



REGULAR MEETING AGENDA

Date: 7/25/2016
Time: 7:00 p.m.
City Council Chambers
701 Laurel St., Menlo Park, CA 94025

A. Call To Order

B. Roll Call

C. Reports and Announcements

Under “Reports and Announcements,” staff and Commission members may communicate general information of interest regarding matters within the jurisdiction of the Commission. No Commission discussion or action can occur on any of the presented items.

D. Public Comment

Under “Public Comment,” the public may address the Commission on any subject not listed on the agenda, and items listed under Consent Calendar. Each speaker may address the Commission once under Public Comment for a limit of three minutes. Please clearly state your name and address or political jurisdiction in which you live. The Commission cannot act on items not listed on the agenda and, therefore, the Commission cannot respond to non-agenda issues brought up under Public Comment other than to provide general information.

E. Consent Calendar

- E1. Approval of minutes from the June 20, 2016 Planning Commission meeting. ([Attachment](#))

F. Public Hearing

- F1. Variance/Lori Hsu/207 Lexington Drive:
Request for a variance for a rear addition to an existing nonconforming single-story residence in the rear yard setback on a lot in the R-1-U (Single-Family Urban) zoning district. The addition would consist of filling in an existing covered porch, with the new wall located approximately 18.4 feet from the rear property line, where 20 feet is required. ([Staff Report #16-058-PC](#))
- F2. Use Permit and Variance/Marshall Schneider/208 Oakhurst Pl: Request for a use permit to remodel and add a second story to an existing nonconforming single story, single-family residence in the R-1-U (Single Family Urban) zoning district. The proposed work would exceed 50 percent of the existing replacement value in a 12-month period. The proposal includes a request for a variance to allow a new covered entry with a corner side setback of approximately nine feet, three inches, where 12 feet is required. ([Staff Report #16-059-PC](#))

- F3. Use Permit/Brendan and Carmen Visser/1177 Middle Avenue:
Request for a use permit to demolish an existing single-family, single story residence and construct a new two-story, single-family residence on a substandard lot with regard to lot width in the R-1-S (Single-Family Suburban) zoning district. As part of the project, five heritage trees are proposed for removal: two Canary Island date palms, a coast live oak, and two coastal redwoods. ([Staff Report #16-060-PC](#))
- F4. Use Permit/City of Menlo Park/333 Burgess Drive:
Request for a use permit for a diesel emergency generator at the City's Corporation Yard, associated with an emergency well (as a back-up source of potable and firefighting water supply). This property is located in the PF (Public Facilities) Zoning District. ([Staff Report #16-061-PC](#))
- F5. Use Permit/Calysta Energy/1140 O'Brien Dr., Suite B:
Request for a revision to a use permit, previously approved in July 2014, to modify the types, quantities, and locations of hazardous materials used and stored at the site. The subject property is located in the M-2 (General Industrial) zoning district and the hazardous materials are used for the research and development of sustainable fuels and chemicals. All hazardous materials would be used and stored within the existing building. ([Staff Report #16-062-PC](#))
- F6. Use Permit and Architectural Control/Facebook, Inc./980 Hamilton Avenue:
Request for a use permit and architectural control for the conversion of an existing warehouse building with nonconforming parking into a food services use, including a kitchen and dining room, that is intended to serve employees associated with a nearby multi-building office use. The proposal also includes exterior changes to the building entry. The site is nonconforming with regard to parking, and the kitchen would serve employees located in nearby buildings. The existing building is located in the M-2 (General Industrial) zoning district. ([Staff Report #16-063-PC](#))
- F7. Use Permit and Architectural Control/DES Architects & Engineers/1430 O'Brien Drive:
Request for a use permit and architectural control to partially convert, expand, and architecturally update an existing research and development (R&D) building to create a new cafe and fitness and health center, additional R&D spaces, and provide new landscaping to the subject property which is located in the M-2 (General Industrial) zoning district. As part of the project, the applicant is requesting a parking reduction based on the uses within the building and the proposed tenants' operations. Approximately 199 parking spaces would be provided, where 282 parking spaces are required by the M-2 square-footage-based parking requirements. The project includes a Below Market Rate (BMR) Housing Agreement for the payment of an in-lieu fee or the delivery of equivalent off-site units. ([Staff Report #16-064-PC](#))

G. Regular Business

- G1. General Plan Amendment, Zoning Ordinance Amendment, Rezoning, Environmental Review/City of Menlo Park: Review and comment on the Draft Fiscal Impact Analysis (FIA) prepared for the General Plan and M-2 Area Zoning Update. No action on the FIA or project will occur at the meeting. The objective of any FIA is the projection of changes in public revenues and costs associated with development of a project, and is an informational tool. *[Item continued to a future meeting.](#)*

H Informational Items

H1. Future Planning Commission Meeting Schedule – The upcoming Planning Commission meetings are listed here, for reference. No action will be taken on the meeting schedule, although individual Commissioners may notify staff of planned absences.

- Regular Meeting: August 15, 2016
- Regular Meeting: August 29, 2016
- Regular Meeting: September 12, 2016

I. Adjournment

Agendas are posted in accordance with Government Code Section 54954.2(a) or Section 54956. Members of the public can view electronic agendas and staff reports by accessing the City website at www.menlopark.org and can receive e-mail notification of agenda and staff report postings by subscribing to the "Notify Me" service at menlopark.org/notifyme. Agendas and staff reports may also be obtained by contacting the Planning Division at (650) 330-6702. (Posted: 7/20/16)

At every Regular Meeting of the Commission, in addition to the Public Comment period where the public shall have the right to address the Commission on any matters of public interest not listed on the agenda, members of the public have the right to directly address the Commission on any item listed on the agenda at a time designated by the Chair, either before or during the Commission's consideration of the item.

At every Special Meeting of the Commission, members of the public have the right to directly address the Commission on any item listed on the agenda at a time designated by the Chair, either before or during consideration of the item.

Any writing that is distributed to a majority of the Commission by any person in connection with an agenda item is a public record (subject to any exemption under the Public Records Act) and is available for inspection at the City Clerk's Office, 701 Laurel St., Menlo Park, CA 94025 during regular business hours.

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REGULAR MEETING MINUTES - DRAFT

Date: 6/20/2016

Time: 7:00 p.m.

City Council Chambers

701 Laurel St., Menlo Park, CA 94025

A. Call To Order

Acting Chair Henry Riggs called the meeting to order at 7:00 p.m.

B. Roll Call

Present: Andrew Combs (Vice Chair), Susan Goodhue, Larry Kahle, John Onken, Henry Riggs (Acting Chair)

Absent: Katherine Strehl, (Chair)

Staff: Deanna Chow, Principal Planner; Kyle Perata, Senior Planner

C. Reports and Announcements

Principal Planner Chow reported that the June 21 City Council meeting agenda included the budget, an update on the Facebook campus and upcoming development agreement process, and review and approval of a tree preservation access easement for the 1020 Alma Street project, which was a condition of the use permit approval by the Planning Commission.

D. Public Comment

- Patti Fry expressed her concern about the number of significant issues on the agenda. She said the volume of materials needing review for these items tonight was unfair to the Commission, the public and the issues. She said in particular the General Plan Update would have impacts and those needed to be carefully reviewed. She suggested scheduling additional meetings to consider the General Plan Update to keep it on schedule.
- Pam Jones wanted to know how much information the Commission receives from the City/County Association of Governments, which is the Congestion Management Agency for San Mateo County. She said the City had multiple EIRs on a variety of development projects but nothing comprehensive so it was known what the real impact of the two agenda items was.

E. Consent Calendar

- E1. Approval of minutes from the May 23, 2016 Planning Commission meeting. ([Attachment](#))

ACTION: Motion and second (Kahle/Goodhue) to approve the minutes with the following edits; passes 5-1-1 with Commissioner Combs abstaining and Chair Strehl absent.

- Page 1, E1, under Action: Replace the motion from “5-0” to “4-1-2
- Page 2, F1, under Action:, Replace “7-0” with “6-0-1”
- Page 4, 1st line: Replace “Commission Combs” with “Commissioner Combs”
- Page 7, 2nd bullet, 1st line: Replace “Menlo Sparks” with “MenloSpark”
- Page 7, 2nd bullet, 5th line: Replace “and” with “on”
- Page 8, 8th line from bottom of page: Replace “amenitias” with “amenities”
- Page 9, 1st bullet, 6th line: Remove extra period after the word “for”
- Page 10, 1st bullet, 15th and 18th line: Replace “M=2” with “M-2”
- Page 10, 1st bullet, 25th line: Replace “presidents” with “residents”
- Page 10, last bullet, 3rd line from bottom: Replace “city” with “City”
- Page 11, 1st bullet, 2nd to last sentence: Replace “AS state standards change applicable to base standards. She suggested that they not burden the plan or individual projects with elements that could not be achieved.” with “AS state standards change applicable to base standards, she suggested that they not burden the General Plan or individual projects with elements that could not be achieved.”
- Page 20, 1st line: Replace “He said not that would be exclusive but staff and consultant team would support those ideas.” with “He said that would not be exclusive but staff and consultant team would support those ideas.”
- Page 20, 5th paragraph, 4th line: Replace “bay” with “Bay”

Commissioner Barnes noted page 13 and the line “Commissioner Barnes might have a conflict as it relates to Lorelei Manor.” He said he did not.

Principal Planner Chow said Commissioners Riggs and Kahle live in close proximity to the M2 area. She said the General Plan affects everyone in the City. She said the City believes that Commissioner Riggs and Kahle are able to participate and only Commissioners Combs and Onken who would need to recuse themselves due to the potential for financial gain from rezoning the M2.

F. Public Hearing

- F1. Draft Environmental Impact Report (EIR) Public Hearing/Hibiscus Properties, LLC on behalf of Facebook/Facebook Campus Expansion Project (300-309 Constitution Drive): Public hearing to receive public comments on the Draft EIR for the Facebook Campus Expansion Project. The Draft EIR prepared for the project identifies less than significant effects in the following categories: Land Use, Geology and Soils, Population and Housing, Public Services, and Utilities and Service Systems. The Draft EIR identifies potentially significant environmental effects that can be mitigated to a less than significant level in the following categories: Aesthetics, Air Quality, Noise, Cultural Resources, Biological Resources, Hydrology and Water Quality, and Hazards and Hazardous Materials. The Draft EIR identifies potentially significant environmental effects that are significant and unavoidable in the following categories: Transportation and Greenhouse Gas Emissions. The California Environmental Quality Act (CEQA) requires this notice to disclose whether any listed hazardous waste sites are present at the location. The project location does not contain a hazardous waste site included in a list prepared under Section 65962.5 of the Government Code. The Hazardous Materials section of the Draft EIR discusses this topic in more detail. Written comments may also be submitted to the Community Development Department (701 Laurel Street, Menlo Park) no later than 5:30 p.m., Monday, July 11, 2016. ([Staff Report #16-049-PC](#))

Transcript was prepared for this item.

G. Study Session

- G1. Study Session on Zoning Ordinance Text Amendment, Rezoning, Conditional Development Permit, Development Agreement, Lot Reconfiguration, Heritage Tree Removal Permits, and Below Market Rate Housing Agreement /Hibiscus Properties, LLC on behalf of Facebook, Inc./300-309 Constitution Drive. Study session to receive comments on the Facebook Campus Expansion Project to redevelop the approximately 58 acre site with approximately 962,400 square feet of offices in two new buildings and a 200 room hotel of approximately 174,800 square feet. Including the existing Building 23 (approximately 180,108 square feet), the maximum gross floor area for offices would be approximately 1.143 million square feet, which is within maximum 45 percent floor area ratio (FAR) for offices. With the hotel, the maximum gross floor area would be approximately 1.318 million square feet, or 52 percent FAR, which is consistent with the FAR maximum of up to 55 percent for all other uses. The proposal includes a conditional development permit to allow maximum building heights of up to 75 feet and allow building coverage to potentially exceed 50 percent of the site, as well as to define all other development standards. The CDP would also include the existing Building 20 (1 Facebook Way). The project includes a request to remove approximately 274 heritage trees. In addition, the project proposal includes a development agreement and below market rate (BMR) housing agreement. ([Staff Report #16-049-PC](#))

Staff Comment: Senior Planner Perata said the project applicant would make a presentation.

Applicant Presentation: Mr. John Tenanes, Real Estate Manager, Facebook, presented a brief history of Facebook's move to Menlo Park and the properties acquired. He said they would clean up the soil contamination left by the previous tenant at the TE (Raychem) site. He said they would build a 200-room hotel and connect Belle Haven to the Bay. He said they were prepared to add 120,000 square feet of new office space. He said managing traffic congestion was very important to Facebook and their Traffic Demand Management (TDM) program would be used to get more people out of their cars. He said they would partner with SamTrans and Caltrain to look at new transportation modes. He said they would make improvements at Willow Road and Hamilton Avenue and modify the trip cap to reduce traffic during peak hours. He said the buildings would be LEED Gold or better. He said they would continue to use bird safe glazing for the buildings and introduce a water recycle program that would be the first of its kind in Menlo Park. He said they were committed to adding community amenities such as a grocery store and pharmacy. He said they were hosting a Farmer's Market again.

Mr. Craig Webb, Gehry Partners, provided visuals of the current site and buildings and the prospective build out. He said this was a very large project and they wanted building diversity that was incorporated with the landscape. He said parking would be underground to allow for more landscaping. He said the design would include passage from Belle Haven to the Bay with a multi-use bridge across expressway and over to the park on the west side. He noted the potential too to create a bicycle and pedestrian path along the Dumbarton rail corridor. He said the central area of the community accessible park would be a paved area available for community events and the lawn area would be available for passive recreation. He said a portion of the passageway under the bridge would be a wetlands area for treatment of storm water. He said they were thinking about relocating the Farmer's Market to this side; he said other ideas included food trucks for a community food festival and potentially nighttime movies.

Mr. Webb noted Facebook has participated in community initiatives such as the Chilco Street improvements, bicycle and pedestrian paths, potential connection to the Rail Trail, should that happen, Bay Trail improvements, wetland restoration, and utilizing the Dumbarton rail corridor to accommodate different modes of travel such as rail trail for bicyclists and pedestrians, heavy rail lines, and then another lane which might be a rapid bus transit.

Mr. Webb talked about Building 20 that was completed the previous year. He said all the buildings in that area were intended as great engineering space. He noted the extensive landscaping to the building on the ground and roof of the building. He said the first goal for Building 21 would be to create a different architecture and create diversity of form. He said the intent was to break the scale down to elements that related more to the houses in Belle Haven. He said Building 21 would have a different façade that would be large facing the expressway and breaking the scale down on the south side with a larger landscape buffer. He said Building 20 was white and they were looking at more color for accents on Building 21, which would be shades of gray and white. He said the rooftop of Building 20 was a fully landscaped simulation of nature. He said they learned that a strong wind from the Bay creates issues with the usability of the roof. He said the roof of Building 21 would have more architectural pieces to work to block the wind from the Bay. He said they were bringing cafeteria and conference room uses to this roof, and were dropping garden use to the main floor level. He said the goal for the landscaping on the roof and ground was to extend from the east end to the west end. He said large Redwoods would be planted in the courtyard of Building 21, and would create a phenomenal space.

Mr. Webb said Facebook has asked his firm to move toward net zero with the project design and said that was very challenging. He said Building 21 would have extensive photovoltaic panels on the roof and potentially over the parking. He said the ambition for Building 22 was even greater and the intent was to cover the majority of the roof with photovoltaic panels, and then to try to double those on the next building. He said they were also looking at how to bring more natural light into the building which reduces the amount of lighting consumed and also creates a more humane workspace. He said they were looking at water recycling strategy and Buildings 21 and 22 would share this water recycling system. He said they have discussed strategies to reduce single-occupancy car use trips to the site including the use of buses externally and internally, and bicycles.

Public Comment:

- Amy Wright, Life Moves, said her organization was dedicated to helping homeless families and individuals return to stable housing and long-term self-sufficiency. She said they were formerly known as Shelter Network and InnVision Way Home and were in 17 locations from Daly City to San Jose. She expressed support for Facebook's expansion noting Facebook has been and continues to be excellent community partners to organizations like Life Moves in providing volunteers for Haven House, donating technology, and introducing youth to the Facebook campus.
- Paul Coates said he was a long-time resident of the Redwood City and Menlo Park area. He said he was in favor of the Facebook project and was representing JobTrain, nonprofit providing job training to people in need.
- Annelinda Aguayo, Development Director at Rebuilding Together Peninsula (RTP,) said her organization over the past year has partnered with Facebook in powerful ways including a Facebook-related contractor who helped rehabilitate a home in five weeks with labor and materials donated in-kind by the contractor effectively a \$300,000 project. She said Facebook recently contacted them to help on local projects and ultimately offered all the costs of the

repairs for 17 homes.

- Nora Sobolov said she was the Director of JobTrain, which she described as a one-stop career service center in Menlo Park serving the Bay area. She said Facebook has been a great community partner and was helping them achieve their mission. She said this project meant more jobs for people in the community.
- Glen Rojas said he was a Menlo Park resident and former City manager. He said Facebook's commitment to the Menlo Park community was an important part of their process and noted as a Menlo Park Rotarian his firsthand experience of Facebook's commitment to the community. He said Facebook has taken a sensible approach to its growth in Menlo Park including sustainable buildings, significant green space, and the respect for the neighborhood in which they are located.
- Renu Nanda, Executive Director, Ravenswood Education Foundation (REF), said they were a nonprofit whose mission was to insure that Ravenswood schools have equitable and high quality opportunities for students by building on local strengths and engaging the entire community. She said Facebook since moving to Menlo Park is now their largest corporate supporter. She said they supported the project.
- Lucia Sota. Called by the Chair but did not speak. Senior Planner Perata said he believed she had left the meeting.
- Fran Dehn, Menlo Park Chamber of Commerce, said Facebook was a significant social and economic influence in the community. She said Facebook has met all of its contractual obligations including the terms of development agreements. She said Facebook's contributions to the community continue to accrue in terms of investment in schools, charities, a police substation, a community safety officer, Chilco Street improvements, bike paths, Bay trail and bay trail restoration, community garden, and Farmer's Market. She said Facebook has pioneered the path toward Council desired public/private partnerships including the \$1,000,000 contribution that funded the Dumbarton Rail Corridor study, which was a regional public agency obligation that was being accelerated by Facebook's funding to deliver benefits to Menlo Park. She said partners share in risks and rewards and asked the Commission to encourage this project opportunity.
- Lily Gray, Mid-pen Housing, said they were a nonprofit developer, owner and manager of affordable housing. She said earlier in the evening the Commission and public had heard people's concerns with affordable housing in the community. She said they appreciate Facebook's efforts to engage in housing issues and Mid-pen was proactively engaged in housing solutions. She said Facebook was a true community partner and they looked forward to collaborating with them on housing. She said the project would generate significant below market rate (BMR) housing in-lieu fees that were generated by the demand of the expansion project. She said the BMR fees Mid-pen had received from Menlo Park for their 90-unit Sequoia Belle Haven, a very low income senior housing development currently under construction, was critical to leveraging funding and moving forward on the project. She said it was important the BMR funds be used expediently to both create new housing opportunities and to prevent displacement through preservation.
- Maya Perkins, Belle Haven resident, said a number of people had spoken very eloquently about the wonderful things Facebook has done in the community, and noted she agreed and also was very grateful. She said she believed the negotiations were asking for a 15% requirement for BMR in-lieu fees and suggested it be significantly higher, closer to 40%. She said she would like the hotel and amenities to be open to all residents. She said with the new jobs she would like a first-source agreement to hire community members. She asked Facebook and other large employers to work with the City to support hiring Belle Haven, Menlo Park, and East Palo Alto residents into jobs. She said that would help with traffic and to have affordable housing in the community, and for long-time residents of Belle Haven to have jobs at

Facebook.

- Patti Fry said the financial Impact Analysis (FIA) seemed to show that the bulk of the financial benefit would come from the hotel but the hotel did not seem a definite part of the project. She said it would be helpful to understand what the plans really were for a hotel. She said there were estimates about the per employee spending based on some international shopping center, trends and other things that were questionable with people biking and riding shuttles to work. She said the benefits Facebook has provided to the community have been amply described and they were a wonderful community partner. She said the problems were with traffic and housing, and the disparity between supply and demand. She said she hoped Menlo Park could do its share of equalizing those rather than exacerbating the problem. She said this was a wonderful opportunity for community organizations and a partner like Facebook to tackle some of these issues.

Acting Chair Riggs closed the public comment period.

Commission Questions: Commissioner Barnes asked if the hotel was built whether it would be open to the public and if there would be different rates for Facebook employees and others. Mr. Tenanes said they had not yet determined the rates and the plan was for it to be open to the public. Replying to further questions from Commissioner Barnes, he said the hotel would not be managed by Facebook. He said at this point there had been no determination whether the hotel would sit on its own site, or not. Commissioner Barnes said the plans indicated the hotel would sit on its own, separate parcel. Mr. Tenanes said the lot line between Building 20 and Building 21 would need to be removed so the buildings could be attached. He said it was possible that the severed parcel could be relocated to the hotel or it all could be merged as one parcel. Commissioner Barnes asked if the applicant had a preference for a merger or lot line adjustment. Senior Planner Perata said that would be the applicant's choice and that would be covered by the Conditional Development Permit.

Commissioner Onken said in the approval of the preceding Facebook project there had been discussion about it revitalizing local shops and activities happening up and down Willow Road. He said the inference, to use an example, was that people would drive from Facebook to Backyard Barbecue for lunch and then drive back to campus. He said it seemed with the trip cap that would never happen as it would jeopardize the TDM program agreement. Mr. Tenanes said people were leaving the campus but on bicycle and he knew they were going to Starbucks and Jack-in-the-Box. Commissioner Onken asked if he saw the trip cap as actively prohibiting people from leaving the campus and trying to use local facilities. Mr. Tenanes said he thought it did not as people walked and took bicycles to places.

Commissioner Goodhue asked why the hotel was heavily parked as she thought hotel users would be visitors and vendors to Facebook, and would take a shuttle or Uber from the airport. Senior Planner Perata asked if the Commissioner was referring to the City's requirement for hotel parking, parking adjustment guidelines, or the 245 spaces proposed by Facebook. Commissioner Goodhue said it was the applicant's proposal. Senior Planner Perata said the applicant's ratio was one space per hotel room and one space per hotel employee and that was slightly increased over the City's recommended parking.

Fergus O'Shea, Facebook, said they would take a closer look at the parking. He said most people who came to the campus use Uber and they were looking at running a shuttle to the airport on different days. He said there might be need for more parking on the weekends but they would look at it.

Commissioner Goodhue said the parking by building 23 looked like the largest section of open air parking and asked about it has the architect had commented that parking for this project would be completely from the typical ground level parking used by every other Silicon Valley firm. She asked if there would be any photovoltaic panels there. Mr. O'Shea said there would not be.

Commissioner Kahle asked what predominant material would be seen from both the expressway façade and the Belle Haven façade. Mr. Webb said they were looking at using a metal panel system for Building 21 for both the speed of construction and a standing seam detail on the façade. He said it was a material that color could be placed upon to provide texture to the façade, noted the standing seam detail was watertight. Commissioner Kahle asked if the larger mass facing Bayfront was the same material. Mr. Webb said it was. Commissioner Kahle said it looked like a blank wall and asked if plantings would soften it or if some other type of detail would be used. Mr. Webb said the façade was the function of the big room inside and there would be considerable landscaping between that facade and the expressway. He said the scale of the façade facing the expressway was intentionally much bigger than the façade on the community side. Commissioner Kahle asked if they intended to express the large façade more and whether the mature landscaping would eventually screen more. Mr. Webb answered in the affirmative. Commissioner Kahle confirmed with the architect that the wall was 60-feet high. He asked if there was any discussion about undergrounding the power lines in the area. Mr. Webb said that was a discussion four years ago with the Building 20 project. He said it was an extremely expensive proposition to underground the power lines as they were very high voltage. He said they also discussed that those types of towers were evocative of the industrial history of the site. He said he had spoken with a Fire Chief earlier who was not in favor of undergrounding the power lines because of safety for emergency access.

Commissioner Kahle asked Chris Guillard, CMG Landscape Architecture, about the 274 heritage trees being removed, noting some were in the center but many were around the perimeter where there would not no building. Mr. Guillard said many of the trees along the edge of the site were in very poor condition and coupled with the adjacent grading and other work during construction would make it very difficult to preserve those trees. He said as an example with Building 20 when they came to the Commission they recommended removal of all of the trees as part of the EIR. He said they then worked to save a good number of them on that edge and were successful. He said their strategy here as recommended in the EIR was to remove them and replace them in very high quantities. He said they expected to add around 928 trees just at the site level between Building 21 and Building 22. Commissioner Kahle said there were a number of healthy heritage trees on the perimeter and asked why those would be removed. Mr. Guillard said many of those trees would be impacted by adjacent grading for either new infrastructure or the parking.

Acting Chair Riggs asked how many shade trees would be planted as opposed to more decorative trees. Mr. Guillard said under the current plans about 90 to 95% of the trees were envisioned as large shade trees and there would be very few ornamental or under-storied trees in the plans. He said most of the species used were Coast live oak and other native oaks.

Commissioner Onken asked about the public space in the middle of the building and how many people were within 500 yards walking distance of it. He said he knew it was for the Belle Haven

neighborhood but didn't have the same density as other similarly designed spaces they had done. Mr. Guillard said it was envisioned as a place for Facebook and community members to get together. He said it was looked at in terms of both the employee population that would be onsite as well as an opportunity for programming both by community members and Facebook. He said the space's biggest value was its connectivity in providing a safe connection from Belle Haven to the Bay Trail, to Bayfront Park, and access to open space regionally. Commissioner Onken said the Farmer's Market was clearly accessible by car now but it wasn't clear how it would be accessed from Belle Haven. Mr. Guillard said the details for the Farmer's Market program were not developed fully yet but the thought was it would be advertised for this location and adequate parking could be provided in certain visitor parking lots. Commissioner Onken asked whether the large patch of parking asphalt that Commissioner Goodhue commented upon was where people might park. Mr. Guillard said it was one of the visitor lots.

Commissioner Barnes asked when the zoning changed to M2-X on this site and concurrently the General Plan update was enacted designating three different zones, whether the M2-X zoning would continue alongside the three other zones for perpetuity. Senior Planner Perata said the ConnectMenlo Update would rezone the property. He said the X designation that includes the CDP would still stand. He said language was being developed for the ConnectMenlo Update to allow for the continuance of this zoning if approved. Commissioner Barnes said this would be subsumed into the newly created General Plan update land use zoning designations with grandfathering in of the exceptions associated with the particular characteristics of the M2-X. Senior Planner Perata said it would be rezoned with that designation but the entitlement elements would remain in effect. Commissioner Barnes said they had discussed buildings adding 10 feet to address hazard expected from sea level rise. He asked if this project was subject to the blanket 10-foot elevation requirement. Principal Planner Chow said this particular project would not be subject to the proposed zoning in the ConnectMenlo Update related to the 10-foot requirement. She said this project addressed sea level rise through its proposed design but not with the 10-foot elevation. Commissioner Barnes asked if the applicant was intending greater or less than 10 feet. Senior Planner Perata said the project would comply with the FEMA flood zone minimum elevation plus 16-inches with another eight inches of free board for the lobby level. He said that was about 24-inches above the FEMA flood zone minimum.

Commissioner Goodhue asked if other design iterations beside the industrial look were being considered for the pedestrian/bicycle bridge. Mr. Webb said they had looked at much iteration over the year they have worked on the bridge's design. He said they looked at a truss bridge that spans and at one point they were intending to span the entire expressway. He said they looked at several of the pedestrian bridges over Highway 101. He said Frank Gehry really liked the expression of the proposed design. He said they were also thinking about the bridge that would connect the two buildings. He said they were thinking about the two bridges as almost opposites with the public bridge a big steel, muscular structure and lightness in structure for the bridge connecting the buildings.

Commission Comments: Commissioner Onken said they really appreciated the project. He said the public comment was pretty much not about the project itself but about Facebook. He said the concern with this project was how the scale would relate to the Belle Haven neighborhood. He said they have done big residential forms. He said although three times as large as Building 20

they would also be three times the distance from the residential area. He said there was playfulness about the design which he thought would be very successful. He said he welcomed the public space; it was a wonderful footbridge, and he was looking forward to the Farmer's Market and concerts. He said he hoped people would go to it.

Commissioner Barnes asked the reason behind needing to merge Building 20 with the other parcels as that had a net effect of exceeding building coverage 50%. Mr. Webb said in terms of building code that Building 21 would be connected to Building 20. He said it was essentially an addition to Building 20. He said a building was not allowed to straddle the property line so the property line either had to be moved or taken away. Commissioner Barnes asked the rationale behind one space for 348 square feet of gross floor area when the zoning ordinance standard was for one space per 300 square feet. Mr. Webb said there was discussion to reduce parking due to the mitigation of the trip cap as Facebook intended to improve upon alternate modes of transportation to the site.

Commissioner Onken said he had asked Planner Perata about the parking ratio. He said with the stringent trip cap they needed to look at reducing the parking.

Commissioner Kahle said he really liked Building 20, how the scale was broken down and Building 21 would be a great addition. He said he shared Commissioner Onken's concern that the public space might be underutilized but he supported the safe access to the Bay. He said his main concern was the large blank wall. He said he would like attention given to it such as a second material or something for interest.

Acting Chair Riggs said he was unsure how many charging stalls to require for a large building but had found in most public spaces that the number needed has been underestimated. He said Building 21 in particular was a very large building. He said he appreciated the sloping roof and standing seam material. He said that the full bulk of the building might be apparent at a distance from the other side of Hamilton Avenue. He said the FIA that was not discussed much this evening indicated positive revenue for the City, which in his breakdown would come from Transient Occupancy Tax associated with a hotel. He said otherwise the project was more or less neutral. He asked the architect if he had worked with the Fire District regarding the bridge joining the two buildings.

Mr. Webb said they were working with the Fire District on that. He said regarding the pedestrian bridge and the concern it would be underutilized that they had built a pedestrian bridge in Chicago's Millennium Park, which when built did not really have a destination. He said the bridge itself became the destination. He said he thought this project's bridge might have a similar effect.

Commissioner Goodhue said the number of bicycle parking spaces for the hotel seemed really small, and suggested if they wanted more residents to work at the hotel it seemed a good place for more bicycle parking. She suggested bicycle sharing as well. Mr. O'Shea said they would look into that.

Discussion ensued about the building height that was described as 75 feet, which was two feet higher than the prior Raychem building. It was noted that the height in some places was 83 feet and the building was situated on a slight slope. General opinion was the height needed to be more

transparent in its discussion as the project moved toward permit application.

Acting Chair Riggs said the study session had not identified any significant concerns to list and noted the community's support of the proposed project.

Acting Chair Riggs thanked those who stayed for the General Plan item, but that would not be heard this evening. Principal Planner Chow said the item would be continued to the July 11 meeting that would start at 6 p.m. She said the EIR project manager would not be able to attend and suggested the Commission might ask any questions they might have. Acting Chair Riggs said he did not want to continue the meeting and suggested that Commissioner and the public could email their questions. Principal Planner Chow asked that high level questions, if emailed, should be addressed to ConnectMenlo email.

H. Public Hearing

- H1. General Plan Amendment, Zoning Ordinance Amendment, Rezoning, Environmental Review/City of Menlo Park: Public hearing to receive public comments on the Draft EIR for the General Plan Land Use and Circulation Elements and M-2 Area Zoning Update. The Draft EIR prepared for the project identifies less than significant effects in the following categories: Aesthetics, Geology, Soils and Seismicity, Hydrology and Water Quality, and Public Services and Recreation. The Draft EIR identifies potentially significant environmental effects that can be mitigated to a less than significant level in the following categories: Biological Resources, Cultural Resources, Hazards and Hazardous Materials, Land Use Planning, Noise, and Utilities and Service Systems. The Draft EIR identifies potentially significant environmental effects that are significant and unavoidable in the following categories: Air Quality, Greenhouse Gas Emissions, Population and Housing, and Transportation and Circulation. The California Environmental Quality Act (CEQA) requires this notice to disclose whether any listed hazardous material sites are present at the location. The project area does contain a hazardous waste site included in a list prepared under Section 65962.5 of the Government Code. Written comments may also be submitted to the Community Development Department (701 Laurel Street, Menlo Park) no later than 5:00 p.m., Friday, July 15, 2016. ([Staff Report #16-050-PC](#))

Item continued to the July 11, 2016 Planning Commission meeting.

I. Informational Items

- I1. Future Planning Commission Meeting Schedule
- Regular Meeting: July 11, 2016
 - Regular Meeting: July 25, 2016
 - Regular Meeting: August 15, 2016

Principal Planner Chow reminded the Commission of a possible special meeting on August 31, two days after the August 29 regular meeting.

J. Adjournment

The meeting adjourned at 11:12 p.m.

Staff Liaison: Deanna Chow, Principal Planner

Recording Secretary: Brenda Bennett

CITY OF MENLO PARK
PLANNING COMMISSION

DRAFT ENVIRONMENTAL IMPACT)
REPORT (EIR) - FACEBOOK)
CAMPUS EXPANSION PROJECT)
_____)

PUBLIC HEARING
REPORTER'S TRANSCRIPT OF PROCEEDINGS
MONDAY, JUNE 20, 2016
MENLO PARK CITY COUNCIL CHAMBERS

Reported by: MARK I. BRICKMAN, CSR, RPR
License No. 5527

<p>1 ATTENDEES</p> <p>2 THE PLANNING COMMISSION:</p> <p>3 Katherine Strehl - Chairperson (Not present)</p> <p>4 Drew Combs - Vice Chairperson (Recused)</p> <p>5 Susan Goodhue</p> <p>6 John Onken</p> <p>7 Henry Riggs - (Acting Chairperson)</p> <p>8 THE CITY STAFF:</p> <p>9 Deanna Chow - Principal Planner</p> <p>10 Kyle Perata - Senior Planner</p> <p>11 Nikki Nagaya - Transportation Manager</p> <p>12 SUPPORT CONSULTANTS:</p> <p>13 Erin Efner - ICF International</p> <p>14 Kirsten Chapman - ICF International</p> <p>15 Colin Burgett - TJKM Transportation Consultants</p> <p>16</p> <p>17 ---o0o---</p> <p>18</p> <p>19 BE IT REMEMBERED that, pursuant to Notice</p> <p>20 of the Meeting, and on June 20, 2016, 9:17 PM at the</p> <p>21 Menlo Park City Council Chambers, 701 Laurel Street,</p> <p>22 Menlo Park, California, before me, MARK I. BRICKMAN, CSR</p> <p>23 No. 5527, State of California, there commenced a Planning</p> <p>24 Commission meeting under the provisions of the City of</p> <p>25 Menlo Park.</p> <p>26</p> <p>27 ---o0o---</p> <p>28</p> <p>29</p> <p>30</p> <p>31</p> <p>32</p> <p>33</p> <p>34</p> <p>35</p> <p>Page 2</p>	<p>1 COMMISSIONER RIGGS: This is item F1.</p> <p>2 This is our Draft Environmental Impact Report, infamously</p> <p>3 known as an EIR. Public hearing. Applicant is Hibiscus</p> <p>4 Properties LLC on behalf of Facebook. This is the</p> <p>5 Facebook Campus Expansion Project, 300 to 309</p> <p>6 Constitution Drive.</p> <p>7 Public hearing to receive public comments</p> <p>8 on this Draft EIR. The Draft EIR prepared for the</p> <p>9 project identifies less than significant effects in the</p> <p>10 following categories: Land use, geology and soils,</p> <p>11 population and housing. More on that later. Public</p> <p>12 services, utilities and service systems.</p> <p>13 The Draft EIR identifies potentially</p> <p>14 significant environmental effects that can be mitigated</p> <p>15 to less than significant levels in the following</p> <p>16 categories: Aesthetics, air quality, noise, cultural</p> <p>17 resources, biological resources, hydrology and water</p> <p>18 quality, and hazardous -- hazardous -- hazards and</p> <p>19 hazardous materials.</p> <p>20 The Draft EIR identifies potentially</p> <p>21 significant environmental effects that are significant</p> <p>22 and unavoidable in the following categories:</p> <p>23 Transportation and greenhouse gas emissions.</p> <p>24 The California Environmental Quality Act,</p> <p>25 CEQA, requires this notice to disclose whether any listed</p> <p>Page 4</p>
<p>1 MEETING DETAILS (re Facebook discussion)</p> <p>2 Page</p> <p>3 Presentation by Kyle Perata 5</p> <p>4 Presentation by Kirsten Chapman 7</p> <p>5 Presentation by Colin Burgett 15</p> <p>6 Public Comments 23</p> <p>7 Planning Commission EIR questions 67</p> <p>8 Planning Commission EIR comments 93</p> <p>9 Adjourned 107</p> <p>10</p> <p>11</p> <p>12</p> <p>13</p> <p>14</p> <p>15</p> <p>16</p> <p>17</p> <p>18</p> <p>19</p> <p>20</p> <p>21</p> <p>22</p> <p>23</p> <p>24</p> <p>25</p> <p>Page 3</p>	<p>1 hazardous waste sites are present at the location.</p> <p>2 The project location does not contain a</p> <p>3 hazardous waste site included in a list prepared under</p> <p>4 Section 65962.5 of the Government Code.</p> <p>5 The hazardous material section of the</p> <p>6 Draft EIR discusses this in more detail. Written</p> <p>7 comments may also be submitted to the Community</p> <p>8 Development department, 701 Laurel street, Menlo Park no</p> <p>9 later than 5:30 PM Monday, July 11th, 2016.</p> <p>10 Deanna, I turn this over to you for staff</p> <p>11 introduction.</p> <p>12 MR. PERATA: Actually, it's me tonight</p> <p>13 for this project. So first, kind of update for the</p> <p>14 Commission. We did receive fifteen items of additional</p> <p>15 correspondence for both Facebook and Connect Menlo.</p> <p>16 With regard those, the items concerning</p> <p>17 Connect Menlo relate to the agenda for Facebook.</p> <p>18 Related to the agenda side, there are two</p> <p>19 items, the time needed to review the Draft EIRs along</p> <p>20 with concerns about the Facebook project being reviewed</p> <p>21 concurrently with the G -- General Plan update and</p> <p>22 Connect Menlo and concerns about potential cut-through</p> <p>23 traffic from the project, impact on smaller neighborhood</p> <p>24 streets.</p> <p>25 So that summarizes the additional items of</p> <p>Page 5</p>

<p>1 correspondence received after printing -- 2 COMMISSIONER RIGGS: I'm sorry, Kyle. 3 Could you move your microphone a little closer? 4 MR. PERATA: Oh, sorry about that. 5 So that we can get a quick brief overview 6 of the meeting tonight. 7 So those two items on Facebook. Item F1, 8 which is Draft EIR public hearing, and item G1, which the 9 project proposal's study section. So we'll take those 10 two separately. 11 So for the order of operations here, if 12 the Commission for the Draft EIR, we'll take a 13 presentation by the consultant regarding CEQA and the 14 impacts, findings for the project and then receive public 15 comments on the Draft EIR after that, with Commissioner 16 questions following and comments after that we close the 17 public hearing. 18 And then close the public hearing and open 19 the study session with a presentation by the applicant, 20 public comments on the project and then Commissioner 21 questions and comments, as well. 22 And with that, I'll turn it over to the 23 City's consultant for the EIR. 24 COMMISSIONER COMBS: Actually, before we 25 do that, on the advice of the City Attorney, I have to</p> <p style="text-align: right;">Page 6</p>	<p>1 project. I'm the project manager. I'm also here with 2 tonight with the project director, Erin Efner, and the 3 transportation subconsultant, Colin Burgett from TJKM. 4 Should you have any questions after the 5 presentation regarding the environmental review, we will 6 respond to them. 7 So my presentation will cover the 8 environmental review process. I will also provide an 9 overview of the conclusions of the Draft EIR, explain how 10 to submit comments and describe the next steps 11 So we're currently in the Draft EIR public 12 comment phase of the environmental review. Comments are 13 most helpful when they consider the significant 14 environmental impacts of the project and pro -- and 15 provide recommendations to reduce these impacts or 16 address the adequacy of the EIR. 17 Although my presentation includes an 18 overview of the project, I want to note that the intent 19 of this portion of tonight's meeting as well as the Draft 20 EIR review period is not focused on the project itself or 21 its merits. 22 Instead, comments should be focused on the 23 environmental impacts of the project and the adequacy of 24 the document. 25 We will do our best to address your</p> <p style="text-align: right;">Page 8</p>
<p>1 recuse myself from all Facebook matters given that I'm 2 employed at Facebook and I'm also recusing myself from 3 General Plan update matters with is the third item on the 4 agenda, again in connection with my employment at -- at 5 Facebook. 6 COMMISSIONER RIGGS: Thank you, Drew, and 7 I believe we have another recusal at this point or is 8 that later? 9 COMMISSIONER GOODHUE: Later. 10 COMMISSIONER RIGGS: Later, all right. 11 And we have our staff presentation. 12 And while we're waiting, I'll just point 13 out we are first discussing the EIR. This is an 14 information document, and the key purpose to reviewing 15 this EIR is in order to make sure that it is a complete 16 listing of the concerns that subsequently this Commission 17 and City Council should have as they in fact subsequently 18 review. 19 MS. CHAPMAN: Thank you, Commissioners, 20 and members of the public. Thank you for coming out 21 tonight for the public hearing for the Facebook Campus 22 Expansion Project Draft Environmental Impact Report. 23 My name is Kirsten Chapman and I work for 24 the environmental consulting firm ICF International. We 25 prepared the Environmental Impact Report for this</p> <p style="text-align: right;">Page 7</p>	<p>1 questions tonight. However, please note that all 2 comments and questions will be addressed in the responses 3 to comments document and the Final EIR. 4 So this slide shows the EIR process and 5 the general steps involved in the EIR preparation. The 6 Notice of Preparation, the NOP was released last summer 7 in June 2015. 8 Following the close of the NOP scoping 9 period, we prepared the Draft EIR. The Draft EIR was 10 released last month on May 26th. The comment period for 11 the Draft EIR closes on July 11th. 12 The Final EIR will then be prepared and we 13 will address all the comments received on the Draft EIR 14 during the Draft EIR review period. 15 A certification hearing for the Final EIR 16 will be held for the Planning Commission and City Council 17 and this is expected to occur in the summer and the fall 18 of 2016, and after the EIR is certified, the project can 19 then be approved. 20 Following approval, the project, Notice of 21 Determination will be issued. 22 So our EIR team consists of the City of 23 Menlo Park as the lead agency, meaning that they have the 24 principal responsibility of carrying out the project. 25 ICF is the lead EIR consultant and</p> <p style="text-align: right;">Page 9</p>

<p>1 prepared all sections of the EIR with assistance from 2 TJKM for transportation and baseline for the hazardous 3 materials section.</p> <p>4 Bay Area Economics prepared the Fiscal 5 Impact Analysis and they are also here with us tonight, 6 and Keyser Marston & Associates prepared the housing 7 needs assessment, which we incorporated in the population 8 and housing section of the EIR.</p> <p>9 So Facebook will present the project later 10 in the next study section, but I'll give a quick overview 11 here.</p> <p>12 As shown in this map, the project site is 13 located along Bayfront Expressway at 300 to 309 14 Constitution Drive. The project site is bounded by 15 Bayfront Expressway. It's State Route 84 to the north, 16 Facebook Building 20 to the east and Chilco Street to the 17 west and south. A portion of the project site abuts the 18 Dumbarton rail corridor to the south.</p> <p>19 Is the 58 acre project site currently 20 consists of the existing TE Connectivity campus. At the 21 time of the NOP for the project, which was last summer -- 22 that then is considered the baseline -- the project site 23 included ten buildings containing industrial warehouse, 24 office and research development uses totaling 25 approximately one million square feet as well as</p> <p style="text-align: right;">Page 10</p>	<p>1 the first phase of development. Building 22 would be 2 constructed as the second phase of development.</p> <p>3 The proposal Building 22 would include 4 approximately 450,000 square feet of office uses and 5 would be located in the western portion of the project 6 site.</p> <p>7 The third proposed building, which would 8 ultimately be constructed during the phase, would include 9 a 200 room hotel, limited service hotel. The hotel would 10 include approximately 175,000 gross square feet of hotel 11 and office space and amenities.</p> <p>12 Approximately 3,500 parking spaces would 13 be provided in service lots and under the building 14 podiums of the proposed buildings. Maximum building 15 heights would be approximately 75 feet.</p> <p>16 The project would be organized around a 17 publicly accessible open space and a multi-use bicycle 18 pedestrian corridor that would run north/south through 19 the middle of the site.</p> <p>20 The project would also include the 21 construction of a new multi-use bicycle and pedestrian 22 bridge over State Route 84 to allow public access to the 23 Bay Trail from the project site and the Belle Haven 24 community.</p> <p>25 So because the project involves</p> <p style="text-align: right;">Page 12</p>
<p>1 approximately 1,700 parking spaces.</p> <p>2 However, since NOP, Buildings 307 and 309 3 have been demolished under a separate permit. The 4 demolition of these buildings is not part of the project, 5 but is considered in the Draft EIR as a cumulative 6 project.</p> <p>7 In addition, the renovation of Building 8 23, which is on the project site, is not part of the 9 project, but is also considered in the analysis.</p> <p>10 The proposed Facebook Campus Expansion 11 Project includes the demolition of Buildings 307 to 306 12 and the construction of two office buildings, Buildings 13 21 and 22, encompassing approximately 962,400 gross 14 square feet.</p> <p>15 In addition, an approximately 175,000 16 square foot hotel would be constructed at the project 17 site.</p> <p>18 In total, the project would result in a 19 net increase in floor area of approximately 121,300 gross 20 square feet.</p> <p>21 The proposed Building 21 would contain 22 approximately 513 gross square feet of office and event 23 uses and would be located in the eastern portion of the 24 project site.</p> <p>25 Building 21 would be constructed during</p> <p style="text-align: right;">Page 11</p>	<p>1 discretionary actions by the City and is subject to the 2 California Environmental Quality Act or CEQA, in 3 accordance with CEQA, because this project may have a 4 significant effect on the environment, an EIR has been 5 prepared.</p> <p>6 The EIR is a tool for identifying physical 7 impacts to the environment by using the analysis 8 conducted by our EIR team.</p> <p>9 The EIR is also used to inform the public 10 and decision-makers about a project prior to project 11 approval, identify direct, indirect or cumulative 12 physical environmental impacts of the project, recommend 13 ways to reduce impacts and consider alternatives to 14 lessen than identified physical impacts.</p> <p>15 So as shown here, the EIR covers most of 16 the environmental topics required by CEQA. The EIR 17 analysis covers topics such as aesthetics, greenhouse 18 gases, biological resources, transportation, all the 19 topics that are listed.</p> <p>20 The project site is fully developed in an 21 urbanized area. Therefore, agricultural, forestry and 22 mineral resources do not exist on the site and are not 23 identified in the EIR.</p> <p>24 The Draft EIR identifies and classifies 25 the environmental impacts of significant, less than</p> <p style="text-align: right;">Page 13</p>

<p>1 significant or no impact. For each impact identified as 2 being significant, the Draft EIR provides mitigation 3 measures to reduce, eliminate or avoid adverse effects. 4 If the mitigation measures would 5 successfully reduce the impacts to a less than 6 significant level, this is straight -- stated in the 7 Draft EIR. 8 Aesthetics, air quality, noise, cultural 9 resources, biological resources, hydrology and hazardous 10 materials were identified to be less than significant 11 after mitigation has been applied. 12 However, if the mitigation measures would 13 not diminish these effects to less than significant 14 levels, then the Draft EIR classifies the impacts as 15 significant and unavoidable. 16 Consequently, the City will need to 17 determine whether to approve the project as proposed, and 18 if so, provide its rationale in the Statements of 19 Overriding Considerations. 20 Significant unavoidable impacts of the 21 project include transportation impacts, which TJKM will 22 discuss, and greenhouse gas emission impacts. 23 So this slide shows the project would 24 result in the following significant and unavoidable 25 impacts. Peak hour motor vehicle traffic at study</p> <p style="text-align: right;">Page 14</p>	<p>1 where the analysis looked at the impact of additional 2 peak hour traffic generated by the exp -- proposed 3 expansion. 4 Earlier a commenter had a question about 5 the cumulative effect of -- you know, essentially that 6 had been a series of developments observed in the area, 7 including the prior Facebook project and the -- the west 8 campus as well as other development projects in the area, 9 and -- and this study included a look at conditions -- 10 you know, the study starts out with existing conditions 11 and then looks at background conditions with all of that 12 improved development, but without this project, and then 13 the study looks at background plus project issues. 14 So in answer to that earlier comment, this 15 study does consider all the prior approvals and -- and 16 potential growth that's in the pipeline. 17 And then this map here shows the bicycle 18 network as it -- as it relates to the study area, and a 19 key point here is that the study area is not limited to 20 Menlo Park. It -- it includes University Avenue corridor 21 in East Palo Alto and -- and the City of Palo Alto and 22 extends to the north and all the way west to 280. 23 And so this slide here summarizes the 24 different scenarios that we're evaluating in 25 transportation analysis.</p> <p style="text-align: right;">Page 16</p>
<p>1 interactions during both the project and cumulative 2 conditions. Peak hour motor vehicle traffic on routes of 3 regional significance during both the project and 4 cumulative issues. Daily motor vehicle traffic on 5 roadway segments during both the project and cumulative 6 conditions. Increased delay of transit vehicles under 7 project conditions, and conflicts with applicable plans 8 and policies adopted for the purpose of reducing 9 emissions of greenhouse gases. 10 So I'll turn it over to Colin from TJKM. 11 MR. BURGETT: Hi. My name is Colin 12 Burgett with TJKM Transportation Consultants and I'll 13 just briefly describe the -- essentially the -- the scope 14 of work for the transportation study and what the issues 15 were and the -- the key impact findings. 16 So the study includes an evaluation of the 17 project impacts to bicycle circulation, pedestrian 18 circulation, motor vehicle traffic operations, transit 19 impacts and also the potential impacts of vehicle miles 20 traveled. 21 And the traffic study was quite 22 comprehensive in that this -- this map here shows the 23 study area for the assessment of peak hour traffic 24 operations. 25 There were a total of 66 interactions</p> <p style="text-align: right;">Page 15</p>	<p>1 So again, existing conditions, background 2 conditions focusing on all the projects that are in the 3 pipeline. Background plus project is really the key 4 scenario for looking at what the impact of this project 5 will be. 6 And then there's three different 2040 7 scenarios, more than you would typically see due to the 8 General -- the General Plan update. 9 And so this project here just provides a 10 comparison of peak hour traffic operations between these 11 different scenarios, and a -- a key point is that the 12 level of service doesn't change as dramatically as you 13 might -- you might expect. 14 Condition -- conditions are already 15 reasonably delayed in a lot of study intersections, and 16 so what that means is level of service won't change much, 17 but it won't -- it doesn't take much of an increase in 18 delay to be considered a significant impact in that case. 19 So the -- so the key findings. With 20 regard to bicycle and pedestrian impacts, there -- there 21 was significant impacts identified related to the 22 connections between the project site and the area wide 23 transportation system. 24 So the project itself includes a -- a 25 bicycle/pedestrian bridge over Bayfront and it includes</p> <p style="text-align: right;">Page 17</p>

<p>1 pedestrian and -- and bicycle paths within the site and 2 immediately adjacent to the site.</p> <p>3 The EIR recommends some additional 4 mitigations to improve pedestrian/bicycle connections on 5 Constitution and Bayfront.</p> <p>6 With regard to traffic impacts, the -- the 7 project would generate -- you know, vehicle miles 8 traveled is a measure of the -- the -- the average 9 vehicle miles per day per employee generated by a 10 project.</p> <p>11 And this project, because Facebook 12 provides extensive shuttle bus service and so forth, the 13 actual vehicle miles generated on a daily basis are below 14 the regional averages, but works out about fifteen miles 15 per employee, and -- and that includes non-employee 16 trips.</p> <p>17 Basically the way it goes, you take all 18 the vehicle miles generated by the project, add that up 19 and then divide it by the number of employees.</p> <p>20 Peak hour traffic impacts were identified, 21 and as I mentioned, you know, in most cases, these 22 impacts occur at locations where there's already a delay 23 exceeding the threshold, which means if you add more 24 traffic, it -- it's going to be considered a significant 25 impact.</p> <p style="text-align: right;">Page 18</p>	<p>1 Facebook can generate about 3,000 peak hour vehicle trips 2 for the east campus plus the west campus. With -- with 3 the expansion, the proposal is increase that by about 4 1,800 vehicle trips.</p> <p>5 And the recommended mitigation would 6 reduce the share of Facebook trips to both campuses, 7 reduce the share that can occur during a single peak 8 hour.</p> <p>9 So the effect would be that -- to 10 essentially spread out that morning traffic a bit so -- 11 so there would be less of the impact at intersections and 12 apply the same effect in the -- in the PM.</p> <p>13 So with this litigation, the actual 14 increase in peak hour traffic would -- would not be very 15 dramatic.</p> <p>16 During the AM peak hour, existing trip cap 17 is about 3,100 peak hour trips. With mitigation, they'd 18 be allowed to join an additional 400. In the PM peak 19 hour, existing cap allows 3,400. They'd being allowed 20 another 150. So the effect on peak hour traffic would be 21 much more benign with this mitigation.</p> <p>22 So -- so again, to summarize the -- the 23 recommended mitigations, some bicycle path improvements 24 made to the site, enhancements to connections between 25 essentially Chilco and the bicycle bridge, the reduction</p> <p style="text-align: right;">Page 20</p>
<p>1 There -- there were several intersections 2 where impacts were identified related to potential cut- 3 through traffic in the neighborhood, and there were also 4 impacts related to daily traffic volumes on study 5 segments.</p> <p>6 And transit impacts that were identified 7 focus on in -- increased transit demand and increased 8 delays to transit vehicles.</p> <p>9 In this case, because Facebook provides so 10 much of their own shuttle service, the project isn't 11 going to generate a significant impact on transit demand, 12 but traffic impacts would be potentially significant in 13 terms of increased delay to transit vehicles on Bayfront 14 and -- and other streets in the area.</p> <p>15 So the key mitigation with regard to peak 16 hour traffic impacts focuses on the project trip cap. 17 So -- so since Facebook first located in Menlo Park, 18 they've been subject to -- to a trip cap that applies 19 both to daily trips and peak hour trips, and on the -- on 20 the -- on the far left shows AM peak hour trip cap.</p> <p>21 The light blue is the existing trip cap 22 for the site. The dark blue or purple is what's proposed 23 as part of the project, and then the red is what's 24 recommended as the mitigation.</p> <p>25 So under the current Facebook permits,</p> <p style="text-align: right;">Page 19</p>	<p>1 in the peak hour trip cap, some mitigations to discourage 2 cut-through traffic in the neighborhood, and some -- some 3 intersection mitigations are recommended.</p> <p>4 MS. CHAPMAN: Good, okay. So you can 5 submit comments on the Draft EIR via e-mail, letter or 6 fax to Kyle Perata, Senior Planner with the City. You 7 can also speak here tonight and we will note your 8 comments and consider them during the preparation of the 9 Responses to Comments Documents in the Final EIR.</p> <p>10 All comments must be received by 5:30 PM 11 on July 11th.</p> <p>12 So the next steps is compiling the 13 Responses to Comments Document. We will consider and 14 respond to all comments, both oral and written received 15 on the Draft EIR.</p> <p>16 Comments that are repeated by several 17 commenters will be addressed in master responses. Any 18 changes to the Draft EIR as a result of comments received 19 or staff initiated changes will be shown in strike- 20 through for deleted text and underlined for new text.</p> <p>21 The responses to comments plus the Draft 22 EIR, which was released last month, will constitute the 23 Final EIR.</p> <p>24 So thank you for coming out here tonight 25 and we look forward to receiving your comments.</p> <p style="text-align: right;">Page 21</p>

<p>1 COMMISSIONER RIGGS: Does that conclude 2 the presentation? 3 MR. PERATA: Yes, it does. 4 COMMISSIONER RIGGS: All right. Let's -- 5 for most of our hearings, we have Commission questions, 6 then to clarify issues, then public comments, and then we 7 come back for Commission comments. Tonight we'll go 8 directly to public comment. 9 There are quite a few of you here and no 10 doubt you would like to close your evening sooner than 11 later. 12 I have eleven cards on the topic of the 13 EIR for Facebook. I have actually fewer cards for 14 discussing the actual project. 15 So I'd like to ask first of all that if 16 anyone else would like to speak tonight on the EIR rather 17 than the project itself, please bring your card up to the 18 staff desk within the next five minutes. 19 And with that, I will call you up in what 20 I believe is in order of the cards received or something 21 close to it. 22 I will call both the person to come up and 23 speak and the person who is on deck so that you can be 24 ready. 25 We will hold to three minutes. I do have</p> <p style="text-align: right;">Page 22</p>	<p>1 Francisco and Peninsula area. We dispatch workers to 2 projects around the area. 3 I'm here in support to move the Draft EIR 4 forward. Facebook, to reiterate, has been a good partner 5 for the community. 6 I happen to be a resident of Redwood City, 7 so I'm very in tune to all the projects on the Peninsula 8 and how they do affect the community, and I do believe 9 this would be a positive influence for this area. 10 Thank you. 11 COMMISSIONER RIGGS: Thank you. And is 12 it James Ruigomez? 13 MR. RUIGOMEZ: James Ruigomez, and thank 14 you, Commissioners, for allowing me to speak tonight. My 15 name is James Ruigomez and I represent San Mateo County 16 Building Construction Trade Council. 17 We have hundreds of construction workers 18 that live here in Menlo Park and thousands that live here 19 in San Mateo County ready to build the job right the 20 first time with their highly skilled men and women of our 21 trades. 22 We're here tonight to show our support for 23 Facebook and to underscore the importance of what 24 Facebook's presence in Menlo Park has meant to our union 25 workers and for the community.</p> <p style="text-align: right;">Page 24</p>
<p>1 a nuisance little timer, but please feel free to speak 2 for less of that time and then we'll be able to move on 3 to the next part of the -- of the evening. 4 So the first speaker is Mark Leach, who 5 will be followed by Viktor Torreano. 6 MR. LEACH: Good evening. My name is 7 Mark Leach. I'm with the Electrical Workers' Union here 8 in San Mateo County. Thank you for allowing me to speak 9 tonight. 10 I'm here to support Facebook and the EIR. 11 This is one of the best community partners that we have. 12 They listen, they help, they keep promises. 13 Hundreds if not thousands of construction 14 families have benefited from their commitment to use 15 highly trained building trades work force. 16 We're very excited to be part of -- part 17 of the future that Facebook is proposing. We do support 18 the EIR. 19 Thank you. 20 COMMISSIONER RIGGS: Thank you. And 21 Victor, followed by -- I hope I have this right, James 22 Ruigomez. 23 MR. TORREANO: Good evening, 24 Commissioners and staff. My name is Victor Torreano. I 25 represent the Sheet Metal Workers' Union out of the San</p> <p style="text-align: right;">Page 23</p>	<p>1 Since moving to Menlo Park, Facebook has 2 been a responsible citizen and a good neighbor in the 3 local community. We've been able to create iconic 4 buildings in Menlo Park that we're really proud to build. 5 This project will -- will create more than 6 4,000 good paying union construction jobs over the next 7 five years, benefiting our workers and our families. 8 We support -- support the project and look 9 forward to getting to work once it's approved. Thank you 10 for your time. 11 Facebook is good for Menlo Park. Facebook 12 has grown responsibly, and that shows in its good 13 relations with its neighbors. 14 As Facebook expands, it will bring more 15 job opportunities. Facebook will continue to be a good 16 neighbor in the local community. 17 Thank you for allowing me to speak 18 tonight. 19 COMMISSIONER RIGGS: Thank you, James. 20 Next is David Lawrence followed by 21 Katherine Fields. 22 MR. LAURANCE: Hi. My name is David 23 Laurance and I've been working in the Belle Haven 24 community of Menlo Park in the last 23 years. I'm one of 25 the fortunate people who's part of the family known as</p> <p style="text-align: right;">Page 25</p>

<p>1 Beechwood School.</p> <p>2 In the years that I've served the</p> <p>3 Beechwood as teacher and now as head of school, we've</p> <p>4 seen many changes, including the complete overhaul of our</p> <p>5 campus which was made possible by the City of Menlo Park</p> <p>6 and by generous donors, many of whom are developers and</p> <p>7 many of whom have benefited from this latest boom.</p> <p>8 However, this latest search surge in</p> <p>9 development has me increasingly concerned about the</p> <p>10 future of our school. This year alone, we saw the loss</p> <p>11 of four talented ended teachers, all of whom could be</p> <p>12 considered victims of an economy that leaves them with</p> <p>13 very few options.</p> <p>14 Each of them loved Beechwood. Each of</p> <p>15 them was making a positive impact in our community. They</p> <p>16 simply could not make it work any longer.</p> <p>17 As I look to replace these people, I'm</p> <p>18 finding the pool of candidates is shrinking, as well.</p> <p>19 How on earth could a teacher right out of teacher</p> <p>20 credential program afford to come to this area and start</p> <p>21 their career by helping the worthy families of Belle</p> <p>22 Haven?</p> <p>23 Speaking of those worthy families, just two</p> <p>24 weeks ago, I stood on our graduation stage with a sixth</p> <p>25 grade boy who was being honored for outstanding</p> <p style="text-align: right;">Page 26</p>	<p>1 Katherine followed by Opha Wray.</p> <p>2 MS. FIELDS: Hello. My name is Katherine</p> <p>3 FieldS. I am the Executive Director of the California</p> <p>4 Family Foundation and I'm here to read a letter written</p> <p>5 by Sue Jacobson, one of the founders of Beechwood School.</p> <p>6 Mrs. Jacobson couldn't be here with us</p> <p>7 this evening, but she's entitled her letter "Belle</p> <p>8 Haven," which means beautiful refuge.</p> <p>9 "The issues that face the Belle Haven</p> <p>10 community are not much different than what is being</p> <p>11 experienced all over the Bay Area. The rapidly</p> <p>12 escalating housing costs, unbearable traffic, excessive</p> <p>13 new building, shortage of amenities, struggling schools,</p> <p>14 conflict, rapidly expanding gap between wealthy and low</p> <p>15 income people.</p> <p>16 "Much of the work needs to be done on a</p> <p>17 more global scale, but in the meantime, we have in our</p> <p>18 midst a beautiful little community about to be swallowed</p> <p>19 by it all.</p> <p>20 "Surely we have right here in this</p> <p>21 creativity and brain power to preserve and even enhance</p> <p>22 what we have.</p> <p>23 "The handwriting on the wall seems to say</p> <p>24 that gentrification is inevitable. That will be a great</p> <p>25 loss to all of us.</p> <p style="text-align: right;">Page 28</p>
<p>1 citizenship. As he stood in front of the audience</p> <p>2 receiving this award, he was overcome by emotion, knowing</p> <p>3 that this was his last night at Beechwood, knowing that</p> <p>4 he is unable to complete his seventh and eighth grade</p> <p>5 years, knowing that his family, like many other Beechwood</p> <p>6 families, has been displaced.</p> <p>7 People want to stay here. There is a</p> <p>8 great school here. There is extended family here. There</p> <p>9 are memories here.</p> <p>10 Unfortunately for so many, there is no</p> <p>11 affordable place to live. The Facebook EIR and the</p> <p>12 General Plan EIR do not seem to recognize that diversity</p> <p>13 in Menlo Park is on the verge of extinction.</p> <p>14 Unless affordable housing is made more of</p> <p>15 a priority, families of modest means -- the gardeners,</p> <p>16 the nurses, the nannies, the cooks, the people who work</p> <p>17 in elder care, the teachers -- will have no choice but to</p> <p>18 leave or to tolerate an intolerable commute or to move</p> <p>19 into an unsafe substandard situation.</p> <p>20 I hope the Commission will do whatever is</p> <p>21 in their power to help preserve Belle Haven and all that</p> <p>22 it has to offer to this City.</p> <p>23 Thank you for your time and for all you</p> <p>24 have done for my school.</p> <p>25 COMMISSIONER RIGGS: Thank you.</p> <p style="text-align: right;">Page 27</p>	<p>1 "Don't we have a unique opportunity to do</p> <p>2 something different? Can't we figure out a way to honor</p> <p>3 and appreciate the amazing people who have already been</p> <p>4 residing here in some cases for generations? And can't</p> <p>5 we at the same time welcome those who would like to make</p> <p>6 this their home, too? Do we have to follow the path of</p> <p>7 pushing some out so others can move in? Do we have to</p> <p>8 fight for competing rights?"</p> <p>9 "We might say that the economics will</p> <p>10 drive the results despite what we may think we want. Do</p> <p>11 we have to be slave to those economics? Can we show the</p> <p>12 rest of the world a better way? Can we demonstrate that</p> <p>13 there are more satisfying riches than the riches of</p> <p>14 monetary wealth? Can we live side by side enjoying the</p> <p>15 richness of diversity on every level? Can we open</p> <p>16 ourselves to the possibility that everyone has something</p> <p>17 to give?</p> <p>18 "Isn't the world hungry for some evidence</p> <p>19 that people can truly live in peace and harmony, teaching</p> <p>20 and strengthening each other and showing the next</p> <p>21 generation that the principles we preach are really</p> <p>22 attainable if we work together?</p> <p>23 "Let's combine our collective talents and</p> <p>24 strength and resources to make this community truly a</p> <p>25 beautiful refuge."</p> <p style="text-align: right;">Page 29</p>

<p>1 Thank you.</p> <p>2 COMMISSIONER RIGGS: Thank you,</p> <p>3 Katherine. Opha followed by Sigurd Schelstraete. I hate</p> <p>4 to think what I'm mangling that name to.</p> <p>5 MS. WRAY: Hello. My name is Opha Wray</p> <p>6 and I'm here to represent the Mount Olive Apostolic</p> <p>7 Church of God in Menlo Park and the Crime Prevention</p> <p>8 Narcotics Drug Education Center. We're located at 605</p> <p>9 Hamilton here in Menlo Park.</p> <p>10 And to the Commissioner, we, the Crime</p> <p>11 Prevention and Mount Olive, support the Facebook</p> <p>12 expansion and the EIR.</p> <p>13 We are glad Facebook moved into the</p> <p>14 community. They have proven themselves to be good</p> <p>15 neighbors and a great asset to Menlo Park community.</p> <p>16 Just like good neighbors, Facebook</p> <p>17 introduced themselves to the community and immediately</p> <p>18 became part of the community, extending themselves to</p> <p>19 help and support with -- support and resources.</p> <p>20 Time will not permit me to talk about all</p> <p>21 of the partnerships and programs Facebook is involved</p> <p>22 with, but let me just mention a few.</p> <p>23 Facebook has partnered with Mount Olive</p> <p>24 and Crime Prevention in our education program, after</p> <p>25 school mentoring, internship and training. In education,</p> <p style="text-align: right;">Page 30</p>	<p>1 Mount Olive and Crime Prevention</p> <p>2 participated in several of the community job fairs which</p> <p>3 were sponsored by Facebook. The job fairs offered</p> <p>4 resource opportunities for job-seekers, resume writing</p> <p>5 and coaching, which was a great benefit for the entire</p> <p>6 community.</p> <p>7 Mount Olive and Crime Prevention</p> <p>8 appreciates Facebook and will continue to work together</p> <p>9 as community partners and as good neighbors. We support</p> <p>10 Facebook's expansion project and encourage the Planning</p> <p>11 Commission to move forward.</p> <p>12 Yes, we've heard a lot of issues regarding</p> <p>13 transportation and traffic and the traffic concerns, but</p> <p>14 we believe these challenges were here before Facebook</p> <p>15 came in and we're looking to the City of Menlo Park to</p> <p>16 address these traffic issues and make it a priority.</p> <p>17 Thank you so much for the opportunity to</p> <p>18 speak to you tonight. Thank you.</p> <p>19 COMMISSIONER RIGGS: Thank you, Opha, and</p> <p>20 I'll just point out we'll -- I let you go a little bit</p> <p>21 past five minutes. We'll close the additional comment</p> <p>22 cards. We're now up to about 24, so bear with us.</p> <p>23 And Sigurd followed by Harry Bims.</p> <p>24 MS. SCHELSTRAETE: Thank you. Good</p> <p>25 evening. My name is Sigurd Schelstraete, so you had that</p> <p style="text-align: right;">Page 32</p>
<p>1 Facebook has funded our scholarship program since 2012.</p> <p>2 The program is designed to award</p> <p>3 outstanding students, keeping the legacy of our late</p> <p>4 founder, Dr. H.L. Bostick, her commitment to education</p> <p>5 through this scholarship fund.</p> <p>6 In our awards program in this past April</p> <p>7 of this year, we awarded many of the students in Belle</p> <p>8 Haven community with awards ranging from 800 to a</p> <p>9 thousand.</p> <p>10 Our program was made possible by the</p> <p>11 funding and generous donations from Facebook. We're able</p> <p>12 to provide scholarships to outstanding students who would</p> <p>13 not normally or necessarily be able to attend colleges</p> <p>14 because they face many financial challenges.</p> <p>15 And we are happy and glad to -- and</p> <p>16 pleased to share with you that two of our scholarship</p> <p>17 recipients this year are currently on internships at</p> <p>18 Facebook.</p> <p>19 We're delighted to Facebook continues</p> <p>20 their support with our after-school mentoring program.</p> <p>21 The after-school program focuses on reading and math and</p> <p>22 technology.</p> <p>23 The program would be offered for the first</p> <p>24 time this summer during the summer months in addition to</p> <p>25 our regular school sessions.</p> <p style="text-align: right;">Page 31</p>	<p>1 mostly right.</p> <p>2 I live in Belle Haven. We rised at the</p> <p>3 beginning of the session to comment on the adequacy of</p> <p>4 the EIR, so that's what I would like to do, specifically</p> <p>5 on the section of population and housing.</p> <p>6 So I was a little surprised reading the</p> <p>7 conclusion in the report that the impact would be less</p> <p>8 than significant.</p> <p>9 So I looked at the methodology that was</p> <p>10 used, and I don't have time to go through all, but the</p> <p>11 bottom line is that the report estimates that somewhere</p> <p>12 between 175 to 277 houses would be needed to accommodate</p> <p>13 the people that move into the area, and since this is</p> <p>14 less than the current vacancy rate in Menlo Park, that</p> <p>15 shouldn't be a problem.</p> <p>16 I think that's a bit fast and loose to</p> <p>17 begin with, if you ask me, but the most important point</p> <p>18 is that I think the report misses a very important point</p> <p>19 here, and the point is that all of this impressive</p> <p>20 construction here is going to take place in Belle Haven,</p> <p>21 and my concern is that the impact also is going to be</p> <p>22 felt disproportionately in Belle Haven.</p> <p>23 So these 175 or 277 homes or whatever the</p> <p>24 number is will not be spread equally throughout the City.</p> <p>25 I think it's fair assume that a disproportionate number</p> <p style="text-align: right;">Page 33</p>

<p>1 of these houses will be found in Belle Haven. 2 Unfortunately the report in its current 3 form does not break out how the impact would be on 4 different areas in the City, and as such, we don't know 5 what it would mean to Belle Haven. 6 So the situation in Belle Haven is that we 7 have a relatively high number of lower income residents, 8 a very number of renters, so if the housing market heats 9 up any more than it already has, there is a real 10 possibility that people will be impacted. 11 So I would like to see in the EIR more 12 detailed analysis of what's going to happen in Belle 13 Haven, and the reason this is important is that the EIR 14 itself specifies a number of so-called thresholds of 15 significance. 16 So events or conditions that would result 17 in significant impact, and one of those is the question 18 whether the project will displace a substantial number of 19 people necessitating the construction of replacement 20 housing elsewhere. 21 I think for the current reports, we cannot 22 answer that question as far as Belle Haven is concerned. 23 So I think Facebook probably has every intention of being 24 a good neighbor as it has been in the past, but the 25 problem is that the current reports give no reason for</p> <p style="text-align: right;">Page 34</p>	<p>1 have some just preliminary thoughts that I wanted to 2 bring before the Commission tonight, and I'll probably 3 follow up by the end of the comment period with more 4 informed comments. 5 But basically I just wanted to talk in 6 four -- kind of four areas with respect to the EIR. 7 Starting with aesthetics. 8 My feeling is that Facebook has gone above 9 and beyond with aesthetics in the -- in the design of the 10 building, to have it face the neighborhood in a way 11 that's aesthetically pleasing, and I think that -- so I 12 think that's a really good thing that they should be 13 commended on. 14 With respect to education, school impacts, 15 which are listed in the EIR, one thing that the EIR did 16 mention was that they anticipate that 82 percent of 17 Facebook employees will send their kids to the Menlo Park 18 City School District and only nine percent to Ravenswood, 19 which is in one -- in some sense understandable given 20 the -- the disparity in the education levels in two 21 different school districts. 22 And I -- I think one possibility for 23 mitigating that is to improve the quality of schools in 24 the Belle Haven side of the freeway such that people 25 don't have to send their kids across the freeway during</p> <p style="text-align: right;">Page 36</p>
<p>1 concern when it comes to housing. It shows that it's 2 less than significant, so they have no reason to assume 3 that anything is wrong here. 4 In conclusion, I think in its current 5 form, the report is not adequate. It is not sufficient 6 to allow us to make an informed decision on what the 7 impact would be in Belle Haven, and I hope that that 8 specifically could be addressed in the next iteration of 9 the Environmental Impact Report. 10 And if there is or if there would turn out 11 to be any impact on the current residents of Belle Haven, 12 I hope that that could also be addressed. 13 But as I said in the beginning, I'm more 14 than a little surprised that the conclusion in the report 15 would be that all of this construction will have less 16 than significant impact on Belle Haven. I don't think 17 that can be correct. 18 Thank you. 19 COMMISSIONER RIGGS: Thank you, Sigurd. 20 Next up is Harry Bims with time donated by 21 Cheryl Bims. The following speaker would be Mike Simon. 22 MR. BIMS: Good evening, Commissioners. 23 Harry Bims, a Belle Haven resident. 24 I like many people have not had a chance 25 to read through the entire Facebook Draft EIR, but I did</p> <p style="text-align: right;">Page 35</p>	<p>1 rush hour just to go to school. 2 That would address the disparity in 3 education, and also long-term, address some of the issues 4 with displacement, because people would have better 5 quality education to attain to the -- to the kind of jobs 6 that are coming into the area. 7 Just a couple days ago, East Palo Alto 8 mentioned that only that for every four residents, 9 there's only one job available and that there's -- 10 there's a need for jobs to come to -- to the area. 11 And so I think that education's a big key 12 component to that. So if we can solve that K through 8 13 education piece as a part of what we're working on here 14 with these EIRs, I think that would be important. 15 The other thing is with respect to traffic 16 circulation. I think that multi-mobile traffic is going 17 to be the way to go. 18 Simply traffic demand management with 19 buses and bicycle access is -- is an important step, but 20 I also think that we're going to have to really activate 21 the Dumbarton rail corridor to make that -- to make a 22 viable dent on the traffic issues going on in the 23 neighborhood. 24 You know, at -- at rush hour, I tried to 25 drive along Marsh Road between Highway 84 and Willow Road</p> <p style="text-align: right;">Page 37</p>

<p>1 on -- on Highway 84 during rush hour, and it took me an 2 hour to go from Marsh to Willow on Highway 84 during the 3 evening rush hour. 4 So traffic is -- is incredibly significant 5 in the area in general. So I think that that's going to 6 have to be something to be addressed. 7 The other issue is on affordable housing. 8 I think that we have to start to think more globally 9 about how to solve this problem. 10 Belle Haven is one neighborhood, and the 11 affordable housing need is indeed immense. And so if we 12 think more broadly about how to cooperate, for example, 13 with East Palo Alto. 14 They mentioned they want to build more 15 affordable housing. They have the space to do it. One 16 issue is the lack of water to -- for those projects, and 17 if there's a way that the cities can cooperate on that -- 18 on that front, then maybe we can get more affordable 19 housing built as a regional solution and instead of 20 focusing specifically on one neighborhood. 21 So those are my comments. Thank you. 22 COMMISSIONER RIGGS: Thank you, Harry. 23 And Mark Simon followed by Diana Bailey. 24 MR. SIMON: Good evening, Commissioners. 25 My name is Mark Simon. I represent samTrans, Caltrain</p> <p style="text-align: right;">Page 38</p>	<p>1 that they're working on. 2 They've also been an active supporter of 3 our Caltrain electrification program and continue to be 4 enthusiastic and begin their support and in their 5 thoughtful approach to it. 6 We have a true public/private partnership 7 with Facebook that demonstrates their commitment to 8 collaborate with others and to address regional 9 challenges, and I believe it's not an exception the way 10 they're working with us, but that the way they're working 11 with us is exceptional. 12 Thank you for the opportunity to address 13 you on this issue. We are fully in support of their EIR 14 and encourage the Council -- the Commission to approve 15 it. 16 I have a letter to the mayor addressing 17 these same issues I'd like to submit to the -- the clerk 18 for the sake of the permanent record. 19 Thank you. 20 COMMISSIONER RIGGS: Thank you, Mark, and 21 I hope you get to stay around for a little bit. 22 MR. SIMON: Everybody's got to be 23 somewhere. 24 COMMISSIONER RIGGS: We might have a -- 25 might have a -- a comment for you tonight, as well.</p> <p style="text-align: right;">Page 40</p>
<p>1 and the San Mateo County Transportation Authority. 2 The organizations I represent have worked 3 with Facebook on a number of issues. An awful lot of 4 private employers in this area think their company is 5 their company is their contribution to the community, but 6 I can tell you having partnered with Facebook that they 7 see their role much more broadly as a -- an active 8 partnership in the broader community and making the 9 broader community in a better place. 10 They're willing to -- in particular to 11 step up to address our region's transportation challenges 12 with innovative ideas and substantial support, 13 demonstrates their willingness to be an active partner in 14 finding solutions. 15 Their transit -- Transportation Demand 16 Management Program is a model for other companies. 47 17 percent of their employees can commute to work using an 18 alternative mode to the automobile. 19 They have provided a million dollars to 20 our agency to study transportation alternatives on the 21 Dumbarton corridor, a significant investment that will 22 extend beyond their own interests. 23 They are also an active participant in the 24 study we're undertaking on managed lanes on 101. 25 Obviously that's a regional transportation transit issue</p> <p style="text-align: right;">Page 39</p>	<p>1 Next up is Diane Bailey followed by Daniel 2 Saver. 3 MS. BAILEY: Good evening, Commissioners 4 and staff. My name's Diane Bailey. I'm director of 5 Menlo Spark. We're an independent non-profit group 6 locally here working to help to Menlo Park become climate 7 neutral. 8 Menlo Spark is partnering with Facebook 9 because they're a leader in sustainability and addressing 10 climate change, and we really appreciate the community 11 outreach and projects that they support. 12 For example, Facebook is working with us 13 to bring free solar power to low income homeowners in 14 Belle Haven. That's just one example. 15 I want to note that we don't comment on 16 specific developments with Menlo Spark, but we do 17 strongly support green sustainable and low carbon 18 building measures, including many of the green building 19 practices that Facebook has employed, and I just want to 20 mention a couple. And the previous speaker mentioned a 21 few, as well. So I'll try not to repeat those. But 22 these are really noteworthy and deserve a lot of 23 attention. 24 Clean renewable energy. Their office 25 buildings at the old Sun Microsystems campus support over</p> <p style="text-align: right;">Page 41</p>

<p>1 one megawatt of solar currently and another more than two 2 megawatts of solar through carport systems covering the 3 parking lot, which is a fairly novel way to bring in 4 renewable power.</p> <p>5 That's planned to come online very soon, 6 followed by several more megawatts at their other 7 developments.</p> <p>8 Facebook also led the way signing on to 9 the Corporate Renewable Energy Buyer's Principles. This 10 is a national effort, and it now has more than fifty 11 companies signed on in support of clean and renewable 12 energy from utilities all across the country, but 13 Facebook was one of the early signers and business 14 leaders of this effort.</p> <p>15 On the sustainable building front, they 16 retrofitted eight buildings to achieve LEED Gold 17 certification through a whole host of measures that I 18 won't list here, but I'll say that that's quite a feat 19 and deserves acknowledgement.</p> <p>20 The two -- 2015 of Building 20, as you 21 know, includes a nine acre green rooftop with hundreds of 22 trees, many of which are mature trees and provide very 23 important habitat for migrating birds as well as many 24 water efficient native plant species that provide that 25 provide additional habitat, and this is a very nice</p> <p style="text-align: right;">Page 42</p>	<p>1 as well.</p> <p>2 We hope to continue working with Facebook 3 on further measures that advance sustainability, improve 4 equity and preserve community heritage.</p> <p>5 And finally, I just want to note that the 6 jobs/housing imbalance is a very serious issue throughout 7 our region and we hope that we can all work together to 8 solve that in the near future.</p> <p>9 Thank you.</p> <p>10 COMMISSIONER RIGGS: Thank you, Diane. 11 Daniel followed by Patti Fry.</p> <p>12 MR. SAVER: Good evening, Commissioners, 13 members of the public. My name is Daniel Saver. I'm an 14 attorney at Community Legal Services in East Palo Alto.</p> <p>15</p> <p>16 My agency is a free non-profit law firm 17 that works with lower income residents in East Palo Alto, 18 Belle Haven, North Fair Oaks, really throughout the 19 entirety of San Mateo County.</p> <p>20 Particularly I focus in our housing 21 program, and I just have a couple of quick comments this 22 evening.</p> <p>23 The first is that I just wanted to express 24 a little bit of my office's concern with the volume of 25 information that the public is expected to digest in an</p> <p style="text-align: right;">Page 44</p>
<p>1 alternative to the solar rooftops, as well.</p> <p>2 Through their strong recycling and 3 composting programs, Facebook's waste diversion rate is 4 more than ninety percent. That's one of the best and 5 highest that we've seen of corporate diversion rates.</p> <p>6 On transportation, you've heard the many 7 examples, and I'll just note that Facebook runs a 8 transportation program that really rivals some of the 9 best transit agencies, entire agencies to reduce single 10 car trips to their campus.</p> <p>11 They have one of the best rates of 12 alternative commuting, about fifty percent as you heard 13 from the prior speaker, that's getting up to Stanford 14 campus levels and quite a big effort, helping employees 15 bike, take public transportation with shuttles, employing 16 their own shuttles, and of course resources for ride 17 sharing and incentives for those.</p> <p>18 Facebook has also actively been helping to 19 develop new alternatives, including support for the 20 Dumbarton rail corridor, as you heard, with about one 21 million dollars supporting that development effort.</p> <p>22 So that's noteworthy, and several million 23 dollars more to support safe local bike paths, such as 24 the most recent development on Chilco Street that 25 benefits not just their own employees, but the community,</p> <p style="text-align: right;">Page 43</p>	<p>1 incredibly short amount of time between the EIR for the 2 Facebook expansion project as well as with the M-2 area 3 rezoning.</p> <p>4 We're talking about thousands and 5 thousands of pages of technical documents with two 6 concurrent 45-day comment periods.</p> <p>7 My office is still reviewing the 8 documents, so our comments are just preliminary. We 9 imagine for other members of the public who don't have 10 paid staff working on this that that also may be a little 11 bit difficult. So we just wanted to register that 12 concern.</p> <p>13 The main substantive comment that I wanted 14 to make as kind of a preliminary note on the EIR 15 specifically for the Facebook expansion project is 16 similar to a comment that was made by a gentleman 17 earlier, which is that we're -- we're a little bit 18 perplexed by the assertion that the project will not have 19 any impact on the displacement of people.</p> <p>20 The way that I have understood the 21 analysis -- based on our preliminary read -- is that 22 there seems to be a sense that since we're not actually 23 bulldozing a physical house, we don't need to worry about 24 the displacement of people.</p> <p>25 And we think that that type of analysis is</p> <p style="text-align: right;">Page 45</p>

<p>1 a little bit missing the point.</p> <p>2 In some of the documents associated with</p> <p>3 the EIR, there is an acknowledgement that this sort of</p> <p>4 project could increase land values, could have the -- the</p> <p>5 effect of actually increasing rents.</p> <p>6 Certainly adding a lot more jobs to the</p> <p>7 region without a concomitant increase in housing supply</p> <p>8 on the same timeframe is likely to have an upward</p> <p>9 pressure in housing prices, which are already crushing,</p> <p>10 as we heard from some of the other members of the public</p> <p>11 earlier.</p> <p>12 We would like to see the EIR in its final</p> <p>13 form take a more robust analysis on the displacement of</p> <p>14 not just physical housing units, but of people themselves</p> <p>15 through the increase in land values and the increase in</p> <p>16 rents.</p> <p>17 We think in order for the Commission and</p> <p>18 the City Council to adequately understand the impacts of</p> <p>19 this project, we need to have a more thorough analysis of</p> <p>20 the way that this project is actually going to impact</p> <p>21 people who are living in the City of Menlo Park as well</p> <p>22 as really across the broader region due to the increasing</p> <p>23 costs of housing.</p> <p>24 Thank you very much.</p> <p>25 COMMISSIONER RIGGS: Thank you.</p> <p style="text-align: right;">Page 46</p>	<p>1 But had the ABAG data been shown out to</p> <p>2 2040, it actually shows that it improves by 2040 back to</p> <p>3 what it was in 2015.</p> <p>4 So if -- if this isn't addressed, it will</p> <p>5 get worse, and Menlo Park doesn't have a great record.</p> <p>6 We got sued for a Housing Element not being up to speed,</p> <p>7 and I looked back at the 1994 General Plan that forecast</p> <p>8 by the year 2010, we would have more than 15,000 dwelling</p> <p>9 units.</p> <p>10 We only have 13,100 in the year 2016, so</p> <p>11 we're way short of what we thought we were going to do</p> <p>12 before, and I think this is a fairly consistent thing.</p> <p>13 The EIR -- Draft EIR suggests that because</p> <p>14 seven percent or something like that of Facebook</p> <p>15 employees live in Menlo Park, that that hundred percent</p> <p>16 will continue.</p> <p>17 Well, there's a lot of competition. It's</p> <p>18 almost like saying a high school class that has seven</p> <p>19 percent of its students go to Stanford, that the next</p> <p>20 class or the year -- class four years later is going to</p> <p>21 be able to have seven percent go to Stanford is kind of</p> <p>22 ludicrous. There's a lot of competition for housing.</p> <p>23 There's a lot of growth in this area.</p> <p>24 Santa Clara County and Menlo and -- excuse me. And San</p> <p>25 Mateo Counties have -- have the worst jobs/housing ratio</p> <p style="text-align: right;">Page 48</p>
<p>1 And I -- I hesitated to say this, but we</p> <p>2 prefer if you withhold your applause or in the case may</p> <p>3 be of boos just so that speakers feel more comfortable</p> <p>4 that they can be frank in what they say, even though you</p> <p>5 probably are preaching to the choir tonight.</p> <p>6 Terri, welcome, and our next speaker would</p> <p>7 be Terri -- oh, I'm going to ruin this one. Epidendio.</p> <p>8 MS. FRY: Thank you.</p> <p>9 I also just have a few preliminary</p> <p>10 comments, and I'll put more detail into writing in this</p> <p>11 very short time we have to comment.</p> <p>12 Facebook has been a wonderful addition to</p> <p>13 Menlo Park and -- and a great thing, so my comments right</p> <p>14 now are more oriented towards the possibility that --</p> <p>15 that there could be a negotiation in how to address some</p> <p>16 of the perplexing issues that others have raised tonight.</p> <p>17 There will be a development agreement</p> <p>18 created with Facebook, and so I think some of these</p> <p>19 issues could be addressed through that process.</p> <p>20 Housing is an issue. I am also very</p> <p>21 perplexed that there's a conclusion there isn't an issue.</p> <p>22 What's interesting is that the EIR only</p> <p>23 projects out to the year 2020 and sort of dismisses that</p> <p>24 there is a problem because ABAG has pro -- forecast that</p> <p>25 our jobs/housing ratio will get worse.</p> <p style="text-align: right;">Page 47</p>	<p>1 of many counties. I think they're at the bottom of about</p> <p>2 fifty counties in this state.</p> <p>3 So I think it's something that needs to be</p> <p>4 addressed, and perhaps that's something that there could</p> <p>5 be a good partnership working with Facebook to figure out</p> <p>6 a way, because this project does not add any housing at</p> <p>7 all.</p> <p>8 In terms of traffic, there is a conclusion</p> <p>9 that the vehicle miles traveled would continue to be low,</p> <p>10 and the comparison is to regional traffic, but shouldn't</p> <p>11 it be compared to what Menlo Park's traffic is?</p> <p>12 And if there isn't housing nearby, that</p> <p>13 means people are going to have to commute. So that means</p> <p>14 that the commute's going to be long.</p> <p>15 So some of this logic doesn't totally make</p> <p>16 sense. The mit --</p> <p>17 COMMISSIONER RIGGS: I'm sorry, Patti.</p> <p>18 Would you -- would you wrap up here, please?</p> <p>19 MS. FRY: Oh, I'm sorry. There's no way</p> <p>20 to tell how long we're taking here.</p> <p>21 I guess -- could I speak to the FIA in the</p> <p>22 study session, then?</p> <p>23 COMMISSIONER RIGGS: Yes.</p> <p>24 MS. FRY: Okay. So I will -- I will do</p> <p>25 that.</p> <p style="text-align: right;">Page 49</p>

<p>1 So the last thing is about water. When 2 the Menlo Gateway DEIR came about in 2010, it concluded 3 that there would be a water shortage in dry years. 4 We've had a number of dry years. I don't 5 know why this one concludes there is no issue. 6 Thank you. 7 COMMISSIONER RIGGS: Thank you, Patti. 8 Next up is Terri Epidendio followed by 9 Janelle London. 10 MR. PERATA: So through the chair, if I 11 can jump in. Terri requested that her card be withdrawn. 12 COMMISSIONER RIGGS: Thank you. 13 So Janelle followed by Rachel Scheuring. 14 MS. LONDON: Thank you, Commissioners. 15 I'm Janelle London. I am a new member of the 16 Environmental Quality Commission for Menlo Park, but 17 tonight I'm speaking as an individual. 18 First I wanted to thank Facebook for being 19 such a fantastic community partner. There's probably not 20 a person in this room who hasn't benefited from a 21 Facebook sponsored event, project, perk or job. 22 We're lucky to have such a generous, 23 caring forward-thinking company in Menlo Park. 24 The issue I'd like to raise tonight 25 relates to the potential impact on greenhouse gas</p> <p style="text-align: right;">Page 50</p>	<p>1 binding on state agencies, and it's only a matter of time 2 before they're binding on local governments and private 3 entitles, as well. 4 Menlo Park Climate Action Plan calls for a 5 27 percent emissions reduction from 2005 levels by 2020. 6 The State of California also requires that 7 33 percent of electricity come from renewable sources by 8 2020 and fifty percent by 2030, and for new construction, 9 the state requires that new residential buildings have to 10 be zero net energy by 2020 and new commercial buildings 11 have to be zero net energy by 2030, and fifty percent of 12 existing buildings have to be retrofitted to be zero net 13 energy by 2030. 14 So it's not acceptable to just throw up 15 our hands and say, "Eh, development. It increases 16 greenhouse gas emissions. What are you going to do? We 17 can do better. 18 Facebook is perfectly poised to make this 19 project a net emissions reducer. This would further 20 establish it as a leader in sustainability as well as one 21 of the best neighbors our city could possibly have. 22 Luckily and unluckily, greenhouse gas 23 emissions are global. Thus reducing greenhouse gas 24 emissions anywhere in the world would have the effect of 25 mitigating the impact on this pro -- of this project.</p> <p style="text-align: right;">Page 52</p>
<p>1 emissions. 2 The EIR states that the project would 3 conflict with an applicable plan, policy or regulation 4 adopted for the purpose of reducing emissions of 5 greenhouse gases, and this would be significant and 6 unavoidable. 7 What this statement suggests to me is that 8 development requires a net increase in greenhouse gas 9 emissions. If we want development, we'll just have to 10 take our carbon emission lumps. 11 Respectfully, I disagree with that 12 statement. We can have it all, both development and a 13 reduction in greenhouse gas emissions, and we have to 14 have it all. 15 The population of the Bay Area and indeed 16 the whole planet is growing. Development has to happen. 17 If it doesn't happen in Menlo Park, it will happen in 18 neighboring cities and we'll still feel the impact, but 19 we won't have a say in how it happens and we won't reap 20 the benefits of a more vibrant, prosperous community. So 21 no growth is not an option. 22 And we have to reduce greenhouse gas 23 emissions. The State of California calls for reducing 24 emissions to 1990 levels by 2020 and to forty percent 25 below 1990 levels by 2030. These orders are legally</p> <p style="text-align: right;">Page 51</p>	<p>1 Luckily we don't even have to go that far. 2 There's a virtual smorgasbord of options to reduce the 3 net impacts of the greenhouse gas emissions resulting 4 from this development right here in Menlo Park. 5 Here are a few ideas of actions Facebook 6 could commit to as part of this project to reduce 7 emissions. 8 Menlo Park and all of San Mateo County 9 will be purchasing electricity from Peninsula Clean 10 Energy beginning this fall. There will be an option to 11 pay a small premium, currently set at I believe one cent 12 per kilowatt hour, to choose electricity from one hundred 13 percent clean renewable sources. 14 Facebook could commit to select the one 15 hundred percent clean option. It could also unleash the 16 mighty power of its social network to launch a campaign 17 to encourage other Menlo Park businesses and residents to 18 also choose the one hundred percent clean option. 19 And if it wanted to go even further, it 20 could offer to pay the one cent per kilowatt house 21 premium for other businesses and residents to get their 22 electricity from one hundred percent clean sources. 23 Facebook has already sponsored, as was 24 mentioned here tonight, the installation of solar panels 25 on ten low income homes in Menlo Park. It could sponsor</p> <p style="text-align: right;">Page 53</p>

<p>1 additional home solar systems, and some on local non-</p> <p>2 profit buildings, as well, such as house of worship.</p> <p>3 COMMISSIONER RIGGS: Janelle, would you</p> <p>4 also -- would you wrap up?</p> <p>5 MS. LONDON: Yes, I will.</p> <p>6 COMMISSIONER RIGGS: Thank you.</p> <p>7 MS. LONDON: It could sponsor solar</p> <p>8 covered parking lots in public places, and there's lots</p> <p>9 of older homes and offices in Menlo Park that are not</p> <p>10 energy efficient. Facebook could partner with a</p> <p>11 non-profit to identify those zones and provide energy</p> <p>12 efficient solutions.</p> <p>13 And it could launch a social media</p> <p>14 campaign to encourage and incentivize those who live in</p> <p>15 Menlo Park to do alternative methods of transportation.</p> <p>16 I see that strategies similar to some of</p> <p>17 these were rejected in the EIR on grounds that they're</p> <p>18 not applicable to local development because they are</p> <p>19 City-sponsored program designated for further study.</p> <p>20 COMMISSIONER RIGGS: Janelle, why -- why</p> <p>21 don't we close --</p> <p>22 MS. LONDON: Final sentence.</p> <p>23 COMMISSIONER RIGGS: -- with that, and</p> <p>24 then --</p> <p>25 MS. LONDON: Okay.</p> <p style="text-align: right;">Page 54</p>	<p>1 problem.</p> <p>2 Because just because you don't see it</p> <p>3 doesn't mean it's not there, and I can tell you living</p> <p>4 on -- in Suburban Park and witnessing the traffic growth</p> <p>5 of the last couple of years, it is really significant.</p> <p>6 And if we see more students coming over</p> <p>7 from that area around Facebook to go to the high school</p> <p>8 or to go to Menlo Park City School Districts, it's only</p> <p>9 going to get worse.</p> <p>10 So I would encourage the Planning</p> <p>11 Commission to revise the traffic study to incorporate the</p> <p>12 Bay Road and the Bay Road/Ringwood intersection.</p> <p>13 Thank you.</p> <p>14 COMMISSIONER RIGGS: Thank you. So</p> <p>15 noted.</p> <p>16 Eileen followed by Pam Jones.</p> <p>17 Ms. McLAUGHLIN: Good evening. I'm</p> <p>18 Eileen McLaughlin representing the Citizen's Committee to</p> <p>19 Complete the Refuge.</p> <p>20 I am here -- what we do is we work with</p> <p>21 the Don Edwards National Wildlife Refuge, all of those</p> <p>22 salt ponds that line the shoreline, and that's a neighbor</p> <p>23 to Facebook.</p> <p>24 That area, as you may know, has also --</p> <p>25 is home for the Salt Pond Restoration Project, which</p> <p style="text-align: right;">Page 56</p>
<p>1 COMMISSIONER RIGGS: -- you'll have a</p> <p>2 chance to speak to the General Plan, which might be more</p> <p>3 appropriate.</p> <p>4 MS. LONDON: Very well. Thanks very</p> <p>5 much.</p> <p>6 COMMISSIONER RIGGS: So next would be</p> <p>7 Rachel Scheuring followed by Eileen McLaughlin.</p> <p>8 MS. SCHEURING: I'm Rachel Scheuring.</p> <p>9 I'm a resident of Suburban Park, which is a neighborhood</p> <p>10 along Bay Road, and my comment is not so much about</p> <p>11 whether Facebook has good intentions or not, because I'm</p> <p>12 sure they do, but rather about the adequacy of the</p> <p>13 traffic study.</p> <p>14 My eyeglasses are not that good and the</p> <p>15 graphic is kind of small, so forgive me if I'm missing</p> <p>16 something, but looking at the transportation study, it</p> <p>17 looks as if the Bay Road/Ringwood intersection is not</p> <p>18 included in the transportation study.</p> <p>19 For people not familiar with that</p> <p>20 intersection, that intersection's very close to two</p> <p>21 schools, and it is significantly impacted in the morning</p> <p>22 as people commute, either to go through town or to go to</p> <p>23 the schools, and I think you cannot make an adequate</p> <p>24 judge of the impact of what having more employees, more</p> <p>25 workers, more traffic will do without looking at the</p> <p style="text-align: right;">Page 55</p>	<p>1 they just completed their Phase II EIR, which will</p> <p>2 restore those ponds and add futures like new trails and</p> <p>3 access.</p> <p>4 I come here tonight following up on a</p> <p>5 scoping letter that we sent, and I plan to send another</p> <p>6 letter regarding comments here.</p> <p>7 I wanted to bring one aspect to your</p> <p>8 attention here which has transpired since we sent in our</p> <p>9 scoping letter.</p> <p>10 We've put in concerns about the impacts of</p> <p>11 the pedestrian/bike bridge on the wildlife values of</p> <p>12 the refuge, because the bridge would abut that refuge as</p> <p>13 it came to the east end.</p> <p>14 Facebook turned around and reached out as</p> <p>15 we sug -- we asked them to, to the Don Edward Refuge,</p> <p>16 brought them in to talk about what the issues were, what</p> <p>17 could be done to avoid impacts to the wildlife values and</p> <p>18 the habitats of those lands, and they I understand had</p> <p>19 multiple meetings.</p> <p>20 One meeting I was brought in as well as</p> <p>21 someone from -- representing Audubon to also comment, and</p> <p>22 I wanted to say I appreciate that Facebook would look</p> <p>23 toward what they could be in the design of that bridge to</p> <p>24 both protect wildlife and embellish it to make it a</p> <p>25 welcoming place toward the refuge.</p> <p style="text-align: right;">Page 57</p>

<p>1 So I -- this has been a very positive 2 thing to comments that we've submitted. I can't speak 3 for the refuge, but certainly the citizen's committee is 4 very pleased at that turnabout.</p> <p>5 I also want to say that the bridge should 6 not be just described as a pedestrian and bike through- 7 way for transit.</p> <p>8 It will provide a new safe access for the 9 community of Belle Haven and for anyone, employees of 10 many of the businesses there or even families that might 11 bicycle from western Menlo Park across bridge access to 12 go visit the refuge and the bayfront -- Bedwell Bayfront 13 Park Open Space in that adjoining area.</p> <p>14 It brings new -- new opening to access 15 that those lands should provide and invite, and it's -- 16 it's a very welcoming change to the shoreline.</p> <p>17 So I thank you. I just wanted to let you 18 know that communication works.</p> <p>19 COMMISSIONER RIGGS: All right. Thank 20 you.</p> <p>21 Pam Jones, followed by Rose Bickerstaff.</p> <p>22 MS. JONES: I would like to thank 23 Facebook. I live in Belle Haven, three blocks from your 24 newest building, and other than removing my view of the 25 East Bay, I can say you attempt to be really good</p> <p style="text-align: right;">Page 58</p>	<p>1 being priced out of our community, and we just -- we just 2 keep that kind of quiet, you know, and there's a lot of 3 homes being bought in the community that -- and as soon 4 as they do, they raise the rent on it.</p> <p>5 There's also people that have done 6 remodels or, you know, some project on their house that 7 maybe they didn't understand the code or all the things 8 that they had to do in order to have that approved, and 9 so they lost their property.</p> <p>10 So there's all of these other things that 11 do go on. You know, our children play dodge car when 12 they're trying to get across some of the roads in our 13 community.</p> <p>14 One of the reports that you had actually 15 said to have the traffic -- the through-traffic to go 16 down Chilco through Menlo Park. Well, it already does 17 that, which is past a school and homes and so forth.</p> <p>18 So I think again I'd like to urge you 19 to -- to extend the comment period in order to give you 20 ample time to make sure that all of the information is in 21 there so that you can benefit all of Menlo Park.</p> <p>22 COMMISSIONER RIGGS: Thank you.</p> <p>23 And I can't resist noting that 24 Commissioners who generally have full-time jobs actually 25 did read those two nice big books.</p> <p style="text-align: right;">Page 60</p>
<p>1 neighbors, and I think that's more important than being 2 able to do all of the right things because nobody can do 3 all of the right things.</p> <p>4 I think it's unfortunate on the one hand, 5 although it does take a disrupter to come in and show us 6 the things that we should have been doing long before you 7 came here.</p> <p>8 So many of the issues around housing, 9 traffic, clean air, those were already problems long 10 before you came here that failed to be addressed by the 11 City of Menlo Park.</p> <p>12 I do want to re -- formally request 13 that -- and this is the Quality, California Environment 14 Quality Act. The public review period for a Draft EIR 15 should not be less than thirty days nor longer than sixty 16 days except in unusual circumstances, and this is under 17 Guidelines 151.05.</p> <p>18 I would suggest that there are unusual 19 circumstances since there's some additional questions 20 that need to be asked, and I think the category that gets 21 missed when it comes to our reports is we never talk 22 about the human environment, which would then -- if that 23 was a category in itself, then we would address the 24 housing.</p> <p>25 Because we know -- you know that we're</p> <p style="text-align: right;">Page 59</p>	<p>1 Rose, welcome.</p> <p>2 MS. ROSE BICKERSTAFF: Thank you.</p> <p>3 COMMISSIONER RIGGS: Rachel is speaking 4 after you.</p> <p>5 MS. ROSE BICKERSTAFF: Well, I was just 6 sitting there listening to all of the positive comments 7 for Facebook, and I was thinking that we are here talking 8 about environmental impact, and that is something that 9 happens no matter what we do.</p> <p>10 I don't care if we add a family member. 11 It's going to be some kind of impact, although this is on 12 a bigger scale.</p> <p>13 But either we want development, which can 14 be positive or negative impact, and so far with Facebook, 15 it's been a positive impact.</p> <p>16 Now that may not sound right to some -- 17 some of us, but it has been.</p> <p>18 And what I am thinking, the things that we 19 are concerned about certainly was here long before 20 Facebook.</p> <p>21 We have been talking about traffic, cut- 22 through traffic for quite a few years, and sometimes we 23 are not proactive.</p> <p>24 We should have been addressing this long 25 before Facebook and we wouldn't be having this strong</p> <p style="text-align: right;">Page 61</p>

<p>1 conversation like we're having tonight and we've been 2 having all for the past several years.</p> <p>3 But -- and then we go on to talk about 4 housing. I think first we have to identify who's been -- 5 who's being displaced, and I don't think we have done 6 that.</p> <p>7 People aren't selling and moving out of 8 their homes. They aren't being forced out of their 9 homes. So we have to find out who's being displaced, and 10 if it's a tenant, then we are dealing with absentee 11 owners.</p> <p>12 Maybe there can be something worked out to 13 offset -- a tax or some type of -- I should say waiver or 14 tax situation that we can work out with people who own 15 property that tenants finding themselves with high rent.</p> <p>16 But housing -- this little community can't 17 fix all the housing. So what is the answer? Should we 18 build houses and sell them at -- below market? Should we 19 build some apartment buildings, have the rent lowered? 20 What can we do?</p> <p>21 This is a regional situation, not just 22 Menlo Park, and this will push that neighborhood.</p> <p>23 But I think Facebook, if they were, I 24 should say, reckless developer, I would be wary, but they 25 have been concerned developers, and when you have someone</p> <p style="text-align: right;">Page 62</p>	<p>1 It's like everyone else has said during the comment time, 2 it's something that Facebook can't fix. It's a regional 3 problem that needs to be addressed regionally, and they 4 will do the best they can as far as helping with the 5 study, with the Dumbarton corridor.</p> <p>6 That's their way of trying to alleviate 7 the traffic, and I appreciate Facebook for all that they 8 do in our community.</p> <p>9 They've done a lot, and some that have 10 been expressed tonight and some things that -- that are 11 done behind the scenes, because they're that type -- type 12 of company that just do because it's the right thing to 13 do.</p> <p>14 Thank you.</p> <p>15 COMMISSIONER RIGGS: Thank you, Rachel. 16 Johnny Cruz followed by Monica Ivanski.</p> <p>17 MR. CRUZ: Good evening, Councilmembers 18 and all the participant at this meeting this evening. I 19 would like to speak as a Belle Haven homeowner.</p> <p>20 Two things. First, I would like to thank 21 Facebook for what they're doing for our community in less 22 than two years.</p> <p>23 I am resident of Belle Haven in June 23, 24 six years ago, and before moving from Burlingame to Belle 25 Haven, I didn't really realize the positive impact of --</p> <p style="text-align: right;">Page 64</p>
<p>1 really concerned about the community, you don't have to 2 worry about it.</p> <p>3 They will do whatever's necessary to make 4 sure within their power that people aren't displaced and 5 communities is left intact.</p> <p>6 So I'm not worried about Facebook because 7 they've been the best neighbor that any community could have.</p> <p>8 Thank you.</p> <p>9 COMMISSIONER RIGGS: Thank you, Rose. 10 And Rachel Bickerstaff followed by Johnny 11 Cruz.</p> <p>12 MS. RACHEL BICKERSTAFF: Good evening, 13 Council -- Planning Commission, sorry. Wrong meeting. I 14 hate that we're all here on a beautiful summer evening 15 addressing issues that we all didn't have time to really 16 address with the amount of pages that each EIR contains.</p> <p>17 So my comments are on Facebook. Knowing 18 that I didn't have time to look through the entire EIR, I 19 support and trust that they will do the right thing 20 because we've been involved with Facebook from day one 21 with all of -- with them consulting with the neighborhood 22 about what we want and would like to see, and they have 23 accommodated our wants and also our dislikes.</p> <p>24 They know about the traffic. There's 25 nothing that the company itself can do about the traffic.</p> <p style="text-align: right;">Page 63</p>	<p>1 the positive impact on the neighborhood like Facebook.</p> <p>2 I want to say thank you to Facebook to 3 paying to Caltrain one million dollars to remove the 4 ballast next to the -- the Beechwood School, which was 5 the benefit for the help of the children.</p> <p>6 Thank you for start implementing the 7 pedestrian and the bicycle path in Chilco. That's 8 important, because a few year ago, we had tragic accident 9 there, caused the life of two member of our community.</p> <p>10 Thank you very much for start this point 11 and please continue to finish the second part.</p> <p>12 Thank you, Facebook for participate in all 13 activity of the community and hear what we speak up, what 14 we need.</p> <p>15 Thank you for the donations of a thousand 16 laptop to our childrens, and being willing to give to the 17 eighth grade children around the district -- school 18 district.</p> <p>19 And the second point I am here, I would 20 like to ask Facebook and the City of Menlo Park continue 21 with this project because it benefits the community, help 22 the jobs, the revenue for the City, the revenue for the 23 Belle Haven, and please, no stop.</p> <p>24 The only consideration my asking Facebook 25 goes to the maxim to find all the necessary mitigation</p> <p style="text-align: right;">Page 65</p>

<p>1 resources in order to minimize all unavoidable impact 2 should it come from this project.</p> <p>3 Please put all the effort, professional 4 input, a cumulative effort to find out and minimize the 5 land use, transportation, air quality, climate resources, 6 biomedical resources, hazard material, population and 7 housing and public service.</p> <p>8 Please, I thank you very much.</p> <p>9 COMMISSIONER RIGGS: Thank you.</p> <p>10 And our last speaker tonight on the 11 Facebook EIR is Monica Ivanski.</p> <p>12 MS. IVANSKI: Good evening. My name is 13 Monica Ivanski and I have been living here in Belle Haven 14 for six years, and I notice a big change in your 15 community.</p> <p>16 Facebook has already contributed in many 17 ways to the neighborhood in all the project -- projects 18 have been proposed and have been done by Facebook have 19 been a big improvement to the community.</p> <p>20 Example, under the Bayfront Expray -- 21 Expressway is my favorite way to go and walk and run and 22 take my dog. I feel safe now.</p> <p>23 I appreciate all the support from Facebook 24 to our community garden in Belle Haven. I'm really glad 25 with our Sunday farmers market. They are really open to</p> <p style="text-align: right;">Page 66</p>	<p>1 thank everyone for coming out tonight and making some 2 very good comments.</p> <p>3 I think the first question of staff is 4 it's been -- this whole issue of economic impact on the 5 housing stock and on the Belle Haven neighborhood.</p> <p>6 We do -- we were given a report by BA 7 Urban Economics with regard to this, but that -- I'm 8 assuming that that's all part of the -- of the public 9 benefit discussion in regards to the development or would 10 that be part of the EIR, as well?</p> <p>11 MR. PERATA: So with regard to the Fiscal 12 Impact Analysis, comments or questions on that are 13 probably more project related than environmental CEQA 14 related. So we'll sort of hold those for the study 15 session component.</p> <p>16 COMMISSIONER ONKEN: Right. So if there 17 was to be an extended study on the impact on housing in 18 the area and -- and local economics, would that -- where 19 would that fit in the EIR? Or how would we -- how do we 20 dovetail it in?</p> <p>21 MS. EFNER: Good evening. My name is 22 Erin Efner. I'm with ICF International. We prepared the 23 EIR.</p> <p>24 So the economic analysis of housing 25 wouldn't necessarily be disclosed in the EIR, but we do</p> <p style="text-align: right;">Page 68</p>
<p>1 listen to the community.</p> <p>2 I personally had opportunity to speak with 3 them and give some suggestions, and they were really open 4 to listening.</p> <p>5 And the bike and pedestrian path 6 installation on Chilco looks amazing. I know it's not 7 done yet, but it looks amazing and safe.</p> <p>8 That -- as Johnny said, the ballast rocks 9 along Chilco Street we have been fighting for many years 10 to remove them. Really dangerous for the children, and 11 Facebook is working with samTrans -- samTrans positive 12 permanently remove those rocks.</p> <p>13 And finally, Facebook has built a positive 14 relationship with residents of the Belle Haven 15 neighborhood and they are really good neighborhood for 16 our community.</p> <p>17 Thank you.</p> <p>18 COMMISSIONER RIGGS: Thank you.</p> <p>19 And with that, having no more cards, I 20 will close the public comment period on item H1. This, 21 then, would be the period when Commissioners can ask 22 questions of staff and consultant regarding the content 23 of the EIR.</p> <p>24 Commissioner Onken.</p> <p>25 COMMISSIONER ONKEN: Thank you, and I</p> <p style="text-align: right;">Page 67</p>	<p>1 disclose growth inducement in Chapter 4 where we talk 2 about the induced housing and -- on areas outside the 3 City of Menlo Park and we go through each of the CEQA 4 topics and sort of give a high-level overview of how -- 5 how induced housing -- how changes to the jobs/housing 6 ratio could affect all sort of these other CEQA topics.</p> <p>7 So to answer your question, specifically 8 we wouldn't get into an economic analysis, but we do 9 address economics sort of in the Chapter 4 of the EIR.</p> <p>10 COMMISSIONER ONKEN: Okay. Thank you.</p> <p>11 MR. PERATA: So for the Chair --</p> <p>12 COMMISSIONER RIGGS: Yes.</p> <p>13 MR. PERATA: -- if I can just kind of --</p> <p>14 COMMISSIONER RIGGS: Kyle.</p> <p>15 MR. PERATA: -- follow up on that 16 further.</p> <p>17 So to Commissioner Onken's point, we are 18 working on displacement analysis that should be released 19 with -- or excuse me. Will be released with the staff 20 report for the Housing Commission meeting, which is on 21 June 29th.</p> <p>22 So the staff report will be later this 23 week. So that staff report will summarize the 24 displacement analysis that we're working on.</p> <p>25 COMMISSIONER RIGGS: Thank you. That's</p> <p style="text-align: right;">Page 69</p>

