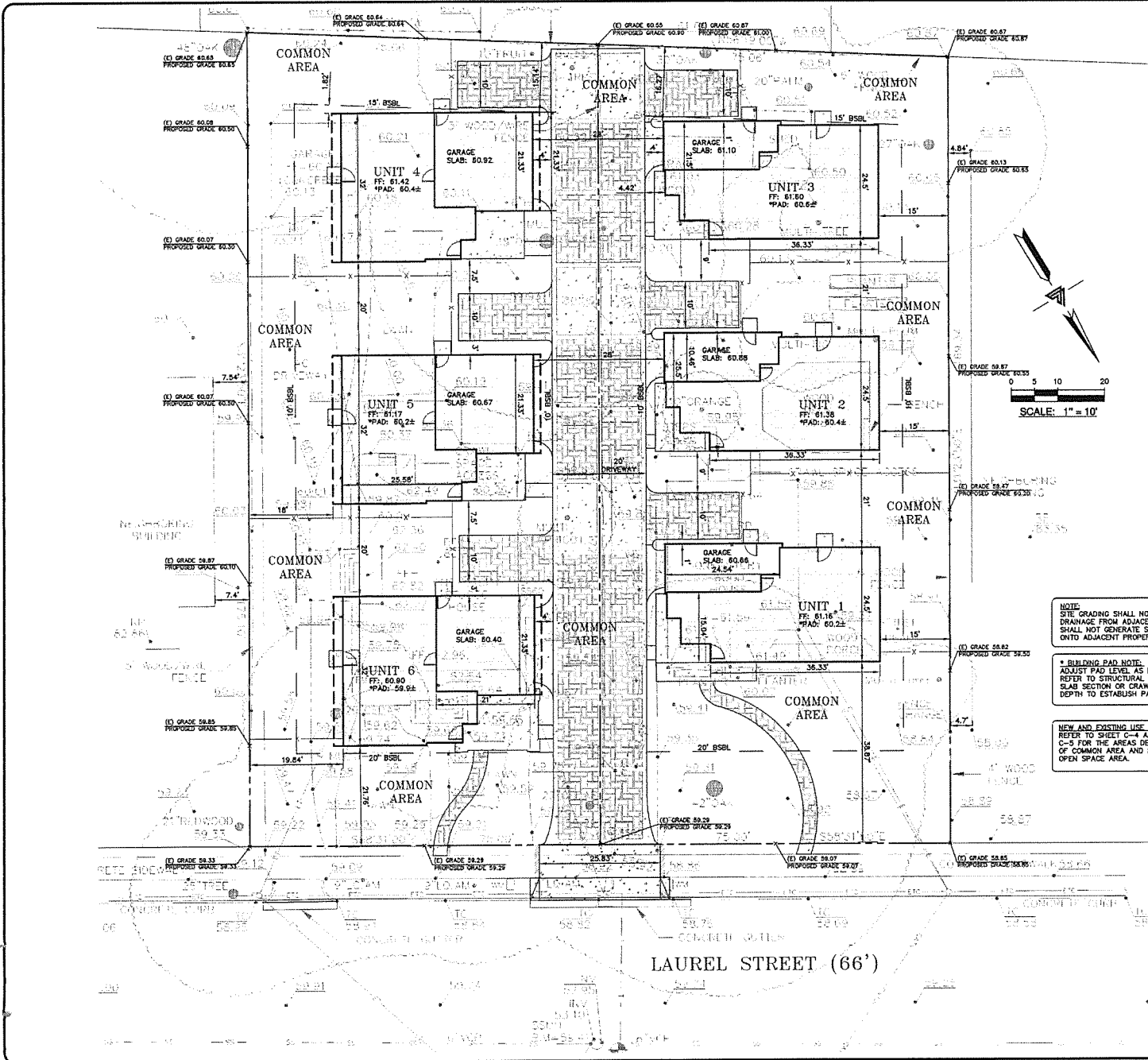
LAUREL OAKS SUBDIVISION
1273 & 1281 LAUREL STREET
MENLO PARK, CALIFORNIA
 SAN MATEO COUNTY

TENTATIVE MAP

NO.	REVISIONS	BY

JOB NO: 2120337
 DATE: 3-15-13
 SCALE: 1" = 10'
 DESIGN BY: PT
 DRAWN BY: TB
 SHEET NO:





NOTES

1. THE APPLICANT SHALL REMOVE AND REPLACE ALL CRACKED, DAMAGED, UPLIFTED OR DEPRESSED FRONTAGE IMPROVEMENTS LOCATED IN CITY'S RIGHT-OF-WAY, EXISTING OR DAMAGED BY THE CONSTRUCTION ACTIVITIES, PER CITY STANDARDS ALONG THE ENTIRE PROPERTY FRONTAGE.
2. CIVIL ENGINEER SHALL COORDINATE WITH PROJECT ARCHITECT TO DETERMINE THE LOCATIONS OF EDGE OF PAVEMENT, STORM DRAIN LINES AND OTHER UTILITY LINES NEAR TREES. THE LOCATIONS OF IMPROVEMENTS NEAR CITY TREES SHALL BE APPROVED BY CITY ARCHITECT.
3. CONTACT PUBLIC WORKS AT (925) 358-8740 TO SCHEDULE AN INSPECTION A MINIMUM OF 24 HOURS IN ADVANCE OF COMMENCEMENT OF PUBLIC IMPROVEMENT WORK. THE CONSTRUCTION SUPERVISOR WILL DISCUSS ANY REPAIR WORK TO FRONTAGE IMPROVEMENTS WHICH ARE NOT SHOWN ON THE PLANS.
4. PRIOR TO FINAL INSPECTION, THE APPLICANT SHALL OBTAIN AN ENCROACHMENT PERMIT FROM THE CITY'S ENGINEERING DIVISION FOR ALL EXISTING PRIVATE STRUCTURES, IMPROVEMENTS AND LANDSCAPING (IF ANY) LOCATED IN THE CITY'S RIGHT-OF-WAY ALONG THE PROPERTY FRONTAGE.
5. THE APPLICANT SHALL REMOVE AND REPLACE ALL CRACKED, DAMAGED UPLIFTED OR DEPRESSED FRONTAGE IMPROVEMENTS (CURB, GUTTER, SIDEWALK, DRIVEWAY, ETC.), EXISTING OR DAMAGED BY THE CONSTRUCTION ACTIVITIES, PER CITY STANDARDS ALONG THE ENTIRE PROPERTY FRONTAGE. IF FRONTAGE IMPROVEMENTS DO NOT CURRENTLY EXIST, THE APPLICANT IS REQUIRED TO INSTALL FRONTAGE IMPROVEMENTS PER CITY STANDARDS ALONG THE ENTIRE PROPERTY FRONTAGE. ALL IMPROVEMENTS ARE TO BE COMPLETED AND APPROVED BY THE CITY OF MENLO PARK'S PUBLIC WORKS' INSPECTOR PRIOR TO THE FINAL INSPECTION BY THE BUILDING INSPECTOR.

NOTES

CONTRACTOR SHALL OBTAIN THE PROPER PERMITS PRIOR TO ANY GRADING.

A SEPARATE PERMIT IS REQUIRED FOR ANY & ALL WORK WITHIN THE CITY RIGHT-OF-WAY. HOWEVER, THE CONTRACTOR(S) SHALL OBTAIN AN APPROVED STREET WORK (ENCROACHMENT PERMIT) PERMIT FROM THE PUBLIC WORKS DEPARTMENT PRIOR TO THE COMMENCEMENT OF THIS WORK WITHIN THE CITY RIGHT-OF-WAY.

ALL GRADED SLOPES SHALL BE PLANTED WITH FAST GROWING, DEEP ROOTED GROUND COVER TO REDUCE THE EROSION DURING HEAVY RAINS.

REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION, INCLUDING BUT NOT LIMITED TO: ADDITIONAL UTILITY SERVICES, DIMENSION CONTROL, DEMOLITION, DETAILS, TREE PROTECTION MEASURES, AND LANDSCAPING.

FINISHED GRADE ELEVATIONS NOTED AS (FG (MAX.)) ARE THE MAXIMUM ALLOWABLE GRADE AT THE BUILDING PERIMETER TO PROVIDE 6" MIN. CLEAR TO GRADE PER C.B.C. SECTION 2317.B. THESE GRADES MAY BE LOWER PROVIDED PROPER FLOW AWAY FROM THE FOUNDATION IS ACHIEVED. REFER TO ARCHITECTURAL & STRUCTURAL DRAWINGS FOR SPECIAL DETAILS AS REQUIRED.

CONTRACTOR SHALL NOTIFY THE OWNER AND/OR MAINTENANCE STAFF IN WRITING OF THE NEED OF PERIODIC MAINTENANCE OF THE DRAINAGE SYSTEM AND STRUCTURES.



- NOTE:** SITE GRADING SHALL NOT IMPED DRAINAGE FROM ADJACENT PROPERTIES AND SHALL NOT GENERATE SURFACE RUN-OFF FLOW ONTO ADJACENT PROPERTIES.
- NOTE:** ALL UTILITIES SHALL BE PLACED UNDERGROUND IF REQUIRED BY THE UTILITY COMPANY.
- BUILDING PAD NOTE:** ADJUST PAD LEVEL AS REQUIRED. REFER TO STRUCTURAL PLANS FOR SLAB SECTION OR GRAIL SPACE DEPTH TO ESTABLISH PAD LEVEL.
- NEW AND EXISTING USE DESIGNATION:** REFER TO SHEET C-4 AND SHEET C-5 FOR THE AREAS DESIGNATION OF COMMON AREA AND PRIVATE OPEN SPACE AREA.

INSPECTION NOTE:

THE CONTRACTOR SHALL INFORM THE OWNER (IN WRITING) OF RECOMMENDED PERIODIC INSPECTION AND MAINTENANCE OF THE ON-SITE STORM DRAINAGE SYSTEM. THE REGULAR CLEARING OF SILT AND DEBRIS IS ESPECIALLY IMPORTANT PRIOR TO EACH RAINY SEASON.

NOTE: CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND DEPTHS OF EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION. CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY CONFLICTS IN ANY EXIST.

LEA & BRAZE ENGINEERING, INC.
 CIVIL ENGINEERS • LAND SURVEYORS
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LAUREL OAKS SUBDIVISION
1273 & 1281 LAUREL STREET
MENLO PARK, CALIFORNIA

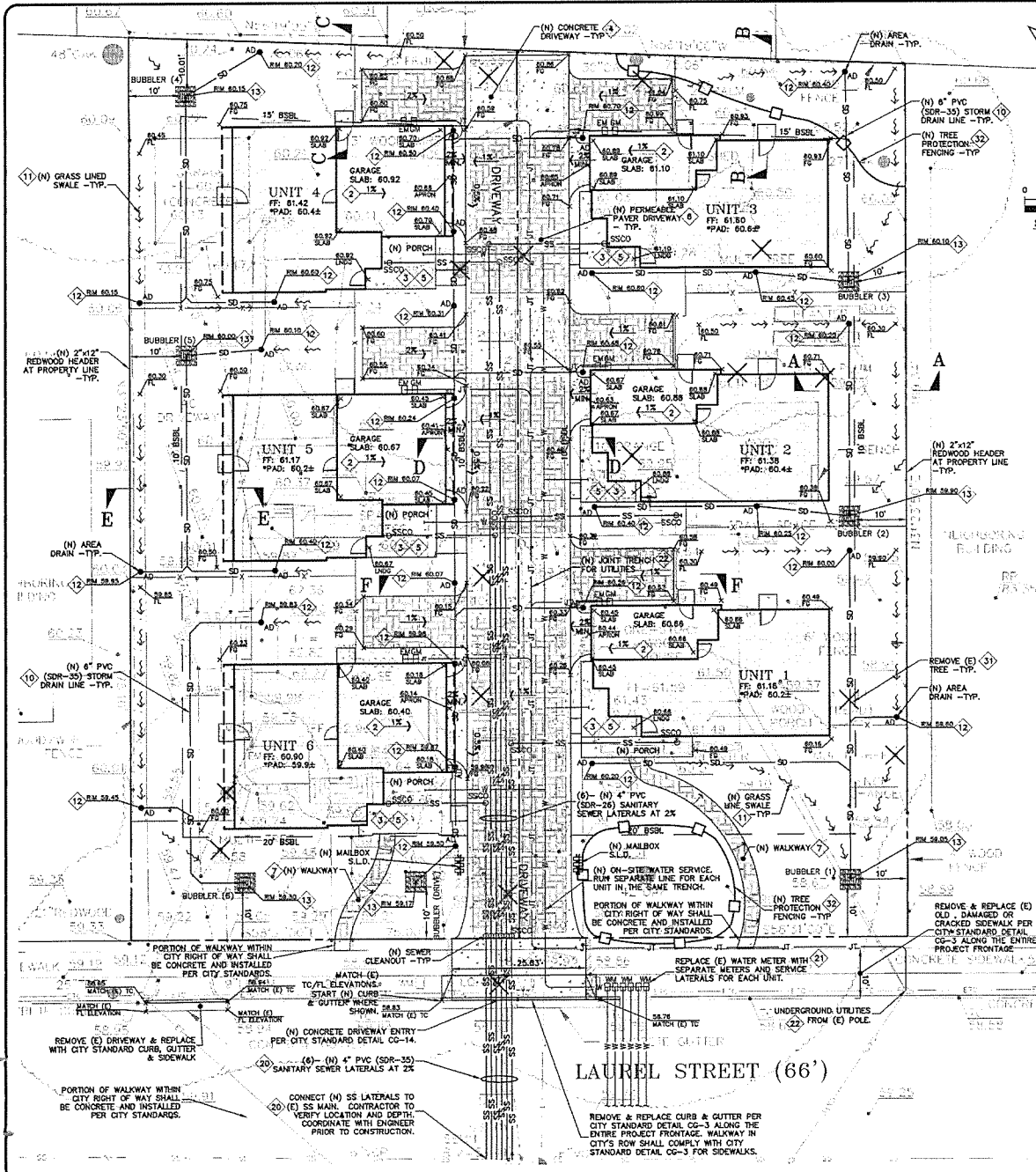
SITE PLAN

REVISIONS	BY

JOB NO: 2120337
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C-3

3 OF 10 SHEETS



- FLATWORK**
- FINISHED GRADES AT BUILDING PERIMETER SHALL BE SLOPED AT A MINIMUM OF 2% FOR THE FIRST 10' AWAY FROM THE BUILDING PER CBC 1804.A3. OR TO AN APPROVED DRAINAGE SWALE OR STRUCTURE. GRADES SHALL CONTINUE TO SLOPE TOWARDS POSITIVE DRAINAGE AND A POSITIVE OUTFALL MAINTAIN 6" CLEARANCE BETWEEN FIRST EARTHEN GRADE AND BOTTOM OF HAD SWL AT ALL TIMES PER CBC 2204.11.2. USGS STRUCTURAL DETAILING ALLOWS LESS. REFER TO STRUCTURAL PLANS FOR FOUNDATION DESIGN AND DETAILS.
 - SLOPE GARAGE SLAB 1% MINIMUM (1/8" PER FOOT) FROM BACK TO FRONT TO ALLOW FOR ADEQUATE DRAINAGE. MAINTAIN 1/2" LIP BETWEEN GARAGE SLAB AND DRIVEWAY. SEE PLANS FOR SPECIFIC DROP
 - PROVIDE 2% (1/4" MIN.) SLOPE ACROSS FLAT WORK AND/OR PAVING PER CBC 1804.A3. SLOPE TOWARDS POSITIVE DRAINAGE AS SHOWN ON PLAN.
 - (N) CONCRETE DRIVEWAY. SEE DETAIL 6 SHEET C-8.
 - (N) CONCRETE PATIOS/WALKWAYS. SEE DETAIL 9 SHEET C-8.
 - (N) PERMEABLE PAVER DRIVEWAY.
 - (N) PERMEABLE PAVER WALKWAY.
- STORM DRAIN**
- INSTALL (N) ON-SITE STORM DRAIN SYSTEM. USE MINIMUM 6" PVC (SDR 35) OR HDPE (ADS N-12 W/ SMOOTH INTERIOR WALLS). MAINTAIN 24" MINIMUM COVER AND SLOPED AT 1% MINIMUM AT ALL TIMES UNLESS OTHERWISE NOTED. PROVIDE CLEAN OUT TO GRADE AT MAJOR CHANGES IN DIRECTION. AVOID USING 90° BENDS AND INSTEAD USE (2) 45° BENDS AND WVC CONNECTORS.
 - CONSTRUCT (N) VEGETATED SWALE SLOPED AT 1% MINIMUM TOWARDS POSITIVE OUTFALL. SEE DETAIL 14 ON SHEET C-8.
 - INSTALL (N) "CHRISTY Y-1" AREA DRAINS. CONNECT TO ON-SITE STORM DRAIN SYSTEM. SEE DETAIL 1 ON SHEET C-8.
 - INSTALL (N) RETENTION & FILTRATION SYSTEM WITH STORAGE PIPE AND BUBBLER BOX PER CITY OF MENLO PARK TYPICAL FILTER MEDIUM AREA DETAIL DR-19.
- UTILITIES**
- INSTALL (N) SANITARY SEWER LATERALS. USE 4" PVC (SDR-26) SLOPED AT 2% MINIMUM. CONNECT TO (E) SEWER MAIN AS SHOWN. PROVIDE CLEANOUT TO GRADE AT BUILDING AND BEHIND PROPERTY LINE AND AT MAJOR CHANGES IN DIRECTION AS SHOWN. REUSE (E) LATERAL IF POSSIBLE. CONNECT PER DISTRICT STANDARDS.
 - CONNECT (N) WATER SERVICE PER WATER DISTRICT STANDARDS. INSTALL (N) WATER METER PER WATER DISTRICT STANDARDS AS APPLICABLE.
 - INSTALL (N) 2" MINIMUM SERVICE LINE TO (N) RESIDENCE OR AS DIRECTED BY FIRE SPRINKLER DESIGNER.
 - INSTALL (N) UNDERGROUND JUMP TRENCH FOR SERVICES INCLUDING GAS, CABLE & ELECTRIC FROM NEAREST POINT OF CONNECTION. DESIGN BY OTHERS.
- DEMOLITION**
- DEMOLISH (E) IMPROVEMENTS AS NECESSARY TO ACCOMMODATE (N) CONSTRUCTION. NO DEMOLITION SHALL COMMENCE WITHOUT REQUIRED DEMOLITION PERMITS.
 - REMOVE (E) TREE. CONTRACTOR SHALL OBTAIN THE PROPER TREE REMOVAL PERMITS AS REQUIRED.
 - PROVIDE TREE PROTECTION AROUND TREES TO REMAIN. SEE DETAIL 5 ON SHEET C-8.

- NOTES**
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 - CONTACT PUBLIC WORKS AT (650) 330-6740 TO SCHEDULE AN INSPECTION A MINIMUM OF 24 HOURS IN ADVANCE OF COMMENCEMENT OF PUBLIC IMPROVEMENT WORK. THE CONSTRUCTION SUPERVISOR WILL DISCUSS ANY REPAIR WORK TO FRONTAGE IMPROVEMENTS WHICH ARE NOT SHOWN ON THE PLANS.
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* BUILDING PAD NOTE: ADJUST PAD LEVEL AS REQUIRED. REFER TO STRUCTURAL PLANS FOR SLAB SECTION OR CRAWL SPACE DEPTH TO ESTABLISH PAD LEVEL.

INSPECTION NOTE: THE CONTRACTOR SHALL INFORM THE OWNER (IN WRITING) OF RECOMMENDED PERIODIC INSPECTION AND MAINTENANCE OF THE ON-SITE STORM DRAINAGE SYSTEM. THE REGULAR CLEARING OF SILT AND DEBRIS IS ESPECIALLY IMPORTANT PRIOR TO EACH RAINY SEASON.

NEW AND EXISTING USE DESIGNATION: REFER TO SHEET C-4 AND SHEET C-9 FOR THE AREAS DESIGNATION OF COMMON AREA AND PRIVATE OPEN SPACE AREA.

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**LAUREL OAKS SUBDIVISION
1273 & 1281 LAUREL STREET
MENLO PARK, CALIFORNIA**

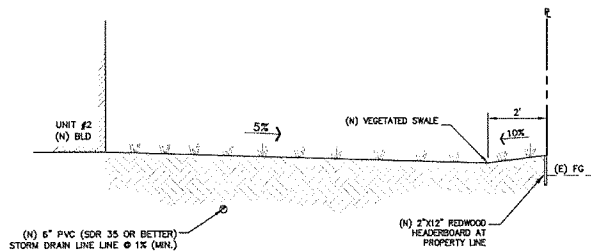
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**PRELIMINARY
GRADING &
DRAINAGE PLAN**

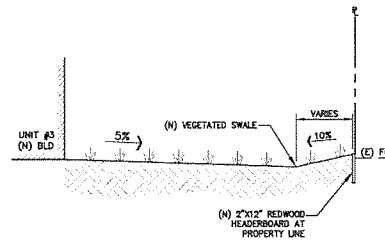
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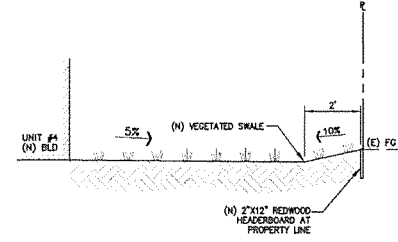
C-4
4 of 10 SHEETS



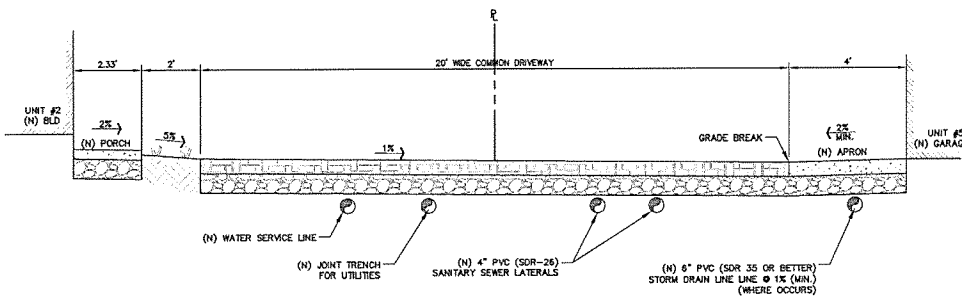
SECTION A-A
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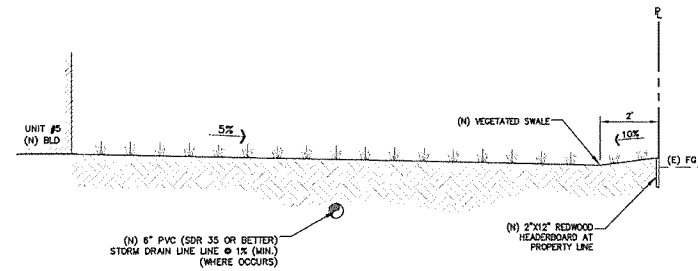
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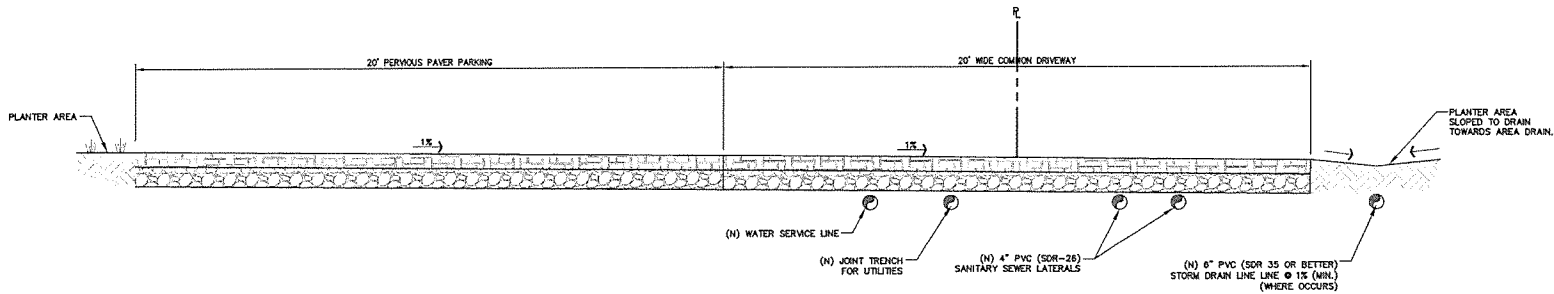
SECTION C-C
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SECTION D-D
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SECTION E-E
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SECTION F-F
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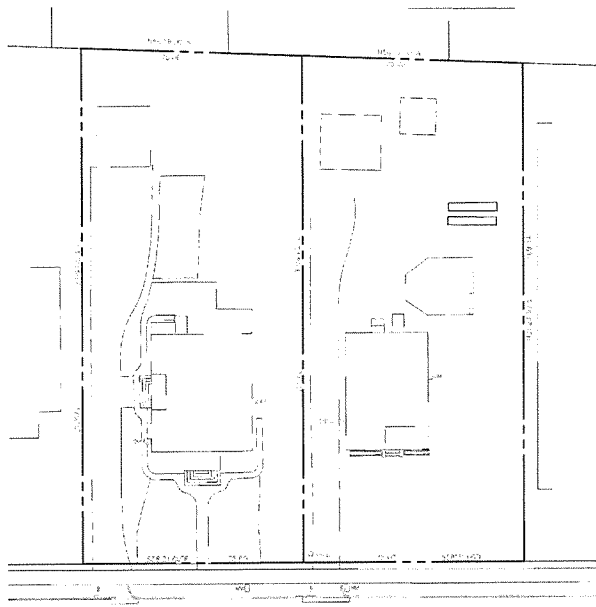
LEA & BRAZE ENGINEERING, INC.
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 APR. 06. 05. 05. 03. 02.
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SECTIONS