<p>1 very helpful.</p> <p>2 The speakers tonight reflect -- I think a</p> <p>3 reaction may be more than one of us had that housing, by</p> <p>4 not being part of the EIR, there was not a mitigation</p> <p>5 suggested and -- or I should say not being found to be a</p> <p>6 significant impact, there was not a mitigation suggested.</p> <p>7 Potentially the Housing Commission would</p> <p>8 make a recommendation regarding the EIR content?</p> <p>9 MR. PERATA: So in terms of</p> <p>10 recommendations, the Housing Commission will get an</p> <p>11 overview of the Draft EIR. They won't be -- won't be</p> <p>12 taking public comments at that meeting like we are today</p> <p>13 at this meeting.</p> <p>14 So they're -- we're giving them an</p> <p>15 overview of the content of the Draft EIR as relates to</p> <p>16 population and housing, as well as the displacement</p> <p>17 analysis once it's finalized.</p> <p>18 And the BMR agreement will be on that</p> <p>19 agenda, as well, for their recommendation on that item</p> <p>20 specifically.</p> <p>21 The other two are more informational for</p> <p>22 background and comments can be submitted separately</p> <p>23 before July 11th at 5:30 PM.</p> <p>24 COMMISSIONER RIGGS: So what I'm asking</p> <p>25 for this Commission's benefit tonight is whether the</p> <p style="text-align: right;">Page 70</p>	<p>1 Do you know if that has been included in</p> <p>2 this -- in the analysis or not? I don't see our traffic</p> <p>3 consultant's still here. There he is.</p> <p>4 MR. BURGETT: That particular</p> <p>5 intersection was not one of the 66 studied intersections.</p> <p>6 COMMISSIONER KAHLE: Okay. Thank you.</p> <p>7 And the last thing I want to ask about</p> <p>8 is -- I probably missed this in the -- in the pages, but</p> <p>9 is there anything that addresses any new park space in</p> <p>10 the EIR?</p> <p>11 MR. PERATA: The -- the public services</p> <p>12 analysis does address potential impacts to parks.</p> <p>13 COMMISSIONER KAHLE: How about</p> <p>14 proposing -- proposing new parks?</p> <p>15 MR. PERATA: No. That -- well, the</p> <p>16 impact is less than significant.</p> <p>17 COMMISSIONER KAHLE: Okay. All right.</p> <p>18 Thank you.</p> <p>19 COMMISSIONER RIGGS: I don't see any</p> <p>20 lights. I do have -- you know, I'm sorry, Andrew. Your</p> <p>21 light for some reason isn't as bright at my angle as it</p> <p>22 should be.</p> <p>23 COMMISSIONER BARNES: Question as it</p> <p>24 relates to the intersections and the scoping of traffic</p> <p>25 impacts. We see that the intersections are called out in</p> <p style="text-align: right;">Page 72</p>
<p>1 Housing Commission would be making recommendations?</p> <p>2 If it's an information item for them only,</p> <p>3 then perhaps it's appropriate for us to make</p> <p>4 recommendations here tonight.</p> <p>5 MR. PERATA: Yeah. That -- that would be</p> <p>6 correct.</p> <p>7 COMMISSIONER RIGGS: All right. Thank</p> <p>8 you.</p> <p>9 Seeing no other lights at the moment --</p> <p>10 oh, yes.</p> <p>11 COMMISSIONER KAHLE: Thank you.</p> <p>12 I have just two straightforward questions.</p> <p>13 One is, when we're talking about the housing and having a</p> <p>14 less than significant im -- impact, does that assume that</p> <p>15 the existing buildings there now are occupied?</p> <p>16 MR. PERATA: So for CEQA purposes, the</p> <p>17 existing population on the site is assumed to be zero.</p> <p>18 COMMISSIONER KAHLE: So --</p> <p>19 MR. PERATA: The existing buildings to be</p> <p>20 demolished are -- so there wouldn't be any employees</p> <p>21 assumed on the site for CEQA analysis purposes.</p> <p>22 COMMISSIONER KAHLE: Okay. And the</p> <p>23 second question is a traffic just to follow up on the --</p> <p>24 on the question about the intersection of Bay Road and</p> <p>25 Ringwood.</p> <p style="text-align: right;">Page 71</p>	<p>1 traffic flows through them.</p> <p>2 I'm certain -- I live in The willows and</p> <p>3 I'm constantly hearing about overflow traffic which may</p> <p>4 not be specifically on Willow. We all know how</p> <p>5 challenging Willow can be from Middlefield on down.</p> <p>6 And also there's -- there's a great deal</p> <p>7 of traffic which gets for instance off of Woodland and</p> <p>8 flows through the neighborhoods seeking ways to get back</p> <p>9 on to Willow.</p> <p>10 Is there a mechanism in the EIR to look at</p> <p>11 what happens with that overflow traffic and where it</p> <p>12 disperses through the particular neighborhoods that may</p> <p>13 not be at those specific intersections, but tries to find</p> <p>14 a home and climb back, for instance, on to Willow?</p> <p>15 MR. BURGETT: To some degree the -- the</p> <p>16 EIR does take a look at that on quite a few streets.</p> <p>17 There's -- there are streets that are identified as</p> <p>18 including the study segments. There's about 87 study</p> <p>19 segments.</p> <p>20 And so for -- for those streets, there are</p> <p>21 estimates of -- of the increase in daily traffic, and</p> <p>22 the -- the traffic then study does assume that there's</p> <p>23 going to be an increase in cut-through traffic on key</p> <p>24 routes from, for example, Willow cutting through the</p> <p>25 Chilco, that there is the potential for essentially about</p> <p style="text-align: right;">Page 73</p>

<p>1 two or three primary cut-through routes, you know, based 2 on where the intersections are on Willow where people 3 would -- would cut through. 4 So in terms of additional cut-through 5 routes, I -- I think the study hits the -- hits the main 6 ones, and probably, you know, to the extent that there 7 would be additional cut-through routes that wouldn't 8 involve, you know, one of the key intersections on 9 Willow, I sus -- I suspect the actual volume would be 10 relatively low. 11 You know, for example in -- on the East 12 Palo Alto side, there's -- there's a few measures already 13 in place to discourage cut-through traffic. 14 There's -- if you're leaving some of these 15 streets, you actually come to an intersection and you see 16 a sign that says: "Right turn prohibited," you know, 17 "3:00 to 5:00 PM." 18 Cutting through the -- the neighborhood on 19 the East Palo Alto side, there's al -- there's already 20 some mechanisms in place to displace that. 21 And I suspect on the west side of the 22 freeway, would you include that in your question? I 23 mean -- 24 COMMISSIONER BARNES: Yes, 25 specifically -- and I apologize if I'm not being more</p> <p style="text-align: right;">Page 74</p>	<p>1 sophisticated I would call travel demand model that's 2 used to analyze the traffic patterns in order to assess 3 whether or not diversion to local neighborhoods would 4 occur when congestion builds some of the arterial 5 streets. 6 In particular, Willow, Bayfront Expressway 7 and Marsh Road are the three areas where we'd 8 anticipate -- we're already seeing cut-through traffic 9 occur, and we'd anticipate with additional traffic 10 growth, those would be most likely locations for 11 additional cut-through in the neighborhood. 12 And so in The Willows specifically, we did 13 see in the scenarios without ConnectMenlo, in particular 14 the housing growth in place, that there is additional 15 cut-through traffic that uses Woodland, and we see 16 impacts at the University and Woodland intersection in 17 East Palo Alto that go away once you introduce housing 18 through the ConnectMenlo process. 19 So I think that -- the overall cut-through 20 analysis did look at the effects through The Willows and 21 found without the additional housing located close to the 22 job center, that we would potentially see additional cut- 23 through traffic. 24 COMMISSIONER BARNES: And is that 25 quantified?</p> <p style="text-align: right;">Page 76</p>
<p>1 specific with geography. 2 It's not east of 101. It's west of 101. 3 COMMISSIONER BARNES: I see. There's a 4 great deal of cut-through emphasis in the Belle Haven 5 neighborhood as it relates to west of 101 where you would 6 have Woodland going -- between Woodland and where 101 is, 7 the neighborhood -- The Willows neighborhood, and a great 8 deal of traffic coming through there. 9 Am I to assume that it's not called out 10 here, it would -- it could be in the scope, but didn't 11 seem to be of import, or it may not have been looked at. 12 MS. NAGAYA: Good evening. Nikki Nagaya, 13 the transportation -- 14 COMMISSIONER RIGGS: Hi, Nikki. 15 MS. NAGAYA: -- manager for the City. 16 So we've looked a lot at cut-through 17 traffic in -- in the City, I so I wanted to jump in 18 and -- and beyond what's just in the Environmental Impact 19 Report, also talk a little bit about efforts to -- to 20 help with cut-through traffic. 21 So this document for Facebook as well as 22 the EIR that ConnectMenlo Project, which you'll see a 23 little bit later, is a different methodology for the 24 traffic analysis than we have used in the past. 25 And there's in this instance a very</p> <p style="text-align: right;">Page 75</p>	<p>1 MS. NAGAYA: Through the assessment of 2 the impact at the University and Woodland section it is. 3 The intersections along Woodland and the other 4 intersections in The Willows are either stop sign 5 controlled or unsignalized, and so you don't typically 6 see with the City's impact criteria the intersection 7 impacts showing what -- what we define as a -- an impact 8 in terms of traffic, but as Colin was describing, we do 9 analyze that daily roadway segment volumes on many 10 streets in The Willows, and those are quantified, as 11 well, as well as other neighborhoods, including Belle 12 Haven. 13 COMMISSIONER BARNES: Okay. Thank you. 14 COMMISSIONER RIGGS: Nikki, While you're 15 there, just following up on Commissioner Barnes' 16 question. 17 If housing is added in Menlo Park, are we 18 assuming or concluding that the ratio of Facebook 19 employees living in Menlo Park will increase from the 20 current -- I think I heard seven percent earlier tonight? 21 MS. NAGAYA: So I think it -- it's not 22 just necessarily reducing the length for -- for Facebook 23 trips, but that local housing does have the benefit of 24 being in a job rich area which spans Palo Alto to Redwood 25 City and even farther beyond that -- that area.</p> <p style="text-align: right;">Page 77</p>

<p>1 And so by placing housing in the -- the 2 M-2 area of Menlo Park, you -- you have the benefit of 3 reducing trips in the region. 4 They may not be destined for just 5 employers within the City. We haven't defined a 6 particular percentage in the model of the numbers of 7 trips that would be associated -- that housing would 8 generate for Facebook. 9 But overall, we see trip lengths reducing. 10 COMMISSIONER RIGGS: Yeah. I think I 11 follow, because that way, although they may affect 12 Bayfront Expressway and 101, they won't be using 13 necessarily Willow Road. 14 MS. NAGAYA: Or the Dumbarton Bridge, 15 yes. Absolutely. 16 COMMISSIONER RIGGS: Okay. Thank you. 17 Did you have a follow-up, Andrew? 18 COMMISSIONER BARNES: Unrelated topic. 19 Question. The gentleman from 20 transportation referred to trip counts and trip caps and 21 the ability to both monitor that and pull that in as 22 needed. 23 Could you educate me a little bit about 24 the mechanics of understanding trip counts, for instance, 25 as generated by projects such as Facebook and how it's</p> <p style="text-align: right;">Page 78</p>	<p>1 the -- Buildings 10 through 19, the former Sun campus, 2 there are two driveway access points, the main signal at 3 Bayfront and Willow, and then a secondary right-in/right- 4 out driveway access farther towards University Avenue. 5 And so both of those are monitored with 6 a -- a camera that doesn't store visual data, but stores 7 the number of activations, the number of times a car 8 passes the entrance point or leaves the -- the campus 9 every day. 10 COMMISSIONER BARNES: And then how do you 11 figure in the transportation -- transportation mechanisms 12 like the buses that Facebook run? How does that work 13 into the overall numbers? 14 MS. NAGAYA: So today the -- the buses 15 count as a standard vehicle trip. So whether there's one 16 person or forty on a bus, it counts as one vehicle. 17 If you have a single occupant vehicle 18 coming in and out also counts as -- as one vehicle 19 accessing the campus. 20 The only vehicles that are excluded are -- 21 are bikes because they don't have the same congestion 22 impacts as a vehicle or a bus would. And I believe 23 motorcycles are also excepted just because they're 24 generally the same size as a bike. 25 So from a detection standpoint, hard to</p> <p style="text-align: right;">Page 80</p>
<p>1 modulated? 2 MS. NAGAYA: Absolutely. So the -- the 3 current buildings that Facebook occupies, Buildings 10 4 through 19 as well as Building 20 are subject to what 5 we'd call a vehicular trip cap. 6 And so as part of this proposed project, 7 we're also proposing that the project would maintain a 8 similar vehicular trip cap. 9 And so what that means is there are 10 cameras that monitor all of the driveway points on a 11 daily basis, count the number of vehicles coming in and 12 out of the site, and they must maintain -- they can't 13 exceed a certain level of defined trips in the peak 14 periods from 7:00 to 9:00 in the morning and 4:00 to 6:00 15 in the evening and on a daily basis, so a 24-hour rolling 16 period. 17 And the numbers of trips that are -- the 18 maximum allowed are defined and studied in the EIR. 19 So as a condition of -- of the project 20 approvals, Facebook must stay below those caps and 21 they're monitored on a 24/7 basis, 365 days a year. 22 COMMISSIONER BARNES: Ingress and egress 23 of a particular site has the specific cameras that you're 24 referring to; is that correct? 25 MS. NAGAYA: Correct. So if you think of</p> <p style="text-align: right;">Page 79</p>	<p>1 distinguish between those two. 2 COMMISSIONER BARNES: So in the fifty 3 percent reduction in trip counts, I believe that that's 4 the figure that was -- 5 MS. NAGAYA: So the -- the fifty percent 6 reduction that was mentioned earlier is based on the 7 number of person trips coming in and out of the campus. 8 So that's what we call mode share or mode 9 split. So if you think of the total number of people 10 coming in and out of the campus over the course of the 11 day, about fifty percent are doing so in a mode other 12 than driving alone. 13 COMMISSIONER BARNES: And although for 14 the purposes of -- of trip counts, you're not tracking 15 whether it's single occupancy or a bus with forty people 16 on it under -- with a different mechanism, you're 17 understanding how they're getting to and from a 18 particular site; correct? 19 MS. NAGAYA: So from the City's 20 perspective, that -- that's correct. We're only 21 interested in the -- the maximum number of vehicles that 22 are coming in and out of the -- the campus on a daily 23 basis. 24 But Facebook does count on a regular basis 25 the number and the breakdown of the different modes</p> <p style="text-align: right;">Page 81</p>

<p>1 coming in and out, and that information I believe is 2 going to be part of the study session presentation. 3 It was also presented to the 4 Transportation Commission as part of their review last 5 week. 6 COMMISSIONER BARNES: Thank you. 7 COMMISSIONER RIGGS: Commissioner Onken. 8 COMMISSIONER ONKEN: Just to catch Nikki 9 before you go. So you mentioned looking at this 10 cut-through traffic and looking possibly for solutions. 11 What -- what are the solutions on the 12 table that you -- that are being maybe considered or 13 proposed? 14 MS. NAGAYA: So as part of the -- the 15 EIR, we took an approach of basically looking at three 16 different mitigation strategies, as Colin talked about in 17 the introductory presentation. 18 The first is -- is first and foremost 19 reducing the peak hour trips as much as we can. 20 And so by lowering that peak trip cap to a 21 level both of the proposed project as well as the 22 existing campuses, we're able to reduce the commute 23 traffic hour traffic quite significantly such that that 24 will help eliminate that kind of morning and evening 25 rush in the cut-through traffic.</p> <p style="text-align: right;">Page 82</p>	<p>1 there today. 2 We looked closely at options -- some of 3 those recommended mitigation measures that would normally 4 come out of an EIR to mitigate a level of service impact, 5 would expand the capacity, thereby encouraging more cut- 6 through traffic. And so we made efforts to not improve 7 those intersections. 8 So Chilco and Hamilton is one example 9 where a full traffic signal may be warranted if cut- 10 through traffic were using the neighborhood like was 11 shown in some of the early model runs. 12 And so in order to discourage cut-through 13 traffic, we've concluded that installing a traffic signal 14 is not the right approach at this time, and instead 15 working through a -- a neighborhood process to identify 16 cut-through traffic -- excuse me. Neighborhood traffic 17 calming measures to reduce cut-through traffic would be a 18 more preferred option. 19 So a mitigation measure is to conduct and 20 implement measures from that neighborhood cut-through 21 analysis. 22 But it needs to be a community driven 23 process so that we're not building things that prohibit 24 local residents from getting around, as well. 25 COMMISSIONER ONKEN: Sure. I -- I think</p> <p style="text-align: right;">Page 84</p>
<p>1 It does push traffic out to non-peak 2 hours, so we can see congestion spread over the course of 3 the day. 4 And so that's where we're looking at other 5 strategies to improve capacity or improve the multi-modal 6 system to give people options in how to get around. 7 And so some of the capacity improvements 8 that we've identified are starting the process of looking 9 at a grade separation at University and Bayfront 10 Expressway as -- as you approach the Dumbarton Bridge as 11 well as other kind of spot intersection improvements at 12 locations throughout the study area. 13 And then in terms of details on the 14 neighborhood cut-through side, as I mentioned earlier, in 15 The Willows, the Woodland corridor was shown to have cut- 16 through traffic impacts without the -- the housing 17 introduced as part of ConnectMenlo. 18 And so the introduction of -- of housing 19 can mitigate that impact. So theoretically adoption of 20 the General Plan could mitigate the -- the impact on 21 the -- the Woodland corridor. 22 And then finally in the Belle Haven 23 neighborhood is one that we were frankly most concerned 24 about in looking at the potential cut-through on Chilco 25 given the -- the cut-through traffic is already occurring</p> <p style="text-align: right;">Page 83</p>	<p>1 that normally in -- in the sort of -- with the sort of 2 tone of the comments we've heard, there's a huge concern 3 over cut-through -- what would be jumping to much more 4 Draconian measures such speed bumps and one-way systems 5 and resident's parking and all that sort of stuff. 6 But we're not really to that stage yet or 7 you're -- we're doing everything indirectly first and 8 then -- 9 MS. NAGAYA: So what -- 10 COMMISSIONER ONKEN: -- we'd worry about 11 that later. 12 MS. NAGAYA: What we've identified is 13 actually a process where we'd get the neighborhood 14 involved to identify those measures which can be as 15 Draconian or non-Draconian as we hear from the -- the 16 neighborhood as we go through that effort. 17 Everything -- the turn restrictions that 18 exist at Chilco and Hamilton are -- are one example, but 19 additional turn restrictions as you progress south of 20 Chilco at Ivy or at Newbridge can help with the 21 situation, but we also see that drivers figure out the -- 22 their way around them fairly quickly. 23 For example, going down Terminal and 24 looping back to Willow, or if there's not an officer 25 sitting there, making the -- the turn, anyway.</p> <p style="text-align: right;">Page 85</p>

<p>1 So we want to try and find measures that 2 balance neighborhood access with discouragement of cut- 3 through traffic. 4 COMMISSIONER ONKEN: Right. Okay. Thank 5 you. 6 COMMISSIONER RIGGS: Andrew. 7 COMMISSIONER BARNES: From a mechanic's 8 standpoint, something for instance like the Dumbarton 9 corridor, which is a study. Largely a twinkle in a lot 10 of folks' eyes, but a promising one nonetheless. 11 How does something like that and the 12 future prospects for that get worked into the EIR? Does 13 it or does it not show up and in what form? 14 MS. NAGAYA: So in -- in the context of 15 both this EIR and -- and the ConnectMenlo EIR that we'll 16 see a little bit later, the Dumbarton corridor study is 17 acknowledged and talks about the range of improvements 18 that are being studied and -- and being evaluated and the 19 potential that they could ultimately help improve or 20 mitigate impacts that are being disclosed in the 21 documents as significant and unavoidable today. 22 But because we can't guarantee what 23 strategies are going to come out of that document and on 24 what timeline they'll be implemented, to present a 25 conservative analysis, we're not assuming that any of</p> <p style="text-align: right;">Page 86</p>	<p>1 COMMISSIONER RIGGS: That would be 2 helpful. I'm not sure everyone realizes just what's 3 going to happen to Marsh Road, for example. 4 And then on page -- I need my glasses. I 5 think it's 316 and 317, residential zones are defined.. 6 So there are now -- I believe it's low density/medium 7 density/high density or something like that. 8 Are these proposed to replace the R-1-E, 9 R-1-S, R-1-U and R-4 and so forth that we currently use 10 throughout the General Plan, or is this specific to M-2? 11 MR. PERATA: So perhaps you can clarify 12 further. So are you focused on the Facebook EIR right 13 now or is this a ConnectMenlo the terms you're 14 referencing? 15 COMMISSIONER RIGGS: Well, that's a good 16 question. 17 MS. CHOW: I -- I think you're referring 18 to the ConnectMenlo EIR. 19 COMMISSIONER RIGGS: I will hold that 20 question. Thank you. 21 And I think the others would be comments, 22 so if there are no other questions. First I want to ask 23 by any chance is Mark Simon still here? Oh, wonderful. 24 I have a question just for background. 25 The EIR necessarily has to deal with</p> <p style="text-align: right;">Page 88</p>
<p>1 those improvements are in place today, but only 2 acknowledging that -- that additional things may come out 3 of them in the future. 4 COMMISSIONER RIGGS: All right. This 5 time I'm checking a little better for lights. 6 MS. NAGAYA: I can come back. It's not a 7 problem. 8 COMMISSIONER RIGGS: Thank you. 9 So I notice that when the impacts on 10 intersections or segments that are already level F are 11 indicated, that the impacts aren't quantified typically 12 because the intersection or segment is already an F. 13 I believe if you actually turn to the 14 individual page and read the numbers, you can see how 15 many additional cars are anticipated. 16 But is there -- is there a mechanism that 17 would make the impacts visible to the reader, whether 18 that's the Council or Commissioners? 19 Because if a -- if a level F adds three 20 more cars, it's not a terribly big issue, but if it adds 21 25 percent more cars, it really is. 22 MR. BURGETT: Yeah. We could quantify 23 either the -- you know, a -- a more precise increase in 24 seconds delay or a percent increase in traffic at 25 specific intersections.</p> <p style="text-align: right;">Page 87</p>	<p>1 mitigations that are within the power of either Facebook 2 or Menlo Park. 3 So an EIR, for example, of all the EIRs 4 that I've seen, I've never seen one that the mitigation 5 is "Caltrans shall do this" or "Caltrain shall do this." 6 Is there any way in which agencies outside 7 of Menlo Park, and particularly our transportation 8 agencies, could make a response to a development 9 situation? 10 I ask this because I could see a -- a 11 long-term benefit and the City making a commitment to 12 work with other agencies in response to a need. 13 Frankly at this point, we actually don't 14 have that process. 15 MR. SIMON: Well, I'm not sure how to 16 answer that question, because I think some of it is a 17 legal issue about what you can or can't require of 18 associated agencies or even unassociated agencies. 19 What I can tell you is the study underway 20 on the Dumbarton Transportation Corridor is to explore 21 the full range of options. 22 Clearly the goal is to provide a 23 significant means of traffic relief both now and 24 anticipation of future growth both from Facebook and from 25 any variety of other agencies and companies that are</p> <p style="text-align: right;">Page 89</p>

<p>1 reliant on the east/west corridor.</p> <p>2 As I'm sure the Chairman knows, we don't</p> <p>3 really have a north/south traffic problem; We have an</p> <p>4 east/west traffic problem that's manifesting itself in</p> <p>5 north/south traffic.</p> <p>6 Both Caltrain and Facebook are acutely</p> <p>7 aware of that, and that study that we're undertaking is</p> <p>8 really focused on what's the maximum amount of traffic</p> <p>9 relief we can provide, how quickly can we provide it, and</p> <p>10 what can we do to use those as building blocks for</p> <p>11 longer, more substantial improvements.</p> <p>12 I don't think it's something that can bear</p> <p>13 on the EIR, and again, that's a legal interpretation, so</p> <p>14 I can tell you that there are -- there's definitely work</p> <p>15 underway in that area.</p> <p>16 COMMISSIONER RIGGS: That is wonderful to</p> <p>17 hear knowing -- I'm quite aware that it can't be embodied</p> <p>18 in the EIR and it simply would not be appropriate for a</p> <p>19 City Commission to make a requirement outside of the City</p> <p>20 limits.</p> <p>21 But it is certainly a worthy question, and</p> <p>22 if nothing else, we can look forward to our city working</p> <p>23 with samTrans.</p> <p>24 MR. SIMON: Well, thank you.</p> <p>25 I'm certain you -- I'm certain you know</p> <p style="text-align: right;">Page 90</p>	<p>1 COMMISSIONER RIGGS: Wonderful to hear.</p> <p>2 Thank you.</p> <p>3 Do I have a follow-up question? From Mr.</p> <p>4 Kahle.</p> <p>5 COMMISSIONER KAHLE: Thank you.</p> <p>6 On that study, was that -- are we looking</p> <p>7 at heavy rail along the Dumbarton or are we looking at</p> <p>8 something else?</p> <p>9 MR. SIMON: We're looking at everything,</p> <p>10 everything from by bike/ped trail to a BRT to heavy rail</p> <p>11 and also things that we could do that might provide a</p> <p>12 phased solution, something that would provide traffic</p> <p>13 relief on the west side while we proceed about trying to</p> <p>14 build a larger project.</p> <p>15 Right now I have to tell you that the</p> <p>16 study does not have any predetermined outcome. That's</p> <p>17 the whole point. You can't do that.</p> <p>18 So we're exploring everything from --</p> <p>19 well, literally everything.</p> <p>20 COMMISSIONER KAHLE: Thank you.</p> <p>21 MR. SIMON: Yes, sir.</p> <p>22 COMMISSIONER RIGGS: Ken, thank you very</p> <p>23 much.</p> <p>24 COMMISSIONER KAHLE: Thank you.</p> <p>25 All right. At this point, I think we</p> <p style="text-align: right;">Page 92</p>
<p>1 that we did an EIR several years ago on the full buildout</p> <p>2 of this rail corridor and the rail bridge.</p> <p>3 We weren't allowed to go forward to</p> <p>4 certify it because the Federal Government required us to</p> <p>5 have a committed funding plan, and we just didn't have</p> <p>6 one.</p> <p>7 I think we can say that over the years</p> <p>8 that have passed since that work was done, this is the</p> <p>9 closest we've come to genuinely reviving that plan and</p> <p>10 even to begin working on the range of funding issues that</p> <p>11 might arise and the potential solution.</p> <p>12 COMMISSIONER RIGGS: This is the one ten</p> <p>13 or twelve years ago that involved heavy rail?</p> <p>14 MR. SIMON: Yes. It's actually -- I</p> <p>15 believe the EIR was seven years and it was a 700 million</p> <p>16 dollar project then. So we can assume with inflation,</p> <p>17 it's well in excess of that now.</p> <p>18 That being said, I think there's a</p> <p>19 different -- the political will to go forward with that</p> <p>20 and to identify the regional funding pretty much</p> <p>21 evaporated.</p> <p>22 I think we can say based on what's going</p> <p>23 on in the Peninsula corridor now, that the political will</p> <p>24 is in a whole different place than it was seven, eight</p> <p>25 years ago.</p> <p style="text-align: right;">Page 91</p>	<p>1 should move on from questions to Commission comments on</p> <p>2 the content of the EIR.</p> <p>3 John, do you want to kick it off?</p> <p>4 COMMISSIONER ONKEN: Thank you. I guess</p> <p>5 I'll -- I'll kick off.</p> <p>6 I mean, I -- I think tonight what we've</p> <p>7 seen from this EIR -- and it's, you know, quite important</p> <p>8 that, you know, it's very easy to see this EIR as very</p> <p>9 one long complaint and not really a call to action.</p> <p>10 It -- you know, that said, I mean, it's</p> <p>11 basically to my mind, the methods specifically with</p> <p>12 traffic is that things are bad, and they're still going</p> <p>13 to be bad no matter what you do and they're going to get</p> <p>14 a little bit worse with -- with things like this.</p> <p>15 But again, this is just a Draft EIR so</p> <p>16 we're kind of waiting to push it through, but I -- I</p> <p>17 think we really have to see this in concert with the --</p> <p>18 you know, as you mistakenly picked up, the other EIR</p> <p>19 that's happening later tonight.</p> <p>20 And just really find ways of putting some</p> <p>21 teeth to this rather than leaving it as, you know, yet</p> <p>22 another thick document propping up someone's desk, and</p> <p>23 really go after it as a way of informing what we do and,</p> <p>24 you know -- I don't know, making some possibly hard</p> <p>25 choices in terms of the way that we manage our traffic in</p> <p style="text-align: right;">Page 93</p>

<p>1 the neighborhood and, you know, what we -- what we've 2 begged Caltrans years to do.</p> <p>3 So that said, I mean, I'll -- mostly to do 4 with traffic. Other than that, I think that the -- the 5 EIR in terms of the other chapters that I went through, 6 you know, is substantially mild in terms of the impact of 7 this project, as large as it is as it sits in front of 8 us.</p> <p>9 So, you know, I'm very grateful for that, 10 but -- I mean, it has brought up the one sticking point 11 of our community, which is the traffic.</p> <p>12 So I've got other comments, but I'll wait 13 for the other Commissioners.</p> <p>14 COMMISSIONER RIGGS: Larry. 15 COMMISSIONER KAHLE: Thank you. 16 I -- I fully agreed with Commissioner 17 Onken's comments about the traffic. That is the 18 significant issue, and -- and your thoughts on that.</p> <p>19 I -- I'm curious about how this EIR will 20 overlap with the -- the M-2, so I'll kind of want to 21 address those comments after we hear that presentation.</p> <p>22 And just as a final note, I'm -- I guess I 23 was mildly surprised that the -- there was such big 24 support for Facebook on the comments we heard this 25 evening.</p> <p style="text-align: right;">Page 94</p>	<p>1 hundred, and the -- the mechanism for bringing that about 2 is essentially spreading the traf -- traffic out a bit 3 more during the peak period.</p> <p>4 Previously, you know, the cap was -- was 5 defined as a two-hour cap, and seventy percent of two- 6 hour trips could occur during a single hour.</p> <p>7 And so the mitigation would limit the one- 8 hour vehicle trip generation to fifty percent of the 9 two-hour cap and apply it retroactively to the existing 10 Facebook buildings.</p> <p>11 COMMISSIONER RIGGS: Ah. So this is very 12 interesting. So -- so now the cap is spread -- let me 13 follow this.</p> <p>14 The vehicles within the two-hour period 15 are actually -- existing vehicles are actually reduced or 16 they are allowed to be calibrated over a different 17 period?</p> <p>18 MR. BURGETT: They -- they would be 19 allowed to be counted over a different period, 20 essentially have the effect of -- of spreading that 21 traffic out to reduce the -- the peak hour impact.</p> <p>22 COMMISSIONER RIGGS: All right. So 23 vehicle trips per day, then, what is the change there? 24 MR. BURGETT: The -- the project would be 25 allowed to generate a net increase of about 16,000</p> <p style="text-align: right;">Page 96</p>
<p>1 COMMISSIONER RIGGS: It was good to hear, 2 not entirely a surprise. So Facebook has been working on 3 this for a long time, and I have to say it's a lot nicer 4 to review a project that is so thoroughly green.</p> <p>5 Few landowners are in a position to build 6 in this manner, and I'm particularly impressed -- in 7 fact, I -- I have to ask the follow-up question regarding 8 the trip cap.</p> <p>9 If I understood the presentation, albeit 10 two hours ago, the trip cap will grow just incrementally 11 with the -- with the Buildings 21 and 22 compared with 12 Buildings 10 through 20.</p> <p>13 Am I right, Kyle? Is that what that graph 14 was showing? Yes, that one.</p> <p>15 MR. BURGETT: Yeah. This -- this graph 16 shows the peak hour trip generation and vehicle trip 17 generation, and -- and with the mitigation, it is a very 18 relatively light increase.</p> <p>19 For example, on the right, you can see the 20 net increase in peak hour vehicle trips during the AM 21 peak hour. As -- as originally proposed, it would have 22 been 1,800 vehicle trips, and that would be reduced to 23 four -- about 400.</p> <p>24 In the PM, the increase was originally to 25 be about 1,500. It would reduced down to about a</p> <p style="text-align: right;">Page 95</p>	<p>1 vehicle trips per day, and so -- what that number's based 2 on is the project would add about 6,500 employees, and 3 Facebook's previous approvals have allowed about 2.5 4 daily vehicle trips per employee.</p> <p>5 But that doesn't -- that includes non- 6 employee trips. That includes visitors, people attending 7 events. Includes people going to campus for a job 8 interview, as they're -- as they're hiring 6,000 9 employees.</p> <p>10 Includes Fed-Ex deliveries and food 11 deliveries and so forth.</p> <p>12 But -- so with the -- with this project, 13 Face -- Facebook would be allowed to continue generating 14 that same rate of -- of about 2.5 daily vehicle trips per 15 employee applied to the increase in number of employees.</p> <p>16 COMMISSIONER RIGGS: May I suggest that 17 there be an additional chart which shows vehicle trips 18 per day?</p> <p>19 Because the implication of particularly 20 the graph on the left is that there'll be somewhere 21 between three and ten percent increase in traffic, even 22 though there's 900,000 square feet being filled with 23 employees.</p> <p>24 And I think reviewers should be aware of 25 this because the midday traffic is going to change. I've</p> <p style="text-align: right;">Page 97</p>

<p>1 seen this in other venues. Maybe one of the most extreme 2 cases being the 405 freeway in Southern California where 3 it doesn't matter if it's ten o'clock in the morning or 4 two o'clock in the afternoon.</p> <p>5 Then I don't know that the Commission is 6 specifically asked -- well, we asked questions, but I 7 don't know that anyone has made the suggestion, so I'll 8 throw it out that Bay Road/Ringwood intersection be 9 included in the analysis.</p> <p>10 This is a stop sign, and on some mornings, 11 it backs up maybe ten cars. It happens to be a key route 12 for Federal Express, not to pick on them.</p> <p>13 They might only have three vehicles at a 14 time, but it is -- it's an active route, and during the 15 school year, it can already back up.</p> <p>16 And then regarding our questions earlier 17 on housing, it seems appropriate for the EIR to address 18 the impact on local housing.</p> <p>19 Menlo Park is over 45 -- I believe over 20 forty percent rental and creating new BMRs does not 21 affect that unless those BMRs are then turned around and 22 rented, and I don't know that that's allowed in the CCRs.</p> <p>23 So I know all too well that there is a 24 direct connection between demand and monthly rent. 25 So is that something, Kyle, that can be</p> <p style="text-align: right;">Page 98</p>	<p>1 All right. I think that's what I have. 2 John.</p> <p>3 COMMISSIONER ONKEN: Just a follow up on 4 the housing issue. I think a lot of people have 5 obviously spoken to that tonight.</p> <p>6 There are lots of potential remedies that 7 we can see about housing in Belle Haven as to how to try 8 to keep people in their own homes, not get priced out and 9 all the rest, but most of which are somewhat impossible 10 to do because of, you know, Fair Housing Act and not 11 being allowed to, you know, discriminate against people 12 and not trying to keep some people out and some people 13 in, and it's -- it's incredibly challenging.</p> <p>14 You know, and my only suggestion would be 15 to -- I think Rose mentioned -- is to build our way out 16 of this, but that being said, that's what we look at the 17 M-2 to achieve.</p> <p>18 You know, there again, we can't self- 19 select who goes -- you know, who we're building for other 20 than a very broad sense -- in a very broad sense of low 21 income or market rate.</p> <p>22 But I think that's the -- that's possibly 23 the greatest hope we've got at this rate. In terms of -- 24 again, back to -- back to traffic, I think that, you 25 know, that -- again, with the traffic the way it is, I</p> <p style="text-align: right;">Page 100</p>
<p>1 encapsulated in the EIR?</p> <p>2 MR. PERATA: So if I follow the last 3 comment correctly, so you're talking about increasing 4 rent due to the project, demand on rental housing in the 5 community?</p> <p>6 COMMISSIONER RIGGS: Well, more 7 importantly, the inevitability of -- if you're currently 8 spending 65 percent of your income on rent and the rent 9 goes up ten percent, your -- you may be out of there.</p> <p>10 MR. PERATA: Sure. So I think as our -- 11 as ICF mentioned before, the EIR will focus on growth 12 induced impacts --</p> <p>13 COMMISSIONER RIGGS: Mm-hmm.</p> <p>14 MR. PERATA: -- but as we are doing, we 15 are working on an displacement analysis that will talk 16 about things like that in terms of rental increases and 17 induced demand.</p> <p>18 COMMISSIONER RIGGS: Oh, I see. So the 19 displacement analysis will be similar to an FIA. It will 20 be a separate document?</p> <p>21 MR. PERATA: Correct, yeah. And like I 22 said, that should be released later this week.</p> <p>23 COMMISSIONER RIGGS: Well, that's good. 24 That informs the decision-makers, so I think that's 25 excellent. Several comments on that.</p> <p style="text-align: right;">Page 99</p>	<p>1 think it sort of behooves us and the Belle Haven 2 neighborhood to look at very specific mitigation 3 measures, you know, and it -- it may mean trying to sort 4 of -- I don't know -- close the neighborhood off a little 5 bit from -- from all this cut-through traffic in very 6 difficult ways.</p> <p>7 Or maybe even -- I mean, there's this joke 8 about WAZE right now forcing everybody through Belle 9 Haven.</p> <p>10 Well, it would be nice if the people that, 11 let's say, maybe ran WAZE or owned WAZE might find, you 12 know, a way of hacking into it and just preventing 13 anybody from going through Belle Haven.</p> <p>14 I mean, that seems like it could be very 15 easily done given, you know, the connection of WAZE to 16 this project.</p> <p>17 So -- but I think that, you know, more 18 than anything, the -- you know, on the housing side of 19 it, it's probably -- you know, it's probably a wait and 20 see situation where I -- I don't really see any direct 21 solutions that can come out of this EIR that's going to 22 affect housing positively other than what's going to 23 happen in the M-2.</p> <p>24 COMMISSIONER RIGGS: Agreed. 25 Andrew.</p> <p style="text-align: right;">Page 101</p>

<p>1 COMMISSIONER BARNES: A couple of 2 clarifications. The displacement analysis is across all 3 the Bayfront inclusive of this project and other projects 4 and the zoning events occurring there; correct? 5 MR. PERATA: No. The displacement 6 analysis will be only focused on the Facebook Campus 7 Expansion Project as it relates to Belle Haven 8 neighborhood and East Palo Alto. 9 COMMISSIONER BARNES: Why would it single 10 out this project as opposed to the entire Bayfront 11 project? 12 MR. PERATA: Because it's being prepared 13 specifically for the Facebook Campus Expansion Project. 14 COMMISSIONER BARNES: Is the basic 15 assumption there that this project has a disproportionate 16 impact in that area? 17 MS. CHOW: So just with respect to 18 ConnectMenlo, so that project looked collectively at both 19 adding housing and jobs. 20 And so there's more balanced impact 21 potentially. And so it did not have a separate 22 displacement or having these assessments that the 23 Facebook project is proposing as part of their review 24 process. 25 COMMISSIONER BARNES: I would say on</p> <p style="text-align: right;">Page 102</p>	<p>1 Commissioners just now have put into words what I would 2 have said. 3 With the addition of the suggestions that 4 we've made, the EIR appears to me to be adequate and 5 appropriate. 6 At this time, before we move to the study 7 session just briefly, I would like to confirm with my 8 fellow commissioners that we will conclude our meeting 9 tonight at 11:00, as usual? 10 I'll note this -- I add this context that 11 we have -- that the public would not be aware of. 12 We have assigned the first part of meeting 13 of June 11 should we need additional time to talk about 14 the General Plan EIR. 15 So we will not simply stop in the middle 16 of a -- of a hearing. 17 So maybe I'll just take a show of hands 18 whether we'll continue with eleven o'clock. 19 COMMISSIONER ONKEN: Through the Chair. 20 COMMISSIONER RIGGS: Comment, yes. 21 COMMISSIONER ONKEN: There's one more 22 comment regarding the EIR which is -- which was brought 23 up that I think we need to address which was the comment 24 period that -- that I forgot at the moment and whether 25 it's adequate at 45 days or whether --</p> <p style="text-align: right;">Page 104</p>
<p>1 balance, I'm satisfied with the level and the detail 2 within is this EIR. 3 I think that the topics that were brought 4 up this evening with respect to Facebook's EIR were 5 thorough. I'm satisfied with the EIR. 6 COMMISSIONER RIGGS: Susan, any comments 7 on the EIR? 8 COMMISSIONER GOODHUE: No. I would -- I 9 would agree with -- with the previous comments. I think 10 a lot of the issues that we're struggling with are not -- 11 Commissioner Onken's point -- that the housing, we 12 cannot -- we can't address in this EIR. I think it's 13 much more the next EIR we're going to look at. 14 And similarly the traffic -- traffic has 15 so many interdependencies, and this I think is fairly 16 aggressive, and yet I would like to see more detail on 17 adding trips as that relates to environmental impacts and 18 greenhouse gases, et cetera, but I don't -- once again, 19 the -- the traffic is -- we can't deal with that in 20 isolation, either, so -- 21 COMMISSIONER RIGGS: All right. Larry, 22 any other comments? 23 COMMISSIONER KAHLE: No. I think I'm -- 24 I have nothing new. 25 COMMISSIONER RIGGS: All right. I think</p> <p style="text-align: right;">Page 103</p>	<p>1 COMMISSIONER RIGGS: Thank you. 2 COMMISSIONER ONKEN: -- it should be us 3 directing staff to extend it. 4 COMMISSIONER RIGGS: And so I'd like to 5 ask staff for some context. We have done other large 6 project EIRs, and here at the moment, we're talking about 7 Facebook. 8 What is staff's position on the 45-day 9 comment period? 10 MR. PERATA: So in terms of the project's 11 overall schedule, it has gone before the Council multiple 12 times with the 45-day review period identified as the 13 anticipated comment period for this project. 14 So it has been reviewed by the Council and 15 was potentially authorized in terms of a review timeline. 16 COMMISSIONER RIGGS: I would like to 17 think that besides hearing extensive comments tonight, 18 some of which applied actually to the project and some to 19 the EIR, and the Commission appears to have looked 20 carefully at the document and -- and asked some 21 challenging questions. 22 I -- I don't know that I see -- unless 23 another Commissioner would -- would like to indicate a 24 reason for delay. 25 I -- I know personally -- although those</p> <p style="text-align: right;">Page 105</p>

<p>1 of us in the design world have a slightly different 2 brain, but when you have a document this big and you have 3 four weeks to read it, you read it in the fourth week. 4 If you have six weeks to read it, you read it in the 5 sixth week.</p> <p>6 So I'm not entirely sure what the benefit 7 would be.</p> <p>8 Other thoughts?</p> <p>9 COMMISSIONER ONKEN: I'll -- I'll agree 10 with that. I think typically if there was a -- an 11 incredibly controversial section of this EIR where people 12 were firmly disagreeing about -- housing did come up, but 13 that's going to be addressed in a separate document in a 14 separate meeting.</p> <p>15 So I'm -- I'm content that there's nothing 16 of material significance within this that demands a 17 longer comment period.</p> <p>18 Obviously if it was thirty days, that 19 would be a problem, but we've still got time, and I urge 20 people that if they are interested, that this is your 21 opportunity to bring something up that we haven't heard 22 in written form to -- to our staff.</p> <p>23 COMMISSIONER RIGGS: Commissioner Barnes. 24 COMMISSIONER BARNES: My sense is that 25 we're longing for solutions more than we are more</p> <p style="text-align: right;">Page 106</p>	<p>1 STATE OF CALIFORNIA) 2 COUNTY OF SAN FRANCISCO) 3</p> <p>4 I, the undersigned, hereby certify that the 5 discussion in the foregoing Planning Commission meeting 6 was taken at the time and place therein stated; that the 7 foregoing is a full, true and complete record of said 8 matter.</p> <p>9 I further certify that I am not of counsel or 10 attorney for either or any of the parties in the 11 foregoing Planning Commission, or in any way 12 interested in the outcome of the cause named in said 13 action.</p> <p>14</p> <p>15 IN WITNESS WHEREOF, I have 16 hereunto set my hand this 17 _____ day of _____, 18 2016. 19 20 _____ 21 MARK I. BRICKMAN CSR 5527 22 23 24 25</p> <p style="text-align: right;">Page 108</p>
<p>1 questions. My sense is that the questions that are being 2 asked are comprehensive. We don't have answers for them.</p> <p>3 I think it's more problematic than the 4 time frame associated with asking the questions, and I 5 feel comfortable with the 45 days.</p> <p>6 COMMISSIONER RIGGS: I think that's well 7 put. The EIR actually -- essentially is an assemblage of 8 questions, and it would be quite troubling if, for 9 example, they didn't feel that there was a significant 10 impact on traffic.</p> <p>11 But it seems like a good 25 percent of 12 this inch and a half thick document deals with traffic.</p> <p>13 The questions have been posed. We are all 14 going to have to work out the answers, but the questions 15 have been well-posed.</p> <p>16 All right. With that, then, shall we move 17 on to the study session?</p> <p>18 ---o0o---</p> <p>19 20 21 22 23 24 25</p> <p style="text-align: right;">Page 107</p>	

A				
ABAG 47:24 48:1	acutely 90:6	aesthetically 36:11	43:1,12 54:15	applicant 4:3 6:19
ability 78:21	add 18:18,23 49:6	aesthetics 4:16	alternatives 13:13	applied 14:11
able 23:2 25:3	57:2 61:10 97:2	13:17 14:8 36:7,9	39:20 43:19	97:15 105:18
31:11,13 48:21	104:10	affect 24:8 69:6	Alto 16:21,21 37:7	applies 19:18
59:2 82:22	added 77:17	78:11 98:21	38:13 44:14,17	apply 20:12 96:9
absentee 62:10	adding 46:6 102:19	101:22	74:12,19 76:17	appreciate 29:3
Absolutely 78:15	103:17	afford 26:20	77:24 102:8	41:10 57:22 64:7
79:2	addition 11:7,15	affordable 27:11	amazing 29:3 67:6	66:23
abut 57:12	31:24 47:12 104:3	27:14 38:7,11,15	67:7	appreciates 32:8
abuts 10:17	additional 5:14,25	38:18	amenities 12:11	approach 40:5
acceptable 52:14	16:1 18:3 20:18	after-school 31:20	28:13	82:15 83:10 84:14
access 12:22 37:19	32:21 42:25 54:1	31:21	amount 45:1 63:16	appropriate 55:3
57:3 58:8,11,14	59:19 74:4,7 76:9	afternoon 98:4	90:8	71:3 90:18 98:17
80:2,4 86:2	76:11,14,21,22	agencies 43:9,9	ample 60:20	104:5
accessible 12:17	85:19 87:2,15	89:18,25	analysis 10:5 11:9	approval 9:20
accessing 80:19	97:17 104:13	agency 9:23 39:20	13:7,17 16:1,25	13:11
accident 65:8	address 8:16,25	44:16	34:12 45:21,25	approvals 16:15
accommodate	9:13 32:16 37:2,3	agenda 5:17,18 7:4	46:13,19 68:12,24	79:20 97:3
33:12	39:11 40:8,12	70:19	69:8,18,24 70:17	approve 14:17
accommodated	47:15 59:23 63:16	aggressive 103:16	71:21 72:2,12	40:14
63:23	69:9 72:12 94:21	ago 26:24 37:7	75:24 76:20 84:21	approved 9:19 25:9
achieve 42:16	98:17 103:12	64:24 65:8 91:1	86:25 98:9 99:15	60:8
100:17	104:23	91:13,25 95:10	99:19 102:2,6	approximately
acknowledged	addressed 9:2	agree 103:9 106:9	analyze 76:2 77:9	10:25 11:1,13,15
86:17	21:17 35:8,12	agreed 94:16	Andrew 72:20	11:19,22 12:4,10
acknowledgement	38:6 47:19 48:4	101:24	78:17 86:6 101:25	12:12,15
42:19 46:3	49:4 59:10 64:3	agreement 47:17	angle 72:21	April 31:6
acknowledging	106:13	70:18	answer 16:14 34:22	area 10:4 11:19
87:2	addresses 72:9	agricultural 13:21	62:17 69:7 89:16	13:21 15:23 16:6
acre 10:19 42:21	addressing 40:16	Ah 96:11	answers 107:2,14	16:8,18,19 17:22
Act 4:24 13:2 59:14	41:9 61:24 63:15	air 4:16 14:8 59:9	anticipate 36:16	19:14 24:1,2,9
100:10	adds 87:19,20	66:5	76:8,9	26:20 28:11 33:13
Acting 2:5	adequacy 8:16,23	al 74:19	anticipated 87:15	37:6,10 38:5 39:4
action 52:4 93:9	33:3 55:12	albeit 95:9	105:13	45:2 48:23 51:15
108:12	adequate 35:5	alleviate 64:6	anticipation 89:24	56:7,24 58:13
actions 13:1 53:5	55:23 104:4,25	allow 12:22 35:6	anybody 101:13	68:18 77:24,25
activate 37:20	adequately 46:18	allowed 20:18,19	anyway 85:25	78:2 83:12 90:15
activations 80:7	adjacent 18:2	79:18 91:3 96:16	apartment 62:19	102:16
active 39:7,13,23	adjoining 58:13	96:19,25 97:3,13	apologize 74:25	areas 34:4 36:6
40:2 98:14	Adjourned 3:9	98:22 100:11	Apostolic 30:6	69:2 76:7
actively 43:18	adopted 15:8 51:4	allowing 23:8	appears 104:4	arterial 76:4
activity 65:13	adoption 83:19	24:14 25:17	105:19	asked 57:15 59:20
actual 18:13 20:13	advance 44:3	allows 20:19	applause 47:2	98:6,6 105:20
22:14 74:9	adverse 14:3	alternative 39:18	applicable 15:7	107:2
	advice 6:25		51:3 54:18	asking 65:24 70:24

107:4	97:24 104:11	Bedwell 58:12	bicycle 12:17,21	20:25 57:11,12,23
aspect 57:7	awful 39:3	Beechwood 26:1,3	15:17 16:17 17:20	58:5,11 78:14
assemblage 107:7		26:14 27:3,5 28:5	18:1 20:23,25	83:10 91:2
assertion 45:18	B	65:4	37:19 58:11 65:7	brief 6:5
assess 76:2	BA 68:6	begged 94:2	bicycle/pedestrian	briefly 15:13 104:7
assessment 10:7	back 22:7 48:2,7	beginning 33:3	17:25	bright 72:21
15:23 77:1	73:8,14 85:24	35:13 53:10	big 37:11 43:14	bring 22:17 25:14
assessments 102:22	87:6 98:15 100:24	behalf 4:4	60:25 66:14,19	36:2 41:13 42:3
asset 30:15	100:24	behooves 101:1	87:20 94:23 106:2	57:7 106:21
assigned 104:12	background 16:11	believe 7:7 22:20	bigger 61:12	bringing 96:1
assistance 10:1	16:13 17:1,3	24:8 32:14 40:9	bike 43:15,23 58:6	brings 58:14
associated 46:2	70:22 88:24	53:11 80:22 81:3	67:5 80:24	broad 100:20,20
78:7 89:18 107:4	backs 98:11	82:1 87:13 88:6	bike/ped 92:10	broader 39:8,9
Associates 10:6	bad 93:12,13	91:15 98:19	bikes 80:21	46:22
assume 33:25 35:2	Bailey 38:23 41:1,3	Belle 12:23 25:23	Bims 32:23 35:20	broadly 38:12 39:7
71:14 73:22 75:9	41:4	26:21 27:21 28:7	35:21,22,23	brought 57:16,20
91:16	balance 86:2 103:1	28:9 31:7 33:2,20	binding 52:1,2	94:10 103:3
assumed 71:17,21	balanced 102:20	33:22 34:1,5,6,12	biological 4:17	104:22
assuming 68:8	ballast 65:4 67:8	34:22 35:7,11,16	13:18 14:9	BRT 92:10
77:18 86:25	Barnes 72:23 74:24	35:23 36:24 38:10	biomedical 66:6	build 24:19 25:4
assumption 102:15	75:3 76:24 77:13	41:14 44:18 58:9	birds 42:23	38:14 62:18,19
attain 37:5	78:18 79:22 80:10	58:23 64:19,23,24	bit 20:10 32:20	92:14 95:5 100:15
attainable 29:22	81:2,13 82:6 86:7	65:23 66:13,24	33:16 40:21 44:24	building 10:16 11:7
attempt 58:25	102:1,9,14,25	67:14 68:5 75:4	45:11,17 46:1	11:21,25 12:1,3,7
attend 31:13	106:23,24	77:11 83:22 100:7	75:19,23 78:23	12:13,14 23:15
ATTENDEES 2:1	Barnes' 77:15	101:1,8,13 102:7	86:16 93:14 96:2	24:16 28:13 36:10
attending 97:6	based 45:21 74:1	benefit 32:5 60:21	101:5	41:18,18 42:15,20
attention 41:23	81:6 91:22 97:1	65:5 68:9 70:25	blocks 58:23 90:10	58:24 79:4 84:23
57:8	baseline 10:2,22	77:23 78:2 89:11	blue 19:21,22	90:10 100:19
attorney 6:25 44:14	basic 102:14	106:6	BMR 70:18	buildings 10:23
108:9	basically 18:17	benefited 23:14	BMRs 98:20,21	11:2,4,11,12,12
audience 27:1	36:5 82:15 93:11	26:7 50:20	books 60:25	12:14 25:4 41:25
Audubon 57:21	basis 18:13 79:11	benefiting 25:7	boom 26:7	42:16 52:9,10,12
Authority 39:1	79:15,21 81:23,24	benefits 43:25	boos 47:3	54:2 62:19 71:15
authorized 105:15	Bay 10:4 12:23	51:20 65:21	Bostick 31:4	71:19 79:3,3 80:1
automobile 39:18	28:11 51:15 55:10	benign 20:21	bottom 33:11 49:1	95:11,12 96:10
available 37:9	55:17 56:12,12	best 8:25 23:11	bought 60:3	buildout 91:1
Avenue 16:20 80:4	58:25 71:24 98:8	43:4,9,11 52:21	bounded 10:14	builds 76:4
average 18:8	bayfront 10:13,15	63:7 64:4	boy 26:25	built 38:19 67:13
averages 18:14	17:25 18:5 19:13	better 29:12 37:4	brain 28:21 106:2	bulldozing 45:23
avoid 14:3 57:17	58:12,12 66:20	39:9 52:17 87:5	break 34:3	bumps 85:4
award 27:2 31:2	76:6 78:12 80:3	beyond 36:9 39:22	breakdown 81:25	Burgett 2:11 3:5
awarded 31:7	83:9 102:3,10	75:18 77:25	BRICKMAN 1:24	8:3 15:11,12 72:4
awards 31:6,8	bear 32:22 90:12	Bickerstaff 58:21	2:18 108:20	73:15 87:22 95:15
aware 90:7,17	beautiful 28:8,18	61:2,5 63:10,12	bridge 12:22 17:25	96:18,24
	29:25 63:14			

Burlingame 64:24	80:7	challenging 73:5	24:6 26:5 27:22	come 22:7,22 26:20
bus 18:12 80:16,22	carbon 41:17 51:10	100:13 105:21	32:15 33:24 34:4	37:10 42:5 52:7
81:15	card 22:17 50:11	Chambers 1:17	36:18 46:18,21	57:4 59:5 66:2
buses 37:19 80:12	cards 22:12,13,20	2:17	52:21 56:8 59:11	74:15 84:4 86:23
80:14	32:22 67:19	chance 35:24 55:2	65:20,22 69:3	87:2,6 91:9
business 42:13	care 27:17 61:10	88:23	75:15,17 77:25	101:21 106:12
businesses 53:17,21	career 26:21	change 17:12,16	78:5 89:11 90:19	comes 35:1 59:21
58:10	carefully 105:20	41:10 58:16 66:14	90:19,22	comfortable 47:3
Buyer's 42:9	caring 50:23	96:23 97:25	City's 6:23 77:6	107:5
C	carport 42:2	changes 21:18,19	81:19	coming 7:20 21:24
calibrated 96:16	carrying 9:24	26:4 69:5	City-sponsored	37:6 56:6 68:1
California 2:18,19	cars 87:15,20,21	Chapman 2:10 3:4	54:19	75:8 79:11 80:18
4:24 13:2 28:3	98:11	7:19,23 21:4	Clara 48:24	81:7,10,22 82:1
51:23 52:6 59:13	case 17:18 19:9	Chapter 69:1,9	clarifications 102:2	commenced 2:19
98:2 108:1	47:2	chapters 94:5	clarify 22:6 88:11	commended 36:13
call 22:19,22 76:1	cases 18:21 29:4	chart 97:17	class 48:18,20,20	comment 8:12 9:10
79:5 81:8 93:9	98:2	checking 87:5	classifies 13:24	16:14 22:8 32:21
called 72:25 75:9	catch 82:8	Cheryl 35:21	14:14	33:3 36:3 40:25
calls 51:23 52:4	categories 4:10,16	Chilco 10:16 20:25	clean 41:24 42:11	41:15 45:6,13,16
calming 84:17	4:22	43:24 60:16 65:7	53:9,13,15,18,22	47:11 55:10 57:21
Caltrain 38:25 40:3	category 59:20,23	67:6,9 73:25	59:9	60:19 64:1 67:20
65:3 89:5 90:6	cause 108:11	83:24 84:8 85:18	Clearly 89:22	99:3 104:20,22,23
Caltrans 89:5 94:2	caused 65:9	85:20	clerk 40:17	105:9,13 106:17
camera 80:6	CCRs 98:22	children 60:11 65:5	climate 41:6,10	commenter 16:4
cameras 79:10,23	cent 53:11,20	65:17 67:10	52:4 66:5	commenters 21:17
campaign 53:16	center 30:8 76:22	childrens 65:16	climb 73:14	comments 3:6,8 4:7
54:14	CEQA 4:25 6:13	choice 27:17	close 6:16,18 9:8	5:7 6:15,16,20,21
campus 1:5 4:5	13:2,3,16 68:13	choices 93:25	22:10,21 32:21	8:10,12,22 9:2,3
7:21 10:20 11:10	69:3,6 71:16,21	choir 47:5	54:21 55:20 67:20	9:13 21:5,8,9,10
16:8 20:2,2 26:5	certain 73:2 79:13	choose 53:12,18	76:21 101:4	21:13,14,16,18,21
41:25 43:10,14	90:25,25	Chow 2:7 88:17	closely 84:2	21:25 22:6,7 36:4
80:1,8,19 81:7,10	certainly 46:6 58:3	102:17	closer 6:3	38:21 44:21 45:8
81:22 97:7 102:6	61:19 90:21	Church 30:7	closes 9:11	47:10,13 57:6
102:13	certification 9:15	circulation 15:17	closest 91:9	58:2 61:6 63:17
campuses 20:6	42:17	15:18 37:16	coaching 32:5	68:2,12 70:12,22
82:22	certified 9:18	circumstances	code 5:4 60:7	85:2 88:21 93:1
candidates 26:18	certify 91:4 108:3,8	59:16,19	Colin 2:11 3:5 8:3	94:12,17,21,24
cap 19:16,18,20,21	cetera 103:18	cities 38:17 51:18	15:10,11 77:8	99:25 103:6,9,22
20:16,19 21:1	chair 50:10 69:11	citizen 25:2	82:16	105:17
79:5,8 82:20 95:8	104:19	citizen's 56:18 58:3	collaborate 40:8	commercial 52:10
95:10 96:4,5,9,12	Chairman 90:2	citizenship 27:1	collective 29:23	Commission 1:2
capacity 83:5,7	Chairperson 2:3,3	city 1:1,17 2:6,17	collectively 102:18	2:2,20 3:7,8 5:14
84:5	2:5	2:20 6:25 7:17	colleges 31:13	6:12 7:16 9:16
caps 78:20 79:20	challenges 31:14	9:16,22 13:1	combine 29:23	22:5,7 27:20
car 43:10 60:11	32:14 39:11 40:9	14:16 16:21 21:6	Combs 2:3 6:24	32:11 36:2 40:14

46:17 50:16 56:11 63:13 69:20 70:7 70:10 71:1 82:4 90:19 93:1 98:5 105:19 108:4,10 Commission's 70:25 Commissioner 4:1 6:2,15,20,24 7:6,9 7:10 22:1,4 23:20 24:11 25:19 27:25 30:2,10 32:19 35:19 38:22 40:20 40:24 44:10 46:25 49:17,23 50:7,12 54:3,6,20,23 55:1 55:6 56:14 58:19 60:22 61:3 63:9 64:15 66:9 67:18 67:24,25 68:16 69:10,12,14,17,25 70:24 71:7,11,18 71:22 72:6,13,17 72:19,23 74:24 75:3,14 76:24 77:13,14,15 78:10 78:16,18 79:22 80:10 81:2,13 82:6,7,7,8 84:25 85:10 86:4,6,7 87:4,8 88:1,15,19 90:16 91:12 92:1 92:5,20,22,24 93:4 94:14,15,16 95:1 96:11,22 97:16 99:6,13,18 99:23 100:3 101:24 102:1,9,14 102:25 103:6,8,11 103:21,23,25 104:19,20,21 105:1,2,4,16,23 106:9,23,23,24 107:6 commissioners	7:19 23:24 24:14 35:22 38:24 41:3 44:12 50:14 60:24 67:21 87:18 94:13 104:1,8 commit 53:6,14 commitment 23:14 31:4 40:7 89:11 committed 91:5 committee 56:18 58:3 communication 58:18 communities 63:5 community 5:7 12:24 23:11 24:5 24:8,25 25:3,16 25:24 26:15 28:10 28:18 29:24 30:14 30:15,17,18 31:8 32:2,6,9 39:5,8,9 41:10 43:25 44:4 44:14 50:19 51:20 58:9 60:1,3,13 62:16 63:1,7 64:8 64:21 65:9,13,21 66:15,19,24 67:1 67:16 84:22 94:11 99:5 commute 27:18 39:17 49:13 55:22 82:22 commute's 49:14 commuting 43:12 companies 39:16 42:11 89:25 company 39:4,5 50:23 63:25 64:12 compared 49:11 95:11 comparison 17:10 49:10 competing 29:8 competition 48:17 48:22	compiling 21:12 complaint 93:9 complete 7:15 26:4 27:4 56:19 108:6 completed 57:1 component 37:12 68:15 composting 43:3 comprehensive 15:22 107:2 concern 33:21 35:1 44:24 45:12 85:2 concerned 26:9 34:22 61:19 62:25 63:1 83:23 concerning 5:16 concerns 5:20,22 7:16 32:13 57:10 concert 93:17 conclude 22:1 104:8 concluded 50:2 84:13 concludes 50:5 concluding 77:18 conclusion 33:7 35:4,14 47:21 49:8 conclusions 8:9 concomitant 46:7 concurrent 45:6 concurrently 5:21 condition 17:14 79:19 conditions 15:2,6,7 16:9,10,11 17:1,2 17:14 34:16 conduct 84:19 conducted 13:8 confirm 104:7 conflict 28:14 51:3 conflicts 15:7 congestion 76:4 80:21 83:2 Connect 5:15,17,22	connection 7:4 98:24 101:15 connections 17:22 18:4 20:24 Connectivity 10:20 ConnectMenlo 75:22 76:13,18 83:17 86:15 88:13 88:18 102:18 Consequently 14:16 conservative 86:25 consider 8:13 13:13 16:15 21:8,13 consideration 65:24 Considerations 14:19 considered 10:22 11:5,9 17:18 18:24 26:12 82:12 consistent 48:12 consists 9:22 10:20 constantly 73:3 constitute 21:22 Constitution 4:6 10:14 18:5 constructed 11:16 11:25 12:2,8 construction 11:12 12:21 23:13 24:16 24:17 25:6 33:20 34:19 35:15 52:8 consultant 6:13,23 9:25 67:22 consultant's 72:3 Consultants 2:9,11 15:12 consulting 7:24 63:21 contain 5:2 11:21 containing 10:23 contains 63:16 content 67:22 70:8 70:15 93:2 106:15	context 86:14 104:10 105:5 continue 25:15 32:8 40:3 44:2 48:16 49:9 65:11 65:20 97:13 104:18 continues 31:19 contributed 66:16 contribution 39:5 controlled 77:5 controversial 106:11 conversation 62:1 cooks 27:16 cooperate 38:12,17 corporate 42:9 43:5 correct 35:17 71:6 79:24,25 81:18,20 99:21 102:4 correctly 99:3 correspondence 5:15 6:1 corridor 10:18 12:18 16:20 37:21 39:21 43:20 64:5 83:15,21 86:9,16 89:20 90:1 91:2 91:23 costs 28:12 46:23 Council 1:17 2:17 7:17 9:16 24:16 40:14 46:18 63:13 87:18 105:11,14 Councilmembers 64:17 counsel 108:8 count 79:11 80:15 81:24 counted 96:19 counties 48:25 49:1 49:2 country 42:12 counts 78:20,24
--	---	--	--	---

80:16,18 81:3,14 County 23:8 24:15 24:19 39:1 44:19 48:24 53:8 108:2 couple 37:7 41:20 44:21 56:5 102:1 course 43:16 81:10 83:2 cover 8:7 covered 54:8 covering 42:2 covers 13:15,17 create 25:3,5 created 47:18 creating 98:20 creativity 28:21 credential 26:20 Crime 30:7,10,24 32:1,7 criteria 77:6 crushing 46:9 Cruz 63:11 64:16 64:17 CSR 1:24 2:18 108:20 cultural 4:16 14:8 cumulative 11:5 13:11 15:1,4,5 16:5 66:4 curious 94:19 current 19:25 33:14 34:2,21,25 35:4,11 77:20 79:3 currently 8:11 10:19 31:17 42:1 53:11 88:9 99:7 cut 74:3 cut- 19:2 61:21 76:22 83:15 84:5 84:9 86:2 cut-through 5:22 21:2 73:23 74:1,4 74:7,13 75:4,16 75:20 76:8,11,15	76:19 82:10,25 83:14,24,25 84:12 84:16,17,20 85:3 101:5 cutting 73:24 74:18 <hr/> D daily 15:4 18:13 19:4,19 73:21 77:9 79:11,15 81:22 97:4,14 dangerous 67:10 Daniel 41:1 44:11 44:13 dark 19:22 data 48:1 80:6 David 25:20,22 day 18:9 63:20 80:9 81:11 83:3 96:23 97:1,18 108:17 days 37:7 59:15,16 79:21 104:25 106:18 107:5 deal 73:6 75:4,8 88:25 103:19 dealing 62:10 deals 107:12 Deanna 2:7 5:10 decision 35:6 decision-makers 13:10 99:24 deck 22:23 define 77:7 defined 78:5 79:13 79:18 88:5 96:5 definitely 90:14 degree 73:15 DEIR 50:2 delay 15:6 17:18 18:22 19:13 87:24 105:24 delayed 17:15 delays 19:8 deleted 21:20 delighted 31:19 deliveries 97:10,11	demand 19:7,11 37:18 39:15 76:1 98:24 99:4,17 demands 106:16 demolished 11:3 71:20 demolition 11:4,11 demonstrate 29:12 demonstrates 39:13 40:7 density 88:7 density/high 88:7 density/medium 88:6 dent 37:22 department 5:8 describe 8:10 15:13 described 58:6 describing 77:8 deserve 41:22 deserves 42:19 design 36:9 57:23 106:1 designated 54:19 designed 31:2 desk 22:18 93:22 despite 29:10 destined 78:4 detail 5:6 47:10 103:1,16 detailed 34:12 details 3:1 83:13 detection 80:25 Determination 9:21 determine 14:17 develop 43:19 developed 13:20 developer 62:24 developers 26:6 62:25 development 5:8 10:24 12:1,2 16:8 16:12 26:9 43:21 43:24 47:17 51:8	51:9,12,16 52:15 53:4 54:18 61:13 68:9 89:8 developments 16:6 41:16 42:7 Diana 38:23 Diane 41:1,4 44:10 different 16:24 17:6,11 28:10 29:2 34:4 36:21 75:23 81:16,25 82:16 91:19,24 96:16,19 106:1 difficult 45:11 101:6 digest 44:25 diminish 14:13 direct 13:11 98:24 101:20 directing 105:3 directly 22:8 director 8:2 28:3 41:4 disagree 51:11 disagreeing 106:12 disclose 4:25 69:1 disclosed 68:25 86:20 discourage 21:1 74:13 84:12 discouragement 86:2 discretionary 13:1 discriminate 100:11 discuss 14:22 discusses 5:6 discussing 7:13 22:14 discussion 3:1 68:9 108:4 dislikes 63:23 dismisses 47:23 disparity 36:20 37:2	dispatch 24:1 disperses 73:12 displace 34:18 74:20 displaced 27:6 62:5 62:9 63:4 displacement 37:4 45:19,24 46:13 69:18,24 70:16 99:15,19 102:2,5 102:22 disproportionate 33:25 102:15 disproportionately 33:22 disrupter 59:5 distinguish 81:1 district 36:18 65:17 65:18 districts 36:21 56:8 diversion 43:3,5 76:3 diversity 27:12 29:15 divide 18:19 document 7:14 8:24 9:3 21:13 75:21 86:23 93:22 99:20 105:20 106:2,13 107:12 documents 21:9 45:5,8 46:2 86:21 dodge 60:11 dog 66:22 doing 59:6 64:21 81:11 85:7 99:14 dollar 91:16 dollars 39:19 43:21 43:23 65:3 Don 56:21 57:15 donated 35:20 donations 31:11 65:15 donors 26:6 doubt 22:10
--	---	---	---	---

dovetail 68:20	eastern 11:23	22:13,16 23:10,18	52:5,16,19,23,24	13:16,25 35:9
Dr 31:4	easy 93:8	24:3 27:11,12	53:3,7	50:16 61:8 68:13
Draconian 85:4,15	economic 68:4,24	30:12 33:4 34:11	emotion 27:2	75:18 103:17
Draft 1:4 4:2,8,8,13	69:8	34:13 35:25 36:6	emphasis 75:4	Epidendio 47:7
4:20 5:6,19 6:8,12	economics 10:4	36:15,15 40:13	employed 7:2 41:19	50:8
6:15 7:22 8:9,11	29:9,11 68:7,18	45:1,14 46:3,12	employee 18:9,15	equally 33:24
8:19 9:9,9,11,13	69:9	47:22 48:13,13	97:4,6,15	equity 44:4
9:14 11:5 13:24	economy 26:12	51:2 54:17 57:1	employees 18:19	Erin 2:10 8:2 68:22
14:2,7,14 21:5,15	educate 78:23	59:14 63:16,18	36:17 39:17 43:14	escalating 28:12
21:18,21 24:3	education 30:8,24	66:11 67:23 68:10	43:25 48:15 55:24	essentially 15:13
35:25 48:13 59:14	30:25 31:4 36:14	68:19,23,25 69:9	58:9 71:20 77:19	16:5 20:10,25
70:11,15 93:15	36:20 37:3,5,13	70:4,8,11,15	97:2,9,15,23	73:25 96:2,20
dramatic 20:15	education's 37:11	72:10 73:10,16	employers 39:4	107:7
dramatically 17:12	Edward 57:15	75:22 79:18 82:15	78:5	establish 52:20
Drew 2:3 7:6	Edwards 56:21	84:4 86:12,15,15	employing 43:15	estimates 33:11
drive 4:6 10:14	effect 13:4 16:5	88:12,18,25 89:3	employment 7:4	73:21
29:10 37:25	20:9,12,20 46:5	90:13,18 91:1,15	encapsulated 99:1	et 103:18
driven 84:22	52:24 96:20	93:2,7,8,15,18	encompassing	evaluated 86:18
drivers 85:21	effects 4:9,14,21	94:5,19 98:17	11:13	evaluating 16:24
driveway 79:10	14:3,13 76:20	99:1,11 101:21	encourage 32:10	evaluation 15:16
80:2,4	efficient 42:24	103:2,4,5,7,12,13	40:14 53:17 54:14	evaporated 91:21
driving 81:12	54:10,12	104:4,14,22	56:10	evening 22:10 23:3
Drug 30:8	effort 42:10,14	105:19 106:11	encouraging 84:5	23:6,23 28:7
dry 50:3,4	43:14,21 66:3,4	107:7	ended 26:11	32:25 35:22 38:3
due 17:7 46:22 99:4	85:16	EIRs 5:19 37:14	energy 41:24 42:9	38:24 41:3 44:12
Dumbarton 10:18	efforts 75:19 84:6	89:3 105:6	42:12 52:10,11,13	44:22 56:17 63:12
37:21 39:21 43:20	Efner 2:10 8:2	either 55:22 61:13	53:10 54:10,11	63:14 64:17,18
64:5 78:14 83:10	68:21,22	77:4 87:23 89:1	enhance 28:21	66:12 68:21 75:12
86:8,16 89:20	egress 79:22	103:20 108:9	enhancements	79:15 82:24 94:25
92:7	Eh 52:15	elder 27:17	20:24	103:4
dwelling 48:8	eight 42:16 91:24	Electrical 23:7	enjoying 29:14	event 11:22 50:21
	eighth 27:4 65:17	electricity 52:7	enthusiastic 40:4	events 34:16 97:7
E	Eileen 55:7 56:16	53:9,12,22	entire 32:5 35:25	102:4
e-mail 21:5	56:18	electrification 40:3	43:9 63:18 102:10	everybody 101:8
earlier 16:4,14	EIR 1:4 3:7,8 4:3,8	Element 48:6	entirely 95:2 106:6	Everybody's 40:22
45:17 46:11 77:20	4:8,13,20 5:6 6:8	eleven 22:12	entirety 44:19	evidence 29:18
81:6 83:14 98:16	6:12,15,23 7:13	104:18	entitled 28:7	example 38:12
early 42:13 84:11	7:15 8:9,11,16,20	eliminate 14:3	entitles 52:3	41:12,14 66:20
earth 26:19	9:3,4,5,9,9,11,12	82:24	entrance 80:8	73:24 74:11 84:8
easily 101:15	9:13,14,15,18,22	embellish 57:24	environment 13:4	85:18,23 88:3
east 10:16 16:21	9:25 10:1,8 11:5	embodied 90:17	13:7 59:13,22	89:3 95:19 107:9
20:2 37:7 38:13	13:4,6,8,9,15,16	emission 14:22	environmental 1:4	examples 43:7
44:14,17 57:13	13:23,24 14:2,7	51:10	4:2,14,21,24 7:22	exceed 79:13
58:25 74:11,19	14:14 18:3 21:5,9	emissions 4:23 15:9	7:24,25 8:5,8,12	exceeding 18:23
75:2 76:17 102:8	21:15,18,22,23	51:1,4,9,13,23,24	8:14,23 13:2,12	excellent 99:25
east/west 90:1,4				

<p>excepted 80:23 exception 40:9 exceptional 40:11 excess 91:17 excessive 28:12 excited 23:16 excluded 80:20 excuse 48:24 69:19 84:16 Executive 28:3 exist 13:22 85:18 existing 10:20 16:10 17:1 19:21 20:16,19 52:12 71:15,17,19 82:22 96:9,15 exp 16:2 expand 84:5 expanding 28:14 expands 25:14 expansion 1:5 4:5 7:22 11:10 16:3 20:3 30:12 32:10 45:2,15 102:7,13 expect 17:13 expected 9:17 44:25 experienced 28:11 explain 8:9 explore 89:20 exploring 92:18 Expray 66:20 express 44:23 98:12 expressed 64:10 Expressway 10:13 10:15 66:21 76:6 78:12 83:10 extend 39:22 60:19 105:3 extended 27:8 68:17 extending 30:18 extends 16:22 extensive 18:12</p>	<p>105:17 extent 74:6 extinction 27:13 extreme 98:1 eyeglasses 55:14 eyes 86:10</p> <hr/> <p style="text-align: center;">F</p> <hr/> <p>F 87:10,12,19 F1 4:1 6:7 face 28:9 31:14 36:10 97:13 Facebook 1:4 3:1 4:4,5 5:15,17,20 6:7 7:1,2,5,21 10:9,16 11:10 16:7 18:11 19:9 19:17,25 20:1,6 22:13 23:10,17 24:4,23 25:1,11 25:11,14,15 27:11 30:11,13,16,21,23 31:1,11,18,19 32:3,8,14 34:23 35:25 36:8,17 39:3,6 40:7 41:8 41:12,19 42:8,13 43:7,18 44:2 45:2 45:15 47:12,18 48:14 49:5 50:18 50:21 52:18 53:5 53:14,23 54:10 55:11 56:7,23 57:14,22 58:23 61:7,14,20,25 62:23 63:6,17,20 64:2,7,21 65:1,2 65:12,20,24 66:11 66:16,18,23 67:11 67:13 75:21 77:18 77:22 78:8,25 79:3,20 80:12 81:24 88:12 89:1 89:24 90:6 94:24 95:2 96:10 97:13 102:6,13,23 105:7</p>	<p>Facebook's 24:24 32:10 43:3 97:3 103:4 fact 7:17 95:7 failed 59:10 fair 33:25 44:18 100:10 fairly 42:3 48:12 85:22 103:15 fairs 32:2,3 fall 9:17 53:10 familiar 55:19 families 23:14 25:7 26:21,23 27:6,15 58:10 family 25:25 27:5,8 28:4 61:10 fantastic 50:19 far 19:20 34:22 53:1 61:14 64:4 farmers 66:25 farther 77:25 80:4 fast 33:16 favorite 66:21 fax 21:6 feat 42:18 Fed-Ex 97:10 Federal 91:4 98:12 feel 23:1 47:3 51:18 66:22 107:5,9 feeling 36:8 feet 10:25 11:14,20 11:22 12:4,10,15 97:22 fellow 104:8 felt 33:22 fewer 22:13 FIA 49:21 99:19 Fields 25:21 28:2,3 fifteen 5:14 18:14 fifty 42:10 43:12 49:2 52:8,11 81:2 81:5,11 96:8 fight 29:8 fighting 67:9</p>	<p>figure 29:2 49:5 80:11 81:4 85:21 filled 97:22 final 9:3,12,15 21:9 21:23 46:12 54:22 94:22 finalized 70:17 finally 44:5 67:13 83:22 financial 31:14 find 62:9 65:25 66:4 73:13 86:1 93:20 101:11 finding 26:18 39:14 62:15 findings 6:14 15:15 17:19 finish 65:11 firm 7:24 44:16 firmly 106:12 first 5:13 7:13 12:1 19:17 22:15 23:4 24:20 31:23 44:23 50:18 62:4 64:20 68:3 82:18,18 85:7 88:22 104:12 Fiscal 10:4 68:11 fit 68:19 five 22:18 25:7 32:21 fix 62:17 64:2 floor 11:19 flows 73:1,8 focus 19:7 44:20 99:11 focused 8:20,22 88:12 90:8 102:6 focuses 19:16 31:21 focusing 17:2 38:20 folks' 86:10 follow 29:6 36:3 69:15 71:23 78:11 96:13 99:2 100:3 follow-up 78:17 92:3 95:7</p>	<p>followed 23:5,21 25:20 28:1 30:3 32:23 38:23 41:1 42:6 44:11 50:8 50:13 55:7 56:16 58:21 63:10 64:16 following 4:10,15 4:22 6:16 9:8,20 14:24 35:21 57:4 77:15 food 97:10 foot 11:16 force 23:15 forced 62:8 forcing 101:8 forecast 47:24 48:7 foregoing 108:4,6 108:10 foremost 82:18 forestry 13:21 forgive 55:15 forgot 104:24 form 34:3 35:5 46:13 86:13 106:22 formally 59:12 former 80:1 forth 18:12 60:17 88:9 97:11 fortunate 25:25 forty 51:24 80:16 81:15 98:20 forward 21:25 24:4 25:9 32:11 90:22 91:3,19 forward-thinking 50:23 found 34:1 70:5 76:21 Foundation 28:4 founder 31:4 founders 28:5 four 26:11 36:6,6 37:8 48:20 95:23 106:3</p>
--	--	---	---	---

fourth 106:3	38:5 48:7 55:2	God 30:7	greenhouse 4:23	26:22 27:21 28:8
frame 107:4	83:20 88:10	goes 18:17 65:25	13:17 14:22 15:9	28:9 31:8 33:2,20
Francisco 24:1	104:14	99:9 100:19	50:25 51:5,8,13	33:22 34:1,5,6,13
108:2	generally 60:24	going 18:24 19:11	51:22 52:16,22,23	34:22 35:7,11,16
frank 47:4	80:24	33:20,21 34:12	53:3 103:18	35:23 36:24 38:10
frankly 83:23	generate 18:7	37:16,20,22 38:5	gross 11:13,19,22	41:14 44:18 58:9
89:13	19:11 20:1 78:8	46:20 47:7 48:11	12:10	58:23 64:19,23,25
free 23:1 41:13	96:25	48:20 49:13,14	grounds 54:17	65:23 66:13,24
44:16	generated 16:2	52:16 56:9 61:11	group 41:5	67:14 68:5 75:4
freeway 36:24,25	18:9,13,18 78:25	73:23 75:6 82:2	grow 95:10	77:12 83:22 100:7
74:22 98:2	generating 97:13	85:23 86:23 88:3	growing 51:16	101:1,9,13 102:7
front 27:1 38:18	generation 29:21	91:22 93:12,13	grown 25:12	hazard 66:6
42:15 94:7	95:16,17 96:8	97:7,25 101:13,21	growth 16:16 48:23	hazardous 4:18,18
Fry 44:11 47:8	generations 29:4	101:22 103:13	51:21 56:4 69:1	4:19 5:1,3,5 10:2
49:19,24	generous 26:6	106:13 107:14	76:10,14 89:24	14:9
full 84:9 89:21 91:1	31:11 50:22	Gold 42:16	99:11	hazards 4:18
108:6	gentleman 45:16	good 21:4 23:6,23	guarantee 86:22	head 26:3
full-time 60:24	78:19	24:4 25:2,6,11,12	guess 49:21 93:4	hear 65:13 85:15
fully 13:20 40:13	gentrification	25:15 30:14,16	94:22	90:17 92:1 94:21
94:16	28:24	32:9,24 34:24	Guidelines 59:17	95:1
fund 31:5	genuinely 91:9	35:22 36:12 38:24		heard 32:12 43:6
funded 31:1	geography 75:1	41:3 44:12 49:5	H	43:12,20 46:10
funding 31:11 91:5	geology 4:10	55:11,14 56:17	H.L 31:4	77:20 85:2 94:24
91:10,20	getting 25:9 43:13	58:25 63:12 64:17	H1 67:20	106:21
further 44:3 52:19	81:17 84:24	66:12 67:15 68:2	habitat 42:23,25	hearing 1:14 4:3,7
53:19 54:19 69:16	give 10:10 29:17	68:21 75:12 88:15	habitats 57:18	6:8,17,18 7:21
88:12 108:8	34:25 60:19 65:16	95:1 99:23 107:11	hacking 101:12	9:15 73:3 104:16
future 23:17 26:10	67:3 69:4 83:6	Goodhue 2:4 7:9	half 107:12	105:17
44:8 86:12 87:3	given 7:1 36:19	103:8	Hamilton 30:9 84:8	hearings 22:5
89:24	68:6 83:25 101:15	Government 5:4	85:18	heats 34:8
futures 57:2	giving 70:14	91:4	hand 59:4 108:16	heavy 91:13 92:7
	glad 30:13 31:15	governments 52:2	hands 52:15 104:17	92:10
G	66:24	grade 26:25 27:4	handwriting 28:23	heights 12:15
G 5:21	glasses 88:4	65:17 83:9	happen 24:6 34:12	held 9:16
G1 6:8	global 28:17 52:23	graduation 26:24	51:16,17,17 88:3	Hello 28:2 30:5
gap 28:14	globally 38:8	graph 95:13,15	101:23	help 23:12 27:21
garden 66:24	go 22:7 32:20 33:10	97:20	happening 93:19	30:19 41:6 65:5
gardeners 27:15	37:1,17 38:2	graphic 55:15	happens 51:19 61:9	65:21 75:20 82:24
gas 4:23 14:22	48:19,21 53:1,19	grateful 94:9	73:11 98:11	85:20 86:19
50:25 51:8,13,22	55:22,22 56:7,8	great 27:8 28:24	happy 31:15	helpful 8:13 70:1
52:16,22,23 53:3	58:12 60:11,15	30:15 32:5 47:13	hard 80:25 93:24	88:2
gases 13:18 15:9	62:3 66:21 69:3	48:5 73:6 75:4,7	harmony 29:19	helping 26:21
51:5 103:18	76:17 82:9 85:16	greatest 100:23	Harry 32:23 35:20	43:14,18 64:4
Gateway 50:2	91:3,19 93:23	green 41:17,18	35:23 38:22	Henry 2:5
general 5:21 7:3	goal 89:22	42:21 95:4	hate 30:3 63:14	hereunto 108:16
9:5 17:8,8 27:12			Haven 12:23 25:23	

heritage 44:4 hesitated 47:1 Hi 15:11 25:22 75:14 Hibiscus 4:3 high 34:7 48:18 56:7 62:15 high-level 69:4 highest 43:5 highly 23:15 24:20 Highway 37:25 38:1,2 hiring 97:8 hits 74:5,5 hold 22:25 68:14 88:19 home 29:6 54:1 56:25 73:14 homeowner 64:19 homeowners 41:13 homes 33:23 53:25 54:9 60:3,17 62:8 62:9 100:8 honor 29:2 honored 26:25 hope 23:21 27:20 35:7,12 40:21 44:2,7 100:23 host 42:17 hotel 11:16 12:9,9,9 12:10 hour 14:25 15:2,23 16:2 17:10 18:20 19:16,19,20 20:1 20:8,14,16,17,19 20:20 21:1 37:1 37:24 38:1,2,3 53:12 82:19,23 95:16,20,21 96:6 96:6,8,21 hours 83:2 95:10 house 45:23 53:20 54:2 60:6 houses 33:12 34:1 62:18	housing 4:11 10:6,8 27:14 28:12 33:5 34:8,20 35:1 38:7 38:11,15,19 44:20 46:7,9,14,23 47:20 48:6,22 49:6,12 59:8,24 62:4,16,17 66:7 68:5,17,24 69:2,5 69:20 70:3,7,10 70:16 71:1,13 76:14,17,21 77:17 77:23 78:1,7 83:16,18 98:17,18 99:4 100:4,7,10 101:18,22 102:19 103:11 106:12 huge 85:2 human 59:22 hundred 48:15 53:12,15,18,22 96:1 hundreds 23:13 24:17 42:21 hungry 29:18 hydrology 4:17 14:9 <hr/> <p style="text-align: center;">I</p> <hr/> ICF 2:10,10 7:24 9:25 68:22 99:11 iconic 25:3 ideas 39:12 53:5 identified 13:14,23 14:1,10 17:21 18:20 19:2,6 73:17 83:8 85:12 105:12 identifies 4:9,13,20 13:24 identify 13:11 54:11 62:4 84:15 85:14 91:20 identifying 13:6 II 57:1 im 71:14	imagine 45:9 imbalance 44:6 immediately 18:2 30:17 immense 38:11 impact 1:4 4:2 5:23 7:22,25 10:5 14:1 14:1 15:15 16:1 17:4,18 18:25 19:11 20:11 26:15 33:7,21 34:3,17 35:7,9,11,16 45:19 46:20 50:25 51:18 52:25 55:24 61:8,11,14,15 64:25 65:1 66:1 68:4,12,17 70:6 71:14 72:16 75:18 77:2,6,7 83:19,20 84:4 94:6 96:21 98:18 102:16,20 107:10 impacted 34:10 55:21 impacts 6:14 8:14 8:15,23 13:7,12 13:13,14,25 14:5 14:14,20,21,22,25 15:17,19,19 17:20 17:21 18:6,20,22 19:2,4,6,12,16 36:14 46:18 53:3 57:10,17 72:12,25 76:16 77:7 80:22 83:16 86:20 87:9 87:11,17 99:12 103:17 implement 84:20 implemented 86:24 implementing 65:6 implication 97:19 import 75:11 importance 24:23 important 33:17,18 34:13 37:14,19	42:23 59:1 65:8 93:7 importantly 99:7 impossible 100:9 impressed 95:6 impressive 33:19 improve 18:4 36:23 44:3 83:5,5 84:6 86:19 improved 16:12 improvement 66:19 improvements 20:23 83:7,11 86:17 87:1 90:11 improves 48:2 incentives 43:17 incentivize 54:14 inch 107:12 include 12:3,8,10 12:20 14:21 74:22 included 5:3 10:23 16:9 55:18 72:1 98:9 includes 8:17 11:11 15:16 16:20 17:24 17:25 18:15 42:21 97:5,6,7,10 including 16:7 26:4 41:18 43:19 73:18 77:11 inclusive 102:3 income 28:15 34:7 41:13 44:17 53:25 99:8 100:21 incorporate 56:11 incorporated 10:7 increase 11:19 17:17 20:3,14 46:4,7,15,15 51:8 73:21,23 77:19 87:23,24 95:18,20 95:24 96:25 97:15 97:21 increased 15:6 19:7	19:7,13 increases 52:15 99:16 increasing 46:5,22 99:3 increasingly 26:9 incredibly 38:4 45:1 100:13 106:11 incrementally 95:10 independent 41:5 indicate 105:23 indicated 87:11 indirect 13:11 indirectly 85:7 individual 50:17 87:14 induced 69:2,5 99:12,17 inducement 69:1 industrial 10:23 inevitability 99:7 inevitable 28:24 infamously 4:2 inflation 91:16 influence 24:9 inform 13:9 information 7:14 44:25 60:20 71:2 82:1 informational 70:21 informed 35:6 36:4 informing 93:23 informs 99:24 Ingress 79:22 initiated 21:19 innovative 39:12 input 66:4 installation 53:24 67:6 installing 84:13 instance 73:7,14 75:25 78:24 86:8
---	---	---	--	---

intact 63:5	isolation 103:20	judge 55:24	96:4 98:5,7,22,23	legally 51:25
intent 8:18	issue 38:7,16 39:25	July 5:9 9:11 21:11	100:10,11,14,18	length 77:22
intention 34:23	40:13 44:6 47:20	70:23	100:19,25 101:3,4	lengths 78:9
intentions 55:11	47:21 50:5,24	jump 50:11 75:17	101:12,15,17,18	lessen 13:14
interactions 15:1	68:4 87:20 89:17	jumping 85:3	101:19 105:22,25	let's 22:4 29:23
15:25	94:18 100:4	June 1:16 2:16 9:7	knowing 27:2,3,5	101:11
interdependencies	issued 9:21	64:23 69:21	63:17 90:17	letter 21:5 28:4,7
103:15	issues 15:4,14	104:13	known 4:3 25:25	40:16 57:5,6,9
interested 81:21	16:13 22:6 28:9	<hr/>	knows 90:2	level 14:6 17:12,16
106:20 108:11	32:12,16 37:3,22	K	Kyle 2:7 3:3 6:2	29:15 79:13 82:21
interesting 47:22	39:3 40:17 47:16	K 37:12	21:6 69:14 95:13	84:4 87:10,19
96:12	47:19 57:16 59:8	Kahle 71:11,18,22	98:25	103:1
interests 39:22	63:15 91:10	72:6,13,17 92:4,5	<hr/>	levels 4:15 14:14
International 2:10	103:10	92:20,24 94:15	L	36:20 43:14 51:24
2:10 7:24 68:22	item 4:1 6:7,8 7:3	103:23	lack 38:16	51:25 52:5
internship 30:25	67:20 70:19 71:2	Katherine 2:3	land 4:10 46:4,15	License 1:24
internships 31:17	items 5:14,16,19,25	25:21 28:1,2 30:3	66:5	life 65:9
interpretation	6:7	keep 23:12 60:2	landowners 95:5	light 19:21 72:21
90:13	iteration 35:8	100:8,12	lands 57:18 58:15	95:18
intersection 21:3	Ivanski 64:16	keeping 31:3	lanes 39:24	lights 71:9 72:20
55:17,20 56:12	66:11,12,13	Ken 92:22	laptop 65:16	87:5
71:24 72:5 74:15	Ivy 85:20	key 7:14 15:15	large 94:7 105:5	limit 96:7
76:16 77:6 83:11	<hr/>	16:19 17:3,11,19	Largely 86:9	limited 12:9 16:19
87:12 98:8	J	19:15 37:11 73:23	larger 92:14	limits 90:20
intersection's	Jacobson 28:5,6	74:8 98:11	Larry 94:14 103:21	line 33:11 56:22
55:20	James 23:21 24:12	Keyser 10:6	late 31:3	list 5:3 42:18
intersections 17:15	24:13,15 25:19	kick 93:3,5	latest 26:7,8	listed 4:25 13:19
19:1 20:11 72:5	Janelle 50:9,13,15	kids 36:17,25	launch 53:16 54:13	36:15
72:24,25 73:13	54:3,20	kilowatt 53:12,20	Laurance 25:22,23	listen 23:12 67:1
74:2,8 77:3,4 84:7	job 24:19 25:15	kind 5:13 36:6 37:5	Laurel 2:17 5:8	listening 61:6 67:4
87:10,25	32:2,3 37:9 50:21	45:14 48:21 55:15	law 44:16	listing 7:16
interview 97:8	76:22 77:24 97:7	60:2 61:11 69:13	Lawrence 25:20	literally 92:19
intolerable 27:18	job-seekers 32:4	82:24 83:11 93:16	Leach 23:4,6,7	litigation 20:13
introduce 76:17	jobs 25:6 37:5,10	94:20	lead 9:23,25	little 6:3 23:1 28:18
introduced 30:17	46:6 60:24 65:22	Kirsten 2:10 3:4	leader 41:9 52:20	32:20 33:6 35:14
83:17	102:19	7:23	leaders 42:14	40:21 44:24 45:10
introduction 5:11	jobs/housing 44:6	know 16:5,10 18:7	leave 27:18	45:17 46:1 62:16
83:18	47:25 48:25 69:5	18:21 34:4 37:24	leaves 26:12 80:8	75:19,23 78:23
introductory 82:17	John 2:4 93:3	42:21 50:5 56:24	leaving 74:14 93:21	86:16 87:5 93:14
investment 39:21	100:2	58:18 59:25,25	led 42:8	101:4
invite 58:15	Johnny 63:10	60:2,6,11 63:24	LEED 42:16	live 24:18,18 27:11
involve 74:8	64:16 67:8	67:6 72:1,20 73:4	left 19:20 63:5	29:14,19 33:2
involved 9:5 30:21	join 20:18	74:1,6,8,11,16	97:20	48:15 54:14 58:23
63:20 85:14 91:13	joke 101:7	87:23 90:25 93:7	legacy 31:3	73:2
involves 12:25	Jones 56:16 58:21	93:8,10,18,21,24	legal 44:14 89:17	living 46:21 56:3
	58:22	93:24 94:1,6,9	90:13	

66:13 77:19 LLC 4:4 local 25:3,16 43:23 52:2 54:1,18 68:18 76:3 77:23 84:24 98:18 locally 41:6 located 10:13 11:23 12:5 19:17 30:8 76:21 location 5:1,2 locations 18:22 76:10 83:12 logic 49:15 London 50:9,14,15 54:5,7,22,25 55:4 long 49:14,20 59:6 59:9 61:19,24 93:9 95:3 long-term 37:3 89:11 longer 26:16 59:15 90:11 106:17 longing 106:25 look 16:9 21:25 25:8 26:17 57:22 63:18 73:10,16 76:20 90:22 100:16 101:2 103:13 looked 16:1 33:9 48:7 75:11,16 84:2 102:18 105:19 looking 17:4 32:15 55:16,25 82:9,10 82:15 83:4,8,24 92:6,7,9 looks 16:11,13 55:17 67:6,7 looping 85:24 loose 33:16 loss 26:10 28:25 lost 60:9 lot 17:15 32:12	39:3 41:22 42:3 46:6 48:17,22,23 60:2 64:9 75:16 86:9 95:3 100:4 103:10 lots 12:13 54:8,8 100:6 loved 26:14 low 28:14 41:13,17 49:9 53:25 74:10 88:6 100:20 lower 34:7 44:17 lowered 62:19 lowering 82:20 Luckily 52:22 53:1 lucky 50:22 ludicrous 48:22 lumps 51:10 <hr/> M M-2 45:2 78:2 88:10 94:20 100:17 101:23 main 45:13 74:5 80:2 maintain 79:7,12 making 26:15 39:8 68:1 71:1 85:25 89:11 93:24 manage 93:25 managed 39:24 management 37:18 39:16 manager 2:8 8:1 75:15 mangling 30:4 manifesting 90:4 manner 95:6 map 10:12 15:22 16:17 Mark 1:24 2:18 23:4,7 38:23,25 40:20 88:23 108:20 market 34:8 62:18 66:25 100:21	Marsh 37:25 38:2 76:7 88:3 Marston 10:6 master 21:17 Mateo 23:8 24:15 24:19 39:1 44:19 48:25 53:8 material 5:5 66:6 106:16 materials 4:19 10:3 14:10 math 31:21 matter 52:1 61:9 93:13 98:3 108:7 matters 7:1,3 mature 42:22 maxim 65:25 maximum 12:14 79:18 81:21 90:8 mayor 40:16 McLaughlin 55:7 56:17,18 mean 34:5 56:3 74:23 93:6,10 94:3,10 101:3,7 101:14 meaning 9:23 means 17:16 18:23 27:15 28:8 49:13 49:13 79:9 89:23 meant 24:24 measure 18:8 84:19 measures 14:3,4,12 41:18 42:17 44:3 74:12 84:3,17,20 85:4,14 86:1 101:3 mechanic's 86:7 mechanics 78:24 mechanism 73:10 81:16 87:16 96:1 mechanisms 74:20 80:11 media 54:13 meeting 2:16,20 3:1	6:6 8:19 57:20 63:13 64:18 69:20 70:12,13 104:8,12 106:14 108:4 meetings 57:19 megawatt 42:1 megawatts 42:2,6 member 50:15 61:10 65:9 members 7:20 44:13 45:9 46:10 memories 27:9 men 24:20 Menlo 1:1,17 2:17 2:18,21 5:8,15,17 5:22 9:23 16:20 19:17 24:18,24 25:1,4,11,24 26:5 27:13 30:7,9,15 32:15 33:14 36:17 41:5,6,8,16 46:21 47:13 48:5,15,24 49:11 50:2,16,23 51:17 52:4 53:4,8 53:17,25 54:9,15 56:8 58:11 59:11 60:16,21 62:22 65:20 69:3 77:17 77:19 78:2 89:2,7 98:19 mention 30:22 36:16 41:20 mentioned 18:21 37:8 38:14 41:20 53:24 81:6 82:9 83:14 99:11 100:15 mentoring 30:25 31:20 merits 8:21 Metal 23:25 methodology 33:9 75:23 methods 54:15 93:11	microphone 6:3 Microsystems 41:25 midday 97:25 middle 12:19 104:15 Middlefield 73:5 midst 28:18 mighty 53:16 migrating 42:23 Mike 35:21 mild 94:6 mildly 94:23 miles 15:19 18:7,9 18:13,14,18 49:9 million 10:25 39:19 43:21,22 65:3 91:15 mind 93:11 mineral 13:22 minimize 66:1,4 minutes 22:18,25 32:21 missed 59:21 72:8 misses 33:18 missing 46:1 55:15 mistakenly 93:18 mit 49:16 mitigate 83:19,20 84:4 86:20 mitigated 4:14 mitigating 36:23 52:25 mitigation 14:2,4 14:11,12 19:15,24 20:5,17,21 65:25 70:4,6 82:16 84:3 84:19 89:4 95:17 96:7 101:2 mitigations 18:4 20:23 21:1,3 89:1 Mm-hmm 99:13 mode 39:18 81:8,8 81:11 model 39:16 76:1
---	---	--	--	--

78:6 84:11 modes 81:25 modest 27:15 modulated 79:1 moment 71:9 104:24 105:6 Monday 1:16 5:9 monetary 29:14 Monica 64:16 66:11,13 monitor 78:21 79:10 monitored 79:21 80:5 month 9:10 21:22 monthly 98:24 months 31:24 morning 20:10 55:21 79:14 82:24 98:3 mornings 98:10 motor 14:25 15:2,4 15:18 motorcycles 80:23 Mount 30:6,11,23 32:1,7 move 6:3 23:2 24:3 27:18 29:7 32:11 33:13 93:1 104:6 107:16 moved 30:13 moving 25:1 62:7 64:24 multi-mobile 37:16 multi-modal 83:5 multi-use 12:17,21 multiple 57:19 105:11	name 7:23 15:11 23:6,24 24:15 25:22 28:2 30:4,5 32:25 38:25 44:13 66:12 68:21 name's 41:4 named 108:11 nannies 27:16 Narcotics 30:8 national 42:10 56:21 native 42:24 near 44:8 nearby 49:12 necessarily 31:13 68:25 77:22 78:13 88:25 necessary 63:3 65:25 necessitating 34:19 need 14:16 37:10 38:11 45:23 46:19 59:20 65:14 88:4 89:12 104:13,23 needed 5:19 33:12 78:22 needs 10:7 28:16 49:3 64:3 84:22 negative 61:14 negotiation 47:15 neighbor 25:2,16 34:24 56:22 63:7 neighborhood 5:23 19:3 21:2 36:10 37:23 38:10,20 55:9 62:22 63:21 65:1 66:17 67:15 67:15 68:5 74:18 75:5,7,7 76:11 83:14,23 84:10,15 84:16,20 85:13,16 86:2 94:1 101:2,4 102:8 neighborhoods 73:8,12 76:3	77:11 neighboring 51:18 neighbors 25:13 30:15,16 32:9 52:21 59:1 net 11:19 51:8 52:10,11,12,19 53:3 95:20 96:25 network 16:18 53:16 neutral 41:7 never 59:21 89:4 new 12:21 21:20 28:13 43:19 50:15 52:8,9,10 57:2 58:8,14,14 72:9 72:14 98:20 103:24 Newbridge 85:20 newest 58:24 nice 42:25 60:25 101:10 nicer 95:3 night 27:3 Nikki 2:8 75:12,14 77:14 82:8 nine 36:18 42:21 ninety 43:4 noise 4:16 14:8 non- 54:1 97:5 non-Draconian 85:15 non-employee 18:15 non-peak 83:1 non-profit 41:5 44:16 54:11 NOP 9:6,8 10:21 11:2 normally 31:13 84:3 85:1 north 10:15 16:22 44:18 north/south 12:18 90:3,5	note 8:18 9:1 21:7 41:15 43:7 44:5 45:14 94:22 104:10 noted 56:15 noteworthy 41:22 43:22 notice 2:15 4:25 9:6 9:20 66:14 87:9 noting 60:23 novel 42:3 nuisance 23:1 number 18:19 33:24,25 34:7,8 34:14,18 39:3 50:4 79:11 80:7,7 81:7,9,21,25 97:15 number's 97:1 numbers 78:6 79:17 80:13 87:14 nurses 27:16	officer 85:24 offices 54:9 offset 62:13 oh 6:4 47:7 49:19 71:10 88:23 99:18 okay 21:4 49:24 54:25 69:10 71:22 72:6,17 77:13 78:16 86:4 old 41:25 older 54:9 Olive 30:6,11,23 32:1,7 once 25:9 70:17 76:17 103:18 one- 96:7 one-way 85:4 ones 74:6 Onken 2:4 67:24 67:25 68:16 69:10 82:7,8 84:25 85:10 86:4 93:4 100:3 104:19,21 105:2 106:9 Onken's 69:17 94:17 103:11 online 42:5 open 6:18 12:17 29:15 58:13 66:25 67:3 opening 58:14 operations 6:11 15:18,24 17:10 Opha 28:1 30:3,5 32:19 opportunities 25:15 32:4 opportunity 29:1 32:17 40:12 67:2 106:21 opposed 102:10 option 51:21 53:10 53:15,18 84:18 options 26:13 53:2 83:6 84:2 89:21
---	---	---	---	--

N

Nagaya 2:8 75:12
75:12,15 77:1,21
78:14 79:2,25
80:14 81:5,19
82:14 85:9,12
86:14 87:6

oral 21:14 order 6:11 7:15 22:20 46:17 60:8 60:19 66:1 76:2 84:12 orders 51:25 organizations 39:2 organized 12:16 oriented 47:14 originally 95:21,24 outcome 92:16 108:11 outreach 41:11 outside 69:2 89:6 90:19 outstanding 26:25 31:3,12 overall 76:19 78:9 80:13 105:11 overcome 27:2 overflow 73:3,11 overhaul 26:4 overlap 94:20 Overriding 14:19 overview 6:5 8:9,18 10:10 69:4 70:11 70:15 owned 101:11 owners 62:11	30:7,9,15 32:15 33:14 36:17 41:6 46:21 47:13 48:5 48:15 50:16,23 51:17 52:4 53:4,8 53:17,25 54:9,15 55:9 56:4,8 58:11 58:13 59:11 60:16 60:21 62:22 65:20 69:3 72:9 77:17 77:19 78:2 89:2,7 98:19 Park's 49:11 parking 11:1 12:12 42:3 54:8 85:5 parks 72:12,14 part 11:4,8 19:23 23:3,16,16 25:25 30:18 37:13 53:6 65:11 68:8,10 70:4 79:6 82:2,4 82:14 83:17 102:23 104:12 participant 39:23 64:18 participate 65:12 participated 32:2 particular 39:10 72:4 73:12 76:6 76:13 78:6 79:23 81:18 particularly 44:20 89:7 95:6 97:19 parties 108:9 partner 24:4 39:13 50:19 54:10 partnered 30:23 39:6 partnering 41:8 partners 23:11 32:9 partnership 39:8 40:6 49:5 partnerships 30:21 passed 91:8	passes 80:8 path 20:23 29:6 65:7 67:5 paths 18:1 43:23 patterns 76:2 Patti 44:11 49:17 50:7 pay 53:11,20 paying 25:6 65:3 peace 29:19 peak 14:25 15:2,23 16:2 17:10 18:20 19:15,19,20 20:1 20:7,14,16,17,18 20:20 21:1 79:13 82:19,20 95:16,20 95:21 96:3,21 pedestrian 12:18 12:21 15:17 17:20 18:1 58:6 65:7 67:5 pedestrian/bicycle 18:4 pedestrian/bike 57:11 Peninsula 24:1,7 53:9 91:23 people 25:25 26:17 27:7,16 28:15 29:3,19 33:13 34:10,19 35:24 36:24 37:4 45:19 45:24 46:14,21 49:13 55:19,22 60:5 62:7,14 63:4 74:2 81:9,15 83:6 97:6,7 100:4,8,11 100:12,12 101:10 106:11,20 Perata 2:7 3:3 5:12 6:4 21:6 22:3 50:10 68:11 69:11 69:13,15 70:9 71:5,16,19 72:11 72:15 88:11 99:2	99:10,14,21 102:5 102:12 105:10 percent 36:16,18 39:17 43:4,12 48:14,15,19,21 51:24 52:5,7,8,11 53:13,15,18,22 77:20 81:3,5,11 87:21,24 96:5,8 97:21 98:20 99:8 99:9 107:11 percentage 78:6 perfectly 52:18 period 8:20 9:9,10 9:14 36:3 59:14 60:19 67:20,21 79:16 96:3,14,17 96:19 104:24 105:9,12,13 106:17 periods 45:6 79:14 perk 50:21 permanent 40:18 permanently 67:12 permit 11:3 30:20 permits 19:25 perplexed 45:18 47:21 perplexing 47:16 person 22:22,23 50:20 80:16 81:7 personally 67:2 105:25 perspective 81:20 phase 8:12 12:1,2,8 57:1 phased 92:12 physical 13:6,12,14 45:23 46:14 pick 98:12 picked 93:18 piece 37:13 pipeline 16:16 17:3 place 27:11 33:20 39:9 57:25 74:13	74:20 76:14 87:1 91:24 108:5 places 54:8 placing 78:1 plan 5:21 7:3 17:8 27:12 48:7 51:3 52:4 55:2 57:5 83:20 88:10 91:5 91:9 104:14 planet 51:16 planned 42:5 Planner 2:7,7 21:6 Planning 1:2 2:2,19 3:7,8 9:16 32:10 56:10 63:13 108:4 108:10 plans 15:7 plant 42:24 play 60:11 please 9:1 22:17 23:1 49:18 65:11 65:23 66:3,8 pleased 31:16 58:4 pleasing 36:11 plus 16:13 17:3 20:2 21:21 PM 2:16 5:9 20:12 20:18 21:10 70:23 74:17 95:24 podiums 12:14 point 7:7,12 16:19 17:11 32:20 33:17 33:18,19 46:1 65:10,19 69:17 80:8 89:13 92:17 92:25 94:10 103:11 points 79:10 80:2 poised 52:18 policies 15:8 policy 51:3 political 91:19,23 Pond 56:25 ponds 56:22 57:2 pool 26:18
---	--	--	---	---

<p>population 4:11 10:7 33:5 51:15 66:6 70:16 71:17 portion 8:19 10:17 11:23 12:5 posed 107:13 position 95:5 105:8 positive 24:9 26:15 58:1 61:6,14,15 64:25 65:1 67:11 67:13 positively 101:22 possibility 29:16 34:10 36:22 47:14 possible 26:5 31:10 possibly 52:21 82:10 93:24 100:22 potential 5:22 15:19 16:16 19:2 50:25 72:12 73:25 83:24 86:19 91:11 100:6 potentially 4:13,20 19:12 70:7 76:22 102:21 105:15 power 27:21 28:21 41:13 42:4 53:16 63:4 89:1 practices 41:19 preach 29:21 preaching 47:5 precise 87:23 predetermined 92:16 prefer 47:2 preferred 84:18 preliminary 36:1 45:8,14,21 47:9 premium 53:11,21 preparation 9:5,6 21:8 prepared 4:8 5:3 7:25 9:9,12 10:1,4 10:6 13:5 68:22</p>	<p>102:12 presence 24:24 present 2:3 5:1 10:9 86:24 presentation 3:3,4 3:5 6:13,19 7:11 8:5,7,17 22:2 82:2 82:17 94:21 95:9 presented 82:3 preserve 27:21 28:21 44:4 pressure 46:9 pretty 91:20 preventing 101:12 Prevention 30:7,11 30:24 32:1,7 previous 41:20 97:3 103:9 Previously 96:4 priced 60:1 100:8 prices 46:9 primary 74:1 principal 2:7 9:24 principles 29:21 42:9 printing 6:1 prior 13:10 16:7,15 43:13 priority 27:15 32:16 private 39:4 52:2 pro 8:14 47:24 52:25 proactive 61:23 probably 34:23 36:2 47:5 50:19 68:13 72:8 74:6 101:19,19 problem 33:15 34:25 38:9 47:24 56:1 64:3 87:7 90:3,4 106:19 problematic 107:3 problems 59:9 proceed 92:13</p>	<p>PROCEEDINGS 1:15 process 8:8 9:4 47:19 76:18 83:8 84:15,23 85:13 89:14 102:24 professional 66:3 profit 54:2 program 26:20 30:24 31:1,2,6,10 31:20,21,23 39:16 40:3 43:8 44:21 54:19 programs 30:21 43:3 progress 85:19 prohibit 84:23 prohibited 74:16 project 1:5 4:5,9 5:2,13,20,23 6:9 6:14,20 7:22 8:1,1 8:2,14,18,20,23 9:18,20,24 10:9 10:12,14,17,19,21 10:22 11:4,6,8,9 11:11,16,18,24 12:5,16,20,23,25 13:3,10,10,12,20 14:17,21,23 15:1 15:3,5,7,17 16:7 16:12,13 17:3,4,9 17:22,24 18:7,10 18:11,18 19:10,16 19:23 22:14,17 25:5,8 32:10 34:18 45:2,15,18 46:4,19,20 49:6 50:21 51:2 52:19 52:25 53:6 56:25 60:6 65:21 66:2 66:17 68:13 75:22 79:6,7,19 82:21 91:16 92:14 94:7 95:4 96:24 97:2 97:12 99:4 101:16</p>	<p>102:3,7,10,11,13 102:15,18,23 105:6,13,18 project's 105:10 projects 16:8 17:2 24:2,7 38:16 41:11 47:23 66:17 78:25 102:3 promises 23:12 promising 86:10 Properties 4:4 property 60:9 62:15 proposal 12:3 20:3 proposal's 6:9 proposed 11:10,21 12:7,14 14:17 16:2 19:22 66:18 79:6 82:13,21 88:8 95:21 proposing 23:17 72:14,14 79:7 102:23 propping 93:22 prospects 86:12 prosperous 51:20 protect 57:24 proud 25:4 proven 30:14 provide 8:8,15 14:18 31:12 42:22 42:24,25 54:11 58:8,15 89:22 90:9,9 92:11,12 provided 12:13 39:19 provides 14:2 17:9 18:12 19:9 provisions 2:20 public 1:14 3:6 4:3 4:7,7,11 6:8,14,17 6:18,20 7:20,21 8:11 12:22 13:9 22:6,8 43:15 44:13,25 45:9</p>	<p>46:10 54:8 59:14 66:7 67:20 68:8 70:12 72:11 104:11 public/private 40:6 publicly 12:17 pull 78:21 purchasing 53:9 purple 19:22 purpose 7:14 15:8 51:4 purposes 71:16,21 81:14 pursuant 2:15 push 62:22 83:1 93:16 pushing 29:7 put 47:10 57:10 66:3 104:1 107:7 putting 93:20</p> <hr/> <p>Q</p> <p>quality 4:16,18,24 13:2 14:8 36:23 37:5 50:16 59:13 59:14 66:5 quantified 76:25 77:10 87:11 quantify 87:22 question 16:4 34:17 34:22 68:3 69:7 71:23,24 72:23 74:22 77:16 78:19 88:16,20,24 89:16 90:21 92:3 95:7 questions 3:7 6:16 6:21 8:4 9:1,2 22:5 59:19 67:22 68:12 71:12 88:22 93:1 98:6,16 105:21 107:1,1,4 107:8,13,14 quick 6:5 10:10 44:21 quickly 85:22 90:9 quiet 60:2</p>
---	--	--	--	--