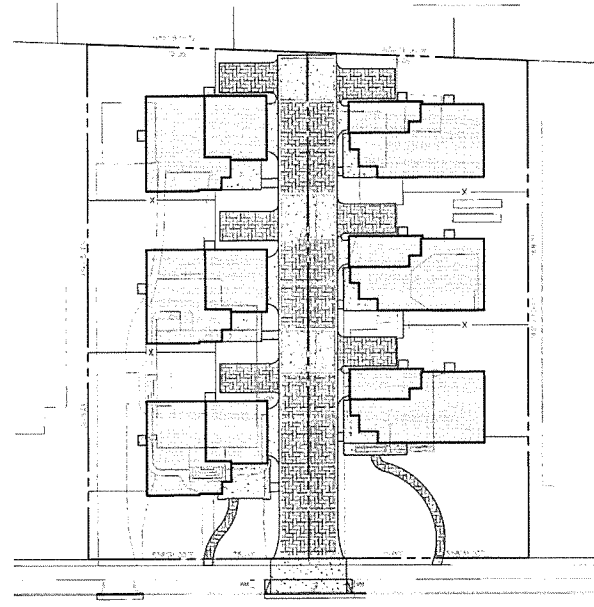
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LAUREL STREET (66')

EXISTING SITE



LAUREL STREET (66')

PROPOSED SITE

DEVELOPMENT INFORMATION

TOTAL AREA OF PARCEL		A	25,833 SF
EXISTING PERVIOUS AREA		B	17,586 SF
EXISTING IMPERVIOUS AREA		C	8,247 SF
EXISTING % IMPERVIOUS	$C/A \times 100 =$	D	31.9% SF
EXISTING IMPERVIOUS AREA TO BE REPLACED W/NEW IMPERVIOUS AREA		E	3,784 SF
EXISTING PERVIOUS AREA TO BE REPLACED W/NEW IMPERVIOUS AREA		F	5,651 SF
NEW IMPERVIOUS AREA (CREATING AND/OR REPLACING)	$E + F =$	G	9,434 SF
IF G IS GREATER THAN 10,000 SF, A HYDROLOGY REPORT SHALL BE SUBMITTED TO ENGINEERING			
EXISTING IMPERVIOUS AREA TO BE REPLACES W/NEW PERVIOUS AREA		H	4,464 SF
NET CHANGE IN IMPERVIOUS AREA	$F - H =$	I	1,187 SF
INPUT NEGATIVE (-) NUMBER IF THE F (NET CHANGE) IS NEGATIVE			
PROPOSED PERVIOUS AREA	$B - I =$	J	16,399 SF
PROPOSED IMPERVIOUS AREA	$C + I =$	K	9,434 SF
VERIFY THAT $J + K = A$			25,833
PROPOSED % IMPERVIOUS	$K/A \times 100 =$	L	36.5%

DEVELOPMENT AREA SUMMARY

PRE-DEVELOPMENT	(SQFT)
BUILDING	3,224
DRIVEWAY & PARKING	3,336
PATIOS, WALKWAYS & PADS	1,054
WOOD DECKS	633
TOTAL	8,247
POST-DEVELOPMENT	(SQFT)
BUILDINGS	6,768
DRIVEWAY & PARKING	1,714
PATIOS, WALKWAYS & PADS	952
WOOD DECKS	0
PERVIOUS DRIVEWAY/WALKWAY (2977 SF)	0
TOTAL	9,434
DIFFERENCE (INCREASE)	1,187

LEA & BRAZE ENGINEERING, INC.
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LAUREL OAKS SUBDIVISION
 1273 & 1281 LAUREL STREET
 MENLO PARK, CALIFORNIA
 APR 05 05:45:03
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 SAN MATEO COUNTY

IMPERVIOUS AREA
 EXHIBIT

NO.	REVISIONS	BY
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C-6

0 OF 10 SHEETS

GENERAL NOTES

ALL GENERAL NOTES, SHEET NOTES, AND LEGEND NOTES FOUND IN THESE DOCUMENTS SHALL APPLY THROUGHOUT. IF INCONSISTENCIES ARE FOUND IN THE VARIOUS NOTATIONS, NOTIFY THE ENGINEER IMMEDIATELY BY WRITING REQUESTING CLARIFICATION.

THESE DRAWINGS AND THEIR CONTENT ARE AND SHALL REMAIN THE PROPERTY OF LEA AND BRAZE ENGINEERS, INC. IN WHICHEVER CASE THE PROJECT OR THE DRAWINGS ARE PROVIDED TO ANY OTHER PARTY, THEY ARE NOT TO BE USED BY ANY PERSON OR OTHER PERSONS OR EXTENDING OF THE PROJECT EXCEPT BY AGREEMENT IN WRITING AND WITH APPROPRIATE COMPARISON TO THE ENGINEER.

ALL WORK SHALL COMPLY WITH APPLICABLE CODES AND TRADE STANDARDS WHICH GOVERN EACH PHASE OF WORK INCLUDING, BUT NOT LIMITED TO, CALIFORNIA MECHANICAL CODE, CALIFORNIA PLUMBING CODE, CALIFORNIA ELECTRICAL CODE, CALIFORNIA FIRE CODE, CALIFORNIA STANDARDS AND SPECIFICATIONS, AND ALL APPLICABLE STATE AND/OR LOCAL CODES AND/OR LEGISLATION.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR AND ALL SUBCONTRACTORS TO CHECK AND VERIFY ALL CONDITIONS, DIMENSIONS, LINES AND LEVELS INDICATED. PREFER FIT AND ADJUSTMENT OF ALL PARTS IS REQUIRED. SHOULD THERE BE ANY DISCREPANCIES, IMMEDIATELY NOTIFY THE ENGINEER FOR CORRECTION OR ADJUSTMENT THE EVENT OF FAILURE TO DO SO, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTION OF ANY ERROR.

ALL DIMENSIONS AND CONDITIONS SHALL BE CHECKED AND VERIFIED IN THE JOB BY EACH SUBCONTRACTOR BEFORE HE/BE/HE/HE/S/HER WORK. ANY ERRORS, OMISSIONS, OR DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER/CONTRACTOR BEFORE CONSTRUCTION BEGINS.

COMPLETION OF WORK BY THE CONTRACTOR AND/OR SUBCONTRACTOR SHALL INDICATE KNOWLEDGE AND ACCEPTANCE OF ALL CONDITIONS DESCRIBED IN THESE CONSTRUCTION DOCUMENTS, OR EXISTING ON SITE, WHICH COULD AFFECT THEIR WORK.

WORK SEQUENCES

IN THE EVENT ANY SPECIAL SEQUENCING OF THE WORK IS REQUIRED BY THE OWNER OR THE CONTRACTOR, THE CONTRACTOR SHALL MAKE A CONFORMANCE BEFORE ANY SUCH WORK IS BEGUN. SITE EXAMINATION: THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL THOROUGHLY EXAMINE THE SITE AND FAMILIARIZE HIM/HERSELF WITH THE CONDITIONS UNDER WHICH THE WORK IS TO BE PERFORMED. THE CONTRACTOR SHALL VERIFY AT THE SITE ALL MEASUREMENTS AFFECTING HEIGHTS, SIZES AND SHALL BE RESPONSIBLE FOR THE CORRECTION OF THE SAME. NO EXTRA COMENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR EXPENSES DUE TO HIS/HER NEGLIGENCE TO EXAMINE OR VERIFY THE CONDITIONS OF THE SITE BEFORE THE WORK BEGINS.

LEA AND BRAZE ENGINEERING, INC. EXPRESSLY RECEIVES ITS COMMON LAW COVENANT AND OTHER PROPERTY RIGHTS IN THESE PLANS. THESE PLANS ARE NOT TO BE REPRODUCED, CHANGED OR COPIED IN ANY FORM OR MANNER WHATSOEVER, NOR ARE THEY TO BE ASSIGNED TO A THIRD PARTY WITHOUT FIRST OBTAINING THE WRITTEN PERMISSION AND CONSENT OF LEA AND BRAZE ENGINEERING, INC. IN THE EVENT OF UNAUTHORIZED REUSE OF THESE PLANS BY A THIRD PARTY, THE THIRD PARTY SHALL HOLD HARMLESS LEA AND BRAZE ENGINEERING, INC.

CONSTRUCTION IS ALWAYS LESS THAN PERFECT SINCE PROJECTS REQUIRE THE COORDINATION AND INSTALLATION OF MANY INDIVIDUAL COMPONENTS BY VARIOUS CONSTRUCTION INDUSTRY TRADES. THESE DOCUMENTS CANNOT FORESIGHT ALL COMPONENTS OR ASSEMBLIES EXACTLY. IN THE EVENT OF THESE ENDS/MEASUREMENTS THAT THEY REPRESENT A REASONABLE STANDARD OF CARE IN THEIR CONDUCT. IT IS ALSO PROVIDED BY THESE DOCUMENTS THAT CONSTRUCTION FROM SERVICES WILL BE PROVIDED BY THE ENGINEER. SHOULD THE OWNER NOT RETAIN THE ENGINEER TO PROVIDE SUCH SERVICES, OR SHOULD HE/BE/HE/HE/S/HER RETAIN THE ENGINEER TO PROVIDE ONLY LIMITED SERVICES, THEN IT SHALL BE THE OWNER'S AND CONTRACTOR'S RESPONSIBILITY TO FULLY RECOGNIZE AND PROVIDE THAT STANDARD OF CARE.

IF THE OWNER OR CONTRACTOR OBSERVES OR OTHERWISE BECOMES AWARE OF ANY FAULT OR DEFECT IN THE PROJECT OR NONCOMFORMANCE WITH THE CONTRACT DOCUMENTS, PROMPT WRITTEN NOTICE THEREOF SHALL BE GIVEN BY THE OWNER AND/OR CONTRACTOR TO THE ENGINEER.

THE ENGINEER SHALL NOT HAVE CONTROL OR CHARGE OF OR SHALL NOT BE RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES, OR PROCEDURES, OR FOR SAFETY, PROTECTIVE AND PROVISIONAL CONSTRUCTION MEASURES. THE ENGINEER SHALL BE RESPONSIBLE FOR THE CONTRACTOR, SUBCONTRACTORS, OR ANY OTHER PERSONS PERFORMING ANY OF THE WORK, OR FOR THE FAILURE OF ANY OF THEM TO CARRY OUT THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.

SITE PROTECTION

PROTECT ALL UNDESIRING THAT IS TO REMAIN. ANY DAMAGE OR LOSS RESULTING FROM EXCAVATION, DRIVING, OR CONSTRUCTION SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION OF ALL EXISTING UTILITIES AND SHALL COORDINATE THEIR REMOVAL OR RELOCATION AS APPLICABLE TO AVOID ANY INTERRUPTION OF SERVICE TO ADJACENT AREAS. THE CONTRACTOR SHALL MAINTAIN MAINTENANCE OF MECHANICAL REGULATIONS AND CARRY OUT HIS/HER WORK IN COMPLIANCE WITH ALL FEDERAL, STATE REQUIREMENTS TO REDUCE FIRE HAZARDS AND HARMFUL TO THE PUBLIC.

STORMWATER POLLUTION PREVENTION NOTES

- 1) STORE, HANDLE, AND DISPOSE OF CONSTRUCTION MATERIALS AND WASTES PROPERLY, SO AS TO PREVENT THEM FROM COMING IN CONTACT WITH STORMWATER.
- 2) CONTROL AND PREVENT THE DISCHARGE OF ALL POTENTIAL POLLUTANTS, INCLUDING SOLID WASTES, PAINTS, CONCRETE, PETROLEUM PRODUCTS, CHEMICALS, SEWER WATER, OR SEDIMENT, AND NON-STORMWATER DISCHARGES TO STORM DRAINS AND WATER COURSES.
- 3) USE SEDIMENT CONTROL OR FILTRATION TO REMOVE SEDIMENT FROM DENATURING EFFLUENT.
- 4) AVOID CLEANING, FUELING, OR MAINTAINING VEHICLES ON SITE, EXCEPT IN A DESIGNATED AREA IN WHICH RAINFALL IS CONTAINED AND TREATED.
- 5) DELINEATE CLEANING LIMITS, FERTILIZERS, SEDIMENT, OR OTHER AREAS, BUFFER ZONES, TREES AND HERCULEAN CORALS WITH FENCE.
- 6) PROTECT EXISTING CHANNELS AND UNDISTURBED AREAS FROM CONSTRUCTION IMPACTS USING VEGETATIVE BUFFER STRIPS, SEDIMENT BARRIERS, FILTERS, DICES, MOUNDING, OR OTHER MEASURES AS APPROPRIATE.
- 7) PERFORM CLEANING AND EARLY MOVING ACTIVITIES DURING DRY WEATHER TO THE MAXIMUM EXTENT PRACTICAL.
- 8) LIMIT AND TIME THE APPLICATIONS OF PESTICIDES AND FERTILIZERS TO PREVENT POLLUTED RAINFALL.
- 9) LIMIT CONSTRUCTION ACCESS ROUTES AND STABILIZED DESIGNATED ACCESS POINTS.
- 10) AVOID TRACKING DIRT OR MATERIALS OFF-SITE. CLEAN OFF-SITE PAVED AREAS AND SIDEWALKS USING DRY SWEEPING METHODS TO THE MAXIMUM EXTENT PRACTICAL.

SUPPLEMENTAL MEASURES

- A. THE PHRASE "NO CLAMPING - DRAINS TO BUY" OR EQUALLY EFFECTIVE PHRASE MUST BE LABELED ON STORM DRAIN BELTS (OR STENCILING, BRANDING, OR PLACING) TO ALERT THE PUBLIC TO THE DIRECTION OF STORM WATER FLOW AND TO PROMPT DIRECT OBSERVANCE OF POLLUTANTS INTO THE STORM DRAIN.
- B. USING FILTRATION MATERIALS ON STORM DRAIN COVERS TO REMOVE SEDIMENT FROM DENATURING EFFLUENT.
- C. STABILIZING ALL EXPOSED AREAS AND MAINTAINING EROSION CONTROL MEASURES CONTINUOUSLY FROM OCTOBER 15 AND AFTER, IS.
- D. REMOVING SPILLS PROMPTLY, AND AVOID STOCKPILING OF FILL MATERIALS. WHEN RAIN IS FORECAST, IF RAIN THEREAFTER, STOCKPILED SOLIDS AND OTHER MATERIALS SHALL BE COVERED WITH A TARP OR OTHER WEATHERPROOF MATERIAL.
- E. STORING, HANDLING, AND DISPOSING OF CONSTRUCTION MATERIALS AND WASTES SO AS TO AVOID THEIR FLOW TO THE STORM DRAIN SYSTEMS OR WATER BODY.
- F. AVOIDING CLEANING, FUELING, OR MAINTAINING VEHICLES ON-SITE, EXCEPT IN AN AREA DESIGNATED TO CONTAIN AND TREAT RAINFALL.

GRADING & DRAINAGE NOTES:

1. SCOPE OF WORK

THESE SPECIFICATIONS AND APPLICABLE PLANS PERTAIN TO AND INCLUDE ALL SITE GRADING AND EARTHWORK ASSOCIATED WITH THE PROJECT INCLUDING, BUT NOT LIMITED TO THE FURNISHING OF ALL LABOR, TOOLS AND EQUIPMENT NECESSARY FOR SITE GRADING AND GRADING, SITE PREPARATION, REMOVAL OF EXCESS OR UNDESIRABLE MATERIAL, EXISTING UTILITY EXCAVATION, EXISTING EXCAVATION, RECONSTRUCTION PREPARATION FOR SOIL, REDUCING FILL PLACEMENT, FOUNDATION OF SLABS EXCAVATION, RECONSTRUCTION OF ANY REQUIRED FILL MATERIAL, PROCESSING, PLACEMENT AND COMPACTION OF FILL, AND SUPPORTIVE WORK NECESSARY TO COMPLETE THE GRADING TO CONFORM TO THE LINES, GRADING AND BENCH MARKS SHOWN ON THE PROJECT GRADING PLANS.

2. GENERAL

- A. ALL SITE GRADING AND EARTHWORK SHALL CONFORM TO THE RECOMMENDATIONS OF THESE SPECIFICATIONS, THE SOILS REPORT, AND THE CITY OF MENLO PARK GRADING ORDINANCE.
- B. ALL FILL MATERIALS SHALL BE DENSITY SO AS TO PRODUCE A DENSITY NOT LESS THAN 90% RELATIVE COMPACTION BASED UPON ASTM TEST DESIGNATION D1557. FIELD DENSITY TEST WILL BE PERFORMED IN ACCORDANCE WITH ASTM TEST DESIGNATION D1558 AND JUNE. THE LOCATION AND FREQUENCY OF THE FIELD DENSITY TEST WILL BE AS DETERMINED BY THE SOIL ENGINEER. THE RESULTS OF THESE TESTS AND COMPLIANCE WITH THE SPECIFICATIONS WILL BE THE BASIS UPON WHICH SATISFACTORY COMPLETION OF THE WORK WILL BE JUDGED BY THE SOIL ENGINEER. ALL CUT AND FILL SLOPES SHALL BE CONSTRUCTED AS SHOWN ON PLANS, BUT NO STEEPER THAN (2) HORIZONTAL TO ONE (1) VERTICAL.
- C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SATISFACTORY COMPLETION OF ALL THE EARTHWORK IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS. NO DEVIATION FROM THESE SPECIFICATIONS SHALL BE MADE EXCEPT UPON WRITTEN APPROVAL BY THE SOILS ENGINEER. BOTH CUT AND FILL AREAS SHALL BE SURFACE COMPLETED TO THE SATISFACTION OF THE SOILS ENGINEER AT THE COMPLETION OF ALL GRADING OPERATIONS AND PRIOR TO FINAL ACCEPTANCE. THE CONTRACTOR SHALL NOTIFY THE SOILS ENGINEER AT LEAST TWO (2) WORKING DAYS PRIOR TO DOING ANY SITE GRADING AND EARTHWORK INCLUDING CLEARING.

3. CLEARING AND CURBING

- A. THE CONTRACTOR SHALL ACCEPT THE SITE IN ITS PRESENT CONDITION. ALL EXISTING PUBLIC IMPROVEMENTS SHALL BE PROTECTED. ANY IMPROVEMENTS DAMAGED SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE LOCAL JURISDICTION WITH NO EXTRA COMPENSATION.
- B. ALL ADJACENT BUILDINGS AND VEGETATION, TREE EXCEPT THOSE SPECIFIED TO REMAIN FOR LANDSCAPING PURPOSES, FENCES, FORDWAYS, AND ANY SURFACE DEBRIS SHALL BE REMOVED AND DISPOSED OF OFF THE SITE BY THE CONTRACTOR.
- C. ALL ADJACENT SEWER TANKS AND ANY OTHER SURFACE STRUCTURES EXISTING IN PROPOSED DEVELOPMENT AREAS SHALL BE REMOVED PRIOR TO ANY GRADING OR FILL OPERATION. ALL APPURTENANT DRAIN FIELDS AND OTHER CONNECTING LINES MUST ALSO BE TOTALLY REMOVED.
- D. ALL UNDESIRABLE UNDERGROUND IRREGULAR OR UTILITY LINES SHALL BE REMOVED OR DEMOLISHED. THE APPROPRIATE FINAL POSITION OF SUCH LINES DEPENDING UPON THEIR DEPTH AND LOCATION AND THE METHOD OF REMOVAL, OR LOCATION SHALL BE DETERMINED BY THE SOILS ENGINEER. ONE OF THE FOLLOWING METHODS WILL BE USED:
 - (1) EXCAVATE AND TOTALLY REMOVE THE UTILITY LINE FROM THE TRENCH.
 - (2) DIGGAGE AND CRUSH THE UTILITY LINE IN THE TRENCH.
 - (3) CAP THE ENDS OF THE UTILITY LINE WITH CONCRETE TO PREVENT THE ENTRANCE OF WATER. THE LOCATIONS AT WHICH THE UTILITY LINE WILL BE CAPPED WILL BE DETERMINED BY THE UTILITY DISTRICT ENGINEER. THE LENGTH OF THE CAP SHALL NOT BE LESS THAN FIVE FEET AND THE COMPACTED MIX EMPLOYED SHALL HAVE MINIMUM STRENGTH.

4. SITE PREPARATION AND STRIPPING

- A. SURFACE ORGANIC SHALL BE STRIPPED AND REMOVED FROM BUILDING PAIS, AREAS TO RECEIVE COMPACTED FILL AND PAVEMENT AREAS.
- B. UPON THE COMPLETION OF THE ORGANIC STRIPPING OPERATION, THE GROUND SURFACE (NATIVE SOIL) SUBSISTED OVER THE ENTIRE AREA OF ALL BUILDING PAIS, STREET AND PAVEMENT AREAS AND ALL AREAS TO RECEIVE COMPACTED FILL SHALL BE PLUMPED OR SCARIFIED UNTIL THE SURFACE IS FREE OF ROOTS, HANGINGS OR OTHER UNWANTED FEATURES WHICH MAY IMPAIR UNIFORM SOIL COMPACTION. THE GROUND SURFACE SHALL THEN BE DISCED OR BLEADED TO A DEPTH OF AT LEAST 4 INCHES UPON THE ENGINEER'S SATISFACTION. THE BLEADING SHALL BE WORKED DOWNWARD AND RECOMPACTED PER REQUIREMENTS FOR COMPACTING FILL MATERIAL.

5. EXCAVATION

- A. UPON COMPLETION OF THE CLEARING AND CURBING, SITE PREPARATION AND STRIPPING, THE CONTRACTOR SHALL MAKE DETAILED NOTATIONS TO THE PLAN. THESE NOTES SHOULD BE BY THE SOILS ENGINEER. UNDESIRABLE NATIVE SOIL OR UNDESIRABLE FILL SHALL BE OVOID EXCAVATION OF THE UNDESIRABLE MATERIAL, RESULTING GROUND LINE SHALL BE SCARIFIED, MOISTURE-CONDITIONED AND RECOMPACTED AS SPECIFIED IN SECTION 4 OF THESE SPECIFICATIONS. COMPACTED FILL MATERIAL SHALL BE PLACED TO BRING GROUND LEVEL BACK TO DESIGN GRADE.
- B. EXCAVATED MATERIALS SUITABLE FOR COMPACTED FILL MATERIAL SHALL BE UTILIZED IN MAINTAINING THE REQUIRED COMPACTED FILL. THOSE NATIVE MATERIALS CONSIDERED UNSUITABLE BY THE SOILS ENGINEER SHALL BE DISPOSED OF OFF THE SITE BY THE CONTRACTOR.

6. FINISH SURFACING AND COMPACTION FILL MATERIAL

- A. FILL MATERIALS
THE MATERIALS PROPOSED FOR USE AS COMPACTED FILL SHALL BE APPROVED BY THE SOILS ENGINEER. THE MATERIALS SHALL BE UNDESIRABLE NATIVE SOIL. THE MATERIAL SHALL BE SUIABLE FOR FILL. HOWEVER, ANY NATIVE MATERIAL, DESIGNATED UNSUITABLE BY THE SOILS ENGINEER SHALL BE REMOVED FROM THE SITE BY THE CONTRACTOR. ANY IMPORTED MATERIAL SHALL BE APPROVED FOR USE BY THE SOILS ENGINEER. IN WRITING, BEFORE BEING IMPORTED TO THE SITE, AND SHALL POSSESS SUFFICIENT FINES TO PRODUCE A COMPACTIBLE SOIL MATRIX AND SHALL BE FREE OF VEGETATIVE AND ORGANIC MATTER AND OTHER DELETERIOUS MATERIALS. ALL FILL VOLUM SHALL BE FILLED AND PROPERLY COMPACTED. NO ROCKS LARGER THAN THREE INCHES IN DIAMETER SHALL BE PERMITTED.
- B. FILL CONSTRUCTION
THE SOILS ENGINEER SHALL APPROVE THE NATIVE SOIL SUBGRADE BEFORE PLACEMENT OF ANY COMPACTED FILL MATERIAL. UNSUITABLE NATIVE SOIL SHALL BE REMOVED AS DIRECTED BY THE SOILS ENGINEER. THE RESULTING GROUND LINE SHALL BE SCARIFIED, MOISTURE-CONDITIONED AND RECOMPACTED AS SPECIFIED IN SECTION 4 OF THESE SPECIFICATIONS. COMPACTED FILL MATERIAL SHALL BE PLACED TO BRING GROUND LEVEL BACK TO DESIGN GRADE. GROUND PREPARATION SHALL BE FOLLOWED CLOSELY BY FILL OPERATIONS TO PREVENT DRIVING OUT OF THE SUBSOIL BEFORE PLACEMENT OF THE FILL.
THE APPROVED FILL MATERIALS SHALL BE PLACED IN UNIFORM HORIZONTAL LAYERS NO THICKER THAN 6 IN LOOSE. THICKNESS. LAYERS SHALL BE SPREAD EVENLY AND SHALL BE THOROUGHLY BLADE MOED DURING THE SPREADING TO OBTAIN UNIFORMITY OF MATERIAL IN EACH LAYER. THE SCARIFIED SUBGRADE AND FILL MATERIAL SHALL BE MOISTURE-CONDITIONED TO AT LEAST OPTIMUM MOISTURE. WHEN THE MOISTURE CONTENT OF THE FILL IS BELOW THAT SPECIFIED, WATER SHALL BE ADDED UNTIL THE MOISTURE DURING THE COMPACTOR PROCESS. WHEN THE MOISTURE CONTENT OF THE FILL IS ABOVE THAT SPECIFIED, THE FILL MATERIAL SHALL BE AERATED BY BLOWING OR OTHER SATISFACTORY METHODS UNTIL THE MOISTURE CONTENT IS AS SPECIFIED.

AFTER EACH LAYER HAS BEEN PLACED, MOIST, SPREAD, EVENED AND MOISTURE-CONDITIONED, IT SHALL BE COMPACTED TO AT LEAST THE SPECIFIED DENSITY.

THE FILL OPERATION SHALL BE CONTROLLED IN COMPACTED LAYERS AS SPECIFIED ABOVE UNTIL THE FILL HAS REACHED TO THE DESIGN ELEVATION AS SHOWN ON THE PLANS. NO LAYER SHALL BE ALLOWED TO DRY OUT BEFORE SUBSEQUENT LAYERS ARE PLACED.

COMPACTOR EQUIPMENT SHALL BE OF SUCH DESIGN THAT IT WILL BE ABLE TO COMPACT THE FILL TO THE SPECIFIED MINIMUM COMPACTION WITHIN THE SPECIFIED MOISTURE CONTENT RANGE. COMPACTION OF EACH LAYER SHALL BE CONTINUOUS OVER ITS ENTIRE AREA UNTIL THE REQUIRED MINIMUM DENSITY HAS BEEN OBTAINED.

7. CUT OR FILL SLOPES

ALL CONTRACTED SLOPES, BOTH CUT AND FILL, SHALL BE NO STEEPER THAN 2 TO 1 (HORIZONTAL TO VERTICAL). DURING THE GRADING OPERATION, COMPACTED FILL SLOPES SHALL BE OVERLAPPED BY AT LEAST ONE FOOT HORIZONTALLY AT THE COMPLETION OF THE GRADING OPERATION. THE EXCESS FILL EXISTING ON THE SLOPES SHALL BE BLENDED OFF TO CREATE THE PROPOSED SLOPE GRADIENT. ALL CUT AND FILL SLOPES SHALL BE TRACK BLADED AFTER BEING BROUGHT TO FINISH GRADE, AND THEN BE PLANTED WITH EROSION CONTROL SOILS PLANTING. THE SOILS ENGINEER SHALL REVIEW ALL CUT SLOPES TO DETERMINE IF ANY ADVERSE GEOLOGICAL CONDITIONS ARE EXPOSED. IF SUCH CONDITIONS DO OCCUR, THE SOILS ENGINEER SHALL RECOMMEND THE APPROPRIATE MITIGATION MEASURES AT THE TIME OF THEIR DESIGN.

8. SEASONAL LIMITS AND DRAINAGE CONTROL

FILL MATERIALS SHALL NOT BE PLACED, SPREAD OR COMPACTED WHILE IT IS AT AN UNDESIRABLE HIGH MOISTURE CONTENT OR UNDER UNDESIRABLE CONDITIONS. WHEN THE WORK IS INTERRUPTED FOR ANY REASON THE FILL OPERATIONS SHALL NOT BE RESUMED UNTIL FIELD TEST PERFORMED BY THE SOILS ENGINEER INDICATE THAT THE MOISTURE CONDITIONS IN AREAS TO BE FILLED ARE AS PREVIOUSLY SPECIFIED. ALL EARTH MOVED AND WORKING OPERATIONS SHALL BE CONTROLLED TO PREVENT WATER FROM RUNNING AND EXCAVATED AREAS. ALL EXCESS WATER SHALL BE PROMPTLY REMOVED AND THE SITE KEPT DRY.

9. DUST CONTROL

THE CONTRACTOR SHALL TAKE ALL STEPS NECESSARY FOR THE ALLEVIATION OR PREVENTION OF ANY DUST MISCANCE ON OR ABOUT THE SITE CAUSED BY THE CONTRACTOR'S OPERATION OTHER DURING THE PERFORMANCE OF THE GRADING OR RESULTS FROM THE CONDITION IN WHICH THE CONTRACTOR LEAVES THE SITE. THE CONTRACTOR SHALL ASSUME ALL LIABILITY INCLUDING COST OF CO-ORDINATION FOR ALL CLAIMS RELATED TO CUT OR WIN-DOW MATERIALS ATTRIBUTABLE TO HIS WORK. COST FOR THIS ITEM OF WORK IS TO BE INCLUDED IN THE EXCAVATION ITEM AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.

10. EROSION CONTROL

THE CONTRACTOR WILL HOLD HARMLESS, INDEMNIFY AND DEFEND THE ENGINEER, THE OWNER AND HIS CONSULTANTS AND EACH OF THEIR OFFICES AND EMPLOYEES AND AGENTS, FROM ANY AND ALL LIABILITY CLAIMS, LOSSES OR DAMAGES ARISING OR ALLIRED TO HEREIN, BUT NOT INCLUDING THE SOLE NEGLIGENCE OF THE OWNER, THE ARCHITECT, THE ENGINEER AND HIS CONSULTANTS AND EACH OF THEIR OFFICES AND EMPLOYEES AND AGENTS.

11. SAFETY

IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND IS NOT LIMITED TO NORMAL WORKING HOURS.

THE DUTY OF THE ENGINEERS TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO IMPOSE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES, IN, ON OR NEAR THE CONSTRUCTION SITE.

12. GUARANTEE

WHETHER THE FINAL PAYMENT, NOR THE PROVISIONS IN THE CONTRACT, NOR PARTIAL, NOR ENTIRE USE OR OCCUPANCY OF THE PREMISES BY THE OWNER SHALL CONSTITUTE AN ACCEPTANCE OF THE WORK. NOT DONE IN ACCORDANCE WITH THE CONTRACT OR RELIEVES THE CONTRACTOR OF LIABILITY IN RESPECT TO ANY EXPRESS WARRANTIES OR RESPONSIBILITY FOR FAULTY MATERIALS OR WORKMANSHIP.

THE CONTRACTOR SHALL REMEDY ANY DEFECTS IN WORK AND PAY FOR ANY DAMAGE TO OTHER WORK RESULTING THEREFROM WHICH SHALL APPEAR WITHIN A PERIOD OF ONE (1) CALENDAR YEAR FROM THE DATE OF FINAL ACCEPTANCE OF THE WORK.

13. TRENCH BACKFILL

OTHER THE ON-SITE ORGANIC SOIL OR APPROVED IMPORTED SOIL MAY BE USED AS TRENCH BACKFILL. THE BACKFILL MATERIAL SHALL BE MOISTURE-CONDITIONED FOR THESE SPECIFICATIONS AND SHALL BE PLACED IN LIFTS OF NOT MORE THAN SIX INCHES IN HORIZONTAL UNCOMPACTED LAYERS AND BE COMPACTED BY MECHANICAL MEANS TO A MINIMUM OF 90% RELATIVE COMPACTION. IMPORTED SAND MAY BE USED FOR TRENCH BACKFILL MATERIAL PROVIDED IT IS COMPACTED TO AT LEAST 90% RELATIVE COMPACTION. WATER SETTING ASSOCIATED WITH COMPACTION USING VIBRATORY EQUIPMENT WILL BE PERMITTED ONLY WITH IMPORTED SAND BACKFILL WITH THE APPROVAL OF THE SOILS ENGINEER. ALL TRENCH SHALL BE BEDED WITH SAND EXTENDING FROM THE TRENCH BOTTOM TO TWELVE INCHES ABOVE THE PIPE. SAND BEDDING IS TO BE COMPACTED AS SPECIFIED ABOVE FOR SAND BACKFILL.

14. EROSION CONTROL

A. ALL GRADING, EROSION AND SEDIMENT CONTROL, AND RELATED WORK UNDERGROUND ON THIS SITE IS SUBJECT TO ALL RULES AND CONDITIONS OF THE COUNTY GRADING ORDINANCE AND MADE A PART HEREOF BY REFERENCE.

B. THE CONTRACTOR WILL BE LIABLE FOR ANY AND ALL DAMAGES TO ANY PUBLICLY OWNED AND MAINTAINED ROAD CAUSED BY THE APPLICABLE CONTRACTOR'S GRADING ACTIVITIES, AND SHALL BE RESPONSIBLE FOR THE CLEANUP OF ANY MATERIAL SPOILED ON ANY PUBLIC ROAD ON THE SHALL ROUTE.

C. THE EROSION CONTROL MEASURES ARE TO BE OPERABLE DURING THE RAINY SEASON, GENERALLY FROM OCTOBER FIRST TO APRIL FIFTEENTH. EROSION CONTROL PLANTING IS TO BE COMPLETED BY OCTOBER FIRST. REPAIRING OR UTILITY TRENCHES SHALL BE REPAIRING BETWEEN OCTOBER FIRST AND APRIL FIFTEENTH UNLESS AUTHORIZED BY THE LOCAL JURISDICTION.

D. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL DEFURBED AREAS ARE STABILIZED AND FINISHED. THE EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED TO MEET FIELD CONDITIONS ONLY WITH THE APPROVAL OF OR AT THE DISCRETION OF THE SOILS ENGINEER.

E. DURING THE RAINY SEASON, ALL PAVED AREAS SHALL BE KEPT CLEAR OF EXCESS MATERIAL AND ICEBARS. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT-LADEN RAINFALL TO ANY STORM DRAINAGE SYSTEM.

F. ALL EROSION CONTROL FACILITIES MUST BE INSPECTED AND REPAIRED AT THE END OF EACH WORKING DAY DURING THE RAINY SEASON.

G. WHEN NO LONGER NECESSARY AND PRIOR TO FINAL ACCEPTANCE OF DEVELOPMENT, SEDIMENT BARRIERS SHALL BE REMOVED OR OTHERWISE DEACTIVATED AS REQUIRED BY THE LOCAL JURISDICTION.

H. A CONSTRUCTION ENTRANCE SHALL BE PROVIDED AT ANY POINT OF EGRESS FROM THE SITE TO ROADWAY. A CONSTRUCTION ENTRANCE SHOULD BE COMPOSED OF COURSE DRAIN ROCK (2 TO 3) INCHES DIA.) AT LEAST EIGHT INCHES THICK BY FIFTY (50) FEET LONG BY TWENTY (20) FEET WIDE UNLESS SHOWN OTHERWISE ON PLAN AND SHALL BE MAINTAINED UNTIL THE SITE IS FAFED.

I. ALL AREAS SPECIFIED FOR HYDROSEEDING SHALL BE MIDDLE PLANTED WITH STABILIZATION MATERIAL CONSISTING OF SEED, FERTILIZER AND WATER, MOED AND APPLIED IN THE FOLLOWING PROPORTIONS:

- SEED: 200 LBS/ACRE
- SEED: 200 LBS/ACRE (SEE NOTE 4, BELOW)
- FERTILIZER (11-9-11): 500 LBS/ACRE
- WATER, AS REQUIRED FOR APPLICATION

4. SEED MIX SHALL PER CALTRANS STANDARDS.

K. WATER USED IN THE STABILIZATION MATERIAL SHALL BE OF SUCH QUALITY THAT IT WILL PROMOTE GERMINATION AND STIMULATE GROWTH OF PLANTS. IT SHALL BE FREE OF POLLUTANT PROMOTE AND WEED SEEDS.

L. HYDROSEEDING SHALL CONFORM TO THE PROVISIONS OF SECTION 202, EROSION CONTROL, AND HIGHWAY PLANTINGS, OF THE STANDARD SPECIFICATIONS OF THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION, AS LATEST REVISION.

M. A DISPENSING AGENT MAY BE ADDED TO THE HYDROSEEDING MATERIAL PROVIDED THAT THE CONTRACTOR FURNISHES SUITABLE EVIDENCE THAT THE AGENT WILL NOT ADVERSELY AFFECT THE PERFORMANCE OF THE SEEDING METHOD.

N. STABILIZATION MATERIALS SHALL BE APPLIED AS SOON AS PRACTICABLE AFTER COMPLETION OF GRADING OPERATIONS AND PRIOR TO THE ONSET OF WINTER RAINS OR AT SUCH OTHER TIME AS DIRECTED BY THE COUNTY ENGINEER. THE MATERIAL SHALL BE APPLIED BEFORE INSTALLATION OF OTHER LANDSCAPING MATERIALS SUCH AS TREES, SHRUBS AND GRASSING COVER.

O. THE STABILIZATION MATERIAL SHALL BE APPLIED WITHIN 4-HOURS AFTER WORKING MOVED MATERIAL NOT USED WITHIN 4-HOURS SHALL BE REMOVED FROM THE SITE.

P. THE CONTRACTOR SHALL MAINTAIN THE SOIL STABILIZATION MATERIAL AFTER PLACEMENT. THE COUNTY ENGINEER MAY REQUIRE SPRAY APPLICATION OF WATER OR OTHER MAINTENANCE ACTIVITIES TO ASSURE THE EFFECTIVENESS OF THE STABILIZATION PROCESS. APPLICATION OF WATER SHALL BE ACCOMPLISHED USING NOZZLES THAT PRODUCE A STRAT that DOES NOT CONCENTRATE OR WASH AWAY THE STABILIZATION MATERIALS.

15. CLEANUP

THE CONTRACTOR MUST MAINTAIN THE SITE CLEAN, SAFE AND IN USABLE CONDITION ANY SPILLS OF SOIL, ROCK OR CONSTRUCTION MATERIAL MUST BE REMOVED FROM THE SITE BY THE CONTRACTOR DURING CONSTRUCTION AND UPON COMPLETION OF THE PROJECT. COST FOR THIS ITEM OF WORK SHALL BE INCLUDED IN THE EXCAVATION AND COMPACTOR ITEM AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.

NOTE:

THESE NOTES ARE INTENDED TO BE USED AS A GENERAL GUIDELINE. THE REFERENCED SOILS REPORT FOR THE PROJECT AND GOVERNING AGENCY GRADING ORDINANCE SHALL SUPERSEDE THESE NOTES. THE SOILS ENGINEER MAY MAKE ON-SITE RECOMMENDATIONS DURING CONSTRUCTION OPERATIONS.

LEA & BRAZE ENGINEERING, INC.
REGISTERED PROFESSIONAL ENGINEERS
CALIFORNIA LICENSE NO. 45507
BAY AREA OFFICE: 4001 COLLETON DRIVE, SUITE 200
SACRAMENTO REGION: 2105 INDUSTRIAL PARK WEST, SUITE 100, SACRAMENTO, CA 95811
CENTRAL REGION: 1101 J STREET, SUITE 200, SACRAMENTO, CA 95811
SAN FRANCISCO REGION: 100 CALIFORNIA STREET, SUITE 300, SAN FRANCISCO, CA 94108
TEL: (916) 887-4200 FAX: (916) 887-4211
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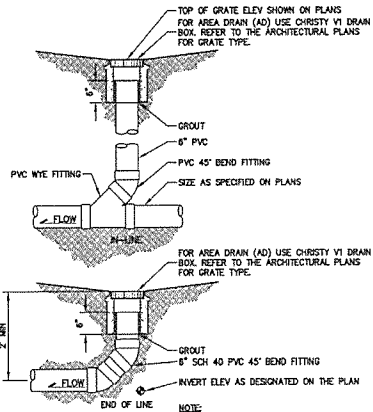
LAUREL OAKS SUBDIVISION
1273 & 1281 LAUREL STREET
MENLO PARK, CALIFORNIA

APR. 08/14/2010 07:00
BY: JMS/BSJ

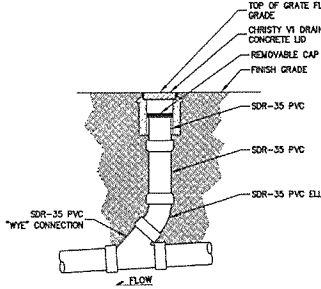
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REVISIONS	BY

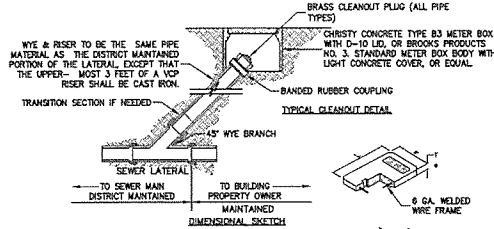
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DESIGN BY: PT
DRAWN BY: TB
SHEET NO: C-7
7 OF 10 SHEETS



1 AREA DRAIN
C-8 NTS



2 ON-SITE CLEANOUT
C-8 NTS



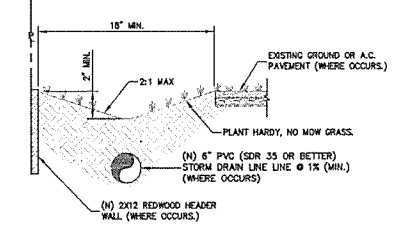
LID DIMENSIONS

	L	W	T	WT
CHRISTY D-10	14 1/2"	8 11/16"	11/16"	7 LB
BROOKS #3	14 1/2"	8 3/4"	1"	13 LB

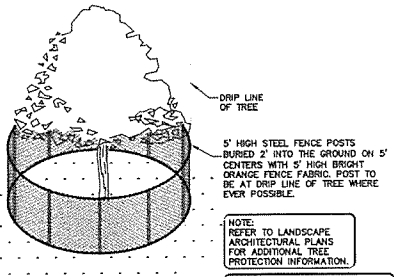
BOX DIMENSIONS

	A	B	C	D	E	F	G	H	I	APPROX WT. LBS.
CHRISTY B3	16"	14 13/16"	16 1/4"	13 1/4"	6"	9 7/8"	1"	12"	10 15/16"	55
BROOKS #3	19 1/8"	13 1/2"	16"	13 1/4"	7 5/8"	9 1/2"	1"	11"	N.A.	68

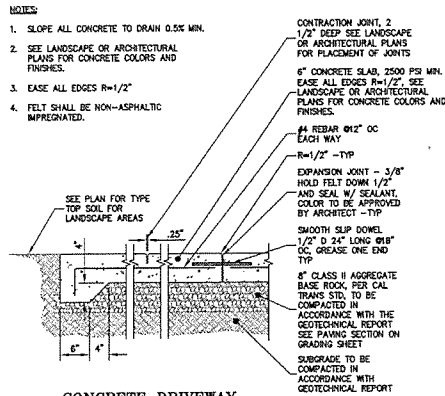
3 TYPICAL SEWER CLEANOUT BOX
C-8 NTS



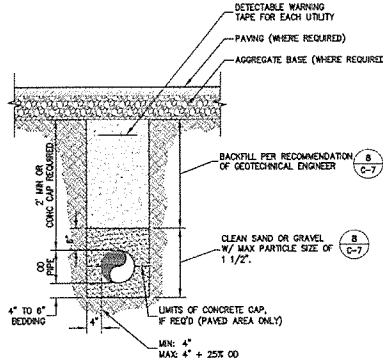
4 VEGETATED SWALE DETAIL
C-8 NTS



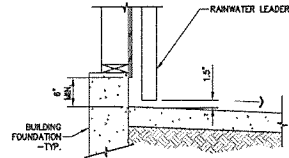
5 EXISTING TREE PROTECTION DETAIL
C-8 NTS



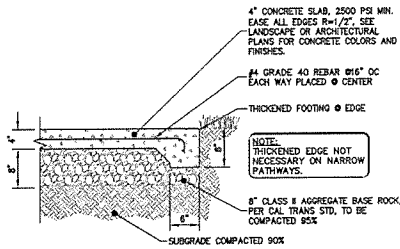
6 CONCRETE DRIVEWAY (ON PRIVATE PROPERTY ONLY)
C-8 NTS



7 PIPELINE BACKFILL
C-8 NTS (ON PRIVATE PROPERTY ONLY)



8 RAIN WATER LEADER TO CONCRETE SURFACE
C-8 NTS



9 CONCRETE PATIOS/WALKWAYS (ON PRIVATE PROPERTY ONLY)
C-8 NTS

LEA & BRAZE ENGINEERING, INC.
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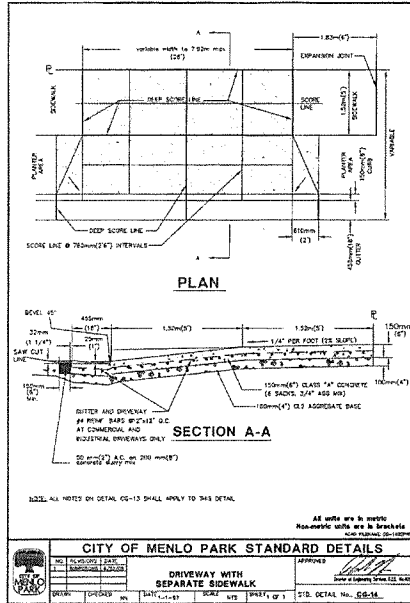
LAUREL OAKS SUBDIVISION
1273 & 1281 LAUREL STREET
MENLO PARK, CALIFORNIA
SAN MATEO COUNTY
APR. 06/14/07
APR. 05/14/15

DETAILS

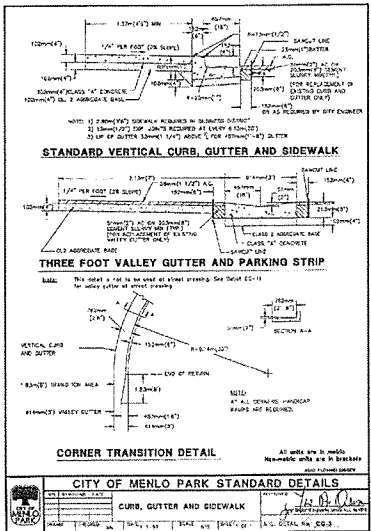
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JOB NO: 2720337
DATE: 3-15-13
SCALE: NTS
DESIGN BY: PT
DRAWN BY: TB
SHEET NO:

C-8
8 OF 10 SHEETS



1 CITY DETAIL CG-14
NTS



2 CITY DETAIL CG-3
NTS

NOTES:

- Concrete shall be Class A (6 weeks) containing not less than 230kg(504) total Portland Cement per 0.60m³ (4 cu yds) (1 maximum aggregate and a 25mm maximum diameter of 24kg(53) 2000) Aggregate. (AS) shall be Class 2
- Concrete for curb, gutter, sidewalk and driveway shall contain 0.4kg(lb)(ft³) of admixture per 0.60m³ (4 cu yds) (4 cubic yard). (1) Check that it is used in base, where applicable.
- All shall be constructed to not less than 85% relative compaction, as tested by City approved job at contractor's expense.
- Reinforcement bars (R) shall be #4 (13mm) steel for sidewalks, 20mm(2") steel for 150mm(6") curb driveway, and 20mm(2") steel for 150mm(6") curb driveway. Reinforcement shall be spaced at 300mm(12") nominal spacing, and all reinforcement shall be placed in top portion of concrete. Reinforcement shall be placed in top portion of concrete. Reinforcement shall be placed in top portion of concrete. Reinforcement shall be placed in top portion of concrete.
- Exposed surfaces of sidewalk, driveway, curb and gutter shall be steel trowel finished by a machine trowel finish. Existing defective finish shall be replaced in kind.
- Edges on street, at exposed edges, including of weathered-plasma joints, shall be trowel to 15mm(1/2") finish.
- Sidewalk shall be score marked at 750mm(2'-7") nominal intervals each way or to match existing score marks. Weathered curb, sidewalk and driveway shall have a partitional score mark 150mm(6") from curb face.
- All concrete shall be cured by keeping continuously moist for three days after casting when by including covering with a waterproof membrane, or applying Type I concrete curing compound.
- Estimated curb and gutter shall be approved by the City Engineer prior to construction.
- For existing AC surfaced gutter, flow line of new concrete gutter shall match existing concrete gutter flow line. Curb top at driveway shall be finished to 15mm(1/2") higher than AC driveway.
- All gutter flow shall be formed and finished at 60mm(2") above full depth AC street surface section shall be placed adjacent to the gutter. The AC section shall be a minimum of 100mm(4") deep on minor streets and 6" deep on major streets or 25mm(1") thicker than existing AC, which ever is greater.
- All new PCC improvements shall be down to existing and adjacent PCC improvements using 20mm(1/2") long #2 rebar spaced at 180mm(7") O.C. for all areas left to be opened in place.
- Driveway shall be installed per note (12) when gutter strips are filled with aggregate or when sidewalk is added to the back of the existing curb. Street low side shall be provided in accordance with City requirements.
- All PCC improvements to be removed shall be saw cut along existing curbs, then section to be replaced shall be saw cut in either length or width, if the saw cut in sidewalk or driveway would fall within 750mm(2'-6") of a construction joint, expansion joint, road joint, or edge, the concrete shall be removed to the full depth or edge. All saw cut residue shall be removed immediately with sweeping operation.
- Have imported full depth. All shall be placed under of new or replacement curb, gutter, sidewalk and driveway.
- The typical City sidewalk curb height is 150mm(6") and the typical City gutter width is 150mm(6") deep, unless curb height (150mm(6") to 225mm(9") to 1") and gutter width (150mm(6") to 300mm(12") to 2") method is used when specified by the City Engineer.