<p>quite 15:21 22:9 42:18 43:14 61:22 73:16 82:23 90:17 93:7 107:8</p> <hr/> <p>R</p> <p>R-1-E 88:8 R-1-S 88:9 R-1-U 88:9 R-4 88:9 Rachel 50:13 55:7 55:8 61:3 63:10 63:12 64:15 rail 10:18 37:21 43:20 91:2,2,13 92:7,10 raise 50:24 60:4 raised 47:16 ran 101:11 range 86:17 89:21 91:10 ranging 31:8 rapidly 28:11,14 rate 33:14 43:3 97:14 100:21,23 rates 43:5,11 ratio 47:25 48:25 69:6 77:18 rationale 14:18 Ravenswood 36:18 reached 57:14 reaction 70:3 read 28:4 35:25 45:21 60:25 87:14 106:3,3,4,4 reader 87:17 reading 31:21 33:6 ready 22:24 24:19 real 34:9 realize 64:25 realizes 88:2 really 17:3 25:4 29:21 36:12 37:20 41:10,22 43:8 44:18 46:22 56:5 58:25 63:1,15</p>	<p>64:25 66:24,25 67:3,10,15 85:6 87:21 90:3,8 93:9 93:17,20,23 101:20 reap 51:19 reason 34:13,25 35:2 72:21 105:24 reasonably 17:15 receive 4:7 5:14 6:14 received 6:1 9:13 21:10,14,18 22:20 receiving 21:25 27:2 recipients 31:17 reckless 62:24 recognize 27:12 recommend 13:12 recommendation 70:8,19 recommendations 8:15 70:10 71:1,4 recommended 19:24 20:5,23 21:3 84:3 recommends 18:3 record 40:18 48:5 108:6 recusal 7:7 recuse 7:1 Recused 2:3 recusing 7:2 recycling 43:2 red 19:23 reduce 8:15 13:13 14:3,5 20:6,7 43:9 51:22 53:2,6 82:22 84:17 96:21 reduced 95:22,25 96:15 reducer 52:19 reducing 15:8 51:4 51:23 52:23 77:22 78:3,9 82:19</p>	<p>reduction 20:25 51:13 52:5 81:3,6 Redwood 24:6 77:24 referencing 88:14 referred 78:20 referring 79:24 88:17 reflect 70:2 refuge 28:8 29:25 56:19,21 57:12,12 57:15,25 58:3,12 regard 5:16 17:20 18:6 19:15 68:7 68:11 regarding 6:13 8:5 32:12 57:6 67:22 70:8 95:7 98:16 104:22 regards 68:9 region 44:7 46:7,22 78:3 region's 39:11 regional 15:3 18:14 38:19 39:25 40:8 49:10 62:21 64:2 91:20 regionally 64:3 register 45:11 regular 31:25 81:24 regulation 51:3 reiterate 24:4 rejected 54:17 relate 5:17 related 5:18 17:21 19:2,4 68:13,14 relates 16:18 50:25 70:15 72:24 75:5 102:7 103:17 relations 25:13 relationship 67:14 relatively 34:7 74:10 95:18 released 9:6,10</p>	<p>21:22 69:18,19 99:22 reliant 90:1 relief 89:23 90:9 92:13 remedies 100:6 REMEMBERED 2:15 remodels 60:6 remove 65:3 67:10 67:12 removing 58:24 renewable 41:24 42:4,9,11 52:7 53:13 renovation 11:7 rent 60:4 62:15,19 98:24 99:4,8,8 rental 98:20 99:4 99:16 rented 98:22 renters 34:8 rents 46:5,16 repeat 41:21 repeated 21:16 replace 26:17 88:8 replacement 34:19 report 1:4 4:2 7:22 7:25 33:7,11,18 34:2 35:5,9,14 68:6 69:20,22,23 75:19 Reported 1:24 REPORTER'S 1:15 reports 34:21,25 59:21 60:14 represent 23:25 24:15 30:6 38:25 39:2 representing 56:18 57:21 request 59:12 requested 50:11 require 89:17</p>	<p>required 13:16 91:4 requirement 90:19 requires 4:25 51:8 52:6,9 research 10:24 resident 24:6 35:23 55:9 64:23 resident's 85:5 residential 52:9 88:5 residents 34:7 35:11 37:8 44:17 53:17,21 67:14 84:24 residing 29:4 resist 60:23 resource 32:4 resources 4:17,17 13:18,22 14:9,9 29:24 30:19 43:16 66:1,5,6 respect 36:6,14 37:15 102:17 103:4 Respectfully 51:11 respond 8:6 21:14 response 89:8,12 responses 9:2 21:9 21:13,17,21 responsibility 9:24 responsible 25:2 responsibly 25:12 rest 29:12 100:9 Restoration 56:25 restore 57:2 restrictions 85:17 85:19 result 11:18 14:24 21:18 34:16 resulting 53:3 results 29:10 resume 32:4 retroactively 96:9 retrofitted 42:16</p>
---	---	---	---	--

<p>52:12 revenue 65:22,22 review 5:19 7:18 8:5,8,12,20 9:14 59:14 82:4 95:4 102:23 105:12,15 reviewed 5:20 105:14 reviewers 97:24 reviewing 7:14 45:7 revise 56:11 reviving 91:9 rezoning 45:3 rich 77:24 riches 29:13,13 richness 29:15 ride 43:16 Riggs 2:5 4:1 6:2 7:6,10 22:1,4 23:20 24:11 25:19 27:25 30:2 32:19 35:19 38:22 40:20 40:24 44:10 46:25 49:17,23 50:7,12 54:3,6,20,23 55:1 55:6 56:14 58:19 60:22 61:3 63:9 64:15 66:9 67:18 69:12,14,25 70:24 71:7 72:19 75:14 77:14 78:10,16 82:7 86:6 87:4,8 88:1,15,19 90:16 91:12 92:1,22 94:14 95:1 96:11 96:22 97:16 99:6 99:13,18,23 101:24 103:6,21 103:25 104:20 105:1,4,16 106:23 107:6 right 7:10 22:4 23:21 24:19 26:19 28:20 33:1 47:13</p>	<p>53:4 58:19 59:2,3 61:16 63:19 64:12 68:16 71:7 72:17 74:16 84:14 86:4 87:4 88:12 92:15 92:25 95:13,19 96:22 100:1 101:8 103:21,25 107:16 right-in/right- 80:3 rights 29:8 Ringwood 71:25 rised 33:2 rivals 43:8 Road 37:25,25 55:10 56:12 71:24 76:7 78:13 88:3 Road/Ringwood 55:17 56:12 98:8 roads 60:12 roadway 15:5 77:9 robust 46:13 rocks 67:8,12 role 39:7 rolling 79:15 rooftop 42:21 rooftops 43:1 room 12:9 50:20 Rose 58:21 61:1,2,5 63:9 100:15 route 10:15 12:22 98:11,14 routes 15:2 73:24 74:1,5,7 RPR 1:24 Ruigomez 23:22 24:12,13,13,15 ruin 47:7 run 12:18 66:21 80:12 runs 43:7 84:11 rush 37:1,24 38:1,3 82:25</p> <hr/> <p style="text-align: center;">S</p> <hr/> <p>safe 43:23 58:8 66:22 67:7</p>	<p>sake 40:18 salt 56:22,25 samTrans 38:25 67:11,11 90:23 San 23:8,25 24:15 24:19 39:1 44:19 48:24 53:8 108:2 Santa 48:24 satisfied 103:1,5 satisfying 29:13 Saver 41:2 44:12 44:13 saw 26:10 saying 48:18 says 74:16 scale 28:17 61:12 scenario 17:4 scenarios 16:24 17:7,11 76:13 scenes 64:11 schedule 105:11 Schelstraete 30:3 32:24,25 Scheuring 50:13 55:7,8,8 scholarship 31:1,5 31:16 scholarships 31:12 school 26:1,3,10 27:8,24 28:5 30:25 31:25 36:14 36:18,21 37:1 48:18 56:7,8 60:17 65:4,17 98:15 schools 28:13 36:23 55:21,23 scope 15:13 75:10 scoping 9:8 57:5,9 72:24 search 26:8 second 12:2 65:11 65:19 71:23 secondary 80:3 seconds 87:24</p>	<p>section 5:4,5 6:9 10:3,8,10 33:5 77:2 106:11 sections 10:1 see 17:7 34:11 39:7 46:12 54:16 56:2 56:6 63:22 72:2 72:19,25 74:15 75:3,22 76:13,15 76:22 77:6 78:9 83:2 85:21 86:16 87:14 89:10 93:8 93:17 95:19 99:18 100:7 101:20,20 103:16 105:22 seeing 71:9 76:8 seeking 73:8 seen 26:4 43:5 89:4 89:4 93:7 98:1 segment 77:9 87:12 segments 15:5 19:5 73:18,19 87:10 select 53:14 100:19 self- 100:18 sell 62:18 selling 62:7 send 36:17,25 57:5 Senior 2:7 21:6 sense 36:19 45:22 49:16 100:20,20 106:24 107:1 sent 57:5,8 sentence 54:22 separate 11:3 99:20 102:21 106:13,14 separately 6:10 70:22 separation 83:9 series 16:6 serious 44:6 served 26:2 service 4:12 12:9 12:13 17:12,16 18:12 19:10 66:7 84:4</p>	<p>services 4:12 44:14 72:11 session 6:19 33:3 49:22 68:15 82:2 104:7 107:17 sessions 31:25 set 53:11 108:16 seven 48:14,18,21 77:20 91:15,24 seventh 27:4 seventy 96:5 share 20:6,7 31:16 81:8 sharing 43:17 Sheet 23:25 shoreline 56:22 58:16 short 45:1 47:11 48:11 shortage 28:13 50:3 show 24:22 29:11 59:5 86:13 104:17 showing 29:20 77:7 95:14 shown 10:12 13:15 21:19 48:1 83:15 84:11 shows 9:4 14:23 15:22 16:17 19:20 25:12 35:1 48:2 95:16 97:17 shrinking 26:18 shuttle 18:12 19:10 shuttles 43:15,16 side 5:18 29:14,14 36:24 74:12,19,21 83:14 92:13 101:18 sign 74:16 77:4 98:10 signal 80:2 84:9,13 signed 42:11 signers 42:13 significance 15:3</p>
--	---	--	---	--

<p>34:15 106:16 significant 4:9,14 4:15,21,21 8:13 13:4,25 14:1,2,6 14:10,13,15,20,24 17:18,21 18:24 19:11,12 33:8 34:17 35:2,16 38:4 39:21 51:5 56:5 70:6 71:14 72:16 86:21 89:23 94:18 107:9 significantly 55:21 82:23 signing 42:8 Sigurd 30:3 32:23 32:25 35:19 similar 45:16 54:16 79:8 99:19 similarly 103:14 Simon 35:21 38:23 38:24,25 40:22 88:23 89:15 90:24 91:14 92:9,21 simply 26:16 37:18 90:18 104:15 single 20:7 43:9 80:17 81:15 96:6 102:9 sir 92:21 site 5:3 10:12,14,17 10:19,22 11:8,17 11:24 12:6,19,23 13:20,22 17:22 18:1,2 19:22 20:24 71:17,21 79:12,23 81:18 sites 5:1 sits 94:7 sitting 61:6 85:25 situation 27:19 34:6 62:14,21 85:21 89:9 101:20 six 64:24 66:14 106:4</p>	<p>sixth 26:24 106:5 sixty 59:15 size 80:24 skilled 24:20 slave 29:11 slide 9:4 14:23 16:23 slightly 106:1 small 53:11 55:15 smaller 5:23 smorgasbord 53:2 so-called 34:14 social 53:16 54:13 soils 4:10 solar 41:13 42:1,2 43:1 53:24 54:1,7 solution 38:19 91:11 92:12 solutions 39:14 54:12 82:10,11 101:21 106:25 solve 37:12 38:9 44:8 someone's 93:22 somewhat 100:9 soon 42:5 60:3 sooner 22:10 sophisticated 76:1 sorry 6:2,4 49:17 49:19 63:13 72:20 sort 46:3 47:23 68:14 69:4,6,9 85:1,1,5 101:1,3 sound 61:16 sources 52:7 53:13 53:22 south 10:17,18 85:19 Southern 98:2 space 12:11,17 38:15 58:13 72:9 spaces 11:1 12:12 spans 77:24 Spark 41:5,8,16 speak 21:7 22:16</p>	<p>22:23 23:1,8 24:14 25:17 32:18 49:21 55:2 58:2 64:19 65:13 67:2 speaker 23:4 35:21 41:20 43:13 47:6 66:10 speakers 47:3 70:2 speaking 26:23 50:17 61:3 species 42:24 specific 41:16 73:13 75:1 79:23 87:25 88:10 101:2 specifically 33:4 35:8 38:20 45:15 69:7 70:20 73:4 74:25 76:12 93:11 98:6 102:13 specifies 34:14 speed 48:6 85:4 spending 99:8 split 81:9 spoken 100:5 sponsor 53:25 54:7 sponsored 32:3 50:21 53:23 spot 83:11 spread 20:10 33:24 83:2 96:12 spreading 96:2,20 square 10:25 11:14 11:16,20,22 12:4 12:10 97:22 staff 2:6 5:10 7:11 21:19 22:18 23:24 41:4 45:10 67:22 68:3 69:19,22,23 105:3,5 106:22 staff's 105:8 stage 26:24 85:6 standard 80:15 standpoint 80:25 86:8 Stanford 43:13</p>	<p>48:19,21 start 26:20 38:8 65:6,10 starting 36:7 83:8 starts 16:10 state 2:19 10:15 12:22 49:2 51:23 52:1,6,9 108:1 stated 14:6 108:5 statement 51:7,12 Statements 14:18 states 51:2 stay 27:7 40:21 79:20 step 37:19 39:11 steps 8:10 9:5 21:12 sticking 94:10 stock 68:5 stood 26:24 27:1 stop 65:23 77:4 98:10 104:15 store 80:6 stores 80:6 straight 14:6 straightforward 71:12 strategies 54:16 82:16 83:5 86:23 street 2:17 5:8 10:16 43:24 67:9 streets 5:24 19:14 73:16,17,20 74:15 76:5 77:10 Strehl 2:3 strength 29:24 strengthening 29:20 strike- 21:19 strong 43:2 61:25 strongly 41:17 struggling 28:13 103:10 students 31:3,7,12 48:19 56:6</p>	<p>studied 72:5 79:18 86:18 study 6:9,19 10:10 14:25 15:14,16,21 15:23 16:9,10,13 16:15,18,19 17:15 19:4 39:20,24 49:22 54:19 55:13 55:16,18 56:11 64:5 68:14,17 73:18,18,22 74:5 82:2 83:12 86:9 86:16 89:19 90:7 92:6,16 104:6 107:17 stuff 85:5 subconsultant 8:3 subject 13:1 19:18 79:4 submit 8:10 21:5 40:17 submitted 5:7 58:2 70:22 subsequently 7:16 7:17 substandard 27:19 substantial 34:18 39:12 90:11 substantially 94:6 substantive 45:13 Suburban 55:9 56:4 successfully 14:5 Sue 28:5 sued 48:6 sufficient 35:5 sug 57:15 suggest 59:18 97:16 suggested 70:5,6 suggestion 98:7 100:14 suggestions 67:3 104:3 suggests 48:13 51:7 summarize 20:22</p>
--	---	--	---	---

69:23 summarizes 5:25 16:23 summer 9:6,17 10:21 31:24,24 63:14 Sun 41:25 80:1 Sunday 66:25 supply 46:7 support 2:9 23:10 23:17 24:3,22 25:8,8 30:11,19 30:19 31:20 32:9 39:12 40:4,13 41:11,17,25 42:11 43:19,23 63:19 66:23 94:24 supporter 40:2 supporting 43:21 sure 7:15 55:12 60:20 63:4 84:25 88:2 89:15 90:2 99:10 106:6 Surely 28:20 surge 26:8 surprise 95:2 surprised 33:6 35:14 94:23 sus 74:9 Susan 2:4 103:6 suspect 74:9,21 sustainability 41:9 44:3 52:20 sustainable 41:17 42:15 swallowed 28:18 system 17:23 83:6 systems 4:12 42:2 54:1 85:4	104:17 taken 108:5 talented 26:11 talents 29:23 talk 30:20 36:5 57:16 59:21 62:3 69:1 75:19 99:15 104:13 talked 82:16 talking 45:4 61:7 61:21 71:13 99:3 105:6 talks 86:17 tax 62:13,14 TE 10:20 teacher 26:3,19,19 teachers 26:11 27:17 teaching 29:19 team 9:22 13:8 technical 45:5 technology 31:22 teeth 93:21 tell 39:6 49:20 56:3 89:19 90:14 92:15 ten 10:23 53:25 91:12 97:21 98:3 98:11 99:9 tenant 62:10 tenants 62:15 Terminal 85:23 terms 19:13 49:8 70:9 74:4 77:8 83:13 88:13 93:25 94:5,6 99:16 100:23 105:10,15 Terri 47:6,7 50:8 50:11 terribly 87:20 text 21:20,20 thank 7:6,19,20 21:24 23:8,19,20 24:10,11,13 25:9 25:17,19 27:23,25 30:1,2 32:17,18	32:19,24 35:18,19 38:21,22 40:12,19 40:20 44:9,10 46:24,25 47:8 50:6,7,12,14,18 54:6 56:13,14 58:17,19,22 60:22 61:2 63:8,9 64:14 64:15,20 65:2,6 65:10,12,15 66:8 66:9 67:17,18,25 68:1 69:10,25 71:7,11 72:6,18 77:13 78:16 82:6 86:4 87:8 88:20 90:24 92:2,5,20 92:22,24 93:4 94:15 105:1 Thanks 55:4 theoretically 83:19 they'd 20:17,19 thick 93:22 107:12 thing 36:12,15 37:15 47:13 48:12 50:1 58:2 63:19 64:12 72:7 things 59:2,3,6 60:7 60:10 61:18 64:10 64:20 84:23 87:2 92:11 93:12,14 99:16 think 29:10 30:4 33:16,18,25 34:21 34:23 35:4,16 36:11,12,22 37:11 37:14,16,20 38:5 38:8,8,12 39:4 45:25 46:17 47:18 48:12 49:1,3 55:23 59:1,4,20 60:18 62:4,5,23 68:3 70:2 74:5 76:19 77:20,21 78:10 79:25 81:9 84:25 88:5,17,21	89:16 90:12 91:7 91:18,22 92:25 93:6,17 94:4 97:24 99:10,24 100:1,4,15,22,24 101:1,17 103:3,9 103:12,15,23,25 104:23 105:17 106:10 107:3,6 thinking 61:7,18 third 7:3 12:7 thirty 59:15 106:18 thorough 46:19 103:5 thoroughly 95:4 thought 48:11 thoughtful 40:5 thoughts 36:1 94:18 106:8 thousand 31:9 65:15 thousands 23:13 24:18 45:4,5 three 17:6 22:25 58:23 74:1 76:7 82:15 87:19 97:21 98:13 threshold 18:23 thresholds 34:14 through- 58:6 through-traffic 60:15 throw 52:14 98:8 time 5:19 10:21 23:2 24:20 25:10 27:23 29:5 30:20 31:24 33:10 35:20 45:1 47:11 52:1 60:20 63:15,18 64:1 84:14 87:5 95:3 98:14 104:6 104:13 106:19 107:4 108:5 timeframe 46:8 timeline 86:24	105:15 timer 23:1 times 80:7 105:12 TJKM 2:11 8:3 10:2 14:21 15:10 15:12 today 70:12 80:14 84:1 86:21 87:1 tolerate 27:18 tone 85:2 tonight 5:12 6:6 7:21 8:2 9:1 10:5 21:7,24 22:7,16 23:9 24:14,22 25:18 32:18 36:2 40:25 47:5,16 50:17,24 53:24 57:4 62:1 64:10 66:10 68:1 70:2 70:25 71:4 77:20 93:6,19 100:5 104:9 105:17 tonight's 8:19 tool 13:6 topic 22:12 78:18 topics 13:16,17,19 69:4,6 103:3 Torreano 23:5,23 23:24 total 11:18 15:25 81:9 totaling 10:24 totally 49:15 town 55:22 tracking 81:14 Trade 24:16 trades 23:15 24:21 traf 96:2 traffic 5:23 14:25 15:2,4,18,21,23 16:2 17:10 18:6 18:20,24 19:3,4 19:12,16 20:10,14 20:20 21:2 28:12 32:13,13,16 37:15
---	--	---	--	--

T

table 82:12
take 6:9,12 17:17
18:17 33:20 43:15
46:13 51:10 59:5
66:22 73:16

37:16,18,22 38:4 49:8,10,11 55:13 55:25 56:4,11 59:9 60:15 61:21 61:22 63:24,25 64:7 71:23 72:2 72:24 73:1,3,7,11 73:21,22,23 74:13 75:8,17,20,24 76:2,8,9,15,23 77:8 82:10,23,23 82:25 83:1,16,25 84:6,9,10,13,13 84:16,16,17 86:3 87:24 89:23 90:3 90:4,5,8 92:12 93:12,25 94:4,11 94:17 96:2,21 97:21,25 100:24 100:25 101:5 103:14,14,19 107:10,12 tragic 65:8 trail 12:23 92:10 trails 57:2 trained 23:15 training 30:25 TRANSCRIPT 1:15 transit 15:6,18 19:6 19:7,8,11,13 39:15,25 43:9 58:7 transpired 57:8 transportation 2:8 2:11 4:23 8:3 10:2 13:18 14:21 15:12,14 16:25 17:23 32:13 39:1 39:11,15,20,25 43:6,8,15 54:15 55:16,18 66:5 75:13 78:20 80:11 80:11 82:4 89:7 89:20	travel 76:1 traveled 15:20 18:8 49:9 trees 42:22,22 tried 37:24 tries 73:13 trip 19:16,18,20,21 20:16 21:1 78:9 78:20,20,24 79:5 79:8 80:15 81:3 81:14 82:20 95:8 95:10,16,16 96:8 trips 18:16 19:19 19:19 20:1,4,6,17 43:10 77:23 78:3 78:7 79:13,17 81:7 82:19 95:20 95:22 96:6,23 97:1,4,6,14,17 103:17 troubling 107:8 true 40:6 108:6 truly 29:19,24 trust 63:19 try 41:21 86:1 100:7 trying 60:12 64:6 92:13 100:12 101:3 tune 24:7 turn 5:10 6:22 15:10 35:10 74:16 85:17,19,25 87:13 turnabout 58:4 turned 57:14 98:21 twelve 91:13 twinkle 86:9 two 5:18 6:7,10 11:12 26:23 31:16 36:20 42:1,20 45:5 55:20 60:25 64:20,22 65:9 70:21 71:12 74:1 80:2 81:1 95:10 98:4	two- 96:5 two-hour 96:5,9,14 type 45:25 62:13 64:11,11 typically 17:7 77:5 87:11 106:10 <hr/> U <hr/> ultimately 12:8 86:19 unable 27:4 unassociated 89:18 unavoidable 4:22 14:15,20,24 51:6 66:1 86:21 unbearable 28:12 underlined 21:20 underscore 24:23 undersigned 108:3 understand 46:18 57:18 60:7 understandable 36:19 understanding 78:24 81:17 understood 45:20 95:9 undertaking 39:24 90:7 underway 89:19 90:15 unfortunate 59:4 Unfortunately 27:10 34:2 union 23:7,25 24:24 25:6 unique 29:1 units 46:14 48:9 University 16:20 76:16 77:2 80:4 83:9 unleash 53:15 unluckily 52:22 Unrelated 78:18 unsafe 27:19 unsignalized 77:5	unusual 59:16,18 update 5:13,21 7:3 17:8 upward 46:8 Urban 68:7 urbanized 13:21 urge 60:18 106:19 use 4:10 23:14 66:5 88:9 90:10 uses 10:24 11:23 12:4 76:15 usual 104:9 utilities 4:12 42:12 <hr/> V <hr/> vacancy 33:14 values 46:4,15 57:11,17 variety 89:25 vehicle 14:25 15:2 15:4,18,19 18:7,9 18:13,18 20:1,4 49:9 80:15,16,17 80:18,22 95:16,20 95:22 96:8,23 97:1,4,14,17 vehicles 15:6 19:8 19:13 79:11 80:20 81:21 96:14,15 98:13 vehicular 79:5,8 venues 98:1 verge 27:13 viable 37:22 vibrant 51:20 Vice 2:3 victims 26:12 Victor 23:21,24 view 58:24 Viktor 23:5 virtual 53:2 visible 87:17 visit 58:12 visitors 97:6 visual 80:6 volume 44:24 74:9	volumes 19:4 77:9 <hr/> W <hr/> wait 94:12 101:19 waiting 7:12 93:16 waiver 62:13 walk 66:21 wall 28:23 want 8:18 27:7 29:10 38:14 41:15 41:19 44:5 51:9 58:5 59:12 61:13 63:22 65:2 72:7 86:1 88:22 93:3 94:20 wanted 36:1,5 44:23 45:11,13 50:18 53:19 57:7 57:22 58:17 75:17 wants 63:23 warehouse 10:23 warranted 84:9 wary 62:24 waste 5:1,3 43:3 water 4:17 38:16 42:24 50:1,3 way 16:22 18:17 29:2,12 36:10 37:17 38:17 40:9 40:10 42:3,8 45:20 46:20 48:11 49:6,19 58:7 64:6 66:21 78:11 85:22 89:6 93:23,25 100:15,25 101:12 108:10 ways 13:13 66:17 73:8 93:20 101:6 WAZE 101:8,11,11 101:15 we'll 6:9,12 22:7 23:2 32:20,21 51:9,18 68:14 86:15 104:18 we're 7:12 8:11 16:24 23:16 24:22
--	---	--	---	---

25:4 30:8 31:11 31:19 32:15,22 37:13,20 39:24 41:5 45:4,17,17 45:22 48:11 49:20 50:22 59:25 62:1 63:14 69:24 70:14 71:13 76:8 79:7 81:20 82:22 83:4 84:23 85:6,7 86:25 90:7 92:9 92:18 93:16 100:19 103:10,13 105:6 106:25 we've 25:3 26:3 32:12 43:5 50:4 57:10 58:2 62:1 63:20 75:16 83:8 84:13 85:2,12 91:9 93:6 94:1 100:23 104:4 106:19 wealth 29:14 wealthy 28:14 week 69:23 82:5 99:22 106:3,5 weeks 26:24 106:3 106:4 welcome 29:5 47:6 61:1 welcoming 57:25 58:16 well-posed 107:15 went 94:5 weren't 91:3 west 10:17 16:7,22 20:2 74:21 75:2,5 92:13 western 12:5 58:11 whatever's 63:3 WHEREOF 108:15 wide 17:22 wildlife 56:21 57:11,17,24	willing 39:10 65:16 willingness 39:13 Willow 37:25 38:2 73:4,5,9,14,24 74:2,9 76:6 78:13 80:3 85:24 willows 73:2 75:7 76:12,20 77:4,10 83:15 withdrawn 50:11 withhold 47:2 WITNESS 108:15 witnessing 56:4 women 24:20 wonderful 47:12 88:23 90:16 92:1 Woodland 73:7 75:6,6 76:15,16 77:2,3 83:15,21 words 104:1 work 7:23 15:14 23:15 25:9 26:16 27:16 28:16 29:22 32:8 39:17 44:7 56:20 62:14 80:12 89:12 90:14 91:8 107:14 worked 39:2 62:12 86:12 workers 24:1,17,25 25:7 55:25 Workers' 23:7,25 working 25:23 37:13 40:1,10,10 41:6,12 44:2 45:10 49:5 67:11 69:18,24 84:15 90:22 91:10 95:2 99:15 works 18:14 44:17 58:18 world 29:12,18 52:24 106:1 worried 63:6 worry 45:23 63:2	85:10 worse 47:25 48:5 56:9 93:14 worship 54:2 worst 48:25 worthy 26:21,23 90:21 wouldn't 61:25 68:25 69:8 71:20 74:7 wrap 49:18 54:4 Wray 28:1 30:5,5 writing 32:4 47:10 written 5:6 21:14 28:4 106:22 wrong 35:3 63:13 <hr/> X <hr/> Y <hr/> yeah 71:5 78:10 87:22 95:15 99:21 year 26:10 31:7,17 47:23 48:8,10,20 65:8 79:21 98:15 years 25:7,24 26:2 27:5 48:20 50:3,4 56:5 61:22 62:2 64:22,24 66:14 67:9 91:1,7,13,15 91:25 94:2 <hr/> Z <hr/> zero 52:10,11,12 71:17 zones 54:11 88:5 zoning 102:4 <hr/> 0 <hr/> 1 <hr/> 1,500 95:25 1,700 11:1 1,800 20:4 95:22 10 79:3 80:1 95:12 101 39:24 75:2,2,5	75:6 78:12 107 3:9 11 104:13 11:00 104:9 11th 5:9 9:11 21:11 70:23 121,300 11:19 13,100 48:10 15 3:5 15,000 48:8 150 20:20 151.05 59:17 16,000 96:25 175 33:12,23 175,000 11:15 12:10 19 79:4 80:1 1990 51:24,25 1994 48:7 <hr/> 2 <hr/> 2.5 97:3,14 20 1:16 2:16 10:16 42:20 79:4 95:12 200 12:9 2005 52:5 2010 48:8 50:2 2012 31:1 2015 9:7 42:20 48:3 2016 1:16 2:16 5:9 9:18 48:10 108:18 2020 47:23 51:24 52:5,8,10 2030 51:25 52:8,11 52:13 2040 17:6 48:2,2 21 11:13,21,25 95:11 22 11:13 12:1,3 95:11 23 3:6 11:8 25:24 64:23 24 32:22 24-hour 79:15 24/7 79:21 25 87:21 107:11	26th 9:10 27 52:5 277 33:12,23 280 16:22 29th 69:21 <hr/> 3 <hr/> 3,000 20:1 3,100 20:17 3,400 20:19 3,500 12:12 3:00 74:17 300 4:5 10:13 306 11:11 307 11:2,11 309 4:5 10:13 11:2 316 88:5 317 88:5 33 52:7 365 79:21 <hr/> 4 <hr/> 4 69:1,9 4,000 25:6 4:00 79:14 400 20:18 95:23 405 98:2 45 98:19 104:25 107:5 45-day 45:6 105:8 105:12 450,000 12:4 47 39:16 <hr/> 5 <hr/> 5 3:3 5:00 74:17 5:30 5:9 21:10 70:23 513 11:22 5527 1:24 2:19 108:20 58 10:19 <hr/> 6 <hr/> 6,000 97:8
--	---	---	--	--

6,500 97:2**6:00** 79:14**605** 30:8**65** 99:8**65962.5** 5:4**66** 15:25 72:5**67** 3:7

7

7 3:4**7:00** 79:14**700** 91:15**701** 2:17 5:8**75** 12:15

8

8 37:12**800** 31:8**82** 36:16**84** 10:15 12:22

37:25 38:1,2

87 73:18

9

9:00 79:14**9:17** 2:16**900,000** 97:22**93** 3:8**962,400** 11:13



STAFF REPORT

Planning Commission

Meeting Date:

7/25/2016

Staff Report Number:

16-058-PC

Public Hearing:

Variance/Lori Hsu/207 Lexington Drive

Recommendation

Staff recommends that the Planning Commission approve a variance for a rear addition to an existing nonconforming single-story residence in the rear yard setback on a lot in the R-1-U (Single-Family Urban) zoning district. The addition would consist of filling in an existing covered porch, with the new wall located approximately 18.4 feet from the rear property line, where 20 feet is required. The recommended actions are included as Attachment A.

Policy Issues

Each variance request is considered individually. The Planning Commission should consider whether the required variance findings can be made for the proposal.

Background

Site location

The project site is located at 207 Lexington Drive. Using Lexington Drive in the north-south orientation, the subject property is on the west side of Lexington Drive between Woodland Avenue and Robin Way, in the Willows neighborhood. A location map is included as Attachment B. The subject property is surrounded by single-family residences that are primarily single-story, although two-story residences can also be found along Lexington Drive and throughout the neighborhood. Older residences in the neighborhood are generally one story in height, while newer residences are typically two stories in height. Single-story residences in the neighborhood tend to have a ranch or bungalow architectural style, while two-story residences have a variety of styles including mixed contemporary and craftsman. Nearby properties are also in the R-1-U (Single Family Urban) district.

Analysis

Project description

The subject site is currently occupied by a single-story residence with an attached one-car garage. The property is substandard with regard to lot width, depth, and area. The applicant is proposing to enclose the existing 23.3-square-foot covered porch at the rear of the house, thereby adding floor area. The proposed addition would be confined to the existing recessed area and would not extend beyond the existing façade. The existing rear yard setback of approximately 18.4 feet, where 20 feet is required, is considered nonconforming, and the applicant is requesting a variance to construct new floor area within the existing

recessed notch along this façade. The proposal would not extend beyond the existing foundation or roof line, and would allow for a unified façade. The variance request is discussed in more detail in the Variance section of this staff report.

Furthermore, the applicant is also proposing to modify the existing, nonconforming 270-square-foot trellis in the rear yard in order to bring it into compliance with the Zoning Ordinance's regulations for accessory structures (§16.68.030). Specifically, the applicant would detach the trellis from the main structure, making it an accessory structure defined by Zoning Ordinance §16.04.665, and reduce the overall size of the trellis to 240 square feet in area to meet the minimum required rear setback of three feet for accessory structures.

The Floor Area Limit (FAL), building coverage, height and daylight plane of the proposed residence would remain well below the maximum amounts permitted by the Zoning Ordinance. In addition, the existing nonconforming left and right side setbacks and parking situation would remain, as may be permitted on remodel/expansion projects. A data table summarizing parcel and project attributes is included as Attachment C. The project plans and the applicant's project description letter are included as Attachments D and E, respectively.

Design and materials

The existing residence has a composition shingle roof, stucco walls on all facades, a brick chimney, and existing wood and steel windows, which are all to remain. The proposed rear addition would be enclosed with a new glass French patio door with wood trim, which would be consistent with some of the remaining fenestration on the house. The proposed modification would create a consistent aesthetic appearance on this façade, which is not particularly visible from other properties. The proposed infill area would allow the interior rooms to be slightly reconfigured. The reconfiguration would result in a minor internal garage door relocation and remodeling of the north side of the kitchen area to improve the internal circulation flow. The rectangular design and wood material of the existing trellis would remain the same for the modified trellis. Staff believes that the scale, materials, and style of the proposed addition would be consistent with the surrounding neighborhood, given its limited scope and the variety of architectural styles and sizes of structures in the vicinity.

Variance

As part of this proposal, the applicant is requesting a variance for the new residence to encroach into the required 20-foot rear yard setback. The rear yard setback of the proposed addition would be approximately 18.4 feet. The applicant has provided a variance request letter that has been included as Attachment F. The required variance findings are evaluated below in succession:

- 1. That a hardship peculiar to the property and not created by any act of the owner exists. In this context, personal, family or financial difficulties, loss of prospective profits and neighboring violations are not hardships justifying a variance. Further, a previous variance can never have set a precedent, for each case must be considered only on its individual merits;*

The lot is substandard as to lot width, depth, and area, the existing residence is nonconforming as to three setbacks (left side, right side, and rear), and the garage and driveway limit the potential for new

construction at the left-front portion of the residence. These conditions represent a hardship unique to the property, as there is no side to expand the existing house without reconfiguring the complete building layout. This hardship was not created by the current owner as the nonconformities are existing conditions of the house and site.

2. *That such variance is necessary for the preservation and enjoyment of substantial property rights possessed by other conforming property in the same vicinity and that a variance, if granted, would not constitute a special privilege of the recipient not enjoyed by his/her neighbors;*

Allowing the 23.3 square foot rear nook to be enclosed would preserve substantial property rights of those neighboring conforming properties as the existing nonconforming rear yard setback of 18.3 feet is unique to this property and would remain the same. Furthermore, the proposal would not add additional building coverage, and the structure would remain approximately 1,000 square feet below the maximum FAL. The variance would simply allow the property owner to preserve the existing building layout and improve the internal circulation flow within the modestly-sized residence.

3. *That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property; and*

As the proposal does not add additional building coverage to the existing house, the granting of the variance would not change the building footprint and massing of the house, therefore the enclosing of the existing rear covered porch would have no effect on the public health, safety, or welfare, and would not impair the supply of light and air to the adjacent property. Furthermore, since the modification is at the rear of the house, there is no negative effect on the public health, safety, or welfare as it may not be seen from the public right-of-way.

4. *That the conditions upon which the requested variance is based would not be applicable, generally, to other property within the same zoning classification.*

The conditions upon which the requested variance would be based upon are specific to this property. The conditions of the existing site plan, substandard lot dimensions, internal circulation layout, and three nonconforming setbacks make the requested variance unique to this property and not generally applicable to other properties within the same zoning classification.

5. *That the condition upon which the requested variance is based is an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process.*

The property is not within any Specific Plan area, and thus a finding regarding an unusual factor does not apply.

Due to the above factors, staff is recommending approval of the variance request, and has included findings to that effect in the recommended actions (Attachment A).

Trees and landscaping

Currently, there are five trees on or near the project site, all of which would remain. Standard heritage tree protection measures will be ensured through recommended condition 3e, and no heritage tree impacts are

expected given the limited scope of the project and the distance between the trees and the area of construction.

Correspondence

The property owner indicates that he performed outreach by sending the adjacent property owners a letter regarding the proposed project. A copy of the letter he sent to his adjacent neighbors is included as Attachment G. The applicant has submitted a copy of the neighbors' correspondence from adjacent neighbors at 203 Lexington Drive, 209 Lexington Drive, and 627 Woodland Avenue in support of the proposal, which is included as Attachment H. Staff has not received any other correspondence thus far.

Conclusion

Staff believes that the variance would allow the property owner to more efficiently reconfigure and use the interior spaces. Staff believes that the variance request is justified due to unusual factors including the existing nonconforming setbacks that were previously created and unique to this property. The proposed addition would be contained within the existing footprint and beneficial to improving the internal circulation flow and use of the modestly-sized home. Staff recommends that the Planning Commission approve the proposed variance.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Recommended Actions
- B. Location Map
- C. Data Table

- D. Project Plans
- E. Project Description Letter
- F. Variance Letter
- G. Correspondence to Adjacent Neighbors
- H. Correspondence from Adjacent Neighbors

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:
Sunny Chao, Assistant Planner

Report reviewed by:
Thomas Rogers, Principal Planner

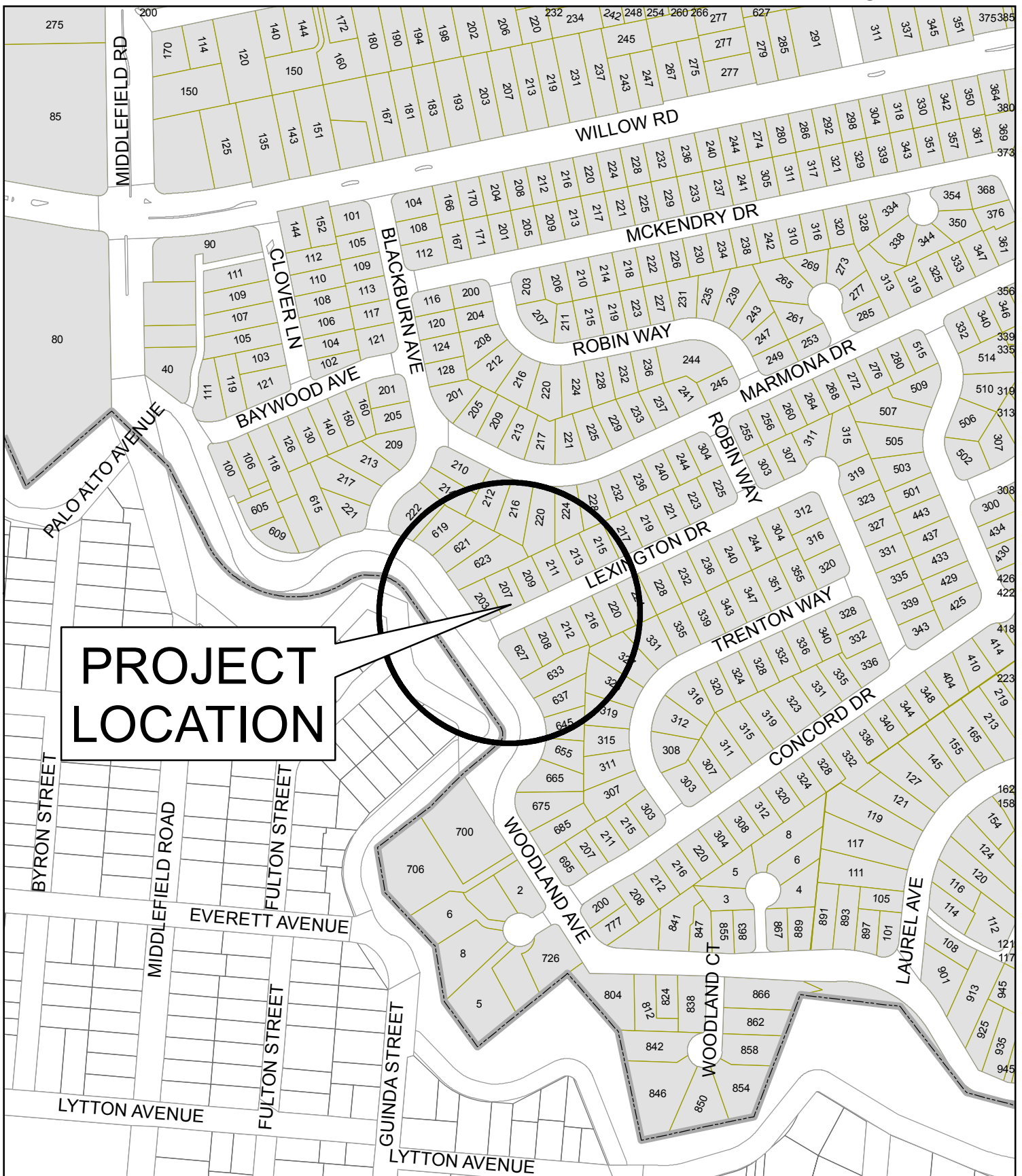
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207 Lexington Drive – Attachment A: Recommended Actions

LOCATION: 207 Lexington Drive	PROJECT NUMBER: PLN2016-00028	APPLICANT: Lori Hsu	OWNER: Timothy Oleno
REQUEST: Request for a variance for a rear addition to an existing nonconforming single-story residence in the rear yard setback on a lot in the R-1-U (Single-Family Urban) zoning district. The addition would consist of filling in an existing covered porch, with the new wall located approximately 18.4 feet from the rear property line, where 20 feet is required.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: <ol style="list-style-type: none">1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, “Existing Facilities”) of the current California Environmental Quality Act (CEQA) Guidelines.2. Make the following findings as per Section 16.82.340 of the Zoning Ordinance pertaining to the granting of variances:<ol style="list-style-type: none">a. The lot is substandard as to lot width, depth, and area, the existing residence is nonconforming as to three setbacks (left side, right side, and rear), and the garage and driveway limit the potential for new construction at the left-front portion of the residence. These conditions represent a hardship unique to the property, as there is no side to expand the existing house without reconfiguring the complete building layout. This hardship was not created by the current owner as the nonconformities are existing conditions of the house and site.b. Allowing the 23.3 square foot rear nook to be enclosed would preserve substantial property rights of those neighboring conforming properties as the existing nonconforming rear yard setback of 18.3 feet is unique to this property and would remain the same. Furthermore, the proposal would not add additional building coverage, and the structure would remain approximately 1,000 square feet below the maximum FAL. The variance would simply allow the property owner to preserve the existing building layout and improve the internal circulation flow within the modestly-sized residence.c. As the proposal does not add additional building coverage to the existing house, the granting of the variance would not change the building footprint and massing of the house, therefore the enclosing of the existing rear covered porch would have no effect on the public health, safety, or welfare, and would not impair the supply of light and air to the adjacent property. Furthermore, since the modification is at the rear of the house, there is no negative effect on the public health, safety, or welfare as it may not be seen from the public right-of-way.d. The conditions upon which the requested variance would be based upon are specific to this property. The conditions of the existing site plan, substandard lot dimensions, internal circulation layout, and three nonconforming setbacks make the requested variance unique to this property and not generally applicable to other properties within the same zoning classification.e. The property is not within any Specific Plan area, and thus a finding regarding an unusual factor does not apply.3. Approve the variance subject to the following standard conditions:<ol style="list-style-type: none">a. Development of the project shall be substantially in conformance with the plans prepared by LSH Studio consisting of six plan sheets, dated received June 29, 2016, and approved by the			

207 Lexington Drive – Attachment A: Recommended Actions

LOCATION: 207 Lexington Drive	PROJECT NUMBER: PLN2016-00028	APPLICANT: Lori Hsu	OWNER: Timothy Oleno
REQUEST: Request for a variance for a rear addition to an existing nonconforming single-story residence in the rear yard setback on a lot in the R-1-U (Single-Family Urban) zoning district. The addition would consist of filling in an existing covered porch, with the new wall located approximately 18.4 feet from the rear property line, where 20 feet is required.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: <p>Planning Commission on July 25, 2016, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.</p> <ul style="list-style-type: none"> b. Prior to building permit issuance, the applicant shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project. c. Prior to building permit issuance, the applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project. d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes. e. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance. 			

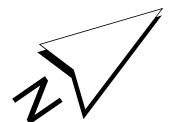


CITY OF MENLO PARK

LOCATION MAP

207 LEXINGTON DRIVE

DRAWN: TAS CHECKED: SYC DATE: 07/25/16 SCALE: 1" = 300' SHEET: 1



207 Lexington Drive – Attachment C: Data Table

	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	5,520 sf	5,520 sf	7,000 sf min.
Lot width	60 ft.	60 ft.	65 ft. min.
Lot depth	92 ft.	92 ft.	100 ft. min.
Setbacks			
Front	24.9 ft.	24.9 ft.	20 ft. min.
Rear	18.3 ft.	18.3 ft.	20 ft. min.
Side (left)	5 ft.	5 ft.	6 ft. min.
Side (right)	4.7 ft.	4.7 ft.	6 ft. min.
Building coverage	2,015.4 sf 37 %	2,045.4 sf 37 %	2,208 sf max. 40 % max.
FAL (Floor Area Limit)	1,775.4 sf	1,752.1 sf	2,800 sf max.
Square footage by floor	1,424.4 sf/1st 351 sf/garage 240 sf/trellis	1,393.4 sf/1st 358.7 sf/garage 23.3 sf/porch 270 sf/trellis	
Square footage of buildings	2,015.4 sf	2,045.4 sf	
Building height	14.8 ft.	14.8 ft.	28 ft. max.
Parking	1 covered	1 covered	1 covered/1 uncovered
Note: Areas shown highlighted indicate a nonconforming or substandard situation.			

Trees						
	Heritage trees	2*	Non-Heritage trees	3**	New Trees	0
	Heritage trees proposed for removal	0	Non-Heritage trees proposed for removal	0	Total Number of Trees	5

*One of which is located on the adjacent rear property.

**Two of which are located in the right-of-way.

PLANNING DATA - City of Menlo Park

(Zoning: R1-0A)

Occupancy Group: R3/U
 Type of Construction: V-B
 Automatic Fire Sprinklers: NO
 Lot Dimensions: 60'x92'
 Min. lot dimensions: 65'x100'
 Lot Area: 5,520 sf

Existing Livable area: 1,309.5 sf
 Existing Garage: 362.8 sf
 Total Existing sf: 1,672.3 sf

Existing Livable area: 1,309.5 sf
 Proposed addition: 79.8 sf
 Under Permit #BLDG15-01347
 convert garage into livable: 3.9 sf
 Proposed variance: 23.3 sf
 convert garage into livable: 7.7 sf
 Existing Garage: 351.0 sf
 Total Proposed sf: 1,775.4 sf

Set Backs:

Front: 20 ft. minimum
 Rear: 20 ft. minimum
 Side: 10 percent of minimum lot width
 for sides but not less than 5 feet or more than 10 feet, except street sides of corner lots which shall be a minimum of 12 feet.

Existing non-conforming set backs:

Rear: (e) 18'-6"
 Sides: (e) 5'-0" / 4'-8"

EXISTING NON-CONFORMING CONDITION

Max. building coverage:
 single story: 40%
 2-story: 35%
 Existing coverage: 1,775 sf = 32%
 Trellis: 248 sf
 Proposed Building coverage = 2,015 sf = 36%

FAL (Floor Area Limit)

Lots between 5,000 and 7,000 sf = 2,800 sf.
 Max. allowable addition:
 2800 - (e)1,752.1 = 1,047.9 sf

SCOPE OF WORK

Enclose existing covered patio at the rear of the property. Legalize existing trellis at the backyard.

CODES IN EFFECT FOR THE WORK:

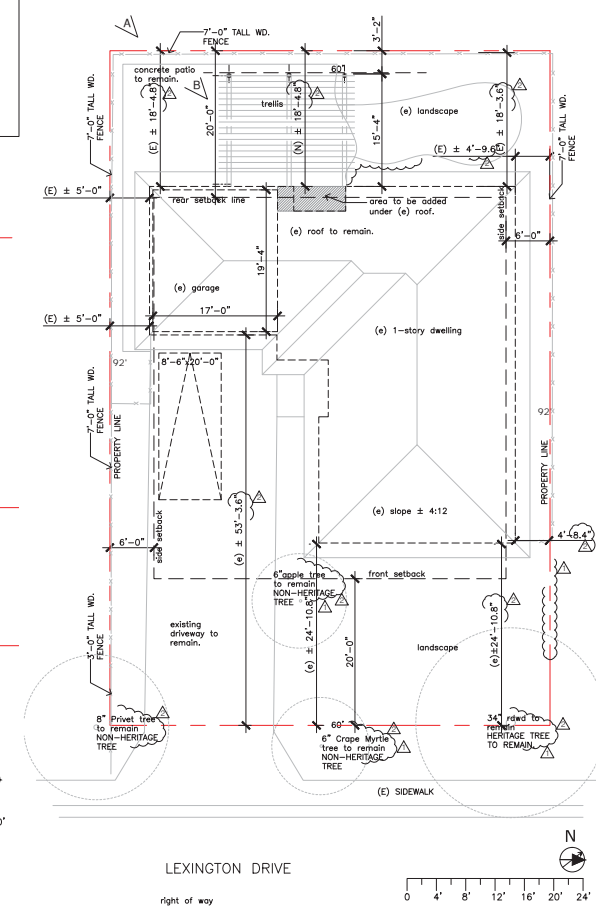
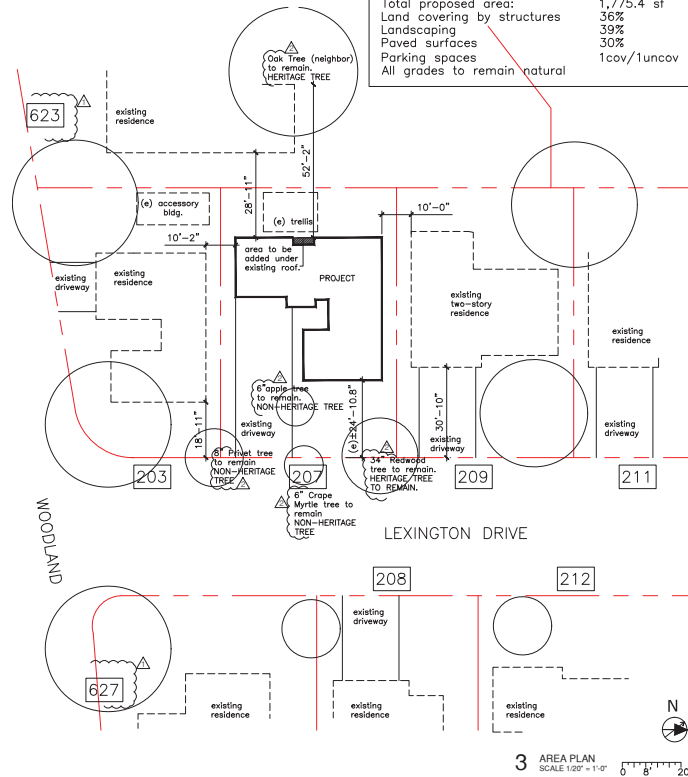
- 2013 California Building Code
- 2013 California Electrical Code
- 2013 California Mechanical Code
- 2013 California Plumbing Code
- 2013 California Fire Code
- 2013 California Energy Efficiency Code

DEMOLITION NOTES:

- The drawings do not intend to show all existing objects. Before commencing the demolition work, verify with owner and/or architect all objects to be removed and all objects to be preserved.
- Use all means necessary to protect existing objects designated to remain and, in the event of damage, immediately make all repairs and replacements necessary to the approval of the owner.
- Remove floor finishes to sub-flooring where occurs and as required.
- Existing kitchen cabinets to be salvaged and reused at the garage as shown in plans.

SITE ANALYSIS: R1-0A

Lot Area: 5,520 sf
 Allowable Floor area: 2,800 sf
 Total proposed area: 1,775.4 sf
 Land covering by structures: 36%
 Landscaping: 39%
 Paved surfaces: 30%
 Parking spaces: 1cov/1uncov
 All grades to remain natural



DRAWING LIST
 A0 EXISTING AND PROPOSED SITE/ROOF PLAN - AREA PLAN - STREETSCAPE - PICTURES
 C1 TOPOGRAPHIC SURVEY
 A1a NONCONFORMING STRUCTURE CALCULATIONS / DIAGRAMS
 A1b LOT COVERAGE - FAL CALCULATIONS / DIAGRAMS
 A2.0 EXISTING FLOOR PLAN AND ELEVATIONS
 A2.1 PROPOSED FLOOR PLAN AND EXTERIOR ELEVATIONS



Timpson-Oleno RENOVATION

for the
 Timpson-Oleno
 Family

at
 207 Lexington Drive
 Menlo Park, CA



Lori Sang Hsu Architect

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 Palo Alto, CA 94301
 (850) 289-6736 (f)
 (866) 767-1899 (t)

lori@lshstudio.com (e)

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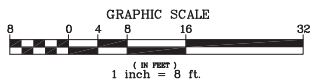
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TITLE

NEW SITE/ROOF
 PLANS

SHEET

A0



12-29-2015
2-8-2016 CORRECTED HOUSE FOOTPRINT

ABBREVIATIONS

AC ASPHALT
BW BACK OF WALK
CONC. CONCRETE
TC TOP OF CURB
FL FLOW LINE
SSMH SANITARY SEWER MANHOLE

NOTES

ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS.
UNDERGROUND UTILITY - LOCATION IS BASED ON SURFACE EVIDENCE.
BUILDING LOCATION DIMENSIONS ARE MEASURED PERPENDICULAR TO THE PROPERTY LINES.
DIMENSIONS TO THE BUILDING ARE TAKEN AT THE EXTERIOR FINISHED SURFACE. (STUCCO/SIDING)
FINISH FLOOR ELEVATION TAKEN AT DOOR THRESHOLD (EXTERIOR).
BENCHMARK: S.C.V.W.D. #1174 ELEVATION: 58.80' 1988 DATUM, 2010 ADJUSTMENT
TREE SPECIES IDENTIFICATION: BEST EFFORT, WE ARE NOT ARBORISTS OR DENDROLOGISTS

LEGEND

- FOUND POINT IN MONUMENT CASTING (AS NOTED)
- () RECORD DATA / REFERENCE
- WM WATER METER OR WATER VALVE BOX
- ⋈ FIRE HYDRANT
- ⊙ 16 12 8 OAK TREE - TRUNK DIAMETER IN INCHES
TREE SPECIES IDENTIFICATION: BEST EFFORT, WE ARE NOT ARBORISTS OR DENDROLOGISTS
- ⊙ 16 12 8 OAK TREE WITH MULTIPLE TRUNKS
- TRUNK TREE DRIP LINE POINTS TOWARDS TREE TRUNKS. TREE DRIP LINES ABOVE PROPERTY LOCATED AS SHOWN.
- TOP OF CURB
- FENCE
- OVERHEAD WIRES
- 12.34 POWER POLE
- 12.34 SPOT ELEVATION
- ⊕ IRRIGATION VALVE BOX
- ⊕ SIGN
- ⊕ ELECTROLIER
- ⊕ POLE ANCHOR
- EDGE OF AC PAVING

I CERTIFY THAT THIS PARCEL'S BOUNDARY WAS ESTABLISHED BY ME OR UNDER MY SUPERVISION AND IS BASED ON A FIELD SURVEY IN CONFORMANCE WITH THE LAND SURVEYOR'S ACT. ALL MONUMENTS ARE OF THE CHARACTER AND OCCUPY THE POSITIONS INDICATED AND ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.

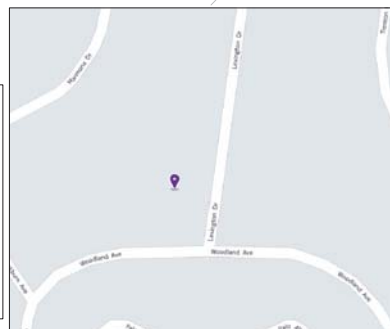
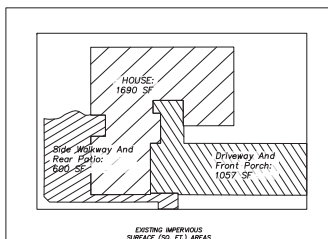


L. Wade Hammond

L. Wade Hammond
Licensed Land Surveyor
No. 6163

36660 Newark Blvd. Suite C
Newark, California 94560
Tel: (510) 579-6112 Fax: (510) 991-8054
wade@whlandsurveyor.com

SURVEY
207 LEXINGTON DR.
MENLO PARK
APN: 062-305-210
LOT 279, 29 MAPS 9
LOT AREA: 5,520 SQ. FT. GROSS & NET



**Timpson-Oleno
RENOVATION**

for the
Timpson-Oleno
Family

at
207 Lexington Drive
Menlo Park, CA



**Lori Sang Hsu
Architect**

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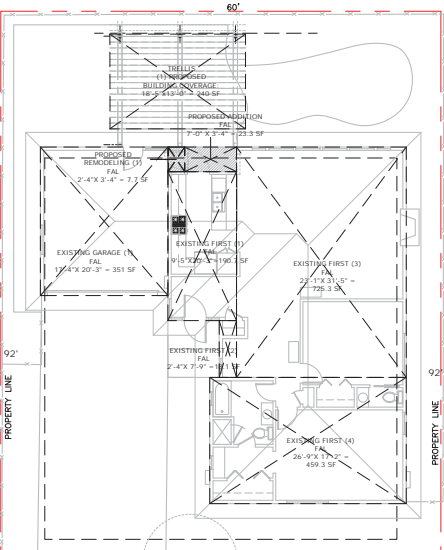
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ISSUED FOR PERMIT	08/18
REVISION	08/18
REVISION	08/18
REVISION	08/18
REVISION	08/18
REVISION	08/18

TITLE

FAL + COVERAGE
CALCULATIONS

SHEET

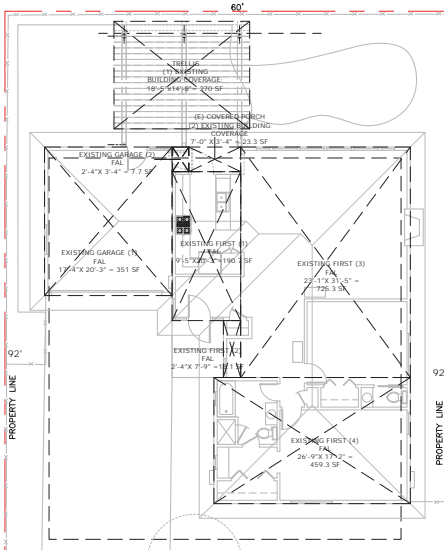
A1.b



4 PROPOSED FAL + COVERAGE
SCALE 1/8" = 1'-0"

PROPOSED FAL + COVERAGE		
	FAL	COVERAGE
(E) GARAGE (1)	351.0	
(E) GARAGE (2)	7.7	
(E) FIRST (1)	190.7	
(E) FIRST (2)	18.1	
(E) FIRST (3)	725.3	
(E) FIRST (4)	459.3	
(N) ADDITION	23.3	
(E) TRELLIS	0.0	240.0
TOTAL	1775.4	240.00

Existing trellis to be altered to
comply with Code regulations.
Existing coverage = 270 sf
It is to be reduced to 240 sf
new coverage.
Please refer to sheet A2.1 for
details



3 EXISTING FAL + COVERAGE
SCALE 1/8" = 1'-0"

EXISTING FAL + COVERAGE		
	FAL	COVERAGE
(E) GARAGE (1)	351.0	
(E) GARAGE (2)	7.7	
(E) FIRST (1)	190.7	
(E) FIRST (2)	18.1	
(E) FIRST (3)	725.3	
(E) FIRST (4)	459.3	
(E) REAR PORCH	0.0	23.3
(E) TRELLIS	0.0	270.0
TOTAL	1752.1	293.30

A photograph of a residential street. In the foreground, a large, leafy tree stands on the left side of the road. A green trash bin is visible near its base. The road is paved and leads towards a house with a light-colored roof and walls, partially obscured by more trees and foliage. The scene is captured in daylight with some shadows.

SCALE 1/8" = 1'-0"

SCALE 1/8" = 1'-0"



SCALE 1/8" = 1'-0"

SCALE 1/4" = 1'-0"

[illegible]

1 EXISTING FIRST FLOOR PLAN
SCALE 1/4" = 1'-0"

© Lori Sang Hsu

VARIANCE - PERMIT	2/9/15
REVISION 	4/4/15
REVISION 	5/19/15

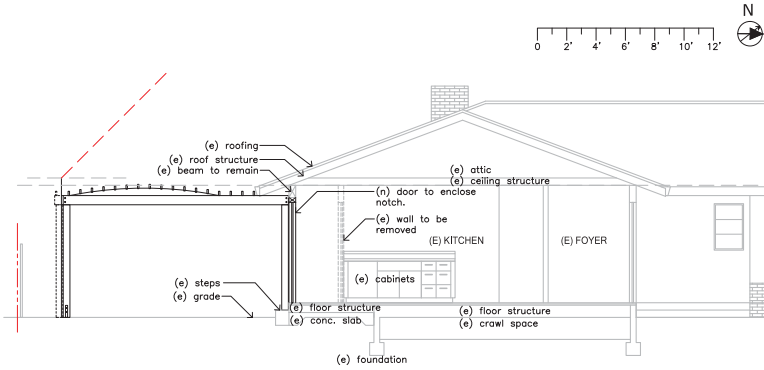
EXISTING FLOOR
PLAN AND
ELEVATIONS

A2.0

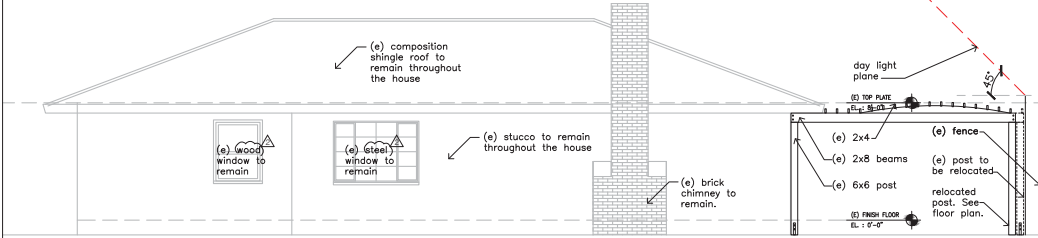
—	existing conditions to remain, typically shown grey
- - - -	existing conditions to be demolished, typically shown grey dotted
—	proposed new walls, typically shown with black lines



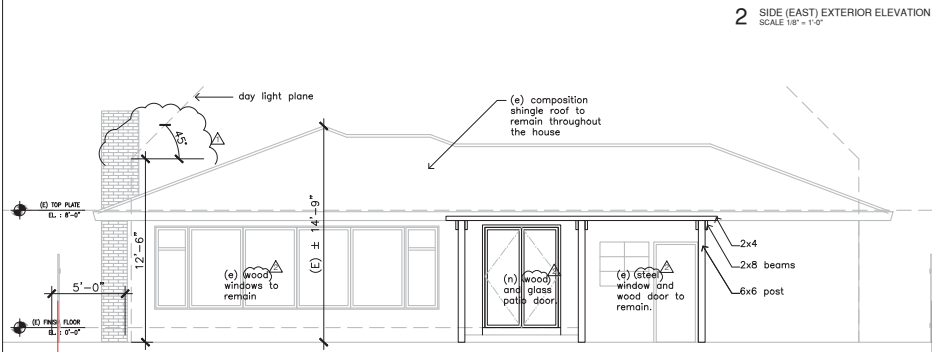
1 FRONT (SOUTH) EXTERIOR ELEVATION
SCALE 1/8" = 1'-0"



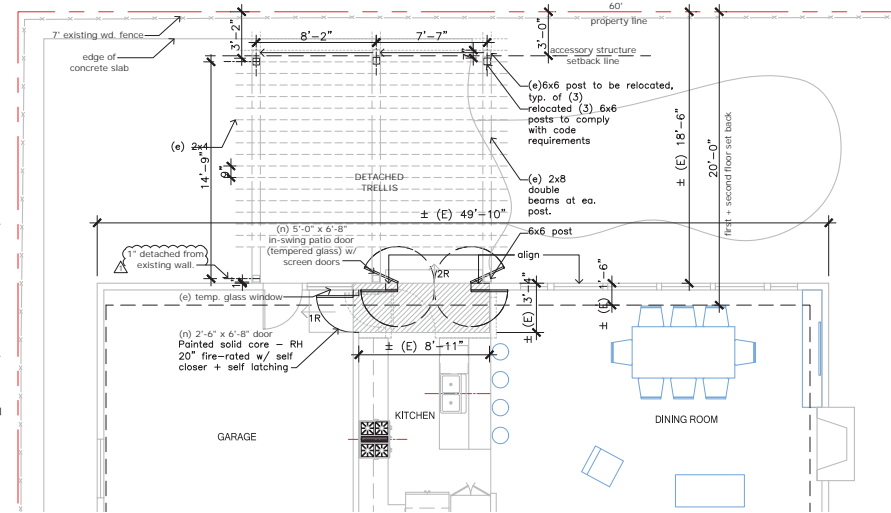
5 BUILDING SECTION
SCALE 1/8" = 1'-0"



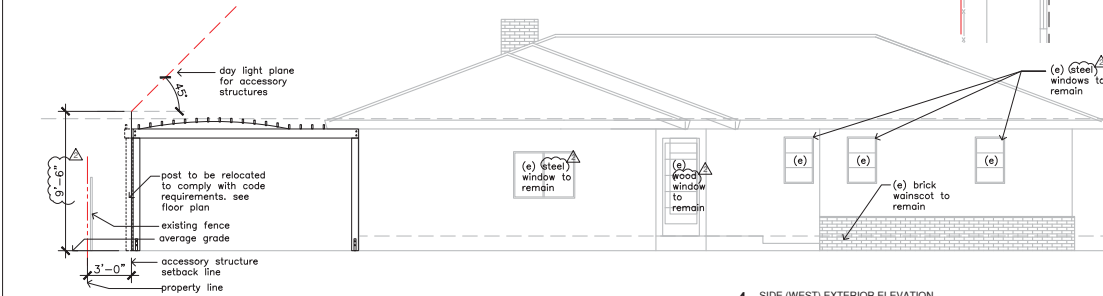
2 SIDE (EAST) EXTERIOR ELEVATION
SCALE 1/8" = 1'-0"



3 REAR (NORTH) EXTERIOR ELEVATION
SCALE 1/8" = 1'-0"



1 PROPOSED PARTIAL FIRST FLOOR PLAN
SCALE 1/4" = 1'-0"



4 SIDE (WEST) EXTERIOR ELEVATION
SCALE 1/4" = 1'-0"

All the existing window and door trims (where occur) are wood and to remain. The new rear door is wood and glass with wood trim. No new window to be added.

Timpson-Oleno RENOVATION

for the
Timpson-Oleno
Family

at
207 Lexington Drive
Menlo Park, CA

**LSH
STUDIO**

**Lori Sang Hsu
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lori@lshstudio.com (e)

© Lori Sang Hsu

ISSUE

DATE	REVISION	BY

TITLE

**ENLARGED
PROPOSED
FLOOR PLAN
AND ELEVATIONS**

SHEET

A2.1

666 High St., Palo Alto, CA 94301
650-269-6736 (phone) • 866-767-1899 (fax) • lori@lshstudio.com (email)



May 16, 2016

To:
Planning Department
650.330.6702

From:
LSH Studio
Lori Hsu
650.269.6736

Regarding: Variance Application
Timpson Oleno Residence
207 Lexington Dr., Menlo Park, CA

Project Description:

This one-story home, located at 207 Lexington Drive, is surrounded by one and two-story single family residences in a RU-1 zoning district. This original dwelling of 1309 square feet was built with permits in 1948, but on a small lot of 5520 square feet, it does not meet current minimum lot size or setback requirements, rendering this property a pre-existing, nonconforming structure on a substandard lot. This modest house encroaches 1-1/2 feet into the rear yard & 1 foot into each side yard.

At the back wall of this house exists a small recessed porch that is 7' wide and 3-1/2 feet deep. The outermost 1-1/2 feet of this porch is in the non-conforming zone (~10 square feet). This porch is located below the existing house roof, and its outer boundary is flush with the existing back wall of the house.

This Variance proposes to enclose this recess at the back of the house. Although small in size, this addition is critical to easing tight interior circulation areas between the kitchen, dining room, garage, and backyard. Since this addition would be confined to the existing recessed area, and would not extend beyond the existing façade, the change would have very minimal impact to the mass and volume of the existing structure. The addition would not extend beyond the existing foundation, and would allow for a unified façade. Neighbors would not be able to see the addition from their properties because the existing recessed porch is out of public sightlines.

The infill area is proposed to be enclosed with French doors in stucco walls consistent with the style and appearance of the existing house.

No change is proposed to Lot Coverage, as the recessed covered porch is already included in Lot coverage (below the maximum of 36 percent)

No change is proposed to the existing maximum height of 14'-9" (maximum allowable height for this zoning district is 28').

The owner of the property has sent a letter describing the project to the immediate neighbors.
(END)

Date: 6/29/2016

666 High St., Palo Alto, CA 94301
 650-269-6736 (phone) • 866-767-1899 (fax) • lori@lshstudio.com (email)



February 8, 2016

To:
 Planning Department
 650.330.6702

From:
 LSH Studio
 Lori Hsu
 650.269.6736

Regarding: Variance Application
Timpson Oleno Residence
207 Leington Dr., Menlo Park, CA

Following are the responses as per Section 16.82.340 of the Zoning Ordinance pertaining to the granting of a variance:

- a. The special substandard lot size makes the existing house non-conforming, encroaching 3' into the rear yard & 1' into both side yards. This pre-existing non-conformance restricts expanding the house to the back & sides, providing for very limited buildable area. Yet at the back center of the house there exists a small recessed porch (approximately 3'x9'), beneath the existing footprint and roof of the house. This exterior nook constricts the interior spaces in what is already a very compact floor plan, but as a pre-existing encroachment into the rear setback (approximately 1'-6"x9' encroachment) it cannot be enclosed without this Variance. These hardships are unique to the property, and have not been created by the Owner.
- b. Allowing this small exterior nook to be enclosed does not constitute special privilege because other properties in the vicinity do not have this pre-existing condition that classifies their property as non-conforming, which in-turn restricts the improvements that can be done to 3 sides of their house. To provide reasonable ability for the Owner to improve the layout of their home, the Variance to enclose this small area is necessary for the preservation and enjoyment substantial property rights possessed by other conforming property.
- c. Granting the Variance would not be martially detrimental to the public health, safety, or welfare, and will not impair an adequate supply of light and air to adjacent property. The proposed project is within the existing building footprint, in plane with the back wall of the house, covered by an existing roof, and the size and shape of the house will not be altered. The existing opening at the exterior nook is proposed to be finished with French doors opening to the backyard. Neighbors cannot see the proposed modification from their properties.
- d. The prevailing neighborhood standard is of R-1-U lots with a rectangular shape and area of approximately 6,500 square feet. This subject parcel is smaller relative to this standard. As such, the conditions on which the variance is based are not generally applicable to other property in the same zoning classification.
- e. The property is not within any Specific Plan area, and as such no finding regarding an unusual factor is required to be made.

(END)

Date: 2/8/2016

15 March 2016

Dear Neighbor,

After living in my house at 207 Lexington Drive for 35 years, I have decided to make some much needed improvements and have applied for permits to renovate my house. Through this process, I have learned that my lot is slightly smaller than the standard size, causing my house to encroach into the minimum required rear and side yard setbacks. This pre-existing condition renders my house a non-conforming structure, and as such, I need to apply for a variance to touch anything that is within the areas of non-conformance.

There is a very small screened patio recessed into the back of my house that is within one of these areas of non-conformance. This existing exterior nook is 3' deep and 7' wide, a small roofed area that has screen doors and has been there since I have lived there. I would very much like to enclose this area, as this nook is not a very nice spot to be in nor is it an attractive element facing the backyard. Given that there is no other space to expand the living spaces on the ground floor, enclosing this small nook is the only way I can improve the layout of my home. Since the nook has an existing roof and foundation, and is in-plane with the existing back wall of the house, it is a completely unobtrusive addition, and will not add any new volume to the existing house.

I hope you understand my situation and will support my project. You will be receiving or have already received information from the City later this month regarding this variance application. Please feel free to contact me if you would like to talk about any issues.

Timothy Oleno

Timothy Oleno

Cell Phone 650-799-1287

Email mitonelo@pacbell.net

Neighbor's feedback:

-----Original Message-----

From: Glen Rojas

Sent: Wednesday, March 30, 2016 7:48 PM

To: Timothy A.Oleno

Cc: Glen Rojas

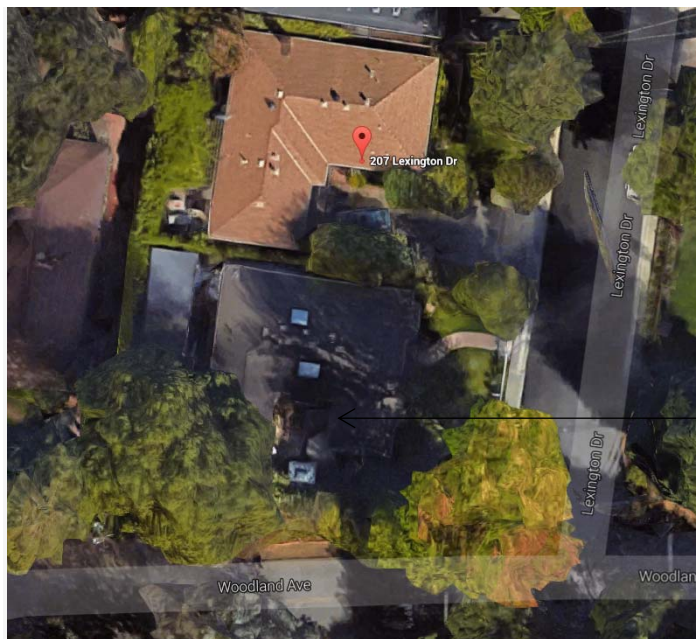
Subject: Variance

Tim

As your direct neighbor to the west at 203 Lexington Drive I am in full support of your request for a variance as submitted to the City. I have reviewed your plans and do not see any conflicts with my property nor do I see any negative impacts to the neighborhood.

I greatly appreciate the manner you have reached out to me and other neighbors early on in the process. You have been open and genuinely concerned for your neighbors. Please let me know if you or the city require any additional feedback or support for the project.

Glen Rojas =



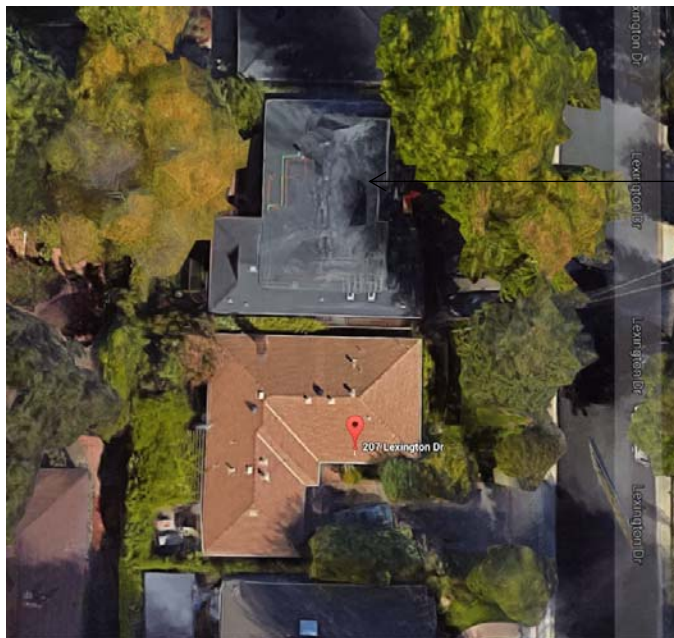
203 Lexington Drive, next
door neighbor support

From: Leon Chen
Sent: Tuesday, March 15, 2016 9:42 PM
To: mitonelo@pacbell.net
Subject: Variance

Hi Timothy,

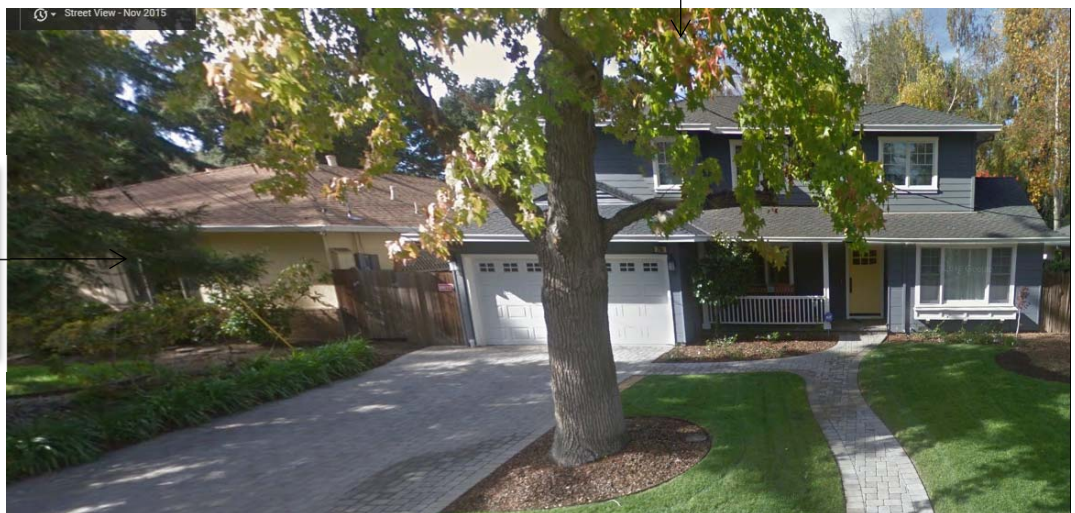
This is Leon Chen, your neighbor at 209 Lexington. We received your later today about the permitting and variance you are dealing with. I wanted to let you know that we have no problem with it at all and are happy to support your application to the city in any way I can. Please let us know how we can help.

Leon=



209 Lexington Drive, next door neighbor support. This property has been remodeled and a 2nd story has been added with no detrimental effects to the public health of the neighborhood.

207
Lexington



From: [Liliana Perazich](#)

Sent: Tuesday, March 15, 2016 5:49 PM

To: mitonelo@pacbell.net

Cc: [Branko Perazich](#)

Subject: Support variance

Dear Timothy

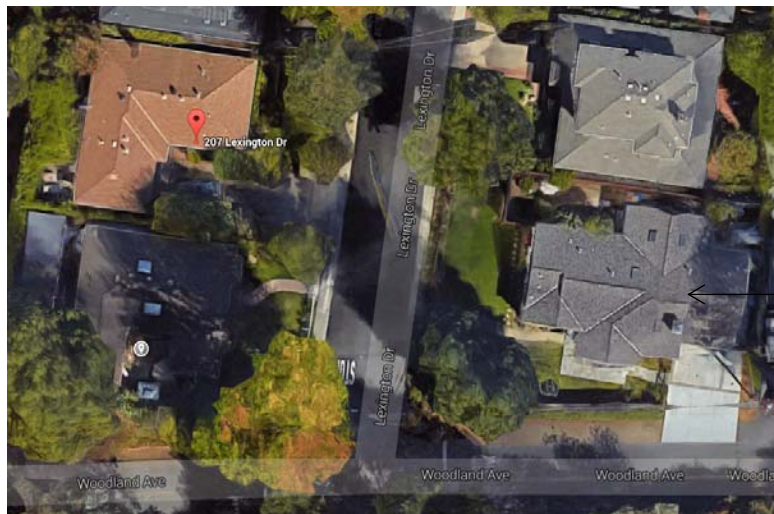
Much appreciate your letter and very much understand your constraints.

We are happy to support your variance.

All the best,

Liliana and Branko Perazich

627 Woodland



627 Woodland, neighbor
support

666 High St., Palo Alto, CA 94301
650-269-6736 (phone) • 866-767-1899 (fax) • lori@lshstudio.com (email)

LSH STUDIO
architectural design

Neighbor's feedback:

-----Original Message-----

From: Glen Rojas

Sent: Wednesday, March 30, 2016 7:48 PM

To: Timothy A.Oleno

Cc: Glen Rojas

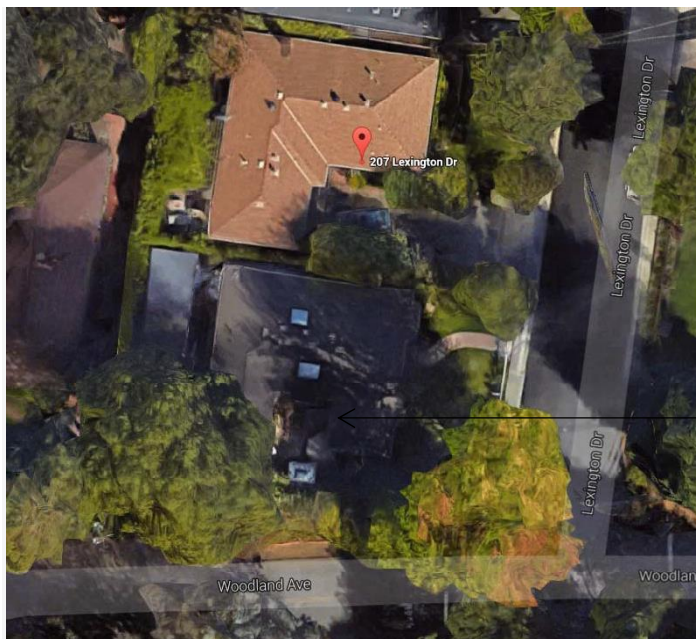
Subject: Variance

Tim

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I greatly appreciate the manner you have reached out to me and other neighbors early on in the process. You have been open and genuinely concerned for your neighbors. Please let me know if you or the city require any additional feedback or support for the project.

Glen Rojas =



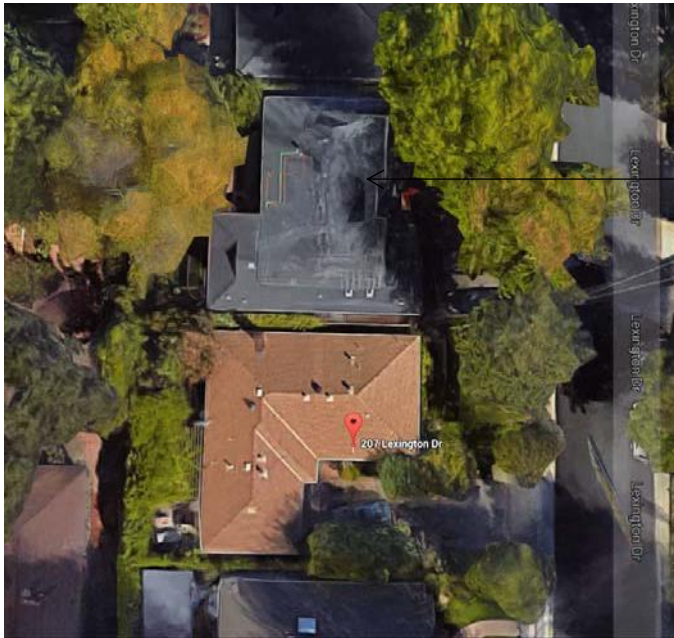
203 Lexington Drive, next
door neighbor support

From: Leon Chen
Sent: Tuesday, March 15, 2016 9:42 PM
To: mitonelo@pacbell.net
Subject: Variance

Hi Timothy,

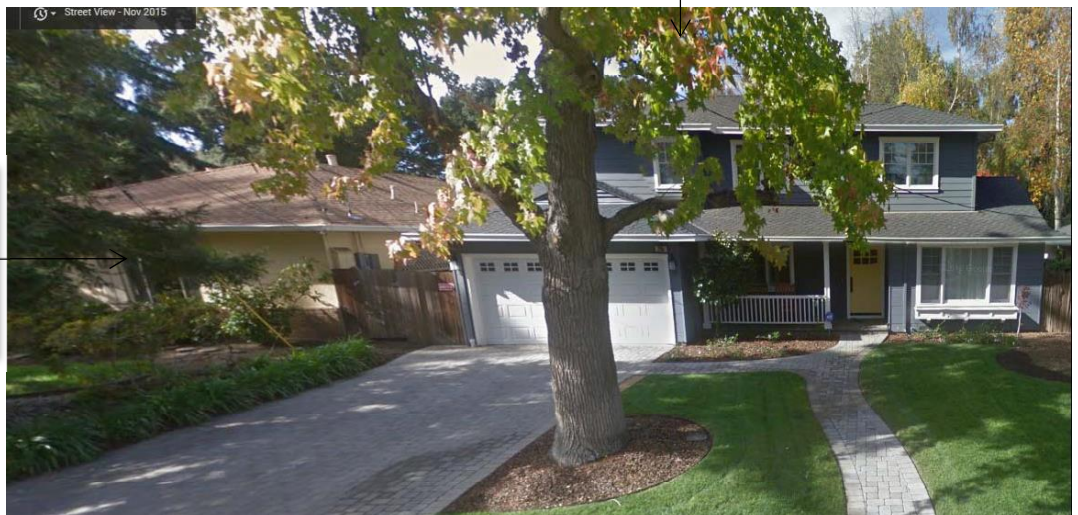
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Leon=



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Lexington



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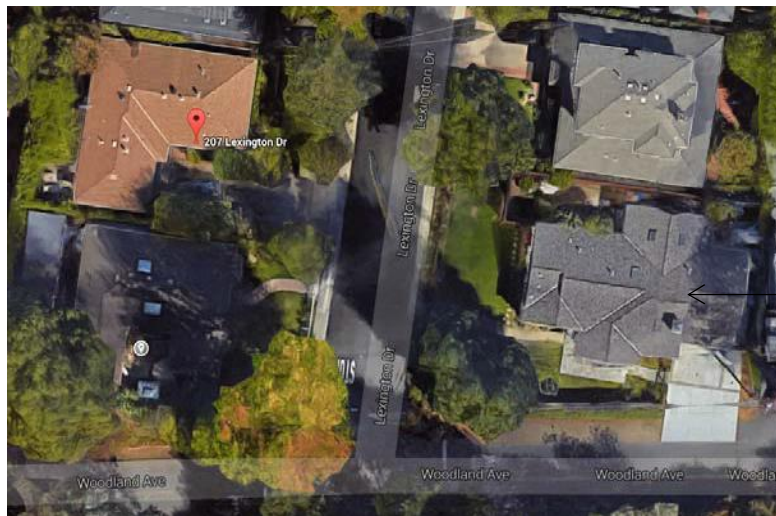
Much appreciate your letter and very much understand your constraints.

We are happy to support your variance.

All the best,

Liliana and Branko Perazich

627 Woodland



627 Woodland, neighbor
support



STAFF REPORT

Planning Commission

Meeting Date:

7/25/2016

Staff Report Number:

16-059-PC

Public Hearing:

Use Permit and Variance/Marshall Schneider/208 Oakhurst Place

Recommendation

Staff recommends that the Planning Commission approve a use permit to remodel and add a second story to an existing nonconforming single story, single-family residence in the R-1-U (Single Family Urban) zoning district at 208 Oakhurst Place. The proposed work would exceed 50 percent of the existing replacement value in a 12-month period. Staff also recommends denial of a request for a variance to allow a new covered entry with a street side setback of approximately nine feet, three inches, where 12 feet is required, with a condition of approval that would allow the project to proceed without additional Planning Commission review. The recommended actions are included as Attachment A.

Policy Issues

Each use permit and variance request is considered individually. The Planning Commission should consider whether the required use permit and variance findings can be made for the proposal.

Background

Site location

The project site is located at 208 Oakhurst Place, between Highway 101 and Bay Road, in the Suburban Park neighborhood. The area is close to the City's boundaries with the Town of Atherton. The surrounding homes are also zoned R-1-U and are predominantly single-story, single-family residences; however, two-story, single-family residences can also be found throughout the neighborhood. This is a neighborhood in transition; older existing residences tend to be one story in height, while newly built and remodeled residences are typically two stories in height. Residences on Oakhurst Place feature a variety of architectural styles including traditional ranch, craftsman, and contemporary residential.

For Zoning Ordinance setback purposes, the front property line for corner lots is the shorter of the two street-facing sides. Front doors and addresses may be located on either street frontage. In this case, the front property line is on Greenwood Drive, and Oakhurst Place is designated the corner side lot line. The front door and address are on Oakhurst Place.

Analysis

Project description

The subject site is currently occupied by a single-story residence with an attached one-car garage. The structure is nonconforming with regard to the rear and street side setbacks. The applicant is proposing to maintain the 2,021-square-foot first story, while adding a 725-square-foot second story addition, 18-square-foot first story addition, and renovate portions of the existing structure. The two existing wood trellises in the side yards are proposed to be removed. A data table summarizing parcel and project attributes is included as Attachment C. The project plans and the applicant's project description letter are included as Attachments D and E, respectively.

The proposed residence would be a four-bedroom home with four bathrooms. The first story living space includes a kitchen, combined living and dining room, three bathrooms, family room, laundry room, two bedrooms and a one car-garage. The second story would feature two bedrooms, one bathroom and a play room. No changes are proposed to the garage, and the parking would remain nonconforming, which can be permitted on remodel/expansion projects. The existing nonconforming walls at the rear and left sides of the residence are proposed to remain with the wall framing retained. The existing entry along the nonconforming left side would be accentuated with a covered entry with posts which would encroach an additional nine inches into the street side setback. This feature would require a variance, as discussed in a following section.

The floor area, building coverage, and height of the proposed residence would all be below the maximum amounts permitted by the Zoning Ordinance, and the structure would comply with the daylight plane for a two-story home in the R-1-U zoning district. The residence would meet all Zoning Ordinance requirements aside from the variance request for the entry.

Design and materials

The existing residence is a traditional ranch home featuring the characteristic long, low profile, simple gabled roof and wood siding typical of this architectural style. As part of the proposed project, the façade would be updated to achieve a more contemporary craftsman aesthetic. The existing wood siding on the exterior of the residence would be replaced with grey stucco and the proposed roofing would match the existing roofing material and color. The proposed windows would be aluminum clad, with interior and exterior grids and spacer bars between the glass. The existing garage door would be replaced and upgraded to match the new windows and doors. Additional architectural interest would be provided by wood brackets and dormers on the second story.

The new second story would be concentrated toward the center of the property and would be stepped in from the first story footprint. The closest adjacent residence, a single-story single-family home at 1036 Greenwood Drive, is approximately 10 feet away. The second story of the proposed structure is designed in such a way that potential privacy impacts should be limited. The second-story windows are proposed to have sill heights of at least three feet, and the dormers with larger windows would be located on the street sides, both of which would promote privacy for the neighboring side properties.

Staff believes that the scale, materials, and style of the proposed residence are consistent with the broader neighborhood, given the architectural styles and sizes of structures in the area.

Variance

As part of this proposal, the applicant is requesting a variance for the new residence to encroach into the required 12-foot street side setback for a new covered entry element. The setback of the proposed entry posts would be approximately nine feet, three inches, and a new roof would connect these posts with the existing roof. The applicant has provided a variance request letter that has been included as Attachment F. The required variance findings are evaluated below in succession:

1. *That a hardship peculiar to the property and not created by any act of the owner exists. In this context, personal, family or financial difficulties, loss of prospective profits and neighboring violations are not hardships justifying a variance. Further, a previous variance can never have set a precedent, for each case must be considered only on its individual merits;*

The applicant states that multiple hardships are presented by the existing floor plan and the orientation of the existing residence toward the corner side lot line. Staff believes that the site layout and existing floor plan are not a hardship, since feasible alternative options exist which would still meet the desired goal of modernizing and expanding the residence. The proposed second story addition could be achieved without the entry roof modifications and additional posts. The existing covered entry could be updated and modernized to match the proposed second story design through the use of updated and consistent material choices and/or other architectural details which would not require a variance. Furthermore, the proposed alterations to the entry appear to be primarily motivated by aesthetics, which is not considered in the variance findings.

2. *That such variance is necessary for the preservation and enjoyment of substantial property rights possessed by other conforming property in the same vicinity and that a variance, if granted, would not constitute a special privilege of the recipient not enjoyed by his/her neighbors;*

The applicant states that the requested variance is necessary to achieve a consistent architectural style between the second story addition and entry. Staff believes that there are reasonable alternatives, as described above, for the enjoyment of property rights relative to other properties in the vicinity. The existing home provides a similar entry area as the proposed entry design, which protects the front door from the elements and weather. The proposed variance is not necessary to update the design of the entry, enjoy the same privileges as neighboring properties, or effectively use the entry area. Additionally, permitting the entry to encroach further into the street side setback on a lot with no physical constraint could constitute a special privilege.

3. *That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property; and*

Although the increased entry encroachment would affect the street side setback, staff believes that the limited size of the encroachment would not be particularly detrimental to the public health, safety, and welfare, or impair an adequate supply of light and air to the adjacent properties.

4. *That the conditions upon which the requested variance is based would not be applicable, generally, to other property within the same zoning classification.*

The applicant cites the uniqueness of their floor plan, the orientation of the existing residence and the

existing encroachment into a required yard as examples of the uniqueness of this situation. Staff believes that the particular site layout and existing floor plan, while presenting some constraints to development, is not particularly unique in this area. The location of the front door on the street side lot line is not unusual on corner lots. Setting the shorter of the two street sides as the front lot line only serves to benefit property owners by maximizing the allowable building envelope. The existing encroachment into the required street side yard cannot itself serve as the basis for new encroachments. Staff believes that the justifications for this particular variance request would be broadly applicable to other corner lots in this area and throughout the City.

5. *That the condition upon which the requested variance is based is an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process.*

The property is not within any Specific Plan area. Hence, a finding regarding an unusual factor does not apply.

Approval of a variance requires that all five findings be made; since staff believes several are not addressed, denial of the variance request is recommended. Findings to this effect are included in the recommended actions. Condition 6a allows the project to be revised and continue with administrative approval with the modification of the entry to conform to all requirements for new construction. For the Planning Commission's reference, staff provided the applicant with feedback during the initial review process that the required variance findings did not appear to be applicable to this primarily-aesthetic entry proposal, and encouraged modest revisions that would keep all new construction conforming. However, the applicant elected to pursue this request, as is their option. The Commission does have the discretion to approve the variance if all of the findings to that effect can be specified.

Trees and landscaping

At present, there are 12 trees on or in near proximity to the project site. Four of these trees are heritage trees, three of which are located in the right-of-way. All 12 trees are proposed to remain. The partial demolition of the existing residence and construction of the proposed addition are not anticipated to adversely affect any of the existing trees located on the subject site or neighboring properties, given that the proposed addition is within the footprint of the existing structure. Standard heritage tree protection measures will be ensured through recommended condition 5g. No new landscaping is currently proposed.

Valuation

To calculate the replacement and new construction costs on which the use permit threshold is based, the City uses standards established by the Building Division. The City has determined that the replacement cost of the existing structure would be \$373,260 meaning that the applicants would be allowed to propose new construction and remodeling at this site totaling less than \$186,630 in any 12-month period without applying for a use permit. The City has determined that the value of the proposed work would be approximately \$386,150. Based on this estimate, the proposed project exceeds 50 percent of the replacement cost of the existing structure, therefore requiring use permit approval by the Planning Commission.

Correspondence

The applicant indicates that the property owners performed outreach by contacting adjacent property owners regarding the proposed project. Four signed letters were submitted with the application, all of which express support for the proposed project (Attachment G). The property owners also coordinated directly with the neighboring property to the south to ensure that any privacy concerns were addressed. As a result, the property owners agreed to install opaque glass windows that open from the top down for the windows which overlook the neighboring rear yard. A project-specific condition (6b) has been added to this effect, requiring the building permit submittal to reflect this agreement, subject to staff review and approval.

Conclusion

Staff believes that the scale, materials, and style of the proposed residence are compatible with those of the greater neighborhood. No heritage tree impacts are anticipated. Aside from the variance request, the floor area, building coverage, and height of the proposed residence would all be at or below the maximum amounts permitted by the Zoning Ordinance, and the new structure would be within the daylight plane requirements. Staff believes that there are feasible alternatives for the existing entry, which can be addressed with the building permit. Given the lack of a unique circumstance peculiar to the property and the existence of feasible design alternatives that would not require a variance, staff recommends that the Planning Commission approve the proposed use permit and deny the proposed variance for the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Recommended Actions
- B. Location Map

- C. Data Table
- D. Project Plans
- E. Project Description Letter
- F. Variance Letter
- G. Correspondence

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:

Kaitie Meador, Associate Planner

Report reviewed by:

Thomas Rogers, Principal Planner

208 Oakhurst Place – Attachment A: Recommended Actions

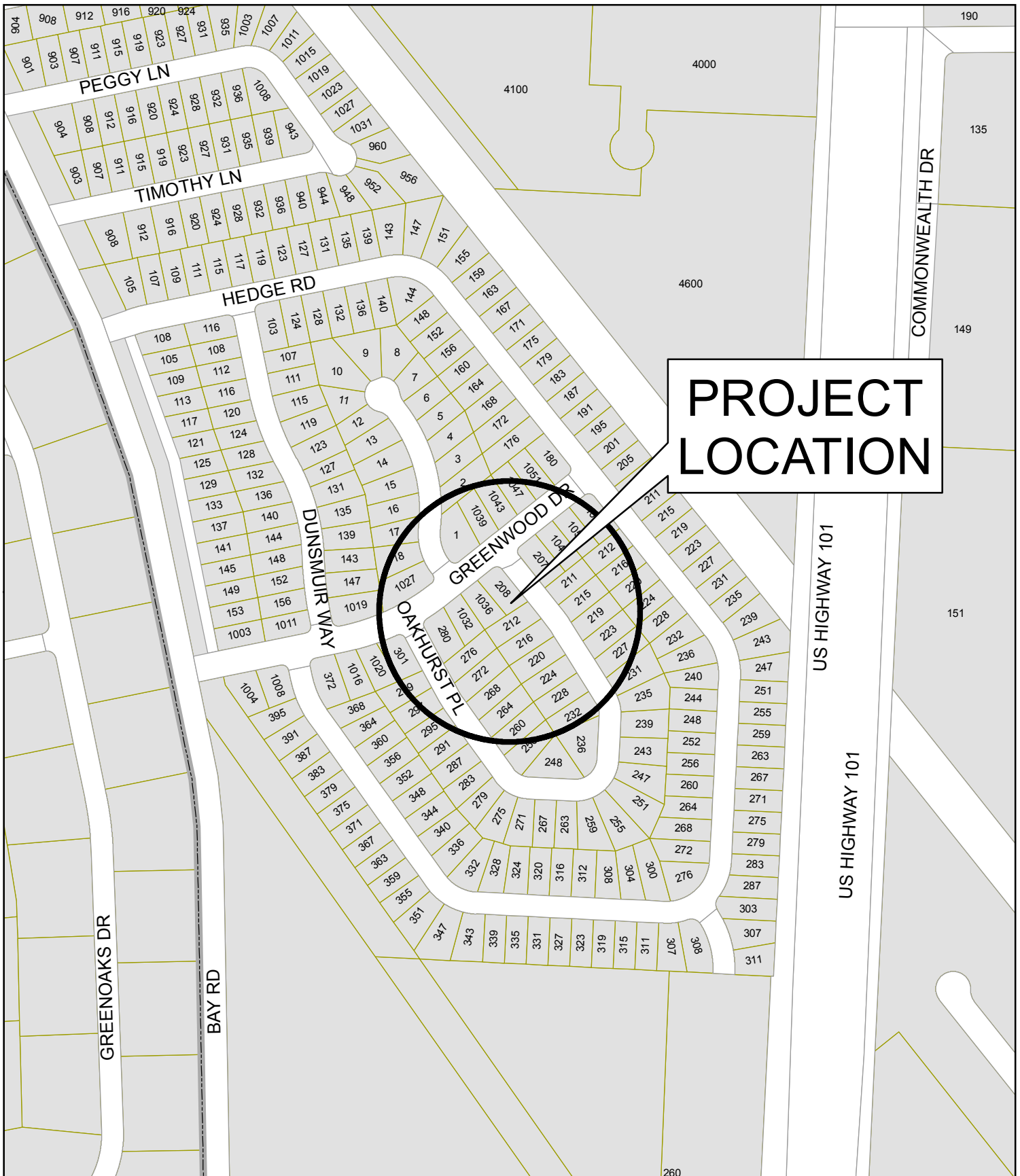
LOCATION: 208 Oakhurst Place	PROJECT NUMBER: PLN2016-00056	APPLICANT: Marshall Schneider	OWNER: John & Julia Molise
REQUEST: Request for a use permit to remodel and add a second story to an existing nonconforming single story, single-family residence in the R-1-U (Single Family Urban) zoning district at 208 Oakhurst Place. The proposed work would exceed 50 percent of the existing replacement value in a 12-month period. The proposal includes a request for a variance to allow a new covered entry with a street side setback of approximately 9 feet 3 inches, where 12 feet is required.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: <ol style="list-style-type: none">1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, “Existing Facilities”) of the current California Environmental Quality Act Guidelines.2. Make the following findings as per Section 16.82.340 of the Zoning Ordinance pertaining to the granting of variance:<ol style="list-style-type: none">a. The applicant states that multiple hardships are presented by the existing floor plan and the orientation of the existing residence toward the corner side lot line. Staff believes that the site layout and existing floor plan are not a hardship, since feasible alternative options exist which would still meet the desired goal of modernizing and expanding the residence. The proposed second story addition could be achieved without the entry roof modifications and additional columns. The existing covered entry could be updated and modernized to match the proposed second story design through the use of updated and consistent material choices and/or other architectural details which would not require a variance. Furthermore, the proposed alterations to the entry appear to be primarily motivated by aesthetics, which is not considered in the variance findings.b. The applicant states that the requested variance is necessary to achieve a consistent architectural style between the second story addition and entry. Staff believes that there are reasonable alternatives, as described above, for the enjoyment of property rights relative to other properties in the vicinity. The existing home provides a similar entry area as the proposed entry design, which protects the front door from the elements and weather. The proposed variance is not necessary to update the design of the entry, enjoy the same privileges as neighboring properties, or effectively use the entry area. Additionally, permitting the entry to encroach further into the street side setback on a lot with no physical constraint could constitute a special privilege.c. Although the increased entry encroachment would affect the street side setback, staff believes that the limited size of the encroachment would not be particularly detrimental to the public health, safety, and welfare, or impair an adequate supply of light and air to the adjacent properties.d. The applicant cites the uniqueness of their floor plan, the orientation of the existing residence and the existing encroachment into a required yard as examples of the uniqueness of this situation. Staff believes that the particular site layout and existing floor plan, while presenting some constraints to development, is not particularly unique in this area. The location of the front door on the street side lot line is not unusual on corner lots. Setting the shorter of the two street sides as the front lot line only serves to benefit property owners by maximizing the allowable building envelope. The existing encroachment into the required street side yard cannot itself serve as the basis for new encroachments. Staff believes that the justifications for this particular variance request would be broadly applicable to other corner lots in this			

208 Oakhurst Place – Attachment A: Recommended Actions

LOCATION: 208 Oakhurst Place	PROJECT NUMBER: PLN2016-00056	APPLICANT: Marshall Schneider	OWNER: John & Julia Molise
REQUEST: Request for a use permit to remodel and add a second story to an existing nonconforming single story, single-family residence in the R-1-U (Single Family Urban) zoning district at 208 Oakhurst Place. The proposed work would exceed 50 percent of the existing replacement value in a 12-month period. The proposal includes a request for a variance to allow a new covered entry with a street side setback of approximately 9 feet 3 inches, where 12 feet is required.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: area and throughout the City. e. The property is not within any Specific Plan area. Hence, a finding regarding an unusual factor does not apply. 3. Deny the variance. 4. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City. 5. Approve the use permit subject to the following standard conditions: a. Development of the project shall be substantially in conformance with the plans prepared by Schneider Design Associates, consisting of 14 plan sheets, dated received July 6, 2016, and approved by the Planning Commission on July 25, 2016, except as modified by the conditions contained herein, subject to review and approval by the Planning Division. b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project. c. Prior to building permit issuance; the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project. d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes. e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division. f. Simultaneous with the submittal of a complete building permit application, the applicant shall			

208 Oakhurst Place – Attachment A: Recommended Actions

LOCATION: 208 Oakhurst Place	PROJECT NUMBER: PLN2016-00056	APPLICANT: Marshall Schneider	OWNER: John & Julia Molise
REQUEST: Request for a use permit to remodel and add a second story to an existing nonconforming single story, single-family residence in the R-1-U (Single Family Urban) zoning district at 208 Oakhurst Place. The proposed work would exceed 50 percent of the existing replacement value in a 12-month period. The proposal includes a request for a variance to allow a new covered entry with a street side setback of approximately 9 feet 3 inches, where 12 feet is required.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits. g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance. 6. Approve the use permit subject to the following <i>project-specific</i> condition: a. Simultaneous with the submittal of a complete building permit application, the applicant shall submit revised plans with the removal of the new entry posts and other elements that do not conform to current requirements, subject to Planning Division review and approval. The revised entry shall be aesthetically compatible with the overall proposal. b. Simultaneous with the submittal of a complete building permit application, the applicant shall submit revised plans indicating the proposed modifications to the second story windows on the South elevation which include opaque glass and top-down openings. The revisions shall be subject to Planning Division review and approval.			

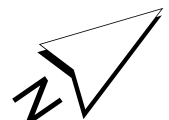


CITY OF MENLO PARK

LOCATION MAP

208 OAKHURST PLACE

DRAWN: TAS CHECKED: KMM DATE: 07/25/16 SCALE: 1" = 300' SHEET: 1



208 Oakhurst Place – Attachment C: Data Table

	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	6,297 sf	6,297 sf	7,000 sf min.
Lot width	58.3 ft.	58.3 ft.	65 ft. min.
Lot depth	104.9 ft.	104.9 ft.	100 ft. min.
Setbacks			
Front	25.1 ft.	25.1 ft.	20 ft. min.
Rear	6.7 ft.	6.7 ft.	20 ft. min.
Side (left)	9.2 ft.	10 ft.	12 ft. min.
Side (right)	5.8 ft.	5.8 ft.	5.8 ft. min.
Building coverage	2,068 sf	2,277 sf	2,204 sf max.
	33 %	36 %	35 % max.
FAL (Floor Area Limit)	2,764 sf	2,021 sf	2,800 sf max.
Square footage by floor	1,801 sf/1 st 725 sf/2 nd 238 sf/garage 29 sf/entry	1,783 sf/1st 238 sf/garage 22.7 sf/entry 256 sf/trellis	
Square footage of buildings	2,793 sf	2,299.7 sf	
Building height	21.3 ft.	14.6 ft.	28 ft. max.
Parking	1 covered	1 covered	1 covered/1 uncovered
Note: Areas shown highlighted indicate a nonconforming or substandard situation.			

Trees						
	Heritage trees	4*	Non-Heritage trees	8	New Trees	0
	Heritage trees proposed for removal	0	Non-Heritage trees proposed for removal	0	Total Number of Trees	12

*Three of which are located in the right-of-way.

SDA

SCHNEIDER
DESIGN
ASSOCIATES

514 23rd AVENUE
SAN FRANCISCO
CALIFORNIA, 94121

APN: 055-295-010
MOLISE RESIDENCE
208 OAKHURST PLACE
MENLO PARK, CA



ISSUED:

PLANNING SUBMITTAL	04.25.16
PLANNING SUBMITTAL	06.02.16
PLANNING SUBMITTAL	07.06.16

COVER

SCALE: N/A
JOB NUMBER: 201518

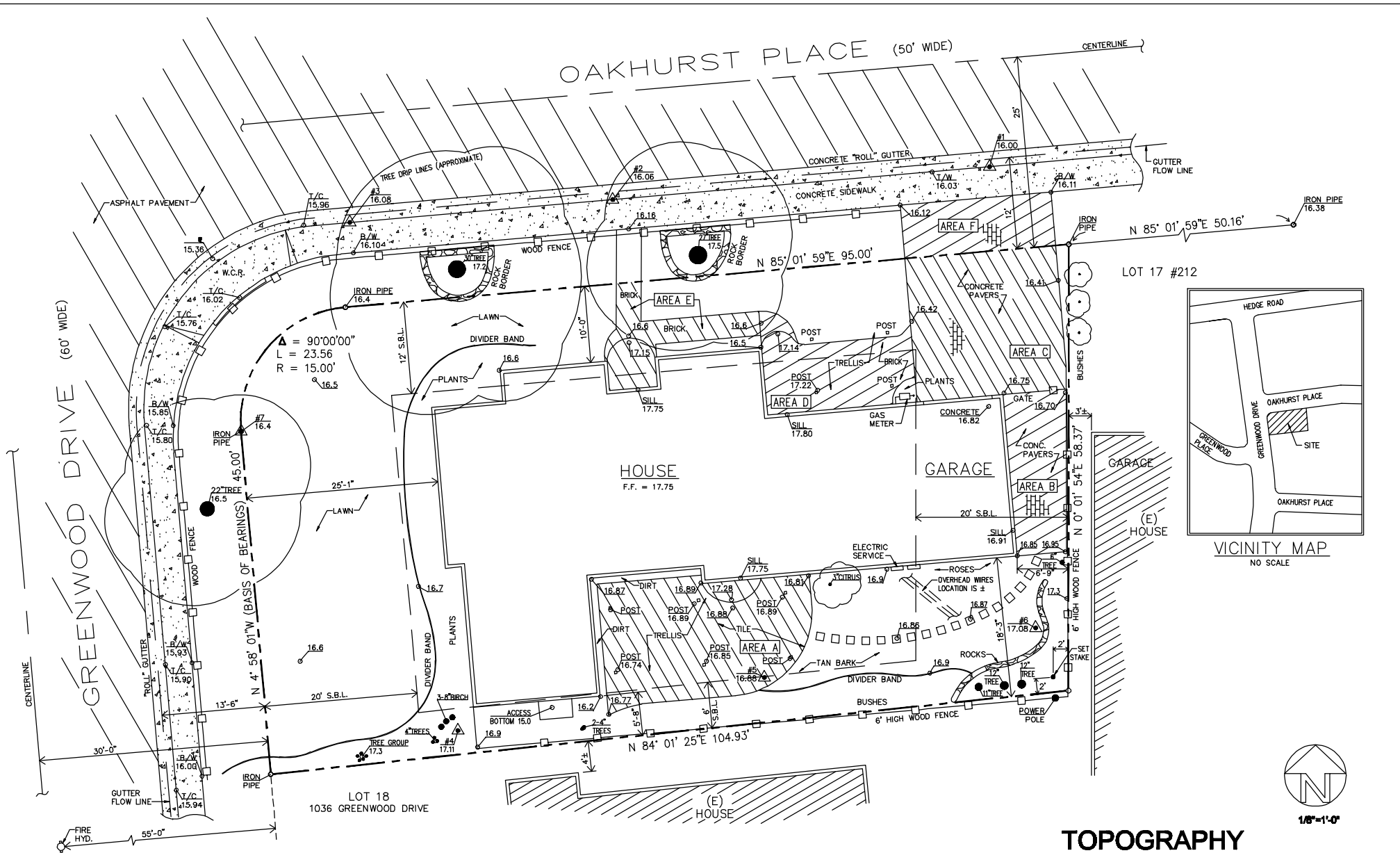
A0.1



MOLISE RESIDENCE

208 OAKHURST PLACE

D2



NOTE:
NOT ALL UNDERGROUND UTILITY LINES MAY BE SHOWN ON THIS MAP. U.S.A. MUST BE CONTACTED TO DETERMINE LOCATIONS OF ALL UTILITY LINES PRIOR TO ANY DIGGING.

NOTE:
EASEMENT, PLAN LINES, SETBACK LINES AND GEOLOGIC CONDITIONS, IF ANY, AND UNLESS SHOWN, NOT SHOWN.

NOTE:
HOUSE TO LOT LINE DIMENSIONS ARE FROM SIDING (WOOD OR STUCCO). FOUNDATION CORNERS ARE 2" ±. IRON PIPES NOT VISIBLE IN ALL CASES.

ELEVATION DATUM:
INTERPOLATION OF USGS MAP WAS USED FOR ELEVATIONS SHOWN ON THIS MAP. CONTROL POINT #1, TOP OF CURB, WAS TAKEN AS 16.00 FOOT ELEVATION. NOT EXACT SEA LEVEL DATUM.

SITE DATA:
- LOT 19, BLOCK 5, TRACT 560, VOL. 25, PG. 69, SAN MATEO CO.
- ASSESSORS PARCEL NO. - 55-295-01
- LOT AREA, 6297 SQ.FT. (0.145 ACRE MORE OR LESS) GROSS AND NET.
- NO CURB CUTS, "ROLL" CURB.

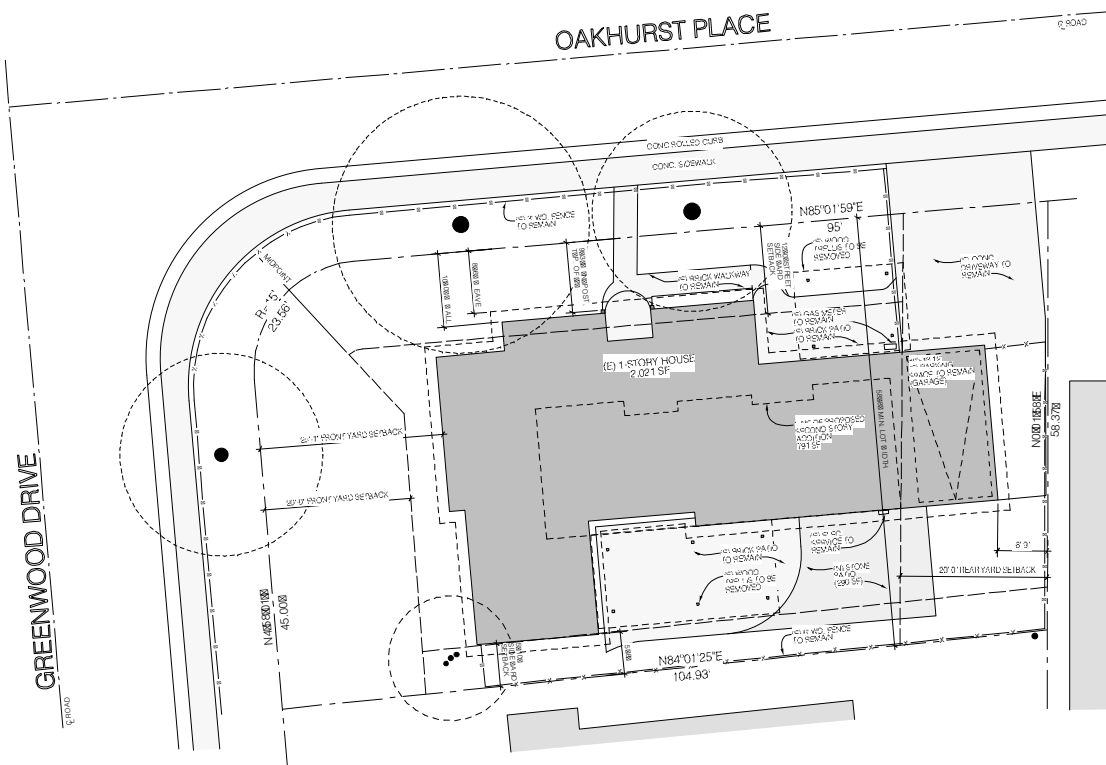
LEGEND
T/C = TOP OF CURB
B/W = BACK OF WALK
F.F. = FINISHED FLOOR ELEVATION
S.B.L. = SET BACK LINE

NOTE:
I HEREBY STATE THAT THIS PARCEL'S BOUNDARY WAS ESTABLISHED BY ME OR UNDER SUPERVISION AND IS BASED ON A FIELD SURVEY IN CONFORMANCE WITH THE LAND SURVEYOR'S ACT. ALL MONUMENTS ARE OF THE CHARACTER AND OCCUPY THE POSITIONS INDICATED AND ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.