CITY OF MENLO PARK STANDARD DETAILS

GENERAL CONCRETE NOTES

REVISIONS

REVISIONS BY

JOB NO.: 2120337

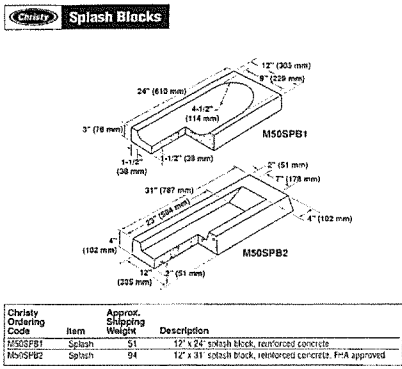
DATE: 3-15-13

SCALE: NTS

DESIGN BY: PT

DRAWN BY: TB

SHEET NO.



Christy Ordering Code	Item	Approx. Shipping Weight	Description
M50SPB1	Splash	51	12" x 24" splash block, reinforced concrete
M50SPB2	Splash	94	12" x 31" splash block, reinforced concrete. FFA approved

4 SPLASH BLOCK DETAIL
NTS

LEA & BRAZE ENGINEERING, INC.

LAND ENGINEERS

SACRAMENTO REGION

2405 SULLY AVENUE WEST

SACRAMENTO, CALIFORNIA 95834

(916) 486-4086

(916) 486-1338

(916) 486-1333

WWW.LEA-BRAZE.COM

LAUREL OAKS SUBDIVISION

1273 & 1281 LAUREL STREET

MENLO PARK, CALIFORNIA

APR. 06.13.01.020

SAN MATEO COUNTY

DETAILS

REVISIONS	BY

C-9

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PLANNING COMMISSION STAFF REPORT

FOR THE PLANNING COMMISSION
MEETING OF APRIL 8, 2013
AGENDA ITEMS D4 & D5

LOCATION:	1273 and 1281 Laurel Street	APPLICANT:	Forrest Mozart
EXISTING USE:	Two Single Family Dwelling Units	OWNER:	Laurel Oaks LP
PROPOSED USE:	Six Detached Residential Dwelling Units on Two Parcels	APPLICATION:	Use Permit, Architectural Control, And Major Subdivision
ZONING:	R-3 (Apartment)		

PROPOSAL

The applicant is requesting use permit and architectural control review to demolish two single-story, single family homes located on two adjacent lots and to construct six detached, two-story dwelling units on the subject parcels in the R-3 (Apartment) zoning district. The development would include a common driveway for access to each parcel. The applicant is also requesting approval of a tentative map to create six residential condominium units, which would allow each unit to be sold separately and approval of a Below Market Rate (BMR) Agreement for the payment of an in-lieu fee for this project. The application also includes requests for five heritage tree removals, as well as one non-heritage street tree removal. Major subdivisions (five or more units) require City Council action. Since the major subdivision requires action by the City Council, the Planning Commission will act as a recommending body to the City Council on the use permit, architectural control, heritage tree removal permits, BMR in-lieu fee agreement, and tentative map requests. The City Council will be the final decision making body on all land use entitlements for the project.

The proposed development consists of two adjacent legal parcels, both part of the R-3 (Apartment) zoning district. Each parcel would be developed with three detached units. The staff report discusses the project as a whole and the Planning Commission should review the project in its entirety, but will need to make separate actions for each parcel. Each parcel will be referred to collectively as “the site,” unless otherwise specified.

BACKGROUND

The Planning Commission initially reviewed the proposed project at its regular meeting of March 4, 2013. At the meeting, the Planning Commission heard testimony from the applicant and members of the public, and discussed the proposed project. At the meeting, the Planning Commission voted 5-2 (with Commissioners Ferrick and Onken in opposition) to continue the item with the following direction:

- Redesign Building One, Plan One to improve its orientation to the street and also deal with fenestration issues of the façade
- Improve the massing of the roof structure for Plan One
- Reconsider the use of applied materials, specifically the stone veneer, for the entire project
- Work with staff to reduce the driveway width and also utilize driveway materials to improve the design of the driveway

Subsequently, the applicant has modified the plans, incorporating the input from the Planning Commission, which is discussed throughout the report. The applicant's project description letter, which includes a response to the Planning Commission's direction, is contained in Attachment D, and explains the modifications to the project in more detail.

ANALYSIS

Site Location

The subject site is located on Laurel Street between Oak Grove Avenue and Glenwood Avenue, across from Nativity School. In addition to the elementary school, parcels across the street are located within the Town of Atherton, and occupied by single-family residences. Parcels to the north and south of the subject site, using Laurel Street in a north to south orientation, are also zoned R-3 and are occupied by a mixture of multi-family and single-family developments. Parcels to the west of the subject site are likewise located in the R-3 zoning district and contain a mixture of multi-family and single-family developments. The dominant land use pattern in the vicinity of the project is multi-family apartment development.

Project Description

The proposed design concept creates six single-family detached dwelling units. Each parcel would contain three detached dwelling units in a row. All dwelling units would meet the R-3 minimum 20-foot separation for detached units on the site, as well as the minimum separation between main dwelling units on neighboring parcels. Both side setbacks would exceed the minimum 10 foot requirement. On the right side parcel (1281 Laurel Street), the units would be separated by the required uncovered parking space and landscaping. The project would be below the respective maximums for floor area ratio (FAR) (45 percent), and building coverage (30 percent), both for the

individual sites and the overall development. The proposed building coverage for the right side parcel was reduced by approximately 11 square feet as part of the modified project.

The project would also meet R-3 minimum landscaping requirements (50 percent) and maximum driveway and open parking areas (20 percent). Consistent with the Planning Commission's direction, the applicant has modified the driveway to include permeable pavers for a portion of the driveway and the uncovered parking spaces. Permeable pavers may be calculated as 50 percent landscaping and 50 percent parking and open driveways; however, the project would exceed the minimum landscaping requirement and be below the maximum parking and driveway square footage limit without accounting for the permeable pavers. The maximum height for the units would be 23 feet above grade for the left side units and 24 feet, ten inches above grade for the right side units, well below the maximum permitted height of 35 feet. The data summary table for the proposed project is included in Attachment C.

The three units on the left-side parcel would contain four bedrooms and two and a half bathrooms. The floor plan is repeated throughout all three units, with the exception of an enlarged covered porch for the front unit, which would help orient the proposed development to the street. The covered porch would be directly connected to the street through a distinct walkway. The three units on the right-side parcel would contain four bedrooms and three bathrooms. Consistent with the development pattern on the left-side parcel, the proposed three units to the right of the driveway would have the same general floor plan, with the exception of a larger covered front porch and window layout for the front unit. The window layout and porch modifications were adjusted in response to the input of the Planning Commission at its March 4 meeting. The changes are discussed in more detail in the Design and Materials Section of the staff report. The front unit would also have direct walkway access to Laurel Street. The entry doors for each unit would be accessed from the parking court, with the exception of the front units, which would be accessed directly from Laurel Street.

Parking and Circulation

The site is designed with six units, located in two rows on each side of a common 20-foot wide driveway. The units to the left of the driveway would each consist of 1,918.8 square feet of gross floor area and would have attached two-car garages, which are not included in the calculation of gross floor area. The units along the left-side of the driveway would contain an additional guest parking space. The units to the right of the subject site would be 1,951.2 square feet of gross floor area and would have attached one-car garages, with an uncovered parking space located adjacent to each unit. The one-car garages would be exempt from gross floor area, including an area designated for bicycle parking.

The proposed site layout allows the parking requirement to be met, while locating all required spaces behind the front units, including the covered and uncovered spaces, which staff believes enhances the streetscape. The proposed site layout results in no

garage doors directly facing the street. The common driveway minimizes the impact of the parking and circulation on the street, since only one curb cut is necessary for the development. The Planning Commission directed the applicant to work with staff to reduce the driveway width and to utilize driveway materials to improve the design of the driveway. The applicant contacted the Menlo Park Fire Protection District to initially evaluate the possibility of reducing the width of the driveway, which would be subject to subsequent review by the Transportation Division for compliance with City requirements. The applicant was informed by the Fire District that the minimum width of a driveway serving three or more units is 20 feet. The applicant's response letter discusses the Fire District requirements and the proposed permeable pavers in more detail (Attachment D). In order to reduce possible visual impacts of the driveway, the applicant is proposing to use interlocking permeable pavers for select portions of the driveway. The front portion of the driveway, between the street and the front units, would utilize permeable pavers, and the remainder would alternate between permeable pavers and colored concrete, for variety. All uncovered parking spaces would utilize permeable pavers.

Driveways and open parking areas are limited to 20 percent of the lot area. As mentioned previously, permeable pavers can be calculated as 50 percent landscaping and 50 percent driveways and parking areas; however, the proposed project would be below the 20 percent maximum without accounting for the permeable pavers. The applicant designed the driveways and open parking areas on 1273 Laurel Street to cover 19.5 percent of the lot and on 1281 Laurel Street to cover 19 percent of the lot. The parking and driveway layout has been reviewed and approved by the City's Transportation Division and the Menlo Park Fire Protection District.

Design and Materials

The applicant states that the proposed six-unit development is designed in a contemporary Craftsman style, which utilizes elements such as the proposed massing, low pitched roofs with wide overhangs, vertically proportioned windows, and a combination of stucco and siding. To accent the Craftsman elements, the project includes trim and details in a more contemporary style. The applicant's project description letter is included in Attachment D.

The units would contain a mixture of stucco and horizontal siding or shingles on the upper portions of the units. Stucco would be the primary material, with painted horizontal siding or stained shingles, determined by the unit, used to provide a secondary exterior finish to complement the stucco. The proposed development would utilize earth tones, specifically muted browns and greens. Each unit would contain wood clad windows with true simulated divided lites, with outside and inside grids, and spacer bar. The proposed grids would be designed in the Craftsman style.

In addition to stucco and siding/shingles, the initial project included a limited amount of stone veneer along the lower portion of the units, specifically around the garage doors. Since the original Planning Commission meeting, the applicant has modified the stone

veneer as part of the Planning Commission direction to reconsider the use of applied materials, specifically the stone veneer, for the entire project. The applicant has included additional stonework along the front and side elevations for the front units. The applicant states that stone and brick masonry are commonly used accent materials on Craftsman style houses, and are traditionally used for porch columns, porch post bases, site walls, and foundations. The applicant states that the additional stone veneer along the front elevation of the front right-side unit is intended to tie the main body of the building visually into the porch base posts and garage corner columns, which contained stone previously. A stone veneer was added to the base of the left side facades of the two additional buildings on the right side parcel. Staff believes that the additional stone veneer results in a more comprehensive use of the stone elements, and is in keeping with traditional Craftsman elements.

The proposed structures would contain pitched roofs, with composition shingles. The roofs would contain multiple ridges and gables to add articulation. Additionally, per the Planning Commission's direction, the applicant has modified the roof massing of the units on the right side parcel, changing the hipped portion of the upper roof to an additional gable that would face the driveway. In addition, the applicant modified the ridge to follow the one-foot jog along the rear façade. Staff believes that the modified roof massing creates a more integrated roof structure and addresses the Planning Commission's direction.

The entryways for the front units are oriented toward Laurel Street, which would help create a connection between the units and the street. The Planning Commission provided direction to the applicant regarding the right side, front unit (1281 Laurel Street) as part of its continuance action. The Planning Commission directed the applicant to improve the orientation of the unit to the street and also address the fenestration issues of the façade. The applicant redesigned the porch to contain a gable facing the street, instead of a shed roof, which intersects at the corner of the porch with the existing gable on the left side of the porch. The applicant states that the modified porch roof design creates a dynamic wrap-around effect, and is a traditional Craftsman style treatment for a corner porch. The windows along the front façade have been redesigned to create a larger window in the living room, featuring a decorative canopy, and a smaller window above the kitchen sink, which helps to orient the house to the street. The bedroom windows on the second floor were enlarged and reconfigured to improve the connection between the façade and the street. The applicant also revised the front façade of the left side unit to contain a street facing gable, where a hipped roof was previously proposed. Staff believes that the proposed porch roof modifications and overall roof massing changes, as well as the window modifications comprehensively address the Planning Commission's direction with regard to the right side front unit.

The proposed site layout differs slightly from the existing pattern in the immediate neighborhood, which includes a mixture of multi-family apartment buildings and single family homes, with multifamily apartment buildings being the dominant land use in the area. However, the proposed layout, with detached units in a row, is consistent with the

style of recent approved development on R-3 parcels in other locations within Menlo Park. The neighborhood contains a mixture of architectural styles, with older homes designed in the Craftsman style and the larger apartment complexes designed in styles typical of the mid-century, but the street does not contain a single defined architectural style. Staff feels that the proposed architectural style is compatible with the existing neighborhood character, which is mixed. A color and materials board will be available at the Planning Commission meeting.

Major Subdivision

The proposed project includes the subdivision of six residential units for condominium purposes on two legal parcels. While each legal lot would contain three units, both lots are necessary to provide the necessary access and back-up distance for the units, and therefore the six units are evaluated through one tentative map. Major subdivision approval is required for the creation of five or more parcels or for the creation of five or more condominium units. The applicant has submitted a tentative map as part of the application for a major subdivision. The map has been reviewed by the City's Engineering Division and has been found to comply with the provisions of the State Subdivision Map Act and the City's Subdivision Ordinance subject to conditions of approval.

Trees and Landscaping

The project site contains 18 trees (including five trees within the public right-of-way and two on a neighboring parcel), nine of which are of heritage size. The applicant has designed the proposed project around the heritage size live oak tree at the front of the parcel (Tree #1). In addition, Tree #4 (28.1-inch coast live oak) and Tree 7 (39.7-inch coast live oak) at the rear-right corner of the property are proposed to remain. The applicant is proposing to remove five heritage trees located at the site and three non-heritage trees, including one street tree.

The applicant is proposing to remove five heritage size trees:

Tree Number	Tree Type	Diameter	Location on Site	Condition	Basis for Removal Request
#2	Incense cedar	27 inches	Front middle	Poor	Health/Structure
#5	Mexican fan palm	22 inches	Back right-corner	Good	Construction
#6	Mexican fan palm	17 inches	Back right-corner	Good	Construction
#9	Coast live oak	42 inches	Middle-rear	Poor	Health/Possible hazardous
#12	Incense cedar	33 inches	Front-left side	Fair	Health/Structure

In order to construct the proposed driveway, the applicant is proposing to remove an approximately 7-inch diameter London plane tree, located within the public right-of-way. The City Arborist has reviewed the removal permits and given tentative approval to remove the trees, including the street tree. The applicant has submitted an initial arborist report and subsequent updates (Attachment E), detailing the species, size, and conditions of the trees on or near this site that could be impacted by the proposed development. The report determines the present condition, discusses the impacts of the proposed improvements, and provides general recommendations for tree preservation. All recommendations identified in the report shall be implemented and have been included in the conditions of approval for the project.

The applicant has submitted a landscape plan that shows the type, location, and size of existing and replacement trees on-site. The landscape plan identifies the location and type of the proposed plantings for the site. The applicant is proposing to replace the five heritage trees with two Brisbane box trees and three true green elm trees. Both trees would be 24-inch box size plantings and would be planted along the side property lines, with two trees located along the left side property line for the site, and three located along the right side property line of the subject site. The front yards of each front unit would contain shrubs and ground cover that would be used to help define the pathways between the entry doors and the sidewalk. The front landscaping would help reinforce the orientation of the front units to the street, as well as help soften the visual effect of the center driveway. The landscape plan is included in Attachment B. The draft resolution for the removal of the heritage trees is included in Attachment F.

Below Market Rate (BMR) Housing Agreement

The City’s BMR Program applies to all new residential developments with five or more new units; therefore the proposed condominium subdivision of six residential units would be subject to the BMR Program. For residential projects of less than 20 units, the developer is required to provide at least 10 percent of the units at below market rates to

very low-, low- and moderate-income households. The BMR program also contains a provision for the payment of in-lieu fees, if the developer substantiates to the City's satisfaction that the BMR units cannot be provided on- or off-site. In accordance with this requirement, the project would require 0.6 units. Section 3.4.1 of the BMR Guidelines states a preference for fractional units to be rounded up to a whole unit.

Section 4 of the BMR Guidelines provides options for meeting the BMR program requirements, including the development of on-site units, development of off-site units, and payment of an in-lieu fee. The BMR Guidelines also state a preference for on-site units, noting that if on-site units are not feasible, an in-lieu fee will be required, which is subject to review and recommendation by the Housing Commission to the final acting body, which in this case is the City Council. For this proposal, if one BMR unit was provided, then the development would be eligible for one bonus market-rate unit, for a total of seven units.

The applicant evaluated the feasibility of incorporating a seventh unit on-site, utilizing the density bonus and corresponding increase in floor area ratio (FAR). The applicant determined that the development of seven detached units on the two lots is constrained by the size of the individual lots, the Zoning Ordinance development standards, and the existing heritage trees on-site. The developer states that the decision to pursue an in-lieu fee instead of an on-site unit was driven by project feasibility, site constraints, and the site and building design of the proposed development with the additional BMR unit. The applicant states in their BMR project description letter (Attachment G) that detached units are the only feasible housing type for this development, given the current market conditions. The applicant states that the anticipated sale price for each unit is \$1,200,000, which is anticipated to drop 25 percent, or \$300,000 per unit, if the development was redesigned to include attached units. The developer evaluated multiple alternatives, including attached and detached options. The existing heritage trees and Zoning Ordinance development standards make it difficult to incorporate an additional unit into the development without requesting numerous development waivers from the current development standards, which staff believes would negatively impact the site and building design. (In evaluating the alternatives, the developer attempted to design to avoid requesting additional heritage tree removal permits.) The alternative development scenarios would result in reduced building setbacks, including side and rear yard setbacks, which could have greater impacts on the neighbors. The design alternatives would increase building coverage, resulting in a reduction in landscaping and open space on-site.

The BMR Guidelines require that, if an on-site unit is not feasible, the Developer shall pay one percent (1%) of the sales price for units 1, 2, and 3, and two percent (2%) of the sales price for units 4, 5, and 6. Since the applicant anticipates selling the units for a comparable price, the in-lieu fee would be approximately 1.5 percent (1.5%) of the sale price per unit. Therefore, since the applicant anticipates selling the units for \$1,200,000, the estimated BMR in-lieu fee would be \$108,000 or 1.5 percent (1.5%) of \$7,200,000. In addition to the minimum in-lieu fee, the Developer is voluntarily proposing to increase the fee by one percent (1%) to an average of 2.5 percent (2.5%) per unit. Therefore, the

applicant would pay a flat rate of 2.5 percent (2.5%) for each unit, resulting in a projected total BMR in-lieu fee of \$180,000. A copy of the draft BMR Agreement is included as Attachment H. The Housing Commission reviewed the proposed agreement on January 16, 2013, and recommended approval of the proposed BMR Agreement, 4-0, with Commissioner Dodick absent.

Correspondence

The Planning Division received a number of items of correspondence on the initial application which were included in the staff report for the March 4 Planning Commission meeting, or provided to the Commission by staff at the meeting. Since the March 4 meeting, staff has received one additional item of correspondence from Aaron Thurlow, of 1264 Mills Street. In his email, he states that he is opposed to the removal of the heritage trees, specifically the incense cedars and the coast live oak trees. He states that the older native heritage trees add to the charm of Menlo Park, and that the developer should design the project to protect the natural beauty of the community and that it is the responsibility of the developer to understand the city rules and guidelines before investing in a property for development. His email is included in Attachment I.

With regard to the heritage trees, the City Arborist has reviewed and approved the coast live oak tree (located in the rear portion of the lot), and the two incense cedars due to the existing condition of the trees. The City Arborist determined that the coast live oak (Tree #9) is possibly hazardous and that the incense cedars (Trees #2 and #12) contain structural problems. The applicant has designed the project to retain the large, healthy oak tree (Tree #1) located along the front, center-right side of the development. Additionally, the applicant has designed the project to retain two heritage coast live oaks (Trees #4 and #7) at the rear-right side of the property. The increased front setback to accommodate the existing oak tree results in greater impacts to the palm trees located at the rear of the lot, which are proposed to be removed due to the construction of the project. In addition, the applicant would plant five new heritage replacement trees.

Conclusion

Staff believes that the revised project addresses the Planning Commission's direction. The proposed modifications result in the simplification of the massing of the roof structure for the units on the right side parcel, and the façade modifications on the front unit address the Planning Commission's concerns related to the fenestration of the façade and the unit's orientation to the street. In addition, the applicant has worked to reduce the possible visual impact of the driveway by incorporating permeable pavers in the design, and explored the possibility of reducing the width of the driveway with the Fire District. The proposed stone veneer has been modified to create a more comprehensive design feature for the project. Staff believes that the proposed project provides housing that is complementary to the neighborhood with respect to the number of units and site design. The style and materials are compatible with the immediate area. The proposed project contains materials elements that help break up the massing

of the building. The proposed colors and wood variety are in keeping with the chosen architectural style and would add to the articulation of the buildings. The proposed project creates a desirable configuration of three dwelling units on each lot (six at the site) and provides a conforming number of parking spaces. The proposed project would preserve the large oak at the front of the property, and the orientation of the front units allows for the main entries to be connected to the street. Additionally, the applicant voluntarily increased the rate for the BMR in-lieu fee. Staff recommends that the Planning Commission recommend that the City Council approve the proposed project.

ENVIRONMENTAL REVIEW

The proposed project is consistent with the General Plan land use designation for the site (Medium Density Residential) and would comply with the R-3 (Apartment) district land use regulations. The site is less than five acres in size and was previously developed, therefore would not result in additional impacts to endangered species. The proposed development would not result in noise impacts greater than typical residential development, and the project would meet all civil and hydrology requirements of the City Engineering Division. The Transportation Division reviewed the project and determined that the increase in four units would not result in a significant impact to the roadway network or the level of service (LOS) at the nearest intersection. As such, the proposed project is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current California Environmental Quality Act (CEQA) Guidelines. The trip generation analysis is available for public review at the Department of Community Development.

RECOMMENDATION

Staff recommends that the Planning Commission recommend approval to the City Council of the following actions:

1273 Laurel Street

1. Adopt a finding that the redevelopment of the site is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current State CEQA Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:

- a. The general appearance of the structure is in keeping with the character of the neighborhood.
 - b. The development will not be detrimental to the harmonious and orderly growth of the City.
 - c. The development will not impair the desirability of investment or occupation in the neighborhood.
 - d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.
 - e. The property is not within any Specific Plan area, and as such no finding regarding consistency is required to be made.
4. Approve the use permit and architectural control requests subject to the following **standard** conditions:
- a. Development of the project shall be substantially in conformance with the plans prepared by AJS Architecture and Planning, consisting of 29 plan sheets, dated received March 27, 2013 and recommended by the Planning Commission on April 8, 2013, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
 - b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.
 - c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
 - d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. Landscaping shall properly screen all utility equipment that is installed outside of a building and that cannot be placed underground. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
 - e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage

improvements. The plans shall be submitted for the review and approval of the Engineering Division.

- f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to issuance of a grading, demolition or building permit.
 - g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.
 - h. Prior to building permit issuance, the applicant shall submit proposed landscape and irrigation documentation as required by Chapter 12.44 (Water-Efficient Landscaping) of the City of Menlo Park Municipal Code. If required, the applicant shall submit all parts of the landscape project application as listed in section 12.44.040 of the City of Menlo Park Municipal Code. This plan shall be subject to review and approval by the Planning and Engineering Divisions. The landscaping shall be installed and inspected prior to final inspection of the building.
5. Adopt findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.

1281 Laurel Street

1. Adopt a finding that the redevelopment of the site is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current State CEQA Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:
 - a. The general appearance of the structure is in keeping with the character of the neighborhood.
 - b. The development will not be detrimental to the harmonious and orderly growth of the City.

- c. The development will not impair the desirability of investment or occupation in the neighborhood.
 - d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.
 - e. The property is not within any Specific Plan area, and as such no finding regarding consistency is required to be made.
4. Approve the use permit and architectural control requests subject to the following **standard** conditions:
- a. Development of the project shall be substantially in conformance with the plans prepared by AJS Architecture and Planning, consisting of 29 plan sheets, dated received March 27, 2013 and recommended by the Planning Commission on April 8, 2013, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
 - a. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
 - b. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. Landscaping shall properly screen all utility equipment that is installed outside of a building and that cannot be placed underground. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
 - c. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for the review and approval of the Engineering Division.
 - d. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to issuance of a grading, demolition or building permit.

- e. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.
 - f. Prior to building permit issuance, the applicant shall submit proposed landscape and irrigation documentation as required by Chapter 12.44 (Water-Efficient Landscaping) of the City of Menlo Park Municipal Code. If required, the applicant shall submit all parts of the landscape project application as listed in section 12.44.040 of the City of Menlo Park Municipal Code. This plan shall be subject to review and approval by the Planning and Engineering Divisions. The landscaping shall be installed and inspected prior to final inspection of the building.
 - g. Prior to building permit issuance, the applicant shall submit proposed landscape and irrigation documentation as required by Chapter 12.44 (Water-Efficient Landscaping) of the City of Menlo Park Municipal Code. If required, the applicant shall submit all parts of the landscape project application as listed in section 12.44.040 of the City of Menlo Park Municipal Code. This plan shall be subject to review and approval by the Planning and Engineering Divisions. The landscaping shall be installed and inspected prior to final inspection of the building.
5. Adopt findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.

Kyle Perata
Assistant Planner
Report Author

Thomas Rogers
Senior Planner

PUBLIC NOTICE & APPEAL PERIOD

Public notification consisted of publishing a legal notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property. Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

ATTACHMENTS

- A. Location Map
- B. Project Plans
- C. Project Summary Data Table

- D. Project Description Letter
- E. Arborist Report
- F. Draft Resolution for Heritage Tree Removals
- G. BMR Proposal Letter
- H. Draft BMR In-Lieu Fee Proposal
- I. Correspondence
 - Aaron Thurlow, 1264 Mills Street

Note: Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public viewing at the Community Development Department.

EXHIBITS TO BE PROVIDED AT MEETING

Color and Materials Board

AVAILABLE FOR REVIEW AT CITY OFFICES AND ON THE CITY WEB SITE

- Planning Commission Staff Report for the Meeting of March 4, 2013
- Planning Commission Draft Minutes for the Meeting of March 4, 2013

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PLANNING COMMISSION DRAFT EXCERPT MINUTES

Regular Meeting
 April 8, 2013 at 7:00 p.m.
 City Council Chambers
 701 Laurel Street, Menlo Park, CA 94025

CALL TO ORDER – 7:03 p.m.

ROLL CALL – Bressler, Eiref (arrived 7:04 p.m.), Ferrick (Chair), Kadvany (Vice Chair), O'Malley (absent), Onken, Riggs

INTRODUCTION OF STAFF – Rachel Grossman, Associate Planner; Momoko Ishijima, Planner; Jean Lin, Associate Planner; Kyle Perata, Assistant Planner; Thomas Rogers, Senior Planner

D. PUBLIC HEARING

D4. Use Permit, Architectural Control, Major Subdivision/Forrest Mozart/1273

Laurel Street: Request to demolish a single-story, single family home and to construct three detached, two-story dwelling units on the subject parcel in the R-3 (Apartment) zoning district. Use Permit and Architectural Control would be required for the construction of new residential units. The development would include a common driveway with the adjacent property (1281 Laurel Street) for access to each residence. A Tentative Map would be required to create six residential condominium units, including three units on the neighboring legal parcel (addressed 1281 Laurel Street). As part of the proposal the applicant is proposing to remove three heritage trees, including a coast live oak (42-inch diameter, poor condition) and two incense cedars (33-inch and 27 inch diameter, fair and poor condition). In addition, the applicant is requesting approval of a Below Market Rate (BMR) Agreement for the payment of an in-lieu fee for this project. An initial version of the proposal was reviewed by the Planning Commission at the meeting of March 4, 2013, and was continued with direction for redesign. The proposal has since been revised, with changes to the driveway material, the windows and orientation of the right-side front unit, and modifications to the applied materials throughout the project.

Staff Comment: Planner Perata said staff had no additional comments.

Public Comment: Mr. Tony Sarboraria, Project Architect, distributed three packets to the Commission. He said the project was continued by the Commission on March 4 with specific direction particularly about the house on the right side in how it addressed the street, its roof massing, the stone treatment and how it related to the home behind it. He said the other issue was the driveway width and design. He said they contacted the

Fire District again and they confirmed with that agency there was a requirement stemming from State Fire Code that roadways or driveways which served four or more residential units were required to be a minimum of 20 feet wide. He noted changes to make the driveway area more attractive and referred the Commission to the handout for the type of pavers proposed for use. He said the sections where the pavers would be located could be seen on the site plan noting that they would be used on the walkways and surface parking stalls. He said they also addressed the roof plan of all of the Plan One buildings. He said the roof on the front unit particularly changed and they had added a gable so there were two gables facing the two sides of the corner. He said significant changes were made to the fenestration facing the street with the addition of a picture window, two smaller windows and a decorative canopy. He said they enlarged the window on the second floor and added a window in the kitchen. He said they added substantially more stone to give the home a strong base. He said the ledge stone would be applied with grout. He said the applicant and he had met with Commissioner Riggs to get some feedback on the changes made thus far. He said two of the four views in the renderings were done specifically to address what they would look like including the view along the back of the houses on the right hand side and the space between Houses #1 and #2 and Houses #2 and #3. He said the landscape sheet on the back of the packet showed the addition of a tree between House #1 and #2 and another tree between House #2 and #3. He said they were open to the type of trees those should be. He said the renderings were done very recently and he was pleased overall but it appeared the windows had white frames. He said they intended to have dark colored clad window sashes that coordinated with the paint scheme for each of the houses. He said the stone texture on the rendering was not quite right and noted the materials board better showed the stone texture.

Chair Ferrick said she liked the addition of the window in the kitchen and the picture window on House #1. She asked if Houses #2 and #3 would also enjoy such windows. Mr. Sarboraria said there was the question of privacy between the houses. He said those houses had the majority of their windows toward the left and fewer and smaller windows on the right. He noted the fence lines were designed to wrap around for a yard on the left side with a 20 foot space between them.

Commissioner Onken said the last time this item was considered Commissioner Riggs expressed concern that the planting area under the heritage oak tree be kept dry and free of irrigation as much as possible. He asked if they would use rocks and pebbles rather than bark mulch. Ms. Shari Van Dom, Landscape Architect, said typically bark mulch was healthier for the tree than doing pebbles and rocks as that reduced the air flow.

Commissioner Kadvany asked about the proportion of stucco to shingling on some of the houses, noting House #6 had a large stucco on front and a small amount of shingling on the side. Mr. Sarboraria said it was intentional.

Chair Ferrick closed the public hearing.

Commission Comment: Chair Ferrick said she appreciated the updated plans and she liked the way House #1 addressed the street. She said some of the houses looked like a layered cake, but noted that it might be because of the colors shown in the rendering. Mr. Sarboraria said there were six houses and three color schemes. He said three of the houses on the second story would have stained gray shingles and the other three would have horizontal siding. He said the siding would be painted and the shingles would be stained. He said in each of the three color schemes there were two upper story shingled siding colors. He said all of the roofs would be the same and all of the bases would be the same with a variation in colors of the walls in between.

Commissioner Onken said comments made previously about the driveway had been addressed. He said that it was not that the driveway was terribly wide but that it seemed more so because it was next to a narrow cul de sac street. He said he found the driveway width acceptable and appreciated the changes made. He said the applied stone was compatible with this new-style Craftsman look. He said the stone should however be extended to the ground. Mr. Sarboraria said there were code issues. Commissioner Onken asked whether there could be a damp layer if the stone was grouted solid. Mr. Sarboraria said there were some tricky ways of leaving weep holes with little pieces of flashing sticking out and filling in more stone underneath. He said he was making a note to add fancy detail at the base of the stone.

Commissioner Bressler asked about the pavers and the Fire District truck weights. Mr. Sarboraria said the Fire District would require that the pavers be engineered to support the vehicular weight. Commissioner Bressler asked the difference in cost between doing pavers and concrete. Mr. Sarboraria said it was significant but he did not have a dollar amount.

Commissioner Riggs said he appreciated the applicant team's responsiveness to the Commission's comments. He thanked the applicants for doing the renderings, and for planting trees against the blank walls. He said the City leaves a majority of the drip line in non-irrigated form for its oak trees. Ms. Van Dom said there were a number of plants that worked under oak trees and those would be used with very low water drip systems. Commissioner Riggs said on the left rear middle of the lot a major tree had been removed near Unit 4. He suggested planting a specimen tree there or in front of Unit 5. Ms. Van Dom said they had considered that but the spaces in front of the houses were just too small for a specimen tree to grow well without impinging on the buildings.

Commissioner Riggs said they had discussed changing the vertical stair window. Mr. Sarboraria said his preference was to keep the three stacked windows as designed to provide more light and better aesthetic inside noting it was around the side of the house. He said however he had brought an alternative with a smaller window. He said they were willing to make that change but the preference was to keep it as it was designed. Commissioner Riggs asked about the window frame and trim colors. Mr. Sarboraria said they had not selected the manufacturer or exact color yet but they would be wood sash exterior clad and with accent colors that would work well with the color adjacent to them. He said he did not think that there would be any white or off white but would

include brown tones or reddish brown tones. Commissioner Riggs confirmed with the architect that the trim and frames would be within the color plan. He asked if others were interested in changing the three stacked windows. Chair Ferrick noted the window was within the side area that was offset by the fence. Commissioner Riggs said that was a good point.

Commissioner Kadvany said related to driveway width that previously he had spoken with staff at California Fire Code who had indicated that it was local policy making that determined the driveway width using the state code as the framework. He said their local policy was that a fire access road was what was required for a development of three or more homes. He suggested that in the future that these not be called driveways but fire access roads. He asked the applicant if they had thought about breaking up the double garage doors so they looked like two single doors either functional or not. Mr. Sarboraria said they had not thought about it but noted it was possible. Commissioner Kadvany asked about the color of the pavers. Mr. Sarboraria said the pavers would be a variegated natural stone color. Commissioner Kadvany said he had raised the question about the three guest parking spaces. Mr. Sarboraria said there would need to be some restrictions written into the CC&R's so the spaces were shared by the property owners.

Chair Ferrick said she was impressed with how well the project was designed to fit within the lot area. She moved to recommend approval for 1273 Laurel Street to the City Council. Commissioner Riggs seconded the motion. He noted that they should include two trees between the homes as noted on the landscape plan and that the applicant had indicated the stone would be grouted as previously stated. He said the applicant team was very responsive and the project would be an attractive addition to Menlo Park.

Commission Action: M/S Ferrick/Riggs to recommend approval to the City Council of the following actions and direction:

1. Adopt a finding that the redevelopment of the site is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current State CEQA Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:

- a. The general appearance of the structure is in keeping with the character of the neighborhood.
 - b. The development will not be detrimental to the harmonious and orderly growth of the City.
 - c. The development will not impair the desirability of investment or occupation in the neighborhood.
 - d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.
 - e. The property is not within any Specific Plan area, and as such no finding regarding consistency is required to be made.
4. Approve the use permit and architectural control requests subject to the following **standard** conditions:
- a. Development of the project shall be substantially in conformance with the plans prepared by AJS Architecture and Planning, consisting of 29 plan sheets, dated received March 27, 2013 and recommended by the Planning Commission on April 8, 2013, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
 - b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.
 - c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
 - d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. Landscaping shall properly screen all utility equipment that is installed outside of a building and that cannot be placed underground. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
 - e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage

improvements. The plans shall be submitted for the review and approval of the Engineering Division.

- f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to issuance of a grading, demolition or building permit.
 - g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.
 - h. Prior to building permit issuance, the applicant shall submit proposed landscape and irrigation documentation as required by Chapter 12.44 (Water-Efficient Landscaping) of the City of Menlo Park Municipal Code. If required, the applicant shall submit all parts of the landscape project application as listed in section 12.44.040 of the City of Menlo Park Municipal Code. This plan shall be subject to review and approval by the Planning and Engineering Divisions. The landscaping shall be installed and inspected prior to final inspection of the building.
1. Adopt findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.

The Planning Commission provided direction that the stone veneer should contain grout between the individual stones, instead of a dry stack design.

Motion carried 6-0 with Commissioner O'Malley absent.

D5. Use Permit, Architectural Control, Major Subdivision/Forrest Mozart/1281

Laurel Street: Request to demolish a single-story, single family home and to construct three detached, two-story dwelling units on the subject parcel in the R-3 (Apartment) zoning district. Use Permit and Architectural Control would be required for the construction of new residential units. The development would include a common driveway with the adjacent property (1273 Laurel Street) for access to each residence. A Tentative Map would be required to create six residential condominium units, including three units on the neighboring legal parcel (addressed 1273 Laurel Street). As part of the proposal the applicant is proposing to remove two heritage size Mexican fan palms (22-inch and 17-inch diameter, good condition). In addition, the applicant is requesting approval of a Below Market Rate (BMR) Agreement for the payment of an in-lieu fee for this project. An initial version of the proposal was reviewed by the Planning Commission at the meeting of March 4, 2013, and was continued with direction for redesign. The proposal has since been revised, with changes to the driveway material, the windows and

orientation of the right-side front unit, and modifications to the applied materials throughout the project.

Commissioner Kadvany reminded the applicant about the option to modify the garage doors in the previous discussion to appear like two doors rather than one wide door.

Commission Action: M/S Ferrick/Riggs to recommend approval of the item to the City Council of the following actions and direction:

1. Adopt a finding that the redevelopment of the site is categorically exempt under Class 32 (Section 15332, "In-Fill Development Projects") of the current State CEQA Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:
 - a. The general appearance of the structure is in keeping with the character of the neighborhood.
 - b. The development will not be detrimental to the harmonious and orderly growth of the City.
 - c. The development will not impair the desirability of investment or occupation in the neighborhood.
 - d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.
 - e. The property is not within any Specific Plan area, and as such no finding regarding consistency is required to be made.
4. Approve the use permit and architectural control requests subject to the following **standard** conditions:
 - a. Development of the project shall be substantially in conformance with the plans prepared by AJS Architecture and Planning, consisting of 29 plan sheets, dated received March 27, 2013 and recommended by the Planning Commission on April 8, 2013, except as modified by the

conditions contained herein, subject to review and approval of the Planning Division.

- b. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
- c. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. Landscaping shall properly screen all utility equipment that is installed outside of a building and that cannot be placed underground. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
- d. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for the review and approval of the Engineering Division.
- e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to issuance of a grading, demolition or building permit.
- f. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.
- g. Prior to building permit issuance, the applicant shall submit proposed landscape and irrigation documentation as required by Chapter 12.44 (Water-Efficient Landscaping) of the City of Menlo Park Municipal Code. If required, the applicant shall submit all parts of the landscape project application as listed in section 12.44.040 of the City of Menlo Park Municipal Code. This plan shall be subject to review and approval by the Planning and Engineering Divisions. The landscaping shall be installed and inspected prior to final inspection of the building.
- h. Prior to building permit issuance, the applicant shall submit proposed landscape and irrigation documentation as required by Chapter 12.44 (Water-Efficient Landscaping) of the City of Menlo Park Municipal Code. If required, the applicant shall submit all parts of the landscape project application as listed in section 12.44.040 of the City of Menlo Park Municipal Code. This plan shall be subject to review and approval by the

Planning and Engineering Divisions. The landscaping shall be installed and inspected prior to final inspection of the building.

5. Adopt findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.

The Planning Commission provided direction to the applicant that:

- The stone veneer should contain grout between the individual stones, instead of a dry stack design; and
- Add two screening trees between the units on the right side parcel (1281 Laurel Street).

Motion carried 6-0 with Commissioner O'Malley absent:

ADJOURNMENT

The meeting adjourned at 9:36 p.m.

Staff Liaison: Planner Rogers, Senior Planner

Recording Secretary: Brenda Bennett

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PUBLIC WORKS DEPARTMENT

Council Meeting Date: May 7, 2013
Staff Report #: 13-075

Agenda Item #: F-1

REGULAR BUSINESS: **Provide Direction On The State Route 101/Willow Road Interchange Project Alternatives**

RECOMMENDATION

Staff recommends that the City Council receive the SR 101/Willow Interchange Project Alternative Report and provide direction to staff to include the Project Preferred Alternative 1B Modified Partial Cloverleaf (Attachment B) as the selected interchange design concept to be evaluated in the Environmental Analysis for this project.

BACKGROUND

The original SR 101/Willow Interchange was constructed in 1955. Willow Road is classified as a major arterial east of the SR 101/Willow Road interchange and a minor arterial west of the SR 101/Willow Road interchange. Approximately 30,000 vehicles per day travel on Willow Road between Middlefield Road and Bayfront Expressway. The existing interchange configuration in Figure 1 shows a “Four Quadrant Cloverleaf (Attachment A).”

Funding for the design and construction of the SR 101/Willow Road Interchange Project (The Project) is proposed to be funded by C/CAG’s Regional Improvements Program (RIP) and by Measure A funds, and was originally approved in the original Measure A Expenditure Plan in 1988 and extended in 2004 by voters of San Mateo County. A project study report was completed in 1989 and a Project Study Report-Project Development Report was completed in 2005. The project proposes to reconstruct the existing SR 101/Willow Road (SR 114) interchange to a partial cloverleaf or diamond interchange.

The Project is being led by Caltrans in partnership with San Mateo County Transportation Authority, C/CAG, East Palo Alto and Menlo Park. The Project is currently in the conceptual stages of design and environmental analysis. Traffic modeling and traffic operational analysis were completed in 2012 for the conceptual stages under two horizon year scenarios -2020 “Opening Year” and 2040 “Design Year.” The traffic operational analysis evaluated six alternative configurations for the interchange. The configurations were designed to minimize the overall traffic impacts to both the local streets and the freeway as well as improve all modes of transportation (vehicular, bicycle and pedestrian). The following Project Alternatives were evaluated:

1. Alternative 1A – Partial Cloverleaf
2. Alternative 1B – Condensed Partial Cloverleaf
3. Alternative 2 – Partial Cloverleaf with Auxiliary Lane
4. Alternative 3 – Partial Cloverleaf with Collector Distributor Road
5. Alternative 4A – Compact Diamond
6. Alternative 4B – Condensed Compact Diamond
7. Alternative 5 – Existing Four-Quadrant Cloverleaf (No Build)

The results of the analysis for the alternatives are shown in Table 1. Some of the alternatives could involve right-of-way impacts to adjacent property owners, while some of the alternatives minimize these impacts. The project impacts will be evaluated as part of the environmental analysis.

Table 1: Comparison of Project Alternatives

	Alternative 1A		Alternative 1B		Alternative 2		Alternative 3		Alternative 4A		Alternative 4B	
Construction Capital Cost(\$)	\$ 43.0 M		\$ 41.9 M		\$ 43.6 M		\$ 44.2 M		\$ 39.0 M		\$ 37.5 M	
Right of Way Capital Cost(\$)	\$ 11.8 M		\$ 0.6 M		\$ 4.8 M		\$ 7.2 M		\$ 4.5 M		\$ 0.7 M	
Geometry	Partial Cloverleaf		Condensed Partial cloverleaf		Partial Cloverleaf w/ Aux Lane		Partial Cloverleaf w/Collector Rd		Compact Diamond		Condensed Compact Diamond	
Right of Way												
Number of Parcels Affected	25		11		20		26		21		11	
Full Take Impacts (SQFT)	93,870		0		24,167		48,000		24,264		0	
Partial Take Impacts (SQFT)	35,142		3,013		21,014		33,291		19,325		4,293	
Traffic Operation												
	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
Overall Network VH Delay (Veh-hr)	15,629	16,681	15,552	16,836	15,464	17,481	16,510	21,529	16,503	16,500	16,754	16,003
N/B New Intersections LOS	D	C	E	C	D	C	E	C	E	D	E	D
S/B New Intersection LOS	C	C	C	C	C	C	D	C	F	D	F	D
Environmental Impacts	Great Surface Streets realignment & access modifications Possible Environmental Justice		Low No access modifications		Medium, some realignment & access modifications		Medium, some realignment & access modifications		Great Surface Streets realignment & access modifications Possible Environmental Justice		Low No access modifications	

On June 12, 2012, the City Council adopted Resolution 8062 in support of SR 101/Willow Road Interchange Project and secured funding in the amount of \$500,000 to assist the City during the environmental phase of the Project. Staff is currently in the process of hiring a consultant for this support. Staff will be completing a funding agreement with San Mateo County Transportation Authority for use of these funds.

The project alternatives were initially presented to the City Council at its regular meeting on October 9, 2012. At this meeting, Council gave direction to ensure all modes of travel are considered and incorporating evaluations of the feasibility of having a median bicycle lane on Willow Road though the interchange, similar to SR 101/3rd Avenue

interchange in San Mateo, and to evaluate the option of a separate bicycle/pedestrian bridge facility.

ANALYSIS

The “Project Need” is to address short weaving segments between loop ramps along SR 101 and Willow Road and to address all modes of transportation. These weaving conflicts cause safety concerns, reduce speed, cause back-ups, and create upstream queuing on 101. Additionally, there are deficient bicycle and pedestrian facilities at this interchange. The “Project Purpose” is to address the operational deficiencies of the interchange by eliminating the traffic weaves and to provide adequate storage on the off-ramps, improve operation of the interchange and as a result this will also improve the different modes of transportation and provide adequate bicycle and pedestrian facilities at the new interchange.

A scoping meeting and several community meetings have been held in both East Palo Alto and Menlo Park as follows:

- October 9, 2012 - City Council Presentation
- October 17, 2012 - Menlo Park Public Scoping Meeting
- October 24, 2012 - East Palo Alto Public Scoping Meeting
- November 29, 2012 - Presentation to Menlo Park Chamber-Transportation Committee
- March 6, 2013 - Joint East Palo Alto/Menlo Park Community Update Meeting
- March 11, 2013 & April 8, 2013 - Menlo Park Bicycle Commission
- March 13, 2013 - Transportation Commission

Comments and key points brought up during the public meetings included the following:

- Provide bicycle and pedestrian facilities
- Design for all 3 modes (Bicycle, Pedestrians, and Vehicles) of transportation
- Use alternatives 1B or 4B with the least residential housing impacts
- Use alternative 4B “Condensed Compact Diamond,” with signalized intersection for bicycle & pedestrian safety
- Use separate bicycle/pedestrian bridge next to the interchange (1 comment)
- Use alternatives 1A or 1B “Partial Cloverleaf”
- Do not use Alternative 1A (too much right-of-way impact)
- Separate Bicycle/pedestrian Bridge Facility
- Median Bicycle lane similar to SR 101/3rd Avenue Interchange in San Mateo

After receiving comments, the Caltrans project team evaluated three new possible options. The options evaluated included the following:

1. Alternative 1B Modified “Condensed Partial Cloverleaf”: This proposed new alternative is a variation between “Alternative 1B” and “Alternative 4B”, which are

shown for reference in Attachments C and D . This alternative is also consistent and in line with the need and purpose of the project by addressing the following:

- a. Improves overall operational benefits that are superior to all other Project Alternatives studied.
 - b. Minimizes overall right-of-way impacts from all other Project Alternatives.
 - c. Minimizes environmental impacts compared to from all other Project Alternatives.
 - d. Provides an improvement for the new signalized intersections in comparison to Project Alternative 4B which requires left turns for on-ramps at the signalized intersection that will increase delays on Willow Road.
 - e. Provides both Class I (off street bike path), and Class II (on street bike lanes) Bicycle/Pedestrian facilities on each side of the overcrossing design.
 - f. Provides a new configuration with squared Right Turns at Intersection crossing to reduce the bicycle/vehicle speed differential at these movements to improve safety.
2. Separate Bicycle/Pedestrian Bridge Facilities: This facility was evaluated, and is not being recommended at this time. Project Alternative 1B Modified, provides similar facilities and it accommodates this function within the project, without a significant increase in cost. A separate bicycle/pedestrian bridge would only be located on one side of the interchange, thus making it a longer travel distance for one direction or the other. Additionally, this option would create additional right-of-way impacts, privacy concerns, and is outside the project limit.
 3. Median Bicycle Lane similar to 101/3rd Avenue Interchange in San Mateo: This option was studied, and is not a feasible option. The proposed recommended Alternative 1B Modified is a condensed partial cloverleaf in comparison with the 101/3rd Avenue Interchange which is a full cloverleaf interchange, which doesn't include any signalized intersections. This option would create a bicycle only intersection in the middle of the road at each off-ramp, which is non-standard and would create some safety concerns. It would also require additional right-of-way, and expansion the project limits to the intersections at Bay Road to the south, and Newbridge Street to the north, which is not within the project limits or scope.

The information above and the inclusion of the Alternative 1B modified as the main design concept were included in the presentations at a joint East Palo Alto/Menlo Park community meeting on March 6, 2013 and at the Transportation Commission, and Bicycle Commission meetings. The following is a summary of the meetings:

- The joint East Palo Alto/Menlo Park community outreach meeting was attended by about 30 participants from both East Palo Alto and Menlo Park. The meeting was an open house, and was accompanied by a project presentation, and questions and answer session. There was no opposition to the project, and appeared to be well received.

- The Transportation Commission generally supported the project and had no comments.
- The Bicycle Commission, recommended approval of a Project Alternative 1B Modified, “Condense Partial Cloverleaf” as shown in Figure 2 in Attachment A. This option was recommended in combination with a lane geometric configuration and a cross section that provides a Class I, and Class II bicycle lanes separated by medians, and a 10 feet sidewalk. This alternative is also shown in Figure 3 in Attachment A.

After the community outreach process was completed, an independent analysis of the project was performed by a team of engineers from Caltrans who have not been involved in the design of this project. The team included representatives from East Palo Alto, Menlo Park, and San Mateo County Transportation Authority. The value analysis was completed over several days from February 4th through 7th of 2013. The team assessed the elements of cost, performance, construction time, and risk as they relate to project value. Key performance attributes assessed included mainline operations, pedestrian/cyclist, operations, local operations, maintainability, construction impacts, and environmental impacts. Project Alternative 1B Modified was used as the baseline for the comparison. The value analysis team concluded that this proposed Project Alternative 1B Modified provides the best value.

This project’s environmental phase is fully funded, and the project team has a very aggressive project schedule. The overall anticipated schedule for this project is as follow:

- Environmental Analysis (PA&ED): Late 2013
- Complete Design (PS&E): Mid 2015
- Advertise, Open Bid & Award: Late 2015
- Start Construction: Early 2016
- Project Completion: Early 2018

IMPACT ON CITY RESOURCES

This project is a regional project that will be added to the Menlo Park Capital Improvement Plan, and additional resources will be required to support this project. Staff is currently in the process of hiring a consultant team to support this project.

POLICY ISSUES

The project is consistent with the City of Menlo Park General Plan, Sections II-A-12 and II-D.

ENVIRONMENTAL REVIEW

The US 101/Willow Road Interchange Project CEQA environmental review will be completed by Caltrans.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed, at least 72 hours prior to the meeting.