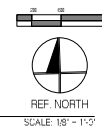
IMPERVIOUS SURFACE AREAS
A = 403 SQ.FT.
B = 163 SQ.FT.
C = 376 SQ.FT.
D = 187 SQ.FT.
E = 107 SQ.FT.

TOTAL 1236 SQ.FT.
F = 140 SQ.FT. APRON
(E) BUILDING = 2021 SQ.FT.

REVISION 4-13-18 -
ADJUST FRONT HOUSE LINE.



NOTE: SITE INFORMATION BASED
ON SURVEY DONE BY:
DODGE ASSOCIATES SURVEYING,
20652 CHAPARRAL CIRCLE,
PENN VALLEY, CA 95946
530.432.5212



SDA

SCHNEIDER
DESIGN
ASSOCIATES

514 23rd AVENUE
SAN FRANCISCO
CALIFORNIA, 94121

APN: 055-295-010
MOLISE RESIDENCE
208 OAKHURST PLACE
MENLO PARK, CA



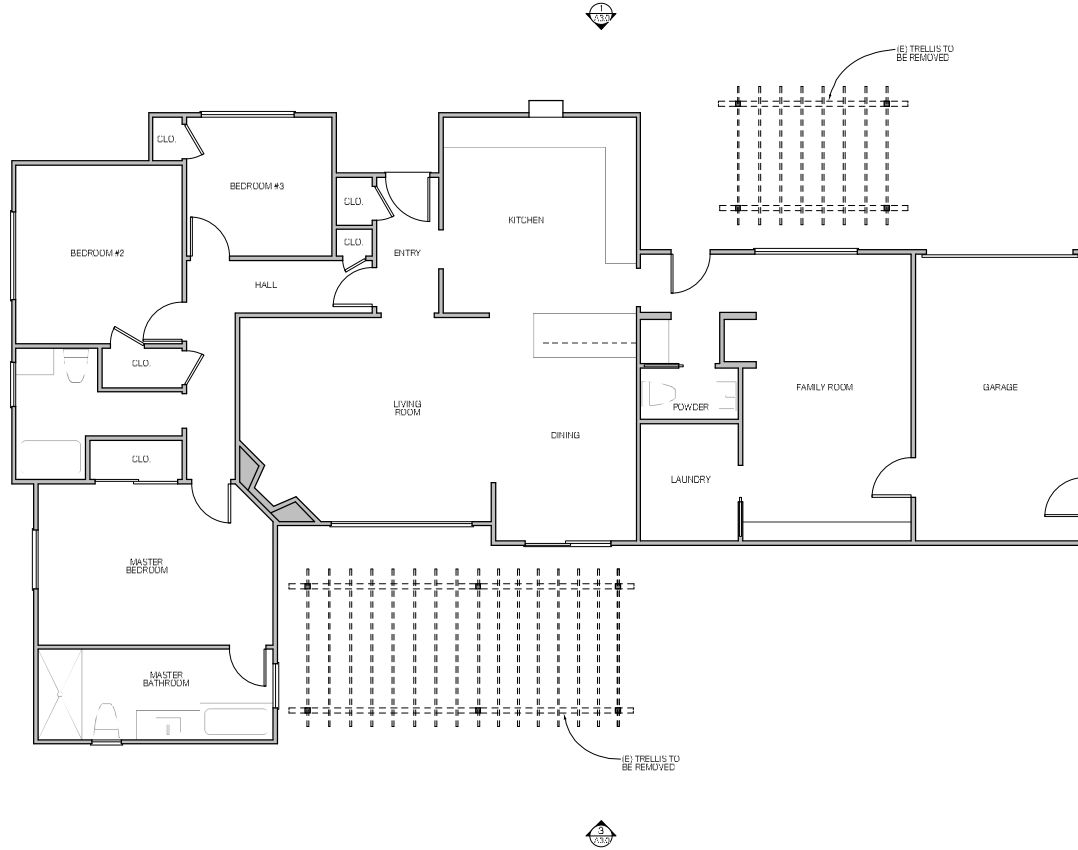
ISSUED:

PLANNING SUBMITTAL	04.25.18
PLANNING SUBMITTAL	05.02.18
PLANNING SUBMITTAL	07.06.18

AS-BUILT FLOOR
PLAN

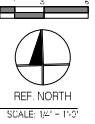
SCALE: 1/4" = 1'-0"
JOB NUMBER: 201518

A2.0



WALL TYPE LEGEND:

(E) 2x4 CONSTRUCTION TO REMAIN



REF: NORTH
SCALE: 1/4" = 1'-0"

1 AS-BUILT FLOOR PLAN



SCHNEIDER
DESIGN
ASSOCIATES

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ISSUED:

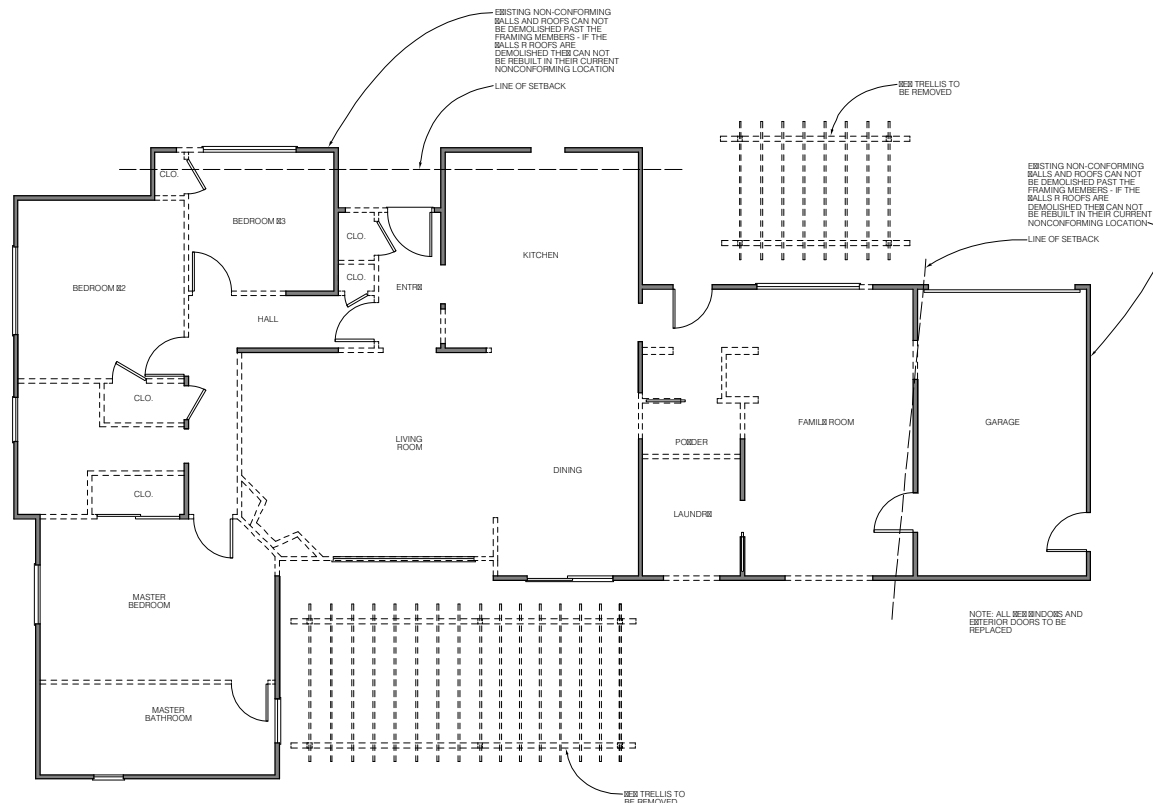
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PLANNING SUBMITTAL	06.02.16
PLANNING SUBMITTAL	07.06.16

DEMOLITION PLAN

SCALE: 1/4" = 1'-0"

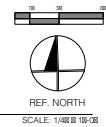
JOB NUMBER: 201518

A2.1



WALL TYPE LEGEND:

- NEW 284 CONSTRUCTION TO REMAIN
- EXISTING 284 CONSTRUCTION TO BE REMOVED





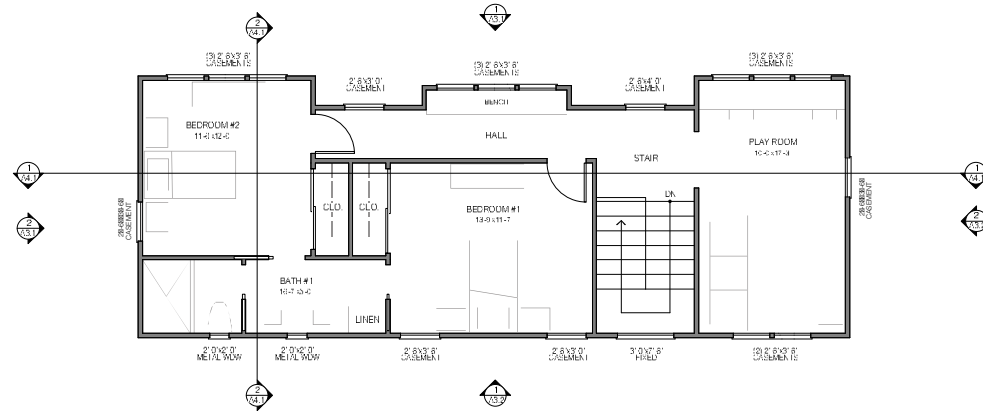
ISSUED:

PLANNING SUBMITTAL	04.25.18
PLANNING SUBMITTAL	05.02.18
PLANNING SUBMITTAL	07.06.18

PROPOSED FLOOR
PLANS

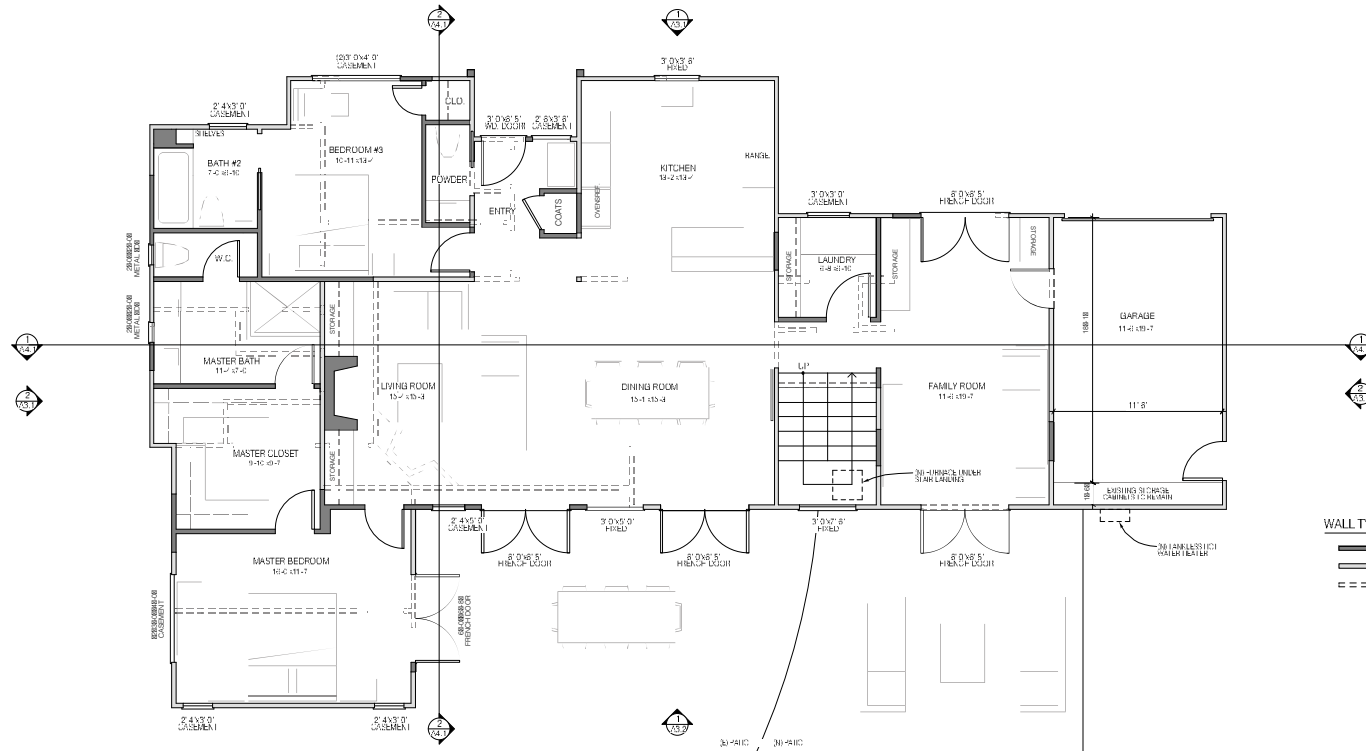
SCALE:	1/4" = 1'-0"
JOB NUMBER:	251518

A2.2



2 PROPOSED SECOND FLOOR PLAN

SCALE: 1/4" = 1'-0"



WALL TYPE LEGEND:

- (N) 2x CONSTRUCTION
- (E) 2x CONSTRUCTION TO REMAIN
- (R) 2x CONSTRUCTION TO BE REMOVED

1 PROPOSED FIRST FLOOR PLAN

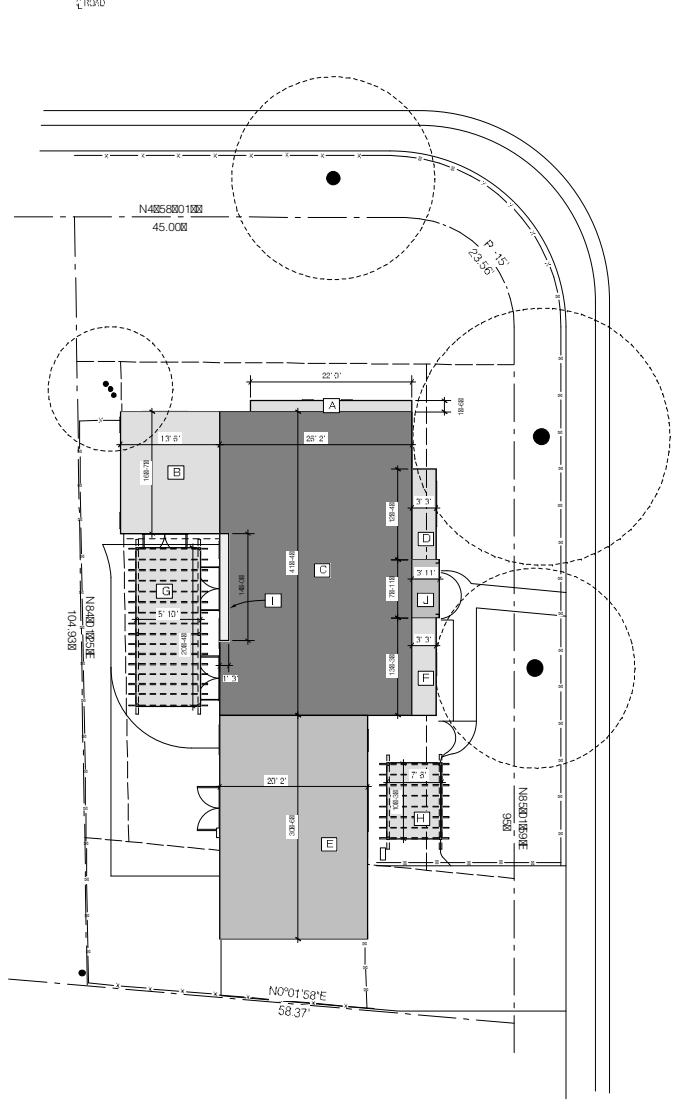


PROPOSED ROOF
PLAN

SCALE : 1/4" = 1'-0"

JOB NUMBER : 201516

GREENWOOD DRIVE



BUILDING COVERAGE CALCULATIONS

LETTER	SIZE	SQ. FT.
A	22' 0" x 1' 8"	33 sq. ft.
B	13' 6" x 15' 7"	211 sq. ft.
C	25' 6" x 41' 4"	1061 sq. ft.
D	3' 5" x 12' 4"	43 sq. ft.
E	22' 0" x 10' 2"	224 sq. ft.
F	3' 5" x 13' 5"	47 sq. ft.
G	8' 10" x 10' 4"	92 sq. ft.
H	2' 5" x 10' 3"	26 sq. ft.
I	1' 3" x 14' 0"	18 sq. ft.
J	3' 11" x 1' 1"	4 sq. ft.
TOTAL BUILDING COVERAGE		2,065 sq. ft.
PERMITTED COVERAGE		2,277 sq. ft.

FLOOR AREA CALCULATIONS

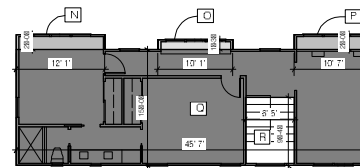
FIRST FLOOR

LETTER	SIZE	SQ. FT.
A	22' 0" x 1' 8"	33 sq. ft.
B	13' 6" x 15' 7"	211 sq. ft.
C	25' 6" x 41' 4"	1061 sq. ft.
D	3' 5" x 12' 4"	43 sq. ft.
E	15' 0" x 20' 2"	303 sq. ft.
F	3' 5" x 13' 5"	47 sq. ft.
G	11' 9" x 20' 2"	238 sq. ft.
H	14' 0" x 1' 3"	18 sq. ft.
TOTAL FIRST FLOOR		2,065 sq. ft.

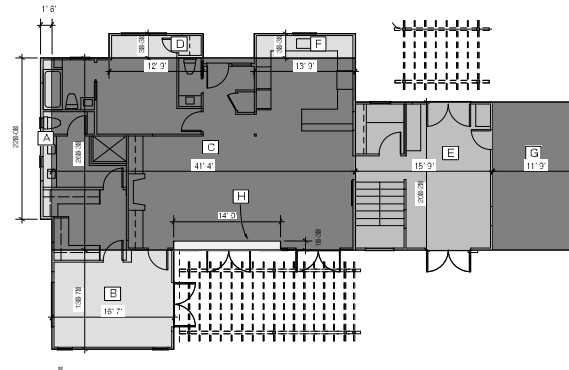
SECOND FLOOR

LETTER	SIZE	SQ. FT.
A	45' 7" x 15' 0"	725 sq. ft.
B	12' 1" x 2' 0"	24 sq. ft.
C	10' 1" x 1' 3"	13 sq. ft.
D	10' 7" x 2' 0"	21 sq. ft.
E	8' 5" x 9' 4"	82 sq. ft.
TOTAL SECOND FLOOR		785 sq. ft.

TOTAL 2,764 SF



SECOND FLOOR AREA DIAGRAM



FIRST FLOOR AREA DIAGRAM

2 BUILDING COVERAGE DIAGRAM

SCALE: 1/8" = 1'-0"

1 FLOOR AREA DIAGRAMS

SCALE: 1/8" = 1'-0"



SCHNEIDER
DESIGN
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MENLO PARK, CA



ISSUED:
PLANNING
SUBMITTAL 04.25.18
PLANNING
SUBMITTAL 05.02.18
PLANNING
SUBMITTAL 07.06.18

FLOOR AREA AND
BUILDING COVERAGE
DIAGRAMS
SCALE: 1/8" = 1'-0"
JOB NUMBER: 251518

A2.4



ISSUED:

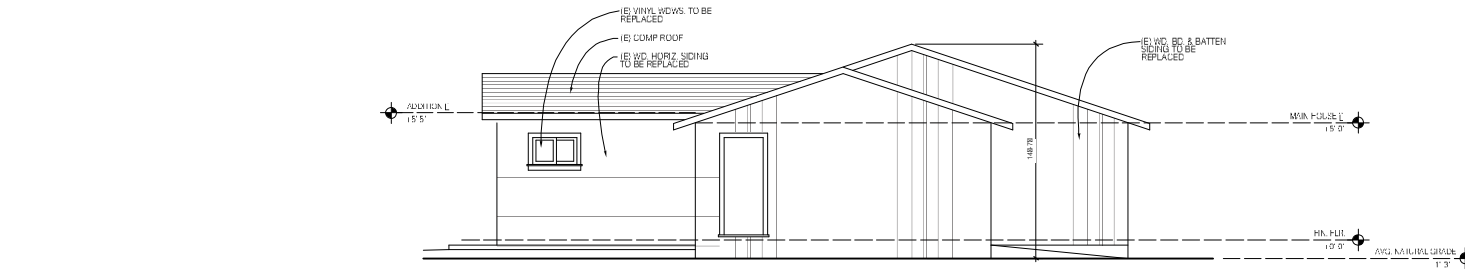
PLANNING SUBMITTAL	04.25.18
PLANNING SUBMITTAL	05.02.18
PLANNING SUBMITTAL	07.06.18

AS-BUILT
ELEVATIONS

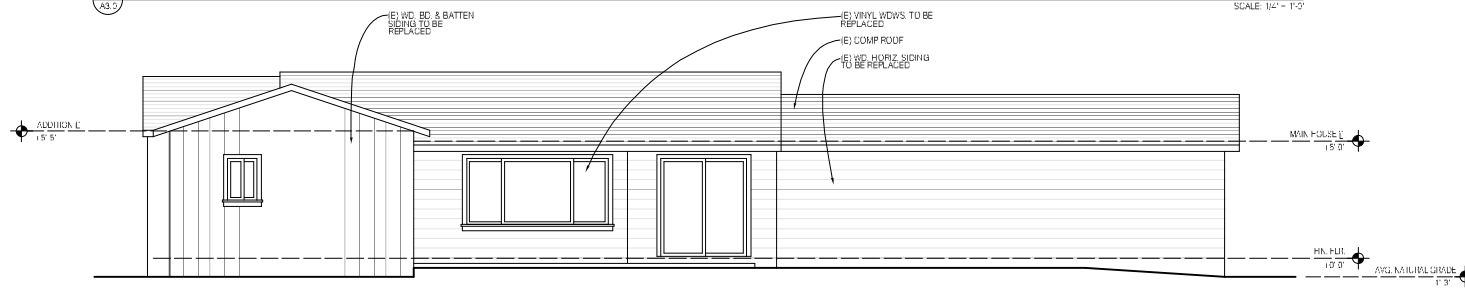
SCALE: 1/4" = 1'-0"
JOB NUMBER: 201518

A3.0

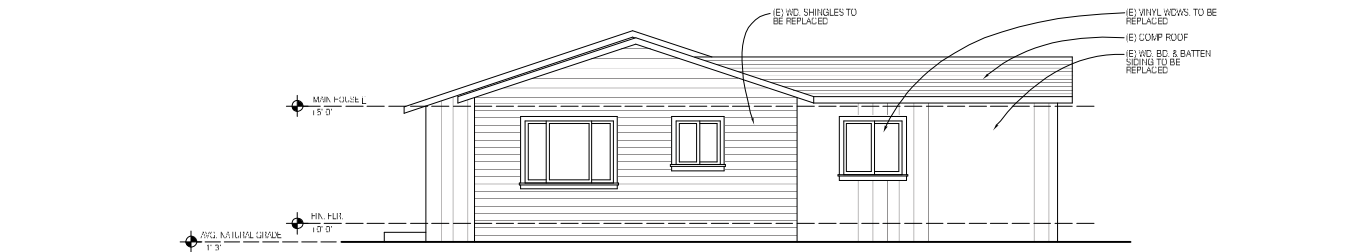
4 AS-BUILT REAR (EAST) ELEVATION



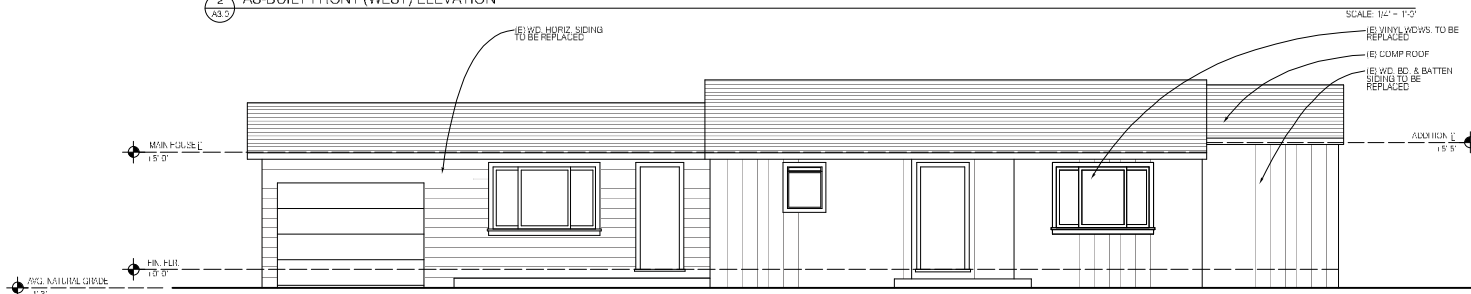
3 AS-BUILT SIDE (SOUTH) ELEVATION



2 AS-BUILT FRONT (WEST) ELEVATION



1 AS-BUILT SIDE (NORTH) ELEVATION



SDA

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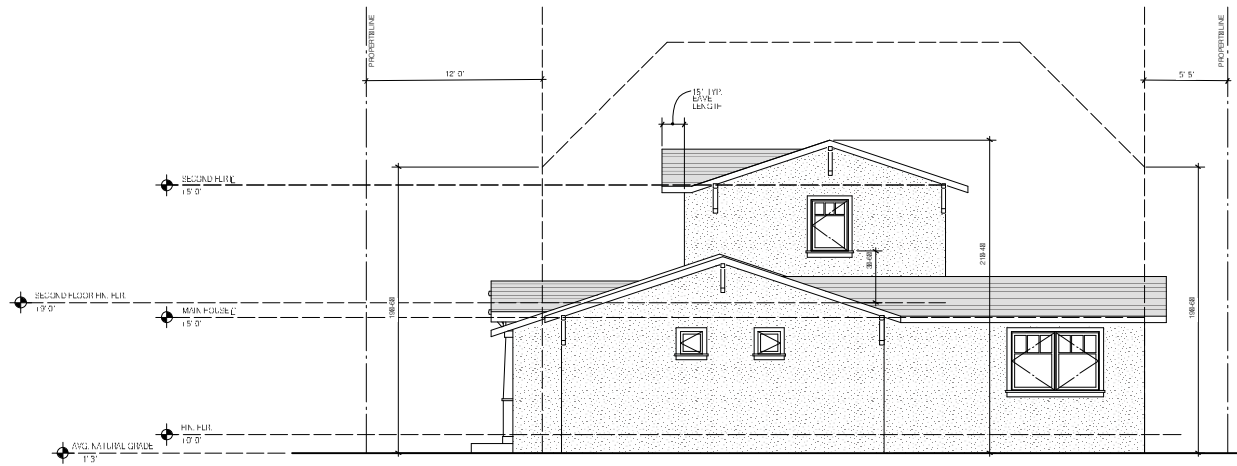
ISSUED:

PLANNING SUBMITTAL	04.25.18
PLANNING SUBMITTAL	05.02.18
PLANNING SUBMITTAL	07.05.18

PROPOSED ELEVATIONS

SCALE: 1/4" = 1'-0"
JOB NUMBER: 201518

A3.1



2 PROPOSED FRONT (WEST) ELEVATION

SCALE: 1/4" = 1'-0"



1 PROPOSED SIDE (NORTH) ELEVATION

SCALE: 1/4" = 1'-0"

SDA

SCHNEIDER
DESIGN
ASSOCIATES

514 23RD AVENUE
SAN FRANCISCO
CALIFORNIA, 94121

APN: 055-295-010
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MENLO PARK, CA



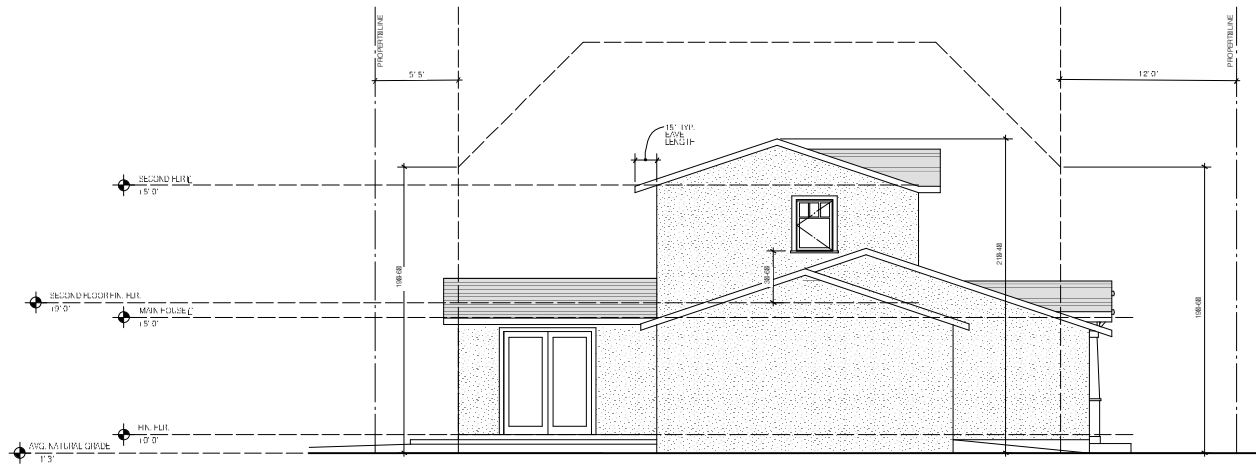
ISSUED:

PLANNING SUBMITTAL	04.25.18
PLANNING SUBMITTAL	05.02.18
PLANNING SUBMITTAL	07.06.18

PROPOSED
ELEVATIONS

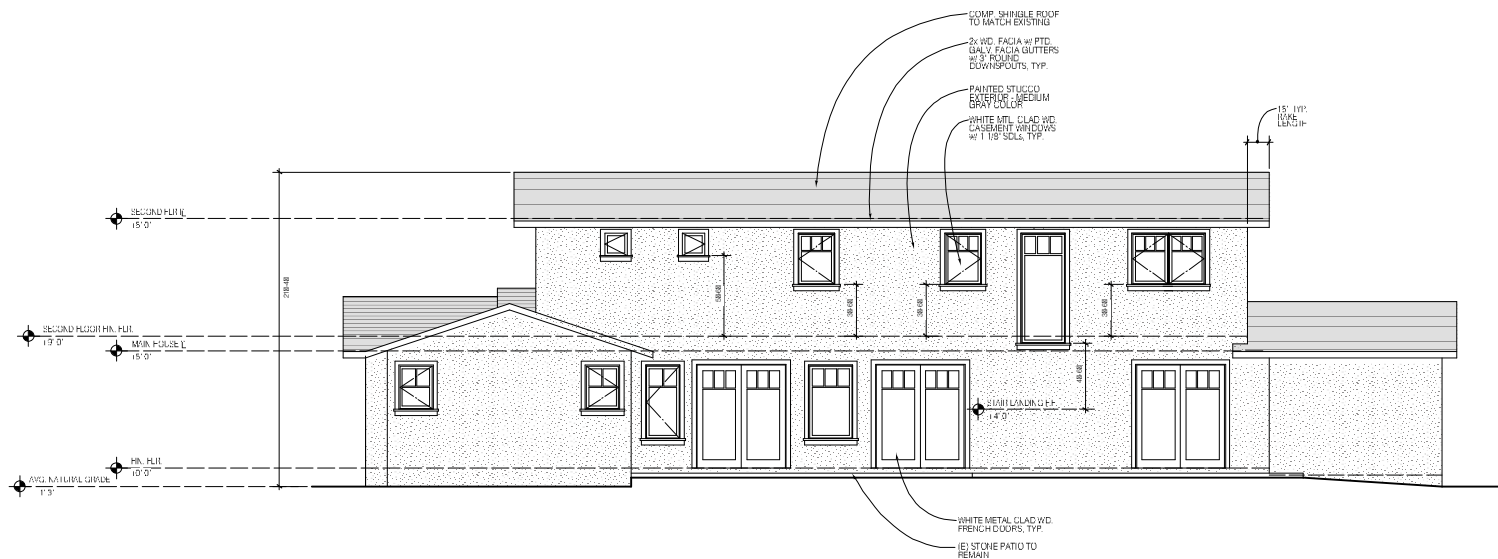
SCALE: 1/4" = 1'-0"
JOB NUMBER: 201518

A3.2



2 PROPOSED REAR (EAST) ELEVATION

SCALE: 1/4" = 1'-0"



1 PROPOSED SIDE (SOUTH) ELEVATION

SCALE: 1/4" = 1'-0"



SCHNEIDER
DESIGN
ASSOCIATES
514 23rd AVENUE
SAN FRANCISCO
CALIFORNIA, 94121

APN: 055-295-010
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MENLO PARK, CA



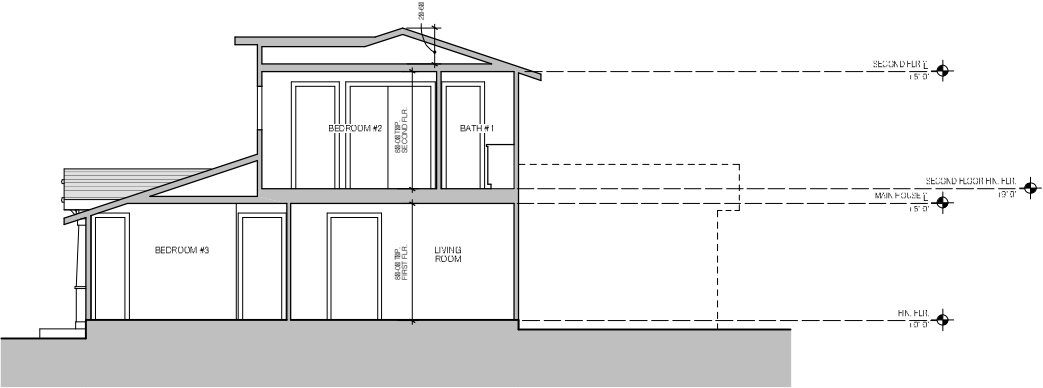
ISSUED:

PLANNING SUBMITTAL	04.25.18
PLANNING SUBMITTAL	05.02.18
PLANNING SUBMITTAL	07.06.18

PROPOSED SECTIONS

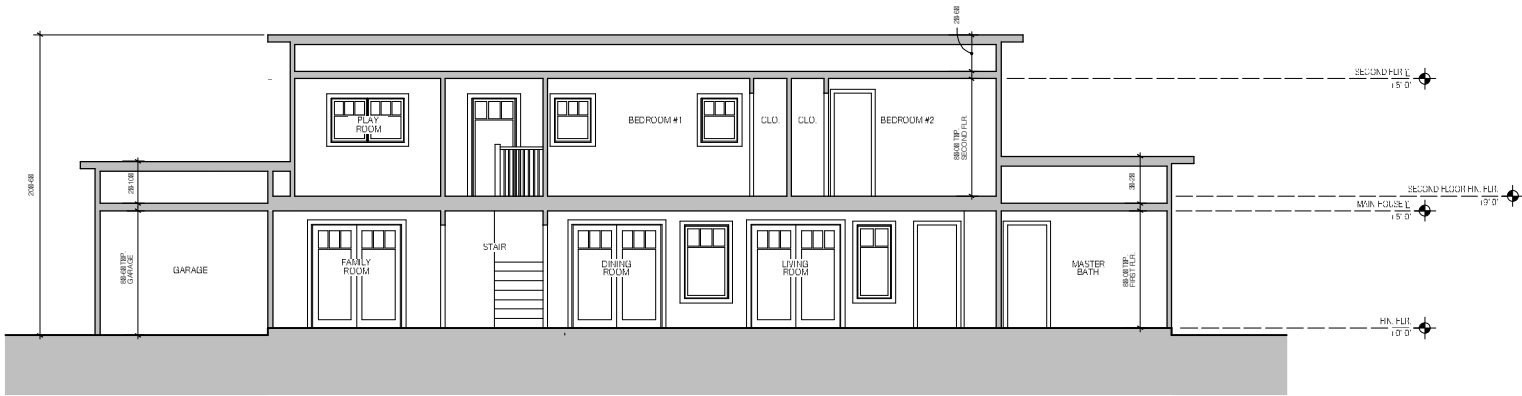
SCALE:	1/4" = 1'-0"
JOB NUMBER:	251518

A4.1



2 PROPOSED SECTION

SCALE: 1/4" = 1'-0"



1 PROPOSED SECTION

SCALE: 1/4" = 1'-0"

PROJECT DESCRIPTION

208 Oakhurst Place

The Molise family has lived at 208 Oakhurst for eight years and loves the neighborhood. As their family has grown with two children over the last several years they have decided not to move but to stay in their neighborhood as they have grown to love it and are very involved with many aspects of it.

The existing one-story home sits on a corner lot and has its front entrance situated on the side that faces Oakhurst Place. The front of the home is in the current side setback on the North side and the garage is in the current rear setback on the East side. This makes the home a legal non-conforming structure. The scope of work involves adding a second floor over the existing home without placing it in the setbacks so as to make it a conforming addition. This second floor addition contains two bedrooms, a shared Jack and Jill bathroom, and a playroom. This space has been designed to allow the two children enough space now and through their childhood as John and Julia plan to stay in the home through their children's departure to college and beyond.

The addition has been carefully designed to keep the bulk and mass of the second floor from dominating the aesthetic of the home. By breaking the roof line of the addition up with well-proportioned gable ends and groupings of windows the new second floor is in proportion to the existing single story home and other homes in the neighborhood.

The proposed style of the home is Craftsman to blend with the neighborhood and have an understated elegance. The project is not designed to draw attention to itself but to be in scale with the neighborhood. The exterior of the home will be stucco and painted a grey color to recede into the natural surroundings. All of the windows in the project will be aluminum clad wood and will have simulated divided lights with grids inside and outside the glass and a spacer bar between the two panes of glass. The mullions on these windows are indistinguishable from true divided lite windows.

Because the existing entry to the home is non-descript and quite dark both inside and out, the current design proposes to change the roof line slightly over the front door to bring in more light and to make the location of the entry an expected focal point of the North elevation. Because the front entrance is inside

of the current side yard setback this requires a variance. In an effort to balance the aesthetics of the home with the fact that the front entry is in the setback, the current proposal slightly changes the roofline inside of the setback without encroaching into the setback further. In our opinion, leaving the Entry as-is has a detrimental impact on the quality of the design and actually decreases the aesthetic appeal of the home from the street. We feel that the minor change to the existing roofline inside of the setback allows for the aesthetics of the home to be coherent and harmonious without increasing the encroachment into the setback. This change makes the home more appealing from the street and acts to enrich the aesthetics of the neighborhood.

All of the existing heritage trees on the site and in the right of way around the home are cherished by the Molises and will be protected and retained. The proposed construction will not impact their health in any way. The birch tree that sits close to the SouthProject Descriptionwest corner of the home will be well protected during construction and no new foundations or framing are proposed near this tree.

John and Julia Molise have reached out to all of the immediate neighbors and have been met with enthusiasm for the project as designed. All of the neighbors like the aesthetics, massing, and style of the new design. Three of the four immediate neighbors have signed letters stating their support for the project as designed.

VARIANCE FINDINGS208 Oakhurst Place

1. That a hardship peculiar to the property and not created by any act of the owner exists. In this context, personal, family or financial difficulties, loss of prospective profits and neighboring violations are not hardships justifying a variance. Further, a previous variance can never have set a precedent, for each case must be considered only on its individual merits;

- The existing house was constructed such that the front of it is in the current setback and is a legal, non-conforming structure. The front of the home faces the side (corner lot) and the Entry to the home is in the current side setback. The entire layout of the interior of the home would need to be changed to move the entry of the home outside of the setback. At the same time the entry to the home is very dated and non-descript and needs updating to bring it up to the current standards of the neighborhood.

Because the home has its entry inside the setback a variance is required to bring the entry of the home into the style of the home being proposed and is consistent with current neighborhood trends regarding remodeling. The minimal remodel of the entry, one that adds no new interior space and does not increase the encroachment, brings the home into character with the neighborhood.

Moving the entry would constitute a hardship as a fair portion of the front of the home is legally in the side setback and the lot is quite narrow. The fact that the lot is narrow would make it difficult to change the entry location and continue to have a floor plan that works efficiently as a usable home. Because the lot was created before the lot width requirement was instituted (legal non-conforming) the variance allows the lot to be utilized with a workable interior floor plan even though the lot is narrow.

2. That such variance is necessary for the preservation and enjoyment of substantial property rights possessed by other conforming property in the same vicinity and that a variance, if granted, would not constitute a special privilege of the recipient not enjoyed by his/her neighbors;

- Most homes in the neighborhood that are recently remodeled have focused considerable design upgrade on the entry. These conforming properties have the ability to remodel the entry to be in the same architectural style as is adopted by the rest of the home. By granting the variance as requested 208 Oakhurst Place will enjoy the same privilege as its neighbors. And, because the work in the setback is kept to a minimum which allows the home to have a consistent architectural style (adding no further encroachment into the setback), the proposal minimizes the impact on the setback.

3. That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property;

- The proposed entry element will add considerably to the character of the neighborhood and will actually be beneficial to the neighborhood as it beautifies a prominent corner lot home. The proposed encroachment into the setback is no deeper than the existing encroachment and is not near a neighboring structure. Because of this it does not materially effect the public health, safety, or welfare or impair adequate supply of light and ventilation to adjacent properties because of this.

4. That the conditions upon which the requested variance is based would not be applicable, generally, to other property within the same zoning classification.

- The conditions are unique in several ways. First, this is a very narrow corner lot that has its front door facing the long street frontage, an uncommon layout. Second, the existing home was built as a legal home and then became non-conforming – this is quite unusual. Finally, the entry is the portion of the home in the setback, another unusual circumstance. The combination of these three unique cases insures the variance request would not be applicable, generally, to other property in the same zoning classification.

5. That the condition upon which the requested variance is based is an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process.

- This property was not discussed in any applicable Specific Plan process as far as we are aware.

lucy baw
&
james kauffman

May 26, 2016

Kaitie Meador, Associate Planner
City of Menlo Park, Planning Division
701 Laurel Street
Menlo Park, CA 94025

Dear Kaitie Meador,

Re: 208 Oakhurst Place

We reviewed John and Julia Molise's plans and have no objections to their addition of a second story to their house.

While a new two story house can create challenges for neighbours, we do not foresee any issues for us. We appreciate their thoughtfulness in making the new changes fit as well as possible into our neighborhood and with their immediate neighbours.

Thank you.


James Kauffman


Lucy Baw

Jessica and Tyson Clark
207 Oakhurst Place
Menlo Park, CA 94025

May 27, 2016

Kaitie Meador
Associate Planner
City of Menlo Park, Planning Division
701 Laurel Street
Menlo Park, CA 94025

Re: 208 Oakhurst Place

Dear Kaitie Meador:

We are John and Julia Molise's neighbors and have reviewed the drawings for the planned second story addition to their house. We have no concerns regarding the exterior changes being proposed.

Please accept our signatures here as our acceptance of these changes.

Thank you,

The image shows two handwritten signatures in black ink. The signature on the left is 'JBC' followed by a long, horizontal, wavy line. The signature on the right is 'Tyson' followed by a stylized 'A' and a short horizontal line.

Jessica Clark and Tyson Clark

Menlo Park Planning
701 Laurel Street
Menlo Park, CA 94025

RECEIVED

APR 28 2016

CITY OF MENLO PARK
BUILDING

April 26, 2016

To Whom It May Concern:

We, as neighbors to John & Julia Molise at 208 Oakhurst Place, have reviewed the drawings for the planned second story addition to the house dated April 25th, 2016 and have no concerns regarding the exterior changes being proposed.

Please accept our signatures here as our acceptance of these changes.

Thank You,



Jennifer & Peter Tanner

211 Oakhurst Place

Menlo Park, CA 94025

Steve Menashe

1036 Greenwood Drive, Menlo Park, CA 94025
Phone: 650 331-0229

July 17, 2016

To whom it may concern:

Regarding the public notice on the pending permit for 208 Oakhurst Place, Menlo Park, CA.

Several weeks ago, John Molise, our next-door neighbor and the owner of the property approached us about the proposed remodel and second floor addition. We discussed the potential impact the second floor addition would have on our privacy, particularly in the rear of our house and backyard area. We understand their need to expand the living space and have no real problems with what they are planning.

This week we received the courtesy notice from the City of Menlo Park with the attached plans. John came over again today to discuss this further and to let us know they are planning to go ahead. The only request we made, today, was to ensure that the last set of windows that will overlook the rear of our house and backyard have opaque glass and be able to open from the top down. The Molise family discussed this and agreed to accommodate this request.

With this accommodation, we fully support the remodel request. Please feel free to contact us with any additional questions.

Sincerely,

A handwritten signature in dark ink, appearing to be 'S. Menashe', with a long horizontal stroke extending to the right.

Steve Menashe
Property owner of 1036 Greenwood Drive



STAFF REPORT

Planning Commission

Meeting Date:

7/25/2016

Staff Report Number:

16-060-PC

Public Hearing:

**Use Permit/ Brendan and Carmen Visser/1177
Middle Avenue**

Recommendation

Staff recommends that the Planning Commission approve a request for a use permit to demolish an existing single-family, single story residence and construct a new two-story, single-family residence on a substandard lot with regard to lot width in the R-1-S (Single-Family Suburban) zoning district, at 1177 Middle Avenue. As part of the project, five heritage trees (two Canary Island date palms, a coast live oak, and two coastal redwoods) are proposed for removal. The recommended actions are contained within Attachment A.

Policy Issues

Each use permit request is considered individually. The Planning Commission should consider whether the required use permit findings can be made for the proposal.

Background

Site location

The subject site is located at 1177 Middle Avenue, located near the intersection of Middle Avenue and Windsor Drive. A location map is included as Attachment B. The site is mainly surrounded by R-1-S zoned properties; however, some nearby properties to the northeast are zoned R-1-U (Single-Family Urban) and R-3 (Apartment). Jack W. Lyle Park is also near the subject property. Surrounding the project site, there is a mix of one and two-story single-family residences, which feature varied architectural styles, including ranch and craftsman.

Analysis

Project description

The applicants are proposing to demolish the existing single-story, single-family, three bedroom residence and attached single car garage and construct a new two-story, four bedroom residence with an attached two-car garage. The driveway for the new garage would be relocated to the front left side of the property. There is an existing accessory building in the rear yard, which would remain. A data table summarizing parcel and project attributes is included as Attachment C. The project plans and the applicants' project description letter are included as Attachments D and E, respectively.

Design and materials

The applicants state that the design would be a traditional architectural style, in keeping with the accessory structure to remain. The house was designed and positioned with the intent to preserve the prominent front yard redwood tree, with a 35-foot setback where only 20 feet is required. Both buildings would feature wood shingle exterior cladding. The new building would also have stucco siding, composition shingle roofing and aluminum exterior, wood interior dual glazed windows with simulated divided lights. The second floor would be inset from the first story on all sides. Sill heights on the side elevations of the second floor would be a minimum of three feet high in order to promote privacy.

The family room at the front of the house would feature a standing seam, metal awning. The covered front porch would be supported by wood posts and beams, and include a paneled wood entryway. Two separate wood carriage doors are proposed for the two-car garage, which would complement the style of the wood entryway. The use of two separate garage doors would help reduce the prominence of the garage as a design feature. The varying materials and planes would help reduce the perception of mass with the new structure. Staff believes that the scale, materials, and design of the proposed residence would be consistent with the neighborhood's mix of architectural styles.

Trees and landscaping

The applicants have submitted an arborist report (Attachment F) detailing the species, size and conditions of the trees on or near the site and have applied for a heritage tree removal permit to remove five heritage trees: two Canary Island date palms, a coast live oak, and two coastal redwoods. There are a total of six non-heritage trees proposed for removal, including two non-heritage size coast live oak trees (trees #27 and 28) that would conflict with the new driveway. The large redwood tree in the front yard (tree #1) would have unique preservation measures, including supplemental irrigation during construction and wood chip mulch layer within its tree protection zone. The arborist also recommends that a root barrier be installed along the edge of the new driveway, in order to prevent this tree from damaging the paving in the future.

All five heritage trees proposed for removal have been evaluated by the City Arborist who has concluded that tree #7, a Canary Island date palm, and trees #20 and #22, both coastal redwoods, are in good health and he has tentatively denied the request of their removal. One Canary Island date palm (tree #14) and a coast live oak (tree #18) have been tentatively approved for removal by the City Arborist. The applicants have stated that they would likely appeal the City Arborist's decision regarding tree #7 to the Environmental Quality Commission, or possibly provide additional information for the reconsideration of the City Arborist. Staff has included a condition of approval (4a) requiring the plans and arborist report to be revised to reflect the retention of these three trees, although these revisions would not be required for any trees that ultimately receive a heritage tree removal permit. In addition, this condition requires that the plans be revised to reflect the final heritage tree replacement planting requirement.

Correspondence

Staff has received one item of correspondence (Attachment G) on the proposed project regarding tree #25, a coast live oak. The arborist report provides protection measures that would minimize the impact of the foundation of the new house on the root system of tree #25, and this information has been shared with the neighbor.

The applicants state in their project description letter that they shared their preliminary plans with the adjacent neighbors. Their neighbors' supportive emails are attached to the project description letter (Attachment E).

Conclusion

Staff believes the scale, materials, and style of the proposed residence are compatible with the neighborhood. The applicants have set the second floor back from the ground floor of the proposed residence and varied the façade materials. Design elements such as the proposed covered porch with wood columns, and stucco and shingle siding would add visual interest to the project, and the two separate garage doors would help reduce the prominence of the garage as a design feature. Tree protection measures would ensure the health of heritage trees. Staff recommends that the Planning Commission approve the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 3 (Section 15303, "New Construction or Conversion of Small Structures") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Recommended Actions
- B. Location Map
- C. Data Table
- D. Project Plans
- E. Project Description Letter
- F. Arborist Report
- G. Correspondence

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:

Michele T. Morris, Assistant Planner

Report reviewed by:

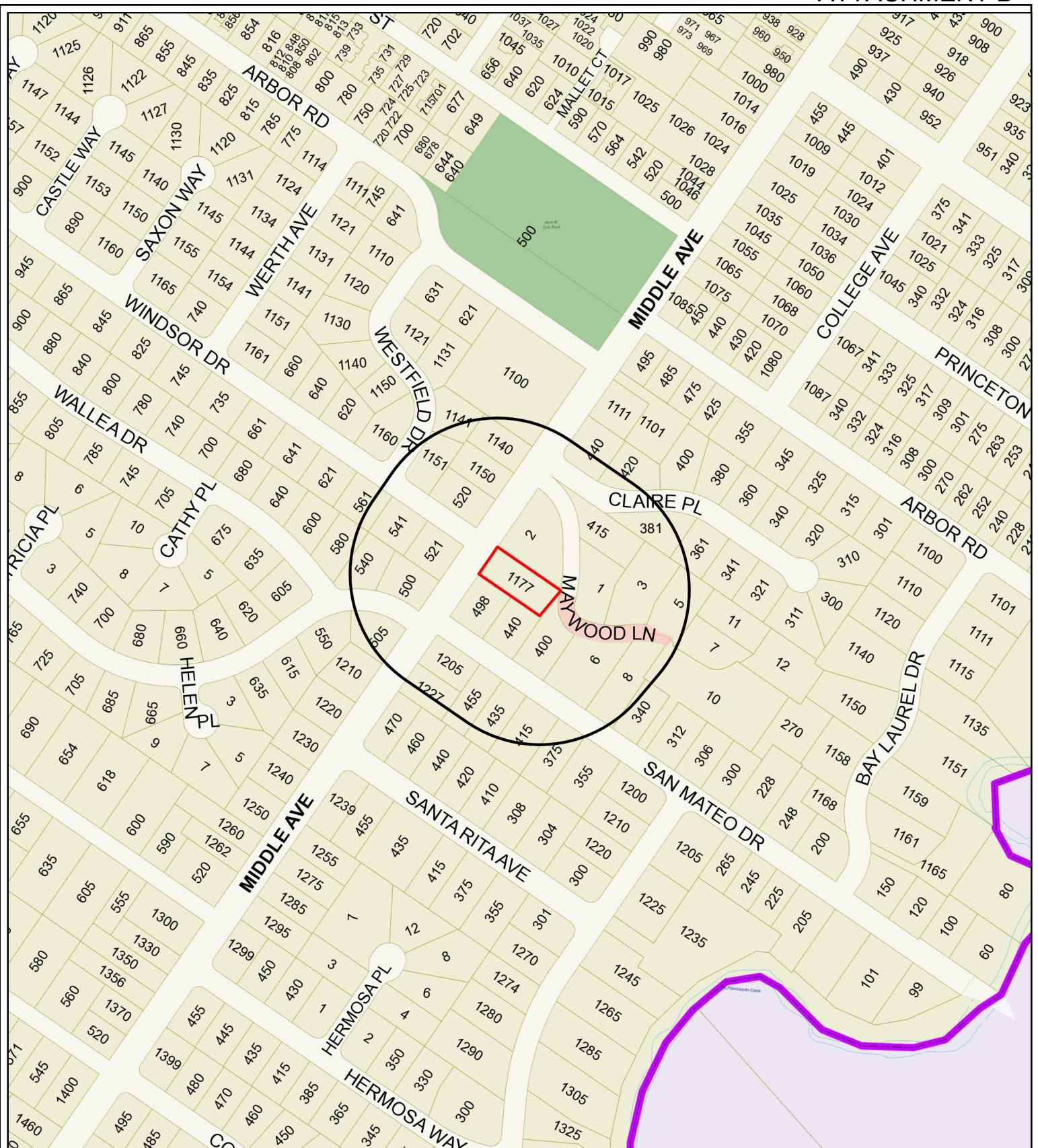
Thomas Rogers, Principal Planner

1177 Middle Avenue – Attachment A: Recommended Actions

LOCATION: 1177 Middle Avenue	PROJECT NUMBER: PLN2016-00058	APPLICANT: Brendan and Carmen Visser	OWNER: Brendan and Carmen Visser
REQUEST: Request for a use permit to demolish an existing single-family, single story residence and construct a new two-story, single-family residence on a substandard lot with regard to lot width in the R-1-S (Single-Family Suburban) zoning district. As part of the project, five heritage trees: two Canary Island date palms, a coast live oak, and two coastal redwoods are proposed for removal.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: <ol style="list-style-type: none">1. Make a finding that the project is categorically exempt under Class 3 (Section 15303, “New Construction or Conversion of Small Structures”) of the current California Environmental Quality Act (CEQA) Guidelines.2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.3. Approve the use permit subject to the following standard conditions:<ol style="list-style-type: none">a. Development of the project shall be substantially in conformance with the plans prepared by Zak Johnson Architects consisting of 15 plan sheets, dated received July 15, 2016, and approved by the Planning Commission on July 25, 2016, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies’ regulations that are directly applicable to the project.c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division.f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits.g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.			

1177 Middle Avenue – Attachment A: Recommended Actions

LOCATION: 1177 Middle Avenue	PROJECT NUMBER: PLN2016-00058	APPLICANT: Brendan and Carmen Visser	OWNER: Brendan and Carmen Visser
REQUEST: Request for a use permit to demolish an existing single-family, single story residence and construct a new two-story, single-family residence on a substandard lot with regard to lot width in the R-1-S (Single-Family Suburban) zoning district. As part of the project, five heritage trees: two Canary Island date palms, a coast live oak, and two coastal redwoods are proposed for removal.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: 4. Approve the use permit subject to the following <i>project-specific</i> condition: a. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a revised arborist report with tree protection measures for trees numbered 7, 20 and 22. The revised arborist report shall be subject to review and approval of the Planning Division. If revisions to the project plans are recommended by the project arborist, City Arborist or as the result of an appeal of the decision regarding this project by the Planning Commission, such changes shall be subject to review and approval of the Planning Division. This condition shall not be applicable for any such tree that ultimately receives a Heritage Tree Removal permit. In addition, the applicant shall submit revised project plans that address the applicable heritage tree replacement requirements, or submit documentation that the City Arborist has waived such requirements, subject to review and approval of the Planning Division.			



City of Menlo Park
Location Map
1177 MIDDLE AVENUE



Scale: 1:3,600

Drawn By: MTM

Checked By: THR

Date: 7/25/2016

Sheet: 1

1177 Middle Avenue – Attachment C: Data Table

	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	12,849 sf	12,849 sf	10,000 sf min.
Lot width	76.5 ft.	76.5 ft.	80 ft. min.
Lot depth	170 ft.	170 ft.	100 ft. min.
Setbacks			
Front	35 ft.	39.6 ft.	20 ft. min.
Rear	81 ft.	85.2 ft.	20 ft. min.
Side (left)	10 ft.	13.5 ft.	10 ft. min.
Side (right)	10 ft.	9.9 ft.	10 ft. min.
Building coverage	3,136.4 sf 24.4 %	2,225.4 sf 17.3 %	4,497 sf max. 35 % max.
FAL (Floor Area Limit)	4,172.4 sf	2,213.5 sf	4,262.3 sf max.
Square footage by floor	2,491.8 sf/1 st 1,189.8 sf/2 nd 465.3 sf/accessory 25.5 sf/attic > 5ft 179.3 sf/porch & fireplace	1,748.2 sf/1 st 11.9 fireplace 465.3 sf/accessory	
Square footage of building	4,351.7 sf	2,225.4 sf	
Building height	26.5 ft.	16 ft.	28 ft. max.
Parking	2 covered	1 covered	1 covered/1 uncovered
Trees	Heritage trees	19	Non-Heritage trees 9* New Trees 2
	Heritage trees proposed for removal	5**	Non-Heritage trees 6 Total Number of proposed for removal Trees 22

* Three trees are in the City's right-of-way.

** Two heritage trees have been tentatively approved for removal by the City Arborist.

Visser Residence

1177 Middle Ave. Menlo Park, California 94025

Project Scope of Work:

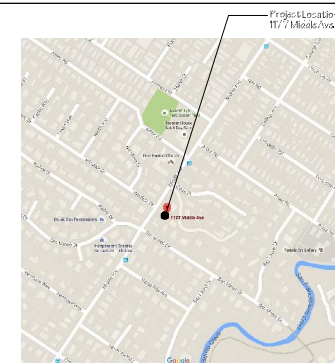
The existing one-story, three bedroom house with 1-car attached garage will be demolished and replaced with a new two-story 4 bedroom house with a 2-car attached garage. The driveway and entrance to the site is being relocated. An existing accessory structure will remain on the property.

Sheet Index:

A-1.0	Site Plan & Project Data Survey
A-1.1	Surrounding Area Plan & Photo Key
A-1.2	Streetscape & Photographs
A-1.3	Existing Conditions of 1-Story House
A-2	First Floor Plan
A-3	2nd Floor Plan & 1st Floor Roof Plan
A-4	2nd Floor Roof Plan
A-5	2nd Floor Reflected Ceiling Plan
A-6	Square Footage Area Calculations
A-7	Exterior Elevations
A-8	Exterior Elevations
A-9	Building Sections
A-10	Building Sections
A-11	Wall Sections

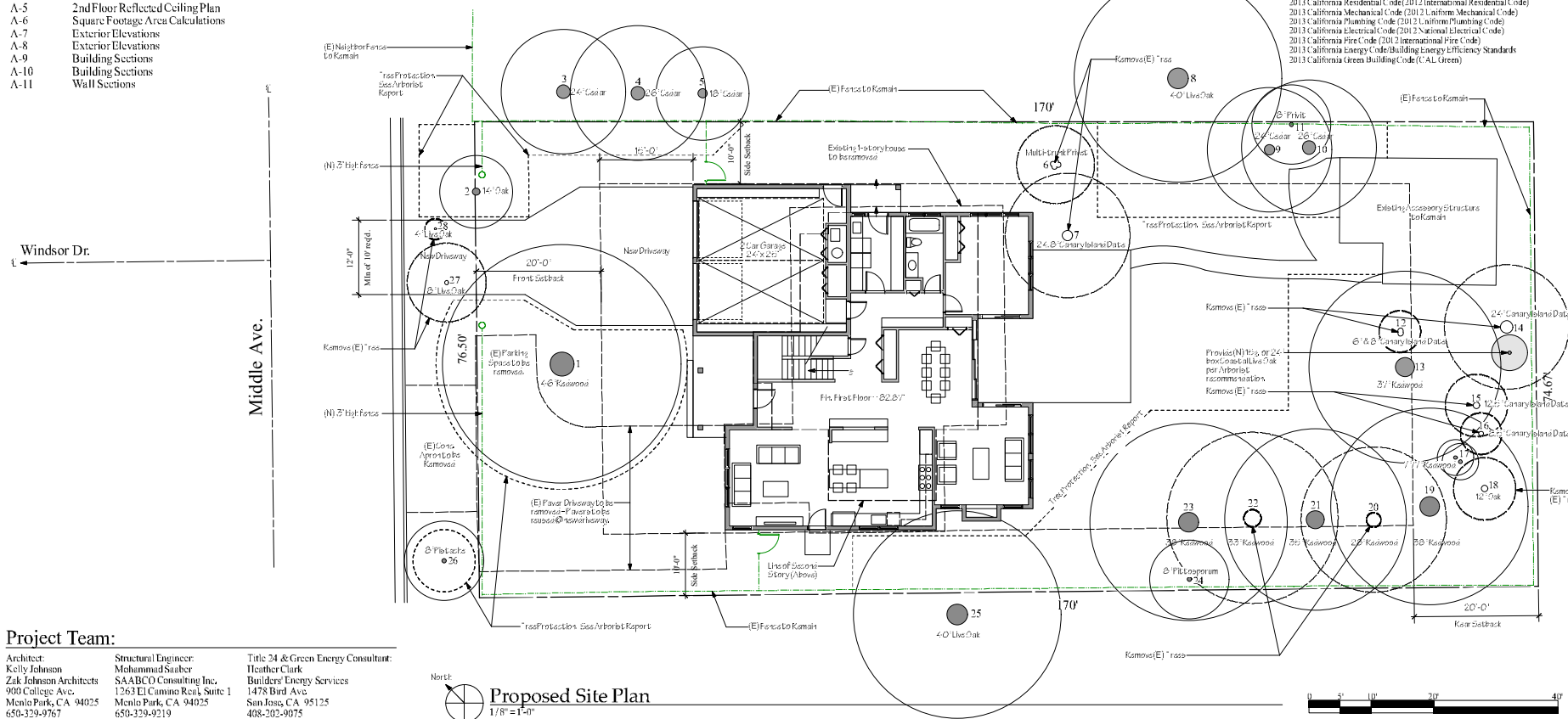
Project Data:

Zoning District:	R1S	Proposed Building Coverage:	2,671.1 sf
Building Occupancy Groups:	R3/U	Coverage of New 2-story House	465.3 sf
Type of Construction:	V-B	(E) Bldg. Coverage Accessory Structure	3,136.4 sf
Lot Area:	12,849.1 sf	Total:	
Allowable Building Coverage (35.0%)	4,497.2 sf	Proposed Floor Area of New House:	2,491.8 sf
Existing Building Coverage	2,213.5 sf	First Floor	1,189.8 sf
Max. Allowed Floor Area Limit:	4,262.3 sf	Second Floor	1,189.8 sf
2,800 sf + .25(12,849.1-7,000)		Attic Space (above 5')	25.5 sf
Max. Allowable 2nd Floor	1,918.0 sf	Total:	3,707.1 sf
76.50' x 170' x 4,262.3		Proposed Total Floor Area	3,707.1 sf
Existing Building Coverage:		New House	465.3 sf
(E) Bldg. Coverage of House & Garage:	1,760.1 sf	Accessory Structure	4,172.4 sf
(E) Bldg. Coverage Accessory Structure	465.3 sf	Total:	
Total:	2,225.4 sf	Existing Height:	Approx. 16'-0"
Existing Floor Area:		Max. Height:	28'-0"
(E) Floor Area of House & Garage:	1,748.2 sf	Proposed Height:	Max. 26'-6"
(E) Floor Area of Accessory Structure	465.3 sf		
Total	2,213.5 sf		



Applicable Codes & Regulations:

2013 California Building Code (2012 International Building Code)
 2013 California Residential Code (2012 International Residential Code)
 2013 California Mechanical Code (2012 Uniform Mechanical Code)
 2013 California Plumbing Code (2012 Uniform Plumbing Code)
 2013 California Electrical Code (2012 National Electrical Code)
 2013 California Fire Code (2012 International Fire Code)
 2013 California Energy Code (Building Energy Efficiency Standards)
 2013 California Green Building Code (C.A.L. Green)



Project Team:

Architect: Kelly Johnson Zak Johnson Architects 900 College Ave. Menlo Park, CA 94025 650-329-9767	Structural Engineer: Mohammad Saaber SAABCO Consulting Inc. 1263 El Camino Real, Suite 1 Menlo Park, CA 94025 650-329-9219	Title 24 & Green Energy Consultant: Heather Clark Builders' Energy Services 1478 Bird Ave. San Jose, CA 95125 408-202-9075
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Use Permit Response
 1/11/17, 2016
 Final Use Permit Prep
 1/11/17, 2016

Brendan & Carmen Visser
 1177 Middle Ave.
 Menlo Park, CA 94025

Zak Johnson Architects
 900 College Avenue
 Menlo Park, CA 94025
 650.329.9767 | kelly@zakjohnson.com

Sheet Title

Scale
 As Noted

Job No.

Date
 May 2, 2016

A-1.0

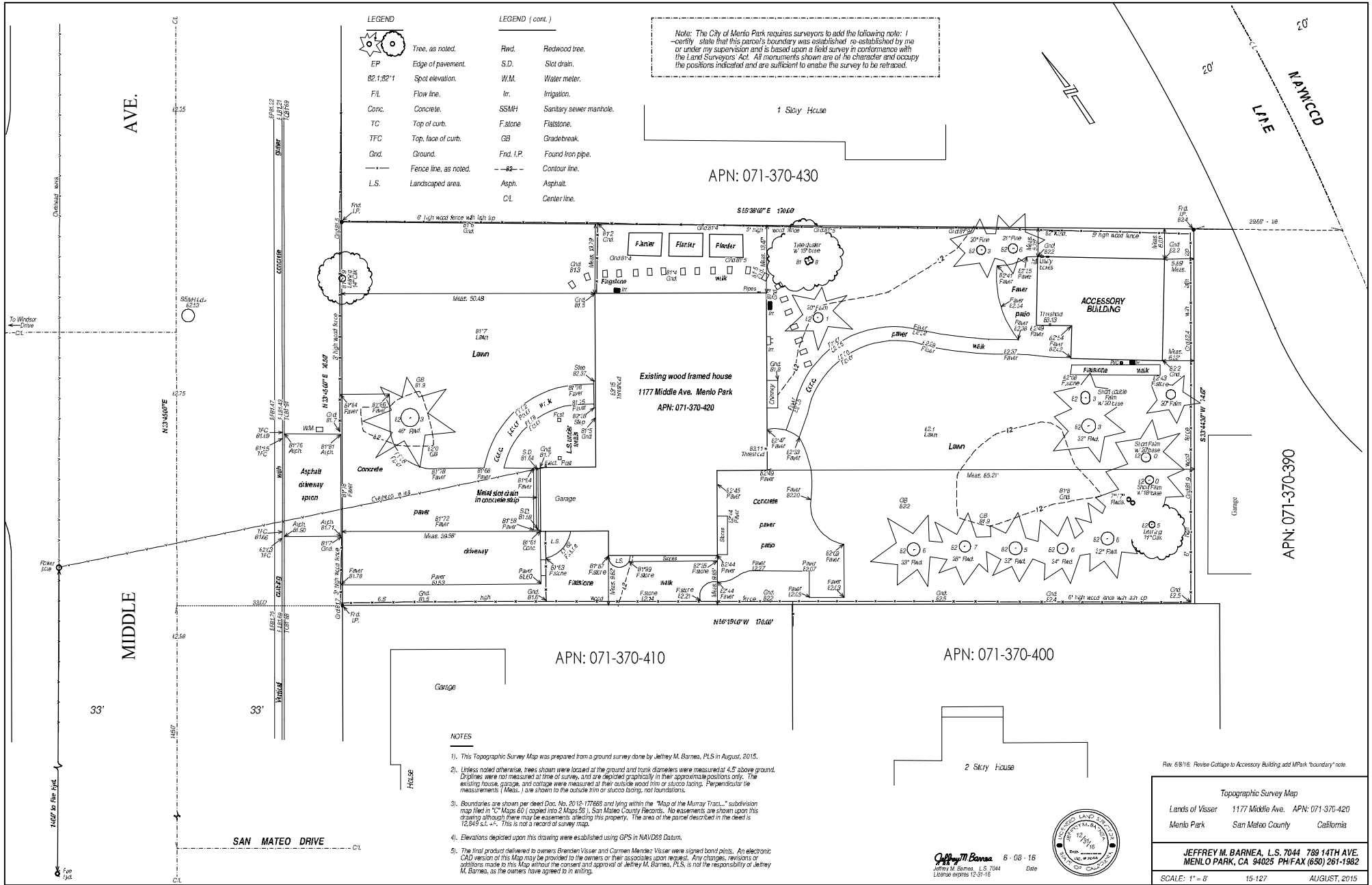




Photo #1



Photo #2



Photo #3



Photo #4

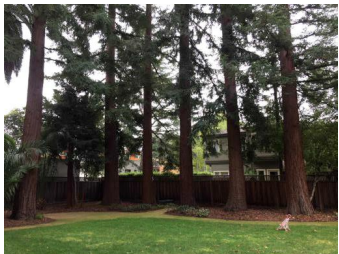


Photo #5



Photo #6



Photo #7



Photo #8

Windsor Dr.

#1

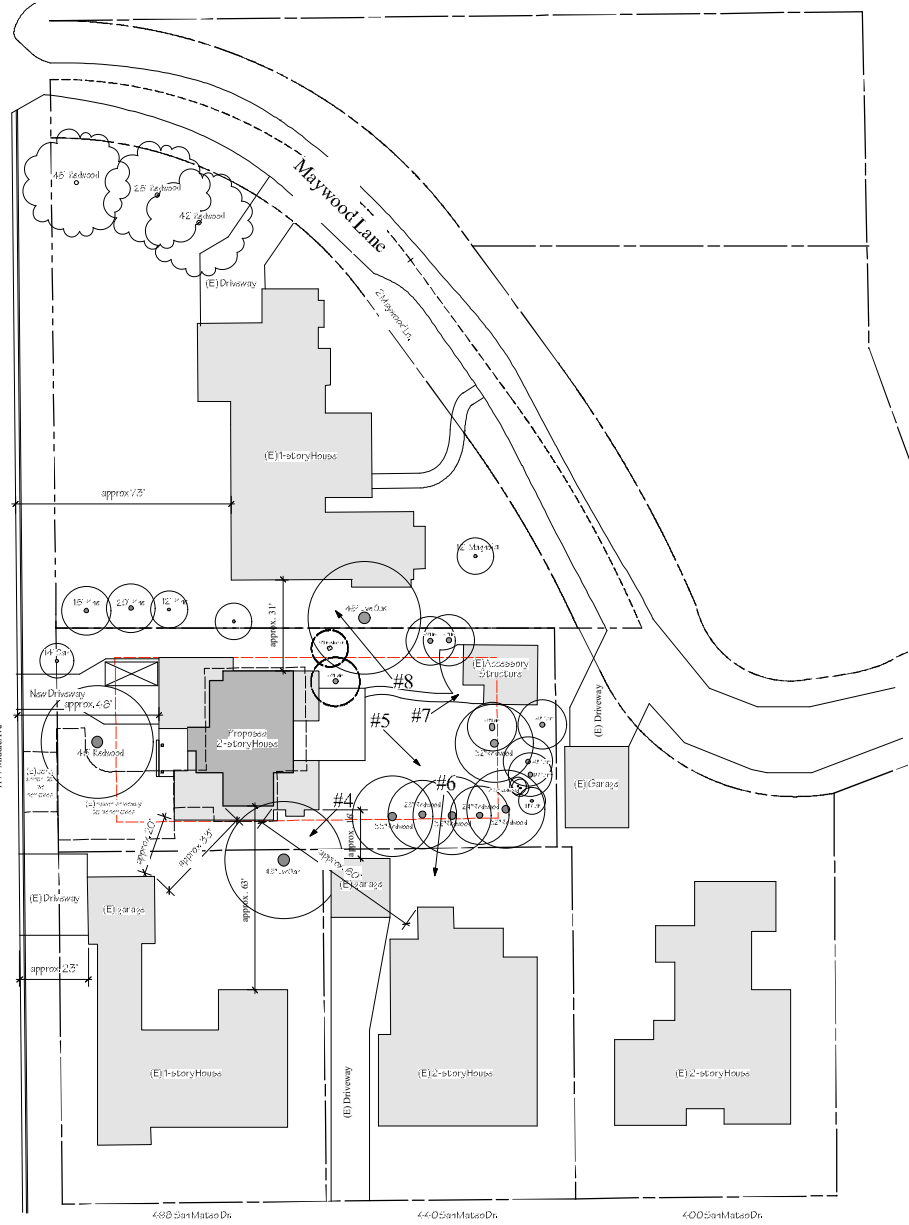
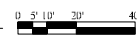
#2

#3

Middle Ave.
1177 Middle Ave



Surrounding Area Plan & Photo Key



Use Permit Response
April 17, 2016
Final Use Permit Prop.
July 13, 2016

Brendan & Carmen Visser
1177 Middle Ave.
Menlo Park, CA 94025

Licensed Architect
Kelly W. Johnson
C-28919
Rev. 8.3.17
State of California

Zak Johnson Architects
900 College Avenue
Menlo Park, CA 94025
650.329.9767

Sheet Title

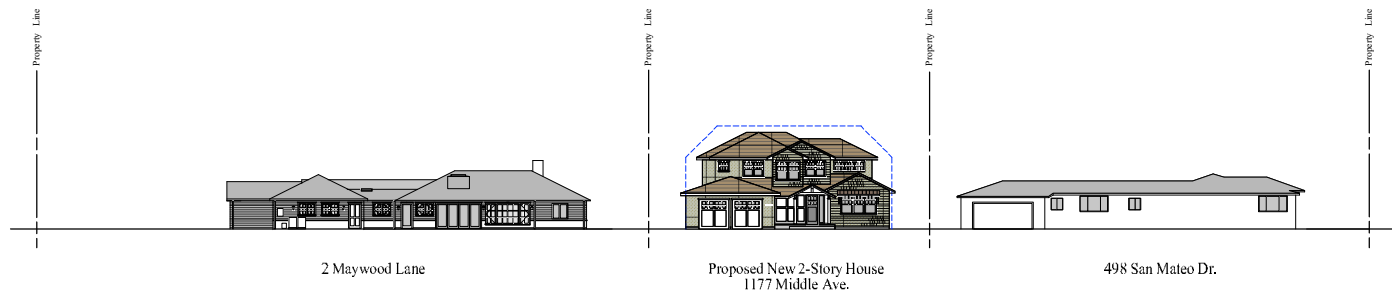
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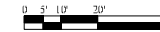
May 2, 2016

A-1.1



Streetscape - Middle Ave.

1/16" = 1'-0"



2 Maywood Lane



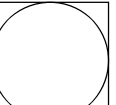
Project Location
1177 Middle Ave.
Existing House to be
demolished.



498 San Mateo Dr.

Photographs of Streetscape - Middle Ave.

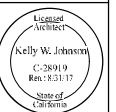
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Use Permit Required
April 17, 2016

Final Use Permit Prop.
July 17, 2016

Brendan & Carmen Visser
1177 Middle Ave.
Menlo Park, CA 94025



Zak Johnson Architects
900 College Avenue
Menlo Park, CA 94025
650.329.9767

Sheet Title

Scale
As Noted

Job No.

Date
May 2, 2016

A-1.2



Northwest Elevation - Existing House



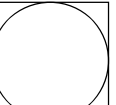
Southwest Elevation - Existing House



Southeast Elevation - Existing House



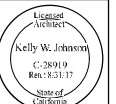
Northeast Elevation - Existing House



Use Permit K-000000
April 17, 2016

Final Use Permit Prop.
July 13, 2016

Brendan & Carmen Visser
1177 Middle Ave.
Menlo Park, CA 94025



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Sheet Title

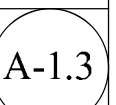
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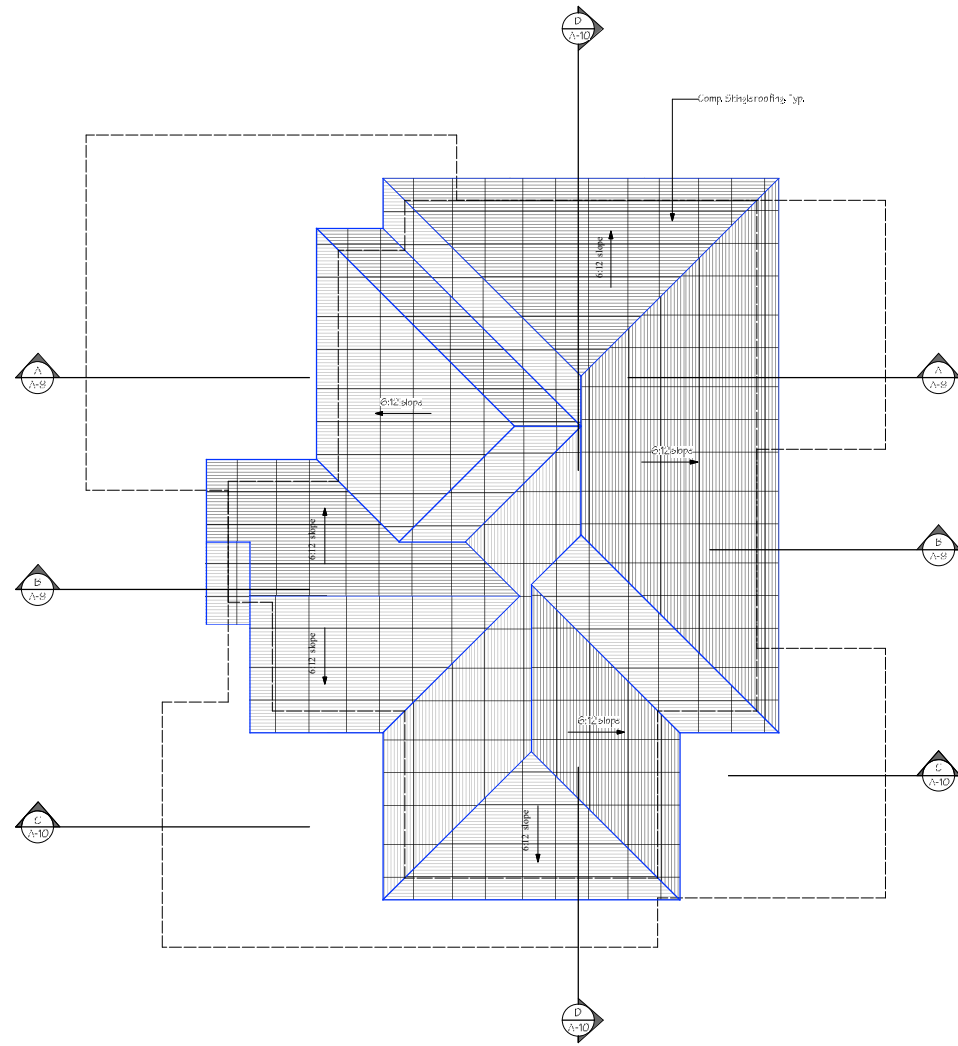
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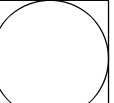
May 2, 2016





Second Floor Roof Plan

1/4" = 1'-0"



Use Permit Required
1/17/2016

Final Use Permit Prep
July 13, 2016

Brendan & Carmen Visser
1177 Middle Ave.
Menlo Park, CA 94025



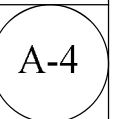
Zak Johnson Architects
900 College Avenue
Menlo Park, CA 94025
650.329.9767 kelly@zakjohnson.com

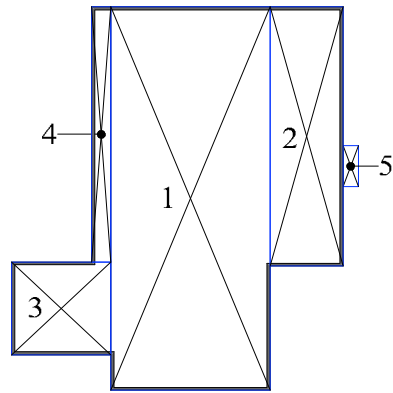
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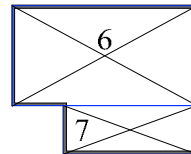
Job No.

Date
May 2, 2016

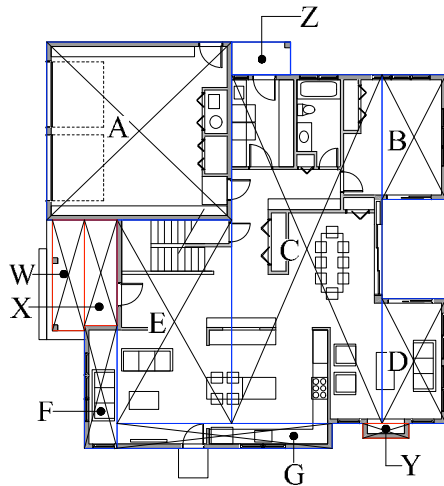




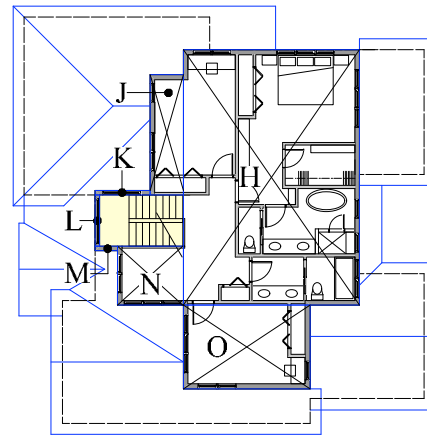
Existing Floor Plan of 1-story House - To be Demolished
1/8" = 1'-0"



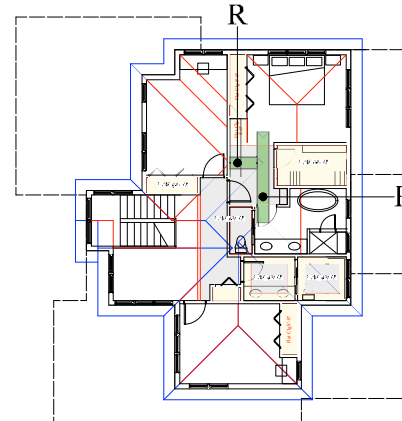
Existing Floor Plan of 1-story Accessory Structure- To Remain on Site
1/8" = 1'-0"



Proposed First Floor Plan
1/8" = 1'-0"



Proposed Second Floor Plan
1/8" = 1'-0"



2nd Floor Reflected Clg. Plan / Roof Plan
1/8" = 1'-0"

EXISTING HOUSE

Floor Area Coverage Calculations:

Area	Dimensions	Area
1	(21'-8 5/8" x 52'-3 1/4")	1,135.3 sf
2	(9'-10 3/4" x 35'-4 1/8")	349.9 sf
3	(13'-6 1/2" x 12'-8 1/4")	171.8 sf
4	(2'-7 1/2" x 34'-8 7/8")	91.2 sf

Existing Floor Area Coverage TOTAL = 1,748.2 sf

Building Coverage Calculations:

Area	Dimensions	Area
1	(21'-8 5/8" x 52'-3 1/4")	1,135.3 sf
2	(9'-10 3/4" x 35'-4 1/8")	349.9 sf
3	(13'-6 1/2" x 12'-8 1/4")	171.8 sf
4	(2'-7 1/2" x 34'-8 7/8")	91.2 sf
5	(2'-11 1/2" x 5'-7")	11.9 sf

Existing Floor Area Coverage TOTAL = 1,760.1 sf

EXISTING ACCESSORY STRUCTURE

Floor Area & Building Coverage Calculations:

Area	Dimensions	Area
6	(25'-2 5/8" x 13'-8 1/4")	345.3 sf
7	(18'-2 5/8" x 6'-7")	120.0 sf

Floor Area & Building Coverage TOTAL = 465.3 sf

PROPOSED NEW 2-STORY HOUSE

Building Coverage Calculations:

Area	Dimensions	Area
A	(25'-3" x 24'-3")	612.3 sf
B	(8'-9 3/8" x 17'-0")	149.3 sf
C	(20'-5 5/8" x 47'-6 5/8")	973.4 sf
D	(8'-9 3/8" x 17'-0")	149.3 sf
E	(15'-7" x 27'-9 1/8")	432.6 sf
F	(4'-6" x 16'-9")	75.4 sf
G	(29'-3 1/2" x 3'-4 3/4")	99.5 sf
W	(4'-4 1/2" x 15'-1 3/16")	66.0 sf
X	(4'-6" x 14'-5")	64.9 sf
Y	(6'-3" x 2'-0")	12.5 sf
Z	(8'-0 1/2" x 4'-5 1/2")	35.9 sf

PROPOSED Building Coverage TOTAL = 2,671.1 sf

Floor Area Coverage Calculations:

Area	Dimensions	Area
A	(25'-3" x 24'-3")	612.3 sf
B	(8'-9 3/8" x 17'-0")	149.3 sf
C	(20'-5 5/8" x 47'-6 5/8")	973.4 sf
D	(8'-9 3/8" x 17'-0")	149.3 sf
E	(15'-7" x 27'-9 1/8")	432.6 sf
F	(4'-6" x 16'-9")	75.4 sf
G	(29'-3 1/2" x 3'-4 3/4")	99.5 sf

First Floor Area = 2,491.8 sf

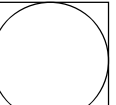
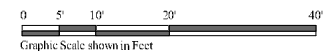
H	(24'-0" x 34'-9 7/8")	835.8 sf
J	(4'-6 3/8" x 16'-4 1/2")	74.2 sf
K	(7'-6 1/4" x 0'-7 1/2")	4.7 sf
L	(7'-0" x 0'-7 1/2")	4.4 sf
M	(3'-0" x 0'-7 1/2")	1.9 sf
N	(9'-0 5/8" x 8'-0 3/8")	72.7 sf
O	(17'-2 7/8" x 11'-4 1/2")	196.1 sf

Second Floor Area = 1,189.8 sf

P	(1'-7" x 12'-4 3/4")	19.6 sf
R	(1'-7" x 3'-8 7/8")	5.9 sf

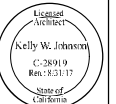
Attic Area (above 5') = 25.5 sf

Proposed Floor Area TOTAL = 3,707.1 sf



Use Permit Corporation
1/17/2016
Final Use Permit Prop.
July 17, 2016

Brendan & Carmen Visser
1177 Middle Ave.
Menlo Park, CA 94025



Zak Johnson Architects
900 College Avenue
Menlo Park, CA 94025
650.329.9767 kelly@zakjohnson.com

Sheet Title

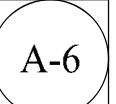
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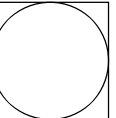
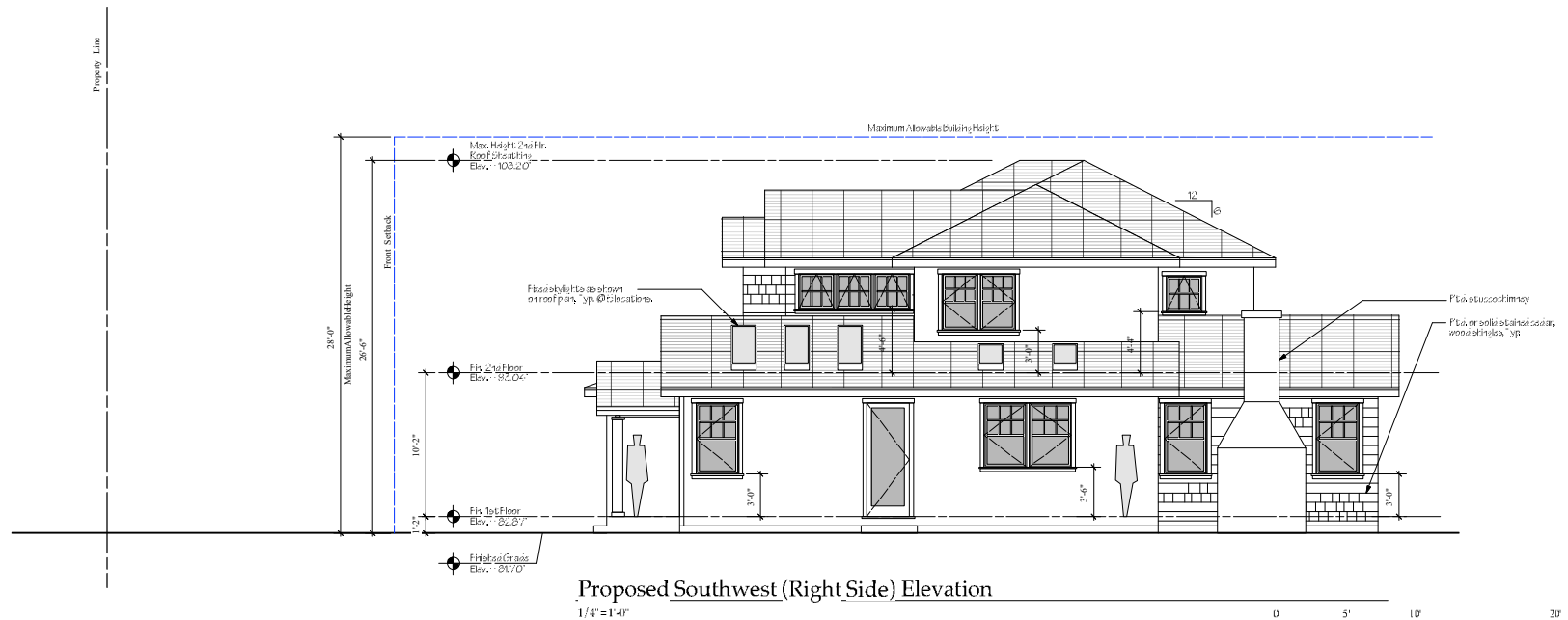
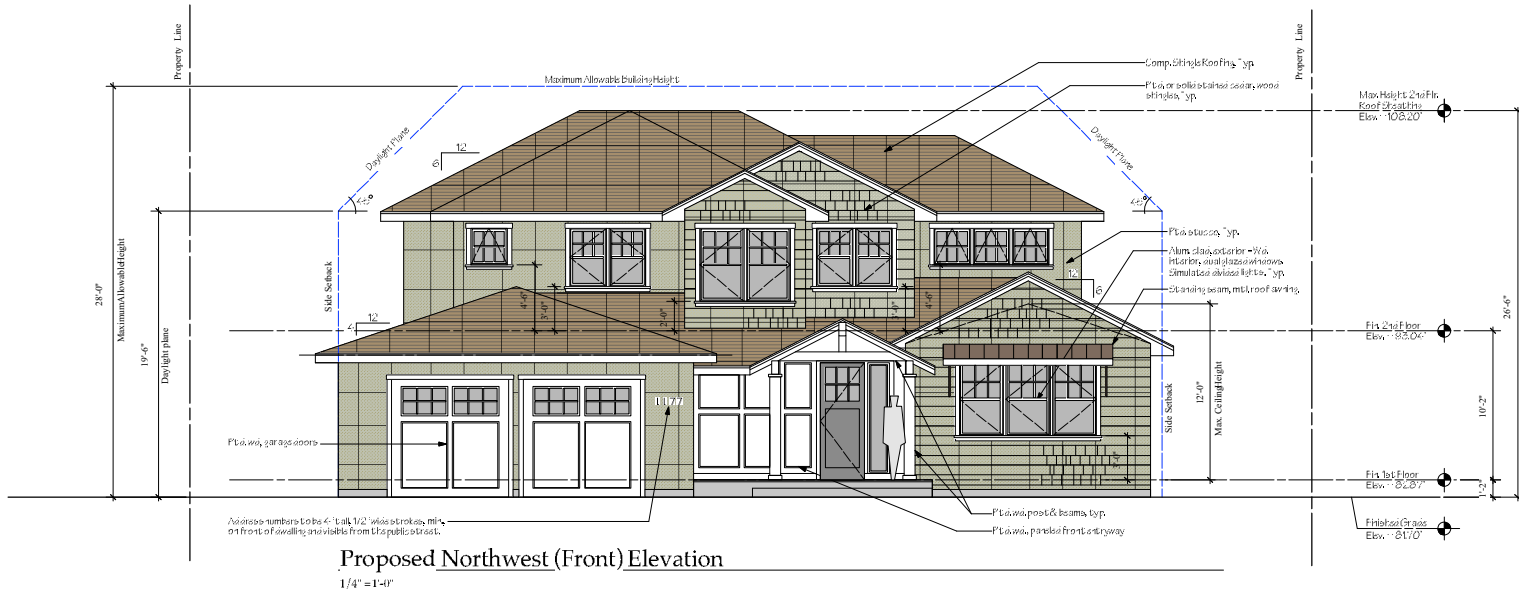
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Date

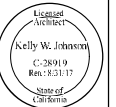
May 2, 2016



A-6



Brendan & Carmen Visser
1177 Middle Ave.
Menlo Park, CA 94025



Zak Johnson Architects
900 College Avenue
Menlo Park, CA 94025
650.329.9767 kelly@zakjohnson.com

Sheet Title

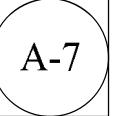
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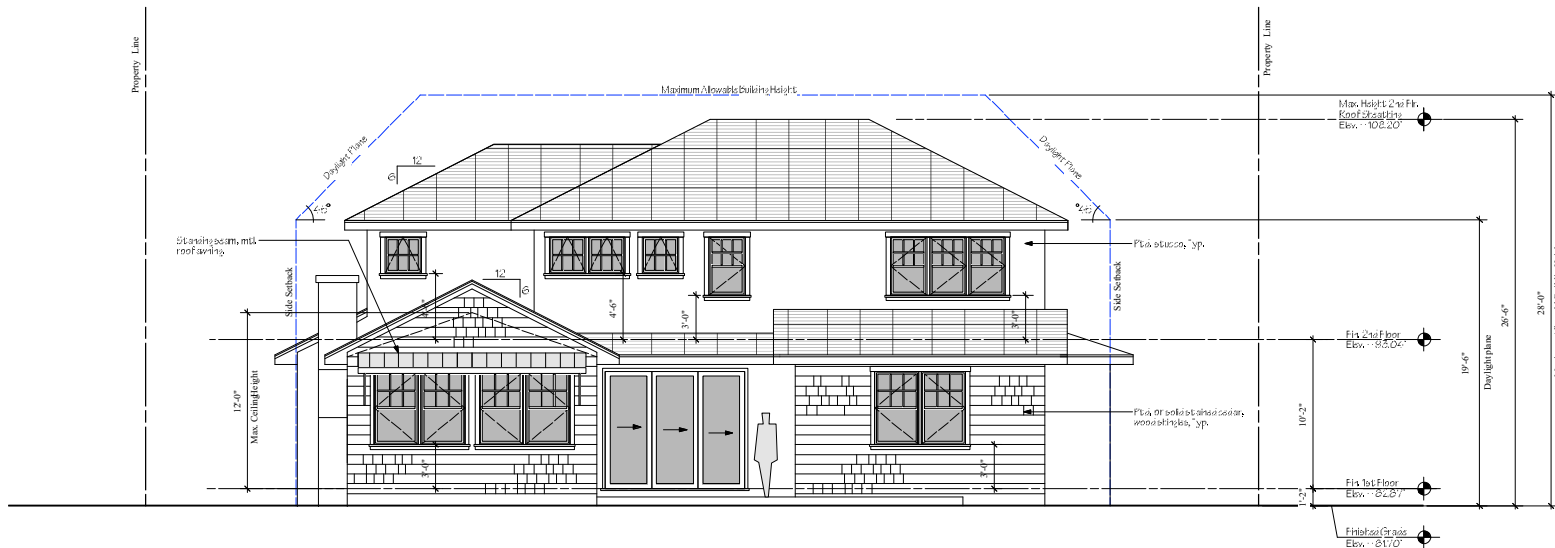
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Job No.

Date

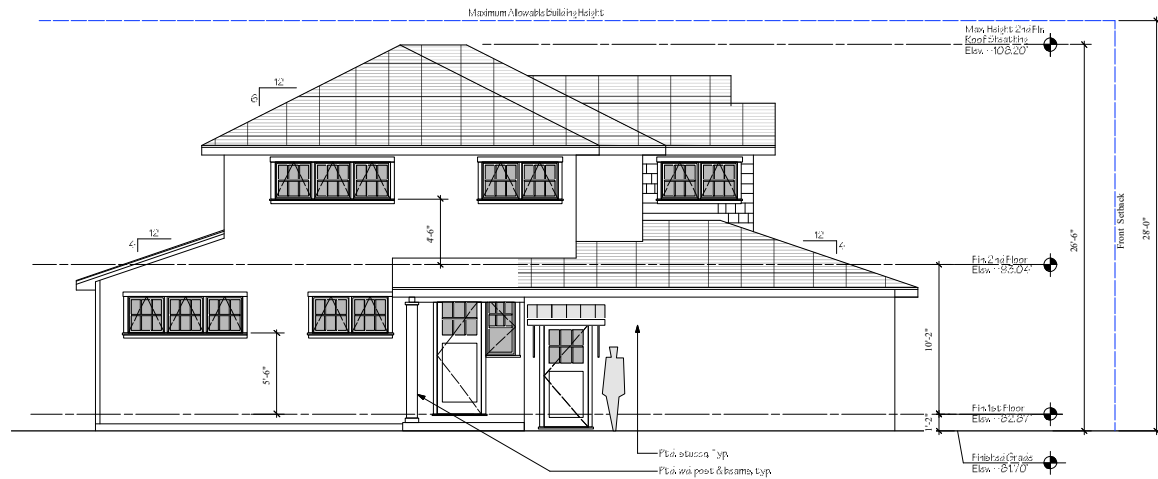
May 2, 2016





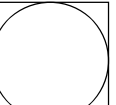
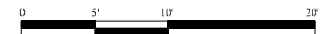
Proposed Southeast (Rear) Elevation

1/4" = 1'-0"



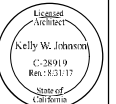
Proposed Northeast (Left Side) Elevation

1/4" = 1'-0"



Use Permit Response
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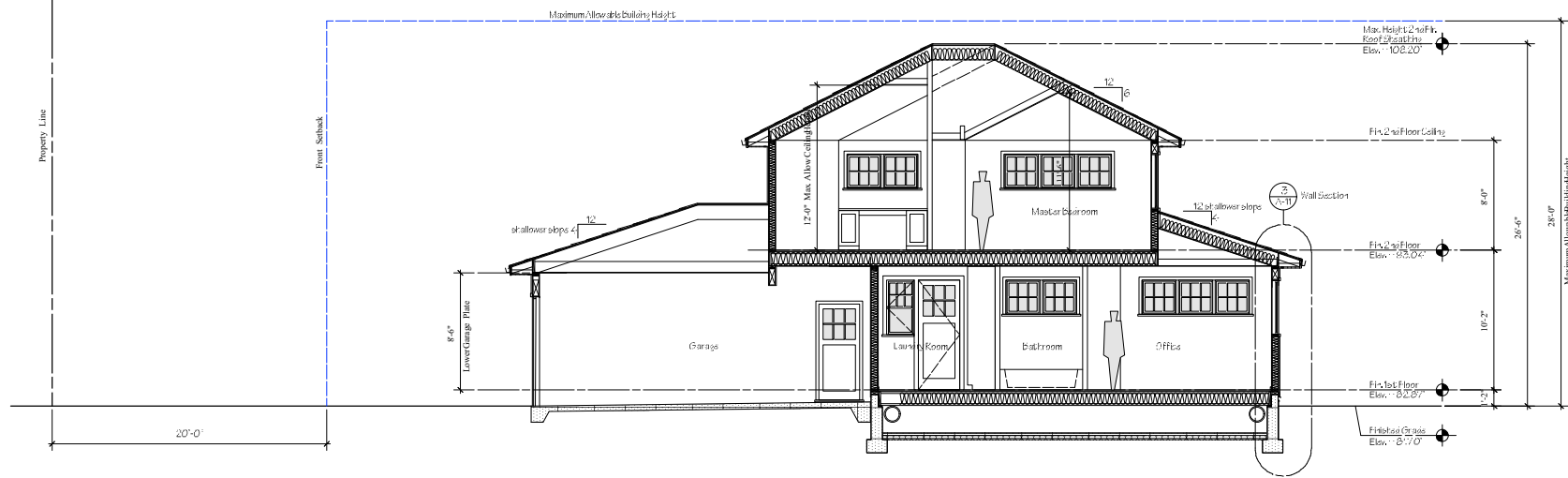
Sheet Title

Scale
As Noted

Job No.

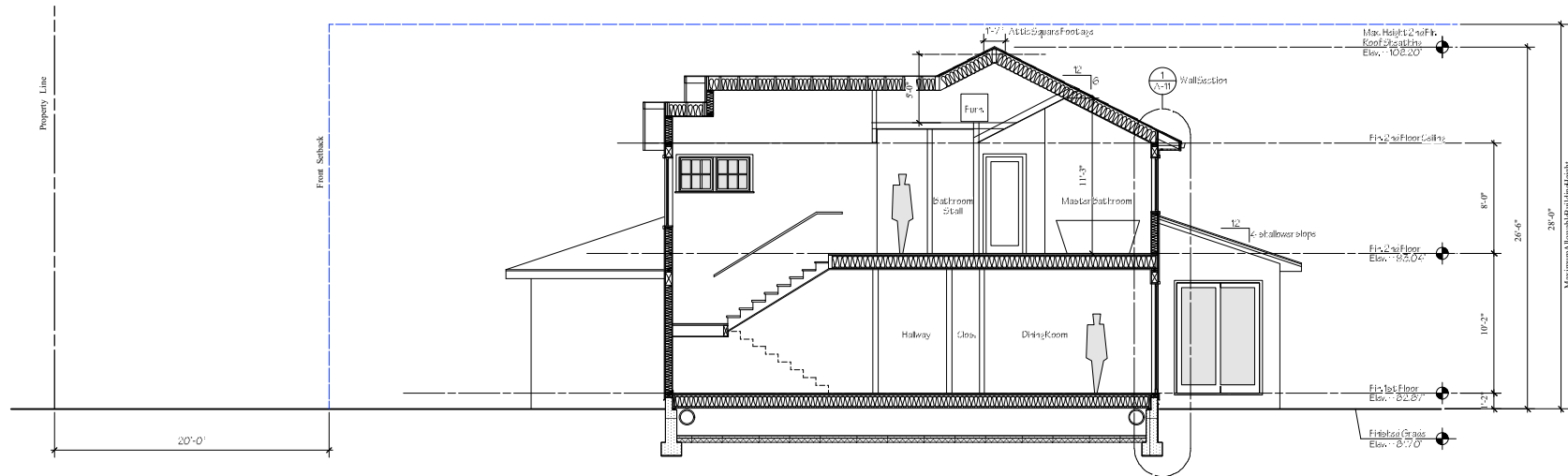
Date
May 2, 2016

A-8



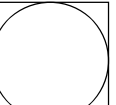
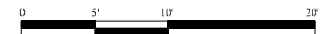
Building Section A-A

1/4" = 1'-0"



Building Section B-B

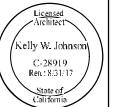
1/4" = 1'-0"



Use Permit Required Under 17, 2016

Final Use Permit Required Under 17, 2016

Brendan & Carmen Visser
1177 Middle Ave.
Menlo Park, CA 94025



Zak Johnson Architects
900 College Avenue
Menlo Park, CA 94025
650.329.9767 kelly@zakjohnson.com

Sheet Title

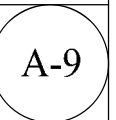
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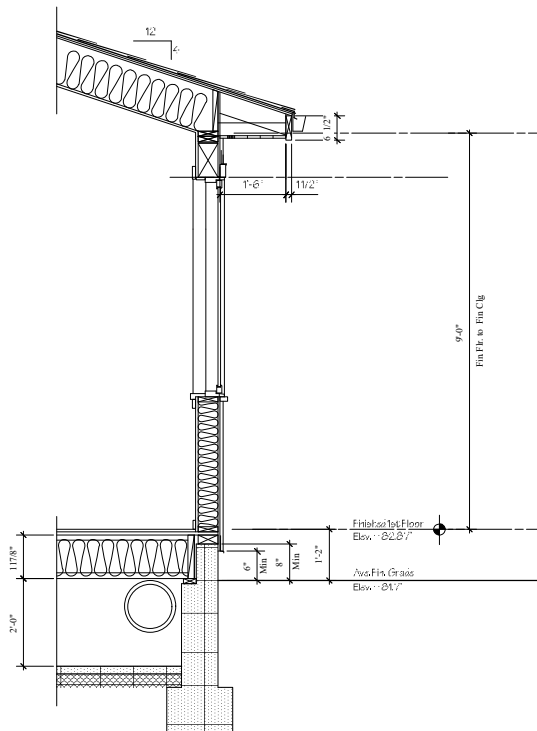
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Job No.

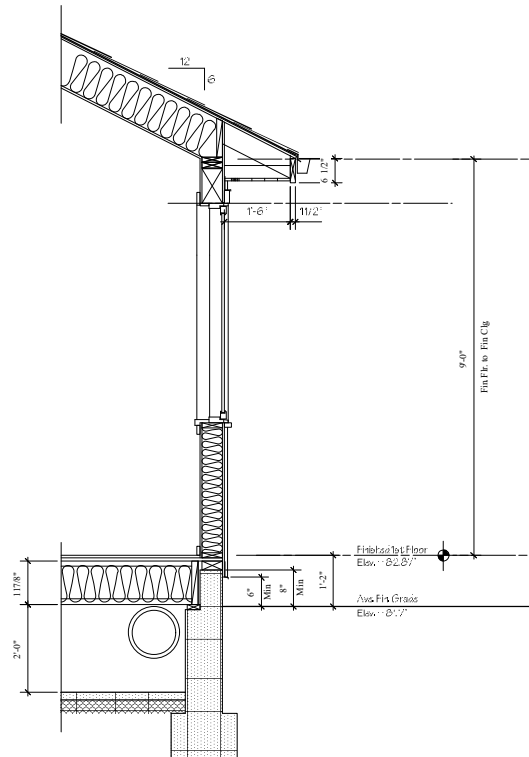
Date

May 2, 2016

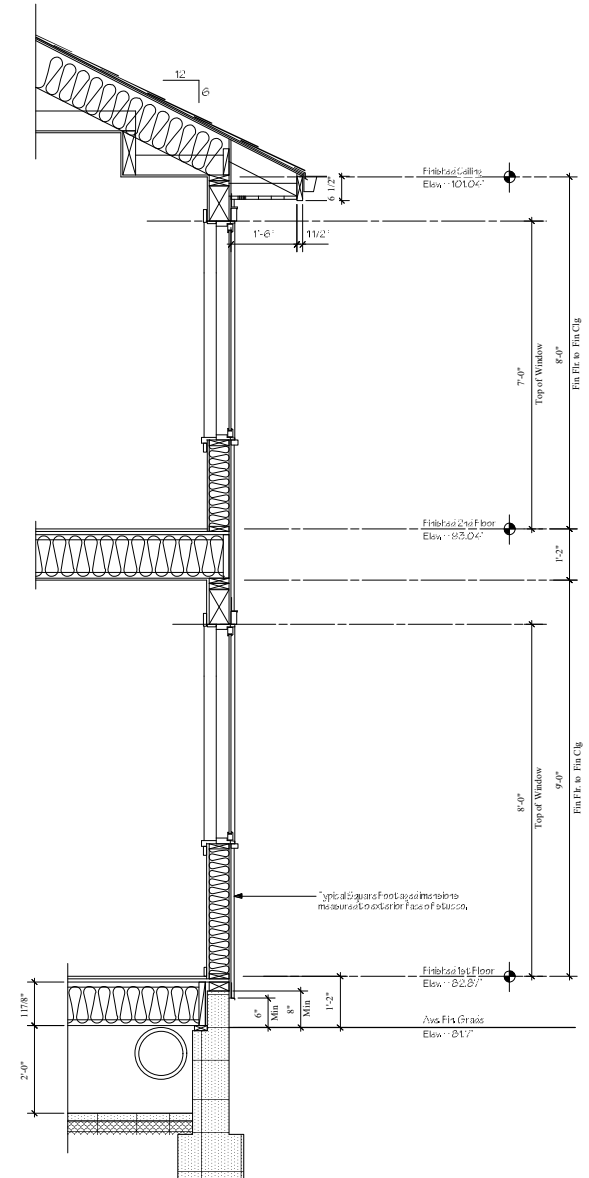




3 Typical 1-Story Wall Section 4:12 slope
A-11 3/4" = 1'-0"



2 Typical 1-Story Wall Section 6:12 slope
A-11 3/4" = 1'-0"



1 Typical 2-Story Wall Section
A-11 3/4" = 1'-0"

Use Permit Kopasada
April 17, 2016
Final Use Permit Prop.
July 13, 2016

Brendan & Carmen Visser
1177 Middle Ave.
Menlo Park, CA 94025

Licensed
Architect
Kelly W. Johnson
C-28919
Rev. 8.3.11
State of
California

Zak Johnson Architects
900 College Avenue
Menlo Park, CA 94025
650.329.9767 kelly@zakjohnson.com

Sheet Title

Scale
As Noted

Job No.

Date
May 2, 2016

A-11

May 2, 2016

Project Description

1177 Middle Ave., Menlo Park

We are submitting this Use Permit Application for your consideration. Due to the overall width of the site, this property is considered non-conforming. These dimensions can be verified on the site plan on Sheet A-1.

The existing one-story, three bedroom house with a 1-car attached garage (2,213 sf) will be demolished and replaced with a new two-story four bedroom house with a 2-car attached garage (3,707 sf). The proposed house has not been designed to include a basement. The driveway entrance to the site is being relocated. An existing accessory structure (465 sf) will remain on the property.

The overall massing of the residence is broken down into several 1 story elements around the perimeter of the house. The house is a traditional architectural style that includes a mix of stucco, and wood shingle exterior cladding. This is complimentary to the remaining accessory structure, which also has a stained, wood shingle exterior cladding. The proposed window style and pattern add to the traditional appearance. The front porch is designed to have a painted wood paneled motif. Simply styled wood columns and a closed, box eave enhance the more traditional feel of the proposed design. The project will be constructed using typical wood stud and poured concrete foundations.

The site layout and location of this proposed house were critical variables in the design of this project. The existing property has a magnificent, park like setting. The property contains 7 large Redwood trees. In addition to the owners' trees, the neighboring property trees also contribute substantially to the overall dense canopy. With very careful consideration, the house was located to least impact these giants. Rob Weatherill with Advanced Tree Care is the Arborist of record. After much discussion with the Owners, Mr. Weatherill has recommended that two of the Redwoods and several smaller non-regulated and regulated Canary Date Palms should be removed as a necessary approach to strengthen the overall health and future of the remaining Redwood trees. At this time, the owners have decided to move forward with this work. These recommendations are further explained and noted in the Arborist Report, Heritage Tree Removal Application and on the Site Plan.

In addition to our design work, the Owners, Brendan & Carmen Visser, have reached out to the immediate neighbors that would be impacted by this project. Through an email exchange, Brendan shared several preliminary drawings with the 4 neighbors who have adjacent properties. Attached you will find the 4 email responses he received.

Kelly Johnson, Architect
Zak Johnson Architects
Menlo Park, CA

RECEIVED

MAY 02 2016

CITY OF MENLO PARK
BUILDING

"Brendan C. Visser" <brendanvisser@yahoo.com>

April 26, 2016 8:46 PM

To: Carmen Mendez Visser <mendezcarmen@yahoo.com>, Kelly Johnson <kelly@zakjohnson.com>

Fwd: rebuild of 1177

Sent from my iPhone

Begin forwarded message:

From: "John B. Sunwoo M.D." <sunwoo@stanford.edu>

Date: April 26, 2016 at 8:25:00 PM PDT

To: Brendan C Visser <bvisser@stanford.edu>

Subject: Re: rebuild of 1177

Hi, Brendan and Carmen

The plans for the rebuild look fantastic! Congratulations. We can't wait to see the new place.

John and Jill

On Apr 6, 2016, at 10:48 PM, Brendan C Visser <bvisser@stanford.edu> wrote:

Hi neighbors,

We are finalizing plans to rebuild our house in the coming months and wanted to share our plans with you. We hope to break ground in early Fall. We are really excited about this project as we love the neighborhood and this new house will give us some much needed space as the girls grow. We have attached our architect's drawings. Please let us know if you have any questions.

Cheers,

Carmen and Brendan

<1177 surrounding houses.pdf><1177 SE and NE.pdf><1177 NW and SW.pdf><1177 first floor.pdf><1177 2nd floor.pdf>

Brendan C. Visser <brendanvisser@yahoo.com>

To: Kelly Johnson

Fwd: rebuild of 1177

April 19, 2016 2:32 PM

Sent from my iPhone

Begin forwarded message:

From: Minna Tong <mihahn@gmail.com>
Date: April 9, 2016 at 3:33:00 PM PDT
To: Brendan C Visser <bvisser@stanford.edu>
Cc: Carmen Mendez <mendezcarmen@yahoo.com>, Jeff Tong <jeffrey.tong@gmail.com>
Subject: Re: rebuild of 1177

Hi, Brendan and Carmen, greetings from Melbourne! The plans for the new house look great. We are so excited for your family! Obviously, if there is anything we can do to help before/during construction, please don't hesitate to let us know.

Hope all is well in MP!

Minna and Jeff

Sent from my iPhone

On Apr 7, 2016, at 3:48 PM, Brendan C Visser <bvisser@stanford.edu> wrote:

Hi neighbors,

We are finalizing plans to rebuild our house in the coming months and wanted to share our plans with you. We hope to break ground in early Fall. We are really excited about this project as we love the neighborhood and this new house will give us some much needed space as the girls grow. We have attached our architect's drawings. Please let us know if you have any questions.

Cheers,

Carmen and Brendan
<1177 surrounding houses.pdf>
<1177 SE and NE.pdf>
<1177 NW and SW.pdf>
<1177 first floor.pdf>
<1177 2nd floor.pdf>

Brendan C. Visser <brendanvisser@yahoo.com>

To: Kelly Johnson

Fwd: rebuild of 1177

April 19, 2016 2:33 PM

Sent from my iPhone

Begin forwarded message:

From: Michael Lambert <mlambert498@sbcglobal.net>

Date: April 10, 2016 at 11:56:21 AM PDT

To: Brendan C Visser <bvisser@stanford.edu>

Subject: Re: rebuild of 1177

Carmen and Brendan,

Thanks for the note. I suspected something was going to occur when I saw the survey crew last year.

Good luck with your project.

Mike Lambert

On Apr 6, 2016, at 10:48 PM, Brendan C Visser wrote:

Hi neighbors,

We are finalizing plans to rebuild our house in the coming months and wanted to share our plans with you. We hope to break ground in early Fall. We are really excited about this project as we love the neighborhood and this new house will give us some much needed space as the girls grow. We have attached our architect's drawings. Please let us know if you have any questions.

Cheers,

Carmen and Brendan

<1177 surrounding houses.pdf><1177 SE and NE.pdf><1177 NW and SW.pdf><1177 first floor.pdf><1177 2nd floor.pdf>

"Brendan C. Visser" <brendanvisser@yahoo.com>

To: Kelly Johnson

Fwd: rebuild of 1177

April 19, 2016 2:35 PM

Sent from my iPhone

Begin forwarded message:

From: "Wilmot, Helen M." <HWilmot@stanfordhealthcare.org>
Date: April 7, 2016 at 4:44:03 AM PDT
To: "Visser, Brendan, M.D." <bvisser@stanford.edu>, "mendezcarmen@yahoo.com" <mendezcarmen@yahoo.com>
Cc: Kelly Johnson <kelly@zakjohnson.com>, "Davidson, G Toll (Gibson Dunn)" <gdavidson@gibsondunn.com>
Subject: RE: rebuild of 1177

Brendan & Carmen,

We love the neighborhood too. We are all excited for you and for the girls. Please let us know if there is anything we can do to help in your logistics as you move out, live away and move back in. Feel free to call me. 650-868-9859.

Helen & Greg

Helen M. Wilmot
Vice President, Facilities Services & Planning

650-725-3063 (*office*)
650-868-9859 (*mobile*)

Sonia Martinez
650-723-7132
somartinez@stanfordhealthcare.org

From: Visser, Brendan, M.D.
Sent: Wednesday, April 06, 2016 10:49 PM
To: Michael Lambert; Wilmot, Helen M.; Sunwoo, John; Minna Hahn Tong
Cc: Kelly Johnson
Subject: rebuild of 1177

Hi neighbors,

We are finalizing plans to rebuild our house in the coming months and wanted to share our plans with you. We hope to break ground in early Fall. We are really excited about this project as we love the neighborhood and this new house will give us some much needed space as the girls grow. We have attached our architect's drawings. Please let us know if you have any questions.

Cheers,

Carmen and Brendan

Advanced Tree Care

P. O. Box 5326 Redwood City, CA 94063

1177 Middle Rd, Menlo Park

March 31, 2016

Carmen and Brendan Visser
1177 Middle Rd
Menlo Park, CA 94025

March 31, 2016

Site: 1177 Middle Rd, Menlo Park

Dear Carmen and Brendan,

At your request I visited the above site for the purpose of inspecting and commenting on the heritage trees around the property. A new house is planned for this property, prompting the need for this tree protection report.

Method:

The location of all the notable trees on this site can be found on the plan provided by you. The trees are measured at 54 inches above ground level (DBH or Diameter at Breast Height). A condition rating of 1 to 100 is assigned to each tree representing form and vitality on the following scale:

1 to 29	Very Poor
30 to 49	Poor
50 to 69	Fair
70 to 89	Good
90 to 100	Excellent

The height and spread of each tree is estimated. A Comments section is provided for any significant observations affecting the condition rating of the tree.

A Summary and Tree Protection Plan are at the end of the end of the survey providing recommendations for maintaining the health and condition of the trees during and after construction such that there is insignificant impact on the health and condition of the trees

If you have any questions, please don't hesitate to call.

Sincerely




Robert Weatherill
Certified Arborist WE 1936A

Advanced Tree Care

P. O. Box 5326 Redwood City, CA 94063

1177 Middle Rd, Menlo Park

March 31, 2016

Tree Survey

Tree#	Species	DBH	Ht/Sp	Con Rating	Comments
1	Coastal redwood <i>Sequoia semprevirens</i>	55.8"	80/30	75	Good health and condition Regulated
2	Coast live oak <i>Quercus agrifolia</i>	17.7"	30/20	75	Good health and condition Regulated
3	Deodar cedar <i>Cedrus deodara</i>	24" est"	50/20	60	Neighbor's tree, approx. 5 feet from fence Regulated
4	Deodar cedar <i>Cedrus deodara</i>	26" est"	50/20	55	Neighbor's tree, approx. 5 feet from fence Regulated
5	Deodar cedar <i>Cedrus deodara</i>	18" est"	50/20	55	Neighbor's tree, approx. 5 feet from fence Regulated
6	Privet <i>Ligustrum lucidum</i>	7.1,6.0, 3.1"	20/10	50	Poor species, good screen Not Regulated
7	Canary Island Date <i>Phoenix canariensis</i>	24.8"	30ft tall	70	Good health and condition Regulated
8	Coast live oak <i>Quercus agrifolia</i>	40" est	50/60	55	Thinning canopy, neighbor's tree Regulated
9	Deodar cedar <i>Cedrus deodara</i>	24.2"	60/25	55	Healthy but poor form Regulated
10	Deodar cedar <i>Cedrus deodara</i>	25.8"	60/25	60	Healthy but one sided canopy Regulated
11	Privet <i>Ligustrum lucidum</i>	6.2,8.2"	20/10	50	Poor species, good screen Not Regulated
12	Canary Island Date <i>Phoenix canariensis</i>	8" and 6"	10ft tall	70	Good health and condition Not Regulated
13	Coastal redwood <i>Sequoia semprevirens</i>	37.1"	60/20	70	Good health and condition Regulated
14	Canary Island Date <i>Phoenix canariensis</i>	24.5"	30ft tall	70	Good health and condition Regulated
15	Canary Island Date <i>Phoenix canariensis</i>	12.5"	18ft tall	70	Good health and condition Not Regulated

Advanced Tree Care

P. O. Box 5326 Redwood City, CA 94063

1177 Middle Rd, Menlo Park

March 31, 2016

Tree#	Species	DBH	Ht/Sp	Con Rating	Comments
16	Canary Island Date <i>Phoenix canariensis</i>	8.5"	14ft tall	70	Good health and condition Not Regulated
17	Coastal redwood <i>Sequoia semoervirens</i>	7.1"/7.2"	25/10	50	Good health but poor form Not regulated
18	Coast live oak <i>Quercus agrifolia</i>	12.4	25/10	50	Suppressed and leaning over neighbor's property. <i>Remove.</i> Regulated
19	Coastal redwood <i>Sequoia semprevirens</i>	37.9"	70/20	70	Good health and condition Regulated
20	Coastal redwood <i>Sequoia semprevirens</i>	27.8"	70/20	70	Good health and condition Regulated
21	Coastal redwood <i>Sequoia semprevirens</i>	34.7"	70/20	70	Good health and condition Regulated
22	Coastal redwood <i>Sequoia semprevirens</i>	32.8"	75/20	60	Good health, co-dominant at 50' Regulated
23	Coastal redwood <i>Sequoia semprevirens</i>	38.3"	75/20	60	Good health, co-dominant at 60' Regulated
24	Pittosporum <i>Pittosporum tenuifolium</i>	8.2"	12/8	50	Good health and condition. Screen Not Regulated
25	Coast live oak <i>Quercus agrifolia</i>	40" est	40/40	70	Neighbor's tree, good health and condition. Canopy requires maintenance. Regulated
26	Chinese pistache <i>Pistache chinensis</i>	8.0"	20/10	70	Good health and condition. Street tree Regulated
27	Coast live oak <i>Quercus agrifolia</i>	8.0"	12/10	50	Good health, poor form, volunteer Not regulated
28	Coast live oak <i>Quercus agrifolia</i>	4.2"/ 2.0"	12/10	50	Good health, poor form, volunteer Not regulated

Summary:

The trees on site are a mixture of natives and non natives; regulated and not regulated, neighbor's trees and street trees.

Tree #s 3, 4, 5, 8 and 25 are neighbor's trees and should be protected during construction.

Tree #25 has a thick heavy canopy that is overdue for maintenance.

Tree # 26 is a planted **street tree** and should be protected during construction

Tree #s 1, 2, 9, 10, 13, 19, 20, 21, 22, 23 are **regulated trees** on this property and should be protected.

Tree #s 6, 11, 12, 15, 16, 17, 24, 27, and 28 are not regulated and can be removed if desired.

Tree # 18 is a **regulated tree** with a significant lean over the neighbor's property, this tree should be removed.

Tree #s 7 and 14 are Canary Island Date palms. Tree #s 7 and 14 are well established trees but do not blend with the existing landscape of redwoods and oaks. Canary Island palms also can be hazardous from dropping large fronds at the base of which are sharp thorns. I would recommend removal of both of these trees.

Tree #s 19 through 23 are a row of well-established large coastal redwoods. They are mostly in good health and condition and have been well maintained. All 5 trees are still quite young and have a lot more growing ahead. The trees are spaced at 10 feet intervals and their canopies are now touching canopies. For the better health of these trees I would consider removing 2 of them at this opportunity to allow for more space and available nutrients for the 3 remaining trees. Consequently, I would recommend removal of Tree #s 20 and 22.

Tree Protection Plan

1. The Tree Protection Zone (TPZ) should be defined with protective fencing. This should be cyclone or chain link fencing on 1½" or 2" posts driven at least 2 feet in to the ground standing at least 6 feet tall. The TPZ should be defined by the **dripline** of the tree, this may not be practical in some cases and so the TPZ's are as follows:

Tree No. 26: TPZ should be at a radius of 6 feet from the trunk of the tree closing on the fence line in accordance with Type I Tree Protection as outlined and illustrated in image 2.15-1 and 2 ⁽⁶⁾

Tree #s 2 and 5: TPZs should be at a radius of 10 feet from the trunks closing on the fence line in accordance with Type I Tree Protection as outlined and illustrated in image 2.15-1 and 2 ⁽⁶⁾ . The TPZ for Tree # 2 can be reduce to the edge of the new driveway if necessary. Please see Notes 3 and 4.

Tree Nos. 9 and 10: TPZ should be at a radius of 12 feet from the trunk of the tree closing on the fence line in accordance with Type I Tree Protection as outlined and illustrated in image 2.15-1 and 2 ⁽⁶⁾

Tree Nos. 3 and 4: TPZ should be at a radius of 15 feet from the trunk of the tree closing on the fence line in accordance with Type I Tree Protection as outlined and illustrated in image 2.15-1 and 2 ⁽⁶⁾

Tree Nos. 8, 13, 19, 20, 21, 22, 23 and 25: TPZ should be at a radius of 20 feet from the trunk of the tree closing on the fence line in accordance with Type I Tree Protection as outlined and illustrated in image 2.15-1 and 2 ⁽⁶⁾ See Note 5 with regards to Tree # 25.

Tree No. 1: TPZ should be at a radius of 25 feet from the trunk of the tree in accordance with Type I Tree Protection as outlined and illustrated in image 2.15-1 and 2 ⁽⁶⁾. The TPZ can be reduced to no less than 15 feet to accommodate the driveway construction. I would also recommend that a root barrier be installed along the edge of the new driveway within the TPZ to prevent future root disturbance of the driveway. It is essential that this tree receives supplemental irrigation during construction and that a 4 inch layer of wood chip is placed within the TPZ. See Notes 2 and 6.



IMAGE 2.15-1

Tree Protection Fence at the Dripline

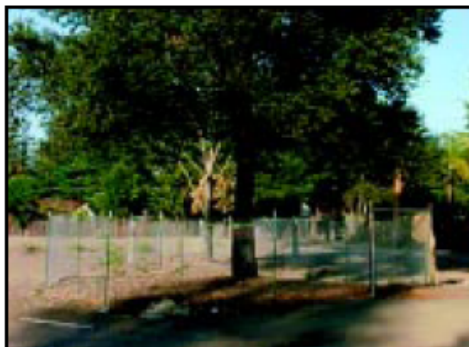


IMAGE 2.15-2

Tree Protection Fence at the Dripline

• Type I Tree Protection

The fences shall enclose the entire area under the **canopy dripline or TPZ** of the tree(s) to be saved throughout the life of the project, or until final improvement work within the area is required, typically near the end of the project (*see Images 2.15-1 and 2.15-2*). Parking Areas: If the fencing must be located on paving or sidewalk that will not be demolished, the posts may be supported by an appropriate grade level concrete base.

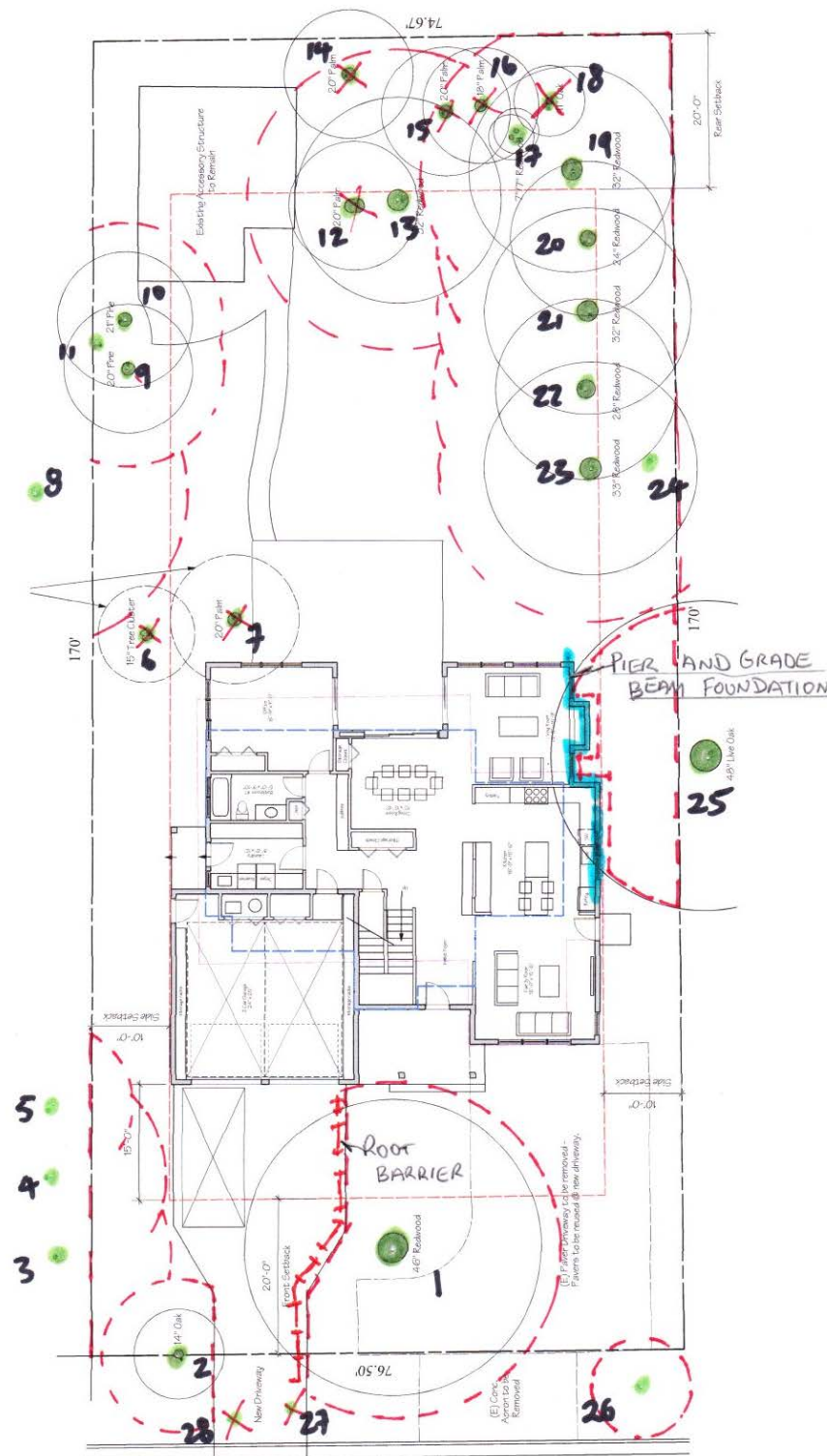
2. A 4 inch layer of mulch should be added to the surface of the soil within the TPZ.
3. Any excavation in ground where there is potential to damage roots of 1" or more in diameter should be carefully hand dug or with an air spade. Where possible, roots should be dug around rather than cut.
4. If working with machinery in the TPZ is unavoidable, the root zone should be protected with 1 inch plywood laid on 4 inches of wood chip
5. The foundation for the new house within the TPZ of Tree # 25 should be a pier and grade beam design to minimize the impact on the root system of Tree # 25. The first 2 feet of each pier should be hand dug to avoid damage to the roots. If a root greater than 2" in diameter is encountered, the pier should be moved and the root not cut.
6. Normal irrigation should be maintained at all times. Supplemental irrigation or deep watering may be necessary if root zones are impacted.
7. If roots are broken, every effort should be made to remove the damaged area and cut it back to its closest lateral root. A clean cut should be made with a saw or pruners. This will prevent any infection from damaged roots spreading throughout the root system and into the tree.⁽²⁾
8. Any pruning and maintenance of the tree shall be carried out before construction begins. This should allow for any clearance requirements for both the new structure and any construction machinery. This will eliminate the possibility of damage during construction. **The pruning should be carried out by an arborist, not by construction personnel.** No limbs greater than 4" in diameter shall be removed.
9. **Do Not:** ⁽⁴⁾
 - a. Allow run off or spillage of damaging materials into the area below any tree canopy.
 - b. Store materials, stockpile soil, park or drive vehicles within the TPZ of the tree.
 - c. Cut, break, skin or bruise roots, branches or trunk without first obtaining permission from the city arborist.
 - d. Allow fires under any adjacent trees.
 - e. Discharge exhaust into foliage.
 - f. Secure cable, chain or rope to trees or shrubs.
 - g. Apply soil sterilants under pavement near existing trees.

10. Where roots are exposed, they should be kept covered with the native soil or four layers of wetted, untreated burlap. Roots will dry out and die if left exposed to the air for too long.⁽⁴⁾
11. Route pipes into alternate locations to avoid conflict with roots.⁽⁴⁾
12. Where it is not possible to reroute pipes or trenches, the contractor is to bore beneath the dripline of the tree. The boring shall take place no less than 3 feet below the surface of the soil in order to avoid encountering “feeder” roots.⁽⁴⁾
13. Compaction of the soil within the dripline shall be kept to a minimum.⁽²⁾
14. Any damage due to construction activities shall be reported to the project arborist or city arborist within 6 hours so that remedial action can be taken. .⁽⁴⁾
15. Ensure upon completion of the project that the original ground level is restored.

P. O. Box 5326 Redwood City, CA 94063

1177 Middle Rd, Menlo Park

March 31, 2016



Location of trees, modified foundation, root barrier and TPZs

Glossary

- Canopy** The part of the crown composed of leaves and small twigs.⁽²⁾
- Dripline** The width of the crown as measured by the lateral extent of the foliage.⁽¹⁾
- Root crown** The point at which the trunk flares out at the base of the tree to become the root system.
- Species** A Classification that identifies a particular plant.

References

- (1) Matheny, N.P., and Clark, J.P. Evaluation of Hazard Trees in Urban Areas. International Society of Arboriculture, 1994.
- (2) Harris, R.W., Matheny, N.P. and Clark, J.R.. Arboriculture: Integrated Management of Landscape Trees, Shrubs and Vines. Prentice Hall, 1999.
- (3) Carlson, Russell E. Paulownia on The Green: An Assessment of Tree Health and Structural Condition. Tree Tech Consulting, 1998.
- (4) Extracted from a copy of Tree Protection guidelines. Anon
- (5) T. D. Sydnor, Arboricultural Glossary. School of Natural Resources, 2000
- (6) D Dockter, Tree Technical Manual. City of Palo Alto, June, 2001

Certification of Performance⁽³⁾

I, Robert Weatherill certify:

- * That I have personally inspected the tree(s) and/or the property referred to in this report, and have stated my findings accurately. The extent of the evaluation and appraisal is stated in the attached report and the Terms and Conditions;
- * That I have no current or prospective interest in the vegetation or the property that is the subject of this report, and I have no personal interest or bias with respect to the parties involved;
- * That the analysis, opinions and conclusions stated herein are my own, and are based on current scientific procedures and facts;
- * That my compensation is not contingent upon the reporting of a predetermined conclusion that favors the cause of the client or any other party, nor upon the results of the assessment, the attainment of stipulated results, or the occurrence of any subsequent events;
- * That my analysis, opinions, and conclusions were developed and this report has been prepared according to commonly accepted Arboricultural practices;
- * That no one provided significant professional assistance to the consultant, except as indicated within the report.

I further certify that I am a member of the International Society of Arboriculture and a Certified Arborist. I have been involved in the practice of arboriculture and the care and study of trees for over 15 years.

Signed



Date: 3/31/16

Terms and Conditions(3)

The following terms and conditions apply to all oral and written reports and correspondence pertaining to consultations, inspections and activities of Advanced Tree Care :

1. All property lines and ownership of property, trees, and landscape plants and fixtures are assumed to be accurate and reliable as presented and described to the consultant, either verbally or in writing. The consultant assumes no responsibility for verification of ownership or locations of property lines, or for results of any actions or recommendations based on inaccurate information.
2. It is assumed that any property referred to in any report or in conjunction with any services performed by Advanced Tree Care, is not in violation of any applicable codes, ordinances, statutes, or other governmental regulations, and that any titles and ownership to any property are assumed to be good and marketable. Any existing liens and encumbrances have been disregarded.
3. All reports and other correspondence are confidential, and are the property of Advanced Tree Care and it's named clients and their assignees or agents. Possession of this report or a copy thereof does not imply any right of publication or use for any purpose, without the express permission of the consultant and the client to whom the report was issued. Loss, removal or alteration of any part of a report invalidates the entire appraisal/evaluation.
4. The scope of any report or other correspondence is limited to the trees and conditions specifically mentioned in those reports and correspondence. Advanced Tree Care and the consultant assume no liability for the failure of trees or parts of trees, either inspected or otherwise. The consultant assumes no responsibility to report on the condition of any tree or landscape feature not specifically requested by the named client.
5. All inspections are limited to visual examination of accessible parts, without dissection, excavation, probing, boring or other invasive procedures, unless otherwise noted in the report. No warrantee or guarantee is made, expressed or implied, that problems or deficiencies of the plants or the property will not occur in the future, from any cause. The consultant shall not be responsible for damages caused by any tree defects, and assumes no responsibility for the correction of defects or tree related problems.
6. The consultant shall not be required to provide further documentation, give testimony, be deposed, or attend court by reason of this appraisal/report unless subsequent contractual arrangements are made, including payment of additional fees for such services as described by the consultant or in the fee schedules or contract.
7. Advanced Tree Care has no warrantee, either expressed or implied, as to the suitability of the information contained in the reports for any purpose. It remains the responsibility of the client to determine applicability to his/her particular case.
8. Any report and the values, observations, and recommendations expressed therein represent the professional opinion of the consultants, and the fee for services is in no manner contingent upon the reporting of a specified value nor upon any particular finding to be reported.
9. Any photographs, diagrams, graphs, sketches, or other graphic material included in any report, being intended solely as visual aids, are not necessarily to scale and should not be construed as engineering reports or surveys, unless otherwise noted in the report. Any reproductions of graphs material or the work product of any other persons is intended solely for the purpose of clarification and ease of reference. Inclusion of said information does not constitute a representation by Advanced Tree Care or the consultant as to the sufficiency or accuracy of that information.

Morris, Michele T

From: Michael Lambert <mlambert498@sbcglobal.net>
Sent: Tuesday, July 19, 2016 3:34 PM
To: Morris, Michele T
Subject: Brendan and Carmen Visser / 1177 Middle Avenue

I am the Visser's adjacent neighbor to the west at 498 San Mateo Drive. They have been good enough to show me there plans for their new home and we do not have any problem with what they are proposing. Their yard has an abundance of heritage trees and the trees that they wish to remove is a reasonable request. To give you a little bit of history, the line of trees along the fence between their lot and 440 San Mateo Drive were planted by a past property owner of 1177 to shield the view from the second floor of the 440 house into the 1177 backyard prior to 1950. How do I know... I have lived at 498 off and on since 1946.

I do have one concern. There is a heritage oak on my property adjacent to the fence line between 498 and 1177, and its root structure obviously extends into 1177 property. Is it possible to add a condition of approval to their application that would protect that root structure from damage by excavation and subsequent construction, perhaps a barricade of some sort during the construction period? I would think that this barricade might be the width of the building sideyard setback, or as recommended by the city arborist.

Thank you,

Michael Lambert



STAFF REPORT

Planning Commission

Meeting Date:

7/25/2016

Staff Report Number:

16-061-PC

Public Hearing:

Use Permit/City of Menlo Park/333 Burgess Drive

Recommendation

Staff recommends that the Planning Commission approve a use permit for the installation and operation of a diesel emergency generator, associated with the Emergency Water Supply Well No. 1 Project at 333 Burgess Drive. The recommended actions are included as Attachment A.

Policy Issues

Each use permit request is considered individually. The Planning Commission should consider whether the required use permit findings can be made for the proposal.

Background

Site location

The project site is the City Corporation Yard (or "Corp Yard") located at 333 Burgess Drive. A location map is included as Attachment B. The Corp Yard is the home of the Public Works Department's Maintenance Division, including vehicles, equipment, and materials associated with the Division's operations. The surrounding zoning and land uses are summarized below.

	Zoning	Existing & Proposed Land Use
Project Site	Public Facilities (P-F)	City Corp Yard
North	Administrative and Professional, Restrictive, Conditional Development (C-1(X))	Research/development office complex
East	Garden Apartment Residential District (R-3-A)	Multi-family apartments
South	Administrative and Professional, Restrictive (C-1)	Public agency office and facilities yard
West	Apartment District, Conditional Development (R-3(X))	Small lot single family residences

History of the project

Emergency Water Supply Well No. 1 is the result of the Emergency Water Supply Program approved by the City Council in November 2011. Following the City Council's action, the Public Works Department identified several high priority sites and initiated the process of siting and designing the emergency well at the Corp Yard. This emergency water supply well and the emergency generator is the first of several similar facilities that are planned to be developed over the next decade.

The components of the Emergency Water Supply Well No. 1 project involve a series of changes to the City

of Menlo Park Corp Yard complex, including the following:

- The installation of a potable water supply well (including a below-ground electric pump, a 5,000-gallon hydro-pneumatic pressure tank with disinfection equipment, a water connection to an existing water main in Burgess Drive, and new fire hydrant);
- The installation of an emergency back-up generator with 500-gallon diesel fuel tank (including associated transformer and electrical conduit);
- New exterior fencing facing Burgess Drive;
- The replacement of the existing landscaping with drought tolerant plantings (including the removal of four heritage trees – one red oak, one tree of heaven, and two Hollywood junipers); and
- Modifications to the existing access drives to improve access into and out of main Corp Yard for some of the larger vehicles.

The City Council approved all of the project elements, except the use permit for the diesel emergency generator, on June 7, 2016. The staff report for this Council meeting is available for reference as Attachment C, and includes information on the overall project.

Analysis

Project description

The proposed use permit is for the installation and operation of an emergency generator at the City Corp Yard to provide a reliable power source for a planned emergency water supply well and Corp Yard-based public works operations. The proposed 250 kilowatt (kW) generator would include a 500-gallon above-ground fuel storage tank for diesel fuel. Diesel fuel at this volume is considered a hazardous material, which can be permitted with emergency generator operations in most zoning districts, including the PF district, with Planning Commission use permit approval. The project plans are included as Attachment D, and the Hazardous Material Information Form (HMIF), which provides more information about the diesel fuel and safety precautions, is included as Attachment E.

The proposed generator would incorporate Level II noise reduction measures that reduce noise levels at 23 feet (7 meters) from 90 dBA (decibel A-weighting) to 71 dBA. The emergency generator would be located near the middle of the Corp Yard property to minimize the potential for impacts to the adjacent residents. The proposed location for the generator is approximately 160 feet to the closest residential unit (located across Burgess Drive). The location of the emergency generator would also be about 200 feet from the proposed well location.

The generator would be used as a back-up power source in case of an outage, and would be tested monthly as part of routine maintenance testing during normal week day business hours. In addition, the emergency generator would require a permit from the Bay Area Air Quality Management Board (BAAQMD).

Agency review

The Menlo Park Fire Protection District, West Bay Sanitary District, San Mateo County Environmental Health Services Division and the City of Menlo Park Building Division were contacted regarding the proposed use and storage of diesel fuel on the project site. Each entity found the proposal to be in compliance with applicable standards. Their correspondence has been included as Attachment F.

Associated site modifications

Fencing

The project also involves the replacement of the existing fences in two locations. The first is at the north driveway between Burgess Drive and the Corp Yard offices building. In this area, the existing gate and fencing would be moved out approximately 20 feet to replace the large vehicle parking spots that will be displaced by the well and 5,000 gallon pressure tank. The second, near the southern/eastern entrance, would involve the extension of the fencing approximately ten feet to enclose the proposed emergency generator. All new/revised fencing would include slats to visually screen the site.

Trees and Landscaping

There are currently 23 protected trees on the Corp Yard site. The proposed emergency back-up water supply well project includes the removal of four protected trees, including two Hollywood junipers, due to the proposed construction. None of the protected trees that were approved for removal were being affected by the siting of the proposed emergency generator. Tree protection measures have been established for all remaining heritage trees. Several new trees and shrubs would be planted in the vicinity of the emergency generator, and would help provide additional visual screening.

Correspondence

No correspondence was received on the proposed diesel emergency generator.

Conclusion

Staff recommends that the Planning Commission approve the application for a Use Permit for the following reasons:

- The location of the proposed emergency generator is near the middle of the site, equidistant from the adjacent residences located east and west of the Corp Yard;
- The proposed emergency generator incorporates Level II noise reduction measures, the highest level of noise reduction available for the proposed 250kW generator;
- The proposed use and quantities of hazardous materials (diesel fuel) are compatible and consistent with other uses in this area;
- The HMIF contains a chemical inventory includes a discussion of the Hazardous Materials Business Plan (HMBP) which includes the applicant's training plan and protection measures in the event of an emergency; and
- The other agencies with authority over the emergency generator have no concerns with the generator or the proposed location and have indicated their approval of the proposed hazardous materials use on the property.

Impact on City Resources

The overall proposal is a City project, and is covered through funding discussed in more detail in Attachment C.

Environmental Review

The Corp Yard Emergency Back-Up Water Supply No. 1 Project is subject to the requirements of the California Environmental Quality Act (CEQA). As a result, an Initial Study was prepared for the entire project (the water supply well, changes to the Corp Yard facility, and the diesel emergency generator). According to

the analysis in the Initial Study, the project would result in the following potentially significant impacts related to biological resources, cultural resources, hazards and hazardous materials, hydrology and water quality, and construction noise. These impacts are expected to be mitigated to a less-than-significant level through implementation of mitigation measures identified in the Initial Study.

The Initial Study and Notice of Intent to Adopt a Negative Declaration were sent to the State Clearinghouse (SCH #2016042078) and circulated for public review and comment period from April 28, 2016 to May 31, 2016. No comments were received. The members of the Planning Commission received a copy of the Notice of Availability at the beginning of the public review and comment period. The City Council adopted the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program for the entire Emergency Water Supply Well No. 1 Project on June 7, 2016.

As a result, staff is recommending that the Planning Commission make a determination that the proposed use permit is consistent with the project evaluated in the adopted Mitigated Negative Declaration.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 1,320-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

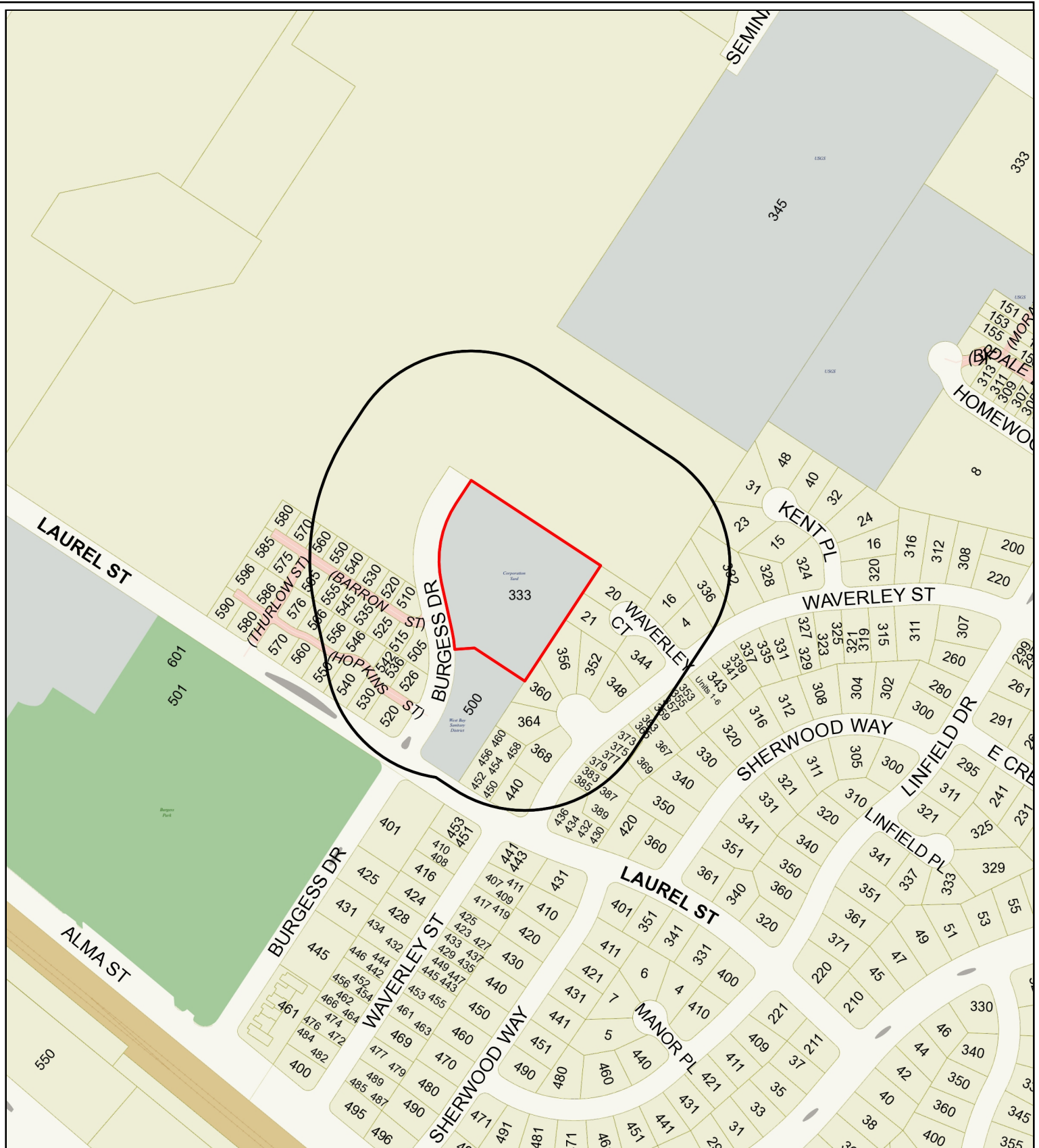
- A. Recommended Actions
- B. Location Map
- C. Hyperlink: City Council Staff Report, June 7, 2016:
<http://www.menlopark.org/DocumentCenter/View/10394>
- D. Project Plans (dated April 2016)
- E. Hazardous Material Information Form
- F. Hazardous Materials Agencies Referral Response Forms

Report prepared by:
David Hogan, Contract Planner

Report reviewed by:
Thomas Rogers, Principal Planner

333 Burgess Drive – Attachment A: Recommended Actions

LOCATION: 333 Burgess Drive	PROJECT NUMBER: PLN2015-00060	APPLICANT: City of Menlo Park	OWNER: City of Menlo Park
REQUEST: Request for a use permit to allow the use of hazardous materials (diesel fuel) for an emergency generator at the City Corp yard located in the P-F (Public Facility) zoning district, associated with the installation and operation of an emergency water supply well.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: <ol style="list-style-type: none"> 1. Make a finding that the project is consistent with the Mitigated Negative Declaration for the Emergency Water Supply Well No. 1 Project that was adopted by the City Council on June 7, 2016. 2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City. 3. Approve the use permit subject to the following standard conditions: <ol style="list-style-type: none"> a. Development of the project shall be substantially in conformance with the plans prepared by Infrastructure Engineering Corporation, dated April 11, 2016, and approved by the Planning Commission on July 25, 2016, except as modified by the conditions contained herein, subject to review and approval of the Planning Division. b. Prior to building permit issuance, the applicants shall comply with all West Bay Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project. c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project. d. If there is an increase in the quantity of hazardous materials on the project site, a change in the location of the storage of the hazardous materials, or the use of additional hazardous materials after this use permit is granted, the applicant shall apply for a revision to the use permit. e. Any citation or notification of violation by the Menlo Park Fire Protection District, San Mateo County Environmental Health Department, West Bay Sanitary District, Menlo Park Building Division or other agency having responsibility to assure public health and safety for the use of hazardous materials will be grounds for considering revocation of the use permit. 4. Approve the use permit subject to the following project-specific condition: <ol style="list-style-type: none"> a. The periodic monthly testing of the emergency generator shall be performed only during normal working hours, Monday through Friday between the hours of 9:00 am and 5:00 pm. 			



City of Menlo Park
Location Map
333 Burgess Drive



Scale: 1:3,600

Drawn By: TAS

Checked By: THR

Date: 7/25/2016

Sheet: 1



APRIL 2016



The map shows a section of San Francisco with the following streets and landmarks:

- Streets:**
 - Top: MONTGOMERY ST, 4TH ST, 5TH ST, 6TH ST, 7TH ST, 8TH ST, 9TH ST, 10TH ST, 11TH ST, 12TH ST, 13TH ST, 14TH ST, 15TH ST, 16TH ST, 17TH ST, 18TH ST, 19TH ST, 20TH ST, 21ST, 22ND, 23RD, 24TH, 25TH, 26TH, 27TH, 28TH, 29TH, 30TH, 31ST, 32ND, 33RD, 34TH, 35TH, 36TH, 37TH, 38TH, 39TH, 40TH, 41ST, 42ND, 43RD, 44TH, 45TH, 46TH, 47TH, 48TH, 49TH, 50TH, 51ST, 52ND, 53RD, 54TH, 55TH, 56TH, 57TH, 58TH, 59TH, 60TH, 61ST, 62ND, 63RD, 64TH, 65TH, 66TH, 67TH, 68TH, 69TH, 70TH, 71ST, 72ND, 73RD, 74TH, 75TH, 76TH, 77TH, 78TH, 79TH, 80TH, 81ST, 82ND, 83RD, 84TH, 85TH, 86TH, 87TH, 88TH, 89TH, 90TH, 91ST, 92ND, 93RD, 94TH, 95TH, 96TH, 97TH, 98TH, 99TH, 100TH.
 - Left: 1ST ST, 2ND ST, 3RD ST, 4TH ST, 5TH ST, 6TH ST, 7TH ST, 8TH ST, 9TH ST, 10TH ST, 11TH ST, 12TH ST, 13TH ST, 14TH ST, 15TH ST, 16TH ST, 17TH ST, 18TH ST, 19TH ST, 20TH ST, 21ST, 22ND, 23RD, 24TH, 25TH, 26TH, 27TH, 28TH, 29TH, 30TH, 31ST, 32ND, 33RD, 34TH, 35TH, 36TH, 37TH, 38TH, 39TH, 40TH, 41ST, 42ND, 43RD, 44TH, 45TH, 46TH, 47TH, 48TH, 49TH, 50TH, 51ST, 52ND, 53RD, 54TH, 55TH, 56TH, 57TH, 58TH, 59TH, 60TH, 61ST, 62ND, 63RD, 64TH, 65TH, 66TH, 67TH, 68TH, 69TH, 70TH, 71ST, 72ND, 73RD, 74TH, 75TH, 76TH, 77TH, 78TH, 79TH, 80TH, 81ST, 82ND, 83RD, 84TH, 85TH, 86TH, 87TH, 88TH, 89TH, 90TH, 91ST, 92ND, 93RD, 94TH, 95TH, 96TH, 97TH, 98TH, 99TH, 100TH.
 - Right: 1ST ST, 2ND ST, 3RD ST, 4TH ST, 5TH ST, 6TH ST, 7TH ST, 8TH ST, 9TH ST, 10TH ST, 11TH ST, 12TH ST, 13TH ST, 14TH ST, 15TH ST, 16TH ST, 17TH ST, 18TH ST, 19TH ST, 20TH ST, 21ST, 22ND, 23RD, 24TH, 25TH, 26TH, 27TH, 28TH, 29TH, 30TH, 31ST, 32ND, 33RD, 34TH, 35TH, 36TH, 37TH, 38TH, 39TH, 40TH, 41ST, 42ND, 43RD, 44TH, 45TH, 46TH, 47TH, 48TH, 49TH, 50TH, 51ST, 52ND, 53RD, 54TH, 55TH, 56TH, 57TH, 58TH, 59TH, 60TH, 61ST, 62ND, 63RD, 64TH, 65TH, 66TH, 67TH, 68TH, 69TH, 70TH, 71ST, 72ND, 73RD, 74TH, 75TH, 76TH, 77TH, 78TH, 79TH, 80TH, 81ST, 82ND, 83RD, 84TH, 85TH, 86TH, 87TH, 88TH, 89TH, 90TH, 91ST, 92ND, 93RD, 94TH, 95TH, 96TH, 97TH, 98TH, 99TH, 100TH.
- Landmarks:**
 - Golden Gate Park (indicated by a hatched area on the left).
 - San Francisco City Hall (indicated by a building icon at the top center).
 - San Francisco City Corporation Yard (indicated by a building icon at the top center, labeled "PROJECT SITE CITY CORPORATION YARD").
 - San Francisco City Hall (indicated by a building icon at the top center).
 - San Francisco City Hall (indicated by a building icon at the top center).
- Other Features:**
 - A north arrow pointing towards the top right, labeled "NTS".
 - A scale bar indicating distances in feet (0, 100, 200, 300, 400, 500, 600, 700, 800, 900, 1000).
 - A legend indicating that the hatched area represents "Golden Gate Park" and the building icon represents "San Francisco City Hall".

LOCATION MAP
NTS

I HEREBY DECLARE THAT THE DESIGN OF THE IMPROVEMENTS AS SHOWN ON THESE PLANS COMPLIES WITH PROFESSIONAL ENGINEERING STANDARDS AND PRACTICES. AS THE ENGINEER IN RESPONSIBLE CHARGE OF THE DESIGN OF THESE IMPROVEMENTS, I ASSUME FULL RESPONSIBLE CHARGE FOR SUCH DESIGN. I UNDERSTAND AND ACKNOWLEDGE THAT THE PLAN CHECK OF THESE PLANS BY THE CITY OF MENLO PARK IS A REVIEW FOR THE LIMITED PURPOSE OF ENSURING THE PLANS COMPLY WITH CITY PROCEDURES AND OTHER APPLICABLE POLICIES AND ORDINANCES. THE PLAN CHECK IS NOT A DETERMINATION OF THE TECHNICAL ADEQUACY OF THE DESIGN OF THE IMPROVEMENTS. SUCH PLAN CHECK DOES NOT, THEREFORE, RELIEVE ME OF MY RESPONSIBILITY FOR THE DESIGN OF THESE IMPROVEMENTS.

ENGINEER _____ C045927 _____ DATE _____
 LICENSE NO. _____

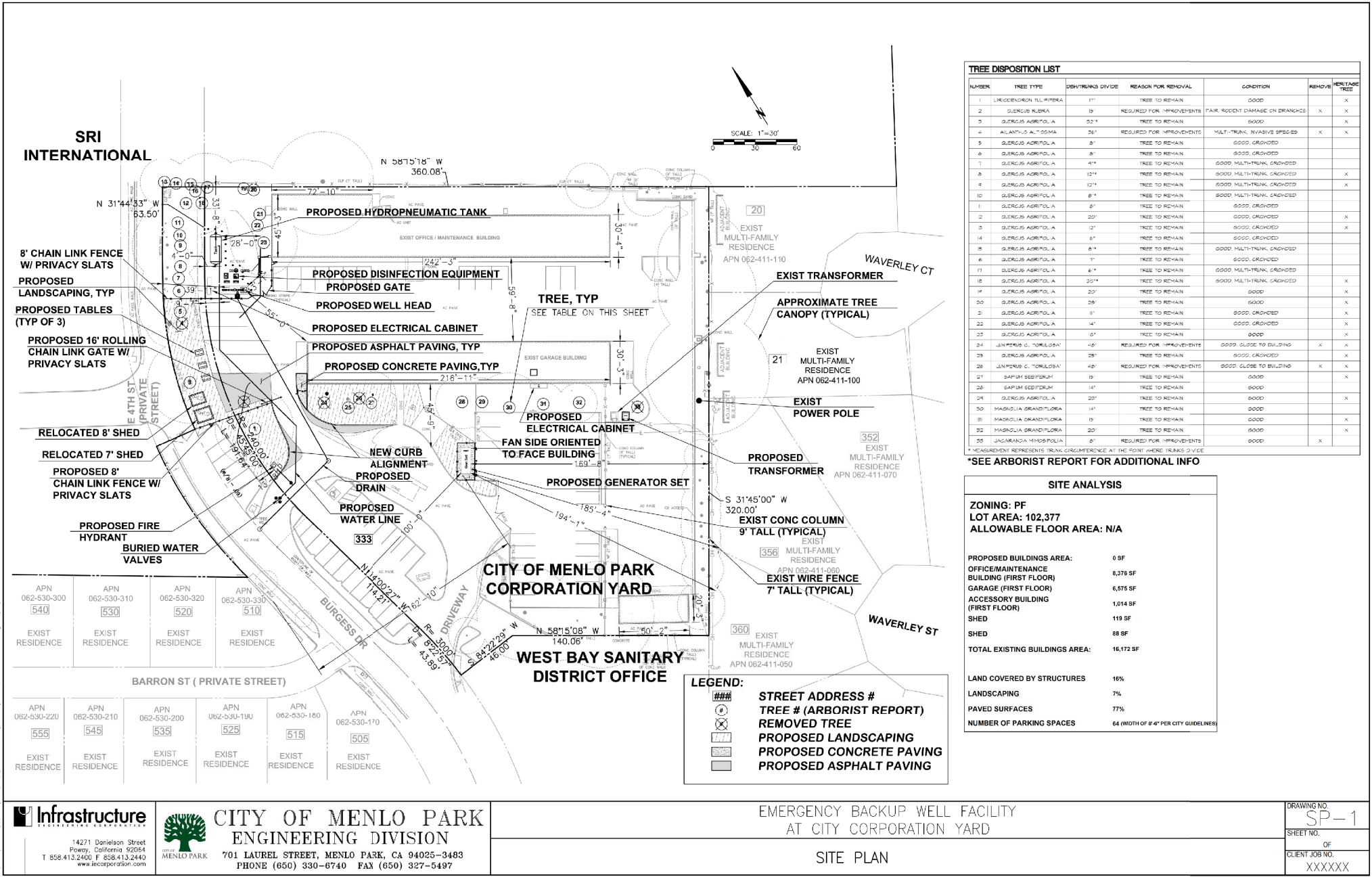


Infrastructure
ENGINEERING CORPORATION

PREPARED UNDER THE
DIRECT SUPERVISION
OF:



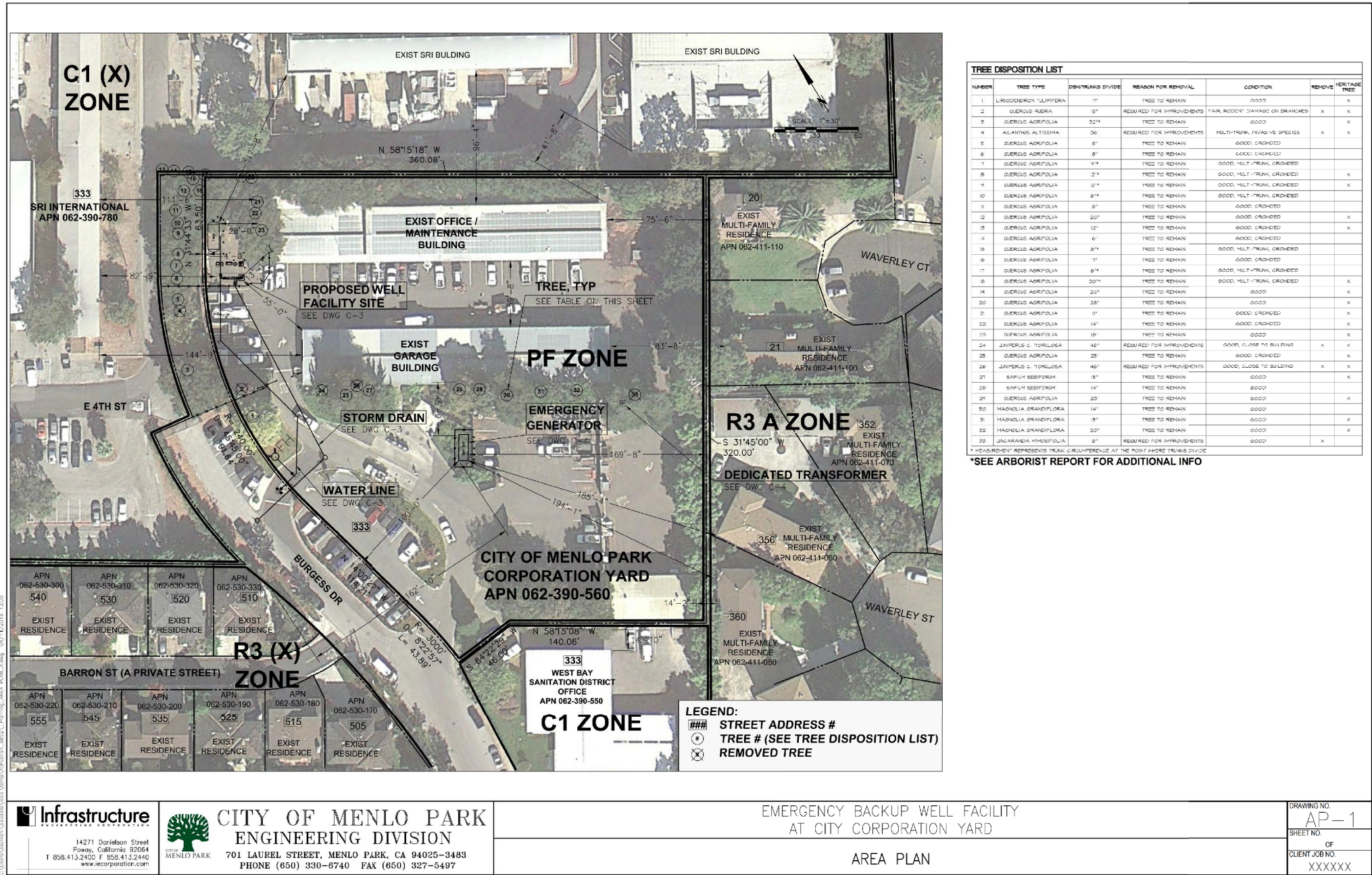
DRAWING NO.	G-1	
SHEET NO.		OF
CLIENT JOB NO.	XXXXXX	



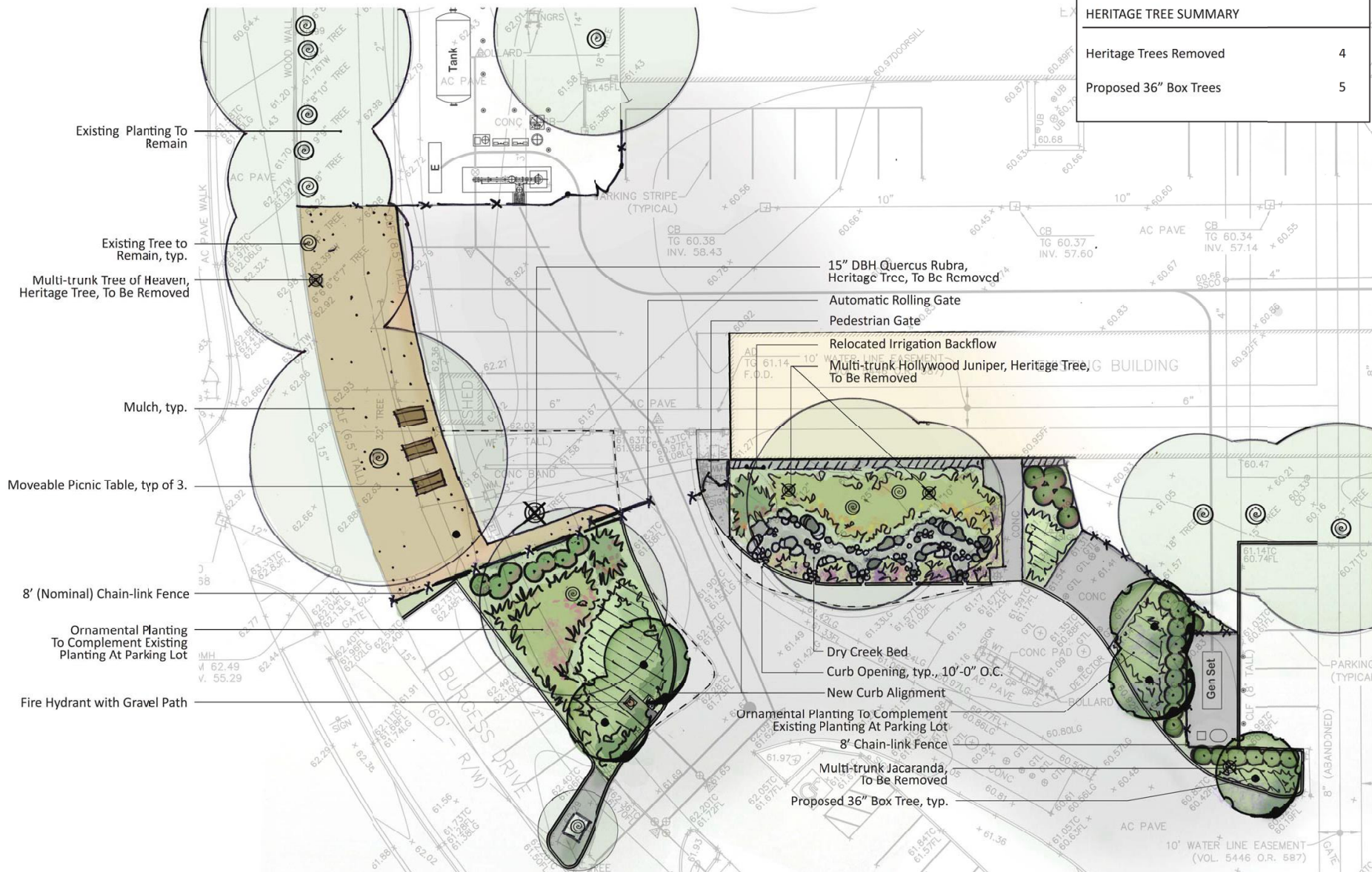
Infrastructure
14271 Danielson Street
Poway, California 92064
T 858.413.2400 F 858.413.2440
www.icorporation.com

CITY OF MENLO PARK
ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

EMERGENCY BACKUP WELL FACILITY AT CITY CORPORATION YARD		DRAWING NO. SP-1
SITE PLAN		SHEET NO.
		OF
		CLIENT JOB NO. XXXXXX



HERITAGE TREE SUMMARY	
Heritage Trees Removed	4
Proposed 36" Box Trees	5

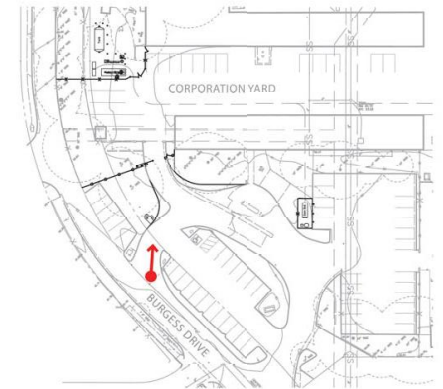


LANDSCAPE CONCEPT PLAN EMERGENCY BACKUP WELL FACILITY Menlo Park, California



13009 Concept Plans and Image Boards 24x36 (2016 04-07).indd

BEFORE

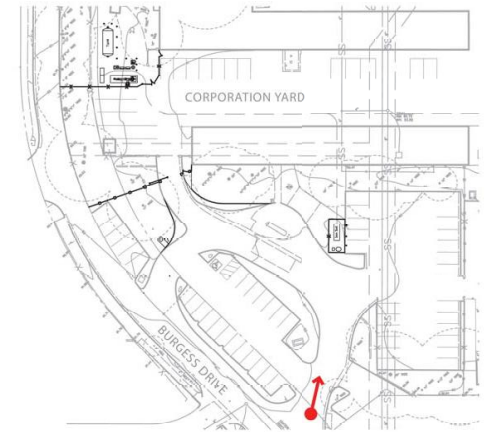


KEY MAP

AFTER

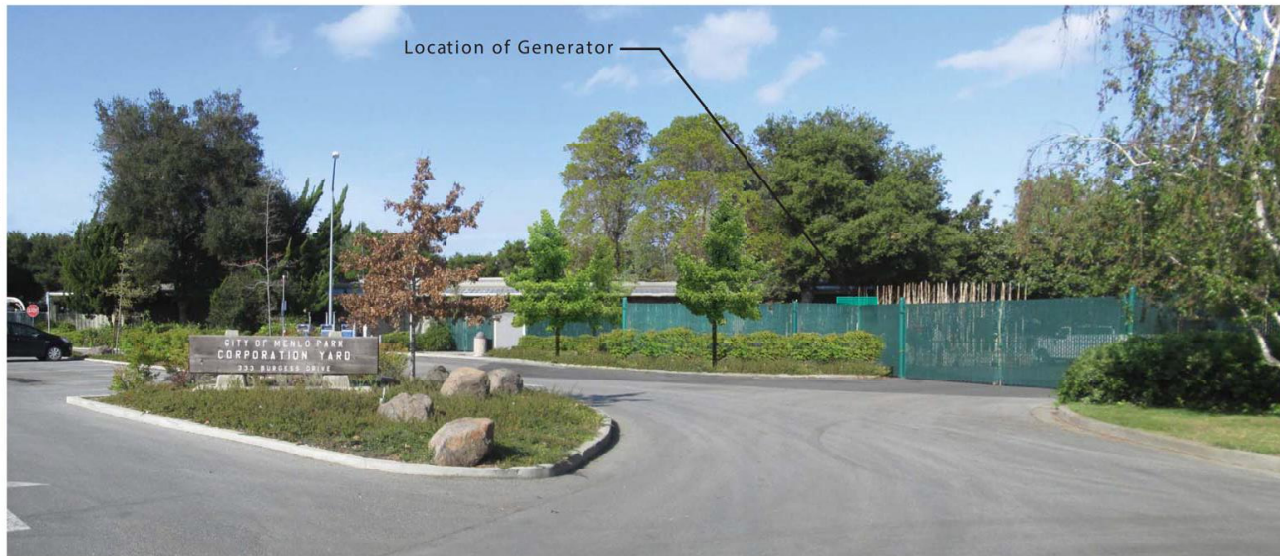


BEFORE



KEY MAP

AFTER



ROLLING GATE

Automatic Rolling Gate With Green Slats



8' CHAIN LINK FENCE

Chain Link Fence With Green Slats



(I) Green slats selected to match existing fence on eastern side of site

MOVEABLE PICNIC TABLE

Picnic Table available from Timberform, Model # 2076-6, Recycled Plastic Slats



DRY CREEK BED



Sound-attenuated and weather-protective enclosures

> For generator sets from 10 to 1000 kW

Our energy working for you™



> Diesel generator set enclosures

Weather-protective Level I, Level II, Level III

> Spark-ignited generator set enclosures

Weather-protective Level I, Level II

Sound-attenuated and weather-protective enclosures from Cummins Power Generation Inc. meet even the strictest sound requirements and provide optimum protection from inclement weather.

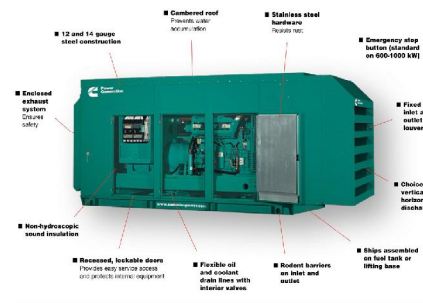
Cummins Power Generation diesel and spark-ignited generator sets are available with sound attenuated and weather-protective enclosures. The standard, pre-engineered and delivered as part of the entire power system, these enclosures are designed to speed installation time and reduce costs.

Choose from three levels of sound-attenuation, depending on market size, to comply with even the strictest noise requirements. Enclosures are constructed of steel or aluminum, which is perforated in coastal regions or other environments where corrosion is a concern.



www.cumminspower.com

Features:



> Three levels of sound attenuation

Level I: 70 to 69 dBA*

Level II: 65 to 75 dBA*

Level III: 58 to 70 dBA*

* All values at 1 meter, 90° azimuth

- > Upgrade kits
 - > Enclosures mounted directly to a sub-base fuel tank or lifting base
 - > UL2590-rated
 - > Customer options available to meet your application needs
- Enclosure options**
- > Aluminum enclosure is wind-rated to 150 mph (per ASCE 7-05 exposure C, category 1 importance factor)
 - > Kits available to up-fit existing generator sets or to upgrade existing enclosures with additional sound attenuation
 - > Interior oil and coolant drains with interior valves for ease of service
 - > Overhead 2-point lifting brackets (some models)

www.cumminspower.com

Choose from weather-protective enclosure or three levels of sound attenuation:

Sound levels (dBA)*				
kW	Model	Weather-protective	Level I	Level II
10	DSGA	70	69	68
15	DSGA	61	60	59
20	DSGA	61	60	59
25	DSGA	62	61	60
30	DSGA	62	61	60
40	DSGA	62	61	60
50	DSGA	62	61	60
60	DSGA	63	62	61
75	DSGA	63	62	61
100	DSGA	63	62	61
150	DSGA	63	62	61
200	DSGA	63	62	61
250	DSGA	63	62	61
300	DSGA	63	62	61
400	DSGA	63	62	61
500	DSGA	63	62	61
600	DSGA	63	62	61
750	DSGA	63	62	61
1000	DSGA	63	62	61

Sound levels (dBA)*				
kW	Model	Weather-protective	Level I	Level II
10	DSGA	70	69	68
15	DSGA	61	60	59
20	DSGA	61	60	59
25	DSGA	62	61	60
30	DSGA	62	61	60
40	DSGA	62	61	60
50	DSGA	62	61	60
60	DSGA	63	62	61
75	DSGA	63	62	61
100	DSGA	63	62	61
150	DSGA	63	62	61
200	DSGA	63	62	61
250	DSGA	63	62	61
300	DSGA	63	62	61
400	DSGA	63	62	61
500	DSGA	63	62	61
600	DSGA	63	62	61
750	DSGA	63	62	61
1000	DSGA	63	62	61

* All values at 1 meter, 90° azimuth

* Also available: Level III

100 kW DSGA, 150 kW DSGA, 200 kW DSGA, 250 kW DSGA, 300 kW DSGA, 400 kW DSGA, 500 kW DSGA, 600 kW DSGA, 750 kW DSGA, 1000 kW DSGA

DSGA 100 kW to 1000 kW generator set (model DSGA)

DSGA 100 kW to 1000 kW generator set (model DSGA)

DSGA 100 kW to 1000 kW generator set (model DSGA)

DSGA 100 kW to 1000 kW generator set (model DSGA)

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DSGA 100 kW to 1000 kW generator set (model DSGA)

DSGA 100 kW to 1000 kW generator set (model DSGA)

DSGA 100 kW to 1000 kW generator set (model DSGA)

Diesel package dimensions (in)				
Model	Weather-protective	Level I	Level II	Level III
10	63	63	63	63
15	63	63	63	63
20	63	63	63	63
25	63	63	63	63
30	63	63	63	63
40	63	63	63	63
50	63	63	63	63
60	63	63	63	63
75	63	63	63	63
100	63	63	63	63
150	63	63	63	63
200	63	63	63	63
250	63	63	63	63
300	63	63	63	63
400	63	63	63	63
500	63	63	63	63
600	63	63	63	63
750	63	63	63	63
1000	63	63	63	63

Spark-ignited package dimensions (in)				
Model	Weather-protective	Level I	Level II	Level III
10	63	63	63	63
15	63	63	63	63
20	63	63	63	63
25	63	63	63	63
30	63	63	63	63
40	63	63	63	63
50	63	63	63	63
60	63	63	63	63
75	63	63	63	63
100	63	63	63	63
150	63	63	63	63
200	63	63	63	63
250	63	63	63	63
300	63	63	63	63
400	63	63	63	63
500	63	63	63	63
600	63	63	63	63
750	63	63	63	63
1000	63	63	63	63

Diesel package dimensions (in)				
Model	Weather-protective	Level I	Level II	Level III
10	63	63	63	63
15	63	63	63	63
20	63	63	63	63
25	63	63	63	63
30	63	63	63	63
40	63	63	63	63
50	63	63	63	63
60	63	63	63	63
75	63	63	63	63
100	63	63	63	63
150	63	63	63	63
200	63	63	63	63
250	63	63	63	63
300	63	63	63	63
400	63	63	63	63
500	63	63	63	63
600	63	63	63	63
750	63	63	63	63
1000	63	63	63	63

Spark-ignited package dimensions (in)				
Model	Weather-protective	Level I	Level II	Level III
10	63	63	63	63
15	63	63	63	63
20	63	63	63	63
25	63	63	63	63
30	63	63	63	63
40	63	63	63	63
50	63	63	63	63
60	63	63	63	63
75	63	63	63	63
100	63	63	63	63
150	63	63	63	63
200	63	63	63	63
250	63	63	63	63
300	63	63	63	63
400	63	63	63	63
500	63	63	63	63
600	63	63	63	63
750	63	63	63	63
1000	63	63	63	63

Cummins Power Generation
1400 Third Avenue N.E.
Atlanta, Georgia 30309
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Latin America
3800 Boulevard 18th Ave., Suite 205
Miami, FL 33157
USA
Phone 1 800 421 5011
Fax 1 800 421 5017

Our energy working for you™
www.cumminspower.com

PROPOSED GENERATOR MODEL

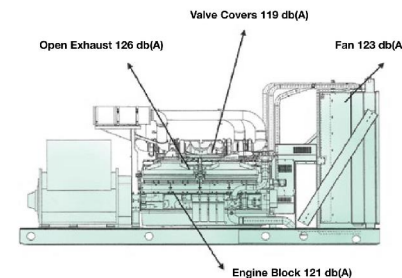


Figure 2: 2000 DQKO noise sources (estimated sound power levels)

Sources of generator set noise

Generator set noise is produced by six major sources (see Figure 2):

- Engine noise – This is mainly caused by mechanical and combustion forces and typically ranges from 100 dB(A) to 121 dB(A), measured at one meter, depending on the size of the engine.
- Cooling fan noise – This results from the sound of air being moved at high speed across the engine and through the radiator. Its level ranges from 100 dB(A) to 105 (A) dB at one meter.
- Alternator noise – This is caused by cooling air and brush friction and ranges from approximately 80 dB(A) to 90 dB(A) at one meter.

- Induction noise – This is caused by fluctuations in current in the alternator windings that give rise to mechanical noise that ranges from 80 dB(A) to 90 dB(A) at one meter.
- Engine exhaust – Without an exhaust silencer, this ranges from 120 dB(A) to 130 dB(A) or more and is usually reduced by a minimum of 15 dB(A) with a standard silencer.
- Structural/mechanical noise – This is caused by mechanical vibration of various structural parts and components that is radiated as sound.



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PHONE (650) 330-8740 FAX (650) 327-5497

EMERGENCY BACKUP WELL FACILITY
AT CITY CORPORATION YARD

EMERGENCY GENERATOR

DRAWING NO.
EMG-1
SHEET NO.
OF
CLIENT JOB NO.
XXXXXX

WORK TO BE DONE

INSTALLATION OF THE CITY CORPORATION YARD WELL FACILITY INCLUDING DEMOLITION WORK;
INSTALLATION OF ELECTRICAL EQUIPMENT; SITE PIPING INCLUDING CONNECTION TO EXISTING AND
PROPOSED FACILITIES; INSTRUMENTATION AND ALL APPURTENANCES AND SPECIALTIES

SURVEY NOTES

BASIS OF BEARINGS

THE BEARING SOUTH 31°45'00" WEST TAKEN ON SOUTHEASTERLY LINE OF LOT 2 AS SHOWN ON THAT
SUBDIVISION MAP FILED IN VOLUME 87 OF MAPS AT PAGE 39, SAN MATEO COUNTY RECORDS, WAS
USED AS THE BASIS OF BEARINGS FOR THIS SURVEY.

BENCHMARK

TEMPORARY BENCHMARK CUT CROSS ON MANHOLE RM
ELEV. = 57.99
(CITY OF MENLO PARK DATUM)
(SEE DRAWING C-1)

EASEMENT NOTE

EASEMENTS SHOWN ARE PER 67 PM 39. OTHER EASEMENTS, IF ANY, ARE NOT INDICATED HEREON.

UTILITY NOTE

THE UTILITIES EXISTING ON THE SURFACE AND SHOWN ON THIS DRAWING HAVE BEEN LOCATED BY FIELD
SURVEY. ALL UNDERGROUND UTILITIES SHOWN ON THIS DRAWING ARE FROM RECORDS OF THE VARIOUS
UTILITY COMPANIES, RECORD DRAWINGS PROVIDED BY THE CITY OF MENLO PARK AND SURFACE UTILITY
MARKINGS. THE SURVEYOR/ENGINEER DOES NOT ASSUME RESPONSIBILITY FOR THEIR COMPLETENESS,
INDICATED LOCATION, OR SIZE. RECORD UTILITY LOCATION SHOULD BE CONFIRMED BY EXPOSING THE
UTILITY.

LEGEND

EXISTING

SYMBOL	DESCRIPTION
FH	FIRE HYDRANT
GA	GUY ANCHOR
JP	JOINT UTILITY POLE
LP	LIGHT POLE
M	MANHOLE
WV	WATER VALVE/GAS VALVE
CLF	CHAIN LINK FENCE
COM	COMMUNICATION LINE
EL	ELECTRIC LINE
GL	GAS LINE
OH	OVERHEAD LINE
SS	SANITARY SEWER LINE
SD	STORM DRAIN LINE
WL	WATER LINE
WT	TREE W/ SIZE
AB	ABANDONED UTILITIES

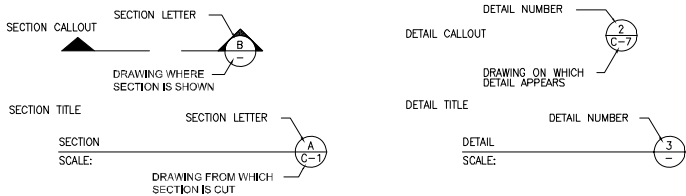
IMPROVEMENT

SYMBOL	DESCRIPTION
HC	HORIZONTAL CONTROL POINT
WD	WATER DRAIN
WE	WATER ELEVATION
FG	FINISHED GRADE (PROFILES)
AB	ABANDON / DEMOLISH / REMOVE
TH	THRUST BLOCK
WV	WATER VALVE, VALVE BOX, AND COVER
1V	1" / 2" COMB AIR/VAC VALVE
2B	2" BLOW OFF
AC	AC PAVING
LA	LANDSCAPED AREA
GR	GRUB, GRUB, AND STRIP AREA
CLF	CHAIN LINK FENCE
COM	COMMUNICATION LINE
EL	ELECTRIC LINE

ABBREVIATIONS

AB	ABANDON	MH	MANHOLE
AC	AC PAVE	MFR	MANUFACTURER
ACP	ASBESTOS CEMENT PIPE	MAX	MAXIMUM
AD	AREA DRAIN	MECH	MECHANICAL
ADDL	ADDITIONAL	MJ	MECHANICAL JOINT
ANG	ANGLE	MG	MILLION GALLON
APPROX	APPROXIMATE	MIN	MINIMUM
AR	ACCESS RAMP	N	NORTH
AV	AIR VALVE	NC	NORMALLY CLOSED
BFP	BACK FLOW PREVENTER	NGRS	NATURAL GAS REFUELING STATION
BTWN	BETWEEN	NTS	NOT TO SCALE
BO	BLOW OFF	NO.	NUMBER
BOT	BOTTOM	OC	ON CENTER
BFV	BUTTERFLY VALVE	OPNG	OPENING
CI	CAST IRON	OD	OUTSIDE DIAMETER
CB	CATCH BASIN	PE	PLAIN END
CTR	CENTER	PT	POINT
CTRD	CENTERED	PVC	POLYVINYL CHLORIDE
C	CENTER LINE	P.S.I.G.	POUNDS PER SQUARE INCH GAUGE
CHL	CHLORINATION	PRESS.	PRESSURE
CL	CLASS, CENTER LINE	PRS	PRESSURE REDUCING STATION
CLF	CHAIN LINK FENCE	PRV	PRESSURE REDUCING VALVE
CLR	CLEAR	P/L	PROPERTY LINE
CO	CLEANOUT	PS	PUMP STATION
CONC	CONCRETE	RAD	RADIUS
CONN	CONNECTION	REIN	REINFORCE
CONT	CONTINUOUS	RCP	REINFORCED CONCRETE PIPE
CMP	CORRUGATED METAL PIPE	REQD	REQUIRED
CPLG	COUPLING	RJ	RESTRAINED JOINT
CTB	CABLE TELEVISION BOX	R.O.W.	RIGHT OF WAY
DIAG	DIAGONAL	RD	ROAD
DIA	DIAMETER	RT	RIGHT
DWG	DRAWING	S	SLOPE
DI	DUCTILE IRON	SD	STORM DRAIN
DIP	DUCTILE IRON PIPE	SDMH	STORM DRAIN MANHOLE
EA	EACH	SCHD, SCH	SCHEDULE
EW	EACH WAY	SF	SQUARE FEET
E	ELECTRIC	SLB	STREET LIGHT BOX
ECC	ECCENTRIC	SPEC	SPECIFICATION
ELEC	ELECTRICAL	SO	SQUARE
EL, ELVE	ELEVATION	SS	STAINLESS STEEL
EXP	EXPANSION	SSCO	SANITARY SEWER CLEANOUT
EXIST	EXISTING	SSMH	SANITARY SEWER MANHOLE
FF	FINISH FLOOR	STD	STANDARD
FG	FINISHED GRADE	STA	STATION
FH	FIRE HYDRANT	STL	STEEL
FS	FINISHED SURFACE	TEL	TELEPHONE OR TELEPHONE VAULT
FLG	FLANGE	TB	THRUST BLOCK
FLEX	FLEXIBLE	TG	TOP OF GRATE
FL	FLOW LINE	THK	THICK
FT	FOOT OR FEET	TC	TOP OF CURB
GA	GAGE	T/CONC	TOP OF CONCRETE ELEVATION
GPM	GALLONS PER MINUTE	T/WALL, TW	TOP OF WALL ELEVATION
GALV	GALVANIZE	TYP	TYPICAL
GM	GAS METER	UB	UTILITY BOX
GP	GAS PUMP	UNK	UNKNOWN
GTL	GAS TANK LID	VERT	VERTICAL
GTT	GAS TANK TERMINAL		
HORIZ	HORIZONTAL	WM	WATER METER
HYD	HYDRANT	WS	WATER SERVICE
IN	INCH OR INCHES	WTR	WATER
ID	INSIDE DIAMETER	WW	WATER VALVE
IE, INV.	INVERT ELEVATION	WWF	WELDED WIRE FABRIC
JP	JOINT UTILITY POLE	WWM	WELDED WIRE MESH
LBS	POUNDS	W/	WITH
LF	LINEAL FEET		
LP	LIGHT POLE		
LT	LEFT		

SECTION AND DETAIL IDENTIFICATION SYSTEM



PRELIMINARY FINAL DESIGN

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY: PRM	 14271 Danielson Street Poway, California 92064 T 858.413.2400 F 858.413.2440 www.icorporation.com	 PREPARED UNDER THE DIRECT SUPERVISION OF: DATE	 CITY OF MENLO PARK 701 LAUREL STREET, MENLO PARK, CA 94025-3483 PHONE (650) 330-6740 FAX (650) 327-5497	EMERGENCY BACKUP WELL FACILITY AT CITY CORPORATION YARD	DRAWING NO. G-2
				DRAWN BY: DHN				SHEET NO.	
				CHECKED BY: AG				CLIENT JOB NO.	
								XXXXXX	

GENERAL NOTES

- CONTRACTOR SHALL CONTACT UNDERGROUND SERVICE ALERT (U.S.A.) NORTH (DIAL 811) AT LEAST ONE WEEK IN ADVANCE OF STARTING EXCAVATION TO PROVIDE FOR MARKING OF UTILITIES. ONLY TWO WEEKS OF WORK WILL BE LOCATED ON EACH REQUEST. THE CONTRACTOR SHALL MARK THE LIMITS OF EACH REQUEST.
 - CONTRACTOR TO NOTIFY ALL UTILITY COMPANIES FOR EXCAVATION ADJACENT TO THE UTILITY AT LEAST 48 HOURS PRIOR TO CONSTRUCTION.
 - CONTRACTOR TO NOTIFY CITY MAINTENANCE DIVISION AT (650) 330-6780 AND ENGINEERING DIVISION AT (650) 330-6740 IF THERE ARE ANY CHANGES TO THE WORK SCHEDULE.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR FIELD STAKING THE PROPOSED FACILITIES IN THE FIELD FOR CONSTRUCTION.
- CONTRACTOR SHALL AT ALL TIMES COMPLY WITH THE RULES AND REGULATIONS ESTABLISHED BY CAL-OSHA AND OTHER AGENCIES HAVING JURISDICTION OVER THE WORK.
- SHUT DOWN OF ANY WATER, GAS, WASTEWATER, ELECTRICAL UTILITIES AND TELECOMMUNICATION UTILITIES SHALL ONLY BE PERFORMED BY THE FACILITY OWNER. THE CONTRACTOR IS RESPONSIBLE TO GIVE ADVANCE NOTICE, AS REQUIRED BY THE FACILITY OWNER, BEFORE A SHUTDOWN IS REQUIRED.
- CONTRACTOR SHALL PROVIDE UNINTERRUPTED UTILITY SERVICE THROUGHOUT THE LENGTH OF THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE TRENCH DEWATERING AND THE BYPASSING OF WASTE WATER AS REQUIRED THROUGHOUT THE LIMITS OF THE PROJECT. PAYMENT FOR SUCH WORK SHALL BE INCLUDED IN THE PROJECT PAY ITEMS AND WILL NOT BE PAID FOR SEPARATELY.
- THE APPROXIMATE LOCATION OF UNDERGROUND UTILITIES AND STRUCTURES SHOWN ON THESE PLANS IS BASED UPON BEST AVAILABLE PUBLIC RECORDS. THE INFORMATION SHOWN ON THE PLANS MAY BE INCOMPLETE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE POSITION OF AND PROVIDE PROTECTION FOR SUCH UTILITIES AND STRUCTURES.
- CONTRACTOR SHALL TAKE EXTREME CAUTION WHEN EXCAVATING ADJACENT TO ACP WATER MAINS, ELECTRIC LINES AND GAS LINES. ANY DAMAGE TO WATER, SEWER, STORM, ELECTRIC, GAS, FIBER AND TRAFFIC LOOPS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CITY.
- ALL DISTANCES, DIMENSIONS AND QUANTITIES SHOWN ON THE DRAWINGS ARE ESTIMATED FROM UTILITY OWNERS AND CITY OF MENLO PARK RECORDS. CONTRACTOR SHALL VERIFY ALL INFORMATION.
- PRIOR TO EXCAVATION, HORIZONTAL DIRECTION DRILLING, PIPE BURSTING OR OTHER METHODS OF PIPELINE CONSTRUCTION, THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS SHOWN.
- EXISTING UTILITY LINES/PIPELINES SHALL BE SUPPORTED AND PROTECTED DURING CONSTRUCTION. EXISTING UTILITIES WHICH WERE PROPERLY SHOWN ON THE PLANS OR FIELD LOCATED, BUT ARE DAMAGED DURING WORK BY THE CONTRACTOR, SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- UNDERGROUND ELECTRIC LIGHTING, TELEPHONE AND TELECOMMUNICATION LINES, UNDERGROUND FIBER OPTIC LINES, CABLE TELEVISION LINES, OVERHEAD ELECTRIC LINES, UNDERGROUND SECONDARY ELECTRIC LINES ARE GENERALLY NOT SHOWN ON ALL DRAWINGS FOR CLARITY.
- THE CONTRACTOR IS CAUTIONED TO PROPERLY SUPPORT ALL EXCAVATIONS WHEN WORKING IN AND AROUND EXISTING PIPELINES AND CONDUITS. SOME OF THE TRENCHES FOR THESE FACILITIES HAVE GRANULAR SAND BACKFILL WHICH MAY COLLAPSE WHEN DISTURBED. CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGE TO EXISTING PIPELINES AND CONDUITS.
- AS THE FIRST ORDER OF WORK, THE CONTRACTOR SHALL POTHOLE ALL LOCATIONS SPECIFICALLY NOTED ON THE DRAWINGS.
- POST "TOW AWAY NO PARKING" SIGNS A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION.
- WATER METER BOXES AND OTHER UTILITY BOXES / STRUCTURES WITHIN THE WORK AREA SHALL BE REPLACED IF DAMAGED, ADJUSTED TO FINISHED ELEVATIONS PRIOR TO REPLACING NEW CONCRETE AND / OR NEW ASPHALT.
- CONTRACTOR SHALL POTHOLE IN ADVANCE TO VERIFY EXISTING UTILITY LINE LOCATIONS AND DEPTHS TO AVOID CONSTRUCTION CONFLICTS. CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY FOR VERIFYING EXISTING AND AVOIDING DAMAGING UTILITY LINES AS SHOWN IN THESE PLANS.
- CONTRACTOR SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING THE SAFETY OF ALL PERSONS AND PROPERTY.

WATER GENERAL NOTES

- ALL MATERIALS, CONSTRUCTION PROCEDURES AND APPURTENANCES SHALL CONFORM TO THE LATEST REQUIREMENTS OF THE PROJECT SPECIFICATIONS, PROJECT DRAWINGS, UTILITIES STANDARDS AND CITY OF MENLO PARK CONSTRUCTION POLICIES.
- ALL EXISTING WATER VALVES AND FIRE HYDRANTS REMOVED FROM THE ABANDONED SYSTEM BY THE CONTRACTOR SHALL BE SALVAGED AND DELIVERED TO THE CITY CORPORATION YARD LOCATED AT 333 BURGESS DR.
- CONTRACTOR SHALL CLOSE ALL VALVES OF ABANDONED PIPELINES, REMOVE VALVE BOX FOR EACH ABANDONED VALVE, FILL RISER WITH CONTROLLED DENSITY FILL (CDF), AND PLACE CONCRETE OR A.C. PATCH OVER EACH HOLE CREATED BY REMOVAL OF EXISTING VALVE BOX.
- SHUTDOWN OF ALL UTILITIES SHALL BE IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS.
- CONTRACTOR SHALL INSTALL APPROPRIATE 2" CORPORATION STOPS AND SADDLES TO BLEED AIR, PERFORM BACTERIOLOGICAL AND CHLORINATION TESTS.
- ALL WATER TO COMPLY WITH AWWA STANDARDS.
- ALL NEW WATER METERS SHALL BE SUPPLIED BY CITY.

STORM DRAIN GENERAL NOTES

- ALL WORK SHALL CONFORM TO THE REQUIREMENTS SHOWN ON THESE DRAWINGS AND DESCRIBED IN THE PROJECT SPECIFICATIONS. NO WORK SHALL BE PERMANENTLY COVERED UNTIL IT HAS BEEN INSPECTED AND APPROVED BY THE CITY OF MENLO PARK.
- ALL WORK SHALL CONFORM TO THE PROJECT DRAWINGS AND SPECIFICATIONS AND THE CITY OF MENLO PARK CONSTRUCTION POLICIES.
- CONTRACTOR SHALL REPAIR, WITH SAME MATERIAL AS EXISTING, ALL BROKEN SEWER MAINS, WATER LINES, AND LATERALS DAMAGED DURING CONSTRUCTION.
- ALL TRAFFIC CONTROL SHALL BE CONDUCTED IN ACCORDANCE WITH THE CITY OF MENLO PARK REQUIREMENTS AND THE CONTRACTOR'S SPECIFIC TRAFFIC CONTROL PLAN AS APPROVED BY THE CITY.
- WHERE SHOWN, FINISHED MANHOLE RIM ELEVATIONS ARE APPROXIMATE ONLY. ALL NEW AND EXISTING MANHOLE RIMS, UTILITY VAULTS, VALVE LIDS, AND UTILITY BOXES SHALL BE ADJUSTED TO MATCH ADJACENT GRADE UNLESS OTHERWISE NOTED ON PLANS.
- ALL EXISTING IMPROVEMENTS, INCLUDING BUT NOT LIMITED TO IRRIGATION LINES, LANDSCAPING, DRIVEWAYS, CURB, GUTTER, SIDEWALK, CULVERTS, DRAINS, AND TRAFFIC LOOPS, DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO THE CONDITION IN WHICH THEY WERE, OR BETTER, BEFORE THE EXCAVATION WAS MADE. PAYMENT FOR RESTORATION OF ANY EXISTING IMPROVEMENTS SHALL BE CONSIDERED AS INCLUDED IN THE ITEMS OF THE WORK INVOLVED AND SEPARATE PAYMENT FOR RESTORATION WILL NOT BE MADE.
- THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS TO CAUSE THE LEAST POSSIBLE OBSTRUCTION AND INCONVENIENCE TO THE PUBLIC. THROUGHOUT THE PERFORMANCE OF THE WORK THE CONTRACTOR SHALL CONSTRUCT AND ADEQUATELY MAINTAIN SUITABLE AND SAFE CROSSINGS OVER TRENCHES. PROVIDE DRIVEWAY ACCESS AND SUCH DETOURS AS ARE NECESSARY FOR PUBLIC PEDESTRIAN AND VEHICULAR TRAFFIC.
- CONTRACTOR SHALL MAINTAIN A CURRENT, COMPLETE, AND ACCURATE RECORD OF ANY CHANGES IN THE CONSTRUCTION OF IMPROVEMENTS AS PROPOSED IN THE DRAWINGS AND SPECIFICATIONS FOR THE PURPOSE OF PROVIDING THE ENGINEER WITH A BASIS FOR RECORD DRAWINGS. NO CHANGES SHALL BE MADE WITHOUT PRIOR WRITTEN APPROVAL OF THE CITY.

AGENCIES & UTILITIES CONTACT

PG&E 800-743-5000
MP WATER & STORM 650-330-6740
WEST BAY SANITARY 650-321-0384
AT&T 770-929-4730
COMCAST 925-349-1175
CITY MAINTENANCE 650-330-6780
POLICE 650-330-6300
FIRE 650-688-8400
CITY ENGINEERING 650-330-6740

REFERENCE STANDARDS

CITY OF MENLO PARK STANDARD DETAILS

CALTRANS STANDARD SPECS, 2010 EDITION

AWWA

ASTM

ASME

ANSI

AWS

NEMA

NFPA

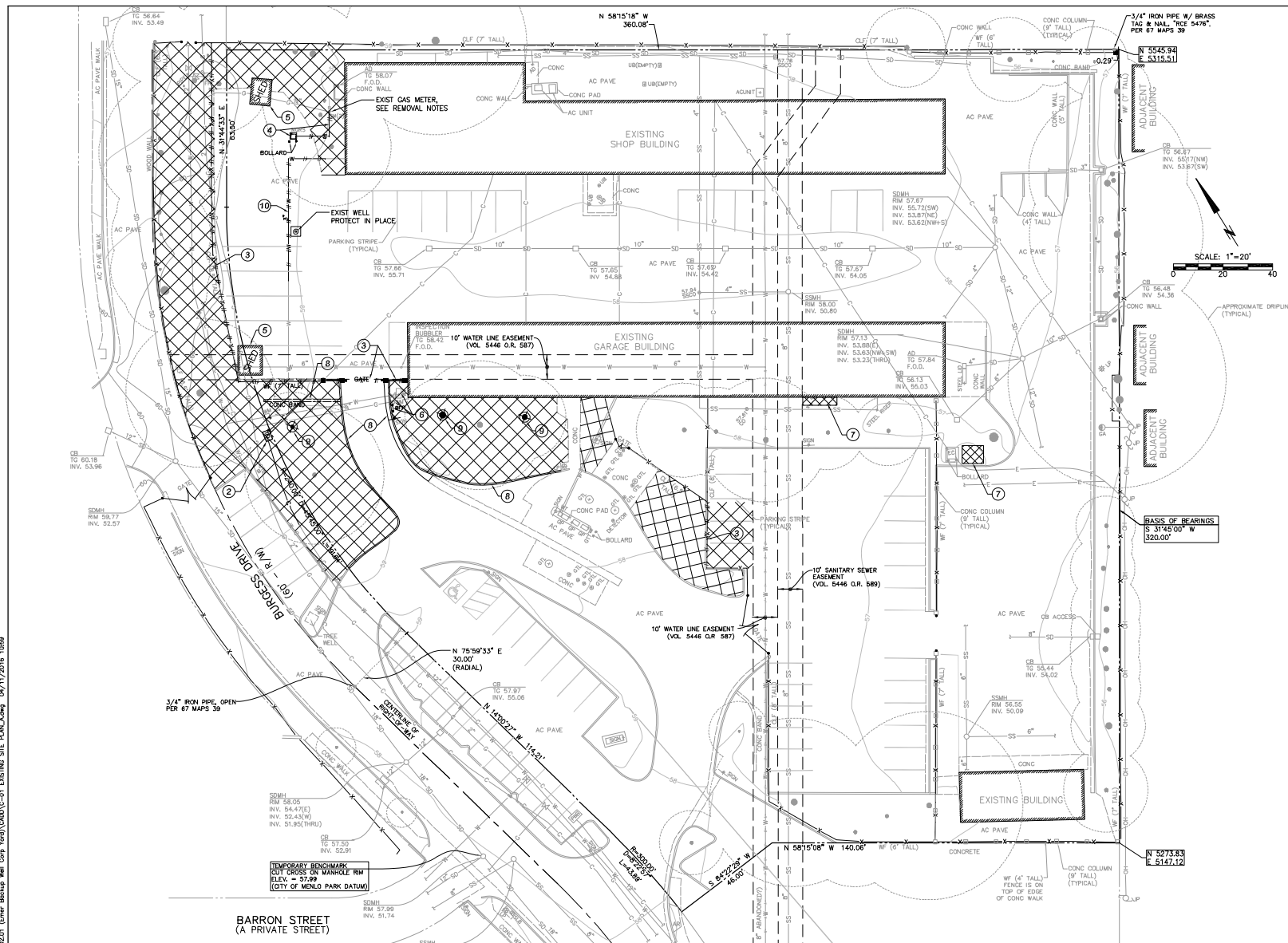
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NEC

PRELIMINARY FINAL DESIGN

								EMERGENCY BACKUP WELL FACILITY AT CITY CORPORATION YARD		DRAWING NO. G-3	
				DESIGNED BY: PRM		PREPARED UNDER THE DIRECT SUPERVISION OF: 				SHEET NO.	
				DRAWN BY: DHN		14271 Danielson Street Palo Alto, California 94304 T 658.413.2400 F 658.413.2440 www.icorporation.com		701 LAUREL STREET, MENLO PARK, CA 94025-3483 PHONE (650) 330-6740 FAX (650) 327-5497		CLIENT JOB NO. XXXXXX	
				CHECKED BY: AG		DATE		GENERAL NOTES			

P:\Projects\MENLO (00007)000201 [Emer Backup Well Corp Yard]CADD\01-EXISTING SITE PLAN.dwg 04/11/2016 15:59




DEMOLITION NOTES

- 1 APPROXIMATE AREA TO BE CLEAR
- 2 RELOCATE EXIST TELEPHONE BOX
- 3 REMOVE FENCES AND GATE
- 4 REMOVE NATURAL GAS REFUELING STATION AND BOLLARDS, CAP GAS LINE AT MAIN OR METER PER UTILITY OWNER REQUIREMENT.
- 5 RELOCATE EXIST STORAGE SHED
- 6 RELOCATE EXIST WATER VALVE, METER & BACK FLOW PREVENTION ASSEMBLY
- 7 APPROXIMATE AREA TO BE CLEARED AS REQUIRED FOR INSTALLATION OF NEW "TRANSFORMER" AND "SWITCHGEAR"
- 8 REMOVE EXIST CURB, CURB & GUTTER, CONCRETE BAND
- 9 EXIST TREE TO BE REMOVED, SEE LANDSCAPE DWGS
- 10 RE-ALIGN 3" WATER SEE DRAWING C-3

REMOVAL NOTES:

1. ABANDONMENT OR DEMOLITION SHALL BE IN ACCORDANCE WITH SPECIFICATIONS AND AS SHOWN ON THIS DWG.
2. ALL MATERIALS REMOVED FROM THE SITE SHALL BE TRANSPORTED AND DISPOSED OF BY THE CONTRACTOR OFF-SITE IN ACCORDANCE WITH ALL APPLICABLE REGULATIONS
3. ALL FACILITIES NOT SHOWN FOR ABANDONMENT OR DEMOLITION SHALL BE PROTECTED IN PLACE BY THE CONTRACTOR. ALL EQUIPMENT / FACILITIES SPECIFIED TO BE DEMOLISHED / ABANDONED SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR UNLESS EQUIPMENT / FACILITIES ARE SPECIFICALLY NOTED TO BE SALVAGED BY THE CONTRACTOR.
4. DIMENSIONS & LOCATIONS PROVIDED ARE FOR REFERENCE ONLY. CONTRACTOR SHALL FIELD VERIFY PRIOR TO CONSTRUCTION & NOTIFY ENGINEER FOR DESIGN REVISION IF NECESSARY.
5. ALL VALVES SHALL BE OPERATED BY CITY STAFF. CONTRACTOR SHALL COORDINATE WITH CITY AT LEAST 48 HRS IN ADVANCE.
6. FOR TREE REMOVAL SEE DRAWING L-1
7. THE CONTRACTOR SHALL USE CAUTION WHEN WORKING ON OR AROUND EXIST GAS FILLING STATIONS, PUMPS, METERS AND OTHER FACILITIES EXCEPT WHERE SPECIFICALLY CALLED FOR IN THE CONTRACT DOCUMENTS, THE CONTRACTOR SHALL PROTECT IN PLACE ALL GAS FACILITIES.

LEGEND:

-  CLEAR, GRUB, AND STRIP AREA OF ALL EXIST VEGETATION EXCEPT FOR TREES DESIGNATED TO REMAIN. PROTECT IN PLACE ALL TREES DESIGNATED TO REMAIN AS SHOWN ON THE LANDSCAPING DRAWINGS. THE CONTRACTOR SHALL PAY SPECIAL ATTENTION TO PROTECTING THE ROOT SYSTEMS OF TREES DESIGNATED TO REMAIN DURING THE CLEARING, GRUBBING, AND STRIPPING OPERATIONS.

PRELIMINARY FINAL DESIGN

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				DHN
				DRAWN BY:
				DHN
				CHECKED BY:
				AG

Infrastructure
14271 Danielson Street
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www.icorporation.com

PREPARED UNDER THE
DIRECT SUPERVISION OF:
DATE

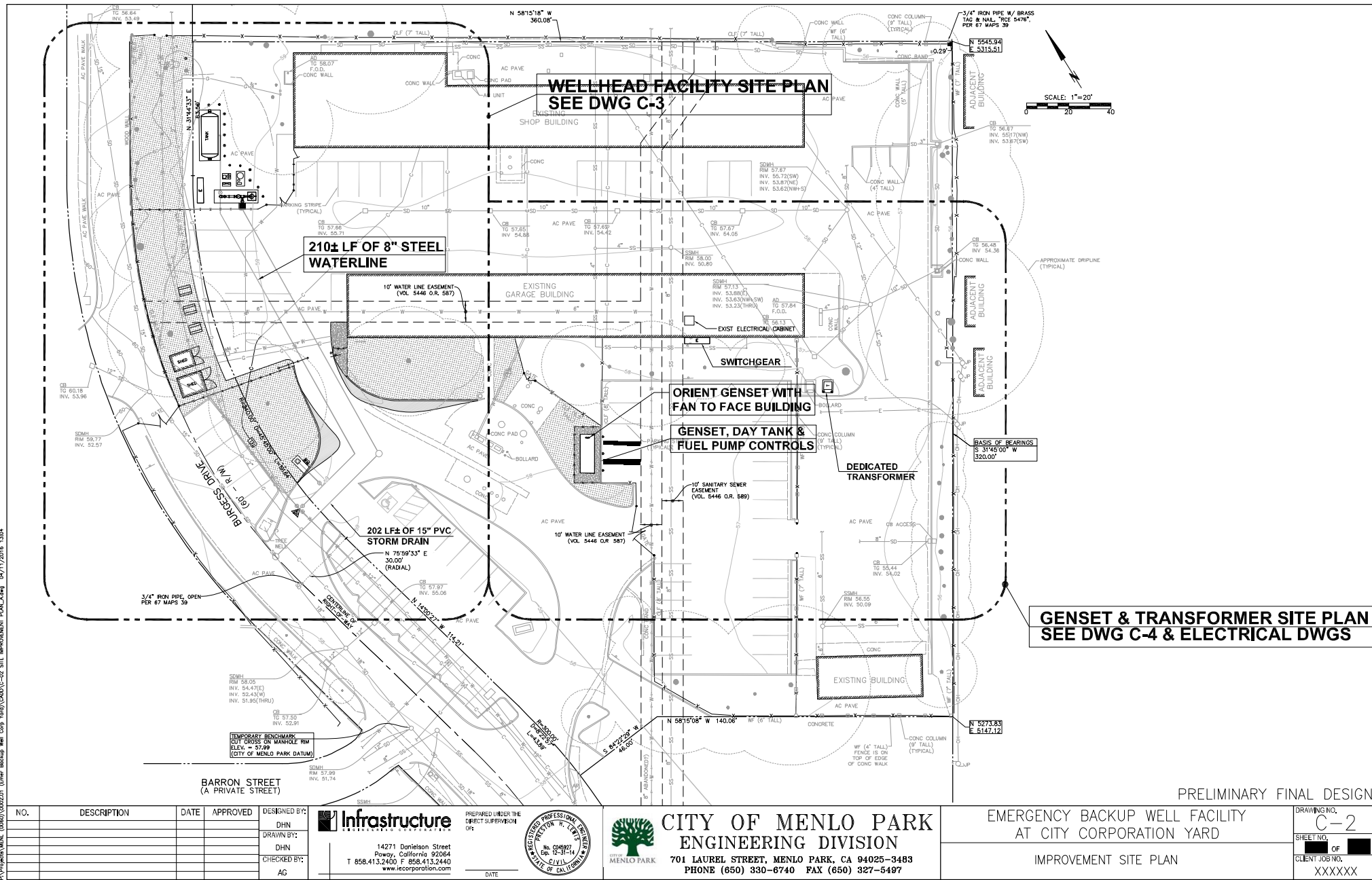


CITY OF MENLO PARK
ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

EMERGENCY BACKUP WELL FACILITY
AT CITY CORPORATION YARD
EXISTING / DEMOLITION SITE PLAN

DRAWING NO.
C-1
SHEET NO.
1 OF
CLIENT JOB NO.
XXXXXX

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NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				DHN
				DRAWN BY:
				DHN
				CHECKED BY:
				AG

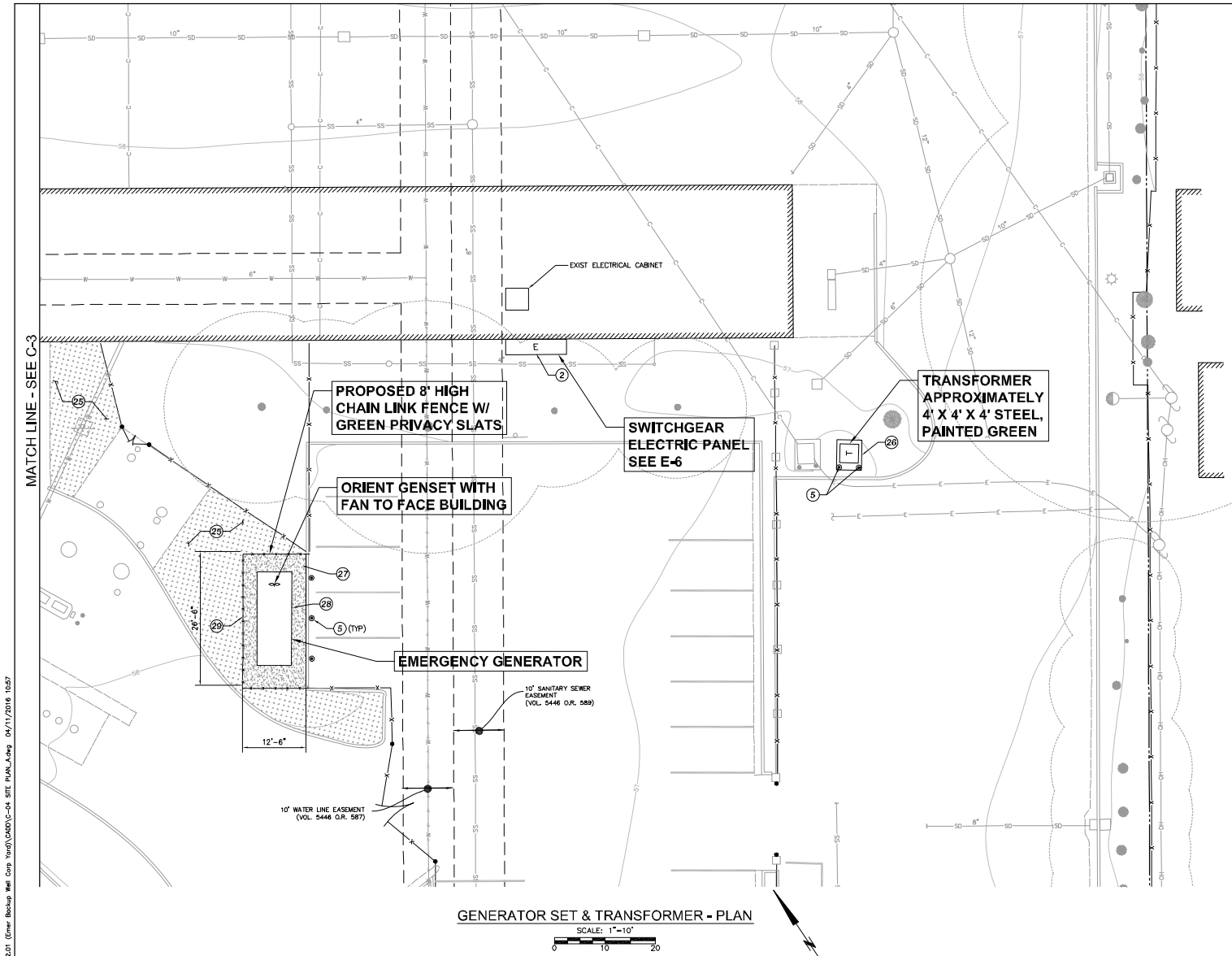
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14271 Danielson Street
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T 858.413.2400 F 858.413.2440
www.icorporation.com

PREPARED UNDER THE
OBJECT SUPERVISION
OF:

DATE: _____

CITY OF MENLO PARK
ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

PRELIMINARY FINAL DESIGN	
EMERGENCY BACKUP WELL FACILITY AT CITY CORPORATION YARD	
IMPROVEMENT SITE PLAN	
DRAWING NO. C-2	SHEET NO. OF
CLIENT JOB NO. XXXXXX	



MATERIAL LIST

- ① THRU ②⑤ SEE DRAWING C-3
- ②⑥ TRANSFORMER CONCRETE PAD
- ②⑦ GENERATOR SET CONCRETE PAD
- ②⑧ 250 KW GENSET WITH SOUND ATTENUATED ENCLOSURE FOR OUTDOOR SERVICE
- ②⑨ 8' HIGH CHAIN LINK FENCE W/ GREEN PRIVACY SLATS

NOTES:

1. ALL FINAL EQUIPMENT SIZE AND RATINGS WILL BE DETERMINED SUBSEQUENT TO WELL TESTING.

GENERATOR SET & TRANSFORMER - PLAN

SCALE: 1"=10'
0 10 20

P:\Projects\MENLO\00000002\01 [Emer Backup Well Corp Yard]CADD\C-4 SITE PLAN.dwg 04/11/2016 10:57

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				DHN
				DRAWN BY:
				DHN
				CHECKED BY:
				AG

Infrastructure
14271 Danielson Street
Poway, California 92064
T 656.413.2400 F 656.413.2440
www.icorporation.com

PREPARED UNDER THE
DIRECT SUPERVISION
OF:
DATE

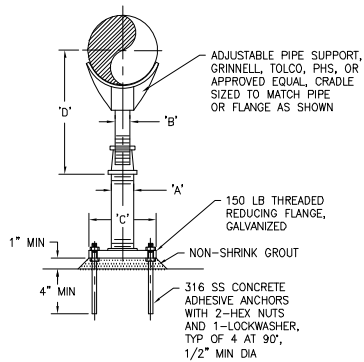


CITY OF MENLO PARK
ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

EMERGENCY BACKUP WELL FACILITY
AT CITY CORPORATION YARD
GENSET & TRANSFORMER SITE PLAN

PRELIMINARY FINAL DESIGN

DRAWING NO.	C-4
SHEET NO.	OF
CLIENT JOB NO.	XXXXXX

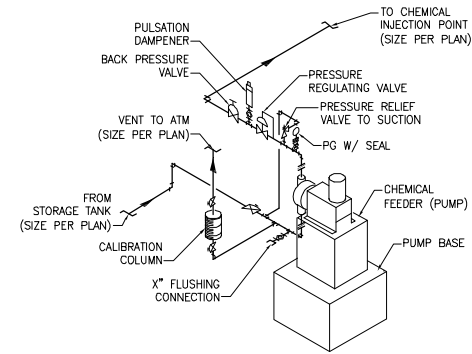


PIPE SIZE	'A'	'B'	'C'	'D' MINIMUM	'D' MAXIMUM
2-1/2	2-1/2	1-1/2	9	8	11-1/2
3	2-1/2	1-1/2	9	8-1/2	11-3/4
3-1/2	2-1/2	1-1/2	9	8-1/2	12
4	3	*2-1/2	9	10-1/4	14
6	3	*2-1/2	9	11-5/8	15-1/4
8	3	*2-1/2	9	13-5/8	16-1/2
10	3	*2-1/2	9	14-5/8	18-1/4
12	3	*2-1/2	9	15-5/8	19-3/4
14	4	3	11	18-7/8	20-3/4
16	4	3	11	19-7/8	22-1/4
18	6	3-1/2	13-1/2	21-1/4	24
20	6	3-1/2	13-1/2	23-1/4	25-1/2
24	6	4	13-1/2	26-1/2	28-1/4
30	6	4	13-1/2	29-5/8	31-1/2
32	6	4	13-1/2	30-5/8	32-3/4
36	6	4	13-1/2	32-5/8	34-3/4

* DENOTES REFERENCE TO MANUFACTURER

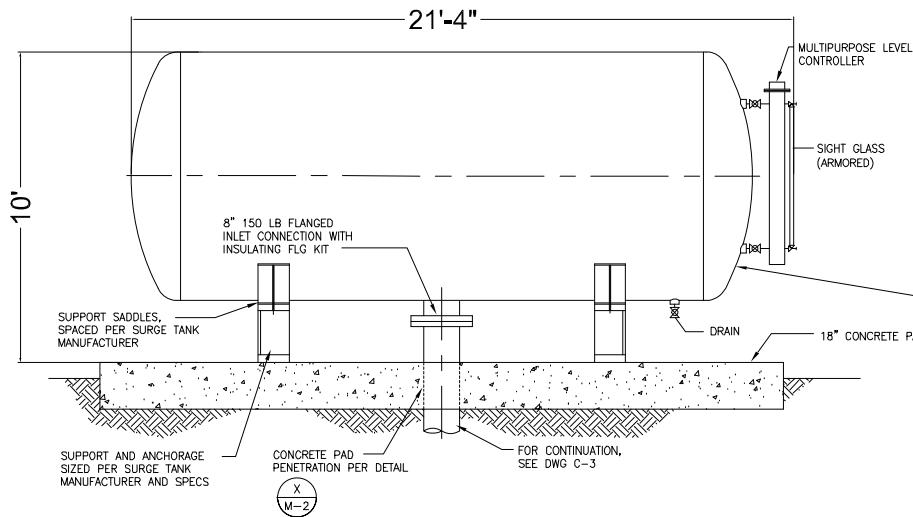
ADJUSTABLE PIPE SUPPORT
SCALE: NTS

1
—



CHEMICAL FEED PUMP PIPING SCHEMATIC
SCALE: NTS

2
—



5,000 GAL (NOMINAL)
HYDROPNEUMATIC STEEL
TANK, PAINTED GREEN

HYDROPNEUMATIC TANK
SCALE: NTS

3
—

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				PRM
				DHN
				AG

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Poway, California 92064
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ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

EMERGENCY BACKUP WELL FACILITY
AT CITY CORPORATION YARD
MECHANICAL DETAILS

PRELIMINARY FINAL DESIGN

DRAWING NO.
M-2
SHEET NO. **1** OF **1**
CLIENT JOB NO.
XXXXXX

TREE DISPOSITION LIST

NUMBER	TREE TYPE	DBH/TRUNKS DIVIDE	REASON FOR REMOVAL	CONDITION	REMOVE	HERITAGE TREE
1	LIRIODENDRON TULIPIFERA	17"	TREE TO REMAIN	GOOD		X
2	QUERCUS RUBRA	15"	REQUIRED FOR IMPROVEMENTS	FAIR, RODENT DAMAGE ON BRANCHES	X	X
3	QUERCUS AGRIFOLIA	32"	TREE TO REMAIN	GOOD		X
4	AILANTHUS ALTISSIMA	36"	REQUIRED FOR IMPROVEMENTS	MULTI-TRUNK, INVASIVE SPECIES	X	X
5	QUERCUS AGRIFOLIA	8"	TREE TO REMAIN	GOOD, CROWDED		
6	QUERCUS AGRIFOLIA	8"	TREE TO REMAIN	GOOD, CROWDED		
7	QUERCUS AGRIFOLIA	9"	TREE TO REMAIN	GOOD, MULTI-TRUNK, CROWDED		
8	QUERCUS AGRIFOLIA	12"	TREE TO REMAIN	GOOD, MULTI-TRUNK, CROWDED		X
9	QUERCUS AGRIFOLIA	12"	TREE TO REMAIN	GOOD, MULTI-TRUNK, CROWDED		X
10	QUERCUS AGRIFOLIA	8"	TREE TO REMAIN	GOOD, MULTI-TRUNK, CROWDED		
11	QUERCUS AGRIFOLIA	8"	TREE TO REMAIN	GOOD, CROWDED		
12	QUERCUS AGRIFOLIA	20"	ABOVE WATER MAINLINE	REMOVED		X
13	QUERCUS AGRIFOLIA	12"	TREE TO REMAIN	GOOD, CROWDED		X
14	QUERCUS AGRIFOLIA	6"	TREE TO REMAIN	GOOD, CROWDED		
15	QUERCUS AGRIFOLIA	8"	TREE TO REMAIN	GOOD, MULTI-TRUNK, CROWDED		
16	QUERCUS AGRIFOLIA	7"	TREE TO REMAIN	GOOD, CROWDED		
17	QUERCUS AGRIFOLIA	6"	TREE TO REMAIN	GOOD, MULTI-TRUNK, CROWDED		
18	QUERCUS AGRIFOLIA	20"	TREE TO REMAIN	GOOD, MULTI-TRUNK, CROWDED		X
19	QUERCUS AGRIFOLIA	20"	TREE TO REMAIN	GOOD		X
20	QUERCUS AGRIFOLIA	28"	TREE TO REMAIN	GOOD		X
21	QUERCUS AGRIFOLIA	11"	TREE TO REMAIN	GOOD, CROWDED		X
22	QUERCUS AGRIFOLIA	14"	TREE TO REMAIN	GOOD, CROWDED		X
23	QUERCUS AGRIFOLIA	18"	TREE TO REMAIN	GOOD		X
24	JUNIPERUS C. 'TORULOSA'	48"	REQUIRED FOR IMPROVEMENTS	GOOD, CLOSE TO BUILDING	X	X
25	QUERCUS AGRIFOLIA	25"	TREE TO REMAIN	GOOD, CROWDED		X
26	JUNIPERUS C. 'TORULOSA'	48"	REQUIRED FOR IMPROVEMENTS	GOOD, CLOSE TO BUILDING	X	X
27	SAPIUM SEBIFERUM	15"	TREE TO REMAIN	GOOD		X
28	SAPIUM SEBIFERUM	14"	TREE TO REMAIN	GOOD		
29	QUERCUS AGRIFOLIA	23"	TREE TO REMAIN	GOOD		X
30	MAGNOLIA GRANDIFLORA	14"	TREE TO REMAIN	GOOD		
31	MAGNOLIA GRANDIFLORA	15"	TREE TO REMAIN	GOOD		X
32	MAGNOLIA GRANDIFLORA	20"	TREE TO REMAIN	GOOD		X
33	JACARANDA MIMOSIFOLIA	8"	REQUIRED FOR IMPROVEMENTS	GOOD	X	

* MEASUREMENT REPRESENTS TRUNK CIRCUMFERENCE AT THE POINT WHERE TRUNKS DIVIDE

TREE DISPOSITION LEGEND

- ⊙ EXISTING TREE TO REMAIN
- ⊗ EXISTING TREE TO BE REMOVED

TREE PROTECTION NOTES

- CITY ORDINANCE.** REFER TO TREE PROTECTION SPECIFICATIONS.
- TREE PROTECTION AND MAINTENANCE REQUIREMENTS.** ALL TRENCHING WITHIN THE DRIPLINE OF EXISTING TREES TO REMAIN SHALL BE BY HAND, WITH CARE TAKEN NOT TO CUT OR DAMAGE ROOTS OVER 1-INCH DIAMETER WITH PRIOR APPROVAL FROM THE CITY ARBORIST. TREES TO REMAIN SHALL BE FENCED AROUND DRIPLINE OF TREE WITH CHAIN LINK, REFER TO SPECIFICATIONS.
- MULCH.** PLACE A 6" LAYER OF MULCH BENEATH THE DRIPLINE OF EXISTING TREES TO REMAIN. KEEP MULCH 12" AWAY FROM TRUNK.