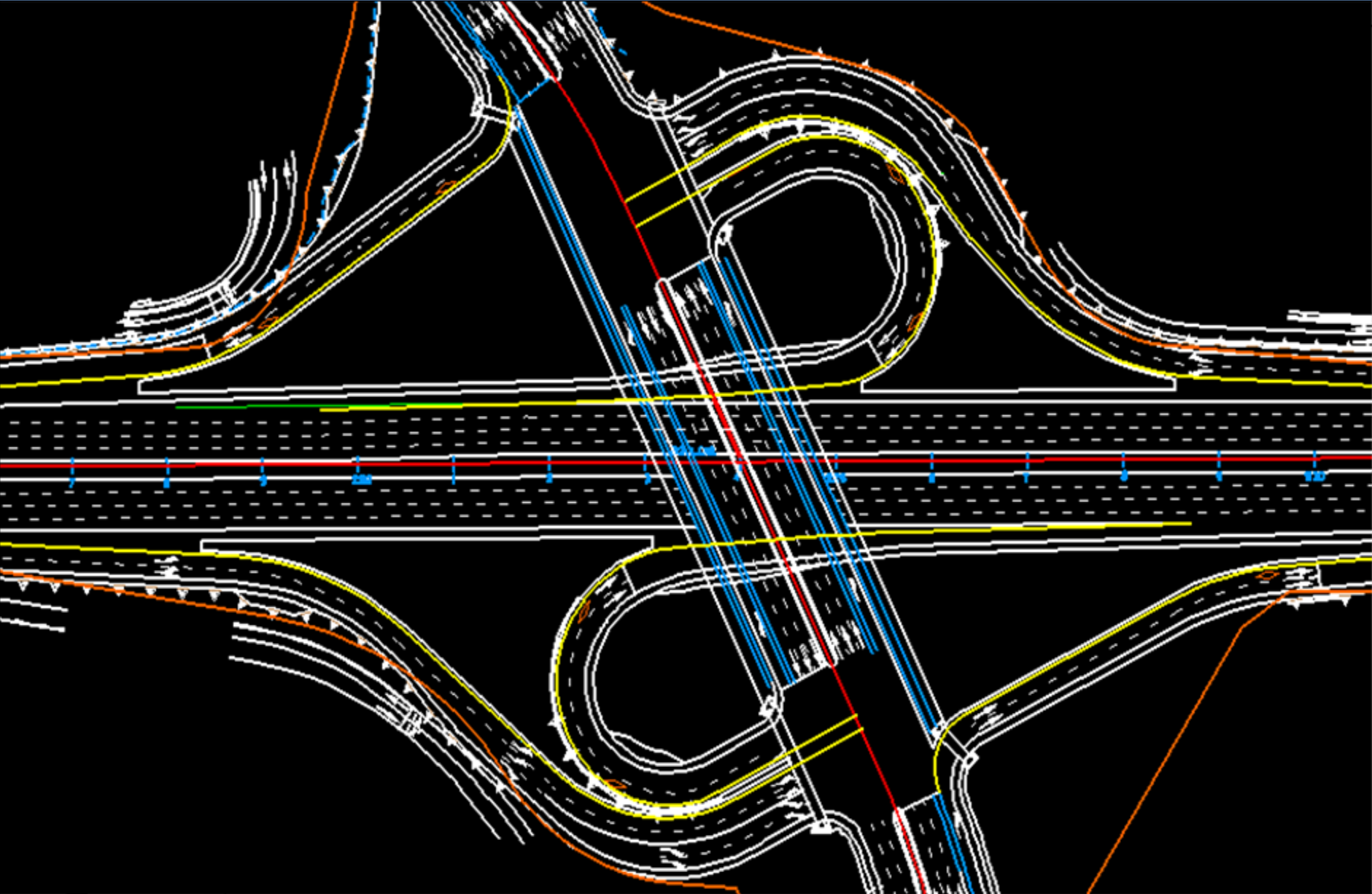
ATTACHMENTS

- A. Existing US 101/Willow Road Interchange
- B. US 101/Willow Road Alternative 1B Modified
- C. US 101/Willow Road Alternative 1B
- D. US 101/Willow Road Alternative 4B

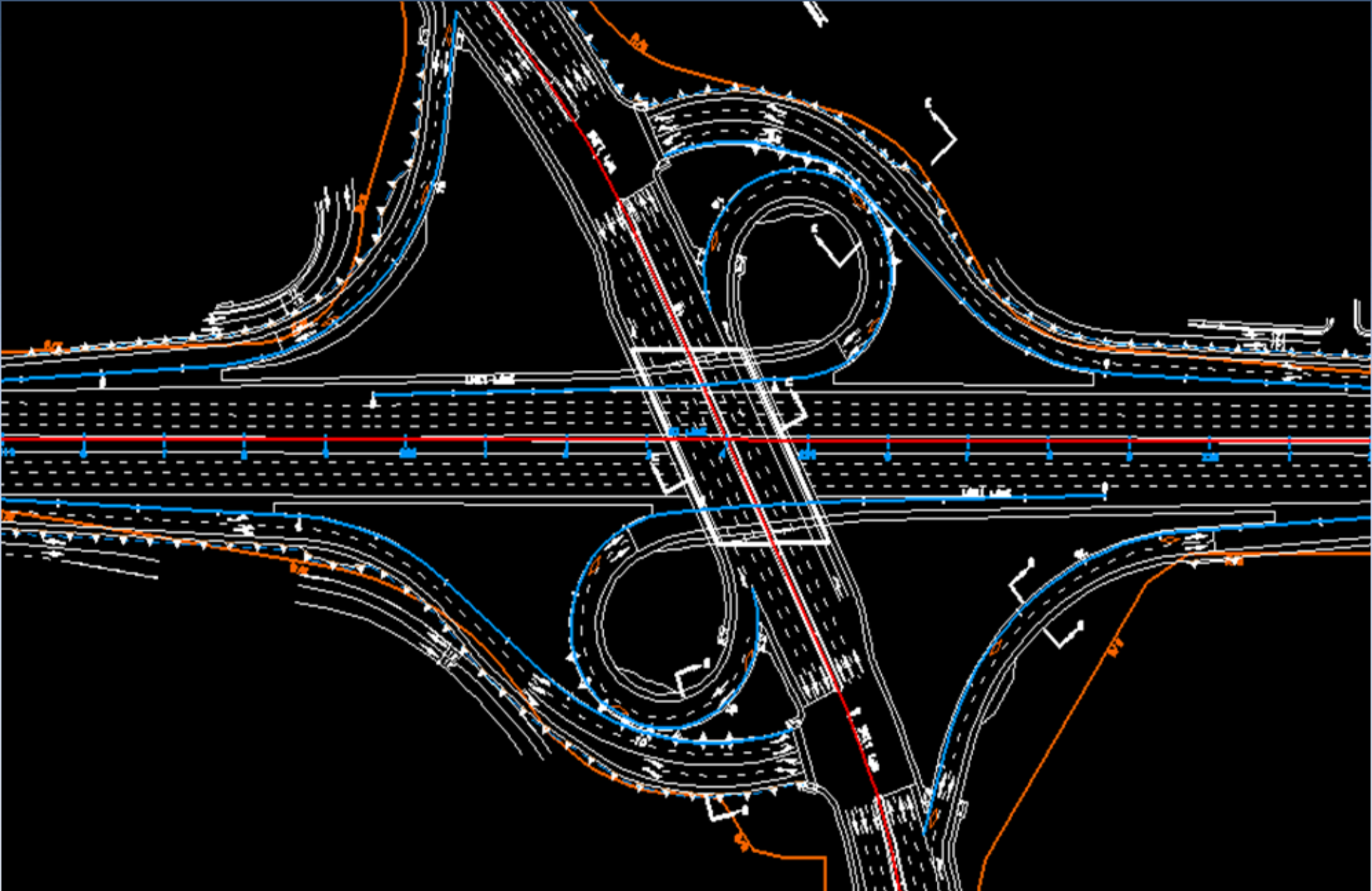
Report prepared by:
Fernando G. Bravo,
Engineering Services Manager

Charles W. Taylor,
Public Works Director

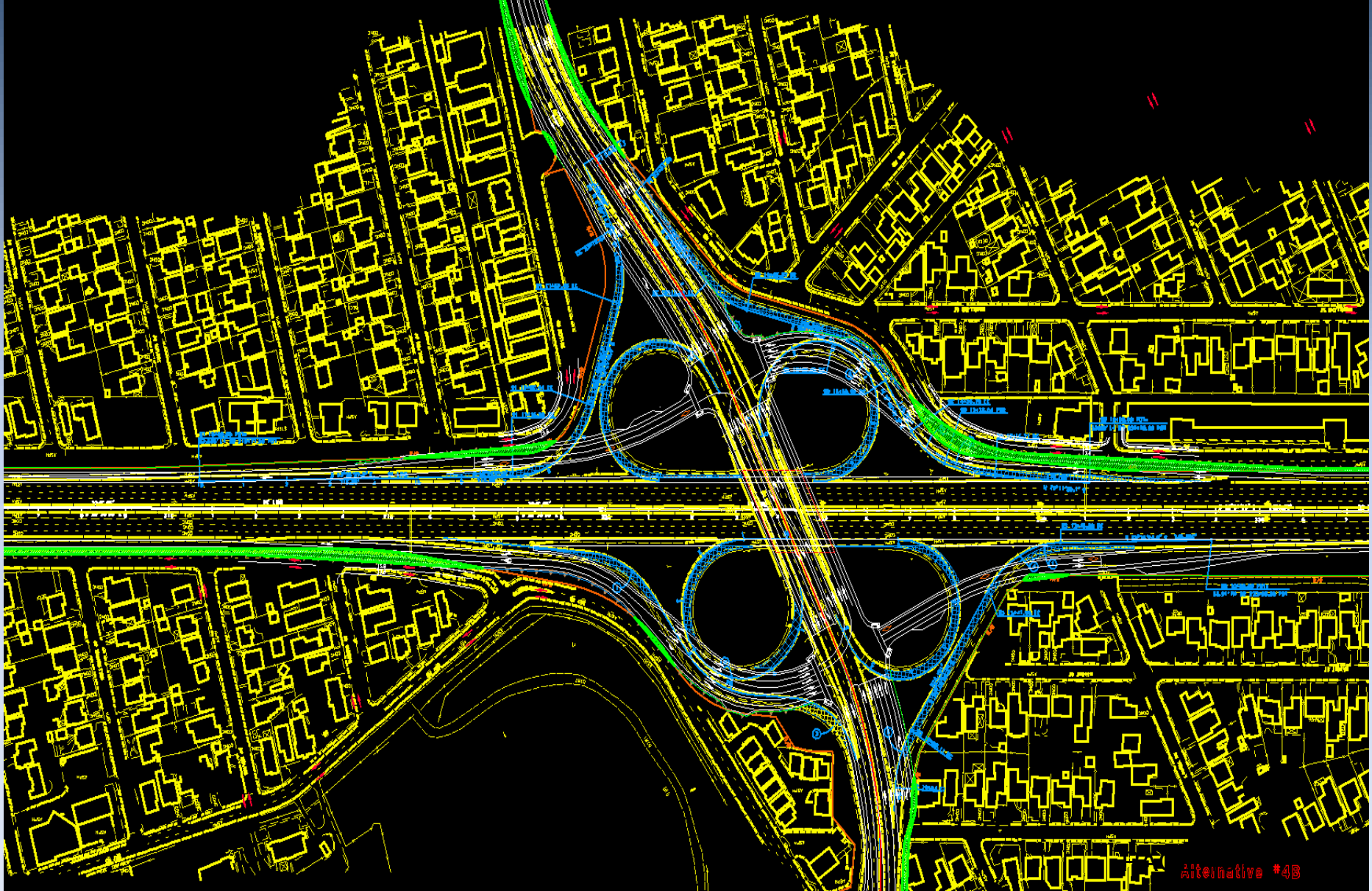
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COMMUNITY SERVICES DEPARTMENT

Council Meeting Date: May 7, 2013
Staff Report #: 13-081

Agenda Item #: F-2

REGULAR BUSINESS: Consider a Resolution Authorizing Preliminary Conditional Commitment of \$2.5 million from the Below Market Rate Fund for the CORE Affordable Housing Development at the Veteran's Administration Facility in Menlo Park

RECOMMENDATION

Staff recommends that the City Council commit to a preliminary, conditional allocation of \$2.5 million from the Below Market Rate (BMR) Housing Fund to support CORE Affordable Housing with the development of a 60-unit 100% affordable development located at the intersection of Willow Road and South Perimeter Road (700 block Willow Road) at the Veteran's Administration facility in Menlo Park.

BACKGROUND

Below Market Rate (BMR) Housing Program, Guidelines, and Fund

The BMR Housing Fund is comprised of commercial development in-lieu fees and has a balance of approximately \$6.3 million as of March 30, 2013. A summary of the fund balance as of March 30, 2013 is included as Attachment A.

The primary purpose of the BMR Housing Program is to increase the supply and assist in the development of housing that is affordable to very low-, low-, and moderate-income households. The BMR Housing Program is contained within Chapter 16.96 of the Zoning Ordinance. The BMR Housing Program Guidelines provide direction on the implementation of the program and use of the BMR Fund. Section 10.3 of the Guidelines lists the following uses of the Fund:

- Provision of below market rate financing for homebuyers;
- Purchase of land or air rights for resale to developers at a reduced cost to facilitate housing development for very low-, low- or moderate-income households;
- Reduction of interest rates for construction loans or permanent financing, or assistance with other costs associated with development or purchase of very low-, low- or moderate-income housing;

- Rehabilitation of uninhabitable structures for very low-, low- or moderate-income housing;
- On-site and off-site improvement costs for production of affordable housing;
- Reduction of purchase price to provide units that are very low-, low- or moderate-cost; and
- Rent subsidies to reduce the cost of rent for households with limited incomes.

In addition to these approved uses listed in the Guidelines, City Council approved additional uses on April 26, 2005, subject to review by the Housing Commission and approval by the Council for specific proposals. They include:

- Funding for the purchase and rehabilitation of existing apartment buildings for low-income tenants;
- Funding for the purchase of existing housing units to resell as BMR units to moderate-income households;
- Funding the purchase of BMR units until the units can be sold; and
- Funding loans to BMR unit owners to cover costs arising from repairs in the common areas of condominium projects.

ANALYSIS

The CORE project is envisioned as a 60-unit permanent multifamily housing development on a 2.011 acre site located near Willow Road and South Perimeter Road in Menlo Park (described as the 700 block of Willow Road). See location map, Attachment C. The site is one of the Housing Opportunity Sites identified in the Draft Housing Element. The proposed unit mix includes 54 studios and 6 one-bedroom units. The project would be 2 stories and a total of 40,000 square feet of gross floor area. The proposed project includes parking for 35 vehicles (see conceptual site plan, Attachment D). The proposed income mix is evenly distributed across unit types, and includes 7 units restricted to 30% Area Median Income (AMI) and 52 units restricted to 40% AMI. One unit is an “exempt” manager unit to be occupied by property staff.

Income restrictions and rental rate restrictions would apply to all 59 low-income units, consistent with applicable Tax Credit regulatory agreements. CORE proposes that tenant applications will be reviewed in order of ranking, based on the requirements of the contributing funding agencies to ensure compliance with the City’s BMR Guidelines. The proposal assumes approximately 11 Menlo Park Priority Units for which the BMR Fund Guidelines will supersede all other leasing preferences. Leasing protocol for these units is depicted in Attachment C, which is consistent with the City of Menlo Park’s BMR Fund Guidelines, Sections 7 and 11.

The project’s estimated cost is \$14,100,000 in hard and soft costs, excluding the value of the land contribution by the Department of Veterans Affairs which will be through a long-term ground lease. The current preliminary per unit costs are estimated at \$235,000/unit excluding land. These cost estimates are typical for developments of

similar scale in the South and West Bay Region. A recent survey of three similar developments in the South Bay and West Bay Region by CORE indicates that typical per-unit pro forma cost of \$210,000 and \$320,000, excluding land. CORE also states that according to a presentation by the California Tax Credit Allocation Committee (CTCAC) staff in 2011, tax credit developments in the South and West Bay Region are the most costly of all regions in the State, with a regional average cost of \$598,000 per unit for new construction in 2011, including land costs.

CORE also states that CTCAC continues to research and collect public comments to identify the contributing factors of higher regional costs for affordable housing. Some reasons cited in public hearings include: (a) prevailing wage requirements, (b) local plan check and impact fees, (c) higher standards and costs for design and materials in high-cost regions, (d) higher construction costs in infill locations, (e) lenders' underwriting requirements, (f) added cost of green building design, construction and certification, and (g) higher design costs associated with higher level of public scrutiny of low-income housing.

According to an appraisal obtained by the VA, the "highest-and-best-use" value of the VA land is \$13,200,000. The project is estimated to generate approximately \$11,000,000 in tax credits, depending on pricing and tax rates at time of sale. Given the estimated hard and soft project costs at \$14,100,000, this leaves a funding gap of \$3,100,000 necessary to make the project financially feasible. CORE is also seeking up to \$600,000 from San Mateo County leaving a \$2.5 million gap which staff is recommending the City of Menlo Park fill through the BMR program. The final structure of the soft loan from the City would be determined once the City's conditions (below) have been met and would return to Council for final approval.

This contribution translates into approximately 11 of the 59 low-income units. CORE has stated that these would be "Menlo Park Priority" units, for which the income-qualifying applicants who are Menlo Park residents/workers would get first-priority, ahead of any Veteran preference. All 59 BMR units would count toward the City's Regional Housing Needs Assessment (RHNA) requirements and would represent roughly 25% progress on the Very Low Income allotment of 233 units by 2022. The complete preliminary proposal from CORE is included as Attachment C.

IMPACT ON CITY RESOURCES

Attachment A summarizes the current and anticipated future status of the BMR fund. The total balance includes \$2,202,969 available for Purchase Assistance (PAL) loans (however the program has been suspended due to elimination of Housing staff); \$996,000 remaining in the Neighborhood Stabilization Program (which staff also recommends eliminating based on lack of staff to administer the program and improving neighborhood conditions which eliminate the need for the program), and \$650,000 for Habitat for Humanity's Neighborhood Revitalization Program (NRP). A total of \$1,917,438 is not currently designated to a particular project or program bringing the total of currently available funds to \$6.96 million. Total liabilities include payments for

services to Palo Alto Housing Corporation (for administration of the BMR wait list and BMR sales) and Hello Housing (for management of the existing housing loan program) and the Habitat commitment previously approved by Council. This leaves a current approximate balance of available funds of \$6.3 million.

The anticipated BMR revenues from approved projects and future sale of BMR properties should yield an additional \$11.5 to \$16 million (depending upon whether Facebook builds units or pays fees) to meet the commitment previously made to prioritize the use of BMR funds for non-profit development of affordable workforce rental housing through the issuance of a Notice of Availability of Funds in the near future.

Although CORE's original request to the City was for \$3.1 million, that proposal did not include the \$600,000 anticipated from the San Mateo County HOME/CDBG fund. Staff recommends an initial commitment of \$2.5 million at this time which can be reconsidered following a final decision by the County on their total contribution, expected in June. Staff would return to Council with a final recommendation on the amount once conditions (below) are met.

POLICY ISSUES

Below-market-rate units at the deepest affordability levels are the most challenging to finance, and the most critical among Bay Area housing needs. This project is well-positioned given the exceptional public contribution of land valued at an estimated \$13,200,000. To finance these units in any other location in Menlo Park would require a similar level of subsidy for land acquisition. Comparable developments in other locations west of 101 where property values are higher would require a much greater subsidy.

The proposal from CORE is consistent with the City's BMR policy and guidelines as the current Development Plan includes income restrictions for 59 out of 60 units. Per the anticipated Low Income Housing Tax Credit Regulatory Agreements, tenants will not be accepted unless their household income levels are at or below 30% AMI for 7 of the units, and 40% AMI for the remaining 52 low-income units. CORE proposes designating 11 of the 59 low-income units as "Menlo Park Priority" Units. When available, these 11 units would be leased FIRST to income-qualifying applicants who live or work in Menlo Park. This pro rata share of units will be leased according to the City of Menlo Park's BMR Fund Guidelines, Sections 7 and 11, while maintaining compliance with all Fair Housing Law and Low Income Housing Tax Credit regulatory agreements.

Benefits of the project include the VA's willingness to make the site available for no cost; it provides permanent supportive housing to homeless and at-risk adults, particularly veterans, and helps meet a pressing and long-term need; it utilizes an opportunity site included in the draft Housing Element; and the \$2.5 million commitment of BMR funds to leverage a 60-unit affordable rental project is quite reasonable based on cost per unit. Additionally, CORE appears to have a suitable track record of development and operation of affordable rental housing and has identified a services

partner with a track record of providing supportive services to special needs households.

An additional benefit of the project to the community is the progress it would represent toward meeting RHNA goals with 59 units restricted to 30% and 40% AMI translating to more than 25% progress on the 233 Very Low Income units needed per the proposed Menlo Park RHNA for 2014-2022. Demonstrated progress on the City's RHNA allocation can position the City for a share of State funds for Congestion Management.

Staff recommends a preliminary, conditional approval of the funds based on this early stage of development and conditioned upon City review and approval of the following:

1. Completion of a satisfactory environmental review process;
2. A full financial pro forma that includes an estimate of sources and uses for each development phase – predevelopment, construction, and permanent financing;
3. A statement of operating income and expenses;
4. A long-term cash flow statement (at least 15 years, preferably 20-30 years);
5. A statement as to what supportive services will be provided and how they will be paid for;
6. An evaluation as to how the project, as proposed, will be competitive for low income housing tax credits.

Prior to full funding commitment, staff feels it is important for CORE to demonstrate that the proposed rents will be affordable to the target population. Besides requiring CORE secure the standard market study required for a future tax credit application, staff suggests the Council seek evidence that 30%-40% AMI rents are specifically affordable to their primary target population (i.e., veterans who are homeless or at risk of homelessness). In short, staff recommends preliminary, conditional commitment until CORE has completed environmental review and planning entitlements and more detailed financial plans supporting the financial feasibility of the project are submitted that demonstrate the amount of funding requested from the City is appropriate.

ENVIRONMENTAL REVIEW

Since the City would be providing BMR funds to CORE, this project must comply with the requirements of the California Environmental Quality Act (CEQA). It is also subject to the National Environmental Protection Act (NEPA) because it is a project carried out, financed or approved in whole or in part by federal agencies.

In the event that a project requires both a CEQA EIR and a NEPA EIS, the lead agency shall, whenever possible, use the EIS as the EIR to avoid duplication. The CEQA Guidelines also provide for preparation of a joint EIR/EIS, which combines federal and state reports into a single document. Based on CEQA, a collaborative process where the federal, state and local approving agencies work together to create a document that satisfies both CEQA and NEPA (and uses the stricter requirement from either set of laws) is allowed.

The Department of Veterans Affairs has secured Dyson Environmental Management and Compliance (DEMC) consultants to manage the NEPA environmental assessment as well as the CEQA review. DEMC will use regulations and implementation procedures set forth by the Council and the VA in preparing the environmental review which will evaluate the potential environmental impacts of the project. Prior to a full funding commitment, the City Council would have to make required CEQA findings.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed, at least 72 hours prior to the meeting.

ATTACHMENTS

- A. BMR Fund Status
- B. CORE Preliminary Proposal
- C. Resolution

Report prepared by:
Starla Jerome-Robinson
Assistant City Manager

**BELOW MARKET RATE HOUSING RESERVE
FUND BALANCE and ANTICIPATED REVENUES
as of 5/01/13**

FUND BALANCE

Designated for PAL Loans and available (not including loans receivable)	2,202,969
Designated for Neighborhood Stabilization Program Balance (recommend elimination)	996,000
Designated for Habitat for Humanity Neighborhood Revitalization (hold/not available)	650,000
Designated for Hamilton Housing Project (not needed -- sale in process)	57,815
Sale of 297 Terminal Ave	484,000
Sale of 1441 Almanor	295,000
Fees collected in FY 2012	365,274
Undesignated	1,917,438
 Current balance	 6,968,496
less annual contracts with PAHC (\$35,250) and Hello Housing (\$12,000)	-47,250
less designation for Habitat for Humanity Neighborhood Revitalization	-650,000
Total currently available	6,271,246

ANTICIPATED BMR REVENUES FROM APPROVED and PENDING PROJECTS

Sale of properties held (Hollyburne, Sage, Riordan) assume all BMR sales	893,201
Menlo Gateway	8,543,207
Laurel 6 Unit	180,000
Kelly Court	74,497
Facebook (option to provide 15 units)	4,507,291
Commonwealth	1,796,267
TOTAL APPROVED PROJECT FUTURE REVENUES	\$15,994,472.00

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CORE affordable

April 29, 2013

Starla Jerome-Robinson
Assistant City Manager
701 Laurel Street, Menlo Park, CA 94025

Sent via email: srobinson@menlopark.org

RE: Updated Revised Proposal for Affordable Housing

Dear Ms. Jerome-Robinson,

Thank you again for your time and interest in evaluating the proposed 60-unit very low-income housing development on the VA Palo Alto Healthcare System's Menlo Park Division property.