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				ZK
				MS

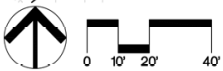


PREPARED UNDER THE
UNOFFICIAL SUPERVISION
OF:
DATE



CITY OF MENLO PARK
ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

EMERGENCY BACKUP WELL FACILITY AT CITY CORPORATION YARD	DRAWING NO. L-1
TREE DISPOSITION AND PROTECTION PLAN	SHEET NO. 20 OF 25
	CLIENT JOB NO. XXXXXX



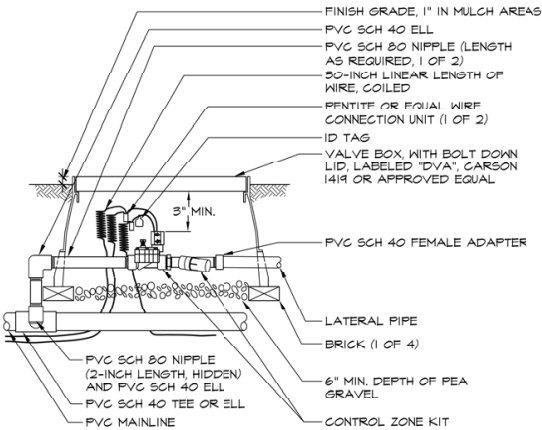
PLANNING APPLICATION SUBMITTAL
APRIL 2016

IRRIGATION LEGEND

- △ MICRO SPRAY, HUNTER, TS-T-Q WITH MICRO SPRAY STAKE
- ▲ MICRO SPRAY, HUNTER, TS-T-H WITH MICRO SPRAY STAKE
- ▲ MICRO SPRAY, HUNTER, TS-T-F WITH MICRO SPRAY STAKE
- TRIFL BUBBLER, HUNTER PGB-50
- ⊕ DRIP CONTROL ZONE KIT, HUNTER, 102-101-40
- ⊕ REMOTE CONTROL VALVE, HUNTER, 10V-1016
- LATERAL LINE, CLASS 200 PVC, SIZE PER PLAN
- MAINLINE, SIZE AND CLASS AS REQUIRED, REFER TO SPECS
- QUICK COUPLING VALVE, HUNTER HQ3-RC
- ⊕ VALVE #
- ⊕ APPROXIMATE GPM THROUGH VALVE
- ⊕ VALVE SIZE

EXISTING IRRIGATION LEGEND

- Ⓢ EXISTING CONTROLLER, RAINMASTER DX2, 12 STATION (6 OPEN)
- Ⓢ BACKFLOW PREVENTER, FEECO 825YA, RELOCATE TO ADJACENT SHURD AREA, REFER TO LAYOUT PLAN
- Ⓢ EXISTING WATER METER, 2"
- Ⓢ EXISTING REMOTE CONTROL VALVE
- Ⓢ EXISTING PRESSURE REGULATOR
- Ⓢ EXISTING GATE VALVE
- EXISTING MAINLINE, REFER TO PLAN FOR SIZE, ACP CLASS 150 PIPE



1 L-2 DRIP CONTROL ZONE KIT SECTION

N.T.S.

15004 Drip Valve-1.dwg

IRRIGATION NOTES

- SPECIFICATIONS:** SEE IRRIGATION SPECIFICATIONS FOR ADDITIONAL INFORMATION.
- VERIFICATION:** SYSTEM DESIGN IS BASED ON 30 P.S.I. AVAILABLE AT EXISTING QUICK COUPLER VALVE AND 14 GPM. VERIFY SAME AND NOTIFY OWNER'S REPRESENTATIVE IF LOWER FIGURES ARE RECORDED DURING VERIFICATION. SUCH NOTICE SHALL BE MADE IN WRITING AND PRIOR TO COMMENCING ANY IRRIGATION WORK.
- UTILITIES:** VERIFY LOCATION OF ALL ON-SITE UTILITIES. RESTORATION OF DAMAGED UTILITIES SHALL BE MADE AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE CITY.
- SCHEMATIC:** SYSTEM FEATURES ARE SHOWN SCHEMATICALLY FOR GRAPHIC CLARITY. INSTALL ALL PIPING AND VALVES IN COMMON TRENCHES WHERE FEASIBLE AND INSIDE PLANTING AREAS WHENEVER POSSIBLE. ALL VALVES SHALL BE LOCATED IN GROUND COVER OR SHRUB AREAS WHENEVER POSSIBLE.
- CODES:** IRRIGATION SYSTEM SHALL BE INSTALLED IN ACCORDANCE WITH ALL LOCAL CODES AND MANUFACTURER'S SPECIFICATIONS. NOTIFY CITY'S REPRESENTATIVE BY TELEPHONE AND IN WRITING OF ANY CONFLICTS PRIOR TO INSTALLATION.
- SLEEVING:** ADEQUATELY SIZE ALL SLEEVES SHOWN ON PLAN. SLEEVES SHALL BE INSTALLED AT THE NECESSARY DEPTHS PRIOR TO PAVEMENT CONSTRUCTION. SLEEVING SHALL EXTEND 1'-0" FROM EDGE OF PAVING INTO LAWN OR PLANTING AREA, AND SHALL HAVE ENDS CLEARLY MARKED ABOVE GRADE.
- QUICK COUPLING VALVES:** INSTALL ON TRIPLE SWING JOINT. LOCATE 12 INCHES AWAY FROM EDGE OF WALKS, FENCES AND CURBS WITHIN PLANTING AREAS. PROVIDE CITY WITH ONE OPERATING KEY, TWO SETS OF LOCKING COVER KEYS, AND ONE SHVEL HOSE ELL.
- HEAD ALLOWANCE:** ALLOW IN BID PRICE AN AMOUNT SUFFICIENT TO PROVIDE AND INSTALL AN ADDITIONAL 4 MICRO SPRAY HEADS OF EACH TYPE SPECIFIED ON PLAN TO ACCOMMODATE FIELD CHANGES. THESE HEADS SHALL BE LOCATED AS DIRECTED BY THE CITY'S REPRESENTATIVE. DELIVER TO THE OWNER ANY UN-USED ADDITIONAL HEADS AT THE END OF THE MAINTENANCE PERIOD.
- MAINLINE BREAK:** SHOULD THE EXISTING MAINLINE BREAK OR BE SHUT OFF FOR ANY REASON DURING THE COURSE OF CONSTRUCTION THE CONTRACTOR SHALL HAND WATER ALL TREES, SHRUBS, TURF, AND GROUND COVER THAT THE EXISTING IRRIGATION SYSTEM WATERS. CONTINUE TO DO SO UNTIL THE IRRIGATION SYSTEM IS OPERABLE.
- EXISTING IRRIGATION EQUIPMENT:** THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE COVERAGE, LOCATION, AND CONDITION OF THE EXISTING IRRIGATION SYSTEM PRIOR TO CONSTRUCTION. CONTRACTOR RETAINS FULL RESPONSIBILITY TO INSURE THAT EXISTING PLANTING TO REMAIN IS IRRIGATED DURING CONSTRUCTION AND THAT THE EXISTING IRRIGATION SYSTEM IS MAINTAINED IN ITS CURRENT OPERATIONAL STATE FOR THE DURATION OF THE PROJECT.

IRRIGATION SCHEDULE

Valve Number/GPM	System	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
2, 4 (2 GPM)	Ms	5	6	5	5	7	4	11	8	8	7	6	7
	Lo	2	2	4	25	35	36	44	40	32	21	12	7
		40	40	80	100	40	144	176	160	128	84	48	28
3 (14 GPM)	Ms	4	6	6	8	10	11	13	12	10	11	6	5
	Med	4	3	5	5	5	5	5	5	3	3	2	2
		16	18	30	40	50	55	65	60	50	33	18	10
		64	72	120	160	200	220	260	240	200	132	72	40
1 (2 GPM)	Bu	3	4	3	4	3	3	3	3	3	3	3	2
	Lo	1	1	2	2	3	2	3	2	3	2	1	1
		3	4	6	8	9	10	12	10	4	6	3	2
		12	16	24	32	36	40	48	40	36	24	12	8
5 (3 GPM)	Bu	5	3	3	4	4	6	5	6	5	5	5	3
	Med	1	1	3	3	4	3	4	3	3	2	1	1
		5	6	9	12	16	20	16	15	10	5	3	3
		20	24	36	48	64	72	80	72	60	40	20	12
Approx. Minutes of Operation Per Day		17	20	19	23	26	34	35	34	28	28	22	18

System type:

Ms -- Micro Spray
Bu -- Bubblers

System run times by season are given using the following method:

5	-- Run time in minutes
5	-- Days per week
25	-- Minutes per week
100	-- Total minutes per month

This Irrigation Schedule is based on an 8 hour watering window with 5 days of operation per week.

During the landscape establishment period, increase the operation run time by 20% and days of operation by one day per week.

Due to variable and unforeseen site conditions, the Irrigation system run times may need to be adjusted to ensure that proper moisture is maintained in the landscape.

IRRIGATION P.O.C. #1 TIE INTO EXISTING MAINLINE. RUN WIRES BACK TO EXISTING CONTROLLER FOR NEW VALVE AND UTILIZE OPEN STATIONS ON CONTROLLER

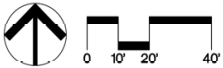
IRRIGATION P.O.C. #2 TIE INTO EXISTING MAINLINE. RUN WIRES BACK TO EXISTING CONTROLLER FOR NEW VALVE AND UTILIZE OPEN STATIONS ON CONTROLLER

IRRIGATION P.O.C. #3 TIE INTO EXISTING MAINLINE. RUN WIRES BACK TO EXISTING CONTROLLER FOR NEW VALVE AND UTILIZE OPEN STATIONS ON CONTROLLER

"I HAVE COMPLIED WITH THE CRITERIA OF THE WATER CONSERVATION IN LANDSCAPING ORDINANCE AND APPLIED THEM FOR THE EFFICIENT USE OF WATER IN THE IRRIGATION DESIGN PLAN"

SIGNATURE

MARK SLICHTER



PLANNING APPLICATION SUBMITTAL
APRIL 2016

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				ZK
				DRAWN BY:
				ZK
				CHECKED BY:
				MS



Callander Associates
Landscape Architecture
31 South Ave.
San Mateo, CA 94401
760.337.1313
760.337.1313

PREPARED UNDER THE
LANDSCAPE ARCHITECTURE
OF:

DATE



CITY OF MENLO PARK
ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

EMERGENCY BACKUP WELL FACILITY
AT CITY CORPORATION YARD
IRRIGATION PLAN

DRAWING NO.
L-2
SHEET NO.
21 OF 25
CLIENT JOB NO.
XXXXXX







PLANT LIST

ABBREY.	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	WATER USE
TREES					
QUE LOB	QUERCUS LOBATA	VALLEY OAK	36" BOX	AS SHOWN	LOW
QUE RUB	QUERCUS RUBRA	RED OAK	36" BOX	AS SHOWN	MODERATE
SHRUBS					
LAS IND	LASERSTROEMIA INDICA 'MUSKOGEE'	GRAPE MYRTLE	5 GALLON	5'-0" O.C.	LOW
MIM AUR	MIMULUS AURANTIACUS	MONKEY FLOWER	5 GALLON	4'-0" O.C.	VERY LOW
MH CAP	MULLENBERGIA CAPILLARIS 'REGAL MIST'	PINK MILLY BRASS	5 GALLON	4'-0" O.C.	LOW
GROUNDCOVERS					
	ANIGOZANTHOS HYBRIDS	KANGAROO PAW	1 GALLON	2'-6" O.C.	LOW
	COTONEASTER HORIZONTALIS	ROCK COTONEASTER	1 GALLON	5'-0" O.C.	LOW
	GERANIUM 'JOHNSON'S BLUE'	JOHNSON'S BLUE CRANESBILL	1 GALLON	2'-0" O.C.	MODERATE
	LIMONIUM FEREZII	SEA LAVENDER	1 GALLON	2'-0" O.C.	LOW

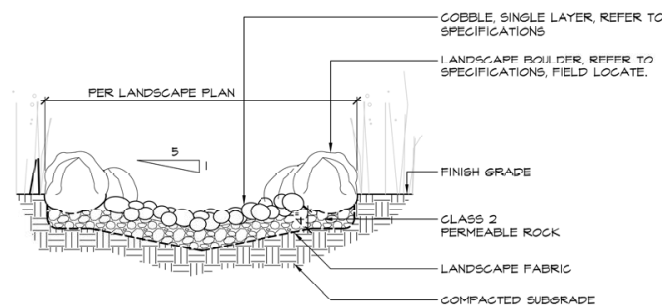
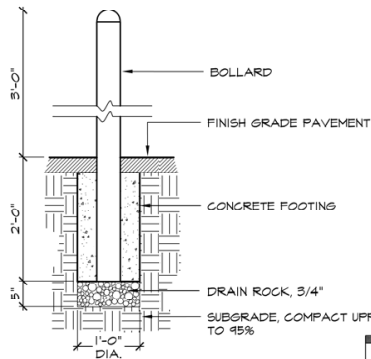
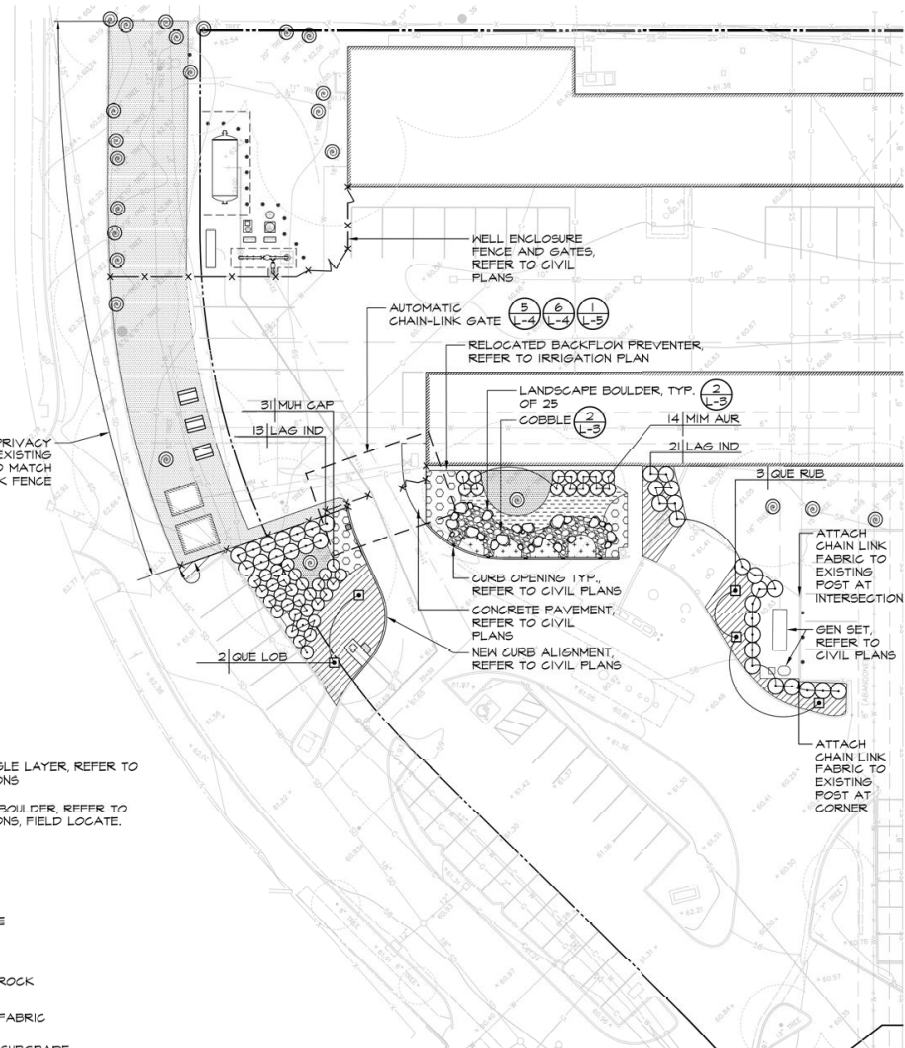
PLANTING NOTES

- MULCH:** INSTALL A UNIFORM THREE INCH COVERING OF WALK-ON MULCH, 1-1/2" MAX PARTICLE SIZE, IN ALL AREAS TO BE PLANTED AND AS INDICATED ON DRAWINGS. MATERIAL AVAILABLE FROM RED-GRD, (800) 654-4358, OR EQUAL.
- EXISTING PLANT MATERIAL:** PROTECT ALL EXISTING PLANT MATERIAL TO REMAIN. REPAIR ANY DAMAGES INCURRED AS A DIRECT RESULT OF THIS CONTRACT TO THE CITY'S SATISFACTION AT NO ADDITIONAL COST.
- GROUNDCOVER:** PROVIDE GROUNDCOVER AT INDICATED ON-CENTER SPACING THROUGHOUT ALL AREAS TO BE PLANTED. GROUNDCOVER SHALL BE PROVIDED UP TO THE WATERING BASIN OF ALL TREES AND SHRUBS.
- QUANTITIES:** THE QUANTITIES SHOWN ON THE LABELS ARE NOT TO BE CONSTRUED AS THE COMPLETE AND ACCURATE LIMITS OF THE CONTRACT. FURNISH AND INSTALL ALL PLANTS SHOWN SCHEMATICALLY ON THE DRAWINGS.

LAYOUT LEGEND

-  EXISTING TREE TO REMAIN
-  TREE, 36" BOX SIZE (2 L-6)
-  SHRUB MASS (4 L-6)
-  PICNIC TABLE, PER SPECS
-  CHAIN LINK FENCE (4 L-4)
-  MULCH, SEE PLANTING NOTE #1

ADD PRIVACY SLATS TO EXISTING FENCE TO MATCH CHAIN LINK FENCE



1
L-3
BOLLARD SECTION

0 1/2' 1' 2'
15004 BollardPermanent_L12.dwg

2
L-3
DRY CREEK BED SECTION

0 1/2' 1' 2'
xxxx.dwg



PLANNING APPLICATION SUBMITTAL
APRIL 2016

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				ZK
				DRAWN BY:
				ZK
				CHECKED BY:
				MS



PREPARED UNDER THE
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CITY OF MENLO PARK
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701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

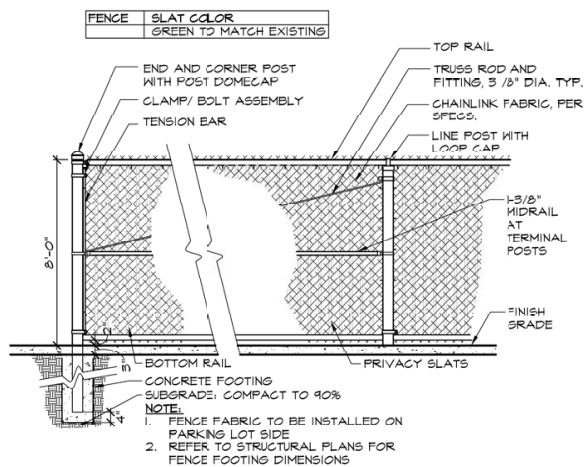
EMERGENCY BACKUP WELL FACILITY
AT CITY CORPORATION YARD

LAYOUT PLAN

DRAWING NO.
L-3
SHEET NO.
22 OF 25
CLIENT JOB NO.
XXXXXX

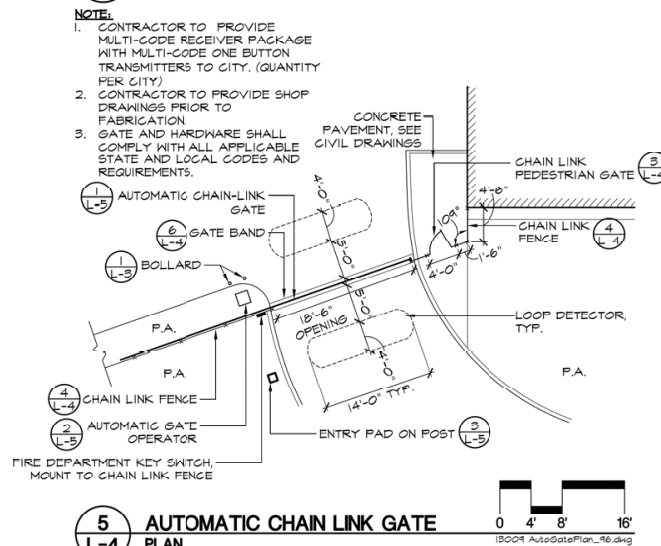
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1 NOT USED
L-4

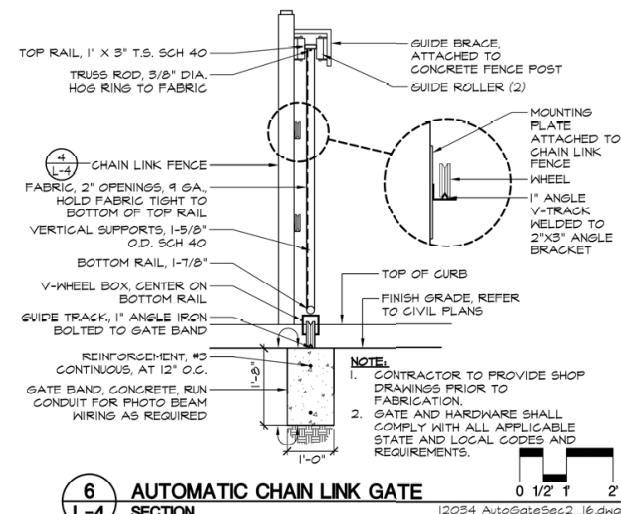
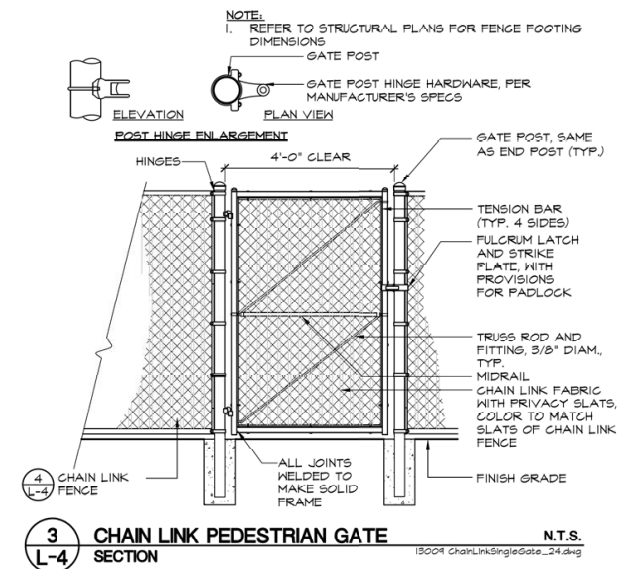


4 CHAIN LINK FENCE SECTION
L-4 N.T.S.
18004 ChainLinkFence_24.dwg

2 NOT USED
L-4



5 AUTOMATIC CHAIN LINK GATE PLAN
L-4 18004 AutoGatePlan_16.dwg



NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				ZK
				DRAWN BY:
				ZK
				CHECKED BY:
				MS



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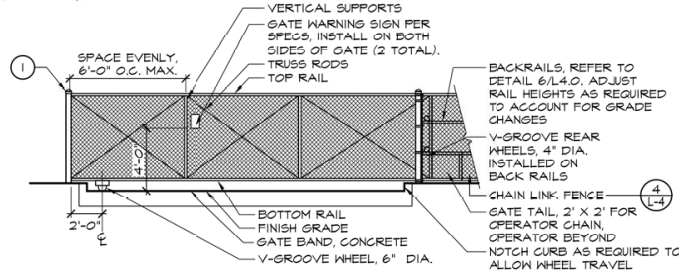
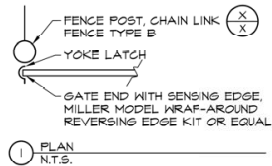


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EMERGENCY BACKUP WELL FACILITY
AT CITY CORPORATION YARD
LANDSCAPE DETAILS

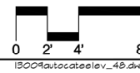
DRAWING NO.
L-4
SHEET NO.
23 OF 25
CLIENT JOB NO.
XXXXXX

1. GATE SHALL BE CONFIGURED TO AUTOMATICALLY OPEN IN THE EVENT OF A POWER FAILURE.
2. INSTALL SENSING EDGES ON BOTH GATE ENDS (FRONT AND BACK POST).

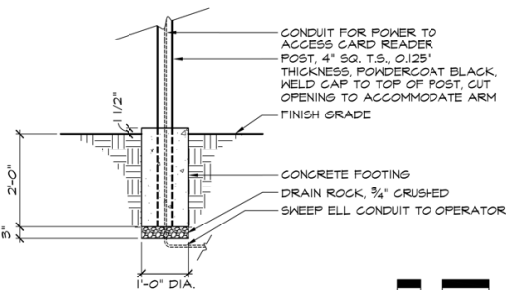
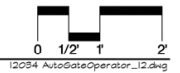


ELEVATION: LOOKING SOUTH FROM CORPORATION YARD INTERIOR

1 AUTOMATIC CHAIN LINK GATE
L-5 ELEVATION



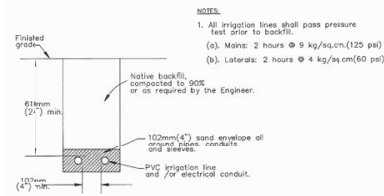
2
L-5 GATE OPERATOR FOUNDATION
SECTION



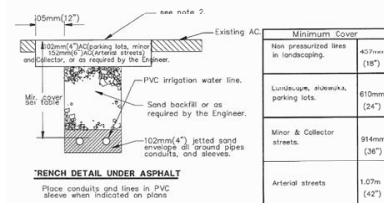
3 ENTRY PAD POST
L-5 SECTION



4 TRENCHING
L-5 CITY STANDARD



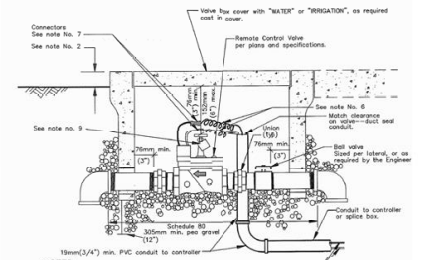
TRENCH DETAIL IN LANDSCAPE AREAS



Minimum Cover	
Non pressurized lines in landscaping.	457mm (18")
Sanitary, storm, sewer, water, parking lots.	610mm (24")
Minor & Collector streets.	914mm (36")
Arterial streets	1.07m (42")

All units are in metric
Non-metric units are in brackets

5 REMOTE CONTROL VALVE
L-5 CITY STANDARD




NO

1. A threaded pipe and fittings shall be schedule 40 PVC. Solvent welded joints and fittings shall be approved by the manufacturer.
2. Pipes shall be 1/2" for hand service and 3/4" for general use as recommended by the manufacturer.
3. Close gaskets shall not be used.
4. Pipe gaskets shall cover valve face gaskets to prevent seal injury.
5. Valve gaskets shall be made of a material resistant to compressed air in compressed areas and reinforced concrete in paved areas and traffic. rigid steel checker plates in areas subject to vehicle traffic.
6. Provide 1/4" (3/16") of excess air in one inch diameter drill for each conductor.
7. Meter gaskets shall not be used in areas subject to vehicle traffic.
8. The color of new fittings, needles and control conductor shall remain the same throughout the life of the installation. Check station controls are to be homogeneous with the color of the control area.
9. Each valve shall be tagged by the appropriate letters and numbers to identify conductor and station number according to specifications.
10. Approximate valve compound shall be used on all threaded connections. The filler shall not be used.
11. Valve boxes to ground cover/drains shall be woven polypropylene. Set boxes approximately 1/2" above grade, inside, outside, and inside.

All units are in metric
 (Non-metric units are in brackets)

5 REMOTE CONTROL VALVE N.T.S.
L-5 CITY STANDARD

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				ZK
				DRAWN BY:
				ZK
				CHECKED BY:
				MS



Callander Associates
Landscape Architecture

311 Seventh Ave.
San Mateo, CA 94403
750.379.1313
FOLLOW@CALL

40
YEARS

PREPARED UNDER THE
DIRECT SUPERVISION
OF:

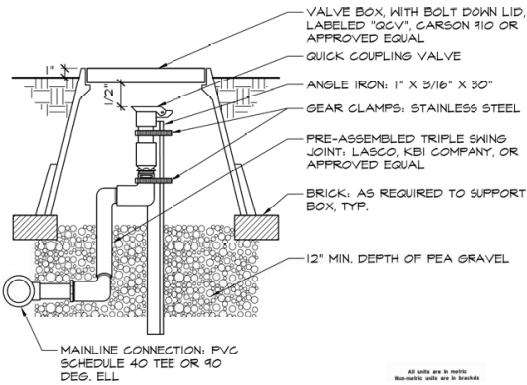


CITY OF MENLO PARK
ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

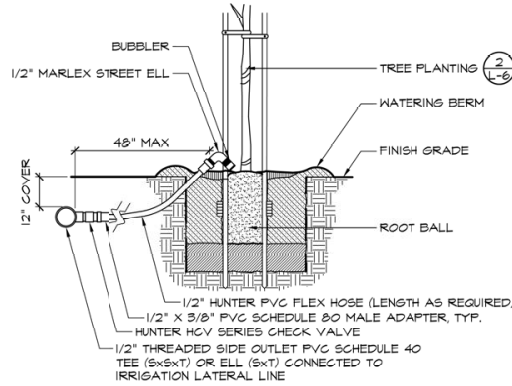
EMERGENCY BACKUP WELL FACILITY
AT CITY CORPORATION YARD

LANDSCAPE DETAILS

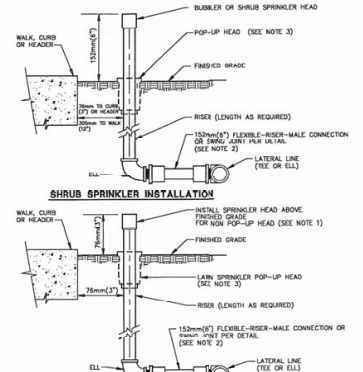
DRAWING NO.	L-5
SHEET NO.	24 OF 25
CLIENT JOB NO.	XXXXXX



1
L-6 **QUICK COUPLING VALVE** **CITY STANDARD** **N.T.S.**
13009 QuickCouplingValve_4.dwg

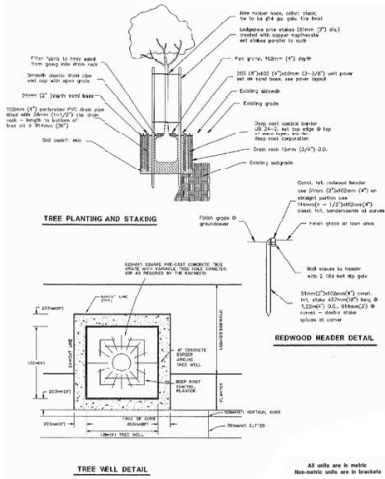


2
L-6 **TREE BUBBLER** **SECTION** **N.T.S.**
13009 Bubbler.dwg

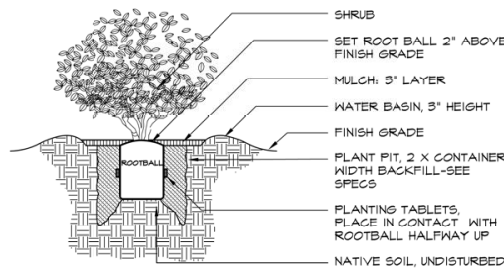


- NOTES:**
1. INSTALL LAWN SPRINKLER HEADS 75mm (3") ABOVE FINISHED GRADE, BEFORE THE FIRST MOWING ADJUST ALL LAWN HEADS TO FINISHED GRADE (NON POP-UP HEADS ONLY).
 2. THE MAKE AND MANUFACTURER OF FLEXIBLE RISERS SHALL BE APPROVED BY THE ENGINEER PRIOR TO PURCHASE, UNLESS OTHERWISE SPECIFIED ON THE PLANS, USE OF THE FLEXIBLE RISER FOR THE TIGHT JOINT IS OPTIONAL.
 3. FOR POP-UP HEADS, BODY SHALL BE 60mm (2 1/4") ABOVE FINISHED GRADE.
 4. ALL PIPE AND FITTINGS SHALL BE SCHEDULE 80 PVC UNLESS OTHERWISE SPECIFIED.
 5. ELLS AND RISERS SHALL BE OF THE SAME MATERIAL.

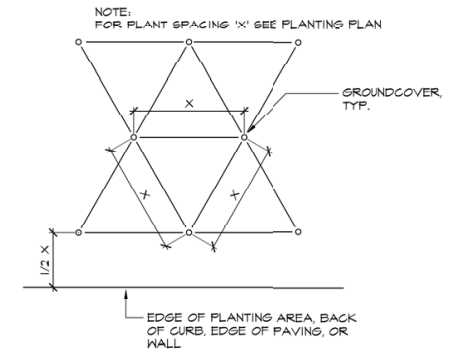
3
L-6 **SPRAY HEAD** **SECTION** **N.T.S.**
13009 SprayHead.dwg



4
L-6 **TREE STAKING** **CITY STANDARD** **N.T.S.**
13009 TreeStaking.dwg



5
L-6 **SHRUB PLANTING** **SECTION** **N.T.S.**
13009 ShrubPlanting_4B.dwg



6
L-6 **GROUND COVER SPACING** **PLAN** **N.T.S.**
13009 GroundcoverSpacing_4B.dwg

NO.	DESCRIPTION	DATE	APPROVED	DESIGNED BY:
				ZK
				DRAWN BY:
				ZK
				CHECKED BY:
				MS



PREPARED UNDER THE
OF:
DATE



CITY OF MENLO PARK
ENGINEERING DIVISION
701 LAUREL STREET, MENLO PARK, CA 94025-3483
PHONE (650) 330-6740 FAX (650) 327-5497

EMERGENCY BACKUP WELL FACILITY AT CITY CORPORATION YARD	DRAWING NO. L-6
LANDSCAPE DETAILS	SHEET NO. 25 OF 25
	CLIENT JOB NO. XXXXXX

PLANNING APPLICATION SUBMITTAL
APRIL 2016

RECEIVED

ATTACHMENT E

JUL 01 2015



CITY OF MENLO PARK COMMUNITY DEVELOPMENT DEPARTMENT PLANNING DIVISION

Corp Yard Emergency Well Project
Contact: Josh Lazarus, 510-574-0820 x109
Infrastructure Engineering Corporation
Applicant: Pam Lowe, P.E.
City of Menlo Park, Public Works Department
701 Laurel Street
Menlo Park, CA 94025

701 Laurel Street
Menlo Park, CA 94025
phone: (650) 330-6702
fax: (650) 327-1653
planning@menlopark.org
<http://www.menlopark.org>

HAZARDOUS MATERIALS INFORMATION FORM

In order to help inform City Staff and the external reviewing agencies, the Planning Division requires the submittal of this form. If the use permit application is approved, applicants are required to submit the necessary forms and obtain the necessary permits from the Menlo Park Fire Protection District, San Mateo County Environmental Health Services Division, West Bay Sanitary District, and other applicable agencies. Please complete this form and attach additional sheets as necessary.

1. List the types of hazardous materials by California Fire Code (CFC) classifications. This list must be consistent with the proposed Hazardous Materials Inventory Statement (HMIS), sometimes referred to as a Chemical Inventory. (The HMIS is a separate submittal.)

Paving media would be used for construction, diesel fuel and lubricants would be used for the back-up generator, and some pesticides for the newly installed landscaping. Similar substances are currently stored at the Corp Yard and regulated under the HMBP. Small quantities of ammonia and chlorine will be brought to the site for the disinfection system, but only when the Corp Yard Emergency Well is activated for emergency potable water purposes.

2. Describe how hazardous materials are handled, stored and monitored to prevent or minimize a spill or release from occurring (e.g., secondary containment, segregation of incompatibles, daily visual monitoring, and flammable storage cabinets).

Diesel fuel, lubricants, and pesticides would be stored in accordance with the existing HMBP. Ammonia will be delivered via delivery truck and chlorine in hypochlorite containers to be used for the chloramination water disinfection system. If it is brought to the site, it is proposed that hypochlorite would be stored in an existing chemical storage shed per the current HMBP, which is to be relocated to the southernmost of the two sheds.

3. Identify the largest container of chemical waste proposed to be stored at the site. Please identify whether the waste is liquid or solid form, and general safeguards that are used to reduce leaks and spills.

Diesel fuel, lubricants, and pesticides would be stored in accordance with the existing HMBP. No additional chemical waste will be stored at the site for the Corp Yard Emergency Well Project as part of an ongoing basis. Hazardous materials will be identified per Section II. Activities Declaration on the Unified Program Consolidation Form.

4. Please explain how hazardous waste will be removed from the site (i.e. licensed haulers, or specially trained personnel).

Diesel fuel, lubricants, and pesticides would be removed from the site in accordance with the existing HMBP as well as chlorine and ammonia during emergency purposes. Which includes removal per Section II. Activities Declaration on the Unified Program Consolidation Form.

5. Describe employee training as it pertains to the following:

- a. Safe handling and management of hazardous materials or wastes;
- b. Notification and evacuation of facility personnel and visitors;
- c. Notification of local emergency responders and other agencies;
- d. Use and maintenance of emergency response equipment;
- e. Implementation of emergency response procedures; and
- f. Underground Storage Tank (UST) monitoring and release response procedures.

Employee training will be in compliance with the existing HMBP. The City's adopted emergency evacuation plan for the Corp Yard directs staff to assemble in the front parking lot following building evacuation. Training will be provided within 6 months for new hires; amended as necessary prior to change in process or work assignment, and given upon modification to the Emergency Response I Plan. Evacuation route maps will be posted as required.

6. Describe documentation and record keeping procedures for training activities.

Documentation and record keeping procedures will be in compliance with existing Corp Yard HMBP on file with San Mateo County Department of Public Health. MSDS will be used documentation of hazardous materials.

7. Describe procedures for notifying onsite emergency response personnel and outside agencies (e.g. Fire, Health, Sanitary Agency-Treatment Plant, Police, State Office of Emergency Services "OES") needed during hazardous materials emergencies.

Notification will be in compliance with existing Corp Yard HMBP. Internal notification will include verbal warnings and portable radio. External notification will include: activating internal facility alarms or communications systems, to notify all facility personnel, notifying appropriate local authorities (i.e., call 9-1-1), notifying the California Emergency Management Agency at (800) 852-7550. Additional agency phone numbers listed in HMBP.

8. Describe procedures for immediate inspection, isolation, and shutdown of equipment or systems that may be involved in a hazardous materials release or threatened release.

Procedures will be in compliance with existing Corp Yard HMBP on file with San Mateo County Department of Public Health which include: Monitoring for leaks, ruptures, pressure build-up, etc.; built-in berm in work storage area; shut-off all water, gas, electrical utilities as appropriate; call 9-1-1 for public emergency responder assistance/medical aid; notify and evacuate all persons in threatened areas. Wash decontaminate equipment.

9. Identify the nearest hospital or urgent care center expected to be used during an emergency.

Per the existing HMBP, Stanford Medical Center will be used during an emergency.

v:\handouts\approved\hazardous materials information form.doc



Menlo Park Fire Protection District Fire Prevention Bureau

170 Middlefield Road

Menlo Park, CA 94025

Website: www.menlofire.org

Date: October 7, 2015

Applicant: City of Menlo Park

Contact name: David Hogan

Phone:

Project: Burgess Corp Yard – Generator

Address: 333 Burgess

City: Menlo Park

Accepted ☒ X ☐

Scope: Generator installation

Permit #:

Reviewed by: Jon Johnston 650-688-8431

Proposed is installation of an Above Ground Storage Tank housed with a Generator. Proposed is a HMMP. The project is to comply with the 2013 CA Building / Fire Codes and local amendments. The following plan review comments are applicable to this submittal:

1. CFC Annual Operational Permit is required for the Hazardous Materials Management Plan (HMMP), and the Above Ground Storage Tank. This shall be submitted to MPFD prior to operational use.
 - a. The Above Ground Storage of diesel in quantities of 55 gallons or greater, require a San Mateo County Hazardous Materials Business Plan (HMBP). HMBP's must be electronically submitted through the San Mateo County Environmental Health Department portal. Contact County Environmental Health for HMBP reporting details.
2. The applicant shall meet all requirements of NFPA 110 Standards and the 2013 California Fire Code that apply to project.
3. Fuel tank primary vent shall terminate not less than 12 feet above grade. Fuel tank emergency vents shall not discharge within the generator housing and shall be equipped with emergency pressure relief device(s) rated to meet the tank manufacturer's specifications listed on the fuel tank data plate.
4. The generator shall be seismically anchored in accordance with the Building Code, using a 1.25 importance factor.
5. The tank shall have secondary containment, and shall meet the requirements of UL 142 for above ground tanks.
6. There shall be a method to prevent over filling the fuel tank.
7. Alarms shall be installed for over filling the fuel tank and for spillage into the secondary space. Alarms shall be tested in the presence of Fire District Inspectors. Alarms shall be monitored.

The applicant shall interconnect all monitoring devices of the generator to the building fire alarm system and they shall report to a Central Monitoring Company.

8. The primary and secondary fuel tanks shall be pneumatically tested on site, at a pressure of not less than 5 psi (or manufacturer's published limits, if lower) for a time of not less than 10 minutes. Certificates from the manufacturer of compliance with this test are NOT acceptable.
9. The fuel tank shall be labeled "DIESEL" and the entrance to the generator enclosure shall be posted with a NFPA 704 Diamond indicating a Blue 0, Red 2, Yellow 0 and White "Diesel"
10. **There shall be no fuel delivered to the tank until the above conditions are met and the Fire District Permit has been signed off.**
11. A Menlo Park Fire District Annual Permit for diesel fuel storage is required. Your business will be administratively added to the Annual Permit listing. Permit fees will be invoiced annually, and Code Compliance inspections will be conducted at least once a year.

Upon completion of work and prior to occupancy, **contact Fire Marshal Jon Johnston of the Menlo Park Fire Protection District at 688-8431 to schedule your inspection.** 48 HOURS NOTICE IS REQUIRED FOR ALL INSPECTIONS.

Nothing in this review is intended to authorize or approve any aspects of the design or installation that do not strictly comply with all applicable codes and standards. Menlo Park Fire Protection District is not responsible for inadvertent errors or omissions pertaining to this review and/or subsequent field inspection(s) i.e., additional comments may be added during subsequent drawing review or field inspection. Please call if there are any questions.



**DEVELOPMENT SERVICES
PLANNING DIVISION**

**Contact: Michele Morris 650-330- 6724 or
mtmorris@menlopark.org**

701 Laurel Street
Menlo Park, CA 94025
PHONE (650) 330-6702
FAX (650) 327-1653

**AGENCY REFERRAL FORM
RETURN DUE DATE: Thursday, July 23, 2015**

DATE: July 9, 2015

TO: SAN MATEO COUNTY ENVIRONMENTAL HEALTH SERVICES DIVISION

Darrell Cullen, Hazardous Materials Specialist
San Mateo County Environmental Health
2000 Alameda de las Pulgas, Ste 100
San Mateo, CA 94403
(650) 372-6235

Applicant	City of Menlo Park
Applicant's Address	701 Laurel Street
Telephone/FAX	Tel: 650-330-6745
Contact Person	Pam Lowe
Business Name	City of Menlo Park
Type of Business	Municipality
Project Address	Corporation Yard, 333 Burgess Drive, Menlo Park, CA 94025
FOR OFFICE USE ONLY	
<input type="checkbox"/> The hazardous materials listed are not of sufficient quantity to require approval by this agency.	
<input type="checkbox"/> The Health Department has reviewed the applicant's plans and use of listed hazardous materials/chemicals and has found the proposal to be in compliance with all applicable Codes.	
<input checked="" type="checkbox"/> The Health Department has reviewed the applicant's plans and use of listed hazardous materials/chemicals outlined, and suggests conditions and mitigation measures to be made a part of the City's Use Permit approval (please list the suggested conditions and mitigation measures). The Health Department will inspect the facility once it is in operation to assure compliance with applicable laws and regulations.	
The applicant's proposal has been reviewed by the San Mateo County Environmental Health Services Division by:	
Signature/Date	Name/Title (printed)
Comments: Insure to add diesel volumes to your HMBP once filled	



DEVELOPMENT SERVICES PLANNING DIVISION

701 Laurel Street
Menlo Park, CA 94025
PHONE (650) 858-3400
FAX (650) 327-5497

AGENCY REFERRAL FORM

DATE: July 19th, 2016

TO: WEST BAY SANITARY DISTRICT
500 Laurel Street
Menlo Park, CA 94025
(650) 321-0384


Applicant	City of Menlo Park
Applicant's Address	701 Laurel Street
Telephone/FAX	650-330-6745
Contact Person	Pam Lowe
Business Name	City of Menlo Park
Type of Business	Municipality
Project Address	Corporation Yard, 333 Burgess Drive, Menlo Park, CA 94025

FOR OFFICE USE ONLY

- ☐ The hazardous materials listed are not of sufficient quantity to require approval by this agency.
- ☒ The Sanitary District has reviewed the applicant's proposed plans and use of listed hazardous materials/chemicals and has found that the proposal meets all applicable Code requirements.
- ☐ The Sanitary District has reviewed the applicant's plans and use of listed hazardous materials/chemicals outlined, and suggests conditions and mitigation measures to be made a part of the City's Use Permit approval (please list the suggested conditions and mitigation measures).

The applicant's proposal has been reviewed by the West Bay Sanitary District by: Jed Beyer
Inspector

Signature/Date

 7-19-16

Name/Title (printed)

Phil Scott / District Manager




**DEVELOPMENT SERVICES
PLANNING DIVISION**

**Contact: Michele Morris 650-330- 6724 or
mtmorris@menlopark.org**
701 Laurel Street
Menlo Park, CA 94025
PHONE (650) 330-6702
FAX (650) 327-1653

**AGENCY REFERRAL FORM
RETURN DUE DATE: Thursday, July 23, 2015**

DATE: July 9, 2015

TO: CITY OF MENLO PARK BUILDING DIVISION
701 Laurel Street
Menlo Park, CA 94025
(650) 330-6704

Applicant	City of Menlo Park
Applicant's Address	701 Laurel Street
Telephone/FAX	Tel: 650-6745
Contact Person	Pam Lowe
Business Name	City of Menlo Park
Type of Business	Municipality
Project Address	Corporation Yard, 333 Burgess Drive, Menlo Park, CA 94025
FOR OFFICE USE ONLY	
<input type="checkbox"/> The hazardous materials listed are not of sufficient quantity to require approval by this Division.	
<input checked="" type="checkbox"/> The Building Division has reviewed the applicant's plans and listed hazardous materials/chemicals and has found that the proposal meets all applicable California Building Code requirements.	
<input type="checkbox"/> The Building Division has reviewed the applicant's plans and use of listed hazardous materials/chemicals outlined, and suggests conditions and mitigation measures to be made a part of the City's Use Permit approval (please list the suggested conditions and mitigation measures).	
The applicant's proposal has been reviewed by the City of Menlo Park's Building Division by:	
Signature/Date  7/20/15	Name/Title (printed) Ron LaFrance, Assistant Community Development Director
Comments:	



STAFF REPORT

Planning Commission

Meeting Date:

7/25/2016

Staff Report Number:

16-062-PC

Public Hearing:

Use Permit Revision/Calysta Energy/1140 O'Brien Drive, Suite B

Recommendation

Staff recommends that the Planning Commission approve a request for a revision to a use permit, previously approved in July 2014, for hazardous materials used and stored within an existing building in the M-2 (General Industrial) zoning district at 1140 O'Brien Drive. The revision would include modifications to the quantities and types of hazardous materials, along with the location of the use and storage of hazardous materials. All hazardous materials would continue to be used and stored within the building. The recommended actions are contained within Attachment A.

Policy Issues

Each use permit request is considered individually. The Planning Commission should consider whether the required use permit findings can be made for the proposal.

Background

Site location

The subject site is located at 1140 O'Brien Drive, between Kelly Court and Casey Court. A location map is included as Attachment B. The subject site is one of two suites in the building, which is addressed 1140A O'Brien Drive and 1140B O'Brien Drive. DNA2.0 was most recently located in Suite A, and received use permit approval for hazardous materials in 2010 and 2012 at this location. This suite is currently vacant. Calysta Energy (a spin-off from DNA 2.0) is located in Suite B and is expanding into a portion of Suite A.

The immediately adjacent parcels along O'Brien Drive are also part of the M-2 zoning district, and are occupied by a variety of warehouse, light manufacturing, research and development (R&D), and office uses. A number of these properties also use hazardous materials, similar to the subject proposal. The properties to the rear of the subject site, along Albern Street, are located in East Palo Alto and contain residential land uses. Additionally, the Girls Club of the Mid-Peninsula, which is located within the City of Menlo Park but accessed from Ralmar Avenue in East Palo Alto, is located to the rear of the subject site. Green Oaks Academy, a K-4th grade public school in the Ravenswood School District, is located at the end of Ralmar Avenue in East Palo Alto, approximately 350 feet from the subject suite.

Analysis

Project description

Calysta Energy is a biotech company that uses natural gas as a feedstock to create essential building blocks for sustainable fuels and chemicals. Calysta's products aim to enable creation of sustainable biofuels and industrial chemicals from natural gas, reducing petroleum dependence without competing for food, land, or water. The company is an outgrowth of DNA2.0, a company previously located at the site. At this time, Calysta is expanding into a portion of the vacant space and is using this expansion to update its hazardous materials inventory. The applicant has submitted a project description letter (Attachment C), which describes the project proposal in more detail.

Hazardous materials

The applicant is comprehensively updating its hazardous materials inventory. Hazardous materials used at the site would include carcinogens, corrosives, combustibles, flammable gases, flammable solids, other flammables, highly toxic chemicals, non-flammable gases, oxidizers, and toxics. The applicant has submitted a chemical inventory (Attachment F) that identifies the complete list of hazardous materials.

The project plans (Attachment D) identify the location of the chemical storage and safety equipment. The plans identify the location of safety equipment, such as spill kits, emergency showers, and the location of hazardous waste storage, gas cylinders, and exit routes. The Hazardous Materials Information Form (HMIF) for the project is provided as Attachment E. The HMIF contains a description of how hazardous materials are stored and handled on-site, including the storage of hazardous materials within fire-rated storage cabinets, segregated by hazard class. The applicant indicates that the storage areas would be monitored by lab staff and weekly documented inspections would be performed. The largest waste container would be a 15-gallon container, and all liquid wastes would be secondarily contained. A spill kit would be stored on-site. Licensed contractors are intended to be used to haul off and dispose of the hazardous waste. The HMIF includes a discussion of the applicant's intended training plan, which encompasses the handling of hazardous materials and waste, as well as how to respond in case of an emergency. The applicant indicates that the procedures for notifying emergency response personnel and outside agencies are kept in the site's emergency response plan. Given the proximity of the site to the Hetch Hetchy Right-Of-Way and pipeline, the San Francisco Public Utilities Commission would be included in the emergency contact list. A complete list of the types of chemicals is included in Attachment F.

Staff has included recommended conditions of approval that would limit changes in the use of hazardous materials, require a new business to submit a chemical inventory to seek compliance if the existing use is discontinued, and address violations of other agencies in order to protect the health and safety of the public. Staff believes that the proposed use and storage of hazardous materials would be consistent with other uses in the area.

Agency Review

The Menlo Park Fire Protection District, City of Menlo Park Building Division, West Bay Sanitary District, and San Mateo County Environmental Health Services Division were contacted regarding the proposed expansion and modification to the types and quantities of hazardous materials use at the site. Their

correspondence has been included as Attachment G. Each entity found the proposal to be in compliance with all applicable standards. Although the subject parcel is located in proximity to residences and schools, there would be no unique requirements for the proposed use, based on the specific types and amounts of chemicals that are proposed.

Correspondence

Staff has not received any correspondence on this project.

Conclusion

Staff believes that the proposed modification to the types and quantities of hazardous materials and the locations of the use and storage would be compatible and consistent with other uses in this area. The proposal would allow for an existing business to continue to expand and grow within Menlo Park. The Hazardous Materials Information Form and chemical inventory have been reviewed and approved by the relevant agencies, and include a discussion of the applicant's training plan and protection measures in the event of an emergency. Staff recommends that the Planning Commission approve the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 1,320-foot radius of the subject property.

Attachments

- A. Recommend Actions
- B. Location Map
- C. Project Description Letter
- D. Project Plans
- E. Hazardous Materials Information Form (HMIF)
- F. Chemical Inventory
- G. Hazardous Materials Agency Referral Forms:
 - Menlo Park Fire Protection District
 - San Mateo County Environmental Health Department
 - West Bay Sanitary District

- Menlo Park Building Division

Report prepared by:
Kyle Perata, Senior Planner

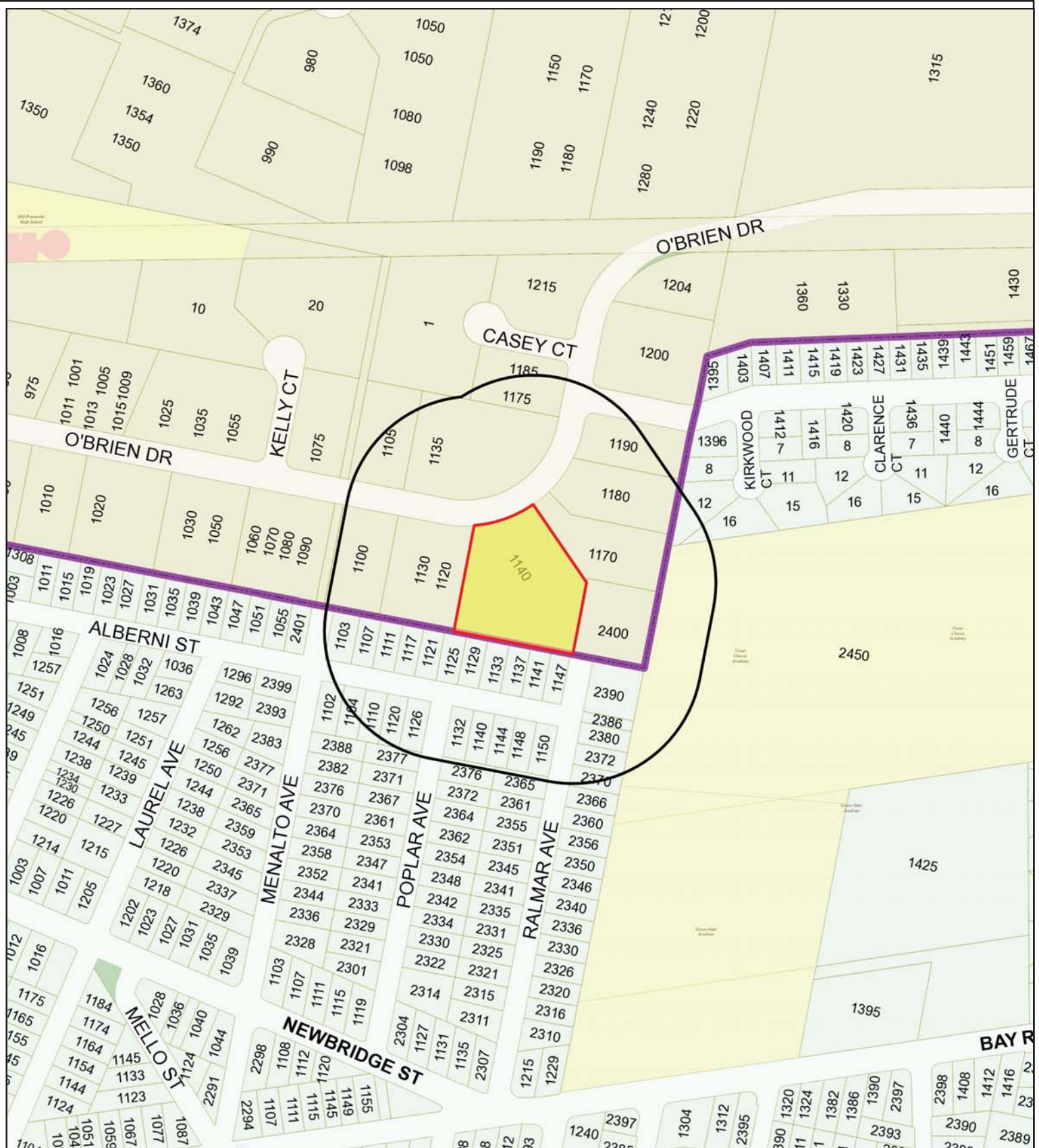
Report reviewed by:
Thomas Rogers, Principal Planner

1140 O'Brien Drive – Attachment A: Recommended Actions

LOCATION: 1140 O'Brien Drive	PROJECT NUMBER: PLN2016-00053	APPLICANT: Lori Giver for Calysta Energy	OWNER: O'Brien Drive Portfolio
REQUEST: Request for a revision to a use permit, previously approved in July 2014, for hazardous materials used and stored within an existing building in the M-2 (General Industrial) zoning district. The revision would include modifications to the quantities and types of hazardous materials, along with the location of the use and storage of hazardous materials. All hazardous materials would continue to be used and stored within the building.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: <ol style="list-style-type: none">1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.3. Approve the use permit subject to the following standard conditions:<ol style="list-style-type: none">a. Development of the project shall be substantially in conformance with the plans provided by DES Architects/Engineers, consisting of six plan sheets, dated received July 14, 2016, as well as the Hazardous Materials Information Form (HMIF), dated received April 18, 2016, approved by the Planning Commission on July 25, 2016 except as modified by the conditions contained herein, subject to review and approval of the Planning Division.b. Prior to building permit issuance, the applicant shall comply with all sanitary district, Menlo Park Fire Protection District, and utility companies regulations that are directly applicable to the project.c. Prior to building permit issuance, the applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project. If there is an increase in the quantity of hazardous materials on the project site, a change in the location of the storage of the hazardous materials, or the use of additional hazardous materials after this use permit is granted, the applicant shall apply for a revision to the use permit.d. If there is an increase in the quantity of hazardous materials on the project site, a change in the location of the storage of the hazardous materials, or the use of additional hazardous materials after this use permit is granted, the applicant shall apply for a revision to the use permit.e. Any citation or notification of violation by the Menlo Park Fire Protection District, San Mateo County Environmental Health Department, West Bay Sanitary District, Menlo Park Building Division or other agency having responsibility to assure public health and safety for the use of hazardous materials will be grounds for considering revocation of the use permit.f. If the business discontinues operations at the premises, the use permit for hazardous materials shall expire unless a new business submits a new hazardous materials information form and chemical inventory to the Planning Division for review by the applicable agencies to			

1140 O'Brien Drive – Attachment A: Recommended Actions

LOCATION: 1140 O'Brien Drive	PROJECT NUMBER: PLN2016-00053	APPLICANT: Lori Giver for Calysta Energy	OWNER: O'Brien Drive Portfolio
REQUEST: Request for a revision to a use permit, previously approved in July 2014, for hazardous materials used and stored within an existing building in the M-2 (General Industrial) zoning district. The revision would include modifications to the quantities and types of hazardous materials, along with the location of the use and storage of hazardous materials. All hazardous materials would continue to be used and stored within the building.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: determine whether the new hazardous materials information form and chemical inventory are in substantial compliance with the use permit.			



City of Menlo Park

Location Map
Calysta Energy



Scale: 1:3,600

Drawn By: KTP

Checked By: KTP

Date: 7/25/2016

Sheet: 1

CALYSTA

February 23, 2016

City of Menlo Park
Planning Division
701 Laurel St.
Menlo Park, CA 94025

RECEIVED

APR 18 2016

CITY OF MENLO PARK
BUILDING

Re: Calysta, Inc Business Plan

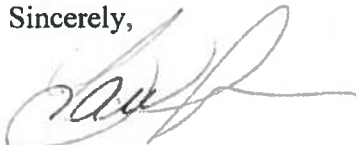
Dear Planning Division:

This project consists of a company located at 1140 O'Brien Dr. Suite B, named Calysta, Inc. Calysta uses methane (from natural gas and biogas) as a feedstock to create essential building blocks for high value sustainable chemicals and animal feed products. Calysta's proprietary biological conversion platforms enables creation of these desired products from alternative sources, reducing dependence on petroleum for chemical products or upon natural resources (such as food, land, and water) for animal feed products.

The facilities in Menlo Park is Calysta's headquarters and site of R&D work for new products. This R&D work focuses on molecular biology, metabolic engineering, and small scale gas-fed fermentation. No manufacturing occurs at the site. Currently the site is home to 30 employees, 24 of whom work in the laboratory facilities. With our recent series C financing completed, we are expanding the company, and are planning for a total of 48 people at the site by the end of 2016, with 40 of those employees working in the laboratory.

Should you have any additional questions regarding this project, you may address them to Catherine Smith, office manager, or Lori Giver.

Sincerely,



Lori Giver
VP Biological Engineering
Calysta, Inc.

Calysta HazMat CUP Revision
1140 O'BRIEN DRIVE SUITE A MENLO PARK, CA 94025

SHEET INDEX

- NON-RESIDENTIAL ENERGY STANDARDS COMPLIANCE
TITLE 24, PARTS 1 AND 9 OF THE CALIFORNIA CODE OF REGULATIONS
APPLIES TO BUILDING ENVELOPE REQUIREMENTS

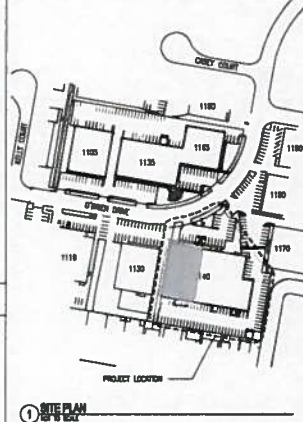
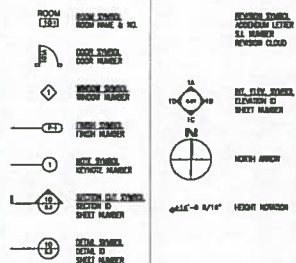
GENERAL	
00.01	TITLE SHEET
ARCHITECTURAL	
A1.01	EXISTING SITE PLAN (FOR REFERENCE ONLY)
A2.01	FIRST LEVEL FLOOR PLAN
A2.02	ROOF LEVEL PLAN
A3.01	EXTERIOR ELEVATIONS

RECEIVED

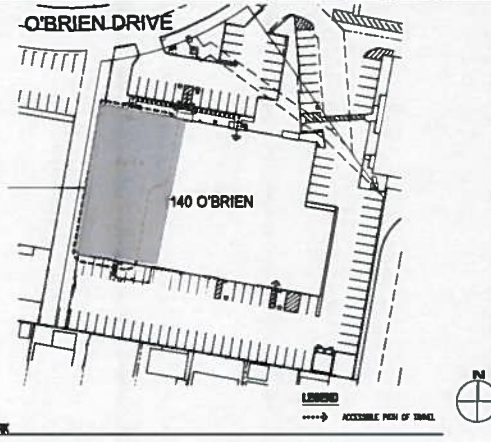
JUL 14 2016

CITY OF MENLO PARK
BUILDING

SITE PLAN



AREA OF WORK



PROJECT DATA

BUILDING CODES:
 2010 CA. BUILDING CODE (CBC)
 2010 CA. ELECTRICAL CODE (CEC)
 2010 CA. MECHANICAL CODE (CMC)
 2010 CA. PLUMBING CODE (CPC)
 2010 CA. FIRE CODE WITH LOCAL AMENDMENTS
 2010 CA. GREEN BUILDING CODE (CAL GREEN)
 2008 STATE OF CALIFORNIA TITLE-24 ENERGY REGULATIONS

LEGAL JURISDICTION:	CITY OF MEDA PARK, CA
PLANNING ZONE:	B-2
CONSIDERATION TYPE:	IN-2
BUILDING OCCUPANCY:	OCCUPANCY GROUP II
FIRE PROTECTION:	FIRE SPRINKLER SYSTEM THROUGHOUT
BUILDING AREA:	FIRST LEVEL = 20,304 SF MEZZANINE LEVEL = 500 SF
PARKING PROVIDED AND REQUIRED (1 PER 300)	REQUIRED = - 81 SPACES LESS 4 SPACES = -4 TOTAL REQUIRED PARKING = 77 SPACES
EXISTING SUBMITTALS	FIRE SPRINKLER AND FIRE ALARM SYSTEM
AREA OF AMENDMENT	2,302 SF

For Reference Only



TARLTON
PROPERTIES
1830 O'Brien Drive, Suite C
Merido Park, CA 94025

**O'BRIEN DRIVE
PORTFOLIO LLC C/O
TARLTON PROPERTIES**

Calysta
1140 O'Brien Drive
Menlo Park, CA 94025

TITLE SHEET[illegible]

DRAWN BY: G. HOLLIN
 REVISION BY: T. PLUMBICA
 APPROVED BY:
 DIS-PROJECTING: P2011A2

3/9/16

G0.01



For Reference Only

A5.01



RECEIVED

APR 18 2016

CITY OF MENLO PARK
BUILDING

**COMMUNITY DEVELOPMENT DEPARTMENT
PLANNING DIVISION**

701 Laurel Street
Menlo Park, CA 94025
phone: (650) 330-6702
fax: (650) 327-1653
planning@menlopark.org
<http://www.menlopark.org>

HAZARDOUS MATERIALS INFORMATION FORM

In order to help inform City Staff and the external reviewing agencies, the Planning Division requires the submittal of this form. If the use permit application is approved, applicants are required to submit the necessary forms and obtain the necessary permits from the Menlo Park Fire Protection District, San Mateo County Environmental Health Services Division, West Bay Sanitary District, and other applicable agencies. Please complete this form and attach additional sheets as necessary.

1. List the types of hazardous materials by California Fire Code (CFC) classifications. This list must be consistent with the proposed Hazardous Materials Inventory Statement (HMIS), sometimes referred to as a Chemical Inventory. (The HMIS is a separate submittal.)

See attached spreadsheet.

The Hazardous Materials Business Plan (HMBP) will be updated once the changes to the facility have been made.

2. Describe how hazardous materials are handled, stored and monitored to prevent or minimize a spill or release from occurring (e.g., secondary containment, segregation of incompatibles, daily visual monitoring, and flammable storage cabinets).

Flammable materials will be stored within rated storage cabinets and segregated by hazard class. Storage areas for chemicals will be monitored by lab staff during normal business hours (visual). Weekly documented inspections of hazardous waste storage areas are performed.

3. Identify the largest container of chemical waste proposed to be stored at the site. Please identify whether the waste is liquid or solid form, and general safeguards that are used to reduce leaks and spills.

The largest waste container will be 15-gallon capacity. All liquid wastes are secondarily contained, and a Spill Kit is stored on site.

4. Please explain how hazardous waste will be removed from the site (i.e. licensed haulers, or specially trained personnel).

Licensed waste haulers are used. If Calysta qualifies as a Very Small Quantity Generator, it may use the San Mateo County VSQG disposal program.

5. Describe employee training as it pertains to the following:

- a. Safe handling and management of hazardous materials or wastes;
- b. Notification and evacuation of facility personnel and visitors;
- c. Notification of local emergency responders and other agencies;
- d. Use and maintenance of emergency response equipment;
- e. Implementation of emergency response procedures; and
- f. Underground Storage Tank (UST) monitoring and release response procedures.

Lab employees receive training on management of chemicals and waste. All employees receive training on what to do in case of emergencies, including chemical spills. The site's emergency response plan includes procedures to notify first responders and make reports to outside agencies. There are no USTs at the site.

6. Describe documentation and record keeping procedures for training activities.

All training is documented, and training records are kept by the Safety Committee.

7. Describe procedures for notifying onsite emergency response personnel and outside agencies (e.g. Fire, Health, Sanitary Agency-Treatment Plant, Police, State Office of Emergency Services "OES") needed during hazardous materials emergencies.

The procedures for notifying emergency response personnel and outside agencies are contained in the site's written emergency response plan. This plan describes various emergency scenarios and specifically who to call and how to respond, internally and in conjunction with responding agencies, including SFPUC.

8. Describe procedures for immediate inspection, isolation, and shutdown of equipment or systems that may be involved in a hazardous materials release or threatened release.

EHS/Facilities personnel are authorized to shut down utilities if a spill requires such action. Spills are contained using materials from Spill Kit, and if larger than internal capabilities, the outside emergency response contractor is called. If danger exists, MP FPD is also called.

9. Identify the nearest hospital or urgent care center expected to be used during an emergency.

Stanford Hospital, Palo Alto

v:\handouts\approved\hazardous materials information form.doc

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Calysta Chemical Inventory

ATTACHMENT F

APR 18 2016

Item Description	Primary Hazard	Sec. Hazard	Unit	Projected Qty
Chloroform	carcinogen			2 L
Chloroform	carcinogen			8 L
Dichloromethane	carcinogen			1 L
Total Carcinogens				3 gal
4-iodophenol	corrosive			1 L
Adipic acid	corrosive	Comb IIIB		1 L
Ammonia Chloride	corrosive			1 L
Ammonium Hydroxide 28-30%	corrosive	toxic	bottle	1 L
Clorox® Germicidal Bleach, Regular	corrosive		1	12 L
Crotonic acid	corrosive	Comb IIIA		1 L
Dicamba pestanal	corrosive			1 L
DL-Glyceraldehyde 3-phosphate solution, 50mg/mL in	corrosive		1 bottle	1 L
Ethylene Diamine Tetraacetic Acid, Tetrasodium Salt	corrosive			0.5 lb
Hydrochloric Acid	corrosive		1 bottle	1 L
L-Glutamic acid HCl	corrosive			1 L
Manganese (II) chloride tetrahydrate	corrosive			1 L
Methoxyamine hydrochloride (for GC derivatization), 9	corrosive		1 bottle	1 L
N-(4-aminobutyl)acetamide	corrosive			0.5 lb
N-(4-aminopentyl)acetamide	corrosive			0.5 lb
Oxaloacetic acid, 1 gram	corrosive		1 bottle	1 L
PCC-54 Detergent concentrate	corrosive		1 pack	12 L
Phosphoric Acid 85%	corrosive		bottle	1 L
Potassium Hydroxide Solution, 1N	corrosive			1 L
Sodium DL-Malate (Malic acid sodium salt)	corrosive			0.5 lb
Sodium hydroxide	corrosive		1	1 L
Spor-Klenz® Concentrated Sterilant/Disinfectant, STER	corrosive	OX1	Case	4 L
Sulfuric acid	corrosive			2 L
Trichloroacetic acid	corrosive			0.5 lb
Valeric acid	corrosive	Comb IIIB		1 L
Zinc Sulfate	corrosive			0.5 lb
Zinc sulfate heptahydrate	corrosive			1 lb
Waste acids/bases			poly jug, ve	5 gal
Total Corrosives including secondary hazards				23 gal + 8.6 l
Acetic Acid Glacial	Comb II	corrosive		1 L
Acetoin	Comb II			1 L
Acrylic acid	Comb II			1 L
Formic Acid	Comb II	corrosive		1 L
Heptylamine	Comb II	corrosive	1 bottle	1 L
Myrcene	Comb II			1 L
N,N-Dimethyl Formamide	Comb II			4 L
Propionic acid	Comb II	corrosive		1 L
(-)-Methyl L-lactate	Comb II			1 L
Total Combustible II				3 gal

Calysta Chemical Inventory

Item Description	Primary Hazard	Sec. Hazard	Unit	Projected Qty
(-)-Butyl L-lactate	Comb IIIA			1 L
1-Heptanol	Comb IIIA			1 L
1-Hexanol	Comb IIIA			1 L
1-Octanol	Comb IIIA			1 L
2,3-Butanediol	Comb IIIA			1 L
2R,3R-(-)-2,3-Butanediol	Comb IIIA			1 L
Cadaverine	Comb IIIA	corrosive		1 L
Cadaverine, 10 mL, purum >97%	Comb IIIA	corrosive	1 bottle	1 L
Chloroform:Isoamyl alcohol 24:1	Comb IIIA		1 bottle	1 L
Dimethylsulfoxide	Comb IIIA			1 L
Hexamethylenediamine	Comb IIIA	corrosive		1 L
Hexamethylenediamine (1,6-hexanediamine), 25 g, 98	Comb IIIA	corrosive	1 bottle	1 L
Linalool	Comb IIIA			1 L
Methyl benzoate	Comb IIIA			1 L
Pentafluorobenzyl Bromide	Comb IIIA	corrosive		1 L
SimplyBlue™ SafeStain	Comb IIIA		1	2 L
Styrene Oxide	Comb IIIA	toxic		1 L
Total Combustible IIIA including secondary hazards				5.5 gal
1,2-Propanediol	Comb IIIB			1 L
18-crown-6 ether; Analytical Reagent	Comb IIIB		1 ea	1 L
1-Dodecanol	Comb IIIB			1 L
1-Naphthol	Comb IIIB	toxic		1 L
1-Nonanol	Comb IIIB			1 L
Antifoam A concentrate	Comb IIIB			1 L
Antifoam SE-15	Comb IIIB			4L
Brij 58	Comb IIIB			1 L
DMSO, BioReagent grade	Comb IIIB		1 bottle	1 L
Dodecane	Comb IIIB			1 L
Ethylene glycol	Comb IIIB			1 L
Formamide	Comb IIIB			1 L
Glycerol 99%	Comb IIIB		1 bottle	1 L
Glycerol for molecular biology	Comb IIIB			1 L
Hexadecyltrimethylammonium bromide	Comb IIIB	toxic	1 bottle	1 L
IGEPAL CA-630	Comb IIIB			1 L
Lactic Acid	Comb IIIB	corrosive		1 L
Lauric acid	Comb IIIB	corrosive		1 L
Methyl salicylate	Comb IIIB			1 L
Nicotinamide	Comb IIIB			1 L
Nitriloacetic acid	Comb IIIB			1 L
o-Dianisidine	Comb IIIB			1 L
Polyethylene Glycol 3350, NF	Comb IIIB		1 each	1 L
Sodium acetate (anhydrous)	Comb IIIB			1 L
Sodium Benzoate	Comb IIIB			1 L
Spermidine	Comb IIIB	corrosive		1 L
Triton X100	Comb IIIB			1 L

Calysta Chemical Inventory

Item Description	Primary Hazard	Sec. Hazard	Unit	Projected Qty
Tween 20	Comb IIIB			4 L
Tween 80	Comb IIIB			1 L
Total Combustible IIIB including secondary hazards				9.5 gal
Ethylene Oxide	Flam gas	toxic		5 cf
Methane	Flam gas			1600 cf
Propylene	Flam gas	UR1		5 cf
Total Flammable Gas				1610 cf
Acetaldehyde	Flam IA	UR2		1 L
Ethylene	Flam IA			1 L
Ethylene	Flam IA			1 L
Methyl formate	Flam IA			1 L
Pentane	Flam IA			1 L
Total Flammable IA				1.5 gal
2,2,4-Trimethylpentane (isooctane)	Flam IB			1 L
2-Propanol	Flam IB			4 L
Acetone	Flam IB			5 L
Acetonitrile	Flam IB			2 L
Benzene	Flam IB			1 L
Boron trifluoride-methanol solution	Flam IB	corrosive, toxic		1 L
Butyl acetate	Flam IB			2 L
Carbon disulfide	Flam IB			1 L
Ethanol, Absolute (100%) Molecular Grade	Flam IB		1 bottle	30 L
Ethyl Acetate HPLC Grade	Flam IB			6 L
Ethyl Alcohol	Flam IB			4 L
ethyl chloroformate	Flam IB	toxic, corrosive		1 L
Ethylbenzene	Flam IB			4 L
Heptane	Flam IB			1 L
Hexanes (Optima™), Fisher Chemical	Flam IB		1	8 L
Isopropanol	Flam IB			8 L
Methanol	Flam IB			16 L
Methanolic HCl	Flam IB	corrosive, toxic	1	2 L
Methyl chloroformate	Flam IB	corrosive, toxic		1 L
Octane	Flam IB			1 L
Propylene Oxide	Flam IB	corrosive, toxic		1 L
Pyridine	Flam IB			1 L
Toluene	Flam IB			2 L
Triethylamine	Flam IB	corrosive		1 L
Waste solvents			poly jug, ve	5 gal
Total Flammable IB				33 gal
1-Butanol	Flam IC	corrosive		1 L
Isoamyl acetate	Flam IC			2 L
MSTFA (for GC derivatization), 10x1mL ampoules	Flam IC		1 pack	1 L
Propyl chloroformate	Flam IC	corrosive	1 bottle	1 L
Styrene	Flam IC			2 L
Total Flammable IC				2 gal

Calysta Chemical Inventory

Item Description	Primary Hazard	Sec. Hazard	Unit	Projected Qty
1,4-butanediamine (1,1,2,2,3,3,4,4,-D8, 98%), 0.1g	Flam solid	toxic, corrosive	1 vial	.1 lb
Putrescine, 100 mg, analytical standard	Flam solid	corrosive, toxic	1 bottle	.5 lb
Total Flammable Solid				0.6 lb
2,4-Dinitrophenol	Highly toxic		ea	0.5 lb
2-mercaptoethanol	Highly toxic	Comb IIIA		0.5 lb
Total Highly Toxic				1 lb
Air (zero)	NFG			600 cf
Argon	NFG			200 cf
Helium	NFG			600 cf
Total Non-Flammable Gas				1400 cf
Cobalt (II) nitrate hexahydrate	OX 2			1 lb
Total Oxidizer II				1 lb
Potassium nitrate	OX1			2 lb
Sodium Nitrate	OX1		1 bottle	10 lb
Total Oxidizer I including secondary hazards				22 lb
2,4-D (2,4-Dichlorophenoxyacetic acid)	Toxic			0.5 lb
Bis(trimethylsilyl)trifluoroacetamide, 1% trimethylchloro	Toxic			0.25 lb
Carbonyl cyanide 3-chlorophenylhydrazone	Toxic		ea	0.25 lb
Copper (II) sulfate pentahydrate	toxic			0.25 lb
Copper Sulfate	toxic			0.25 lb
Dihydroxyacetone, 100g	Toxic		1 bottle	0.5 lb
Nickel (II) chloride hexahydrate	Toxic			0.25 lb
Nickel (II) sulfate hexahydrate, 100 g, ACS grade	toxic		1 bottle	0.5 lb
Potassium hydroxide	Toxic	Corrosive, WR1		4 lb
Sodium selenate decahydrate	Toxic			0.25 lb
Sodium selenite	Toxic			0.25 lb
waste toxics			poly drum	15 gal
Total Toxic				7.9 lb + 7.5 g
Materials Not Regulated by Fire Code are not listed				

Comparison of Previously Permitted Hazardous Materials with Current Request

A direct comparison of quantities is difficult, as the prior submissions did not fully categorize the hazards of each listed chemical. Also, the prior submittals were organized by department use, and not aggregated by chemical. Since several hundred chemicals entries are involved, the time necessary to collate the data is extensive.

The current request is not a large increase; the primary reason for the CUP revision request is Calysta is expanding to more space within the building, and felt this was an opportune time to review its chemical needs and ensure it can use the materials it needs to continue its research.

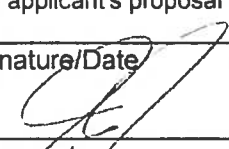


**DEVELOPMENT SERVICES
PLANNING DIVISION**
Contact: Kyle Perata 650-330- 6721 or
ktperata@menlopark.org
 701 Laurel Street
 Menlo Park, CA 94025
 PHONE (650) 330-6702
 FAX (650) 327-1653

AGENCY REFERRAL FORM
RETURN DUE DATE: Thursday, May 12, 2016

DATE: April 28, 2016

TO: MENLO PARK FIRE PROTECTION DISTRICT
 Jon Johnston
 170 Middlefield Road
 Menlo Park, CA 94025
 (650) 323-2407

Applicant	Calysta, Inc.
Applicant's Address	1140 O'Brien Drive, Suite B, Menlo Park, CA 94025
Telephone/FAX	Tel: 650-508-8018 (Consultant)
Contact Person	Ellen Ackerman (1140 O'Brien Drive)
Business Name	Calysta
Type of Business	Research and development of sustainable fuels and chemicals
Project Address	1140 O'Brien Drive, Suite B, Menlo Park, CA 94025
FOR OFFICE USE ONLY	
<input type="checkbox"/> The hazardous materials listed are not of sufficient quantity to require approval by this agency. <input checked="" type="checkbox"/> The Fire District has reviewed the applicant's plans and use of listed hazardous materials/chemicals and has found the proposal to be in compliance with all applicable Fire Codes. <input type="checkbox"/> The Fire District has reviewed the applicant's plans and use of listed hazardous materials/chemicals outlined, and suggests conditions and mitigation measures to be made a part of the City's Use Permit approval (please list the suggested conditions and mitigation measures).	
The applicant's proposal has been reviewed by the Menlo Park Fire Protection District by:	
Signature/Date  5/26/2016	Name/Title (printed) FIRE MARSHAL JON JOHNSTON
Comments:	

SUBJECT TO INITIAL AND ONGOING ANNUAL FIRE DEPT.
 PERMIT AND INSPECTION REQUIREMENTS.



**DEVELOPMENT SERVICES
PLANNING DIVISION**
Contact: Kyle Perata 650-330- 6721 or
ktperata@menlopark.org
701 Laurel Street
Menlo Park, CA 94025
PHONE (650) 330-6702
FAX (650) 327-1653

AGENCY REFERRAL FORM
RETURN DUE DATE: Thursday, May 12, 2016

DATE: April 28, 2016

TO: SAN MATEO COUNTY ENVIRONMENTAL HEALTH SERVICES DIVISION
Darrell Cullen, Hazardous Materials Specialist
San Mateo County Environmental Health
2000 Alameda de las Pulgas, Ste 100
San Mateo, CA 94403
(650) 372-6235

Applicant	Calysta, Inc.
Applicant's Address	1140 O'Brien Drive, Suite B, Menlo Park, CA 94025
Telephone/FAX	Tel: 650-508-8018 (Consultant)
Contact Person	Ellen Ackerman (1140 O'Brien Drive)
Business Name	Calysta
Type of Business	Research and development of sustainable fuels and chemicals
Project Address	1140 O'Brien Drive, Suite B, Menlo Park, CA 94025
FOR OFFICE USE ONLY	
<input type="checkbox"/> The hazardous materials listed are not of sufficient quantity to require approval by this agency.	
<input type="checkbox"/> The Health Department has reviewed the applicant's plans and use of listed hazardous materials/chemicals and has found the proposal to be in compliance with all applicable Codes.	
<input checked="" type="checkbox"/> The Health Department has reviewed the applicant's plans and use of listed hazardous materials/chemicals outlined, and suggests conditions and mitigation measures to be made a part of the City's Use Permit approval (please list the suggested conditions and mitigation measures). The Health Department will inspect the facility once it is in operation to assure compliance with applicable laws and regulations.	
The applicant's proposal has been reviewed by the San Mateo County Environmental Health Services Division by:	
Signature/Date	Name/Title (printed)
Darrell A. Cullen	
<small>Digitally signed by Darrell A. Cullen DN: cn=Darrell A. Cullen, ou=Environmental Health Services, ou=San Mateo County, email=dcullen@smcgov.org, c=US Date: 2016.05.12 07:50:06 -0700</small>	
Comments: Ensure to submit an electronic HMBP to the County	



DEVELOPMENT SERVICES PLANNING DIVISION

701 Laurel Street
Menlo Park, CA 94025
PHONE (650) 858-3400
FAX (650) 327-5497

AGENCY REFERRAL FORM

DATE: May 12th, 2016

TO: WEST BAY SANITARY DISTRICT
500 Laurel Street
Menlo Park, CA 94025
(650) 321-0384

Applicant	Calysta, In.
Applicant's Address	1140 O'Brien Drive, Suite B, Menlo Park, CA 94025
Telephone/FAX	Tel: 650-508-8018 (Consultant)
Contact Person	Ellen Ackerman (1140 O'Brien Drive)
Business Name	Calysta
Type of Business	Research and development of sustainable fuels and chemicals
Project Address	1140 O'Brien Drive, Suite B, Menlo Park, CA 94025

FOR OFFICE USE ONLY

- ☐ The hazardous materials listed are not of sufficient quantity to require approval by this agency.
- ☒ The Sanitary District has reviewed the applicant's proposed plans and use of listed hazardous materials/chemicals and has found that the proposal meets all applicable Code requirements.
- ☐ The Sanitary District has reviewed the applicant's plans and use of listed hazardous materials/chemicals outlined, and suggests conditions and mitigation measures to be made a part of the City's Use Permit approval (please list the suggested conditions and mitigation measures).

The applicant's proposal has been reviewed by the West Bay Sanitary District by: Jed Beyer
Inspector

Signature/Date

 5-12-16

Name/Title (printed)

Phil Scott / District Manager

Comments: This facility will require a walkthrough inspection within the first month of occupancy. Please see that WBSD and SVCW are listed as Emergency Contacts in the event of an accidental spill/discharge to the sanitary sewer system.



**DEVELOPMENT SERVICES
PLANNING DIVISION**

**Contact: Kyle Perata 650-330- 6721 or
ktperata@menlopark.org**

701 Laurel Street
Menlo Park, CA 94025
PHONE (650) 330-6702
FAX (650) 327-1653

**AGENCY REFERRAL FORM
RETURN DUE DATE: Thursday, May 12, 2016**

DATE: April 28, 2016

TO: CITY OF MENLO PARK BUILDING DIVISION
701 Laurel Street
Menlo Park, CA 94025
(650) 330-6704

Applicant	Calysta, Inc.
Applicant's Address	1140 O'Brien Drive, Suite B, Menlo Park, CA 94025
Telephone/FAX	Tel: 650-508-8018 (Consultant)
Contact Person	Ellen Ackerman (1140 O'Brien Drive)
Business Name	Calysta
Type of Business	Research and development of sustainable fuels and chemicals
Project Address	1140 O'Brien Drive, Suite B, Menlo Park, CA 94025
FOR OFFICE USE ONLY	
<input type="checkbox"/> The hazardous materials listed are not of sufficient quantity to require approval by this Division.	
<input checked="" type="checkbox"/> The Building Division has reviewed the applicant's plans and listed hazardous materials/chemicals and has found that the proposal meets all applicable California Building Code requirements.	
<input type="checkbox"/> The Building Division has reviewed the applicant's plans and use of listed hazardous materials/chemicals outlined, and suggests conditions and mitigation measures to be made a part of the City's Use Permit approval (please list the suggested conditions and mitigation measures).	
The applicant's proposal has been reviewed by the City of Menlo Park's Building Division by:	
Signature/Date <i>Ron LaFrance 5/24/16</i>	Name/Title (printed) Ron LaFrance, Building Official
Comments:	



STAFF REPORT

Planning Commission

Meeting Date:

7/25/2016

Staff Report Number:

16-063-PC

Public Hearing:

**Use Permit and Architectural Control/Facebook
Inc./ 980 Hamilton Avenue**

Recommendation

Staff recommends that the Planning Commission approve a request for a use permit and architectural control for the conversion of an existing warehouse building into food services use, including a kitchen and dining room, associated with a nearby multi-building office use. The proposal includes exterior changes to the building entry. The site is nonconforming with regard to parking, and the kitchen would serve employees located in nearby buildings. The existing building is located in the M-2 (General Industrial) zoning district. The recommended actions are contained within Attachment A.

Policy Issues

Each use permit and architectural control request is considered individually. The Planning Commission should consider whether the required use permit and architectural control findings can be made for the proposal.

Background

Site location

The project site is a warehouse and manufacturing building located at 980 Hamilton Avenue, within the Menlo Technology and Science Park (formerly known as AMB Willow Park, or Prologis). A location map is included as Attachment B. The business park was purchased in 2015 by Peninsula Innovation Partners, LLC, a wholly owned subsidiary of Facebook, Inc. The building is currently vacant, but was most recently occupied by Altair Technologies, a manufacturing company.

The adjacent parcels are also located in the M-2 zoning district, and primarily contain research and development (R&D), manufacturing, and office uses. Willow Road is located west of the subject site. The subject parcel is located approximately 500 feet from Mid-Peninsula High School at 1340 Willow Road, which is southwest of the project site, and is located approximately 800 feet from JobTrain, located at 1200 O'Brien Drive, which is southeast of the project site. The subject site is located approximately 1,000 feet from the nearest residences. The closest residential areas are located to the west on the west side of Willow Road and to the east and south in the City of East Palo Alto, along its border with Menlo Park.

Analysis

Project description

The applicant is requesting a use permit to convert an existing warehouse and manufacturing building into a café and dining facility for employees. Facebook is currently located in eight buildings on the ProLogis campus and as it has expanded the company desires to provide amenities, such as food service within close proximity of the employees located at the Prologis campus. In the M-2 zoning district, cafes intended to serve employees within the vicinity are conditional uses, and therefore Facebook has submitted a use permit request.

The proposed dining facility would be located in an existing 20,000 square foot building and would accommodate approximately 250 people during peak hours. The facility would be open to employees and guests, but not the general public. The dining facility would be open from 7:00 a.m. to 8:00 p.m., Monday through Friday. As part of the interior remodel, the existing mezzanine would be removed, and a kitchen, server, and dining area would be constructed. The dining room would have up to 325 seats.

Design and materials

The proposed project would include minimal cosmetic changes to the existing building. To better identify the main entrance, a new storefront and entry canopy would be constructed along the south façade. The entry door is intended to be aluminum storefront and glass, and the glass at the entry would be “low-e” clear glass. However, the applicant has indicated that the glass could be frosted for privacy. The canopy would be painted steel with a corrugated metal roof and wood trim. Staff believes the exterior changes would provide a more visually appealing entry to the facility.

Parking and trip generation

The subject site is nonconforming with regard to parking, containing 55 stalls where 68 stalls are required, and changes of use on such sites require use permit review. However, the majority of employees using the dining facility are expected to come from the nearby buildings on the Prologis campus, not Building 20 or the East Campus (Buildings 10-19), since these buildings already have multiple on-site dining options. Therefore, most trips to and from the subject facility are expected to be on foot or by bike; however, employees may also arrive via the intra-campus shuttle which has stops nearby on the Prologis campus.

The applicant has prepared a trip generation memo (Attachment E) quantifying expected trips to and from the site. The trip generation memo also includes potential visitor trips to and from the site based on visitor badge-in data for the East Campus and Building 20. Based on the data, it is expected the facility would generate two AM and 16 PM peak visitor trips to the site. The memo identifies that dining facility staff are eligible to use Facebook’s Transportation Demand Management (TDM) program to commute to and from the site. The memo concludes that the dining facility would not likely exceed the peak hour trips allowed under the traffic impact analysis (TIA) thresholds. Therefore, no TIA is required for the project. In addition, given that most patrons would be located within walking or biking distance, the existing non-conforming parking would accommodate the proposed dining facility. In addition, the applicant would update the accessible parking and access on-site as required by the California Building Code.

Staff has added project-specific condition of approval 5a requiring the applicant to monitor trips during the

peak periods one year after beginning operation to confirm the proposed project does not increase trips from the site by more than an equivalent 10,000 square foot office building. The proposed conversion would be required to pay the applicable transportation impact fee (TIF), which is estimated at \$26,186.92 and referenced in condition of approval 5b.

Correspondence

Staff has not received any correspondence on this project.

Conclusion

Staff believes that the proposed dining facility is compatible with the surrounding area and would provide Facebook at the Prologis campus with access to similar amenities as the other campuses. The dining facility would allow Facebook to continue to offer its standard amenities to employees while expanding within Menlo Park. While parking on-site is nonconforming, employees are not anticipated to drive to the site and visitors would be minimal compared to other dining facilities at Facebook; therefore, staff believes that the parking on-site would accommodate employees. The proposed exterior changes would provide a more visually appealing entry to the building. Staff recommends that the Planning Commission approve the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Attachments

- A. Recommended Actions
- B. Location Map
- C. Project Description Letter
- D. Project Plans
- E. Trip Generation Memorandum dated July 19, 2016

Staff Report #: 16-063-PC

Report prepared by:
Kyle Perata, Senior Planner

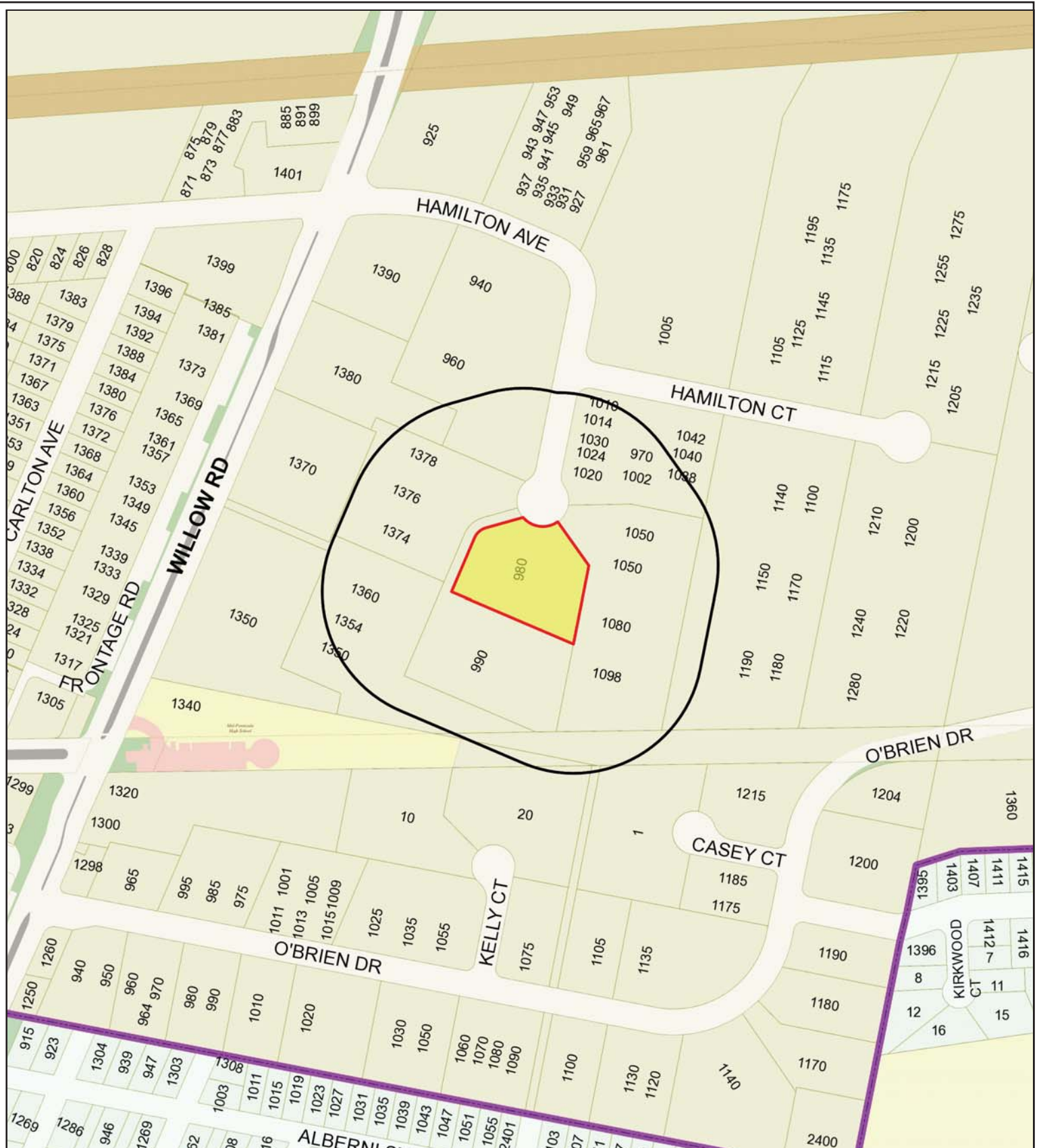
Report reviewed by:
Thomas Rogers, Principal Planner

980 Hamilton Avenue – Attachment A: Recommended Actions

LOCATION: 980 Hamilton Avenue	PROJECT NUMBER: PLN2016-00070	APPLICANT: Facebook, Inc.	OWNER: Peninsula Innovation Partners LLC
REQUEST: Request for a use permit and architectural control for the conversion of an existing warehouse building into food services use, including a kitchen and dining room, associated with a nearby multi-building office use. The proposal includes exterior changes to the building entry. The site is nonconforming with regard to parking and the kitchen would serve employees located in nearby buildings. The existing building is located in the M-2 (General Industrial) zoning district.			
DECISION ENTITY: Planning Commission	DATE: July 15, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: <ol style="list-style-type: none">1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.3. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:<ol style="list-style-type: none">a. The general appearance of the structure is in keeping with the character of the neighborhood.b. The development will not be detrimental to the harmonious and orderly growth of the City.c. The development will not impair the desirability of investment or occupation in the neighborhood.d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.e. The property is not within any Specific Plan area, and as such no finding regarding consistency is required to be made.4. Approve the use permit and architectural control subject to the following standard conditions:<ol style="list-style-type: none">a. Development of the project shall be substantially in conformance with the plans prepared by Gensler consisting of 10 plan sheets, dated received July 20, 2016, as well as the Project Description Letter, dated received June 16, 2016, and Trip Generation Memorandum, dated July 15, 2016, and approved by the Planning Commission on July 12, 2016, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.d. Prior to building permit issuance, the applicant shall submit a plan for any new utility			

980 Hamilton Avenue – Attachment A: Recommended Actions

LOCATION: 980 Hamilton Avenue	PROJECT NUMBER: PLN2016-00070	APPLICANT: Facebook, Inc.	OWNER: Peninsula Innovation Partners LLC
REQUEST: Request for a use permit and architectural control for the conversion of an existing warehouse building into food services use, including a kitchen and dining room, associated with a nearby multi-building office use. The proposal includes exterior changes to the building entry. The site is nonconforming with regard to parking and the kitchen would serve employees located in nearby buildings. The existing building is located in the M-2 (General Industrial) zoning district.			
DECISION ENTITY: Planning Commission	DATE: July 15, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.			
5. Approve the use permit and architectural control subject to the following project-specific conditions: a. After the dining facility has been in operation for one year, the applicant shall conduct a survey of dining hall patrons to determine mode choice and trips during the 7am to 9am and 4pm to 6pm peak periods. If vehicle trips exceed the 10,000 square foot office threshold, the applicant shall submit a plan identifying steps to be taken to bring the project into compliance, subject to review and approval of the Transportation and Planning Divisions. b. Prior to building permit issuance, the applicant shall pay the applicable transportation impact fee (TIF) in effect, which is currently estimated as \$26,186.92.			



City of Menlo Park
 Location Map
 Facebook Dining Facility



Scale: 1:3,600

Drawn By: KTP

Checked By: KTP

Date: 7/25/2016

Sheet: 1



Facebook - Project Narrative – Dining Café and Kitchen

Menlo Park, CA June 15, 2016 2016

To: Menlo Park Planning Division

From: Facebook Inc, Applicant
Form4 Architecture, Architect

Subject: 980 Hamilton Ave – MPK 56
Change of Use Permit

RECEIVED

JUN 16 2016

CITY OF MENLO PARK
BUILDING



Facebook requests approval for change of use from an existing factory warehouse building to a food service dining hall with servery and kitchen.

As Facebook populates various existing office buildings throughout the Prologis site the need for amenity increases, in particular the need for food service.

The proposed dining facility is centrally located within the office park. The existing ground floor area of approximately 20,000sf is ideal for satisfying the program requirements. The facility is anticipated to serve 250 persons at peak hours, all of whom are approaching from neighboring building on the Prologis site. Employees from building other than those located on Prologis are not expected use the building on a regular basis.

Proposed exterior work is as needed to support the new use. Proposed changes include a new entry storefront and doors with overhead canopy. New mechanical system located on site, which will adhere to all zoning ordinances regarding sound and visual screening. New accessible parking and associated site work as required by California Building Code. Each existing and new entry point will be modified with a 2" curb to address any concern about the building's location within a flood plain.

MPK 56

980 HAMILTON AVE.

MENLO PARK, CA 94025

CHANGE OF USE: DINING CAFE AND KITCHEN

LOT AREA: 1.367 ACRES (59,546.52 SF)
 EXISTING BUILDING AREA: 23,400 SF (INCLUDES MEZZANINE)
 PROPOSED BUILDING AREA: 20,276 SF (MEZZANINE DEMOLISHED)
 SITE COVERAGE: 20,276 SF
 EXISTING FAR: 0.391
 PROPOSED FAR: 0.341

EXISTING PARKING: 55 STALLS (NO CHANGE)

PROJECT DESCRIPTION: ADAPTIVE REUSE PROJECT CONVERTING FACTORY WAREHOUSE TO FOOD SERVICE DINING WITH SERVERY AND KITCHEN. NEW EXTERIOR WORK AS NEEDED TO SUPPORT NEW USE, INCLUDING NEW MECHANICAL, ELECTRICAL, PLUMBING, EXTERIOR DOORS AND CANOPY.

TABLE OF CONTEXT:

A0.0	COVER AND INDEX
A1.1	AREA MAP
A1.2	ENLARGED AREA MAP
A2.1	EXISTING SITE PLAN
A2.2	EXISTING SITE PHOTOS
A2.3	EXISTING 1st FLOOR PLAN
A2.4	EXISTING 2ND FLOOR PLAN
A3.1	NEW SITE PLAN
A3.2	PROPOSED NEW FLOOR PLAN
A5.1	NEW WORK EXTERIOR RENDERINGS

RECEIVED

JUL 20 2016

CITY OF MENLO PARK
BUILDING

Form 4

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 San Francisco, CA 94108
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facebook

MPK 56
 980 HAMILTON AVE
 MENLO PARK
 CALIFORNIA 94025

PROJECT



Stamp

EXISTING FLOOR PLAN

Sheet Name

Dates

6/15/2016
 7/06/2016

Description

Date

A0.0

Sheet #



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facebook

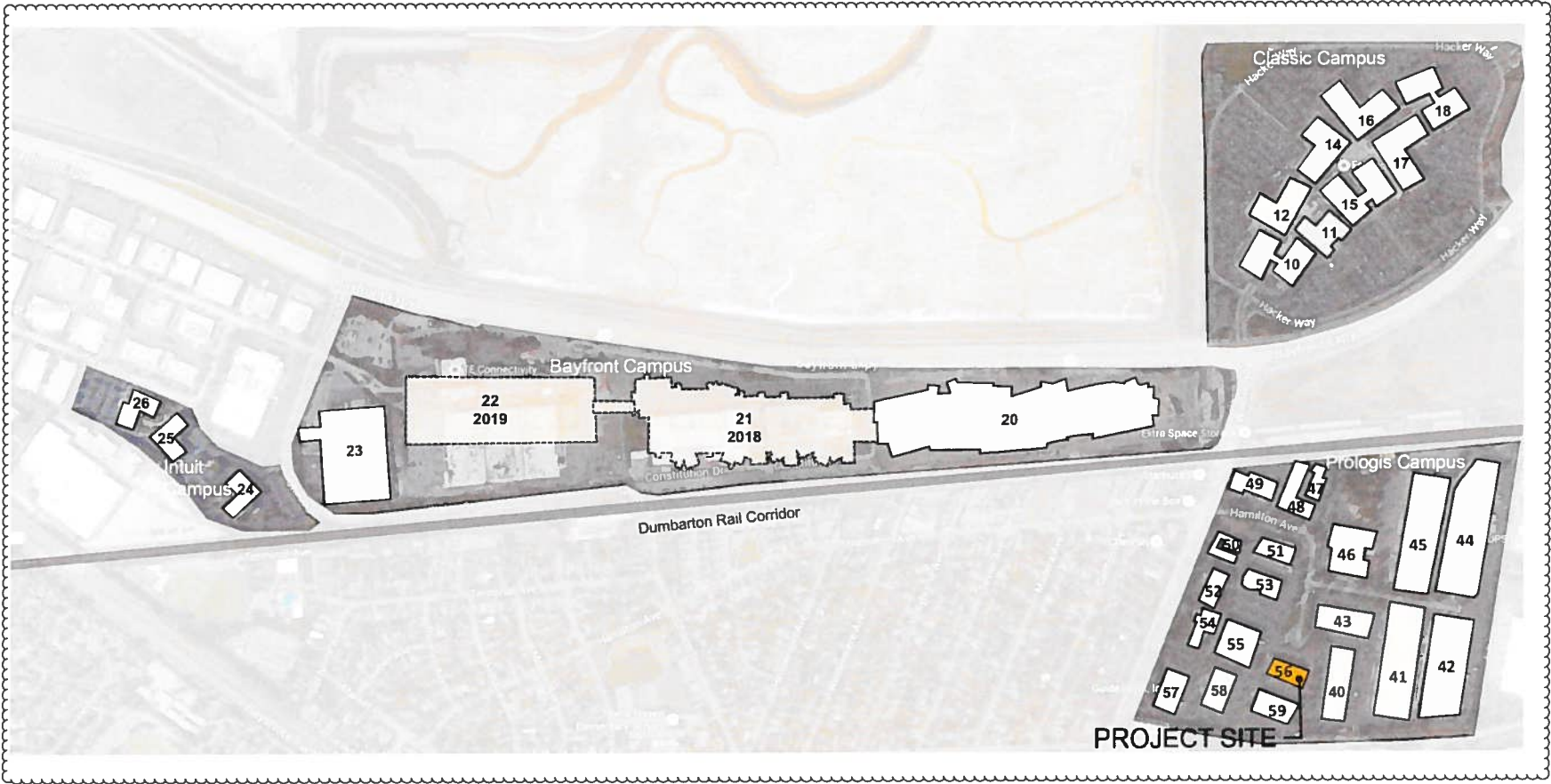
MPK 56
980 HAMILTON AVE
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CALIFORNIA 94025



AREA PLAN

6/15/2016
7/06/2016

Description Date

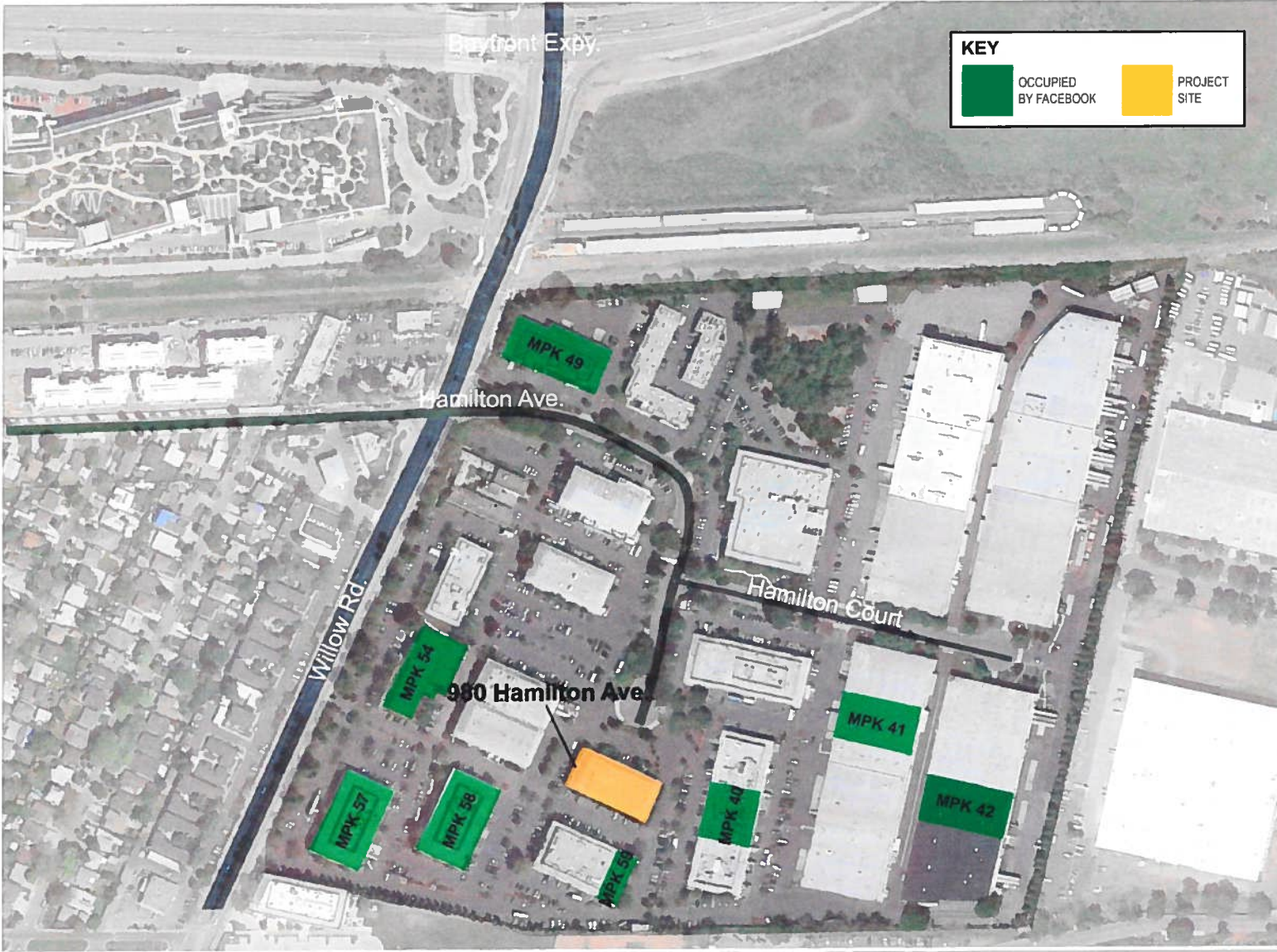


AREA PLAN
SCALE: NTS



A1.1

Sheet #



MPK 56
980 HAMILTON AVE
MENLO PARK
CALIFORNIA 94025

PROJECT

Stamp

ENLARGED
AREA PLAN

Sheet
Name

Dates
6/15/2016

Description Date

ENLARGED AREA PLAN
SCALE: NTS



A1.2

Sheet
#



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MPK 56
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CALIFORNIA 94025



EXISTING SITE PLAN

6/9/2016

Description

Date

EXISTING SITE PLAN

SCALE: 1/16" = 1'-0"



A2.1

Sheet #



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PROJECT
MPK 56
980 HAMILTON AVE
MENLO PARK
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Sheet
Name
EXISTING SITE
PHOTOS

Date
6/9/2015

Description
Date

A2.2

Sheet
#



View 4



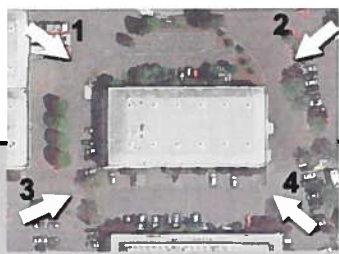
View 2



View 3



View 1



EXISTING SITE PHOTOS

SCALE: NTS



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MPK 56 PROJECT
980 HAMILTON AVE
MENLO PARK
CALIFORNIA 94025



EXISTING 1ST FLOOR PLAN Sheet Name

Dates
6/9/2016

Description Date



EXISTING OFFICE AREA TO BE DEMOLISHED

EXISTING 1st FLOOR PLAN

SCALE: 3/32" = 1'-0"



A2.3

Sheet #



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MPK 56
980 HAMILTON AVE
MENLO PARK
CALIFORNIA 94025

PROJECT



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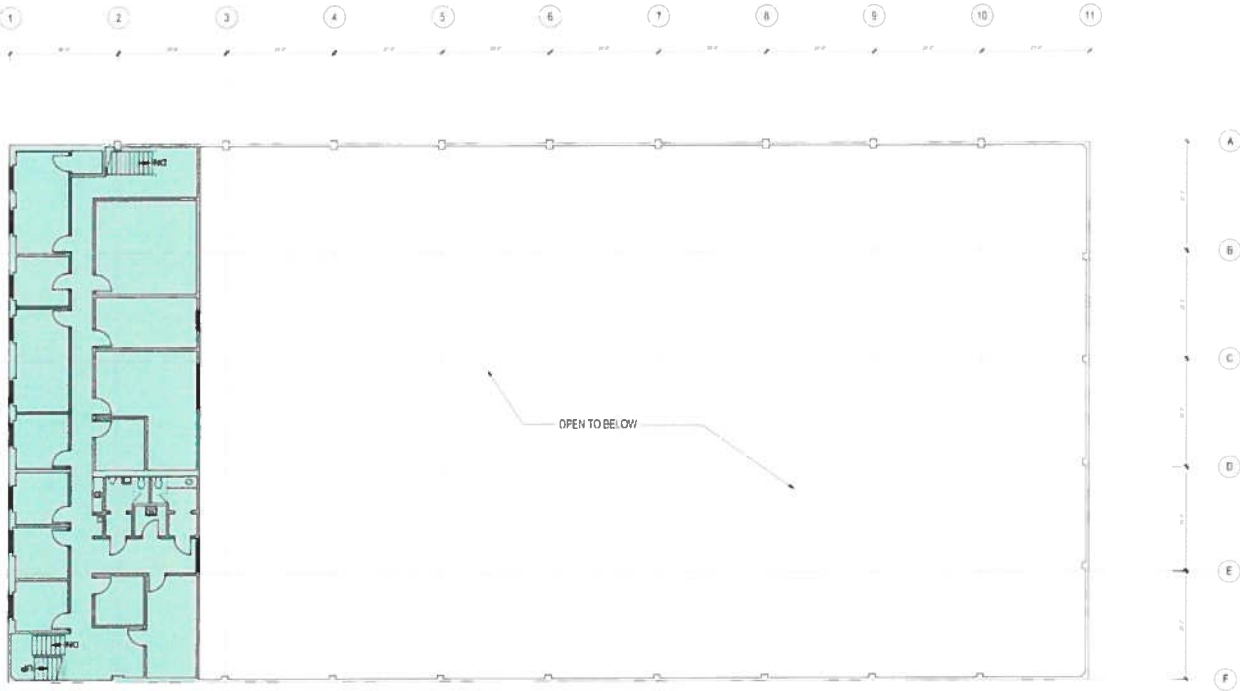
EXISTING 2nd FLOOR PLAN

Sheet Name

Dates
6/9/2015

Description

Date



EXISTING OFFICE AREA TO BE DEMOLISHED

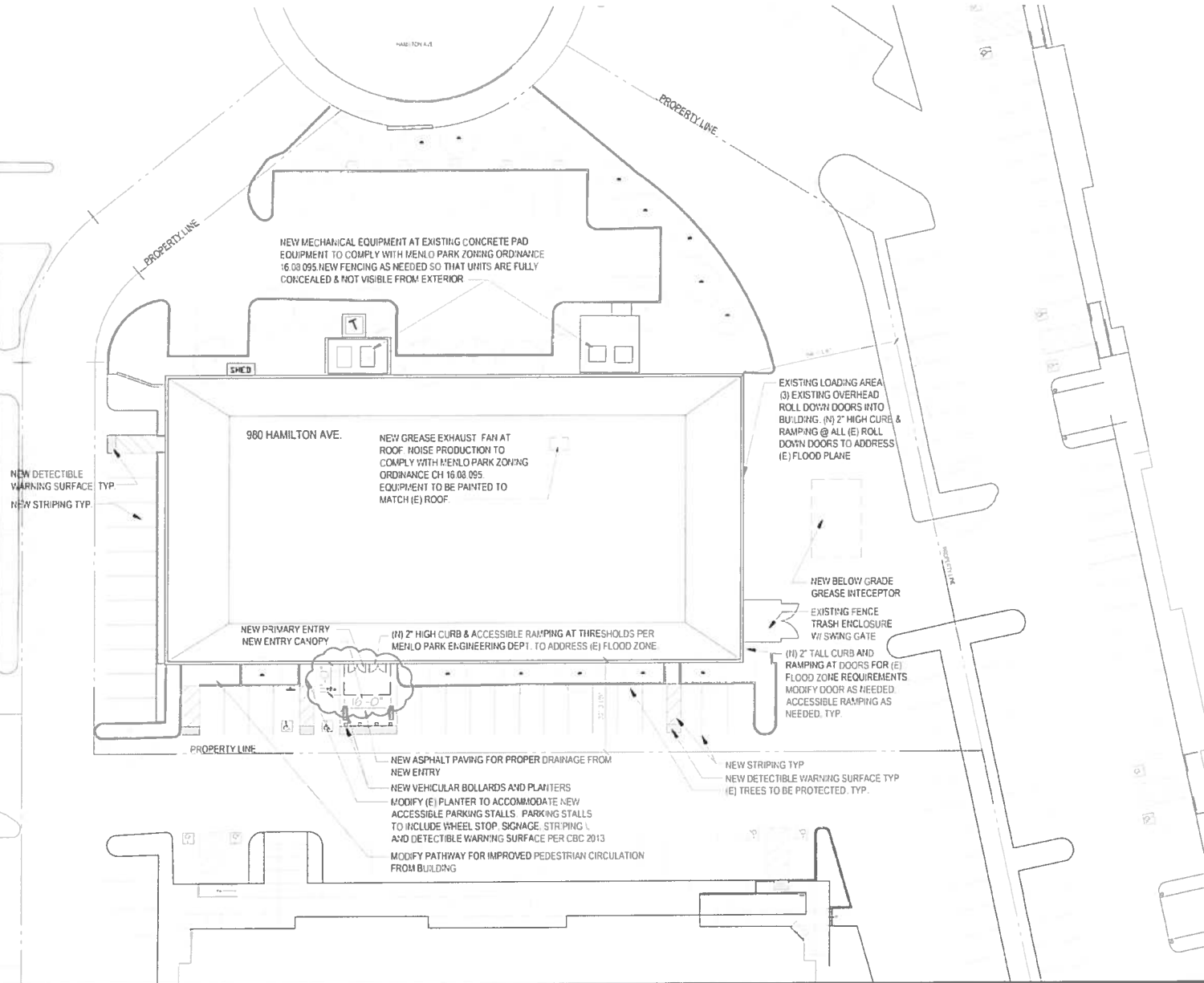
EXISTING 2nd FLOOR PLAN

SCALE: 3/32" = 1'-0"



A2.4

Sheet #



(NEW) PROPOSED SITE PLAN

SCALE: 1/16" = 1'-0"



A3.1



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MPK 56
980 HAMILTON AVE
MENLO PARK
CALIFORNIA 94025

EXISTING FLOOR PLAN

6/15/2016
7/06/2016

Description Date



KEY

	DINING AREA		SERVERY		KITCHEN AREA
--	-------------	--	---------	--	--------------

(NEW) PROPOSED FLOOR PLAN
SCALE: 3/32"=1'-0"



A3.2

Sheet #



View 2 Existing



View 2 Proposed



View 1 Existing



View 1 Proposed



EXTERIOR RENDERINGS



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facebook

MPK 56
980 HAMILTON AVE
MENLO PARK
CALIFORNIA 94025

PROJECT



Stamp

EXTERIOR RENDERINGS

Sheet
Name

Date

Description

Date

A5.1

Sheet
#



MEMORANDUM

Date: July 19, 2016
To: Kyle Perata, City of Menlo Park
From: Robert H. Eckols, P.E.
Sarah Peters
Subject: **Facebook Dining Hall – 980 Hamilton Avenue – Trip Generation**

SJ14-1527

Facebook is proposing to redevelop an existing light industrial building at 980 Hamilton Avenue to serve as an employee dining hall with an associated kitchen. The proposed dining hall is an amenity for Facebook employees located in nearby buildings and will operate Monday through Friday from approximately 7:00 am to 8:00 pm. It will be open only to Facebook employees and their guests, and will ultimately serve the approximately 2,000 employees that will be located in adjacent buildings on the former Prologis campus. The general public will not be able to use the facility.

Since the Facebook employees will already be in the immediate area, they will not be adding to the peak period trips on public streets to reach the dining hall. Many of the dining hall patrons are expected to arrive on foot or by bicycle. Others will arrive via Facebook's inter-campus circulator shuttle, which stops at a nearby buildings. The 25 dining hall staff are eligible to use Facebook's extensive TDM program to commute to and from the site, which has reduced the Facebook employee drive-alone rate to 54 percent. In addition, most of the dining hall staff will arrive and depart in the off-peak travel periods.

The proposed site for the dining hall is an existing one story, 23,539 sq. ft. warehouse building, which currently houses Altair Technologies, a light manufacturing firm. The existing tenant is in the process of relocating from the building. Repurposing the existing building would include demolishing a second floor mezzanine level, reducing the total building area to 20,239 sq. ft. The proposed dining hall will provide 340 seats.

A transportation impact analysis (TIA) is required for proposed changes of use over 10,000 square feet, unless the new use would add fewer peak hour trips than the equivalent to 10,000 sq. ft. of



general office space (16 AM peak hour trips or 15 PM peak hour trips) or includes a TDM plan effective at reducing the added trips to below the threshold.

The following describes the trip generation for the proposed change and TDM programs available to the employees and patrons of the dining hall.

DISCUSSION

Trip Generation Threshold

Table 1 was prepared using the trip generation rates from the Institute of Transportation Engineers (ITE) *Trip Generation* (2012) and shows the following:

- Trip generation for the existing 23,539 sq. ft. of warehousing uses,
- The threshold of added trips that would require preparation of a TIA, and
- The maximum number of trips allowed for the proposed change in use.

Table 1
Trip Generation Threshold

Land Use (LU)	ITE LU #	Units	AM Peak Hour			PM Peak Hour		
			Rate	Total	In	Out	Rate	Total
Existing Use		Sq. Ft.	Trips/KSF			Trips/KSF		
Warehousing	150	23,539	0.3		79%	21%	0.32	
				7	6	1		8
Threshold		Sq. Ft.						
General Office	710	10,000	1.56		88%	12%	1.49	
				16	14	2		15
Change in Use Maximum Trips				23	20	3		23
								5
								18

Source: Fehr & Peers, 2016

Based on the information summarized in **Table 1**, the proposed dining hall could generate a maximum of 23 AM peak hour trips and 23 PM peak hour trips without requiring the preparation of a TIA.



Trip Generation Rates for Similar Land Uses

Trip Generation does not include trip generation for an employee dining hall or cafeteria. It includes rates for several land uses that serve food; however, none of these uses accurately reflects the operations of the proposed dining hall that is provided for a single company's employees and their guests. The most relevant land uses are:

- **High-Turnover (Sit-Down) Restaurant – Land Use 932:** A high-turnover sit-down restaurant is a full-service eating establishment, typically moderately-priced and frequently part of a chain. Patrons typically stay for up to an hour, and are served at their tables.
- **Fast Food Restaurant without Drive-Through Window – Land Use 933:** A fast-food restaurant has a large carry-out clientele, long-hours of service, and high turnover for customers who eat onsite. Table service is not provided.

Both of these ITE land uses describe facilities that are open to the general public and whose patrons would arrive from locations scattered throughout the community. For the proposed dining hall, all Facebook employees and the most, if not all, guests will be coming from the adjacent Facebook buildings with the opportunity to walk, bike or use a campus shuttle to access the dining hall. Therefore, the trip generation rates presented in the ITE Trip Generation Manual do not apply.

Patron Trips

As stated previously, the proposed dining hall would serve as an amenity for approximately 2,000 employees that ultimately could be seated in office buildings in the immediate area. Facebook's two main campuses at 1 Hacker Way and 1 Facebook Way are served by multiple on-site dining facilities; therefore, this dining hall is unlikely to attract a significant number of Facebook employees from outside the former Prologis site. In the event they did want to use the facility these employees will be able to use shuttle operating between the campuses.

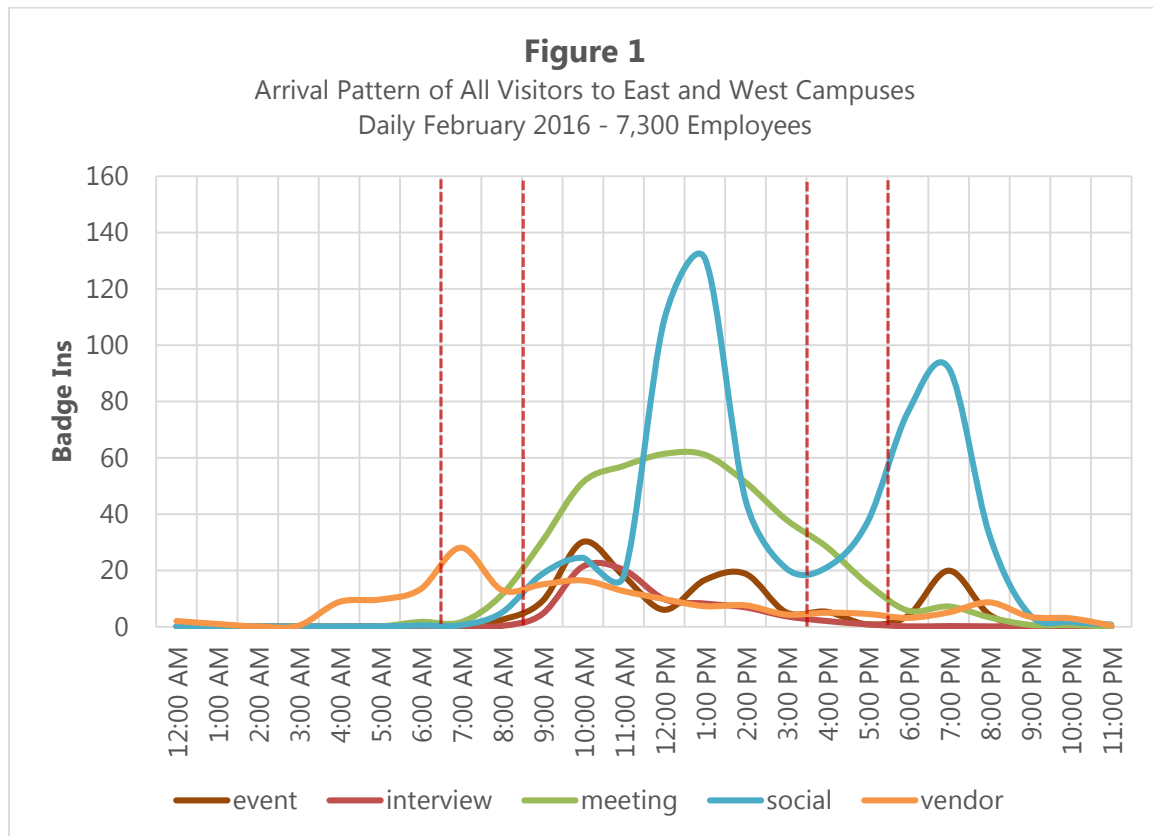
Given that patrons are likely to walk, bike, or take a campus shuttle to the dining hall after arriving on campus for work, most vehicle trips directly to the site would be made by social visitors who do not have other business at Facebook. Visitors for other purposes, such as events, interviews, meetings, and vendor trips, would have business at other Facebook buildings, and therefore would not be making a separate vehicle trip to the dining hall.

To understand the potential for social visitors to generate peak hour trips to the site, we reviewed visitor badge-in data for the existing East Campus at 1 Hacker Way (MPK 10-19) and the West Campus at 1 Facebook Way (MPK 20). Daily weekday visitor badge-ins from the month of February



2016 was provided to Fehr & Peers. The data was broken by hour of arrival and purpose of visit. From this data it was possible to identify peak hour and peak period visitor trip rates. The month of February 2016 was used because mode share and employee headcount data were readily available from Facebook's February 2016 TDM monitoring.

Figure 1 shows average hourly visitor badge-ins at both Classic Campus and Building 20 for February 2016 over a 24-hour period. The AM (7-9 AM) and PM (4-6 PM) periods are shown between red dashed lines, and different visit purposes shown in different colors. Social trips (shown in blue) make up 44 percent of all trips, and have two pronounced peaks: one at lunchtime, between noon and 2 PM, and another in the evening, between 6 PM and 8 PM. By contrast, relatively few trips are made in the peak periods of 7-9 AM and 5-6 PM.



Hourly data on social visitor badge-ins yields an inbound person trip rate of 0.67 visitors per 1000 employees between 8-9 AM and 5.08 visitors per 1000 employees between 5-6 PM. For the purpose of this analysis, it is assumed that all visitors drive alone to estimate visitor trip generation. Factored up to account for 2,000 employees on the former Prologis campus, this would yield 2 inbound social visitor trips during the AM peak hour and 10 inbound social visitor trips during the PM peak hour.



Outbound trips during the AM and PM peak hours were estimated using the assumption that most visitors stay for 2 hours, so the outbound visitors between 8 and 9 AM would have arrived between 6 and 7 AM, and the outbound visitors between 5 and 6 PM would have arrived between 3 and 4 PM.

The estimated peak hour social visitor trips are shown in **Table 2**. This presents a high estimate of the trips that could be generated by social visitors, for a few reasons:

- The estimate assumes that each visitor arrives alone in their own vehicle, although some visitors will carpool;
- The Prologis campus is a functional worksite, and Facebook employees are more likely to meet visitors at either the East or West Campus and eat at that location.

Table 2
Dining Hall Visitor Trip Generation Estimate

Trip type	AM Peak Hour (8-9 AM) ¹			PM Peak Hour (5-6 PM) ¹		
	Total	In ²	Out ³	Total	In ²	Out ²³
Social visitor trip rate (per 1000 employees)	-	0.67	0.05	-	5.08	2.83
Social Visitors for 2000 employees at Prologis campus (person trips)	2	2	0	16	10	6

Notes:

1. Highest hour for social visitor trip generation selected from AM (7-9 AM) and PM (4-6 PM) peak periods.
2. Rates based on visitor badge-ins by hour for February 2016, averaged over 19 working days.
3. Outbound rates assume 2-hour stay for visitors. Inbound trips between 6-7 AM are assumed to be outbound trips between 8-9 AM; inbound trips between 3-4 PM are assumed to be outbound trips between 5-6 PM.

Source: Fehr & Peers, 2016.



Dining Hall Employee Trips

Dining hall employee commute trips would also be substantially lower than a typical fast-food or high-turnover restaurant, since employees have access to all of the TDM programs offered to Facebook employees. **Table 3** summarizes the TDM programs that are available to the dining hall employees. The Facebook drive-alone rate is currently 54 percent, as compared to the San Mateo County average of 84 percent. In addition, since the dining hall hours will be 7:00 AM to 8:00 PM, many of the employee trips will occur outside the commute travel peaks. A total of 25 employees are anticipated to work at the site.

CONCLUSION

Based on Fehr & Peers' review of the project description, comparison to the ITE rates for the existing and proposed uses, understanding of the dining hall operations, and the available TDM programs for the dining hall staff, the proposed dining hall is unlikely to generate more than the 28 peak hour trips allowed under the change of use threshold. Due to the nature of its operations, the dining hall will be similar to other amenities provided on the Facebook campus that reduce or eliminate vehicle trips, or shorten trip length.

The key factors considered are:

- Dining hall patrons are primarily Facebook employees located at the former Prologis campus, and therefore would not generate new peak hour trips on public roadways,
- Dining hall patrons will be able to walk, bike, or use shuttle to access the dining hall,
- Social visitor badge-ins during the morning 7-9 am and 4-6 pm evening peak periods are low,
- Social visitors badge-ins peak at mid-day and after 6:00 pm,
- Dining hall design and function is not a "destination" dining facility on campus as compared to some of the dining areas at Classic Campus and Building 20, and
- Dining hall employees have access to all of the TDM programs available to Facebook employees.



Table 3
TDM Program Summary

TDM	Description	Facebook Program
Caltrain Go-Passes and Caltrain Station Shuttles	Provides unlimited rides (stickers affixed to an approved identification badge).	All full time Facebook employees receive free Caltrain Go-Passes and shuttle service provided from Caltrain to the Facebook campus. Facebook also reimburses up to \$50/month for parking at Caltrain stations (post-tax). Facebook uses Wage Works to provide tax-free funds for other public transit passes. Employee guests are also able to ride shuttles from Caltrain if they request a pass.
Employee Commuter Shuttle Bus Services / Intern Shuttles	Private shuttle service from employee residential neighborhoods and cities to MPK.	Currently, Facebook provides free direct services between Menlo Park and Sunnyvale, Palo Alto, San Francisco, Mountain View, Cupertino, Campbell, Berkeley, Oakland, Dublin, Castro Valley, Redwood City, San Jose, and Fremont for employees and vendors. Facebook provides shuttles service to campus from intern housing located in Mountain View, Sunnyvale, Palo Alto, Menlo Park and San Francisco.
Campus Bike Share Program	Bicycles provided for employee use on campus.	This program provides Facebook Bike Share Bicycles for employees to use for trips around campus.
Bicycle Amenities and Perks	Bike shop, lockers, towel service for showers, bicycle pumps, FixIt self-repair station, etc.	An onsite bike shop has been opened at the Transportation Hub. Dedicated mechanics service personal bikes for free and charge only for the cost of parts. A 24/7 DIY FixIt station is also available along with a free vending machine with emergency parts for repair. A monthly Bike to Work Day with giveaway is also held with bike shop staff leading group rides each month. The Bike Shop has also implemented a loaner program where employees can check out a bike for up to a week. Interns can also check out a long-term loaner for the duration of their internship. Each employee-occupied building has interior bike parking, and a bike cage offers additional bike parking space. These support services improve the convenience of riding a bicycle.
Vanpool Program	A program that allows groups of people to share rides to and from work.	Facebook provides vanpools to and from surrounding areas, mostly South Bay and East Bay.



Table 3
TDM Program Summary

TDM	Description	Facebook Program
Education and Promotion	Educational and promotional events to encourage employees to use alternative modes to travel to and from the workplace.	Drop-in commute advice is available through the Transportation Desk at the Transportation Hub. Events and competitions for prizes include bike commuting classes, monthly Bike to Work Day, and the Great Race for Clean Air. New employees receive information on various commute options during orientation.
Zimride Rideshare Program	A social rideshare community that allows users to quickly find other drivers or passengers who are traveling along the same route.	Zimride provides ridesharing, vanpooling and shuttle coordinating capabilities to any employee with a Facebook email address.
Emergency Ride Home	Rides provided for employees in case of emergency.	In the event of an emergency, Facebook provides rides home to all ride share and alternative mode commuters who many not have a vehicle readily accessible.
Zipcar	Car sharing available on campus.	Zipcar vehicles are located at 1601 Willow Road.
Electric Vehicle Parking	Dedicated parking for electric vehicles.	Facebook provides preferred parking for electric vehicles as well as free charging stations at MPK. Facebook now has a total of 162 electric vehicle parking spaces.

Source: Facebook, 2016.



STAFF REPORT

Planning Commission

Meeting Date:

7/25/2016

Staff Report Number:

16-064-PC

Public Hearing:

**Use Permit and Architectural Control/DES
Architects Engineers/1430 O'Brien Drive**

Recommendation

Staff recommends that the Planning Commission approve a use permit and architectural control to partially convert, expand, and architecturally update an existing research and development (R&D) building to create a new café, fitness and health center, and additional R&D area, and to provide new landscaping to the subject property located in the M-2 (General Industrial) zoning district at 1430 O'Brien Drive. As part of the project, the applicant is requesting approval of a use-based parking reduction based on the proposed uses within the building, the proposed tenants' operations, and a Transportation Demand Management (TDM) Program. In addition, two heritage flowering pear trees, 19 and 17 inches in diameter, would be removed at the north side of the building due to construction impacts and fair/poor health. The project also includes a Below Market Rate (BMR) Housing Agreement for the payment of an in lieu fee or the delivery of equivalent off-site units. The recommended actions are included as Attachment A.

Policy Issues

Each use permit and architectural control request is considered individually. The Planning Commission should consider whether the required use permit and architectural control findings can be made for the proposal.

Background

Site location

The project site is an existing office and R&D building located at 1430 O'Brien Drive, south of the intersection of O'Brien Drive and Adams Drive. The subject property is commonly referred to as Building 7 of the Menlo Business Park, which is comprised of buildings mainly located along O'Brien Drive and Adams Drive between Willow Road and University Avenue. A location map is included as Attachment B. Parcels farther north across O'Brien Drive and also adjacent to the east and west are located in the M-2 zoning district and primarily contain warehouse, light manufacturing, R&D, and office uses. Single-family residences in the City of East Palo Alto are located directly south of the subject property.

A parcel owned by the San Francisco Public Utility Commission (SFPUC) containing Hetch Hetchy Regional Water System infrastructure runs adjacent to the north building exterior along the entire width of the parcel. Based on past approvals for development of the subject property, the SFPUC parcel is considered part of the development site in terms of floor area ratio (FAR), setbacks, parking, and other purposes.

Analysis

Project description

Presently, the site contains an existing two-story concrete tilt-up building constructed in 1986 as part of the Menlo Business Park development. The building is a multi-tenant structure previously occupied by a mix of life-science R&D companies, but is currently vacant. The structure has 65,952 square feet of gross floor area (GFA) and an FAR of 42.9 percent, and it conforms to all FAR, setback, and height requirements established for the M-2 zoning district.

At this time, the applicant is proposing to add an additional 19,102 square feet of GFA by enclosing existing covered entrances along the front of the building and expanding the second floor into spaces that are currently open to the first floor below. The modified and expanded building would include a fitness center with a rooftop pool as amenities for Menlo Business Park employees, and a café with outdoor seating anticipated to be used mainly by office workers in the vicinity of the project site. The building is relatively central among the parcels in the area owned by Tarlton Properties, which makes it a practical location for providing employee amenities within a convenient travel distance. The proposed project would result in 84,562 square feet of GFA and an FAR of 55 percent for the entire building, which is the maximum FAR permitted for an M-2 zoned property with primarily non-office uses. Modifications to the building façade would also be made related to the enclosure of existing recessed building entrances, creation of new building entrances, conversion of the interior space, and placement of a swimming pool on the building roof.

All new construction within the M-2 zoning district requires use permit approval from the Planning Commission. The proposed exterior changes also require architectural control approval from the Planning Commission and are described in the section below. The project plans and the applicant's project description letter are included as Attachments C and D, respectively. A generator is shown at the rear of the building on the applicant's proposed plans, but the generator, as well as the storage and use of hazardous materials by future tenants, would require approval of a separate use permit by the Planning Commission at a future date.

Design and materials

As part of the changes to building entrances and circulation, the applicant is proposing exterior façade alterations that require architectural control. New entrances to second-story suites would be created by the addition of exterior stairs and elevated walkways spanning both sides of the building, except near the central building entrance. The elevated walkways would feature tempered clear glass rails, and complementary awnings would be installed above the new second-story entrances. New light-tinted storefront glass would replace the existing high-tint black glass that is currently featured on the building exterior. The building would be painted in neutral gray tones, in contrast to the beige tones of the existing façade. The central first-floor building entrance would become a double-height space with glass storefront spanning both stories. The prominence of the entrance would be further increased by the addition of vegetation screens at the ground level that would rise in height toward the center of the building, drawing the eye toward the main entrance doors.

In addition, an open-air vertical circulation tower with stairs and an elevator leading to the rooftop pool

would be located to the left of the main entrance. Elevator equipment rooms are explicitly excluded from height limits by the Zoning Ordinance, and stair towers have historically not been counted toward overall building height since they are needed for maintenance and access purposes. While serving as somewhat prominent examples of these maximum building height exceptions, staff believes the 47-foot height of the vertical circulation tower would be permitted to exceed the 35-foot height maximum of structures in the M-2 zoning district given its relative modesty compared with the overall building size, its relationship to the overall building aesthetic, and the fact that it is open and uncovered. The height of the tower would add visual interest to the building roofline, and the elevator exterior, painted red, would also provide additional interest to the front façade of the building. However, the Planning Commission may make a different determination if it believes the vertical circulation tower is excessively tall or otherwise overly prominent. In addition, a 41-foot stair tower located toward the center of the building would also be permitted to exceed the maximum 35-foot height based on typical practices described above.

Staff believes that the requested modifications would enhance the façade by helping to break up the relatively flat and uniform exterior of the existing building, and by increasing transparency and openness through the use of light-tinted glass, exterior stairs and walkways, and the open circulation tower near the center of the front exterior.

Trip generation, Transportation Demand Management (TDM) Program, and parking demand

The proposed project would convert the existing building from R&D and office uses to café, fitness and health center, and R&D and office uses. The applicant has submitted a trip generation analysis and transportation demand management (TDM) program (Attachment E) to evaluate if the proposed change of use would increase the trips from the site equivalent to a new 10,000 square-foot office building and explore opportunities to decrease any new trips to the site. The trip generation analysis calculates the existing and proposed trips for the planned project based on the Institute of Transportation Engineering (ITE) trip rates for specific land uses.

The City's TIA Guidelines allow for the implementation of a TDM program as part of the proposal to reduce trips from the site and subsequently reduce the impact of the project on the transportation network. The applicant is proposing to implement a TDM program to reduce the trips for the proposed project to a level below that of a 10,000 square-foot office building. The TDM program is included in Attachment E and includes measures such as bike storage, shuttle service, showers/changing rooms, subsidized transit tickets, preferential carpool parking, a commute assistance center, and guaranteed ride home program, among others. The complete list and discussion of individual items is included in the attachment. The proposal also includes relocation of a shuttle stop from 1505 O'Brien Drive to 1430 O'Brien Drive. The shuttle stop location would be subject to review and approval by the Engineering, Transportation, and Planning Divisions. Condition of approval 6a requires the applicant to submit an encroachment permit for the shuttle stop and sign to the Engineering Division.

The proposed TDM program would result in an overall reduction in daily trips from the site and a net decrease of 16 AM Peak trips and 13 PM Peak trips. As a result, a TIA is not required for the proposed project. Condition of approval 6b requires annual monitoring and reporting from the applicant to confirm the effectiveness of the TDM program and to ensure the project is under the trip limits identified in the TDM program and trip generation analysis.

In terms of project site parking, the M-2 zoning district requires one off-street parking space per 300 square feet of GFA, not in the front one-quarter of any required front yard. The submitted plans indicate an existing gross floor area of 65,952 square feet, meaning that the building has a parking requirement of 219 spaces. The site currently contains 199 parking stalls that comply with the Zoning Ordinance off-street parking requirement. Therefore, the parking situation at the site is considered existing nonconforming. The original entitlements for the building permitted construction of 86 of the 199 parking spaces within an easement over the SFPUC parcel that runs directly adjacent to the right side of the property. These spaces are proposed to remain, with some proposed for restriping to bring them into conformance with the City's Parking Stalls and Driveway Design Guidelines.

Based on the planned expansion of 19,102 square feet of GFA, 64 additional parking spaces would be required under the M-2 zoning district parking ratio. However, the applicant has requested a parking reduction to maintain 197 spaces at the site, which would represent a ratio of 2.33 parking spaces per 1,000 square feet of GFA. Two spaces are proposed to be removed to accommodate the future generator at the rear of the building and a bioretention area. A parking analysis, also included in Attachment E, evaluated the proposed project's parking supply of 197 spaces and found that the parking demand of similar R&D uses in the Bay Area resulted in a demand of 1.40 spaces per 1,000 square feet. Furthermore, the analysis notes that parking for the proposed uses at 1430 O'Brien Drive would involve shared parking, since the proposed uses on the site would be complementary and would reach peak parking demands at different times of the day. Additionally, because the fitness and health center would be limited to Menlo Business Park employees and the café is expected to draw workers mostly from the surrounding buildings, a number of trips to the site would be taken on foot, reducing overall parking needs at the site. Staff recommends approval of the parking reduction request subject to the recommended conditions of approval. The proposed project would be required to pay the applicable transportation impact fee (TIF), which is estimated at \$145,085.81 and referenced in condition of approval 6d.

Trees and landscaping

The project site contains approximately 45 trees, of which 11 are considered heritage trees. The arborist report (Attachment F) identifies the species, size condition, suitability for preservation, and tree protection measures for all trees on site. The arborist report identified two heritage trees, a 19-inch flowering pear (tree #31) and a 17-inch flowering pear (tree #26), for removal near the front exterior of the building. The City Arborist has tentatively approved the removal of the 19-inch heritage tree due to construction impacts and fair overall condition and will review the 17-inch heritage tree removal concurrent with building permit review of the project. Otherwise, construction and landscaping improvements to the existing building and property are not anticipated to adversely affect the remaining heritage trees located on the subject site or neighboring properties. Standard heritage tree protection measures will be ensured through recommended condition 5g.

The project applicant would be required to replace the removed heritage tree at a two-to-one ratio, for a total of two new heritage tree replacements. The replacements are tentatively proposed at the front of the building, flanking either side of the main entrance. Other landscaping and site improvements would include a new entry path of enhanced paving and decomposed granite leading from O'Brien Drive to the main building entrance. Condition of approval 6a requires the applicant to provide a connection from the proposed entry path to the existing crosswalk at the west end of the O'Brien Drive and Adams Drive intersection. The proposed path would replace an existing paved vehicular entrance currently located in

this area. Outdoor seating would be provided along the path, with a larger outdoor seating area for the café located northeast of the building entrance. The proposed project includes a preliminary landscaping plan that identifies proposed trees, groundcover plantings, and other plantings and outdoor furniture. The applicant reviewed the proposed landscape improvements in the SFPUC parcel at the front of the property with the SFPUC Project Review Committee at a June 29, 2016 meeting. The applicant received approval to move forward, subject to completing a list of 10 follow-up items, described in the attached meeting minutes (Attachment G). Condition of approval 6c requires the applicant to confirm completion of the follow-up items with the Project Review Committee or designees identified in the meeting minutes and provide written proof of compliance prior to issuance of a building permit.

Below Market Rate (BMR) Housing Agreement

Per the Zoning Ordinance, commercial projects inclusive of 10,000 square feet or more of GFA are subject to the BMR requirements. The draft BMR agreement term sheet for the proposed project was reviewed by the Housing Commission at its March 2, 2016 meeting. At that meeting, the Commission discussed other recently approved BMR agreements, which included the ability for applicants to meet BMR obligations through delivery of an off-site unit in a zoning district where housing is permitted, a possible agreement with a developer to contribute toward the cost of constructing the required number of units, or payment of the applicable in lieu fee. The in lieu fee is paid based on the square footage of office area (Group A) and non-office commercial area (Group B). For an addition of new square footage, the applicant is required to pay the difference between Group A and Group B square footage for the project. Alternatively, the equivalent number of units for this project would be 0.7 units, which could be rounded to one full unit to be constructed by the applicant. As an additional option, the applicant could partner with other developers to construct a BMR unit in Menlo Park. The Housing Commission voted three to zero to approve the draft BMR agreement term sheet and recommend Planning Commission approval of the BMR Agreement, giving flexibility to the applicant to satisfy the BMR requirement through any of these options.

Since the Housing Commission's review, staff has further reviewed the change in square footages and determined that the increase in office (Group A) square footage is slightly more than originally represented to the Housing Commission. The attached draft BMR Agreement (Attachment H) has been updated to reflect the clarified square footages. The current in lieu rate for office uses (Group A) is \$16.15 per square foot and the in lieu fee rate for non-office commercial uses (Group B) is \$8.76 per square foot. The rate is adjusted annually on July 1 and the applicable fee for the project will be based upon the amount of square footage within Group A and B, as well as the rate that is in effect at time of payment. The estimated BMR in lieu fee for the proposed project is \$228,070.30, based upon the proposed land use breakdown within the building.

Correspondence

Staff has not received any items of correspondence on the proposed project.

Conclusion

The proposed addition of 19,102 square feet of GFA within the existing building would result in an FAR of 55 percent for the entire building, which is the maximum FAR permitted in the M-2 zoning district for primarily non-office uses. Staff believes that the requested modifications to the exterior of the building would enhance the façade by helping to break up the fairly flat and uniform exterior of the existing building,

and by increasing transparency and openness through the use of light-tinted glass, exterior stairs and walkways, and the open circulation tower near the center of the front exterior. The proposed landscape improvements and outdoor seating area would further encourage pedestrian activity and vibrancy at the site, consistent with the proposed uses for fitness and health and café space. Based on the trip generation analysis and proposed TDM Program provided by the applicant, staff believes that the proposed expansion would not negatively affect circulation, parking, or traffic at the site. Staff recommends that the Planning Commission approve the requested use permit, architectural control, parking reduction, and BMR housing agreement.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Recommended Actions
- B. Location Map
- C. Project Plans
- D. Project Description Letter
- E. Transportation Memorandum for 1430 O'Brien Drive
- F. Arborist Report
- G. SFPUC Project Review Committee June 10, 2016 Meeting Minutes
- H. Draft Below Market Rate (BMR) Housing Agreement

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public

Staff Report #: 16-064-PC

viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

Color and Materials Board

Report prepared by:

Tom Smith, Associate Planner

Report reviewed by:

Thomas Rogers, Principal Planner

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1430 O'Brien Drive – Attachment A: Recommended Actions

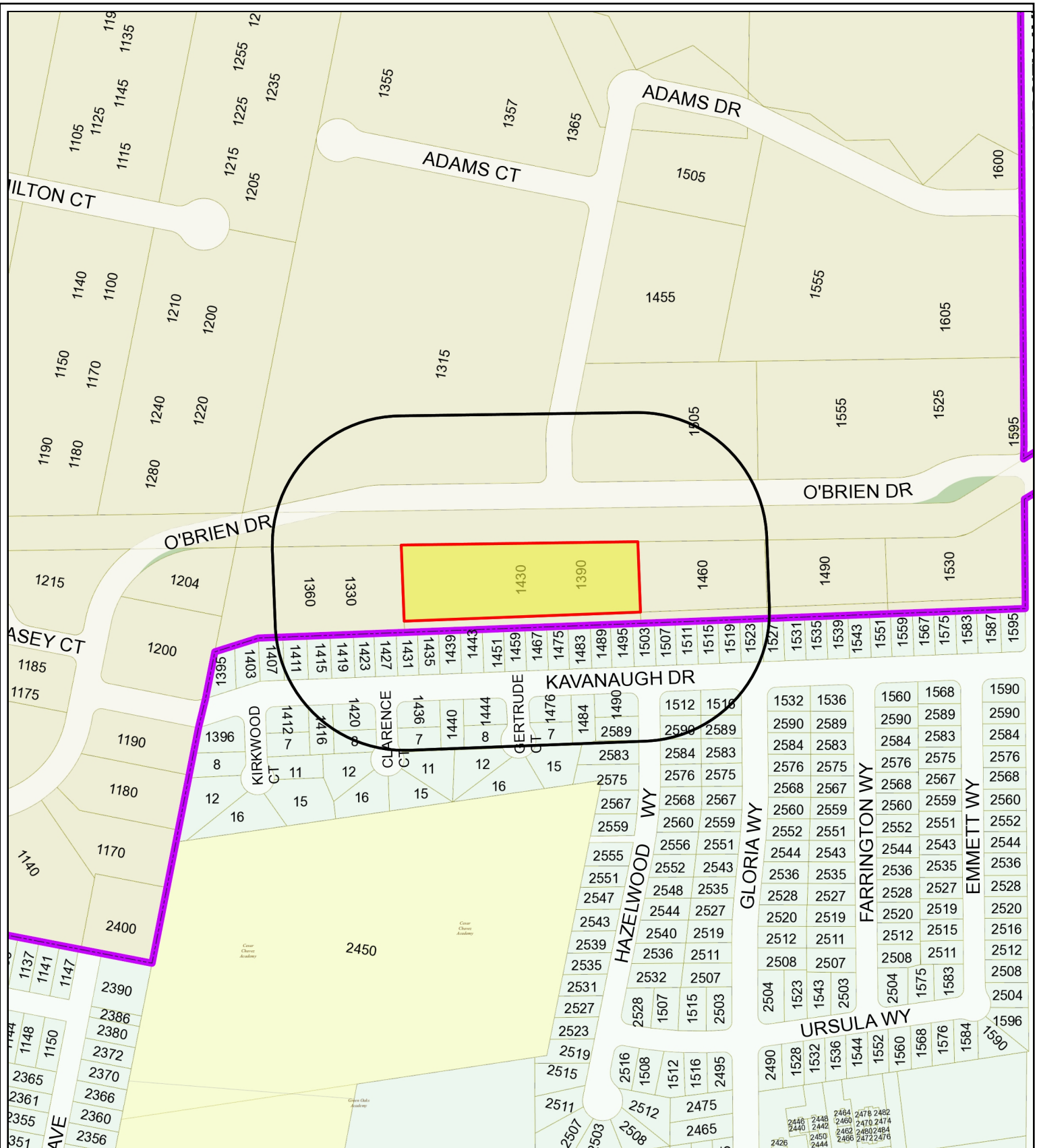
LOCATION: 1430 O'Brien Drive	PROJECT NUMBER: PLN2016-00014	APPLICANT: DES Architects + Engineers	OWNER: Menlo Business Park, LLC
REQUEST: Request for a use permit and architectural control to partially convert, expand, and architecturally update an existing research and development (R&D) building to create a new cafe and fitness and health center, additional R&D spaces, and provide new landscaping to the subject property which is located in the M-2 (General Industrial) zoning district. As part of the project, the applicant is requesting approval of a use-based parking reduction based on the uses within the building, the proposed tenants' operations, and a Transportation Demand Management (TDM) program. In addition, two heritage flowering pear trees, 19 and 17 inches in diameter, would be removed at the north side of the building due to construction impacts and fair/poor health. The project includes a Below Market Rate (BMR) Agreement for the payment of an in lieu fee or the delivery of equivalent off-site units.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: <ol style="list-style-type: none">1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.3. Approve the Below Market Rate (BMR) Housing Agreement.4. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:<ol style="list-style-type: none">a. The general appearance of the structure is in keeping with the character of the neighborhood.b. The development will not be detrimental to the harmonious and orderly growth of the City.c. The development will not impair the desirability of investment or occupation in the neighborhood.d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.e. The property is not within any Specific Plan area, and as such no finding regarding consistency is required to be made.5. Approve the use permit and architectural control subject to the following standard conditions:<ol style="list-style-type: none">a. Development of the project shall be substantially in conformance with the plans prepared by DES Architects + Engineers consisting of thirty-seven plan sheets, dated received July 11, 2016, as well as the Project Description Letter, dated received April 25, 2016, and the Transportation Memorandum for 1430 O'Brien Drive, dated February 1, 2016, approved by the Planning Commission on July 25, 2016, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to			

1430 O'Brien Drive – Attachment A: Recommended Actions

LOCATION: 1430 O'Brien Drive	PROJECT NUMBER: PLN2016-00014	APPLICANT: DES Architects + Engineers	OWNER: Menlo Business Park, LLC
REQUEST: Request for a use permit and architectural control to partially convert, expand, and architecturally update an existing research and development (R&D) building to create a new cafe and fitness and health center, additional R&D spaces, and provide new landscaping to the subject property which is located in the M-2 (General Industrial) zoning district. As part of the project, the applicant is requesting approval of a use-based parking reduction based on the uses within the building, the proposed tenants' operations, and a Transportation Demand Management (TDM) program. In addition, two heritage flowering pear trees, 19 and 17 inches in diameter, would be removed at the north side of the building due to construction impacts and fair/poor health. The project includes a Below Market Rate (BMR) Agreement for the payment of an in lieu fee or the delivery of equivalent off-site units.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: the project. c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project. d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes. e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division. f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits. g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the Project Arborist's recommendations. 6. Approve the use permit and architectural subject to the following <i>project-specific</i> conditions: a. Concurrent with the submittal of a complete building permit application, the applicant shall submit a plan showing the location of the shuttle stop and signage, and apply for an encroachment permit if applicable. The submitted plan shall also show a connection from the proposed central pedestrian entry path to the crosswalk at the western side of the O'Brien Drive and Adams Drive intersection. The shuttle stop location and signage, as well as the connection between the pedestrian path and the crosswalk, would be subject to review and approval of the Engineering, Transportation, and Planning Divisions. b. The property owner shall retain a qualified transportation consulting firm to monitor the trips to and from the project site and evaluate the effectiveness of the TDM program one year from commencement of operations within the subject building and shall submit a memorandum/report to the City reporting on the results of such monitoring for review by the			

1430 O'Brien Drive – Attachment A: Recommended Actions

LOCATION: 1430 O'Brien Drive	PROJECT NUMBER: PLN2016-00014	APPLICANT: DES Architects + Engineers	OWNER: Menlo Business Park, LLC
REQUEST: Request for a use permit and architectural control to partially convert, expand, and architecturally update an existing research and development (R&D) building to create a new cafe and fitness and health center, additional R&D spaces, and provide new landscaping to the subject property which is located in the M-2 (General Industrial) zoning district. As part of the project, the applicant is requesting approval of a use-based parking reduction based on the uses within the building, the proposed tenants' operations, and a Transportation Demand Management (TDM) program. In addition, two heritage flowering pear trees, 19 and 17 inches in diameter, would be removed at the north side of the building due to construction impacts and fair/poor health. The project includes a Below Market Rate (BMR) Agreement for the payment of an in lieu fee or the delivery of equivalent off-site units.			
DECISION ENTITY: Planning Commission	DATE: July 25, 2016	ACTION: TBD	
VOTE: TBD (Barnes, Combs, Goodhue, Kahle, Onken, Riggs, Strehl)			
ACTION: City to determine the effectiveness of the TDM program (Attachment F). This report shall be submitted annually to the City subject to review by the Planning and Transportation Divisions. If the subject site is not in compliance with the anticipated trip reductions from the TDM program the applicant shall submit a detailed mitigation and monitoring plan identifying steps to be taken to bring the project site into compliance with the maximum Daily, AM and PM trips identified in the trip generation analysis and TDM program. c. Prior to issuance of a building permit, the applicant shall provide written status identifying the completion of, or where applicable, on-going compliance with the ten follow-up items listed in June 29, 2016 minutes of the SFPUC Project Review Committee. d. Prior to building permit issuance, the applicant shall pay a Transportation Impact Fee (TIF) at a restaurant rate of \$4.63 per square foot of gross floor area (GFA), at a health/fitness club rate of \$3,107.87 each of the 33 PM peak hour trips, and at an R&D rate of \$3.33 per square foot of GFA for a total estimated TIF of \$145,085.81, subject to the Municipal Code Section 13.26. The fee rate is subject to change annually on July 1 and the final calculation will be based upon the rate at the time of fee payment. The TIF rate is adjusted each year based on the ENR Construction Cost Index percentage change for San Francisco.			



City of Menlo Park
Location Map
1430 O'Brien Drive



Scale: 1:3,600

Drawn By: TAS

Checked By: THR

Date: 7/25/2016

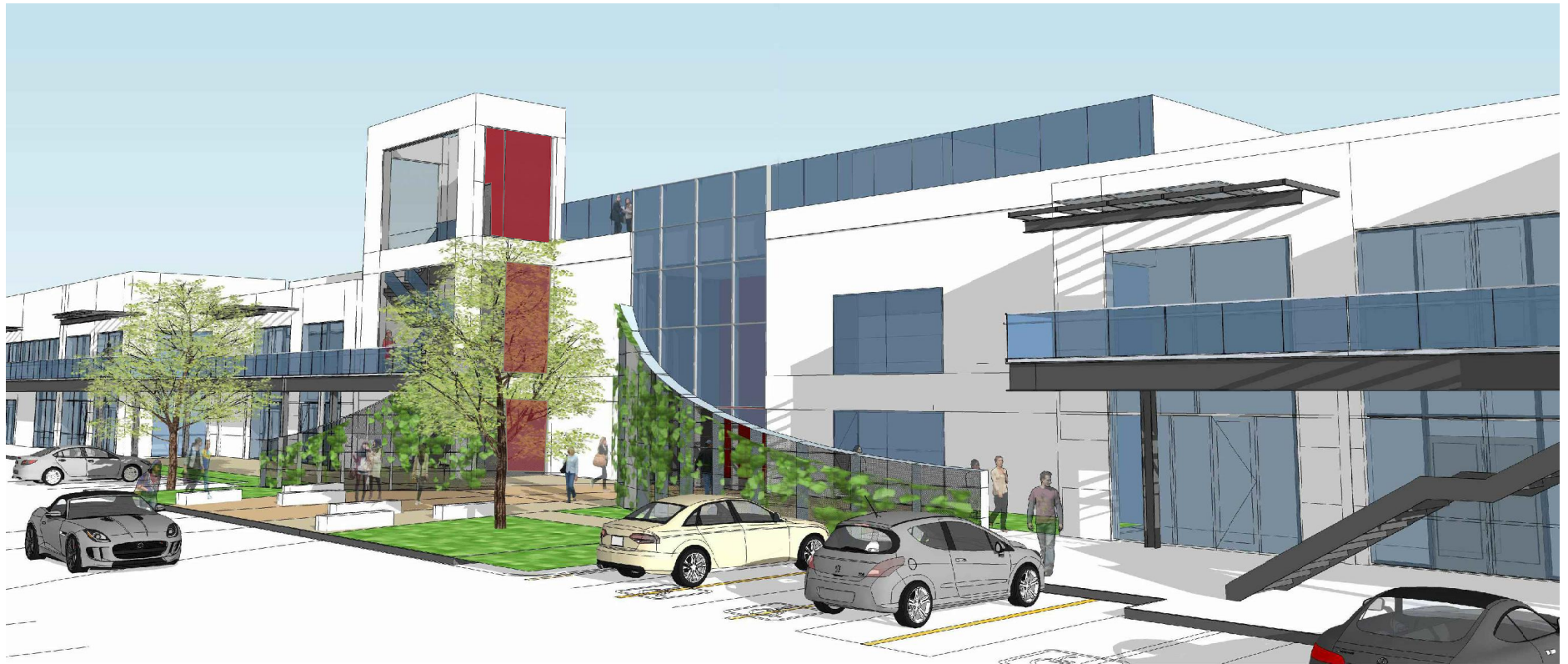
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MENLO BUSINESS PARK BLDG. 7 AMENITIES & RENOVATION

1430 O'BRIEN DRIVE, MENLO PARK, CALIFORNIA 94025

PLANNING APPLICATION RESUBMITTAL

JULY 11, 2016



© 2016



Menlo Business Park Bldg. 7 - Amenities & Renovation

1530 O'Brien Drive, Suite C
Menlo Park, CA 94025

1430 O'Brien Drive, Menlo Park, CA. 94025

DES Project Number: 2730,61

AERIAL VICINITY MAP

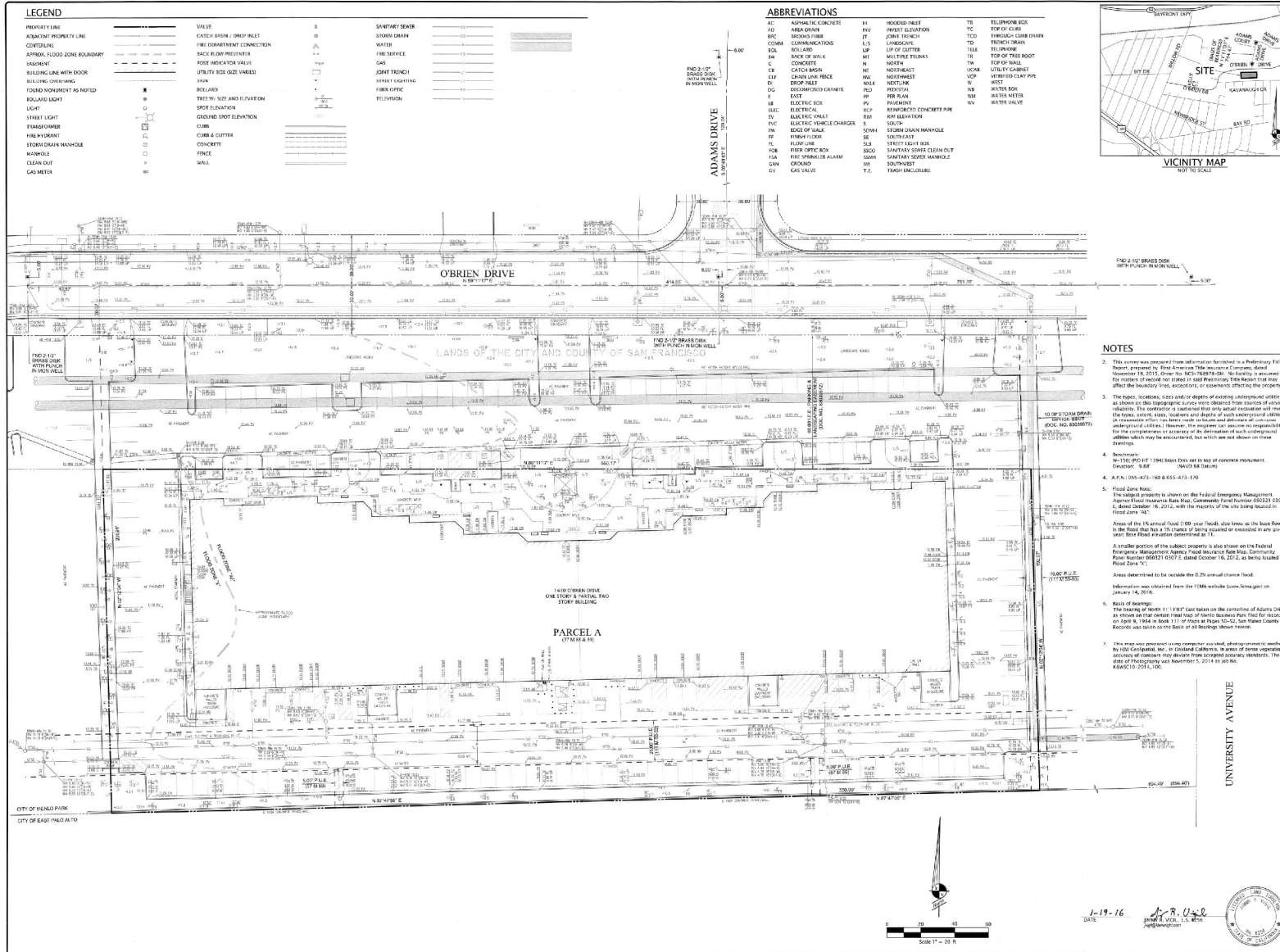
01.20.2016	Planning Pre-Application Submittal
02.01.2016	Planning Application Submittal
04.26.2016	Planning Application Resubmittal
07.11.2016	Planning Application Resubmittal

2

DES

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11/18/2016 8:44am C:\Users\khanh\Documents\273001\Map 01\273001.dwg (11/18/2016 2:25:04) User: khanh Plotting: PlotSheet 1 - TOPOGRAPHIC SURVEY FOR EXISTING SITE.dwg



1 TOPOGRAPHIC SURVEY 1

TARTLTON Menlo Business Park Bldg. 7 - Amenities & Renovation

1530 O'Brien Drive, Suite C
Menlo Park, CA 94025

1430 O'Brien Drive, Menlo Park, CA. 94025

DES Project Number: 2730.61

TOPOGRAPHIC SURVEY FOR EXISTING SITE

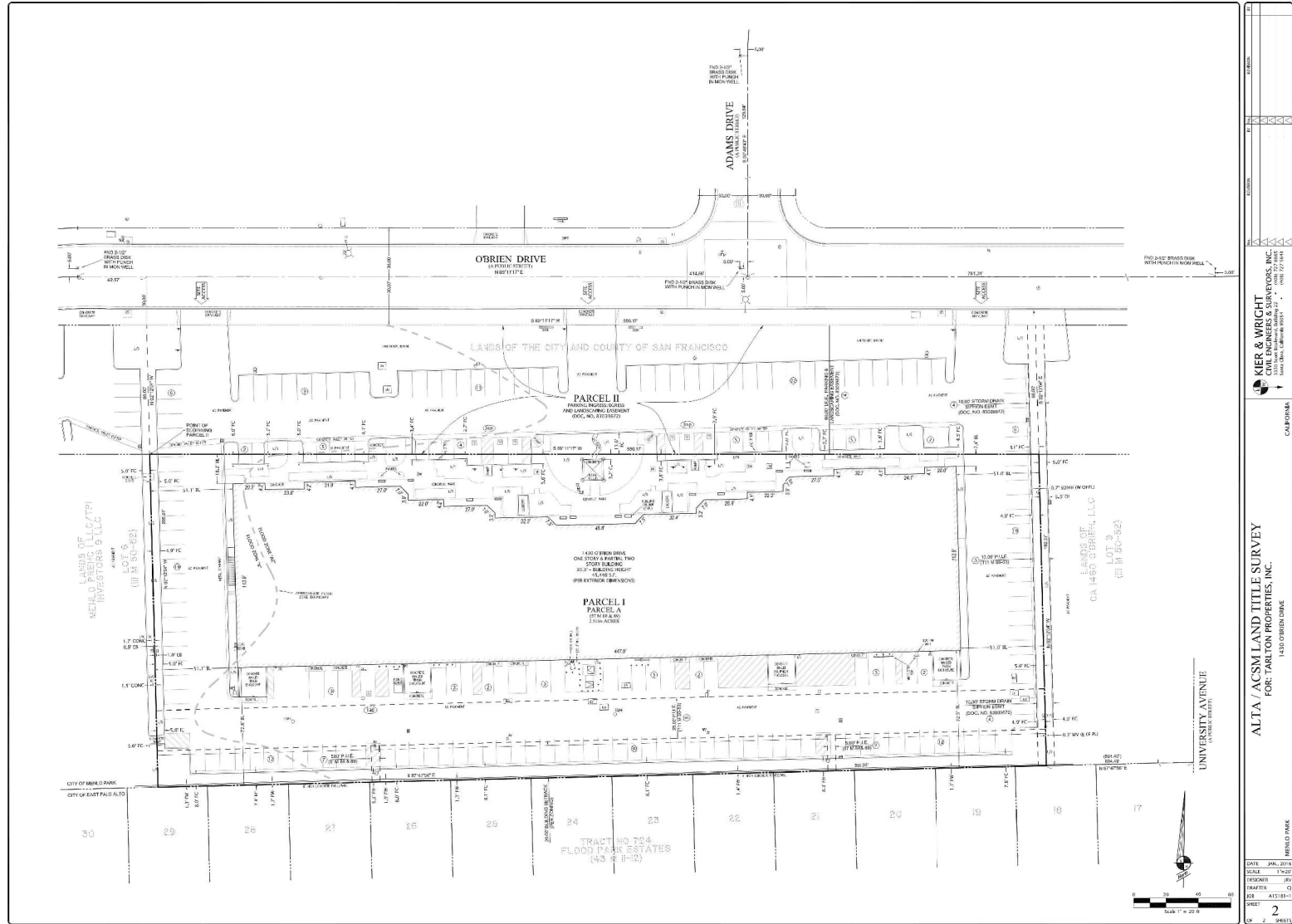
01.20.2016 Planning Pre-Application Submittal
02.01.2016 Planning Application Submittal
04.26.2016 Planning Application Resubmittal
07.11.2016 Planning Application Resubmittal

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© 2016

DES

1/11/16 10:16 AM 273061 DES Project Number: 273061 DES Project Number: 273061 DES Project Number: 273061 DES Project Number: 273061



1 EXISTING ALTA SITE SURVEY



Menlo Business Park Bldg. 7 - Amenities & Renovation

1530 O'Brien Drive, Suite C
Menlo Park, CA 94025

1430 O'Brien Drive, Menlo Park, CA. 94025

DES Project Number: 2730,61

ALTA SURVEY FOR EXISTING SITE

01.20.2016 Planning Pre-Application Submittal
02.01.2016 Planning Application Submittal
04.26.2016 Planning Application Resubmittal
07.11.2016 Planning Application Resubmittal

3C

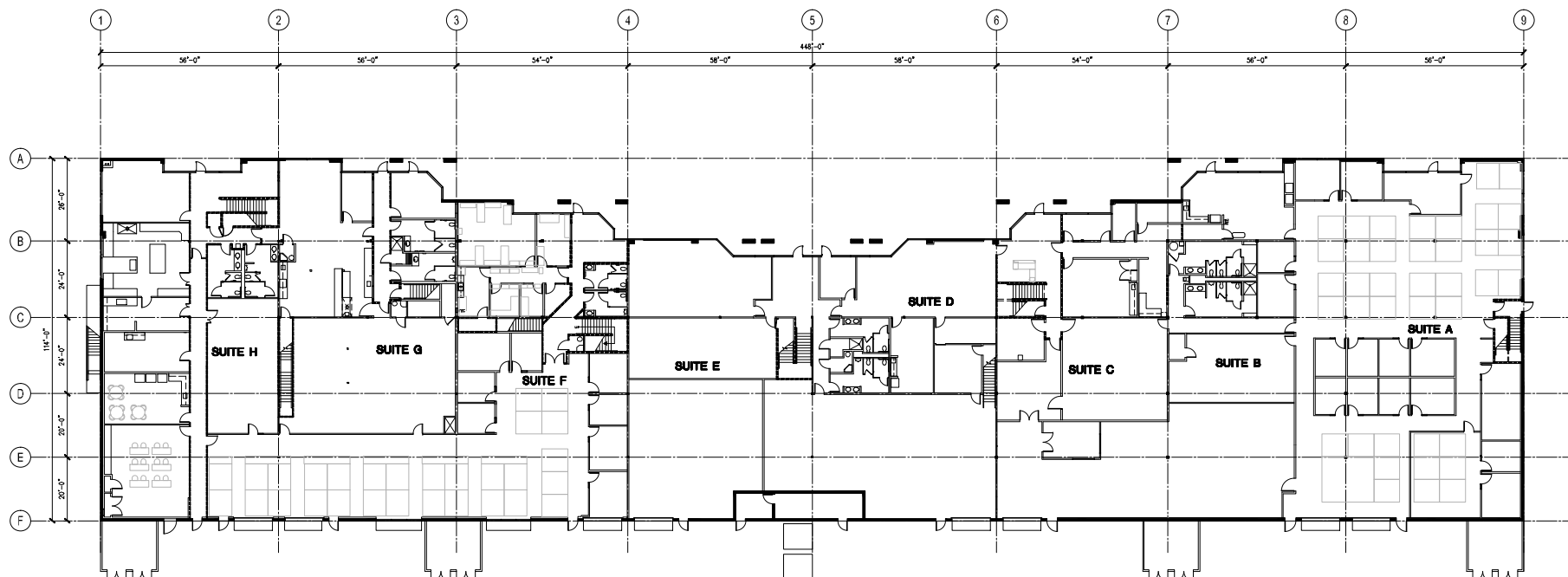
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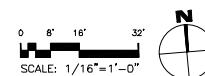
SCALE: NTS

DES

Jul 18, 2016 - 8:00am C:\Users\hansw\Documents\273061\Bldg 7\04025\273061\Bldg 7\Arch\Planning Package\1 - Existing First Floor Plan.dwg



1 EXISTING FIRST FLOOR PLAN
3/32" = 1'-0"



1530 O'Brien Drive, Suite C
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Menlo Business Park Bldg. 7 - Amenities & Renovation

1430 O'Brien Drive, Menlo Park, CA. 94025

DES Project Number: 2730,61

EXISTING FIRST FLOOR PLAN

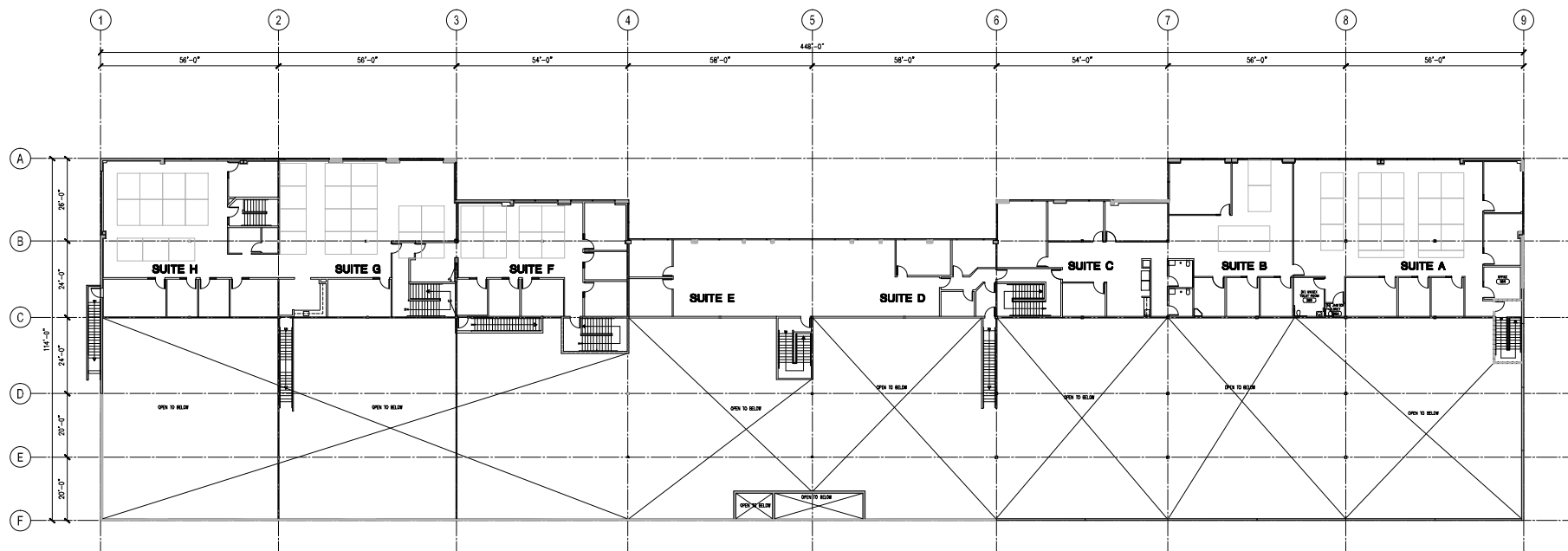
01.20.2016 Planning Pre-Application Submittal
02.01.2016 Planning Application Submittal
04.26.2016 Planning Application Resubmittal
07.11.2016 Planning Application Resubmittal

4

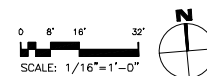
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Jul 18, 2016 - 8:04am C:\Users\hewlett\Documents\27300\Bldg 7\04000\27300\Bldg 7\Arch\Planning Package\15 - Existing Second Floor Plan.dwg



1 EXISTING SECOND FLOOR PLAN
1/16" = 1'-0"



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1430 O'Brien Drive, Menlo Park, CA. 94025

DES Project Number: 2730,61

EXISTING SECOND FLOOR PLAN

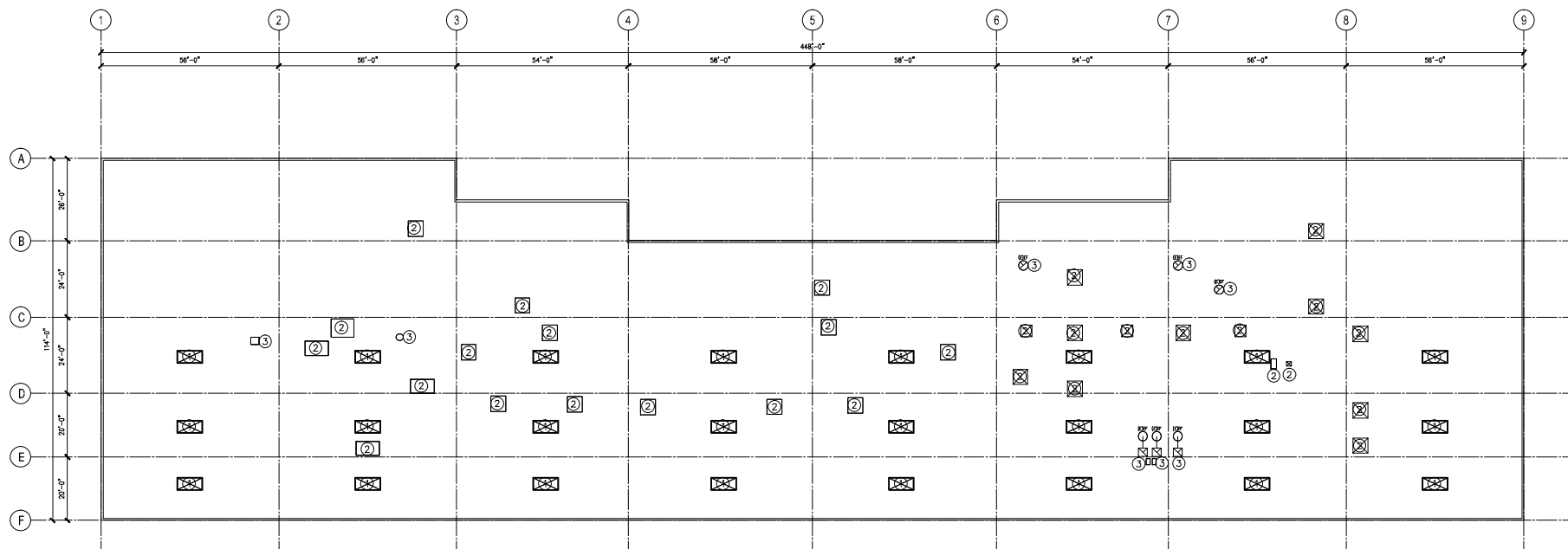
01.20.2016	Planning Pre-Application Submittal
02.01.2016	Planning Application Submittal
04.26.2016	Planning Application Resubmittal
07.11.2016	Planning Application Resubmittal

5

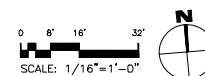
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- PLAN NOTES:
- ① (E) SKYLIGHT
 - ② (E) HVAC UNIT
 - ③ (E) FAN



① EXISTING ROOF PLAN
1/16" = 1'-0"



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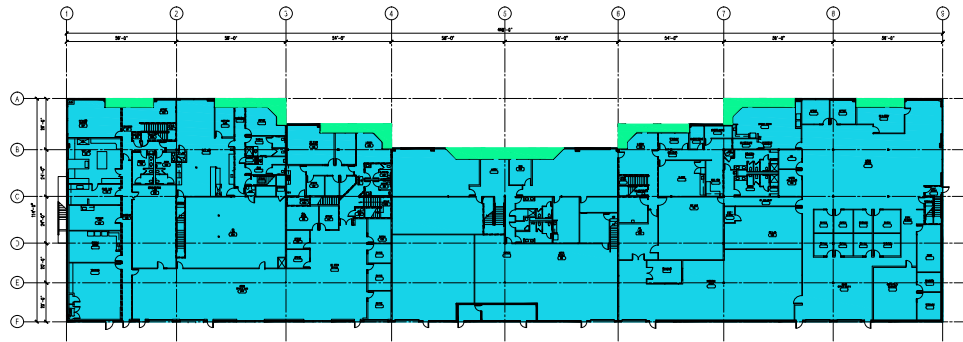
EXISTING ROOF PLAN

01.20.2016 Planning Pre-Application Submittal
02.01.2016 Planning Application Submittal
04.26.2016 Planning Application Resubmittal
07.11.2016 Planning Application Resubmittal

6

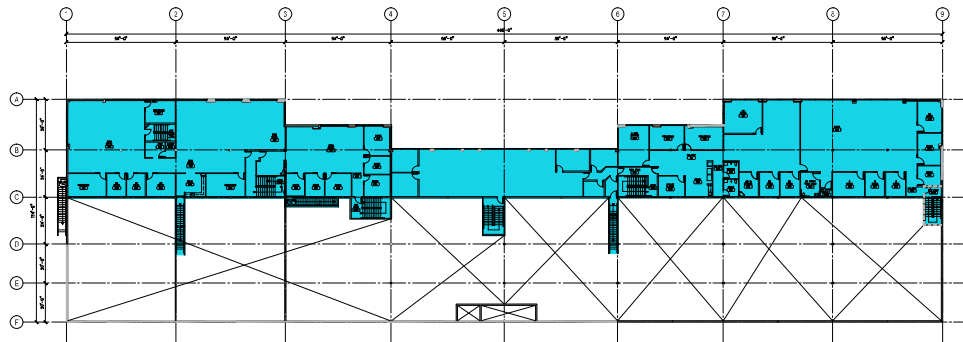
© 2016





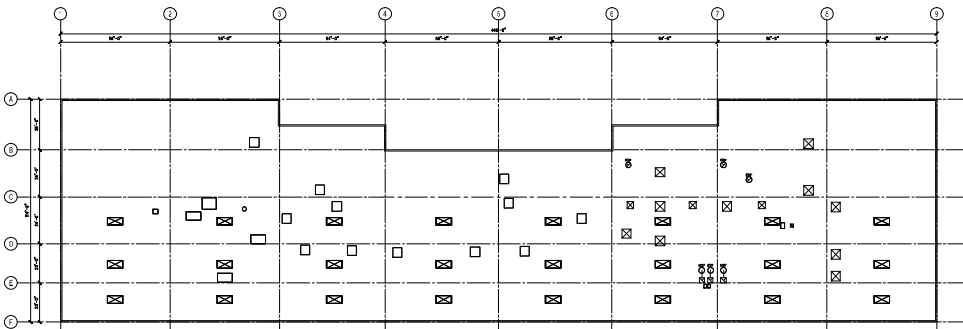
FIRST FLOOR AREA: EXISTING USE: R&D	45,496 SQ. FT.
UNENCLOSED AREA :	1,253 SQ. FT.
FIRST FLOOR AREA :	46,749 SQ. FT.

① EXISTING FIRST FLOOR PLAN
1" = 30'-0"



SECOND FLOOR: EXISTING USE: R&D	19,203 SQ. FT.
------------------------------------	----------------

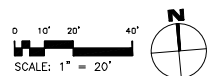
② EXISTING SECOND FLOOR PLAN
1" = 30'-0"



ROOF:
NON-OCCUPIED

EXISTING R&D USE: 65,952 SQ. FT.

③ EXISTING ROOF PLAN
1" = 30'-0"



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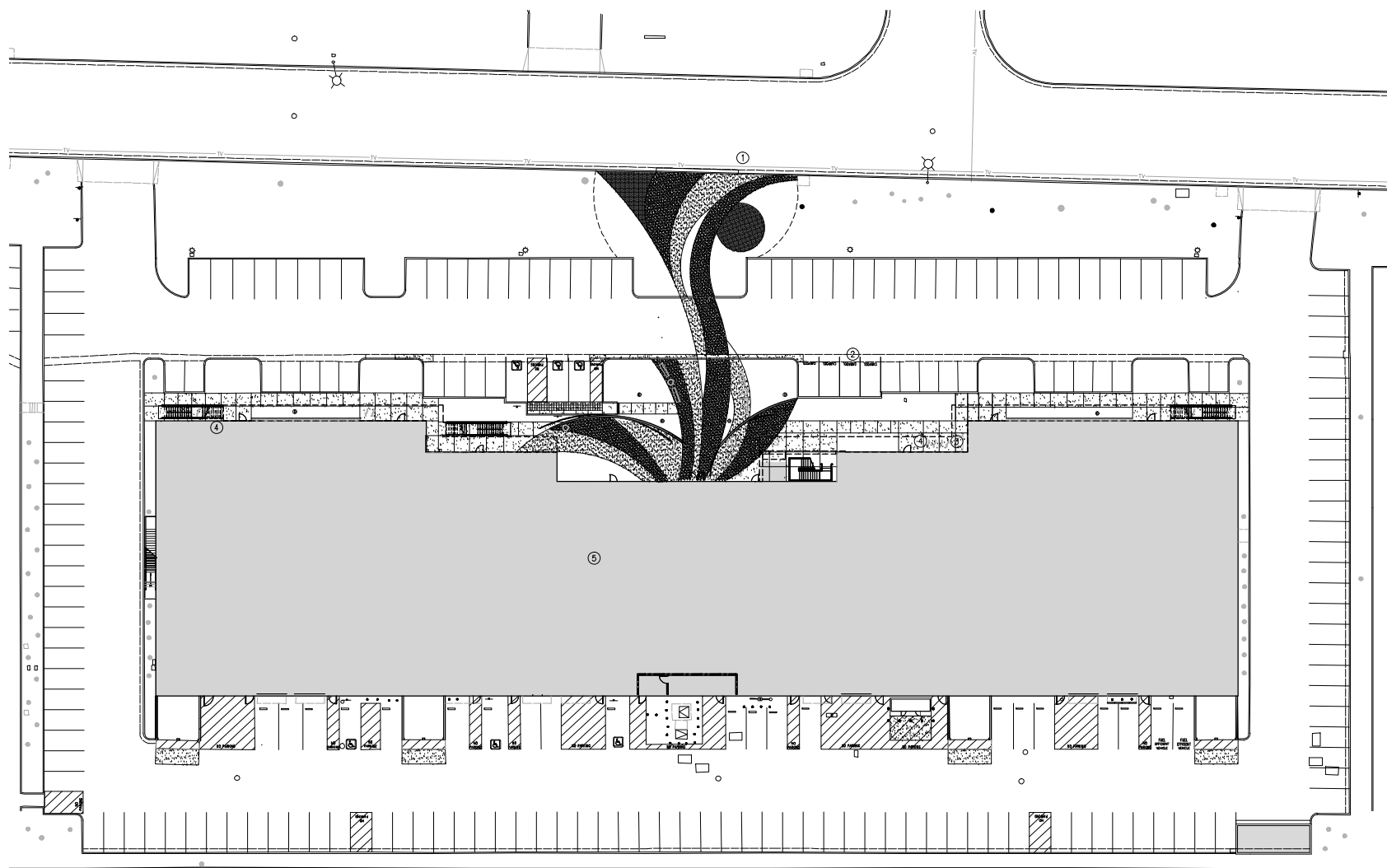
EXISTING GFA DIAGRAMS & BUILDING USE

01.20.2016 Planning Pre-Application Submittal
02.01.2016 Planning Application Submittal
04.26.2016 Planning Application Resubmittal
07.11.2016 Planning Application Resubmittal

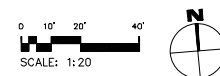
7

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1 TRANSPORTATION DEMAND MANAGEMENT (TDM) SITE PLAN
1/16" = 1'-0"



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TRANSPORTATION DEMAND MANAGEMENT
(TDM) SITE PLAN

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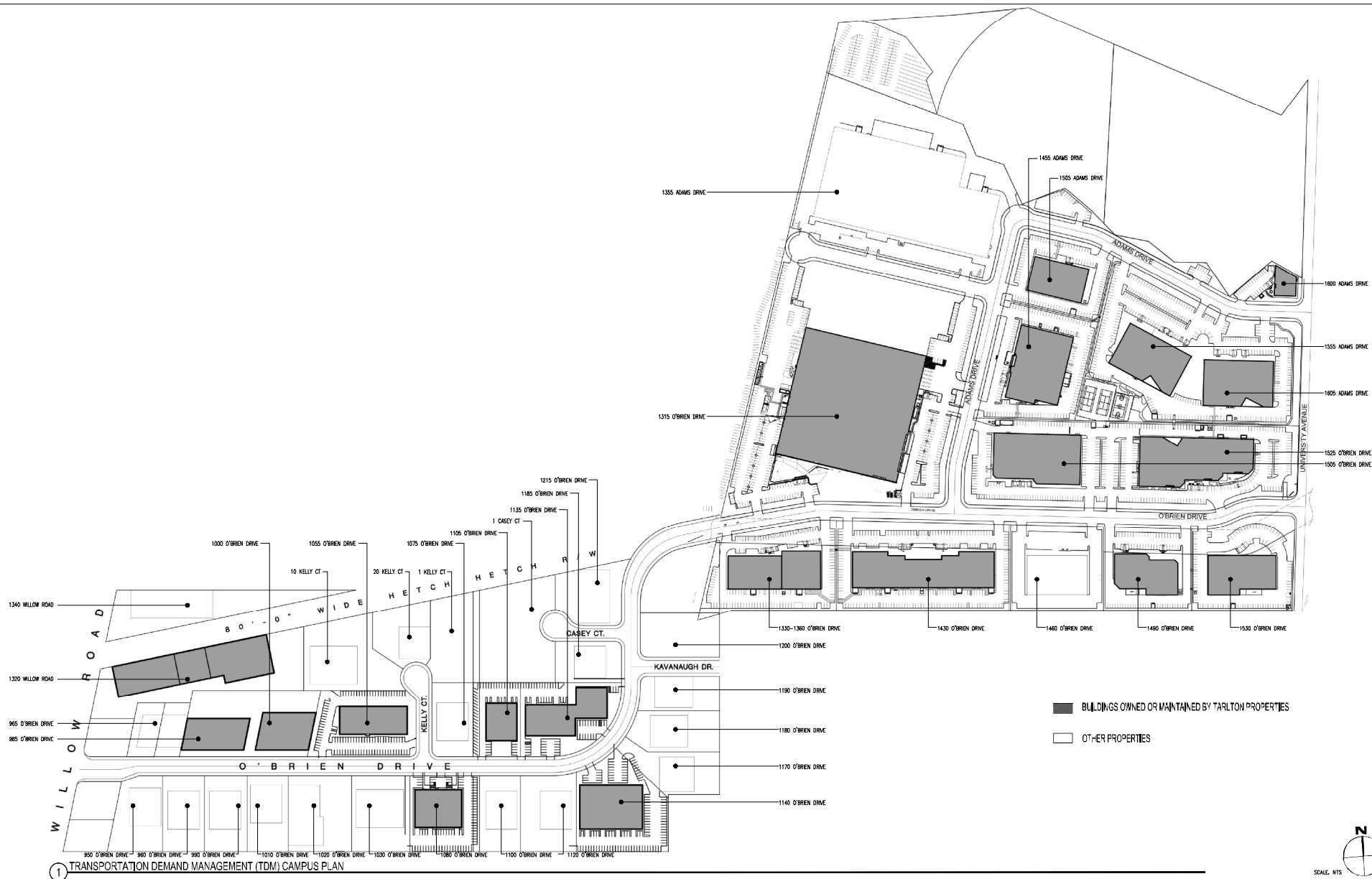
TRANSPORTATION DEMAND MANAGEMENT
(TDM) CAMPUS PLAN

01.20.2016	Planning Pre-Application Submittal
02.01.2016	Planning Application Submittal
04.26.2016	Planning Application Resubmittal
07.11.2016	Planning Application Resubmittal

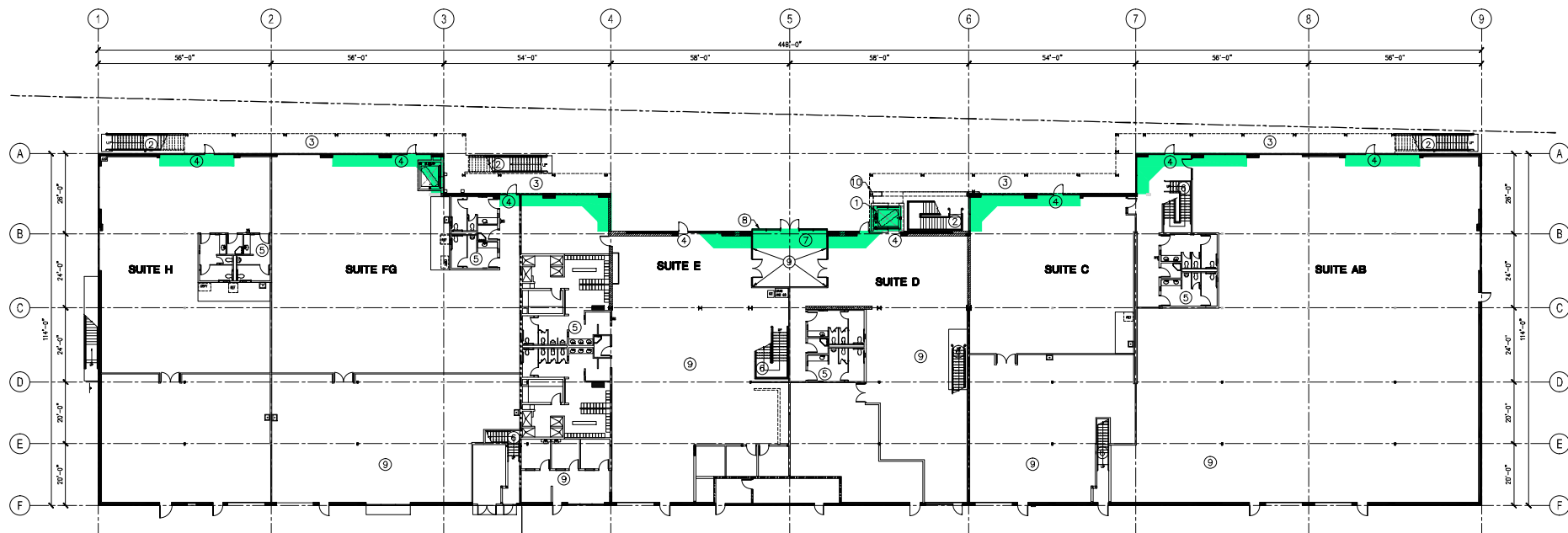
9B

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July 18, 2016 - 8:00am C:\Users\james\OneDrive\Documents\273061\Bldg 7\040202\273061\Bldg 7\Arch\Planning Package\10A - PROPOSED SHELL FIRST FLOOR PLAN.dwg



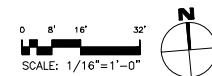
LEGEND:

NEW BUILDING AREA
PROPERTY LINE
SEE SHEET BA FOR
FULL EXTENT OF
PROPERTY LINE

PLAN NOTES:

- | | |
|---|--|
| ① NEW ELEVATOR TOWER | ⑥ NEW INTERIOR STAIRWAY |
| ② NEW OPEN STAIRWAY | ⑦ NEW ALUMINUM STOREFRONT WINDOW AND ENTRY |
| ③ NEW WALKWAY ABOVE | ⑧ NEW ENTRANCE FEATURE |
| ④ NEW TENANT SUITE ENTRY | ⑨ NEW 2 STORIES OPEN SPACE |
| ⑤ NEW RESTROOMS, SHOWER & JANITOR CLOSET WHERE OCCURS | ⑩ NEW VERTICAL CIRCULATION TOWER |

① PROPOSED SHELL FIRST FLOOR PLAN
1/16" = 1'-0"



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DES Project Number: 2730,61

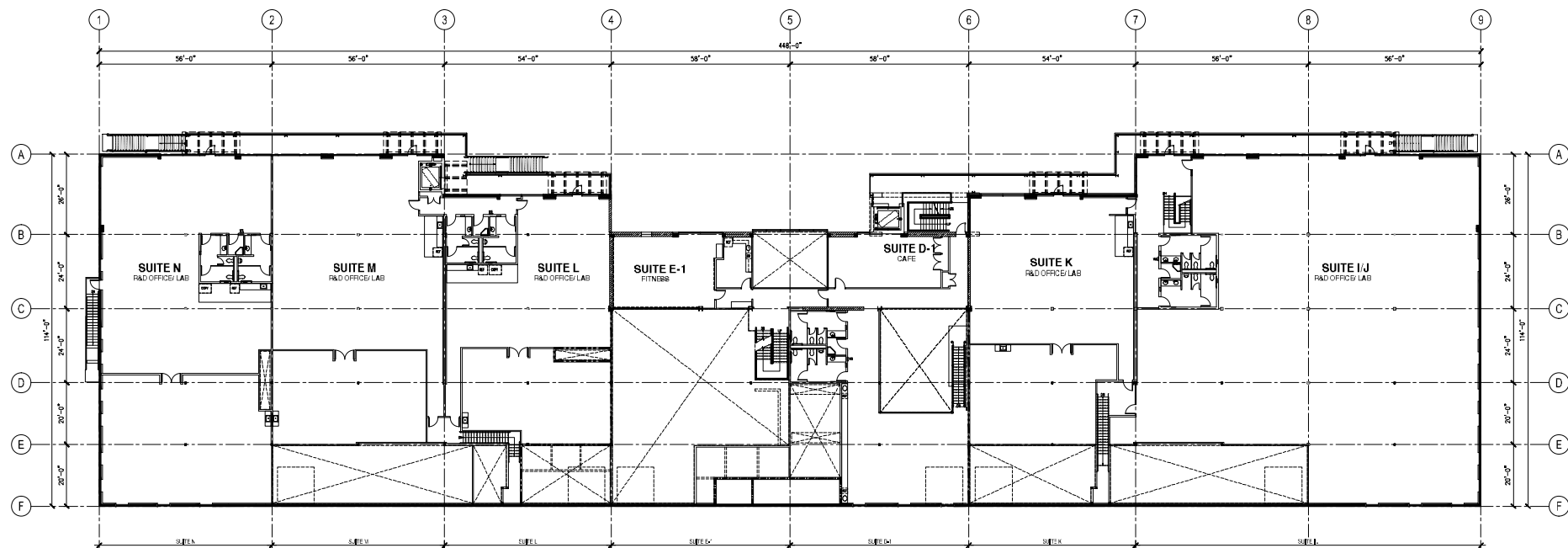
PROPOSED SHELL FIRST FLOOR PLAN

01.20.2016	Planning Pre-Application Submittal
02.01.2016	Planning Application Submittal
04.26.2016	Planning Application Resubmittal
07.11.2016	Planning Application Resubmittal

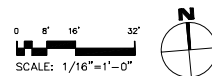
10A

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1 PROPOSED TENANT IMPROVEMENT SECOND FLOOR PLAN
1/16" = 1'-0"



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DES Project Number: 2730.61

PROPOSED TENANT IMPROVEMENT SECOND FLOOR PLAN

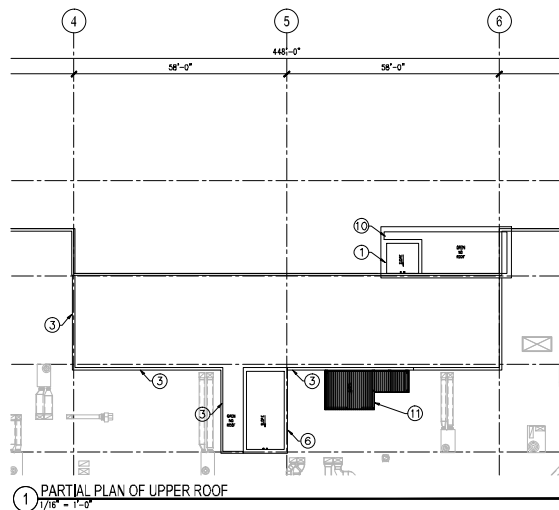
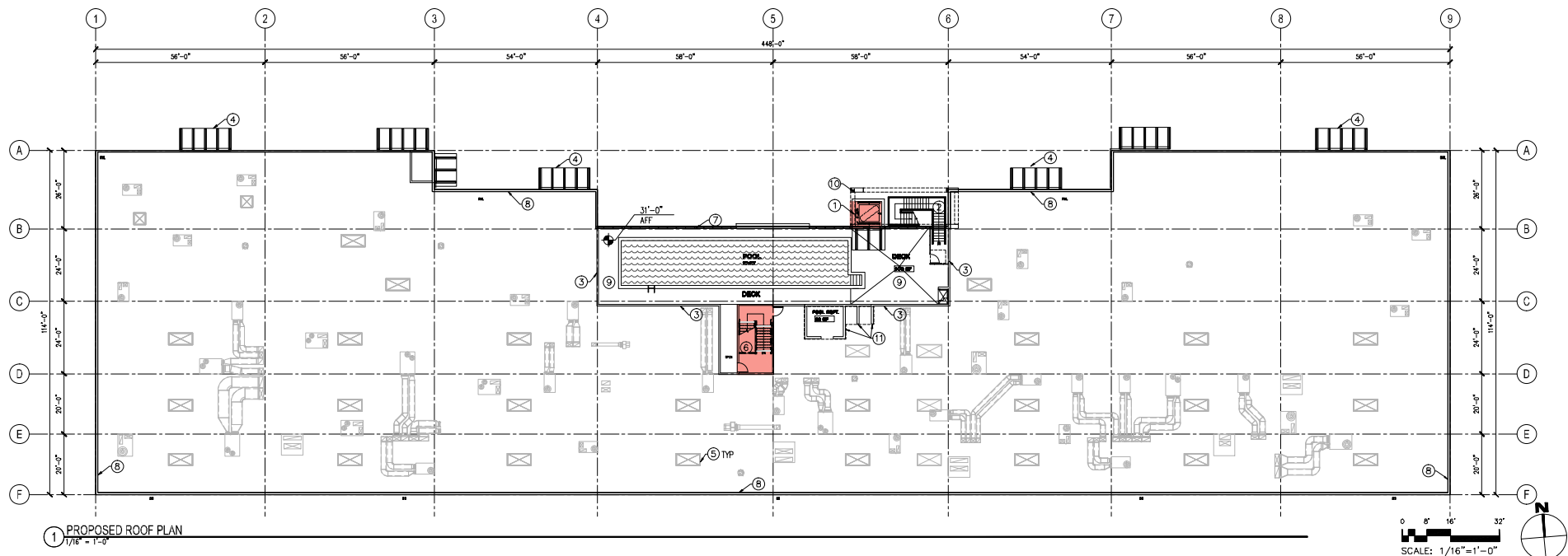
01.20.2016	Planning Pre-Application Submittal
02.01.2016	Planning Application Submittal
04.26.2016	Planning Application Resubmittal
07.11.2016	Planning Application Resubmittal

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J:\18_2016 - 800mm - C:\Users\james@des.com\AppData\Local\Temp\273061\Drawings\Roof\Roofing Package\12 - Proposed Roof Plan.dwg



PLAN NOTES:

- | | | |
|-------------------------------|-----------------------------------|-----------------------------|
| 1 NEW ELEVATOR TOWER | 6 NEW INTERIOR STAIRWAY | 11 POOL EQUIPMENT ENCLOSURE |
| 2 NEW OPEN STAIRWAY | 7 NEW GLASS GUARD RAIL | |
| 3 NEW ROOF SCREEN | 8 EXISTING ROOF SCREEN TO REMAIN | |
| 4 NEW CANOPY BELOW | 9 RAISED POOL DECK AT 31'-0" | |
| 5 EXISTING SKYLIGHT TO REMAIN | 10 NEW VERTICAL CIRCULATION TOWER | |



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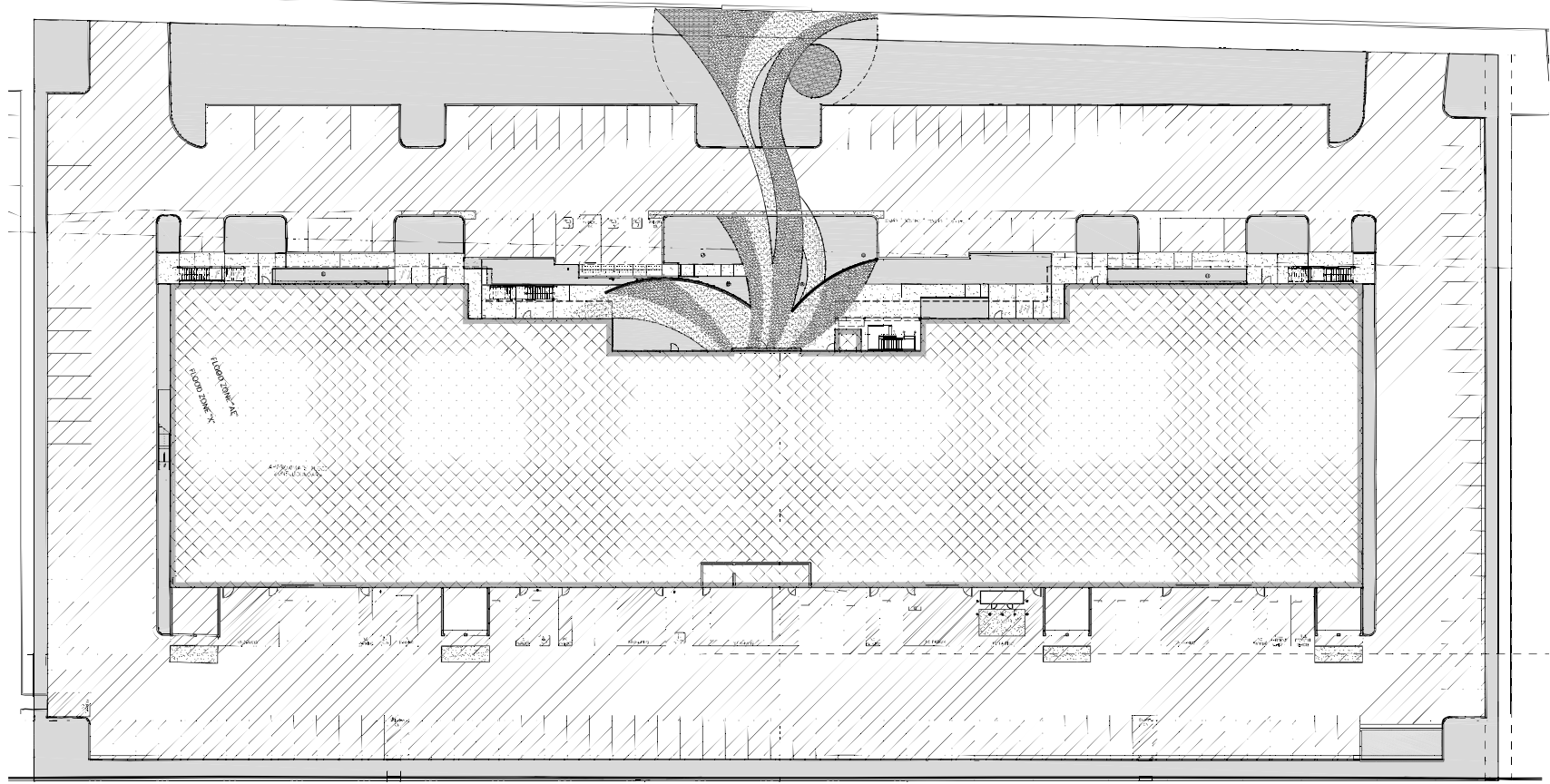
PROPOSED ROOF PLAN

01.20.2016 Planning Pre-Application Submittal
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 04.26.2016 Planning Application Resubmittal
 07.11.2016 Planning Application Resubmittal

12

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PROJECT DATA

1 SITE AREA

A PROJECT SITE AREA:	153,767 SQ. FT.
COVERAGE	
Building	46,848 SQ. FT.
Elevated Walkway	2,048 SQ. FT.
TOTAL	48,894 SQ. FT. (31.8%)
IMPERVIOUS	
Driveways/Parking Spaces	70,990 SQ. FT.
Sidewalks/Walkways	7,308 SQ. FT.
TOTAL	78,298 SQ. FT. (50.9%)
PERVIOUS AREA	
Landscaped Area	25,999 SQ. FT.
Non-Compacted DG	578 SQ. FT.
TOTAL	26,577 SQ. FT. (17.3%)

2 LEGEND

	BUILDING FOOTPRINT
	PERVIOUS AREA
	PAVED SIDEWALKS, WALKWAY, CURBS
	DRIVEWAYS / PARKING SPACES



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DES Project Number: 2730.61

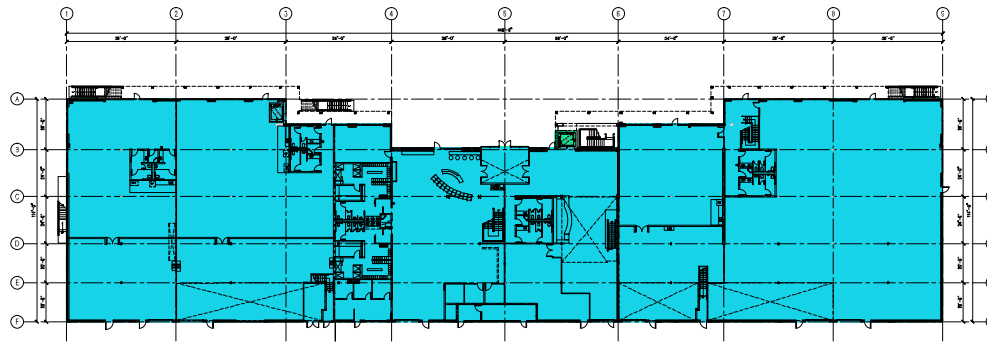
PROPOSED SITE AREA & BUILDING COVERAGE CALCULATION PLAN

01/20/2016 Planning Pre-Application Submittal
02/01/2016 Planning Application Submittal
04/26/2016 Planning Application Resubmittal
07/11/2016 Planning Application Resubmittal

14



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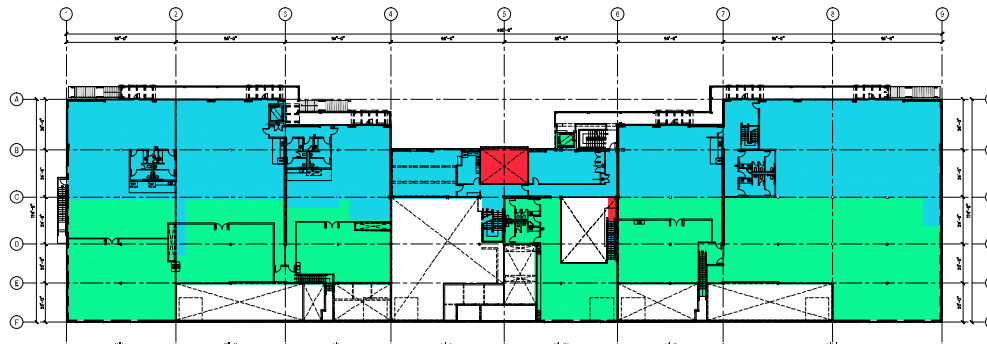
1 PROPOSED FIRST FLOOR PLAN
1" = 30'-0"

AREA TO REMAIN

EXISTING FIRST FLOOR AREA 46,749 SF

ADDITIONAL AREA +99 SF

PROPOSED FIRST FLOOR AREA 46,848 SF



2 PROPOSED SECOND FLOOR PLAN
1" = 30'-0"

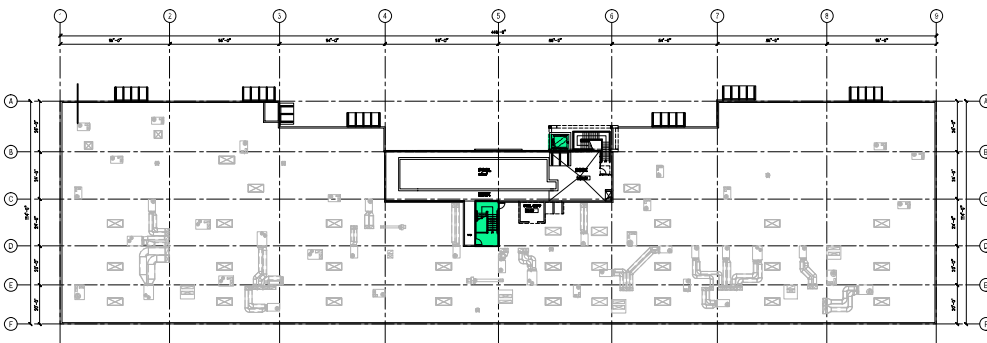
AREA TO REMAIN

EXISTING SECOND FLOOR AREA 19,203 SF

SUBTRACTED AREA -492 SF

ADDITIONAL AREA +18,638 SF

PROPOSED SECOND FLOOR AREA 37,349 SF



3 PROPOSED ROOF PLAN
1" = 30'-0"

EXISTING ROOF

ADDITIONAL AREA +365 SF

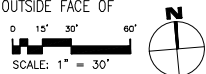
TOTAL EXISTING FLOOR AREA 65,952 SF

TOTAL SUBTRACTED FLOOR AREA -492 SF

TOTAL ADDITIONAL FLOOR AREA +19,102 SF

TOTAL PROPOSED FLOOR AREA 84,562 SF

NOTE: GFA ARE MEASURED TO THE OUTSIDE FACE OF EXTERIOR WALL



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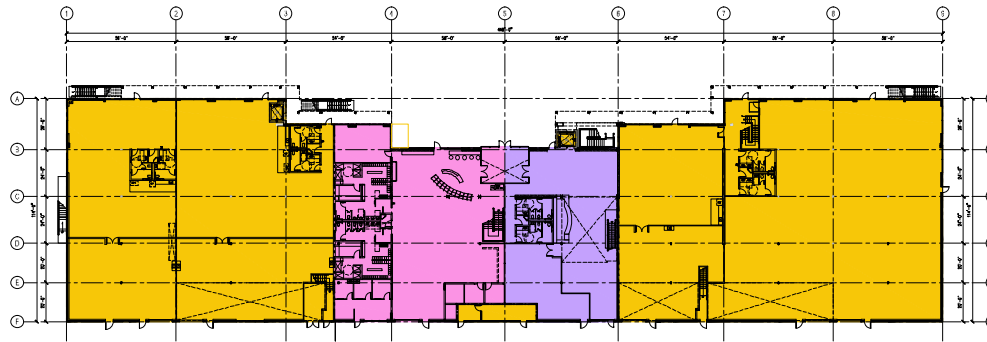
PROPOSED BUILDING GFA DIAGRAMS

01.20.2016 Planning Pre-Application Submittal
02.01.2016 Planning Application Submittal
04.26.2016 Planning Application Resubmittal
07.11.2016 Planning Application Resubmittal

15

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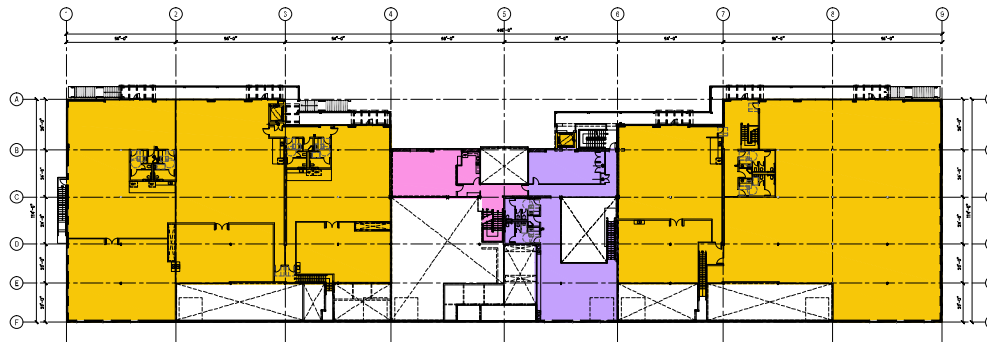
① PROPOSED FIRST FLOOR PLAN
1" = 30'-0"

PREVIOUS FIRST FLOOR AREA: 46,749 SQ. FT.
PROPOSED FIRST FLOOR AREA: 46,846 SQ. FT.

FIRST FLOOR AREA: 33,988 SQ. FT.
USE: R&D

FIRST FLOOR AREA: 7,866 SQ. FT.
PROPOSED USE: FITNESS

FIRST FLOOR AREA: 5,024 SQ. FT.
PROPOSED USE: CAFE



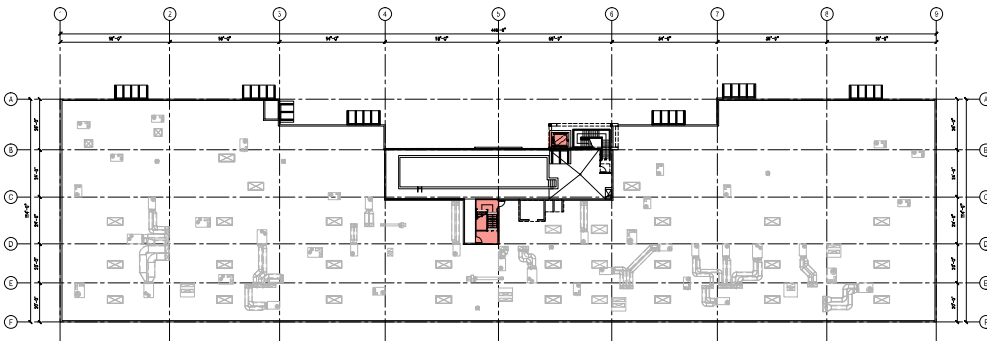
② PROPOSED SECOND FLOOR PLAN
1" = 30'-0"

PREVIOUS SECOND FLOOR AREA: 19,203 SQ. FT.
PROPOSED SECOND FLOOR AREA: 37,349 SQ. FT.

SECOND FLOOR AREA: 32,242 SQ. FT.
USE: R&D

SECOND FLOOR AREA: 1,577 SQ. FT.
PROPOSED USE: FITNESS

SECOND FLOOR AREA: 3,530 SQ. FT.
PROPOSED USE: CAFE

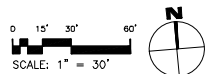


③ PROPOSED ROOF PLAN
1" = 30'-0"

PROPOSED ROOF FLOOR AREA: 365 SQ. FT.

ROOF FLOOR AREA: 365 SQ. FT.
PROPOSED USE: CIRCULATION

TOTAL R&D USE:	66,200 SQ. FT.
TOTAL FITNESS USE:	9,443 SQ. FT.
TOTAL CAFE USE:	8,554 SQ. FT.
TOTAL CIRCULATION ON ROOF USE:	365 SQ. FT.
	84,562 SQ. FT.