As discussed in prior correspondence, we have requested a residual receipts loan from the City of Menlo Park's Below Market Rate Fund, in the amount of \$3,100,000. The purpose of this letter is to provide updated information since my last letter and proposal submitted to you on January 24, 2013. Additional information provided in this update includes:

- QUALIFICATIONS – Supplemental information regarding Core Affordable Housing's Qualifications
- BUDGET - Detailed Project Budget and updated Per Unit Calculations
- SITE DESIGN - Draft Conceptual Site Plan & Proposed Design Summary (including map and context, square footage, height, and parking)
- CEQA - Summary of the proposed CEQA-compliant environmental review
- TIMELINE – Proposed schedule of development

Core and its non-profit partner EHC LifeBuilders are extremely dedicated to bringing this project to fruition, and committed to ensuring it is a property that City's staff and residents are proud to have in their community for years to come. Ultimately, the units can be a testament to the City's commitment to promoting housing for the array of income levels in its community. All 59 low-income units could be counted as evidence of the City's progress in its Regional Housing Needs Allocation (RHNA) – roughly 25% progress on the Very Low Income allotment of 233 units by 2022.

This project presents an extremely unique and valuable opportunity for the City. As you are aware, below-market-rate units at the deepest affordability levels are the most challenging to finance, and also the most critical among Bay Area housing needs. This project, is

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uniquely well-positioned given the exceptional public donation of land that could otherwise fetch an estimated \$13,200,000. To finance these units in any other location in Menlo Park would require that a similar level of subsidy of land acquisition be made.

Thank you again for your time and consideration. We look forward to continuing discussions to help bring this project to fruition.

Yours truly,



Darci Palmer
Assistant Project Manager
Core Affordable Housing

408-292-7841 x42
dpalmer@thecorecompanies.com

CORE affordable

Update to Revised Proposal for Affordable Housing Development
Location: 700 Block of Willow Road (corner of Willow Road and South Perimeter),
Menlo Park

Submitted to City of Menlo Park
Original Proposal: January 24, 2013

Update: April 29, 2013

Contact:
Darci Palmer
Core Affordable Housing
408-292-7841 x42
dpalmer@thecorecompanies.com
CORE AFFORDABLE HOUSING

Qualifications

The Core Companies ("Core") is a group of independent companies that includes an affordable housing development firm, a general contractor and a market-rate homes division. Core specializes in the development of medium and high-density infill projects. Core's reputation for reliability, accountability, integrity, and commitment to our residents and our funding partners is unsurpassed.

Core Affordable Housing, LLC, has developed 18 multifamily, affordable rental properties in the last 18 years, with 3 additional projects in the pipeline. Core remains intimately involved and committed to its properties throughout operations, acting as owner ensuring the properties' ongoing financial health, regulatory compliance, and physical maintenance. The company has extensive experience and expertise in the following areas:

- Land Assembly
- Selection of Consultants
- Site and Project Design
- Feasibility Analysis
- Process of Entitlements
- Construction Management
- Affordable Housing Finance
- Marketing and Lease Up

Though Core does have experience and expertise in asset management and property management, we do not manage our own rental communities "in house." We contract with reputable and qualified third party partners to manage our communities and provide services tailored to residents' needs. Examples of such third party property management and service providers include EAH Housing, Charities Housing, Related Companies, and EHC LifeBuilders.

Project Summary

The project is envisioned as a 60-unit permanent multifamily housing development on a 2.011 acre site located near Willow Road and South Perimeter Road in Menlo Park. Unit mix includes 54 studios and 6 one-bedroom units. Income mix is evenly distributed across unit types, and includes 7 units restricted to 30% Area Median Income (AMI) and 52 units restricted to 40% AMI. One unit is an "exempt" manager unit to be occupied by property staff. Unit mix and affordability are summarized in **Table 1: Affordability / Unit Mix**.

Income restrictions and rental rate restrictions would apply to all 59 low-income units, consistent with applicable Tax Credit regulatory agreements. Tenant applications will be reviewed in order of ranking, based on the requirements of the contributing funding agencies. The primary mission of the project is to serve Veterans who are homeless or are at risk of homelessness. EHC LifeBuilders will provide in-house services to residents, tailored to individual needs, to promote health and self-sufficiency.

Table 1: Affordability / Unit Mix

UNIT TYPE	AMI DESIGNATION	UNIT QUANTITY	MAX INCOME	SQUARE FOOTAGE	MAX RENT Net of Utilities
Studio	30% ELI	6	\$ 22,170	500	\$527
1 Bedroom	30% ELI	1	\$ 25,320 (2ppl)	650	\$555
Studio	40% VLI	48	\$ 29,560	500	\$712
1 Bedroom	40% VLI	4	\$ 33,760 (2ppl)	650	\$753
1 Bedroom	Live-In Staff	1	\$ n/a	650	\$1,423
Notes:					
<ul style="list-style-type: none"> • “AMI” stands for Area Median Income published annually by the California Department of Housing & Community Development. • “ELI” stands for Extremely Low Income defined as below 30% Area Median Income; • “VLI” stands for Very Low Income defined as below 50% Area Median Income 					

Development Budget

The project is estimated to cost approximately \$14,824,110 in hard and soft costs, excluding the value of the land contribution by the Department of Veterans Affairs. A complete project budget is provided in **Attachment A**. This increase in estimated project costs reflects additional costs assumed such as advanced green building, construction loan interest rates, and other financing costs. These costs translate to approximately \$247,000 per unit. Similar developments in the South Bay and West Bay Region have been estimated to cost between \$210,000 and \$320,000, excluding land, depending on design scrutiny, complexity of structural design, and amount of public review and impact fees.

According to a presentation by the California Tax Credit Allocation Committee (CTCAC) staff in 2011, tax credit developments in the South and West Bay Region are the most costly of all regions in the State, with a regional average cost of \$598,000 per unit for new construction in 2011, including land costs. CTCAC continues to research and collect public comments to identify the contributing factors of higher regional costs. Some reasons cited in public hearings include: (a) prevailing wage requirements, (b) local plan check and impact fees, (c) higher standards and costs for design and materials in high-cost regions, (d) higher construction costs in infill locations, (e) lenders’ underwriting requirements, (f) added cost of green building design, construction and certification, and (g) higher design costs associated with higher level of public scrutiny of low-income housing. This project is expected to include all of these factors.

Core’s internal process of development includes commitment to reducing costs while delivering the highest quality of affordable housing. If the City is interested in further explanation of anticipated project costs, we are available to address specific questions.

Development Financing

The land will be donated by the Department of Veteran's Affairs through a long-term ground lease. The "highest-and-best-use" value of the land is appraised at \$13,200,000 (See **Attachment B** for land appraisal summary). The project is estimated to generate approximately \$11,200,000 in tax credit equity, depending on pricing and tax rates at time of sale. Given the estimated hard and soft project costs estimated at \$14,824,110, and deferred developer fee of approximately \$324,110, this leaves a funding gap of \$3,300,000 necessary for financial feasibility. Typical sources for this type of gap include City and County lending programs.

Core has applied for a short-term predevelopment loan from HEART in the amount of \$700,000 and the Housing Trust of Silicon Valley for \$500,000. If secured, these funds would be used to develop building design, commission third party reports, pay financing and application fees, and secure a building permit. They would be repaid with permanent funding sources (i.e., tax credit equity or City loan), at start or completion of construction. Evidence of soft funding commitments from the City and the County would help significantly in closing these predevelopment loans.

In January 2013, Core was one of three applicants seeking the County of San Mateo for HOME/CDBG financing from the U.S. Department of Housing & Urban Development (HUD). Due to Federal sequestration, the County's anticipated funding availability for new construction such as this project is expected to be significantly less than previous years: possibly \$600,000 or less. Upcoming in June of 2013, the County's Housing & Community Development Committee (HCDC) is expected to determine which applicant will be awarded these funds. The most likely recipient is the applicant that can demonstrate evidence of local City funding commitment.

The County of San Mateo recently dedicated \$10,000 of "Boomerang Funds" to Housing. A Notice of Funding Availability and call for applications is expected in approximately June 2013. Core plans to apply for this funding if and when it becomes available and if there is still a financing gap for the project at that time.

City of Menlo Park Funding Request & Consideration

The developer is requesting a soft loan of \$3,100,000 from the City of Menlo Park. This amount represents approximately 22% of project's \$14,300,000 anticipated public funding sources. If we apply the 22% figure *pro rata* to the development's 59 low-income units, it yields 13 units available for "Menlo Park Priority," for which the BMR Fund Guideline's selection criteria would be in "first position," ahead of a preference for Veterans. The BMR Funds would not be allocated to the "other" units, per the City's guidance regarding use-restrictions on the City's funds. However, all 59 low-income units will create benefit to the City with respect to its Housing Element and RHNA "progress."

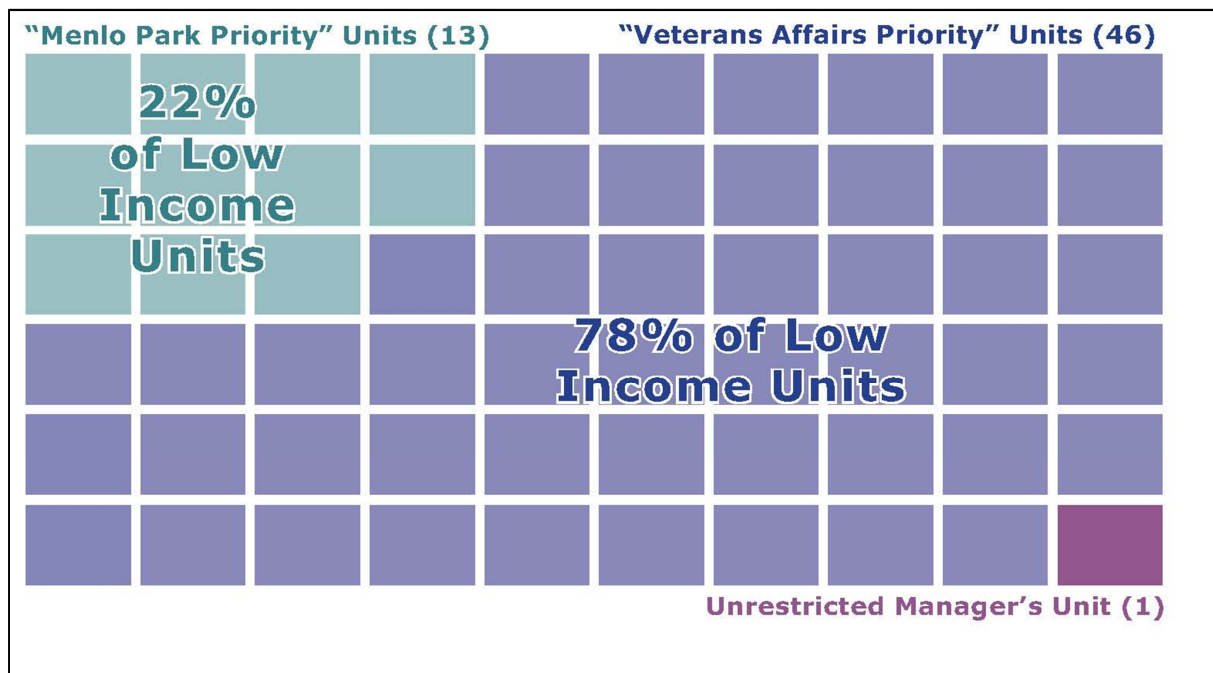
Refer to **Table 2: Proposed Project Funding & Segmentation of Units by Funding Source**, which provides the mathematical logic underlying the designation of Menlo Park Priority status to 13 of the low-income units. **Figure 1: Proportionality of Unit Type** depicts the two unit-type designations.

Table 2: Proposed Project Funding & Segmentation of Units by Funding Source

USES	ESTIMATED VALUE / COSTS		
Land Donation	\$ 13,200,000		
Hard & Soft Costs Excluding Land	\$ 14,824,110		
TOTAL VALUE	\$ 18,024,110		
PUBLIC FINANCING SOURCES		PERCENT OF PUBLIC CASH SOURCES	UNIT PROPORTION of 59 BMR Units
City of Menlo Park Loan Request	\$ 3,100,000	22%	13 units
Tax Credit Equity	\$ 11,200,000	78%	46 units
County of San Mateo (Unknown)	TBD	TBD	TBD
Total	\$ 14,300,000	100%	(low-income) 59

	PER UNIT SUBSIDY	PERCENT OF UNIT COST
City of Menlo Park Loan Request	\$ 51,667	21%

Figure 1: Proportionality of Unit Type



Consistency with BMR Fund Guidelines

This proposal assumes 13 Menlo Park Priority Units for which the BMR Fund Guidelines will supersede all other leasing preferences. Leasing protocol for these units is depicted in **Figure 2: Leasing Protocol**, as well as **Table 3: Tenant Selection Ranking for Menlo Park Priority Units**. The protocol described by **Figure 2** and **Table 3** are consistent with the City of Menlo Park’s BMR Fund Guidelines, Sections 7 and 11.

Figure 2: Leasing Protocol

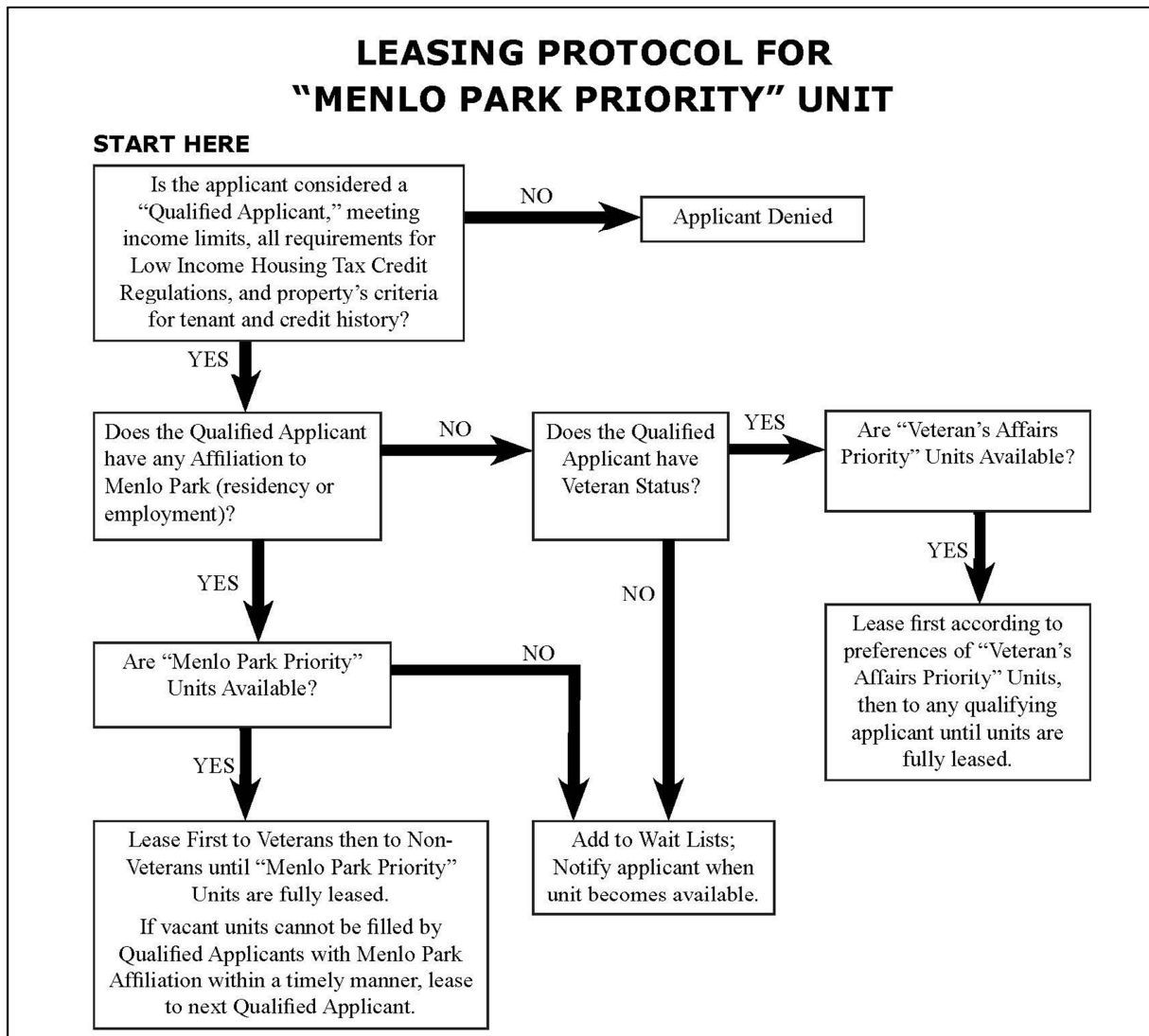


Table 3: Proposed Tenant Selection Ranking for Menlo Park Priority Units (13 of 59)

Applicant Ranking	Came Via City of Menlo Park's Managed Waitlist	Income Qualifying for 40% AMI	Menlo Park Status	Veteran Status
First Priority	✓	✓	✓	✓
Second Priority	✓	✓	✓	✓
Third Priority	✓	✓	✓	
Fourth Priority		✓	✓	✓
Fifth Priority		✓	✓	✓
Sixth Priority		✓	✓	
Seventh Priority		✓		✓
Eighth Priority		✓		✓
Ninth Priority		✓		

Municipal Benefits to City of Menlo Park:

- **Regional Housing Needs Allocation**

59 units restricted to 30% and 40% AMI translates to more than 25% progress on the 233 Very Low Income units needed per the proposed Menlo Park Regional Housing Needs Allocation for 2014-2022. Demonstrated progress on the City's RHNA allocation can position the City for a share of State funds for Congestion Management.

- **Exceptional Land Subsidy by Federal Government**

The VA's contribution of land creates an extremely unique opportunity for provision of the highest-need and most challenging units. Comparable developments in the City of Menlo Park are estimated at Leverage BMR Fund against Federal land contribution for lower relative City subsidy.

- **Use of BMR Funds**

Pro rata share of low income units will be leased according to the City of Menlo Park's BMR Fund Guidelines, Sections 7 and 11, while maintaining compliance with all Fair Housing Law, Low Income Housing Tax Credit regulatory agreements, and the funding requirements of all other participants.

Site Design

The site is located on South Perimeter Road, between Oak Avenue and Willow Road, on the Veterans Affairs campus located at 795 Willow Road in Menlo Park. **Figure 3: Site Context Maps** shows the proposed housing site location in the context of the Veterans Affairs campus and surrounding Menlo Park.

Figure 3: Site Context Maps



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Core is working with VTBS Architects and Underwood & Rosenblum Civil Engineers to develop a conceptual site plan that meets the needs of the Department of Veterans Affairs, the City of Menlo Park's R-4 and proposed R-4S design standards, and feedback from low-income Veterans of the population expected to lease. **Figure 4: Draft Conceptual Site Plan** is a preliminary design* that is still undergoing review by multiple departments within Veterans Affairs.

Based on feedback from the staff of the Department of Veterans Affairs and the City of Menlo Park, priorities in site design include, in no particular order:

- Veterans Affairs Campus Requirements for Utility Access & Security
- City of Menlo Park Development Standards
- Tree Preservation
- Minimizing costs with respect to engineering and construction
- Pedestrian oriented design in relation to VA campus and Willow Road
- Resource efficiency and Green Building standards in design, construction, landscaping, and building operations
- Fire District access
- Architectural scale and style that is compatible with surrounding development
- Sufficient parking for residents and staff
- Avoid additional driveway cutout on Willow Road and mitigate potential circulation impacts on Willow Road

****It is important to be aware of the possible changes with respect to the Draft Conceptual Site Plan under consideration:***

- 1. The housing site's driveway access point may change to be located on South Perimeter Road instead of on Oak Avenue;**
- 2. The building footprint and parking lot may "swap" locations.**
- 3. Other changes based on future feedback from the VA, City of Menlo Park Public Works, Fire District, etc.**

Building Design

The design is currently envisioned to be a 2-story Type V-A construction, wood frame building with 54 studio units at 500 sq. ft. and 6 one-bedroom units at 650 sq. ft., which will include a live-in staff manager's unit for a total of 60 units of apartments. Total building footprint is expected to be approximately 20,000 square feet on a 2.011 acre site. Total interior square footage is estimated at approximately 40,000, resulting in roughly 0.5 Floor Area Ratio.

Included in this rental community will be approximately 4,000 square feet of common area comprised of management and service offices, lobby and postal facilities, community and flexible-use space, a fitness studio, common laundry facilities, and all support facilities such as stairs, elevator, janitorial closets and utility rooms.

There will be 35 uncovered parking spaces provided on-grade with 60 secured bike parking stalls. Care will be taken in the design of the project to preserve the existing redwood and oak trees to the greatest extent feasible. Common outdoor space will be provided in a secured area with easy access from the building for residents to enjoy the outdoors in a private or group setting.

Preliminary discussions regarding architectural style of the project have suggested creating a Spanish style design or similar variation. Sample styles under consideration are shown in **Figure 5: Spanish Architectural Style Samples**.

Figure 5: Spanish Architectural Style Samples



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Environmental Review

Before executing a long term ground lease with Core for the proposed housing development, the Department of Veterans Affairs (VA) is required to undergo an environmental review consistent with the National Environmental Policy Act (NEPA). The VA has secured Dyson Environmental Management and Compliance (DEMC) consultants to manage the NEPA Environmental Assessment as well as the California Environmental Quality Act (CEQA) review, in anticipation of City financing and CEQA requirements. DEMC's proposed scope of work and qualifications are provided as **Attachment C**.

DEMC will use the regulations and implementation procedures set forth by the Council on Environmental Quality and the VA, as well as the CEQA implementing procedures of the to develop an internal draft EA/Initial Study (EA/IS). The EA/IS will be written so that the general public can easily understand the potential environmental impacts. The proposed internal draft EA/IS will evaluate the environmental impacts (both positive and negative) of construction of housing VA-owned property with preference for Veterans. DEMC will prepare the draft Finding of No Significant Impact/Notice of Determination (NoD), upon receipt from direction from the VA/Menlo Park that no further investigation required.

DEMC will then provide a draft Notice of Availability (NoA)/Notice of Determination (NoD) to VA and the Menlo Park staff for review and comment. Upon approval from VA and Menlo Park, DEMC will ensure the publication of the NoA for the draft EA/IS and FONSI/Notice of Determination (NoD) in a daily local newspaper. A legal affidavit will be obtained from the newspaper providing proof of publication and availability. This will begin the 30-day public notice period.

DEMC will prepare written responses to any public comments received and forward to VA and Menlo Park staff for review and approval. Presuming these comments / responses are minor in nature, DEMC will incorporate comments from the public comment period into the Final EA/IS and FONSI/NoD. San Mateo County Clerk will be provided the Final EA/IS and FONSI/NoD.

Timeline

The pace of development depends most significantly on preliminary commitment of local soft financing such as that by the City of Menlo Park and/or County of San Mateo, and the design review and approval process by various stakeholders. Once financing is secured, design review, approvals, third party reports (such as market study, environmental review, etc.) can be developed to prepare for a tax credit application. Construction is expected to start within 6 months of an award of tax credits.

The current timeline is summarized in **Table 4: Tentative Development Schedule**.

Table 4: *Development Schedule*

Milestone	Approximate Date	Completed
Site Control	12/26/2011	X
Apply for Financing	ongoing	ongoing
Zoning Agreement Between Lessee and Local Authorities	8/1/2012	X
Conceptual Plan Drafted	12/22/2012	X
All Soft Financing Commitments in Place	6/15/2013	
Environmental Reviews and NEPA FONSI (led by VA)	6/15/2013	
Site Design and Stakeholder Outreach/Planning Permit	7/15/2013	
Admit Non-Profit Managing General Partner & Select Property Management Agent	7/15/2013	
Financing Commitment Letters (LIHTC Equity Investor LOI and Soft Loan Commitments)	2/1/2014	
Finance Closing & Construction Start	12/1/2014	
Begin Lease-Up & Operations	5/1/2016	

ATTACHMENTS**Attachment A** – Development Budget**Attachment B** – Veterans Affairs Land Appraisal Summary**Attachment C** – DEMC Environmental Consultant Scope and Qualifications

Attachment A – Development Budget

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WILLOW HOUSING LP**2.011 Acres, 60 units, 100% Affordable****Located near intersection of Willow Road & South Perimeter Road in Menlo Park**

USES	CONSTRUCTION (including predev)	STABILIZATION/ CONSTRUCTION LOAN PAY-OFF	TOTAL PROJECT	ELIGIBLE BASIS ESTIMATES
LAND				
Ground Lease	75		75	-
Demolition	43,821		43,821	-
Relocation (Not Applicable)	-		-	-
Title & Recording	65,000		65,000	-
NEW CONSTRUCTION				
Sitework	849,315		849,315	849,315
Offsite Improvements	101,770		101,770	101,770
Structures	6,958,174		6,958,174	6,958,174
Contractor Overhead	158,740		158,740	158,740
General Requirements	449,263		449,263	449,263
Contractor Profit	485,744		485,744	485,744
Bond Premium (P&P)	64,644		64,644	64,644
GL Insurance (Owner & Builder & First Year Operations)	475,000		475,000	463,000
Construction Contingency	300,000		300,000	300,000
ARCHITECTURE & DESIGN				
Architecture	215,253		215,253	215,253
Engineering & Other Design	579,500		579,500	579,500
Environmental Audit	5,500		5,500	5,500
Project Administration	40,000		40,000	40,000
FINANCING COSTS				
Origination & Other Loan Fees	218,984		218,984	50,000
Lender Inspection Fees	22,500		22,500	22,500
Predevelopment Interest	75,000		75,000	37,500
Construction Interest (4.2% rate for 14-month construction period Libor 0.2% + Spread 2.5% + Cushion 1.5% = 4.2% Rate)	235,053		235,053	235,053
LEGAL & ACCOUNTING				
Borrower Legal	255,000		255,000	125,000
Investor Due Diligence	50,000		50,000	-
Accounting/Finance Consultant/Reimbursables	135,000		135,000	135,000
OTHER PROJECT COSTS				
Furnishings	150,000		150,000	150,000
Permit Processing Fees	284,203		284,203	284,203
Local Development Impact Fees	139,446		139,446	139,446
Market Study	8,000		8,000	-
Appraisal	8,000		8,000	8,000
Marketing	75,000		75,000	-
Soft Cost Contingency	144,939		144,939	144,939
TCAC Fees	94,360	24,190	118,550	-
Taxes During Construction	16,000		16,000	-
Operating Reserve	-	96,636	96,636	-
DEVELOPER FEE				
Developer Overhead/Profit	350,000	1,650,000	2,000,000	1,400,000
TOTAL PROJECT COSTS	13,053,284	1,770,826	14,824,110	13,402,544
SOURCES				
TAX CREDIT EQUITY	2,373,284	8,826,716	11,200,000	
Percent of Total Equity	21%	79%		
Construction Loan	7,380,000	(7,380,000)		
PUBLIC SOURCES TBD				
City of Menlo Park BMR Fund	TBD	0	TBD	
County of San Mateo HOME/CDBG	TBD	0	TBD	
Subtotal	3,300,000	0	3,300,000	
DEFERRED DEVELOPER FEE		324,110	324,110	
TOTAL SOURCES	13,053,284	1,770,827	14,824,110	

Attachment B – Veterans Affairs Land Appraisal Summary

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DEPARTMENT OF VETERANS AFFAIRS
WASHINGTON DC 20420

MAR 12 2013

Darci Palmer
CORE Affordable Housing
470 S. Market Street
San Jose, CA 95113

Re: Fair Market Value of Enhanced-Use Lease Parcel, Veterans Affairs Palo Alto Health Care System, Menlo Park Division

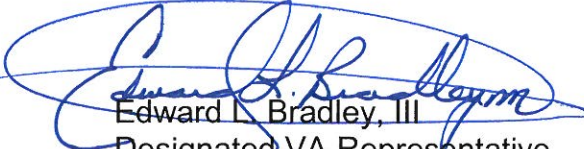
Dear Ms. Palmer,

The purpose of this letter is to provide supporting documentation for Willow Housing Limited Partnership's (Willow, L.P.'s) application for funding via the County of San Mateo Notice of Funding Availability for HOME/CDBG funding.

The Department of Veterans Affairs (VA) is in possession of a Real Estate Market Value Appraisal Report of the subject property – the same property in which Willow, L.P. has a leasehold interest, pursuant to an Enhanced-Use Lease agreement executed between Willow, L.P. and VA on December 27, 2011. The Appraisal Report was prepared for VA by Craig Owyang Real Estate, in conformity with the Uniform Appraisal Standards for Federal Land Acquisitions.

The aforementioned appraisal found that the land value of the subject property, as of February 22, 2012, is **\$13,200,000.00**.

Sincerely,


Edward L. Bradley, III
Designated VA Representative

Attachment C – DEMC Environmental Consultant Scope and Qualifications

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Scope of Work

Preparation of the Draft EA and FONSI/Initial Study and Negative Declaration

DEMC will use the regulations and implementation procedures set forth by the Council on Environmental Quality and the Department of Veterans Affairs (VA), as well as the implementing procedures of the California Environmental Quality Act (CEQA) to develop an internal draft EA/Initial Study (EA/IS). The EA/IS will be written so that the general public can easily understand the potential environmental impacts.

The proposed internal draft EA/IS will evaluate the environmental impacts (both positive and negative) of construction of housing for veterans on VA-owned property. The aspects that will be evaluated include, but are not limited to:

- Purpose and Need of the proposed project
- Description of Alternatives
- Affected Environment
 - Aesthetics
 - Air Quality
 - Community Services
 - Cultural Resources
 - Economic Activity
 - Floodplains and Wetlands
 - Geology and Soils
 - Hydrology and Water Quality
 - Land Use
 - Noise
 - Potential for Generating Substantial Controversy
 - Real Property
 - Solid and Hazardous Waste
 - Transportation and Parking
 - Utilities
 - Vegetation and Wildlife
- Environmental Impacts of Construction and Operation
- Cumulative Impacts
- Regulatory Compliance
- Mitigation measures for any significant effects
- Consistency with existing local jurisdiction plans and policies, and
- Names of parties responsible for preparation

DEMC will prepare the draft Finding of No Significant Impact/Notice of Determination (NoD), upon receipt from direction from the VA/Menlo Park that no further investigation required.

The FONSI/NoD will include the following information:

- Name, locations, and brief description of the project.

- Date of approval.
- The VA's and Menlo Park Commissioners conclusion on whether project as approved will have significant effects on the environment.
- Findings regarding mitigation of significant environmental impacts, any statement of overriding considerations adopted, and any mitigation measures adopted upon which project approval is conditioned.
- Statement that the negative declaration was prepared and certified or adopted pursuant to NEPA and CEQA, and
- Location where the negative declaration and record of project approval are available for review.

Notice of Availability

DEMC will provide a draft Notice of Availability (NoA)/Notice of Determination (NoD) to VA and the Menlo Park Commissioners for review and comment. Upon approval from VA and Menlo Park, DEMC will ensure the publication of the NoA for the draft EA/IS and FONSI/Notice of Determination (NoD) in a daily local newspaper. A legal affidavit will be obtained from the newspaper providing proof of publication and availability. This will begin the 30-day public notice period.

Preparation of the Final EA/IS and FONSI/ND

DEMC will prepare written responses to any public comments received and forward to VA and Menlo Park Commissioners for review and approval. Presuming these comments / responses are minor in nature, DEMC will incorporate comments from the public comment period into the Final EA/IS and FONSI/NoD.

San Mateo County Clerk will be provided the Final EA/IS and FONSI/NoD.

Qualifications

Emily Dyson, CEO of Dyson Environmental Management and Compliance (DEMC) has over 23 years of National Environmental Policy Act and California Environmental Quality Act experience. Ms. Dyson has been responsible for the development of NEPA documents for the Department of Veterans Affairs, the U.S. Marine Corp, the U.S. Postal Service, the U.S. Department of Energy, the U.S. Air Force, and the U.S. National Park Service. Ms. Dyson's CEQA experience was gained while developing combined NEPA/CEQA documentation for the Stanford Linear Accelerator project, the Loma Linda Medical Center, Berkeley National Laboratory and Lawrence Livermore National Laboratory with the Department of Energy.

Mr. Fred Carey, P.E., of Potomac Hudson Engineering (PHE) (DEMC Subcontractor for this project) has experience with combined NEPA/CEQA documentation as well. PHE will provide assistance in the areas of traffic, cultural resources and socio-economic impacts, as well as ensuring that all documentation has adequately addressed the CEQA requirements.

Their resumes are attached.

Emily F. Dyson
CEO/Senior Scientist

Profile

Ms. Dyson has over 23 years of professional experience in environmental management with Federal, State and local governments, as well as with industrial clients. In addition, Ms. Dyson has performed in the “Doer/Seller” role for the past nine years. She has been and continues to be responsible for marketing and sales, as well as providing technical services to a variety of clients.

Education

B.S., 1989, Environmental Sciences. State University of New York, College of Environmental Science and Forestry at Syracuse University.

Training and Certifications

- ASTM Phase I – II Environmental Site Assessments for Commercial Real Estate
- Wetland Training Institute – Wetland Delineator Certificate, 2006
- SHA Yellow Card – Erosion and Sediment Control Certification / 10-321 (current)
- MDE Green Card – Erosion and Sediment Control Certification / 48345 (current)

Experience

Dyson Environmental Management and Compliance, Mt. Airy, Maryland
Chief Executive Officer and Senior Scientist. May 2012 – present

- Responsible for the Entsorga WV/Chemtex International Solid Refuse Fuel facility environmental permitting, project management and coordination.
- Responsible for the development of National Environmental Policy Act (and California Environmental Quality Act, as applicable) Environmental Assessments for the Department of Veteran’s Affairs under the Enhanced Use Lease program. Locations of the proposed actions are Nebraska, Illinois, Virginia, California and Washington.
- Responsible for development an audit program for evaluating Treatment, Storage and Disposal Facilities for the Maryland Department of Transportation Port Administration.

Spectrum Environmental Sciences, Inc., Frederick, Maryland
Manager – General Environmental Programs. February, 2002 – May 2012

- Managed over 200 contracts and tasks ranging in cost between \$1,000 and \$500,000. All projects were managed to within budget and with high client satisfaction.

- Extensive experience with multimedia environmental auditing and facility compliance. Managed and conducted over 100 multimedia environmental audits in the last five years.
- Responsible for all marketing activities associated with State of Maryland contracts and industrial clients for issues concerning waste and water permitting, National Environmental Policy Act documentation development, and multimedia environmental compliance.
- Developed marketing strategies and implemented marketing plans to increase State of Maryland contracts. Increased number and value of State contracts for the company through networking and diligence in pursuing opportunities.
- Managed and was the lead developer for Spectrum Environmental Sciences, Inc. (as a subcontractor to Anchor QEA) for the Maryland Motor Vehicle Administration (MVA) Compliance Focused Environmental Management System. Worked closely with MVA employees and other contractors to conduct a Gap Analysis, develop Environmental Standard Operating Procedures, and develop an overall system that would meet the MVA's needs without being cumbersome.
- Lead auditor and technical expert for multimedia environmental audits for five Maryland Department of Transportation Administrations (MAA, MVA, MDOT Headquarters, MdTA, and MPA). To be the lead auditor, Ms. Dyson had to be approved by the US Environmental Protection Agency – Region 3, as these audits were conducted under a US EPA/MDOT Voluntary Disclosure Agreement.
- Extensive experience with industrial environmental management requirements, reporting, monitoring, and recordkeeping. Provided environmental management support to the explosives industry, cement industry, chemical manufacturing, waste management, and plastics manufacturing.
- Provided on-site environmental management for several clients. The clients requested Ms. Dyson's presence on-site when difficult environmental compliance issues were identified and cost-effective, common sense corrective actions were required. Ms. Dyson was able to develop alternative corrective actions that were cost effective and long-term viable solutions that would meet the regulatory requirements without adding undue costs and regulatory requirements.
- Responsible for obtaining approximately 20 National Pollutant Discharge Elimination System (NPDES) General Industrial Discharge Permits and NPDES Construction Storm Water Permits. This included the development of facility Storm Water Pollution Prevention Plans (SWPPP) and the associated required training. Many of the SWPPP included the development of Environmental Operating Procedures that were later incorporated into an Environmental Management System.

- Responsible for the development of EMSs for industrial and government entities. Many of the EMSs were developed to meet the needs of a client to implement Systems that remain a living system vs. a document that sits on a shelf. These systems were developed to be useful checklists and procedures and could be used daily onsite. Although many of these EMSs are not third party certified, they meet the requirements of an EMS and would meet and/or exceed the expectations of a regulatory agency review.

Roy F. Weston/Weston Solutions, Rockville, Maryland

Senior Environmental Scientist – February 1990 – February 2002

- Provided environmental policy and regulatory review for the US Department of Energy, Office of Science, as well as the Office of Environmental Restoration. These were both five year contracts. Support on these contracts included National Environmental Policy Act documentation, environmental restoration, program management for sites in Oak Ridge, Tennessee, Hanford, Washington, Berkeley National Laboratory, Lawrence Livermore National Laboratory and Stanford National Accelerator in California and Savannah River, Georgia. For projects located in California, Ms. Dyson was responsible for ensuring that all documentation met the requirements of the California Environmental Quality Act (CEQA) and that all proper coordination with local authorities were completed.
- Managed and developed National Environmental Policy Act Environmental Assessments and Environmental Impact Statements for the U.S. Department of Natural Resources National Park Service, U.S. Marine Corp, U.S. Postal Service and the U.S. Department of Energy. Recognized by the Department of Energy for NEPA Excellence in 2000 and 2001.
- Participated in the Weston Customer Service Managers training. This provided an opportunity to learn marketing techniques, project management skills, and corporate costing and accounting. Only six individuals per year were selected, company-wide, for participation in the program.



Fred Carey, P.E.
Principal

EDUCATION

B.S., Civil Engineering, University of Maryland, 1992
M.S., Environmental Engineering, The Johns Hopkins University, 1997

PROFESSIONAL REGISTRATIONS

Professional Engineer, Maryland (License No. 24860)

PROFESSIONAL EXPERIENCE

Mr. Carey is a principal of PHE, with 18 years of experience preparing and managing environmental studies and documents for a variety of projects and actions. He has served in management roles on four power plant specific projects including the FutureGen Environmental Impact Statement (EIS). Each of these projects included detailed analysis of potential impacts to the human and natural environment related to the siting and operation of a power plant. He has continually demonstrated his ability to ensure high-quality analysis while meeting schedule requirements. In his professional career, he has managed over 40 environmental analysis projects across the country.

REPRESENTATIVE PROJECTS

Environmental Impact Statement (EIS) and Environmental Impact Report (EIR) for a Proposed 1.5 Million square-foot Juvenile Justice Campus, Fresno, CA. This project was under an extremely tight schedule in order for Fresno County to maintain eligibility for a state/federal grant award. In addition, the project was subject to both NEPA and CEQA (California's NEPA Equivalent) and needed to satisfy the requirements of both laws. The project included the detailed evaluation of three privately owned alternative sites (approximately 200 acres each). Mr. Carey served key roles in interacting with regulatory agencies (e.g., Caltrans) and managing internal agency reviews including the CA Board of Corrections and U.S. Department of Justice. The project had several difficult technical aspects including the siting of water (up to 300,000 gpd withdrawal) and wastewater treatment systems, completion of detailed Traffic Impact Studies (TIS), severe non-attainment air quality concerns, and proposed controversial land uses. The TIS assessed the need for a new state highway interchange for the preferred site. Critical to this analysis was a comprehensive understanding of the phasing associated with the master plan build-out. Despite the aggressive time frame under which the EIS/EIR was completed, the USEPA commented that they *"found the Draft EIS to be of very high quality."* Mr. Carey was able to quickly mobilize the project team and complete the PDEIS/EIR within six months of award. As the PM, he also served as the principal speaker and moderator for the Public Scoping and Information meetings, and drafted public announcements. He was very successful in cost control, and no change orders or cost adjustments were required or issued on the project. In evidence of this fact, the client made the following statement in a thank you letter: *"Your ability to stay within budget confirms your commitment to provide high quality cost-effective environmental consulting services."*

Environmental Assessment (EA) of the Next-Generation Currency, U.S. Department of the Treasury, Bureau of Engraving and Printing (BEP), Washington DC and Fort Worth, Texas. Project Manager for a Programmatic EA for implementing the Next Generation of currency. The EA included the evaluation of the addition of offset printing processes to existing intaglio printing BEP



facilities in order to add color to U.S. Currency. In addition, the introduction of new security features, such as nylon threads to the currency paper were evaluated.

U.S. DOJ, Nation-wide NEPA Support. Mr. Carey prepared EAs and EISs in support of planning for maximum security prisons being constructed across the U.S., including sites in AL, CA, KS, GA, NM, PR, RI, and WV. For the facility in California, a combined NEPA/CEQA document was prepared to address the regulations and requirements of both agencies. Key issues on these actions included lighting, noise, traffic, cultural resources, biological resources, utilities, and public objections. Under this contract he consistently met project deadlines and budgets. A thank you letter from one of the clients for this contract stated, “...we perceived the NEPA process as another daunting step. However, your exceptional skill and professionalism streamlined the process to such a degree as to effectively remove the responsibility from our hands...”

FutureGen Project Environmental Impact Statement (EIS). Principal-in-Charge for the FutureGen Project EIS; a \$1 billion, 10-year demonstration project for the world’s first coal-based, near-zero-emissions electricity and hydrogen power plant. The EIS presented the analysis and evaluation of the potential environmental impacts resulting from constructing and operating the power plant to achieve near-zero-emissions by sequestering CO₂ underground in deep geological reservoirs. Two sites in Illinois and two sites in Texas were evaluated in the EIS. This very aggressive one-year EIS is in response to the FutureGen Initiative announced by President Bush in February 2003.

Yucca Mountain Rail Line Environmental Impact Statement (EIS). Mr. Carey provided principal oversight for the Department of Energy’s (DOE’s) Office of Civilian Radioactive Waste Management (OCRWM) on the preparation of an EIS and supporting studies for a high-profile and controversial Federal waste transportation proposal. DOE’s proposed action included the construction and operation of a railroad in the State of Nevada, used to transport high level radioactive waste and spent nuclear fuel (from generator sites nationwide) between existing rail connection points and a proposed geologic repository at Yucca Mountain, Nevada. The EIS is also intended to provide the Bureau of Land Management with a basis to determine whether to transfer jurisdiction and use of the lands to DOE for purposes of constructing and operating the railroad, and to provide the Surface Transportation Board with a basis to determine whether to grant common-carrier status to the railroad. Key issues included public health and safety, nuclear waste management, rail transportation, grazing and public land access, cultural resources, Native American consultation, and water resources.

U.S. Department of Energy (DOE) Electric Drive Vehicle Battery and Component Manufacturing Initiative Project, EAs, Nationwide, 2009 – Present. Mr. Carey served as the lead for conducting site visits and evaluating industrial processes for four EAs related to advanced battery manufacturing projects. PHE is preparing eight EAs for DOE to assess the potential for environmental impacts resulting from DOE’s participation in a cooperative agreement with proponents to manufacture batteries for use in electric drive vehicles (EDVs). DOE intends to accelerate the development and production of various EDV systems by increasing domestic manufacturing capacity for advanced automotive batteries, their components, recycling facilities, and EDV components that will enable market introduction of various electric vehicle technologies by lowering the cost of battery packs, batteries, and electric propulsion systems for EDVs through high-volume manufacturing. DOE would provide approximately 50 to 75 percent of the funding for approved projects to construct, renovate, or upgrade operations to support the production of batteries for use in EDVs. The projects would meet the objectives of the American Recovery and Reinvestment Act of 2009, by creating and preserving jobs. PHE evaluated documents provided by each site proponent, reviewed existing environmental permits, conducted site visits to



ascertain existing conditions, conducted online database reviews, and evaluated potential impacts on the natural, physical, cultural, and human environment. PHE prepared an EA for each project and coordinated the publication of each EA in local newspapers for public comment. PHE also distributed the EAs to interested parties, including the EPA, USFWS, SHPO, state agencies, and state and local representatives.

Site-Wide EIS for the Nevada Test Site and Offsite Locations, Nevada. Mr. Carey provided principal oversight for the preparation of a new Site-Wide Environmental Impact Statement (SWEIS) addressing the Nevada Test Site (NTS) and other National Nuclear Security Administration (NNSA) facilities and operations in Nevada. The NTS SWEIS will support NNSA decisions regarding the continued operation of NTS and associated offsite Nevada activities for the next 10 years.

Algenol Biofuels Inc. Proposed Integrated Biorefinery for Producing Ethanol from Hybrid Algae, Freeport, Texas EA (2010-present). Principal-in-Charge of an EA for a pilot-scale integrated biorefinery that would produce ethanol directly from carbon dioxide and seawater using hybrid algae. DOE is proposing to provide federal funding under the *American Recovery and Reinvestment Act of 2009* to Algenol Biofuels Inc. to support the construction and operation of the biorefinery, which would be located on Dow Chemical Company property in Freeport, Texas. The proposed project site would comprise approximately 17 acres of undeveloped land. A proposed location in Fort Myers, Florida is also being considered and is analyzed in the EA as a back-up site option. The purpose of the project is to refine systems, equipment, and processes to maximize ethanol production with minimal costs to ensure the economic and technical viability of commercialization.

Environmental Impact Statement (EIS) for a 150-MW, Next-Generation CFB Unit, Colorado Springs, Colorado. Project Manager for EIS for the construction and operation of a 150-MW, circulating fluidized bed (CFB), coal-fired power plant in Fountain, Colorado. Key areas of evaluation for this EIS include air quality impacts, as well as secondary impacts related to the storage of CFB fuels, which include forest biomass, tire-derived fuels, and municipal wastewater sludge. Has worked closely with the Colorado Spring Utilities and the NETL NEPA Compliance Officer to ensure that this project is completed on schedule.

RESOLUTION NO.

RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO PARK AUTHORIZING A PRELIMINARY CONDITIONAL COMMITMENT OF \$2.5 MILLION FROM THE BELOW MARKET RATE FUND FOR THE CORE AFFORDABLE HOUSING DEVELOPMENT AT THE VETERAN'S ADMINISTRATION FACILITY IN MENLO PARK

WHEREAS, the Below Market Rate (BMR) Housing Fund is comprised of commercial development in-lieu fees and has a balance of approximately \$6.3 Million as of March 30, 2013; and

WHEREAS, the primary purpose of the BMR Housing Program is to increase the supply and assist in the development of housing that is affordable to very low-, low- and moderate-income households; and

WHEREAS, in addition to the approved uses listed in the Guidelines, City Council approved additional uses on April 26, 2005; and

WHEREAS, the CORE project is envisioned as a 60-unit permanent multifamily housing development on a 2.011 acre site located near Willow Road and South Perimeter Road in Menlo Park (described as the 700 block of Willow Road); and

WHEREAS, the proposal from CORE is consistent with the City's BMR policy and guidelines as the current Development Plan includes income restrictions for 59 out of 60 units; and

WHEREAS, all 59 BMR units would count towards the Regional Housing Needs Assessment (RHNA) and would represent roughly 25% progress of the Very Low Income allotment.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Menlo Park that the City Council does hereby authorize the preliminary, conditional allocation of \$2.5 million from the Below Market Rate Housing Fund to support CORE Affordable Housing with the development of a 60-unit 100% affordable development located at the intersection of Willow Road and South Perimeter Road (700 block Willow Road) at the Veteran's Administration facility in Menlo Park.

I, Margaret S. Roberts, City Clerk of Menlo Park, do hereby certify that the above and foregoing Council Resolution was duly and regularly passed and adopted at a meeting by said Council on the seventh day of May, 2013, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this seventh day of May, 2013.

Margaret S. Roberts, MMC
City Clerk



ADMINISTRATIVE SERVICES DEPARTMENT

Council Meeting Date: May 7, 2013
Staff Report #: 13-080

Agenda Item #: F-3

REGULAR BUSINESS: Council discussion and possible recommendation on various seats for determination at the next City Selection Committee meeting scheduled for May 17, 2013

RECOMMENDATION

Staff recommends City Council discuss the applicants to provide guidance to the Mayor on the various seats that will be selected at the next City Selection Committee meeting scheduled for May 17, 2013.

BACKGROUND

The Local Agency Formation Commission (LAFCo) appointment will be for a full four year term through the first Monday in May 2017.

Housing Endowment and Regional Trust (HEART) has one seat to fulfill an unexpired term ending February 28, 2015.

The deadline to submit letters of interest is May 9, which is after the May 7th Council meeting, but before the following City Council meeting of May 21, 2013. Included as Attachment A is the letter received to date. Any additional letters received will be provided to the Council at the meeting of May 7, 2013.

The City Selection Committee meeting will take place on May 17 2013. According to the bylaws for Council of Cities, the Mayor is the voting member for each city.

This item is on the agenda for the Council to provide input to inform the Mayor's voting at the May 17 City Selection Committee meeting.

IMPACT ON CITY RESOURCES

There is no cost associated with this item.

POLICY ISSUES

The proposed action is consistent with existing policy and Council's direction to staff.

ENVIRONMENTAL REVIEW

There is no environmental review required for this item.

PUBLIC NOTICE

Public Notification was achieved by posting the agenda, with this agenda item being listed, at least 72 hours prior to the meeting.

ATTACHMENTS

- A. Letters of interest

Report prepared by:
Margaret S. Roberts, MMC
City Clerk



PEDRO GONZALEZ, MAYOR
KARYL MATSUMOTO, MAYOR PRO TEM
MARK ADDIEGO, COUNCILMEMBER
RICHARD A. GARBARINO, COUNCILMEMBER
PRADEEP GUPTA, PH.D, COUNCILMEMBER

BARRY M. NAGEL, CITY MANAGER

OFFICE OF THE CITY COUNCIL

April 8, 2013

Dear Mayor and Council Members,

I am applying for the position of City representative on the LAFCo Commission created by the resignation of the current representative, Naomi Patridge. I have served as the alternate for Sepi Richardson and presently serve as the alternate for both Naomi Patridge and Nadia Holober. I have filled in for both Sepi and Naomi when they were unable to attend the meetings. In addition, I voluntarily attend meetings and review agendas to keep abreast of the issues addressed by LAFCo and to prepare myself should the need arise to serve in the absence of the full time Commission member.

The issues that LAFCo addresses have an impact on each of our Cities. The decisions rendered by the Commission impacts land use policies throughout San Mateo County. My service as a member of the ABAG Executive Board, Regional Airport Planning Committee and the Airport Land Use Committee as well as the former South San Francisco Redevelopment Agency and the present Successor Agency, have given me the experience in land use issues that I believe make me well qualified to represent you as a LAFCo Commission member.

You have placed your confidence and trust in me in the past. I believe that I have earned that respect and trust by serving you effectively and efficiently as a member of various boards and commissions. I am asking for your continued support and vote to appoint me as your representative to LAFCo.

Thank you for your consideration of this request.

Respectfully submitted,

A handwritten signature in blue ink that reads "Richard Garbarino".

Richard Garbarino, Council Member
City of South San Francisco

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