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PROPOSED BUILDING USE DIAGRAMS

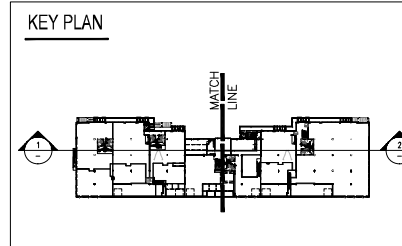
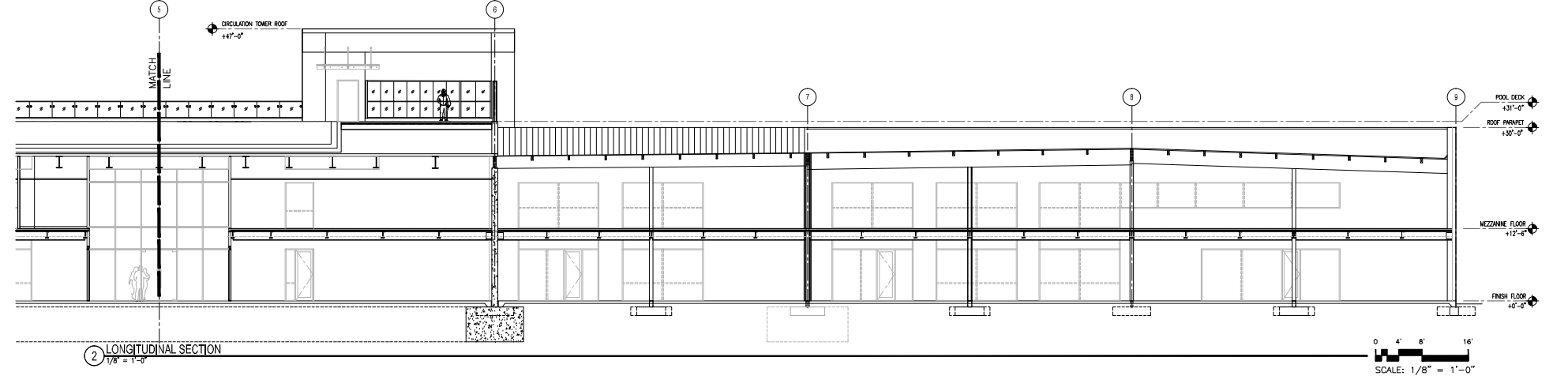
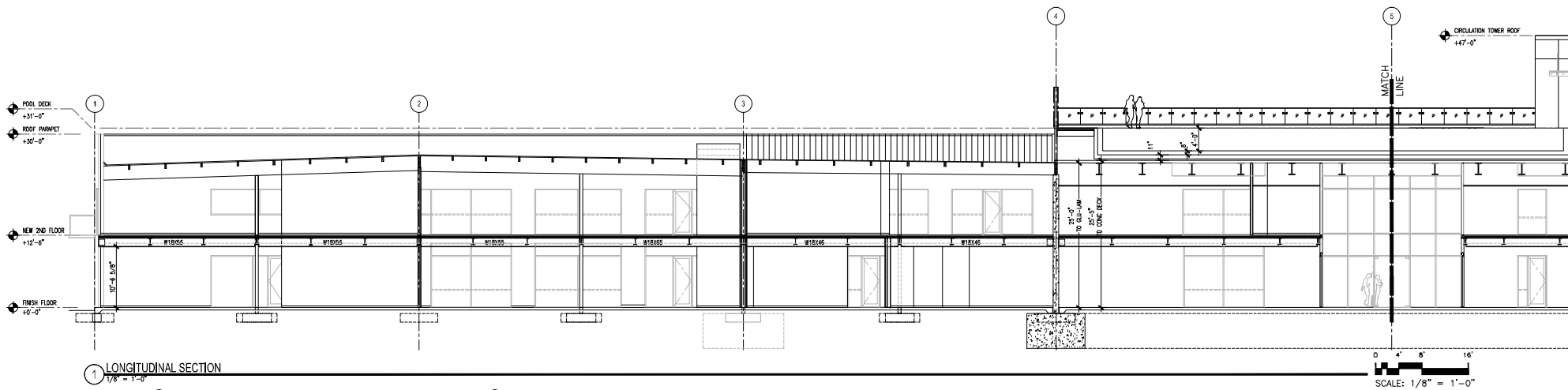
01.20.2016	Planning Pre-Application Submittal
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04.26.2016	Planning Application Resubmittal
07.11.2016	Planning Application Resubmittal

16

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TARLTON Menlo Business Park Bldg. 7 - Amenities & Renovation

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DES Project Number: 2730,61

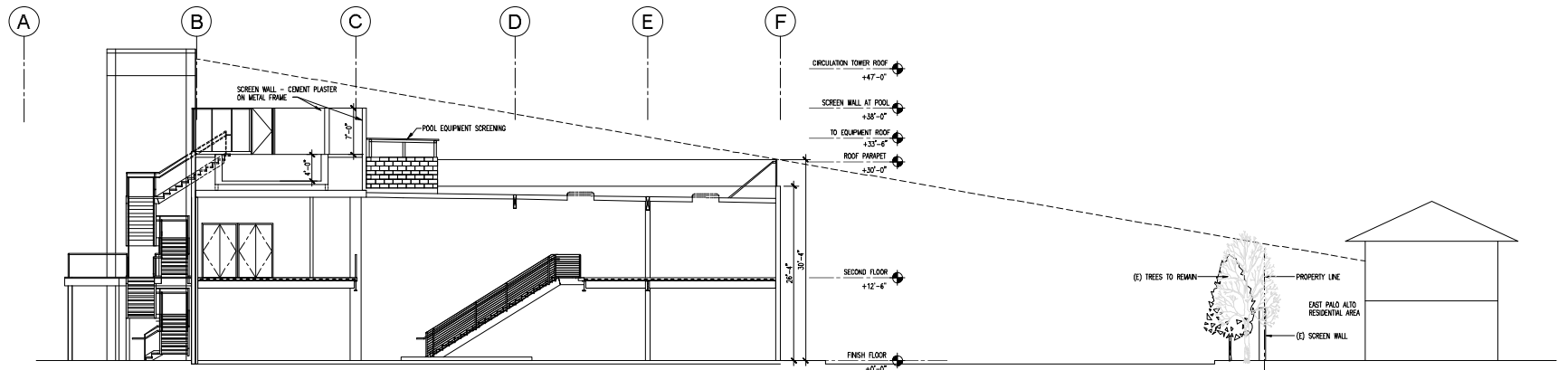
PROPOSED BUILDING SECTIONS

01.20.2016	Planning Pre-Application Submittal
02.01.2016	Planning Application Submittal
04.26.2016	Planning Application Resubmittal
07.11.2016	Planning Application Resubmittal

20

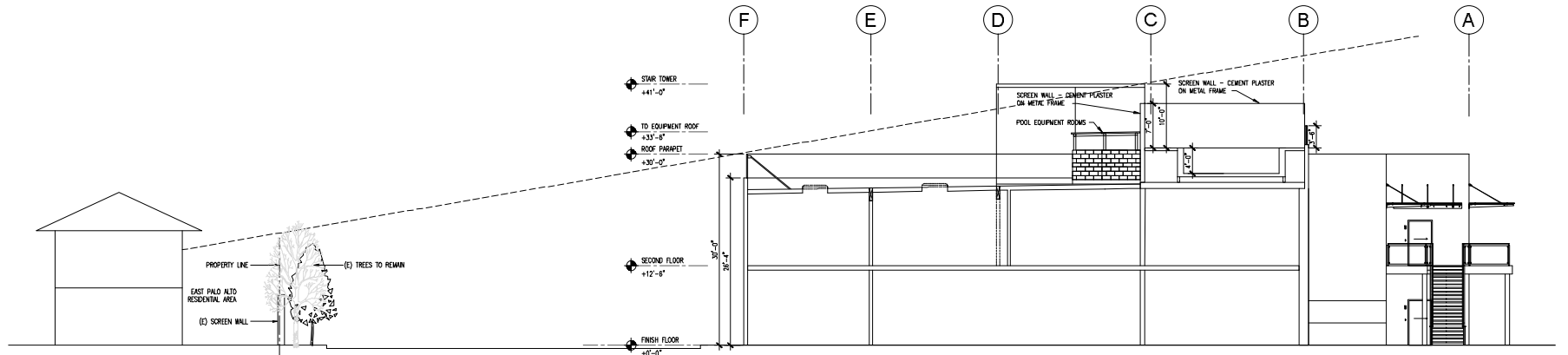
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1 BUILDING SECTION & LINE OF SIGHT AT REAR OF BUILDING
1/8" = 1'-0"

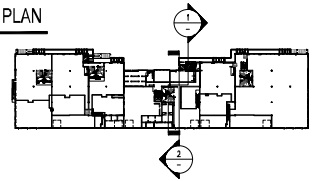
0 4' 8' 16'
SCALE: 1/8" = 1'-0"



2 BUILDING SECTION & LINE OF SIGHT AT REAR OF BUILDING
1/8" = 1'-0"

0 4' 8' 16'
SCALE: 1/8" = 1'-0"

KEY PLAN



Menlo Business Park Bldg. 7 - Amenities & Renovation

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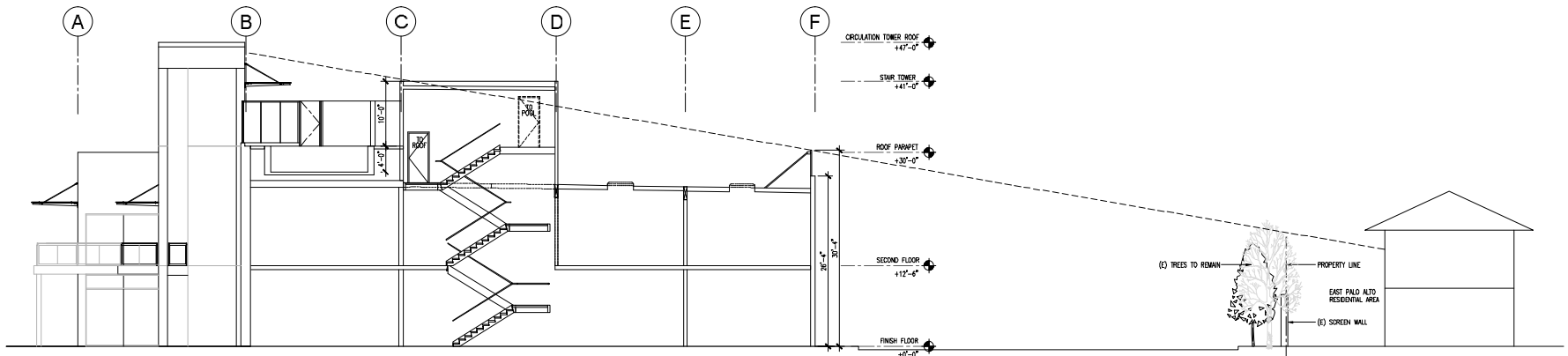
PROPOSED BUILDING SECTIONS

01.20.2016 Planning Pre-Application Submittal
02.01.2016 Planning Application Submittal
04.26.2016 Planning Application Resubmittal
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21

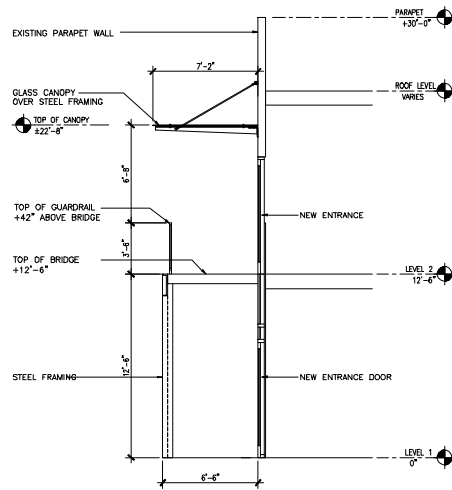
DES

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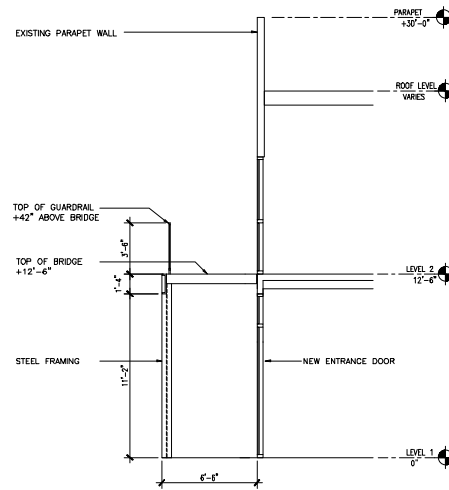


1 BUILDING SECTION & LINE OF SIGHT AT REAR OF BUILDING
1/8" = 1'-0"

0 4' 8' 16'
SCALE: 1/8" = 1'-0"



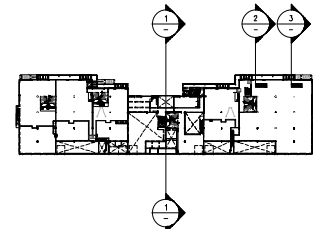
3 BRIDGE SECTION THROUGH CANOPY
1/4" = 1'-0"



2 BRIDGE SECTION THROUGH ENTRY
1/4" = 1'-0"

0 2' 4' 8'
SCALE: 1/4" = 1'-0"

KEY PLAN



Menlo Business Park Bldg. 7 - Amenities & Renovation

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Menlo Park, CA 94025

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DES Project Number: 2730,61

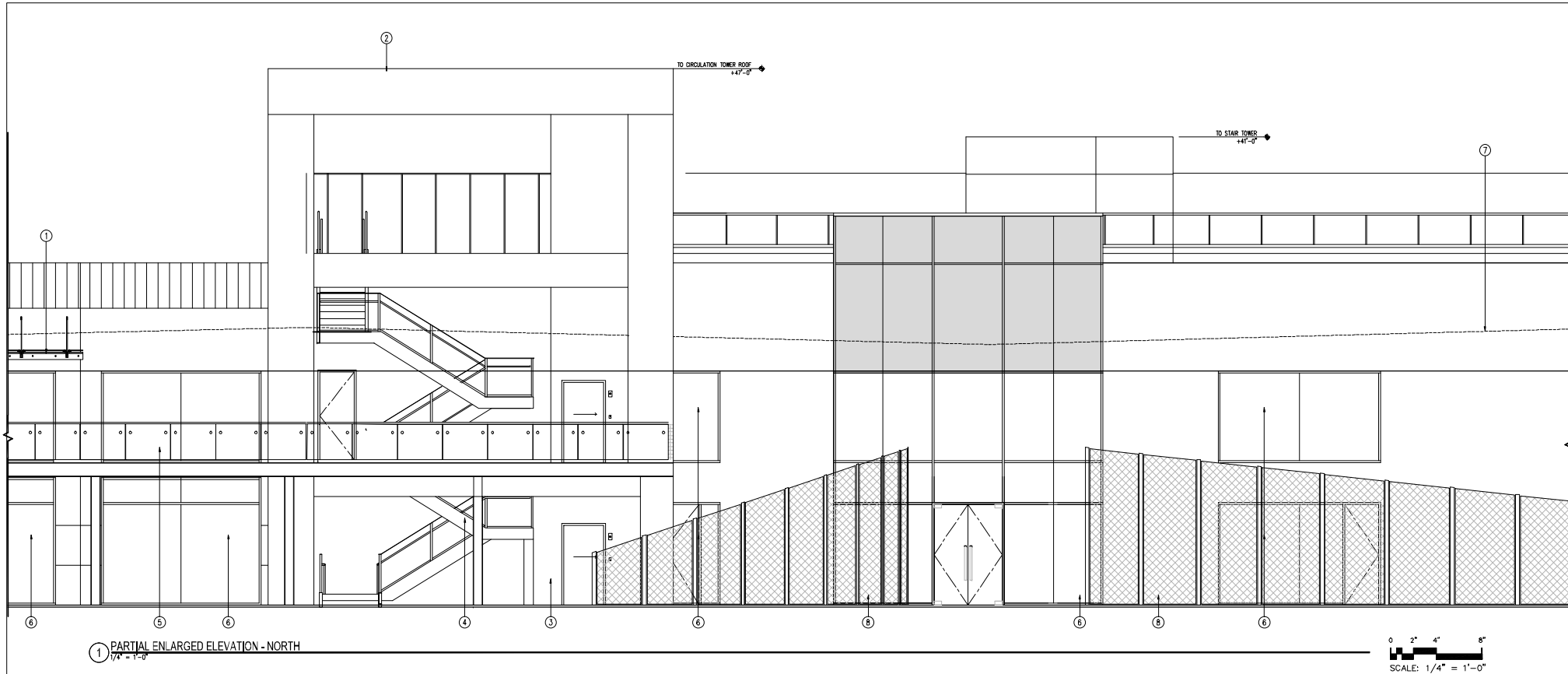
PROPOSED BUILDING SECTIONS

01.20.2016	Planning Pre-Application Submittal
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07.11.2016	Planning Application Resubmittal

22

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NOTES:

THE FEATURED DESIGN ELEMENTS THAT DISTINGUISH THIS BUILDING OF THE CAMPUS AS AN AMENITIES AREA AND DESTINATION FOR BUSINESS PARK CAMPUS TENANTS ARE THE VERTICAL "MASSES" OR TOWERS WITH CIRCULAR OPENINGS. THESE ELEMENTS PROVIDE A VISUAL CUE TO LOCATE VERTICAL CIRCULATION CORES, ENCOMPASSING AN ELEVATOR TOWER AND STAIRS. THE MAIN ARCHITECTURAL ELEMENTAL SO FOLDS OVER TO CREATE A HORIZONTAL ELEMENT WHICH OFFERS PARTIAL PROTECTION FROM THE ELEMENTS TO THOSE USING THE STAIR WHILE STILL ALLOWING VISUAL CONNECTIVITY TO THE PARKING AREA AND STREETSCAPE.

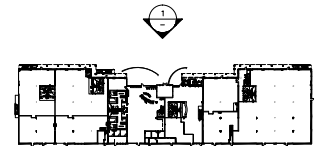
THE CANOPIES AT EACH OF THE NEW (6) 2ND FLOOR ENTRIES PROVIDE PARTIAL PROTECTION FROM THE ELEMENTS AND ALSO OFFER A VISUAL CUE FOR BUILDING TENANTS.

THE ARCHITECTURAL DESIGN ELEMENTS INCORPORATED INTO THE MODERNIZATION OF THIS TILT-UP BUILDING ARE INTENDED TO CREATE VISUAL INTEREST, MAKE THE EXISTING STREET-FACING STRUCTURE MORE CURRENT AND AESTHETICALLY RELEVANT, WHILE ALSO PROVIDING AN ARCHITECTURAL TIE TO THE OTHER CAMPUS STRUCTURES (SUCH AS 1315, 1140, 1135, AND 1165 O'BRIEN, AS WELL AS 1555 ADAMS) THAT HAVE SIMILAR VERTICAL DISTINGUISHING ARCHITECTURAL FEATURES.

KEY NOTES:

- ① 2ND FLOOR ENTRY CANOPY
- ② CIRCULATION TOWER
- ③ NEW ELEVATOR TOWER
- ④ NEW OPEN STAIR
- ⑤ NEW BRIDGE
- ⑥ NEW STOREFRONT / GLASS
- ⑦ ROOF LINE BEHIND PARAPET WALL
- ⑧ VEGETATION SCREEN

KEY PLAN



Menlo Business Park Bldg. 7 - Amenities & Renovation

1530 O'Brien Drive, Suite C
Menlo Park, CA 94025

1430 O'Brien Drive, Menlo Park, CA. 94025

DES Project Number: 2730,61

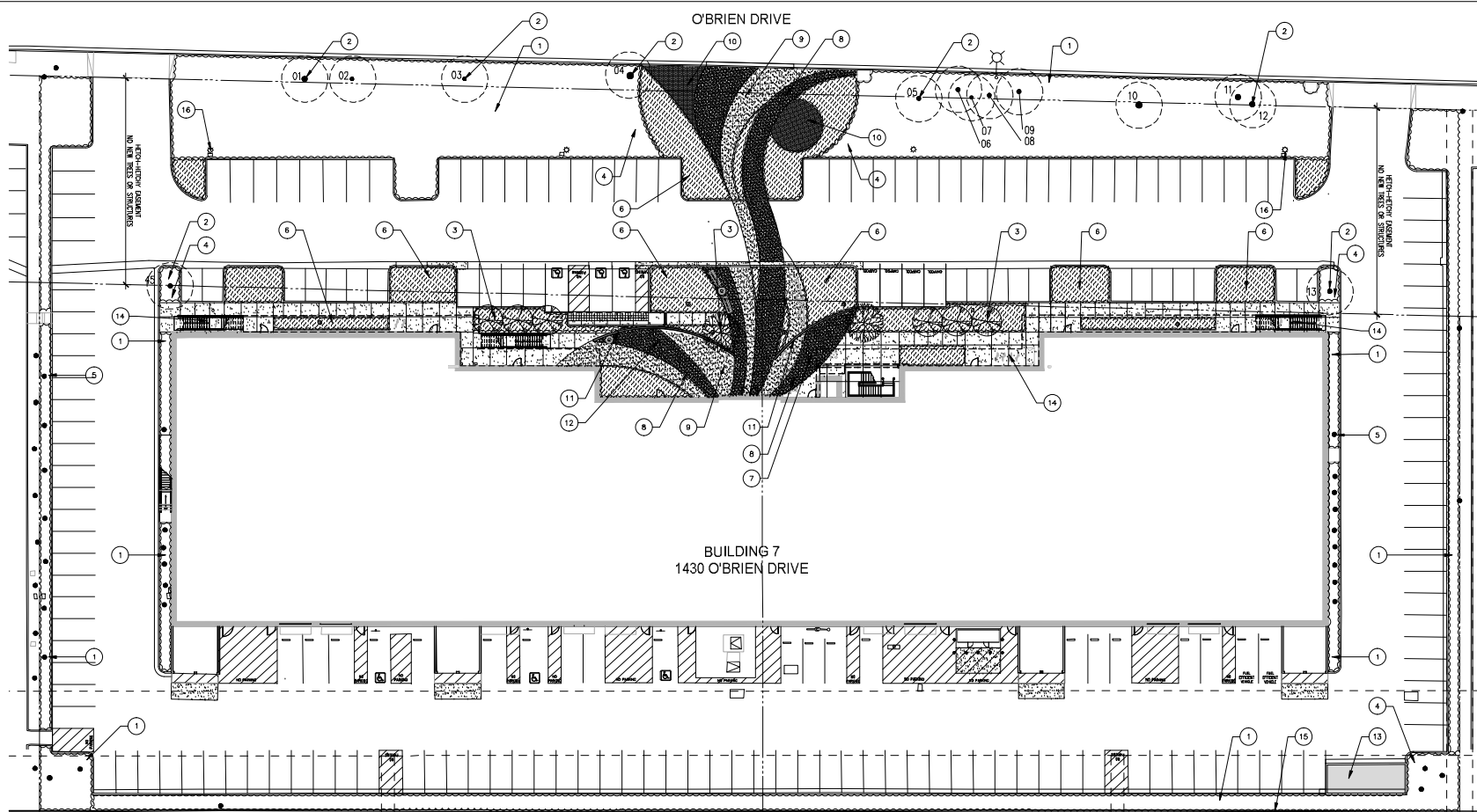
DESIGN ELEMENT DETAIL




01.20.2016 Planning Pre-Application Submittal
02.01.2016 Planning Application Submittal
04.26.2016 Planning Application Resubmittal
07.11.2016 Planning Application Resubmittal

23

DES

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Botanical Name	Common Name	Size	Native	WJCOLS*	Notes	
TREES						
	Ginkgo b. "Princeton Sentry"	Princeton Sentry Maidenhair	15-gal	N	Moderate	Replacement Tree For Heritage Removal
	Arbutus "Marino"	No Common Name	15-gal	N	Low	
	Existing Tree to Remain Refer to Existing Tree Plan and Arborist Report					

WUCOLS[®] = Water Use Classification of Landscape Species

Botanical Name	Common Name	Size	Native WJCOLS?
EXISTING PLANT MATERIAL			
SHRUBS			
Correa 'Dusky Bells'	Australian Fuchsia	5-gal	N Low
Heuchera spp.	Coral Bells	1-gal	Y Low
Loropetalum chinensis	Fringe Flower	5-gal	N Low
Myrtis communis 'Compacta'	Myrtle	5-gal	N Low
Pittosporum tenuifolium	NCN	15-gal	N Moderate
Polystichum munitum	Western Sword Fern	1-gal	Y Low
Rosa flower carpet	Flower Carpet Rose	2-gal	N Moderate
Woodsia ilmifolia	Giant Chain Fern	5-gal	Y Moderate

WUCOLS® = Water Use Classification of Landscape Species

Botanical Name	Common Name	Size	Native	WUCOLS*
GRASSES				
 Calamagrostis x a. 'Stricta'	Feather Reed Grass	1-gal	Y	Low
Carex tumulicola	Berkeley Sedge	4" pot	Y	Low
Manisuris rigens	Deer Grass	4" pot	Y	Low
Pennisetum s. 'Easton Canyon'	Dark Purple Fountain Grass	5-gal	N	Moderate
GROUNDCOVERS				
Cistus salvifolius	Sage-Leaved Rockrose	1-gal	N	Low
Coprosma kirkii	Coprosma	1-gal	N	Low
*WUCOLS = Water Use Classification of Landscape Species				

WUCOLS® = Water Use Classification of Landscape Species

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1430 O'Brien Drive, Menlo Park, CA, 94025

DES Project Number: 2730.61

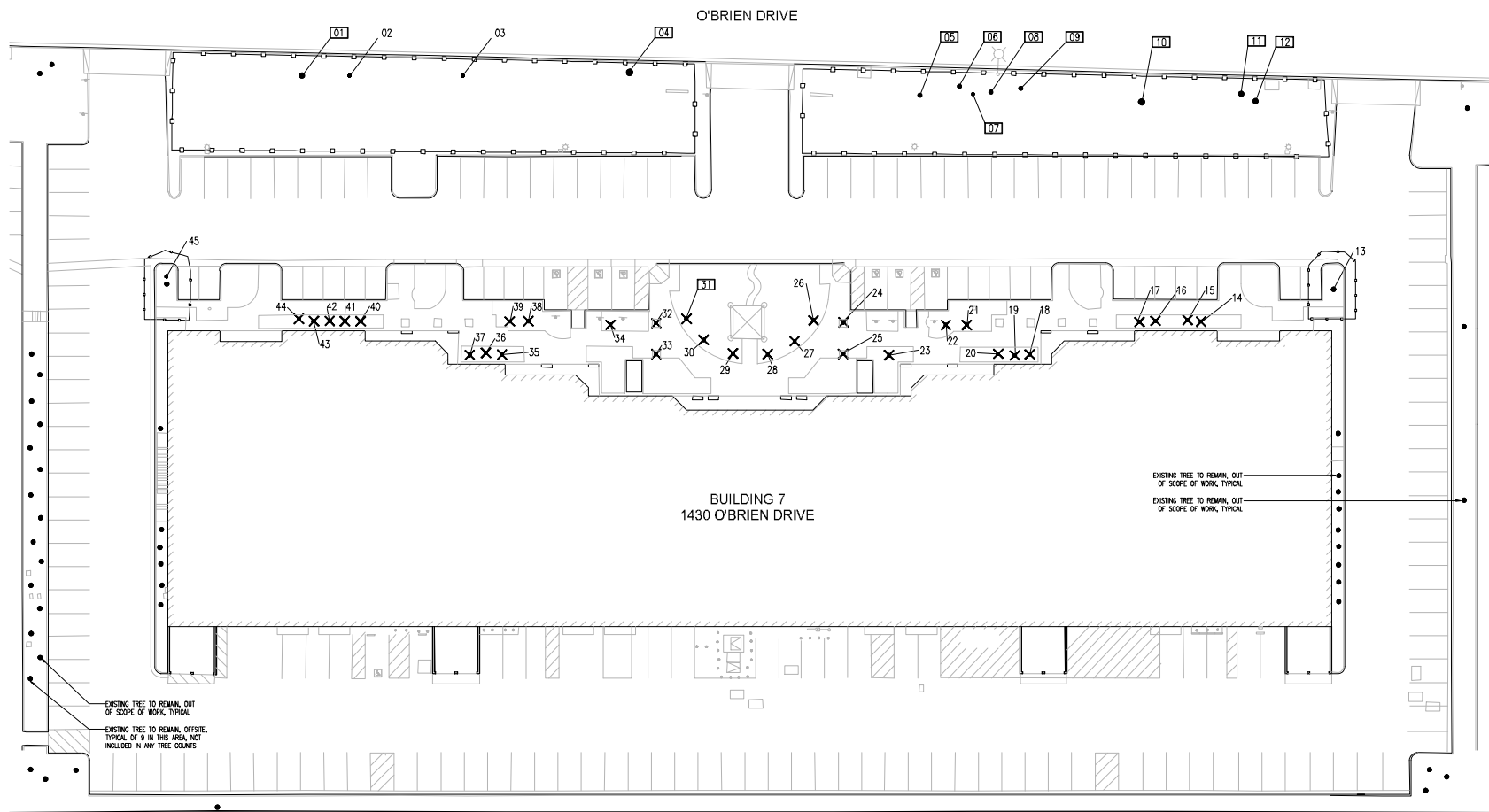
Proposed Landscape Plan

01.20.2016	Planning Pre-Application Submittal
02.01.2016	Planning Application Submittal
04.26.2016	Planning Application Resubmittal
07.11.2016	Planning Application Resubmittal

L1

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TREE EVALUATION NOTES

1. REFER TO ARBORIST REPORT AS PREPARED BY ARBOR RESOURCES 04/28/14
2. HERITAGE TREE REMOVAL PERMITTING, TOTAL OF 1 ON-SITE TREE: #31
3. REFER TO TREE INVENTORY TABLE SHEET L3 FOR EXISTING TREE LIST, TREE PROTECTION MEASURES AND TREE PROTECTION FENCING

TREE REMOVAL SUMMARY

TOTAL TREES TO BE REMOVED = 31
 HERITAGE TREES = 1
 NON-HERITAGE TREES = 30

REPLACEMENT TREES REQUIRED = 2
 TOTAL PROPOSED TREES = 7
 TOTAL EXISTING TREES TO REMAIN = 57

TOTAL TREES (PROPOSED & EXISTING TO REMAIN) = 64

LEGEND

EXISTING TREE NUMBER PER ARBORIST REPORT:
 HERITAGE TREE
 NON-HERITAGE TREE

EXISTING TREE TO BE REMOVED

TREE PROTECTION FENCING



0 10' 20' 40'
 SCALE: 1"=20'



Menlo Business Park Bldg. 7 - Amenities & Renovation

1530 O'Brien Drive, Suite C
 Menlo Park, CA 94025

1430 O'Brien Drive, Menlo Park, CA. 94025

DES Project Number: 2730,61

Existing Tree Plan

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 02.01.2016 Planning Application Submittal
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 07.11.2016 Planning Application Resubmittal

L2

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EXISTING TREE TABLE

TREE/ TAG NO.	TREE NAME	TREE SIZE			TREE CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Tree Height (ft.)	Canopy Spread (ft.)	Health Condition (100%-Best, 0%-Worst)	Structural Integrity (100%-Best, 0%-Worst)	Overall Condition (Good/Fair/Poor/Dead)		
1	Canary Island pine (<i>Pinus canariensis</i>)	26	45	30	70%	60%	Fair	Moderate	X
2	flowering plum (<i>Prunus cerasifera</i>)	14	25	25	50%	30%	Poor	Low	
3	flowering plum (<i>Prunus cerasifera</i>)	11	15	20	40%	30%	Poor	Low	
4	Alcornoque pine (<i>Pinus halepensis</i>)	30	35	35	70%	40%	Fair	Moderate	X
5	flowering pear (<i>Prunus calleryana</i>)	20	40	30	60%	30%	Poor	Low	X
6	Canary Island pine (<i>Pinus canariensis</i>)	18	65	20	70%	60%	Fair	Moderate	X
7	Canary Island pine (<i>Pinus canariensis</i>)	15	60	20	50%	30%	Poor	Low	X
8	Canary Island pine (<i>Pinus canariensis</i>)	19	55	20	50%	50%	Fair	Moderate	X
9	Canary Island pine (<i>Pinus canariensis</i>)	17	50	20	60%	50%	Fair	Moderate	X
10	Alcornoque pine (<i>Pinus halepensis</i>)	32	30	35	50%	50%	Fair	Moderate	X
11	Canary Island pine (<i>Pinus canariensis</i>)	21	60	20	90%	70%	Good	Good	X
12	Canary Island pine (<i>Pinus canariensis</i>)	24	60	25	90%	70%	Good	Good	X
13	Australian willow (<i>Geijera parviflora</i>)	5	8	10	40%	60%	Poor	Low	
14	fern pine (<i>Araucaria arborescens</i>)	6	20	10	50%	40%	Poor	Low	
15	fern pine (<i>Araucaria arborescens</i>)	7	35	15	50%	40%	Poor	Low	
16	fern pine (<i>Araucaria arborescens</i>)	7	30	15	50%	50%	Fair	Low	
17	fern pine (<i>Araucaria arborescens</i>)	8	25	15	50%	40%	Poor	Low	
18	fern pine (<i>Araucaria arborescens</i>)	6	35	15	50%	30%	Poor	Low	
19	fern pine (<i>Araucaria arborescens</i>)	8	35	15	50%	30%	Poor	Low	
20	fern pine (<i>Araucaria arborescens</i>)	7	30	15	50%	30%	Poor	Low	
21	flowering plum (<i>Prunus cerasifera</i>)	6	15	10	50%	30%	Poor	Low	

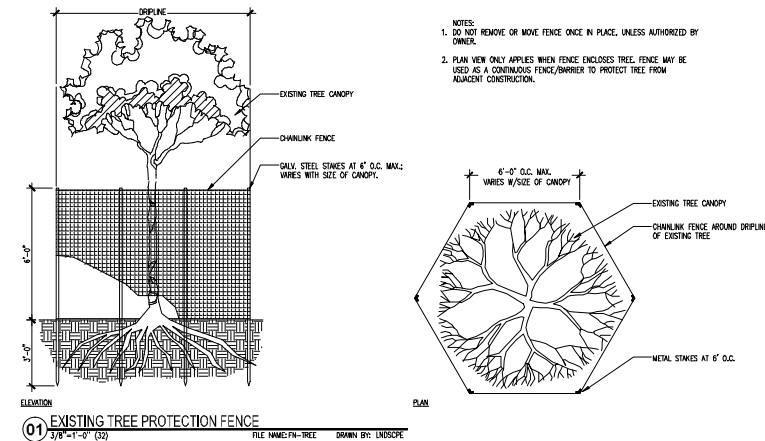
22	flowering plum (<i>Prunus cerasifera</i>)	11	20	20	20%	40%	Poor	Low	
23	flowering plum (<i>Prunus cerasifera</i>)	10	20	20	40%	30%	Poor	Low	
24	Purple leaf Eastern redbud (<i>Cercis c. Forest Pansy</i>)	2	7	10	70%	50%	Fair	Moderate	
25	Purple leaf Eastern redbud (<i>Cercis c. Forest Pansy</i>)	3	15	15	50%	50%	Fair	Low	
26	flowering pear (<i>Prunus calleryana</i>)	17	45	35	50%	20%	Poor	Low	
27	flowering pear (<i>Prunus calleryana</i>)	14	45	30	70%	40%	Fair	Moderate	
28	flowering pear (<i>Prunus calleryana</i>)	13	45	25	70%	40%	Fair	Moderate	
29	flowering pear (<i>Prunus calleryana</i>)	9	35	20	60%	30%	Poor	Low	
30	flowering pear (<i>Prunus calleryana</i>)	13	45	35	40%	50%	Poor	Low	
31	flowering pear (<i>Prunus calleryana</i>)	19	40	45	70%	40%	Fair	Moderate	X
32	Purple leaf Eastern redbud (<i>Cercis c. Forest Pansy</i>)	3	8	10	60%	30%	Poor	Low	
33	Purple leaf Eastern redbud (<i>Cercis c. Forest Pansy</i>)	3	15	10	20%	30%	Poor	Low	
34	flowering plum (<i>Prunus cerasifera</i>)	10	20	15	20%	30%	Poor	Low	
35	fern pine (<i>Araucaria arborescens</i>)	6	30	15	50%	30%	Poor	Low	
36	fern pine (<i>Araucaria arborescens</i>)	7	30	15	50%	40%	Poor	Low	
37	fern pine (<i>Araucaria arborescens</i>)	5	25	10	50%	30%	Poor	Low	
38	flowering plum (<i>Prunus cerasifera</i>)	7	20	15	30%	40%	Poor	Low	
39	Purple leaf Eastern redbud (<i>Cercis c. Forest Pansy</i>)	2	10	10	20%	30%	Poor	Low	
40	fern pine (<i>Araucaria arborescens</i>)	7	35	15	70%	40%	Fair	Low	
41	fern pine (<i>Araucaria arborescens</i>)	8	25	15	40%	40%	Poor	Low	
42	fern pine (<i>Araucaria arborescens</i>)	2, 2	15	5	70%	30%	Fair	Low	
43	fern pine (<i>Araucaria arborescens</i>)	7	30	15	70%	40%	Fair	Low	
44	fern pine (<i>Araucaria arborescens</i>)	7	30	20	60%	40%	Fair	Low	
45	Australian willow (<i>Geijera parviflora</i>)	14	27	30	20%	30%	Poor	Low	

GENERAL NOTES

1. REFER TO ARBORIST REPORT DATED 4/28/14 FOR ADDITIONAL INFORMATION
2. REFER TO EXISTING TREE PLAN SHEET 2 FOR ADDITIONAL INFORMATION

TREE PROTECTION NOTES

1. PRIOR TO BEGINNING CONSTRUCTION ON SITE, CONTRACTOR SHALL IDENTIFY, CONFIRM WITH OWNER AND PROTECT EXISTING TREES AND PLANTS DESIGNATED AS TO REMAIN.
2. ALL TREE PROTECTION MEASURES TO BE COORDINATED WITH CIVIL DEMOLITION PLAN.
3. PROVIDE 8 FOOT TALL TREE PROTECTION FENCE WITH DISTINCTIVE MARKING VISIBLE TO CONSTRUCTION EQUIPMENT, ENCODING DRIP LINES OF TREES DESIGNATED TO REMAIN.
4. WORK REQUIRED WITHIN FENCE LINE SHALL BE HELD TO A MINIMUM AND PERFORMED BY HAND. AVOID UNNECESSARY MOVEMENT OF HEAVY EQUIPMENT WITHIN FENCED AREA AND DO NOT PARK ANY VEHICLES UNDER DRIP LINE OF TREES. DO NOT STORE EQUIPMENT OR MATERIALS WITHIN FENCE LINE.
5. CONSULT WITH THE OWNER'S PROJECT ARBORIST PRIOR TO REMOVING ROOTS AND BRANCHES LARGER THAN 2" IN DIAMETER FROM TREES OR PLANTS THAT ARE TO REMAIN.
6. ANY GRADE CHANGES GREATER THAN 6" WITHIN THE DRIFLINE OF EXISTING TREES SHALL NOT BE MADE WITHOUT FIRST CONSULTING THE LANDSCAPE ARCHITECT.
7. NO UTILITY TRENCHING WITHIN 15' OF EXISTING TREES.
8. PROTECT EXISTING TREES TO REMAIN FROM SPILLED CHEMICALS, FUEL OIL, MOTOR OIL, GASOLINE AND ALL OTHER CHEMICALLY INJURIOUS MATERIALS AS WELL AS FROM FLOODING OR CONTINUOUSLY RUNNING WATER. SHOULD A SPILL OCCUR, STOP WORK IN THAT AREA AND CONTACT THE CITY'S ENGINEER / INSPECTOR IMMEDIATELY. CONTRACTOR SHALL BE RESPONSIBLE TO MITIGATE DAMAGE FROM SPILLED MATERIAL AS WELL AS MATERIAL CLEAN UP.
9. PROVIDE TEMPORARY IRRIGATION TO ALL TREES AND PLANTS THAT ARE IN OR ADJACENT TO CONSTRUCTION AREAS WHERE EXISTING IRRIGATION SYSTEMS MAY BE AFFECTED BY CONSTRUCTION. ALSO PROVIDE TEMPORARY IRRIGATION TO RELOCATED TREES.
10. CONTRACTOR SHALL BE RESPONSIBLE FOR ONGOING MAINTENANCE OF ALL TREES DESIGNATED TO REMAIN AND FOR MAINTENANCE OF RELOCATED TREES STOPPED DURING CONSTRUCTION. CONTRACTOR WILL BE REQUIRED TO REPLACE TREES THAT DIE DUE TO LACK OF MAINTENANCE.
11. CONSULT WITH LANDSCAPE ARCHITECT SHOULD SPECIAL CIRCUMSTANCES OR QUESTIONS ARISE REGARDING THESE PROCEDURES.



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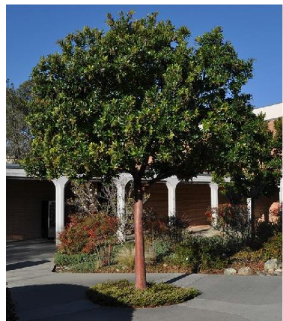
Existing Tree Table

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L3

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ARBUTUS 'MARINA' TREE



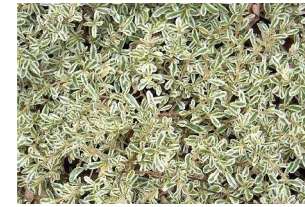
PRINCETON SENTRY MAIDEN HAIR TREE



AUSTRALIAN FUSCHIA



CORAL BELLS



COPROSMA



DEER GRASS



FEATHER REED GRASS



FLOWER CARPET ROSE



FOUNTAIN GRASS (PURPLE)



FRINGE FLOWER



GIANT CHAIN FERN



MOOR GRASS



MYRTLE



PITTOSPORUM



WESTERN SWORD FERN



HEAVY SANDBLAST CONCRETE PAVING



LIGHT SANDBLAST CONCRETE PAVING



DECOMPOSED GRANITE PAVING



TRASH & RECYCLING RECEPTACLE



BIKE RACK



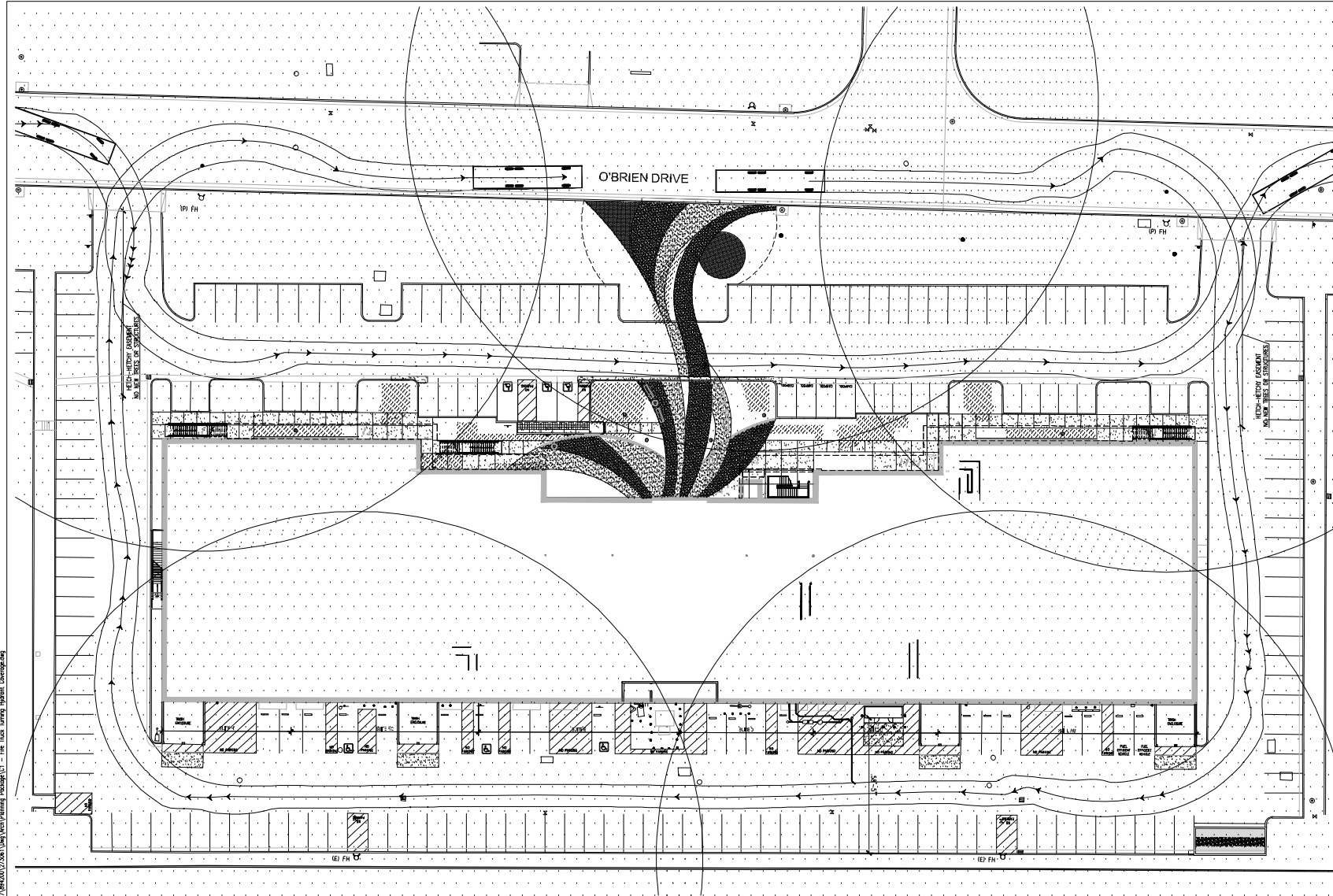
BIKE LOCKER



BENCH



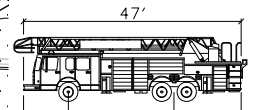
OUTDOOR DINING (TABLES & CHAIRS)



GENERAL NOTES

1. FIRE TRUCK COMPUTER SIMULATED PATH OF TRAVEL CREATED USING AUTOCAD VERSION 7.0 SOFTWARE AND THE FIRE TRUCK PROFILE INFORMATION INDICATED.

FIRE TRUCK PROFILE

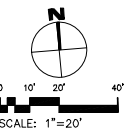


6.8' 22.50' 17.7'
MENLO PARK FIRE TRUCK
 WIDTH: 9'-6"
 TRACK: 8'-3"
 LOCK TO LOCK TIME: 6.0 SECONDS
 STEERING ANGLE: 25.50 DEGREE
 INNER TURNING RADIUS: 44'

LEGEND

FIRE TRUCK COMPUTER SIMULATED PATH OF TRAVEL

150' RADIUS
 FIRE HYDRANT
 COVERAGE AREA



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DES Project Number: 2730,61

Fire Truck Turning and Fire Hydrant Coverage

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07.11.2016	Planning Application Resubmittal

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1



2



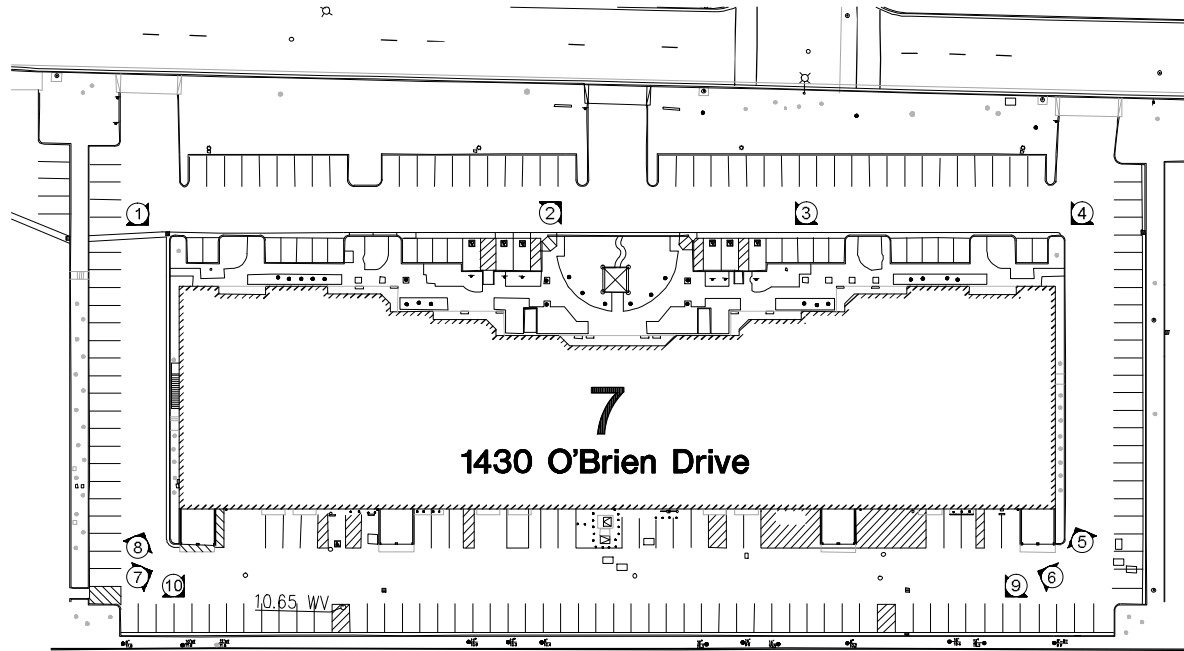
3



4



5



6



7



8



9



10



Menlo Business Park Bldg. 7 - Amenities & Renovation

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Menlo Park, CA 94025

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DES Project Number: 2730,61

Site Photos

01.20.2016 Planning Pre-Application Submittal
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04.26.2016 Planning Application Resubmittal
07.11.2016 Planning Application Resubmittal

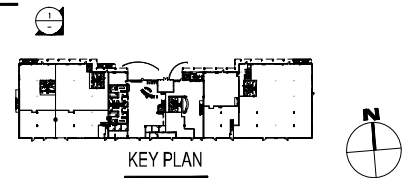
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① VIEW OF NEW ENTRANCE AND WALKWAY
NIS



1530 O'Brien Drive, Suite C
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DES Project Number: 2730,61

Building Perspective

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1430 O'BRIEN DRIVE ■*April 25, 2016***Project Description**

Tarlton Properties is renovating an existing R&D building to create a combined Amenities and R&D facility. The amenities will include a fitness and health center, as well as a café. The fitness center will be available for use by the twenty other Tarlton buildings on the campus. The café will be open to the public. The Amenities center will be flanked by R&D facilities that are safely separated uses. In addition, the updated building will be equipped with an elevator and bridge access to all of the second floor suites. One of the three drive aisles to the site will be repurposed to create easy walking and biking access to the building, the shuttle bus stop, and the amenities.

Existing Site and Building

The project is located at 1430 O'Brien Drive and the site area is 3.53 acres (153,767sf) . It has always been identified as Building 7 of the Menlo Business Park. The site is adjacent to a residential zoning to the south. The existing building was originally designed in 1986 by DES and is approximately 64,600 sq. ft., including a partial second floor. It occupies the central portion of the site with parking areas on the north and south sides. Three driveway entrances are located along O'Brien Drive. There are paved patios and walkways at the building entries facing O'Brien Drive and this street frontage is screened by mature trees and landscaping. More recently this building has been used as a multitenant building for a variety of research and development, life science companies.

The site is zoned as M-2 General Industrial that allows a maximum 55% FAR and currently requires parking at 1 car/300 sq. ft. The existing FAR is 42%.

Proposed Project

Tarlton Properties intends to make a portion of this building an amenities center to serve its 12 buildings in Menlo Business Park and its other 8 buildings along O'Brien drive and Willow Road, which are located just outside of the Park. The building is centrally located to all of these existing buildings and will serve as a

MENLO BUSINESS PARK – AMENITIES ■

DES Project No. 2730.61
Planning Application
April 25, 2016
Page 2 of 4

focal point of the **modernized** Menlo Business Park. The goals and scopes of the project are as follows,

1. An adaptive reuse: The existing building will be re-designed to become a state-of-art fitness and wellness facility along with a cafe that will serve as the 'living room' for the Menlo Park and O'Brien Drive Life Science staff.
2. Separate the R&D suites on both ends of the building from the Amenities Center in the middle of the building.
3. The building will receive some exterior enhancements and upgrades, including an elevator, an outdoor seating area, and a second story access bridge that will improve access and egress from the R&D suites.
4. The building will be expanded on the 2nd level to increase the building FAR from 42% to 55% (gross building areas are: 84,562). This will include an upgrade of the entire second floor to enhance life safety.
5. The new site area created by closing off one of the three existing driveways will be designed as garden and outdoor space for the tenants of this building and the surrounding buildings. This will increase the amount of active outdoor space on site and provide access to a park shuttle stop, as well as providing bike and pedestrian access to the building.
6. There will be carpool parking, bicycle parking, new entry plazas, landscaping, and ADA upgrades to create an attractive and functional project. Electric vehicle (EV) charging spaces have also recently been installed.

1430 O'Brien – R&D

The existing R&D building, will have a major face-lift and also substantial changes on the inside. All new exterior glazing to meet the current Title 24/CalGreen requirements will be added to the north, east and west sides of the building. A second story exterior walkway that runs the entire length of the building will provide elevator access to all of the suites. The existing wood deck second story floor will be replaced with a concrete pan deck and the entire building will be seismically upgraded. Three sets of open stairways will connect the exterior walkway to the first floor areas.

MENLO BUSINESS PARK – AMENITIES ■

DES Project No. 2730.61
Planning Application
April 25, 2016
Page 3 of 4

It is anticipated that the R&D tenants will be similar to the Tarlton Life Science portfolio and have an approximate square footage of 450 per person. This is consistent with other life science companies where the range of square foot per person is typically 400-500 SF and the average density across the Tarlton's entire life science portfolio of 500 SF per person. This calculation takes into consideration both the laboratory work space and an office workstation for each lab technician. Laboratory workers usually have two stations, one in the lab where they will typically wear lab coats and safety goggles and another in an office environment where they can work at a computer and meet with other collaborators.

1430 O'Brien Amenities

A new two story entry with architectural elements that define the Amenities portions of the building will face O'Brien Drive. The building entry will be enhanced by ADA-compliant ramps and paved walkways leading to the parking area, central garden area, and shuttle stop. The restrooms will be upgraded and facilities added to the second floor areas. A portion of the existing roof above the lobby will be used for an outdoor pool. New 2-hour walls will be constructed on the east and west sides of the amenities areas to separate them from the R&D suites.

Site

To meet current city parking and Calgreen guidelines the project will include re-striping existing parking stalls on the existing paved areas, as well as the addition of new ADA and EV charging stalls. Outdoor seating areas will be added in front of the building and at the entry to the site. Other "green" strategies on the site include careful re-planting of drought tolerant and water-wise plantings and trees, adding pedestrian and bicycle access along street frontage, and creating an inviting new entry plaza and transit hub adjacent to O'Brien Drive.

Sustainable Design

Sustainable design is another key aspect of the project. The existing single-pane glazing will be replaced by low-e double-glazing and new storefronts. Carefully-

MENLO BUSINESS PARK – AMENITIES ■

DES Project No. 2730.61
Planning Application
April 25, 2016
Page 4 of 4

planned window openings, such as flushing out the first floor glazing and adding skylights will allow more daylight into the building and views to the outside.

Transportation Demand Management

Renowned transportation engineers, Kimley Horn, have analyzed the trip generation for the project utilizing ITE standards for the proposed uses for the redevelopment of the 1430 O'Brien Drive project. In a proactive effort to reduce any traffic impact associated with the proposed change in use, Kimley-Horn has developed a comprehensive Transportation Demand Management Program (TDM)* for the project. This TDM encompasses state of the art initiatives to encourage alternative modes of transportation and reduce trips to and from the site. In addition to the operational efforts of matching car pools and van pools through a commute assistance center, a number of services will be built into the facility. The fitness center will include shower and locker facilities, which also serves the business park employees arriving by bicycle. Lockers for bicycles and share bikes will be provided onsite. Tarlton properties will provide a 'Guaranteed Ride Home' program and a campus shuttle to and from key transit stops, such as Caltrain and BART. Preferential parking will be provided for car poolers.

*Please see Kimley-Horn Memorandum dated April 26, 2016 for more details on the proposed Transportation Demand Management program (TDM).



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MEMORANDUMCITY OF MENLO PARK
PLANNING

To: Ron Krietemeyer
Tarlton Properties, Inc.

From: Michael Mowery, P.E.
Ben Huie, P.E.

Date: February 1, 2016

Subject: **Transportation Memorandum for 1430 O'Brien Drive**

Kimley-Horn and Associates, Inc. (KHA) was retained by Tarlton Properties, Inc. to evaluate the expected number of project trips based on the existing and proposed land uses at 1430 O'Brien Drive in the City of Menlo Park and mitigate the number of trips by implementing a Transportation Demand Management (TDM) Plan. The proposed project will realign the previous building uses, as well as add additional square footage to the building. The current project proposal totals 84,006 square feet and consists of:

- 66,465 square feet of research and development
- 9,445 square feet of health and fitness club
- 8,096 square feet of café

The previous use for the project site (64,951 square feet) consisted entirely of research and development. These changes in land use for 1430 O'Brien Drive will result in a change in peak hour trips generated from the project site.

PROJECT PEAK HOUR TRIPS

The number of project trips for the project site was estimated using the industry standard Institute of Transportation Engineer's (ITE) *Trip Generation Manual*¹. This reference estimates project trips based on land use from survey data. Since the proposed project is not a new project, but updating an existing land use, trip rates were calculated for both the proposed use and the previous use.

The previous tenants were Life Science, which consisted entirely of research and development land uses. The ITE *Trip Generation* manual was used to determine the number of trips for the previous use. **Table 1** summarizes the trip generation for the previous use. Specific land use and trip generation details are provided in **Attachment A**.

¹ *Trip Generation Manual, 9th Edition*, Institute of Transportation Engineers, 2012.

Table 1 – Trip Generation Summary – Previous Use

Previous Use	Vehicle Trips	
	AM Peak	PM Peak
64.951 KSF R&D	79	69

The previous land uses resulted in 79 AM peak hour trips and 69 PM peak hour trips. No adjustments for trip reductions (e.g. pass-by trips or internal capture) were used in this calculation. The previous use trips will be used as a trip credit for determining the overall net change in project trips.

The current proposal totals 84.006 KSF. The estimated trips were calculated to determine the net new trips generated. This proposal includes a research and development use, a health and fitness center, and a café. The health and fitness center is planned to be exclusive to Menlo Business Park. The trips from the health and fitness center will be internally captured within Menlo Business Park and therefore, will not generate any additional vehicle trips outside of Menlo Business Park. The café is planned to be open for lunch only. The hours of operation for the café will therefore not generate any peak hour trips during the AM or PM peak.

Table 2 summarizes the trip generation for each project proposal. Specific land use and trip generation details are provided in **Attachment A**.

Table 2 – Trip Generation Summary – Proposed Use

Option	Proposed Use	Vehicle Trips	
		AM Peak	PM Peak
Current Project Proposal (84.006 KSF)	66.465 KSF R&D 9.445 KSF Health and Fitness Center 8.096 KSF Café (Lunch Only)	81	71

The proposed land uses result in 81 AM peak hour trips and 71 PM peak hour trips. A TDM program is being proposed to reduce the proposed project vehicle trips.

TRANSPORTATION DEMAND MANAGEMENT PROGRAM

The following summarizes an initial approach to the proposed TDM program for the proposed project at 1430 O'Brien Drive. It is assumed that the TDM program will be refined over time to adapt to changing transportation trends and to maximize the efficiency of the program. The TDM program is specifically designed to focus on incentives and rewards for employees to participate in the program rather than penalties for not participating.

POTENTIAL PROGRAM ELEMENTS

Tarlton Properties, Inc. should offer a combination of program elements to encourage employees to utilize alternative modes of transportation to driving alone. Potential program elements are listed below:

- Bike lockers/racks
- Showers/changing rooms
- Shuttle service
- Subsidized transit tickets for employees
- Preferential carpool parking spaces
- Commute assistance center
- Allowance program for bicyclists, walkers, and carpoolers
- Parking cash out program
- Telecommuting
- Compressed workweek program
- Alternate hours workweek program
- Join the Alliance's guaranteed ride home program

These program elements are listed in the City of Menlo Park's *Transportation Demand Management Program Guidelines*². Additionally, the City/County Association of Governments of San Mateo County (C/CAG) has its own guidelines for a TDM program mentioned in the *Revised C/CAG Guideline for the Implementation of the Land Use Component of the Congestion Management Program*³. Each of these documents summarizes the potential program measures, a description of each measure, and the trip credits associated with each measure.

PROPOSED PROGRAM ELEMENTS

Tarlton Properties, Inc. is interested in working with the City to develop a practical TDM plan that can be both effective and provide the most value for all parties. An initial set of TDM measures are proposed for the 1430 O'Brien Drive site and is summarized in **Table 3**. The number of trip credits was determined from the City of Menlo Park's TDM Guidelines. The following provides a brief description of each proposed TDM element:

- **Bike Storage:** Bike lockers are proposed to be located on the property. The specific location will be shown on the proposed site plan. Two secure bike storages are proposed along with 12 bicycle racks. The bike lockers are furnished by the American Bicycle Security Company and provide a safe storage for bikes at work. The locations of each are shown on the proposed site plan.

² *Transportation Demand Management Program Guidelines*, City of Menlo Park, July 2015.

³ *Revised C/CAG Guideline for the Implementation of the Land Use Component of the Congestion Management Program*, City/County Association of Governments of San Mateo County, September 2004.

Table 3 – Proposed TDM Measure Summary

TDM Measure	Number of Trips Credited	Peak Hour Trip Credits	Program Elements	Trip Credits ¹
Bike Storage	One credit per 3 bike lockers/racks	1/3	14	4
Showers/Changing Rooms	Two credits per 1 shower/changing room	2	8	16
Shuttle service	One trip credit for each round trip seat on the shuttle	1	0	0
<i>Additional credit for combination with Guaranteed Ride Home Program</i>	<i>Additional one trip credit for each seat</i>	<i>1</i>	<i>20</i>	<i>20</i>
Subsidized transit tickets (Go Pass for Caltrain)	One trip credit for each transit pass provided	1	100	100
Preferential carpool parking	Two credits per 1 space reserved	2	4	8
Commute assistance center				
<i>Transit brochure rack</i>	<i>One peak hour trip credited for each feature</i>	<i>1</i>	<i>1</i>	<i>1</i>
<i>Computer kiosk connected to Internet</i>	<i>One peak hour trip credited for each feature</i>	<i>1</i>	<i>1</i>	<i>1</i>
<i>Telephone</i>	<i>One peak hour trip credited for each feature</i>	<i>1</i>	<i>1</i>	<i>1</i>
<i>Desk and chairs</i>	<i>One peak hour trip credited for each feature</i>	<i>1</i>	<i>1</i>	<i>1</i>
Allowance for bicyclists, walkers, and carpoolers	One trip credit for each monthly allowance offered to an employee	1	30	30
Join Alliance's guaranteed ride home program	One credit for every two slots purchased in the program with Alliance ²	-	-	-
Implement flexible work hours	One peak hour credit for each employee offered the opportunity to work flexible hours	1	35	35
Combine any two of these elements and receive additional five credits	Five trip credits for combination of two elements	5	1	5
Bike Share Program	No trip credits ³	0	1	0
Total Trip Credits:				222

¹The number of peak hour trips credited is outlined in the City of Menlo Park's *Transportation Demand Management (TDM) Guidelines*.

²The Alliance's guaranteed ride home program operates differently than when the TDM guidelines were created. The Alliance no longer offers slots to be purchased. Trip credits for this TDM measure are combined with the shuttle service.

³In the City's latest TDM guidelines, there is no mention of any trip credits for a bike share program. Therefore, no trip credits will be taken.

- **Showers/Changing Rooms:** Eight shower/changing rooms are proposed for the building on the first floor. The shower/changing rooms provide a dedicated facility for the cyclists and persons walking to work. This measure, combined with the bike lockers/racks, should provide employees with a great alternative for commuting to work.
- **Guaranteed Ride Home Program:** Tarlton Properties, Inc. will also enroll its tenants in a Guaranteed Ride Home Program administered by the Peninsula Traffic Congestion Relief Alliance. The program provides employees a free taxi ride home in the case of an emergency. Employers will pay 25 percent of the taxi costs and the Peninsula Traffic Congestion Relief Alliance will pay the remaining 75 percent. There is no additional cost to join the program. This program provides a safety net when an emergency arises for those carpooling, taking transit, walking to work, or bicycling to work.
- **Shuttle Service:** A shuttle service will be provided for employees to use for commuting to work. The shuttle service is provided by Bauers and is currently being implemented in the existing business park surrounding the proposed project. The shuttle service has a stop in front of 1505 O'Brien Drive, but this is proposed to be relocated to 1430 O'Brien Drive. This service provides access to BART and Caltrain and provides a total of 60 seats during each of the AM and PM peak hours. This project does not propose any additional seats.
- **Subsidized Transit Tickets:** Caltrain Go Passes will be provided to employees at no cost to the employees. The Caltrain Go Pass allows for unlimited rides, seven days a week. The cost of the Go Pass is \$180 per person, but a minimum of \$15,120 per employer. This equates to 84 Go Passes at a minimum to distribute to all employees. For TDM calculations, it was assumed that 100 Go Passes will be provided for this specific site.
- **Preferential Carpool Parking:** 4 preferential carpool parking spaces are provided. The carpool parking spaces will be located close to the building's entrances to provide an incentive for employees to carpool. Marked carpool parking spaces will be shown on the proposed site plan.
- **Commute Assistance Center:** A Commute Assistance Center will be provided with the following features: transit brochure rack, computer kiosk connected to internet, telephone, and a desk and chairs. The center should encourage employees to use transit to commute to work and provide ease of access to determine the optimal mode of transportation home.
- **Monthly Allowance for Bicyclists, Walkers, and Carpoolers:** A monthly allowance of \$20 will be offered to those employees who walk, bicycle, or carpool to work. This measure provides further incentive to not drive alone to work. The \$20 monthly allowance equates to approximately \$1 per day.
- **Flexible work hours:** Employees will be offered the opportunity to work a flexible work schedule. Employees can work outside the traditional 8 AM to 5 PM work day. This measure will result in employees avoiding the AM peak (7 AM to 9 AM) and PM peak (4 PM and 6 PM) for their daily commute. It is anticipated that 35 employees would participate in this flexible work schedule.
- **Combination of Two Elements:** Combining at least two elements in the TDM program results in five additional peak hour trip credits. By offering complimentary TDM elements, experience has shown that the effectiveness of the program increases.

- **Bike Share Program:** The Bike Share Program, which does not give any trip credits since it is not mentioned in the City's TDM guidelines, will entail an automated bicycle rental program. Specific details on this program have yet to be determined, but generally employees would sign up for the program, use a card to allow access to a bicycle at a secure parking station, and then return the bicycle to a similar parking station. This number of bicycle for this program has yet to be determined. The bicycle parking stations will be located near the entrance to the building, as shown on the site plan.

As shown in **Table 3**, the proposed TDM measures total to 222 trip credits. Although the TDM program results in 222 trip credits, the effectiveness of the TDM program was calculated separately.

EFFECTIVENESS OF TDM PROGRAM ELEMENTS

The effectiveness of the TDM plan was evaluated using the COMMUTER model developed by the United States Environmental Protection Agency (EPA). The COMMUTER model is a spreadsheet based model that evaluates the travel and emission effects resulting from an employer implemented transportation management program. The model allows for inputs to local work-trip mode shares, work trip lengths, vehicle occupancy, financial incentives for alternative modes of transportation, employer participation rates, and the level of each program to determine the predicted trip reduction rates. After inputting the specific TDM measures mentioned in **Table 3** for the proposed project, the anticipated trip reduction percentage is 21.8 percent. The 21.8 percent effectiveness is similar to other TDM plans in the local area. The COMMUTER model output for this project is shown in **Attachment B**.

The anticipated trip reduction of 21.8 percent was applied to the proposed project trips only, not the trip credits. **Table 4** shows the trip generation summary including the previous use trip credits and the TDM trip reduction for the proposed project.

Table 4 – Trip Generation Summary with Trip Credits for Proposed Project

Uses	Vehicle Trips	
	AM Peak	PM Peak
Proposed Use Trips	81	71
TDM Trip Reduction (21.8%)	-18	-15
Previous Use Trip Credits	-79	-69
Net New Trips	-16	-13

The net new trips for the proposed project after taking trip credits for the previous use and the TDM program are -16 AM peak hour trips and -13 PM peak hour trips. The -16 AM peak hour trips and -13 PM peak hour trips are below the City's threshold of 16 peak hour trips (the equivalent number of peak hour trips for a 10 KSF office building).

TRANSPORATION IMPACTS WITHIN MENLO BUSINESS PARK

Although the proposed project for 1430 O'Brien Drive is not anticipated to add any additional vehicle trips to the City of Menlo Park's street network during the AM and PM peak hours, the project may

add internal trips to the street network within Menlo Business Park. A traffic analysis was performed along O'Brien Drive to determine if it is anticipated to exceed the City's daily traffic volume criteria.

The City of Menlo Park states in its Transportation Impact Analysis Guidelines⁴ that for a collector street (a capacity of 10,000 vehicles per day), a traffic impact may be considered potentially significant if the existing Daily Traffic Volume (ADT) is less than 5,000 vehicles and the project traffic increases the ADT by 25 percent. O'Brien Drive is listed as a collector street in the City's General Plan⁵. The existing volumes for O'Brien Drive were taken from the Menlo Park Housing Element Update⁶, which were taken in 2012. The volumes from the study were AM and PM peak hour volumes at the intersections of O'Brien Drive/Willow Road and O'Brien Drive/University Avenue. The higher volume was at the intersection of O'Brien Drive/Willow Road, with 470 vehicles per hour for the east leg in the AM peak hour and 470 vehicles per hour in the PM peak hour. The peak hour volume was converted to an ADT volume by assuming that 10 percent of the daily volume would occur in the AM or PM peak hour. This is a conservative estimate since this is a business park and a higher percentage of the daily traffic should occur in the weekday AM and PM peak hours. This resulted in an ADT volume of 4,700 vehicles along O'Brien Drive.

Since the ADT volume is less than 5,000 vehicles, the project would have a significant impact if it increases the ADT by 25 percent or 1,175 daily trips. The daily trip generation for the proposed project, not including an internal capture trip reduction, would be 324 new daily trips, excluding the cafe. There is no trip generation information specifically for a café within an office open for lunch only. However, it is unlikely that this would increase the daily trips to more than 1,175 trips. Therefore, it would not be a significant impact.

PARKING

The proposed parking for the 1430 O'Brien Drive site was reviewed to determine if the site would be providing enough parking for its use. The City specifies that for M-2 district uses, one parking space per 300 square feet of gross floor area is required. This project site falls within the M-2 district uses and therefore the parking requirement applies. The project is proposing 193 parking spaces, which is below the 282 parking space requirement (84,572 square feet divided by 300 square feet per parking space = 282 parking spaces) by 89 parking spaces.

Parking requirements were reviewed at other similar business sites consisting primarily of research and development uses in the Bay Area, such as Genentech and Gilead. This revealed that the Genentech campus uses a parking ratio of 1.40 parking spaces per 1,000 square feet of laboratory and the Gilead campus uses a parking ratio of 1.20 parking spaces per 1,000 square feet of laboratory. Therefore, if the 1.40 parking spaces per 1,000 square feet is used since it is the more conservative of the two parking ratios, then the 1430 O'Brien Drive site would need 119 parking

⁴ Transportation Impact Analysis Guidelines, City of Menlo Park.

⁵ *City of Menlo Park General Plan*, City of Menlo Park, 1994.

⁶ *City of Menlo Park Housing Element Update, General Plan Consistency Update, and Zoning Ordinance Amendments Environmental Assessment*, City of Menlo Park, April 2014.

spaces. The proposed 193 parking spaces exceeds the 119 parking space demand by 74 parking spaces.

Although the proposed project's 193 parking spaces does not meet the 282 parking space requirement based on the City's parking requirements for an M-2 district use, it does exceed the expected parking demand of 119 parking spaces based on the parking demand of similar uses in the Bay Area.

It should also be noted that the parking for the proposed uses at 1430 O'Brien Drive would involve shared parking. The land uses health/fitness club, research and development, and café are complimentary uses. This means that the land uses peak in their parking demand during different times throughout the day, which allows the parking to be shared amongst the land uses. The health/fitness club parking demand peaks from 6:00 PM to 8:00 PM on a typical weekday. The research and development parking demand using the office land use peaks from 9:00 AM to 1:00 PM on a typical weekday. The café parking demand using the high-turnover (sit-down) restaurant land use peaks from 11:00 AM to 2:00 PM on a typical weekday. Based on the parking demand peaks, the research and development use and the café use share the peak between 11:00 AM and 1:00 PM. However, it should be noted that the café use is located in the same building as the research and development use, and many of the customers are not expected to drive to the café. Many of the patrons will walk from within Menlo Business Park and therefore not need a parking space. The complimentary nature of the proposed land uses would lower the parking demand for the project.

CONCLUSION

The proposed project is anticipated to generate -16 AM peak hour trips and -13 PM peak hour trips, including a 21.8 percent TDM reduction. The -16 AM peak hour trips are below the City's threshold of 16 peak hour trips and therefore the project would **not** necessitate a traffic study. It should be noted that without the TDM reduction, the project would still be **below** the City's threshold of 16 peak hour trips and would not need a traffic study.

In addition to the proposed project generating less trips than the City's trip threshold, the internal circulation within Menlo Business Park was also reviewed. It was determined that the project would not create a significant impact along O'Brien Drive for the daily trips, as established by the City's TIA guidelines.

A review of the proposed project's parking supply revealed that the 193 parking spaces is below the City's parking requirements of 282 parking spaces, or one parking space per 300 square feet of gross floor area for an M-2 district use. However, based on the parking demand of the similar research and development uses in Menlo Business Park, the project's proposed 193 parking spaces are below the 119 parking spaces or 1.40 parking spaces per 1,000 square feet of occupied building expected for a research and development use in Menlo Business Park.

Attachment A

1430 O'Brien Drive - 84,006 SF

TIME PERIOD		LAND USE	Trip Rate			Trips		
			In	Out	Total	In	Out	Total
AM Peak	Existing	Research and Development Center (64.951 KSF)	1.01	0.21	1.22	66	13	79
		Total Existing Use AM Trips				66	13	79
	Proposed	Research and Development Center (66.465 KSF)	1.01	0.21	1.22	67	14	81
		Health and Fitness Club (9.445 KSF)	0.71	0.71	1.41	7	6	13
		Café (Lunch Only) (8.096 KSF)	0.00	0.00	0.00	0	0	0
		Subtotal Before Internal Capture	-	-	-	74	20	94
		Internal Capture Reduction (100%)	-	-	-	(7)	(6)	(13)
		Total Proposed Use AM Trips				67	14	81
		Net New AM Peak Trips				1	1	2
PM Peak	Existing	Research and Development Center (64.951 KSF)	0.16	0.91	1.07	10	59	69
		Total Existing Use PM Trips				10	59	69
	Proposed	Research and Development Center (66.465 KSF)	0.16	0.91	1.07	11	60	71
		Health and Fitness Club (9.445 KSF)	2.01	1.52	3.53	19	14	33
		Café (Lunch Only) (8.096 KSF)	0.00	0.00	0.00	0	0	0
		Subtotal Before Internal Capture	-	-	-	30	74	104
		Internal Capture Reduction (100%)	-	-	-	(19)	(14)	(33)
		Total Proposed Use PM Trips				11	60	71
		Net New PM Peak Trips				1	1	2

COMMUTER MODEL RESULTS

SCENARIO INFORMATION

Description	C/CAG Base TDM Program
Scenario Filename	Tariton1430.vme
Emission Factor File	
Performing Agency	Kimley-Horn and Associates, Inc
Analyst	Ben Huie
Metropolitan Area	Menlo Park, CA
Area Size	1 - Large (over 2 million)
Analysis Scope	2 - Site or Employer-Based
Analysis Area/Site	1430 O'Brien Drive
Total Employment	250

PROGRAMS EVALUATED

<input checked="" type="checkbox"/>	Site Walk Access Improvements
<input type="checkbox"/>	Transit Service Improvements
<input checked="" type="checkbox"/>	Financial Incentives
<input checked="" type="checkbox"/>	Employer Support Programs
<input checked="" type="checkbox"/>	Alternative Work Schedules

☐ User-Supplied Final Mode Shares

MODE SHARE IMPACTS

Mode	Baseline	Final	%Change
Drive Alone	70.5%	55.2%	-15.3%
Carpool	6.5%	9.0%	+2.5%
Vanpool	0.0%	0.0%	+0.0%
Transit	4.3%	17.4%	+13.1%
Bicycle	7.3%	8.6%	+1.3%
Pedestrian	2.7%	2.8%	+0.1%
Other	8.7%	7.0%	-1.7%
No Trip	-	0.0%	+0.0%
Total	100.0%	100.0%	-

Shifted from Peak to Off-Peak	1.5%
-------------------------------	------

TRAVEL IMPACTS (relative to affected employment)

Quantity	Peak	Off-Peak	Total
Baseline VMT	3,113	1,957	5,070
Final VMT	2,543	1,702	4,245
VMT Reduction	570	255	825
% VMT Reduction	18.3%	13.0%	16.3%
Baseline Trips	225	142	367
Final Trips	176	120	296
Trip Reduction	49	22	71
% Trip Reduction	21.8%	15.5%	19.4%



TREE SURVEY REPORT

1430 O'BRIEN DRIVE
MENLO PARK, CALIFORNIA

RECEIVED

JAN 20 2016

**CITY OF MENLO PARK
PLANNING**

Submitted to:

Mr. Ron Krietemeyer
Tarlton Properties, Inc.
Menlo Park, CA 94025

Prepared by:

David L. Babby
Registered Consulting Arborist® #399
Board-Certified Master Arborist® #WE-4001B

April 28, 2014

TABLE OF CONTENTS

<u>SECTION</u>	<u>TITLE</u>	<u>PAGE</u>
1.0	INTRODUCTION	1
2.0	TREE COUNT AND COMPOSITION	2
3.0	SUITABILITY FOR TREE PRESERVATION	3
4.0	DESIGN GUIDELINES	4
5.0	ASSUMPTIONS AND LIMITING CONDITIONS	6

EXHIBITS

<u>EXHIBIT</u>	<u>TITLE</u>
A	TREE INVENTORY TABLE (seven sheets)
B	AERIAL MAP (one sheet)
C	PHOTOGRAPHS (four sheets)

1.0 INTRODUCTION

Mr. Ron Krietemeyer of Tarlton Properties, Inc. has retained me to prepare this *Tree Survey Report* as part of the proposed application to improve the frontage of **1430 O'Brien Drive**, Menlo Park. Specific tasks assigned are as follows:

- Visit the site, performed on 4/25/14, to identify 45 trees located within the project area.
- Determine each tree's trunk diameter in accordance with Section 13.24.020 of the City Code; all diameters are rounded to the nearest inch, and trees having more than one diameter are formed by multiple trunks originating from grade.
- Estimate each tree's height and average canopy spread (most all are rounded to the nearest fifth).
- Ascertain each tree's health and structural integrity, and assign an overall condition rating (e.g. good, fair, poor or dead).
- Determine each tree's suitability for preservation (e.g. good, moderate or low).
- Identify which trees are defined as "heritage trees."¹
- Comment on pertinent health, structure or site conditions.
- Sequentially assign tree numbers, and plot them on the aerial photo (*Google Earth*) in Exhibit B. For trees aligning the street, numbers are roughly placed on top of the canopies, and for trees along the building, arrows generally denote the trunk locations.
- Affix round, silver metal tags with engraved, corresponding numbers to the trees' trunks or major limbs (not to be confused with other round tags found on several trees). Tags for trees #24 and 25 were nailed to the top of an adjoining wood stake.
- Obtain photographs of the trees; see Exhibit C.
- Provide general design guidelines to help mitigate or avoid impacts to retained trees.
- Prepare a written report that presents the aforementioned information, and submit via email as a PDF document.

¹ Section 13.24.020 of the City Code defines a "heritage tree" as follows: [1] any tree having a trunk diameter ≥ 15 " at 54" above natural grade; [2] any oak tree native to California, and has a trunk diameter ≥ 10 " at 54" inches above natural grade; [3] any tree $\geq 12'$ tall with a trunk diameter of ≥ 15 " measured at the point where the trunks divide; and [4] any tree or group of trees specifically designated by the City Council for protection because of historical significance, special character or community benefit.

2.0 TREE COUNT AND COMPOSITION

Forty-five (45) trees of seven various species were inventoried for this report. They are numbered as #1 thru 45, and the table below identifies their names, assigned numbers, counts and overall percentages.

NAME	TREE NUMBER(S)	COUNT	% OF TOTAL
Aleppo pine	4, 10	2	4%
Australian willow	13, 45	2	4%
Canary Island pine	1, 6-9, 11, 12	7	16%
fern pine	14-20, 35-37, 40-44	15	33%
flowering pear	5, 26-31	7	16%
flowering plum	2, 3, 21-23, 34, 38	7	16%
Purple leaf redbud	24, 25, 32, 33, 39	5	11%
Total		45	100%

Specific information regarding each tree is presented within the table in **Exhibit A**. The trees' numbers and approximate locations can be viewed on the aerial photo in **Exhibit B**, and photographs are presented in **Exhibit C**.

As illustrated in the above table, the project area is populated predominantly by fern pine, followed Canary Island pine, flowering pears and flowering plums. All of the inventoried trees are considered ornamental and not native to the area.

Trees #1 thru 12 are situated along the **street frontage**, whereas **#13 thru 45** are along the **building frontage**.

Eleven (11) of the inventoried trees are defined by City Code as **heritage trees** and include **#1, 4-12 and 31**. **Trees #1, 4 and 6-12** are pines, and **#5 and 31** are flowering pears.

3.0 SUITABILITY FOR TREE PRESERVATION

Each tree has been assigned either a “good,” “moderate” or “low” suitability for preservation rating as a means to cumulatively measure their health, structural integrity, anticipated life span, location, size and species. A description of these ratings are presented below, and note that the “good” category comprises **two trees** (or 5%), the “moderate” category **ten** (or 22%), and the “low” category **33** (or 73%).

Good: Applies to **trees #11 and 12.**

These two Canary Island pines are situated immediately adjacent to another; and form a contiguous canopy; appear healthy and structural stable; have no apparent, significant health issues or structural defects; present a good potential for contributing long-term to the site; and require regular care (e.g. pruning and watering) and monitoring to maintain their longevity and structural integrity.

Moderate: Applies to **trees #1, 4, 6, 8-10, 24, 27, 28 and 31.**

These trees contribute to the site but at levels less than those assigned a good suitability, have health and/or structural issues that can be reasonably addressed and properly mitigated, and frequent care is typically required for their remaining lifespan.

Low: Applies to **trees #2, 3, 5, 7, 13-23, 25, 26, 29, 30 and 32-45.**

These trees have serious or significantly weakened health and/or structural defects that are expected to worsen regardless of tree care measures employed (i.e. beyond likely recovery), and in some instances, present an unreasonable threat to persons and property below.

4.0 DESIGN GUIDELINES

Recommendations presented within this section serve as general design guidelines to help mitigate or avoid impacts to trees being retained. They are subject to revision upon reviewing the project plans, and I should be consulted in the event any cannot be feasibly implemented. Please note that all referenced **distances from trunks** are intended to be from the closest edge (face of) of their outermost perimeter at soil grade.

1. The **Tree Protection Zone (TPZ)** is where the following should be avoided: all trenching, soil scraping, compaction, mass grading (cuts and fill), finish-grading, overexcavation, subexcavation, swales, bioswales, storm drains, equipment cleaning, stockpiling and dumping of materials, and equipment/vehicle operation. For this project, the **TPZ** of a particular tree should be a minimum distance from its trunk of **five times the diameter** (for multi-trunk trees, only the largest needs consideration); for trees within the planters along the street frontage, I recommend a larger setback from the trunk of seven to ten times its diameter. Where an impact encroaches slightly within a setback, it can be reviewed by me on a case-by-case basis to determine appropriate mitigation measures.
2. All **existing, unused lines or pipes** within a TPZ shall be **abandoned** and cut off at existing soil grade (rather than being dug up and causing subsequent root damage); this provision should be specified on applicable plans (e.g. demolition plan).
3. The permanent and temporary **drainage design**, including downspouts, should not require water being discharged within TPZs. Additionally, the design shall not require trenching within a TPZ, and **new bioswales** should be established **well beyond** a TPZ.
4. For any **swales** needed for drainage within a TPZ, I should be consulted to review, and must require no more than a two- to three-inch soil cut, and must retain roots two inches and greater in diameter retained and not damaged.
5. **Underground utilities and services** should be routed **beyond TPZs**. Where this is not feasible, the section of line(s) within the TPZ should be directionally-bored by at

least four feet below existing grade, or installed by other means (e.g. pipe-bursting) to avoid an open trench; the ground above any tunnel must remain undisturbed, and access pits and any above-ground infrastructure (e.g. splice boxes, meters and vaults) must be established beyond all TPZs.

6. The future **staging area** and **route(s) of access** should be shown on the final site plan and avoided on unpaved areas beneath or near canopies.
7. To restrict spoils and runoff from traveling into root zones, the future **erosion control design** should establish any silt fence and/or straw rolls away from a tree trunk (not against it), and as close to the canopy edge as possible. Additionally, where within a TPZ, the material should require none or a maximum vertical soil cut of two inches for its embedment.
8. The proposed **landscape design** should conform to the following additional guidelines:
 - a. **Plant material** installed beneath trees should be planted at least 36 inches from their trunks.
 - b. **Irrigation and lighting features** (e.g. main line, lateral lines, valve boxes, wiring and controllers) should be established so that no trenching occurs within a TPZ. In the event this is not feasible, they may require being installed in a radial direction to a tree's trunk, and terminate a specific distance from a trunk (versus crossing past it).
 - c. **New fencing** (posts) should be placed at least two feet from a tree's trunk (depends on the trunk size and growth pattern).
 - d. **Ground cover** beneath canopies should be comprised of a three- to four-inch layer of coarse wood chips or other high-quality mulch (gorilla hair, bark or rock, stone, gravel, black plastic or other synthetic ground cover should be avoided). Mulch should not be placed no closer than six inches from a trunk.
 - e. **Tilling, ripping and compaction** within TPZs should be avoided.
 - f. Bender board or other **edging material** proposed beneath the canopies should be established on top of existing soil grade (such as by using vertical stakes).

5.0 ASSUMPTIONS AND LIMITING CONDITIONS

- All information presented herein reflects my observations and/or measurements obtained on April 25, 2014. Condition and suitability ratings of deciduous trees are subject to change once they can be observed following the regrowth of new leaves.
- My observations were performed visually without probing, coring, dissecting or excavating. I cannot, in any way, assume responsibility for any defects that could only have been discovered by performing the mentioned services in the specific area(s) where a defect was located.
- The assignment pertains solely to trees listed in Exhibit A. I hold no opinion towards other trees on or surrounding the project area.
- I cannot provide a guarantee or warranty, expressed or implied, that deficiencies or problems of any trees or property in question may not arise in the future.
- No assurance can be offered that if all my recommendations and precautionary measures (verbal or in writing) are accepted and followed, that the desired results may be achieved.
- I cannot guarantee or be responsible for the accuracy of information provided by others.
- I assume no responsibility for the means and methods used by any person or company implementing the recommendations provided in this report.
- The information provided herein represents my opinion. Accordingly, my fee is in no way contingent upon the reporting of a specified finding, conclusion or value.
- Tree numbers shown on the aerial photo in Exhibit B are intended to only roughly approximate a tree's location, and shall not be considered as surveyed.
- This report is proprietary to me and may not be copied or reproduced in whole or part without prior written consent. It has been prepared for the sole and exclusive use of the parties to who submitted for the purpose of contracting services provided by David L. Babby.
- If any part of this report or copy thereof be lost or altered, the entire evaluation shall be invalid.

Prepared By:



David L. Babby

Registered Consulting Arborist® #399

Board-Certified Master Arborist® #WE-4001B

Date: April 28, 2014



EXHIBIT A:

TREE INVENTORY TABLE

(seven sheets)



TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	TREE SIZE			TREE CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Tree Height (ft.)	Canopy Spread (ft.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

1	Canary Island pine (<i>Pinus canariensis</i>)	26	45	30	70%	60%	Fair	Moderate	X
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Comments: Sizeable girdling roots developing along the trunk's downhill side.

2	flowering plum (<i>Prunus cerasifera</i>)	14	25	25	50%	30%	Poor	Low	
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Comments: Nearly one-sided canopy away from tree #1. Trunk decay. Has substantial sprouts within canopy. Heavy limb weight and poor form.

3	flowering plum (<i>Prunus cerasifera</i>)	11	15	20	40%	30%	Poor	Low	
---	--	----	----	----	-----	-----	------	-----	--

Comments: Trunk decay. Canopy is thin and has a poor, asymmetrical form.

4	Aleppo pine (<i>Pinus halapensis</i>)	30	35	35	70%	40%	Fair	Moderate	X
---	--	----	----	----	-----	-----	------	----------	---

Comments: Sparse canopy. Tridominant leaders originate at seven feet high and form a weak attachment. Trunk sweeps towards NW. Has old tag #931.

5	flowering pear (<i>Prunus calleryana</i>)	20	40	30	60%	30%	Poor	Low	X
---	--	----	----	----	-----	-----	------	-----	---

Comments: Very weak structure containing multiple leaders 5 to 6 feet high. Excessive branch weight.

6	Canary Island pine (<i>Pinus canariensis</i>)	18	65	20	70%	60%	Fair	Moderate	X
---	--	----	----	----	-----	-----	------	----------	---

Comments: Crowded-growing conditions. Thin canopy.

7	Canary Island pine (<i>Pinus canariensis</i>)	15	60	20	50%	30%	Poor	Low	X
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Comments: Very crowded-growing conditions have resulted in poor trunk taper and development. Canopy is thin and sparse.



TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	TREE SIZE			TREE CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Tree Height (ft.)	Canopy Spread (ft.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

8	Canary Island pine (<i>Pinus canariensis</i>)	19	55	20	50%	50%	Fair	Moderate	X
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Comments: Multiple tops. Crowded-growing conditions.

9	Canary Island pine (<i>Pinus canariensis</i>)	17	50	20	60%	50%	Fair	Moderate	X
---	--	----	----	----	-----	-----	------	----------	---

Comments: Crowded-growing conditions. Has a thin canopy. Trunks sweeps in two directions.

10	Aleppo pine (<i>Pinus halapensis</i>)	32	30	35	50%	50%	Fair	Moderate	X
----	--	----	----	----	-----	-----	------	----------	---

Comments: Canopy is sparse. Formed by multiple leaders. Has a distinct lean towards east, possibly due to a girdling root. Previous large limb was cut away from lower trunk. Has old tag #927.

11	Canary Island pine (<i>Pinus canariensis</i>)	21	60	20	90%	70%	Good	Good	X
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Comments: Immediately adjacent to and has an adjoining canopy with #12.

12	Canary Island pine (<i>Pinus canariensis</i>)	24	60	25	90%	70%	Good	Good	X
----	--	----	----	----	-----	-----	------	------	---

Comments: Immediately adjacent to and has an adjoining canopy with #11.

13	Australian willow (<i>Geijera parviflora</i>)	5	8	10	40%	60%	Poor	Low	
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Comments: Canopy is very sparse. Has old tag #926.

14	fern pine (<i>Afrocarpus falcatus</i>)	6	20	10	50%	40%	Poor	Low	
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Comments: Adjacent to building and has a one-sided canopy. Adjacent walk is raised.



TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	TREE SIZE			TREE CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Tree Height (ft.)	Canopy Spread (ft.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

15	fern pine (<i>Afrocarpus falcatus</i>)	7	35	15	50%	40%	Poor	Low	
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Comments: Adjacent to building and has a one-sided canopy. Multiple tops. Past branch failure. Adjacent walk is raised.

16	fern pine (<i>Afrocarpus falcatus</i>)	7	30	15	50%	50%	Fair	Low	
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Comments: Adjacent to building and has a one-sided canopy. Adjacent walk is raised.

17	fern pine (<i>Afrocarpus falcatus</i>)	8	25	15	50%	40%	Poor	Low	
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Comments: Adjacent to building and has a one-sided canopy away from #16.

18	fern pine (<i>Afrocarpus falcatus</i>)	6	35	15	50%	30%	Poor	Low	
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Comments: Adjacent to building and has a one-sided canopy. Highly crowded-growing conditions at corner of building. Adjacent walk is cracked.

19	fern pine (<i>Afrocarpus falcatus</i>)	8	35	15	50%	30%	Poor	Low	
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Comments: Adjacent to building and has a one-sided canopy. Crowded-growing conditions. Adjacent walk raised in past.

20	fern pine (<i>Afrocarpus falcatus</i>)	7	30	15	50%	30%	Poor	Low	
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Comments: Adjacent to building and has a one-sided canopy. Crowded-growing conditions. Adjacent walk raised in past. Has a visible surface root growing towards building foundation.

21	flowering plum (<i>Prunus cerasifera</i>)	6	15	10	50%	30%	Poor	Low	
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Comments: Within a square planter. One-sided canopy away from #20.



TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	TREE SIZE			TREE CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Tree Height (ft.)	Canopy Spread (ft.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

22	flowering plum (<i>Prunus cerasifera</i>)	11	20	20	20%	40%	Poor	Low	
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Comments: Significant and extensive dieback.

23	flowering plum (<i>Prunus cerasifera</i>)	10	20	20	40%	30%	Poor	Low	
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Comments: Has a large girdling root. Dieback in canopy. Adjacent to building and has a one-sided canopy.

24	Purple leaf Eastern redbud (<i>Cercis c. 'Forest Pansy'</i>)	2	7	10	70%	50%	Fair	Moderate	
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Comments: Staked and is a recent install. Suppressed growth beneath #26's canopy.

25	Purple leaf Eastern redbud (<i>Cercis c. 'Forest Pansy'</i>)	3	15	15	50%	50%	Fair	Low	
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Comments: Staked and is a recent install. Canopy is sparse. Past branch failure. Crowded-growing conditions adjacent to #26 and 27.

26	flowering pear (<i>Prunus calleryana</i>)	17	45	35	50%	20%	Poor	Low	
----	--	----	----	----	-----	-----	------	-----	--

Comments: Adjacent walk is raised. Tridominant leaders originate at five feet high. Has a girdling root, and found fruiting bodies at base of trunk's SW side, an indication of internal decay.

27	flowering pear (<i>Prunus calleryana</i>)	14	45	30	70%	40%	Fair	Moderate	
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Comments: Adjacent walk is raised. Codominant leaders originate at seven feet high. Crowded-growing conditions.

28	flowering pear (<i>Prunus calleryana</i>)	13	45	25	70%	40%	Fair	Moderate	
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Comments: Adjacent walk is raised. Crowded-growing conditions.



TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	TREE SIZE			TREE CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Tree Height (ft.)	Canopy Spread (ft.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

29	flowering pear (<i>Prunus calleryana</i>)	9	35	20	60%	30%	Poor	Low	
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Comments: Highly crowded-growing conditions.

30	flowering pear (<i>Prunus calleryana</i>)	13	45	35	40%	50%	Poor	Low	
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Comments: Crowded-growing conditions with a very sparse canopy. Codominant tops that form a weak attachment. Prior limb was cut away from lower trunk.

31	flowering pear (<i>Prunus calleryana</i>)	19	40	45	70%	40%	Fair	Moderate	X
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Comments: Multiple leaders originating 6 to 9 feet high, and form weak attachments. Adjacent walk is raised and has multiple breaks. Excessive limb weight.

32	Purple leaf Eastern redbud (<i>Cercis c. 'Forest Pansy'</i>)	3	8	10	60%	30%	Poor	Low	
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Comments: Flat top and suppressed growth beneath #31's canopy. Has a low-growing canopy over walk.

33	Purple leaf Eastern redbud (<i>Cercis c. 'Forest Pansy'</i>)	3	15	10	20%	30%	Poor	Low	
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Comments: Crowded-growing conditions at edge of #31's canopy. Has a very sparse canopy.

34	flowering plum (<i>Prunus cerasifera</i>)	10	20	15	20%	30%	Poor	Low	
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Comments: Significant and extensive dieback. Has a girdling root.

35	fern pine (<i>Afrocarpus falcatus</i>)	6	30	15	50%	30%	Poor	Low	
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Comments: Adjacent to building and has a one-sided canopy. Adjacent walk is raised. Past limb failure.



TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	TREE SIZE			TREE CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Tree Height (ft.)	Canopy Spread (ft.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		
36	fern pine (<i>Afrocarpus falcatus</i>)	7	30	15	50%	40%	Poor	Low	
Comments: Adjacent to building and has a one-sided canopy.									
37	fern pine (<i>Afrocarpus falcatus</i>)	5	25	10	50%	30%	Poor	Low	
Comments: Adjacent to building and has a one-sided canopy. Adjacent walk is raised.									
38	flowering plum (<i>Prunus cerasifera</i>)	7	20	15	30%	40%	Poor	Low	
Comments: Extensive dieback. Leans away from #35 and has a pronounced buttress root.									
39	Purple leaf Eastern redbud (<i>Cercis c. 'Forest Pansy'</i>)	2	10	10	20%	30%	Poor	Low	
Comments: Extremely sparse canopy.									
40	fern pine (<i>Afrocarpus falcatus</i>)	7	35	15	70%	40%	Fair	Low	
Comments: Adjacent to building and has a one-sided canopy. Adjacent walk is raised.									
41	fern pine (<i>Afrocarpus falcatus</i>)	8	25	15	40%	40%	Poor	Low	
Comments: Adjacent to building and has a one-sided canopy. Has a very sparse and chlorotic canopy.									
42	fern pine (<i>Afrocarpus falcatus</i>)	2, 2	15	5	70%	30%	Fair	Low	
Comments: Two small trunks. Adjacent to building. Crowded-growing conditions.									



TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	TREE SIZE			TREE CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Tree Height (ft.)	Canopy Spread (ft.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

43	fern pine (<i>Afrocarpus falcatus</i>)	7	30	15	70%	40%	Fair	Low	
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Comments: Adjacent to building.

44	fern pine (<i>Afrocarpus falcatus</i>)	7	30	20	60%	40%	Fair	Low	
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Comments: Adjacent to building and has a one-sided canopy. Adjacent walk is substantially raised. Grows with a lean away from building.

45	Australian willow (<i>Geijera parviflora</i>)	14	27	30	20%	30%	Poor	Low	
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Comments: Extremely sparse canopy. Structure formed by multiple leaders. Has old tag #933.

EXHIBIT B:

AERIAL MAP

(one sheet)

1430 O'BRIEN DRIVE
Menlo Park, California

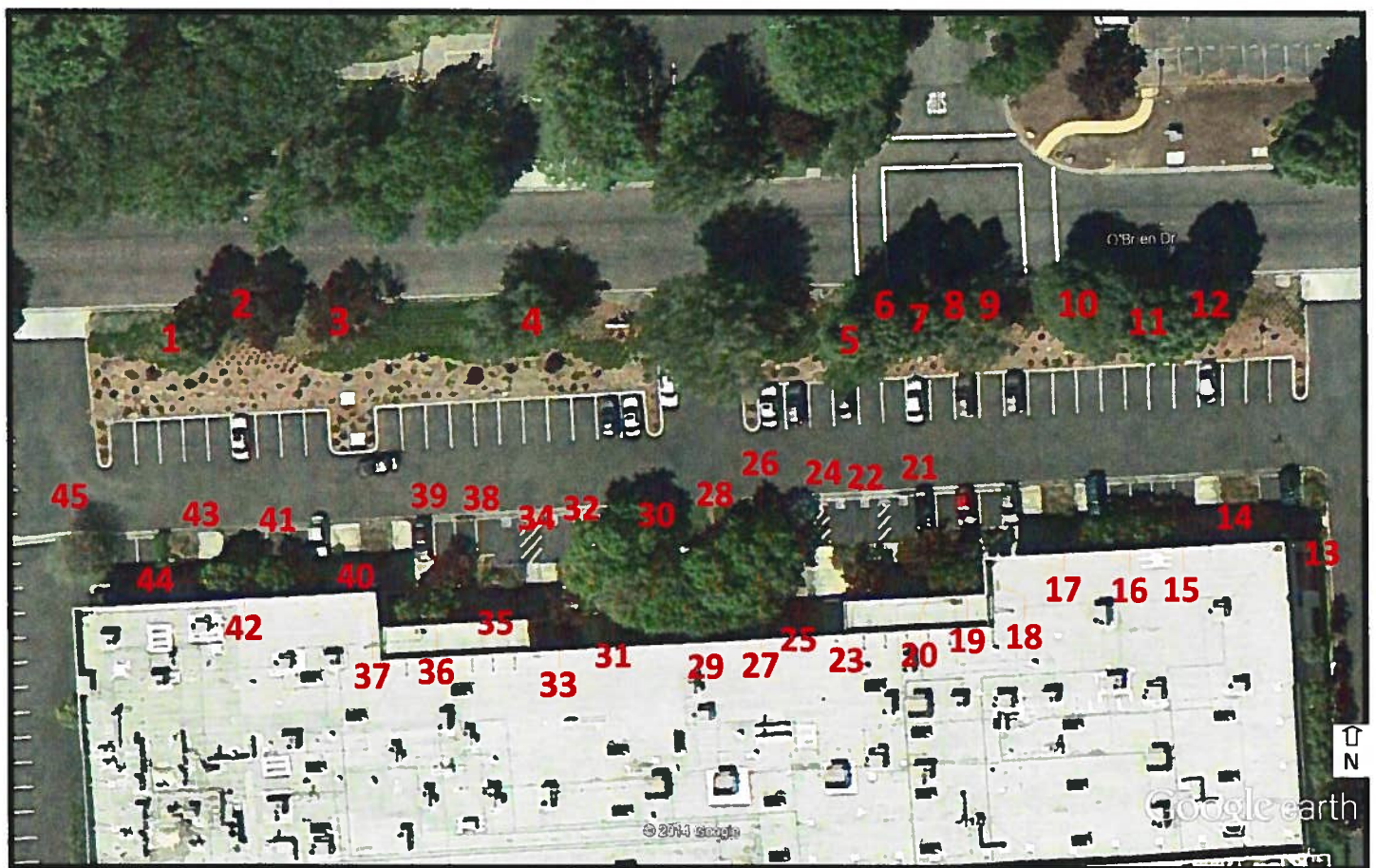


EXHIBIT C:

PHOTOGRAPHS

(four sheets)

Photo Index

Page C-1: #1 thru 10

Page C-3: #24 thru 33

Page C-2: #11 thru 23

Page C-4: #34 thru 45





1430 O'Brien Drive, Menlo Park
Tarlton Properties, Inc.



1430 O'Brien Drive, Menlo Park
Tarlton Properties, Inc.



1430 O'Brien Drive, Menlo Park
Tarlton Properties, Inc.

Page C-4



Hetch Hetchy Regional Water System

Services of the San Francisco Public Utilities Commission

Natural Resources and Lands Management Division

Date: **June 2 , 2016**

To: **Project Re ie Committee:**

Natural Resources and Lands Management Division (NRLMD): Dave Baker, Jason Bielski, Guido Ciardi, Rick Duffey, John Fournet, Jane Herman, Tim Koopmann, Krysten Laine, Diane Livia, Jeremy Lukins, Jonathan Mendoza, Joe Naras, Ellen Natesan, Emily Read, Casey Sondgeroth, Kathleen Swanson, Joanne Wilson and Tina Wuslich

Water Supply and Treatment Division (WSTD): Jonathan Chow, Colm Conefrey, Stacie Feng, Jim Heppert, Tracy Leung, Tony Mazzola, and Chris Nelson

Real Estate Services (RES): Rosanna Russell, Tony Bardo, Tony Durkee, Chester Huie, Brian Morelli, Dina Brasil, Christopher Wong, Janice Levy and Jamin Barnes

Water Quality Bureau (WQB): Jackie Cho

Bureau of Environmental Management (BEM): Sally Morgan, Barry Pearl, Matthew Weinand and YinLan Zhang

City Attorney's Office: Josh Milstein, Carolyn Stein and Richard Handel

Cc: **SFPUC:** Robin Breuer, David Briggs, Chris Nelson, Debbie Craven-Green, Kimberly Stern Liddell, Andrew DeGraca, Ed Forner, Karen Frye, Maria Garcia, Susan Hou, Annie Li, Greg Lyman, Alan Johanson, Scott MacPherson, Joe Ortiz, Barry Pearl, Tim Ramirez, Brian Sak, Carla Schultheis, Bles Simon, Irina Torrey, Rizal Villareal, Mia Ingolia, Scott Simono, and Surinderjeet Bajwa

San Francisco City Planning Environmental Planning : Chris Kern

From: **Jonathan S. Mendoza, Land and Resources Planner**
jsmendoza@sfwater.org | (415) 770-1997 or (650) 652-3215

Subject: **REVISED June 10, 2016 Project Re ie Meeting Summary**
10:00 a.m. – 1:00 p.m.
1657 Rollins Road, Burlingame, Medbery (Large) Conference Room

Participants: Joanne Wilson, Jane Herman, Jonathan Mendoza, Joe Naras, Neal Fujita, Tim Koopman (SFPUC-NRLMD); Christopher Wong (SFPUC-RES); Tracy Leung (SFPUC-WSTD Land Engineering); John Tarlton and Ron Krietemeyer (Tarlton); Ann Marie Taheny and Susan Eschweiler (DES Architects + Engineers); Nathan Tuttle (Prometheus Real Estate Group, Inc.); Joel Roos (Pacific Union Development Company); Prakash Pinto (Pinto + Partners); Charles Humpal, Joshua Holle and Ryan Stauffer (BKF Engineers); Antonia (Toni) Bava (Antonia Bava Landscape Architects)

Project Re ie Meeting Schedule for 2016

Meetings are usually held on the 2nd Friday and 4th/last Wednesday of each month and begin at 10:00 a.m. Meetings are generally located at 1657 Rollins Road, Burlingame (Medbery (Large) Conference Room).

June 29, 2016
July 08, 2016
July 27, 2016
August 12, 2016

August 31, 2016
September 09, 2016
September 28, 2016

October 14, 2016
October 26, 2016
November 04, 2016
December 02, 2016

REVISED June 10, 2016 Project Re ie Meeting Summary
San Francisco Public Utilities Commission – Water Enterprise
Natural Resources and Lands Management Division

NOTE TO APPLICANTS SEEKING A REVOCABLE LICENSE, LEASE, OR OTHER SERVICE FROM SFPUC REAL ESTATE SERVICES: *The SFPUC provides three essential 24/7 service utilities: water, wastewater and power to customers throughout the Bay Area. Our mission is to provide customers with the highest quality and effective service in a sustainable, professional and financially sound manner. Our service extends beyond the City and County of San Francisco and includes seven other counties.*

Due to staffing issues in the Real Estate Services Division (RES), RES has constrained resources and is focusing on projects critical to our core infrastructure mission at the present time. Therefore, we appreciate your patience in our response to your company's project application.

1	Case No.	Project	Applicant/Project Manager
	16.06-AL42.00	SFPUC Cattle Watering System Installation - Garcia Parcel	Tim Koopman (SFPUC-NRLMD)

The proposal is to install a watering system for cattle on SFPUC property, near the Garcia Parcel, which would include installing new pipelines, a solar powered water pump, a 5,000 gallon water tank, three troughs and an air vent. The Garcia Parcel is approximately 615 acres which extends from the Calaveras Valley at West Portal and continues over the ridge toward Andrade Road. The new water infrastructure could also be used as an emergency water supply.

The new pipeline would be approximately 2,700 feet long of 1.25-inch diameter polyvinyl chloride (PVC) pipe. The system would be installed approximately parallel to Andrade Road, Sunol. The water source would be an existing well located adjacent to an existing, uninhabited cottage from the 1930s. The cottage is located on a dirt road approximately 200 feet from Andrade Road. The project sponsor would enhance the well and install a solar powered pump to draw water from the well to a new 5000 gallon tank located at an upland site south of the well. The water would flow by gravity downhill (north) to the three trough sites.

Access to the site would be from existing roads. A trencher would be used to create a trench that is approximately up to 14-inches wide and a minimum of 18-inches deep. The PVC pipe would be installed with a trace wire so that the pipe can be found with a metal detector in the future. All troughs and tank pipe fittings above ground would be made of galvanized steel pipe, polyethylene pipe or painted/wrapped PVC pipe. An air vent/air release/vacuum relief valve would be installed at grade with a protective plastic or concrete box.

There are ground squirrel burrows located in the vicinity of the proposed pipe alignment so there are concerns that California Tiger Salamander (CTS) could be impacted by the construction of the trench. However, the project sponsor explained that an NRCD biologist with take permits would conduct a pre-construction biological survey for special status species and would supervise and approve the alignment and placement of the water system components.

Project work would occur between July and August 2016. Project coverage under the Endangered Species Act (ESA) would be through an appended Natural Resources Conservation Service Programmatic Biological Opinion (#08ESMF00-2012-F-0524). Project coverage under the California Endangered Species Act (CESA) would be obtained through the Alameda County Voluntary Local Program. Under this program, ACRCD holds a programmatic take authorization for California tiger salamander (CTS) and Alameda whipsnake (AWS). The lessee is eligible to enroll in the program. ACRD completed a programmatic Mitigated Negative Declaration (MND) for the Alameda County Voluntary Local Program. The proposed work was addressed in the MND. The project would follow required avoidance and minimization measures included in the above-listed permits and MND for the project, including the presence of a biological monitor on site during work activities.

The purpose of the proposed work is to improve the distribution and availability of livestock and wildlife water, reduce pressure on riparian areas and improve the resiliency of the grazing operation. The project would be funded through the Natural Resources Conservation Service's (NRCS) Environmental Quality Incentives Program (EQIP) with additional funding from the State Coastal Conservancy's Climate Ready Program through a grant from the Alameda County Resource Conservation District (ACRCD). The grazing tenant would pay for and perform the work to install

REVISED June 10, 2016 Project Review Meeting Summary
San Francisco Public Utilities Commission – Water Enterprise
Natural Resources and Lands Management Division

the proposed improvements, and then seek reimbursement for up to 50% of the costs from the funding sources cited above. The grazing tenant would then likely seek reimbursement under the terms of the grazing lease for the balance of the unfunded cost of installing permanent improvements.

Follow-Up:

- 1) SFPUC-NRLMD will provide a GIS map with special status species and sensitive habitat near the project area to the NRLMD Rangeland Manager (contact Jonathan Mendoza, Land and Resources Planner, at jsmendoza@sfgwater.org or (650) 652-3215). **Update: GIS map as sent to the NRLMD Rangeland Manager on 06/21/16 .**
- 2) If the ground disturbance associated with the installation of the pipeline and trough pads will be reseeded with an erosion control seed mix, then the seed mix must be approved by the SFPUC. The project applicant will provide a copy of the proposed erosion control seed mix to SFPUC-NRLMD biologist staff for review (contact Scott Simono, Biologist, at ssimono@sfgwater.org or (415) 934-5778).
- 3) The project sponsor and/or its contractor will contact the SFPUC-NRLMD Watershed Forester 24 hours in advance of work to confirm that conditions are suitable for construction (contact Dave Baker, Watershed Forester, at dbaker@sfgwater.org or (650) 652-3202). In addition, the project sponsor and/or its contractor will submit fire prevention measures, particularly for any hot work (e.g. welding) to the NRLMD Watershed Forester for review and approval. During construction, the project sponsor and/or its contractor will contact the National Weather Service daily to confirm that local weather conditions are suitable for construction activity. The project sponsor and/or its contractor will cease all construction activities during red flag days (high fire hazard periods) or if directed to do so by the NRLMD Watershed Forester.
- 4) The project sponsor will ensure that all construction debris is removed from SFPUC property and disposed of properly and legally. In addition, the project sponsor will restore the project site to pre-construction conditions upon completing its work on SFPUC property and arrange for a post-construction/restoration site inspection by SFPUC staff (contact Neal Fujita, Alameda Watershed Manager, at nfujita@sfgwater.org or (925) 862-5516).

2 Case No.	Project	Applicant/Project Manager
16.06-RW40.00	Menlo Business Park Building 7 Renovation 1430 O'Brien Dr., Menlo Park	John Tarlton (Tarlton Properties) and Ann Marie Taheny (DES Architects + Engineers)

The proposal is to demolish one of three existing driveways at an existing office park located across the SFPUC ROW and replace it with a new pedestrian walkway and landscaping improvements. The SFPUC owns this ROW parcel in-fee which contains three water supply lines: Bay Division Pipelines (BDPLs) Nos. 1, 2 and 5. However, an easement was granted over the SFPUC ROW to the Dumbarton Distribution Center in the 1980s that allows certain uses. It was noted by the Project Review committee that the developed parcel is only accessible by crossing over the SFPUC ROW and that this type of easement would not be granted today because the SFPUC ROW is the sole emergency vehicle access (EVA). The existing EVA over the SFPUC ROW is an existing non-conforming use made possible under the terms of the easement from the 1980s.

The project sponsor, Tarlton Properties, is renovating an existing research and development (R&D) building adjacent to the SFPUC ROW to create combined amenities and R&D facilities at 1430 O'Brien Drive. The existing building occupies the central portion of 1430 O'Brien Drive with parking areas on all sides of the building. Three driveway entrances are located along O'Brien Drive. There are paved patios and walkways at the building entry facing O'Brien Drive and this street frontage is screened by mature trees and landscaping. The project sponsor would remove one large tree in front of the main entrance that is located outside of the SFPUC ROW. The driveway modification includes removing an existing driveway and installing a hardscaped walkway, decomposed granite, landscaping and asphalt concrete replacement.

The building improvements proposed at 1430 O'Brien Drive are **not** within the SFPUC ROW. The improvements include the following: a gym, conference center, restaurant/bar, EV charging station and deck that would serve the tenants of the project sponsor's 12 buildings in the Menlo Business Park and its 8 buildings along O'Brien Drive and

REVISED June 10, 2016 Project Re-Review Meeting Summary
San Francisco Public Utilities Commission – Water Enterprise
Natural Resources and Lands Management Division

Willow Road which are located outside of the Menlo Business Park. The restaurant would be open to the general public. No new electrical conduit is proposed across the SFPUC ROW (the electrical vehicle (EV) charging station would be located outside of the SFPUC ROW).

The project sponsor stated that the property currently operates with less than the minimum required parking. Per the project sponsor, the City of Menlo Park is allowing the proposed uses to continue with less than the minimum required parking because a traffic demand management program would be implemented in this part of Menlo Park.

The project sponsor is proposing to use a crane. Per the project sponsor, there is a compacted soil, base rock and asphalt concrete located in and around the ROW. WSTD-Land Engineering is requesting potholing every 150 feet along the SFPUC ROW.

Demolition would begin in the summer of 2016. Construction would begin in the fall of 2016 and would take approximately 10 months to complete.

Follow-up:

- 1) The project sponsor will maintain the same or less number of parking spots on the SFPUC ROW.
- 2) The project sponsor will submit copies of 1430 O'Brien Dr., Menlo Park as-builts (updated as necessary) for the existing paved parking area over the SFPUC ROW showing the depth of cover over the SFPUC water transmission pipelines to SFPUC-WSTD Land Engineering (contact Tracy Leung, Associate Engineer, at tleung@sfgwater.org or (650) 871-3031).
- 3) The manholes located at the project site will be inspected to determine the depth of the pipelines. If the depth of all pipelines cannot be determined from the manholes, then the project sponsor will obtain a consent letter to perform potholing from SFPUC-WSTD Land Engineering (contact Tracy Leung, Associate Engineer, at tleung@sfgwater.org or (650) 871-3031). WSTD-Land Engineering requires potholing along the SFPUC ROW and has requested that potholing be performed approximately every 150-foot (or as determined by SFPUC staff).
- 4) The project sponsor will submit revised engineering plans to SFPUC-WSTD Land Engineering for review and approval showing the following: SFPUC property boundary lines, all water supply pipelines, pipeline depths, all appurtenances, 12-foot wide vehicular access routes to appurtenances, 10-foot radius clearance around all appurtenances, and staging areas (if any) to be used during construction (contact Tracy Leung, Associate Engineer, at tleung@sfgwater.org or (650) 871-3031).
- 5) The project sponsor will submit load calculations for all heavy equipment crossing or used within the SFPUC ROW (contact Tracy Leung, Associate Engineer, at tleung@sfgwater.org or (650) 871-3031).
- 6) The project sponsor will submit landscaping plans to the SFPUC ROW Manager for review and approval (contact Jane Herman, ROW Manager, at jherman@sfgwater.org or (650) 652-3204).
- 7) The project sponsor will work with SFPUC Real Estate Services to obtain a consent letter for the proposed project within the SFPUC ROW owned in-fee (contact Chris Wong, Principal Administrative Analyst, at CJWong@sfgwater.org or (415) 487-5211).
- 8) The project sponsor and/or its contractor will contact SFPUC Millbrae Dispatch at (650) 872-5900 at least 24 hours prior to commencing work.
- 9) The project sponsor will ensure that all construction debris is removed from SFPUC property and disposed of properly and legally. In addition, the project sponsor will restore the project site to pre-construction conditions upon completing its work on SFPUC property and arrange for a post-construction/restoration site inspection by SFPUC staff (contact Jane Herman, ROW Manager, at jherman@sfgwater.org or (650) 652-3204).
- 10) The project sponsor will add the SFPUC's Millbrae Dispatch phone number to its emergency contact list. The SFPUC's Millbrae Dispatch phone number is (650) 872-5900 and is available 24-hours a day, seven days a week.

BELO MARKET RATE HOUSING AGREEMENT

This Below Market Rate Housing In Lieu Fee Agreement ("Agreement") is made as of this ____ day of _____, 2016 by and between the City of Menlo Park, a California municipality ("City") and Tarlton Properties, Inc., a California Corporation ("Applicant"), with respect to the following:

RECITALS

- A. Applicant owns a building, located at that certain real property in the City of Menlo Park, County of San Mateo, State of California, consisting of approximately 3.53 acres, more particularly described as Assessor's Parcel Number: 055-473-160 ("Property"), and commonly known as 1430 O'Brien Drive, Menlo Park.
- B. The Property currently contains one building with a combination of office and research and development (R&D) spaces. The gross floor area of the existing building is approximately 65,952 square feet.
- C. Applicant proposes to add approximately 19,102 square feet of gross floor area for fitness and health, café, and R&D and office uses through additions and expansions within the existing building. Applicant has applied to the City for a use permit and architectural control to increase the square footage within the building ("Project").
- D. Applicant is required to comply with Chapter 16.96 of City's Municipal Code ("BMR Ordinance") and with the Below Market Rate Housing Program Guidelines ("Guidelines") adopted by the City Council to implement the BMR Ordinance. In order to process its application, the BMR Ordinance requires Applicant to submit a Below Market Rate Housing Agreement. This Agreement is intended to satisfy that requirement. Approval of a Below Market Rate Housing Agreement is a condition precedent to the approval of the applications and the issuance of a building permit for the Project.
- E. Residential use of the Property is not allowed by the applicable zoning regulations. Applicant does not own any sites in the City that are available and feasible for construction of sufficient below market rate residential housing units to satisfy the requirements of the BMR Ordinance. Applicant may explore opportunities to deliver off-site units. Therefore, based on these facts, the City has found that the BMR Agreement should allow for the flexibility for Applicant to deliver one off-site unit, partner with other applicants to deliver the equivalent of at least 0.7 units toward the creation of an off-site unit, or pay the applicable in lieu fee.

- F. Applicant, therefore, is required to pay an in lieu fee or deliver off-site units as provided for in this Agreement. Applicant is willing to pay the in lieu fee or deliver off-site units on the terms set forth in this Agreement, which the City has found are consistent with the BMR Ordinance and Guidelines.

NOW, THEREFORE, the parties agree as follows:

1. Applicant shall satisfy its obligations under the BMR Ordinance and Guidelines ("Developer's BMR Obligations") by either (a) paying the in lieu fee, (b) delivering one off-site unit, or (c) partnering with other applicants to deliver the equivalent of at least 0.7 units toward the creation of an off-site unit. If the applicant pays the in-lieu fee without providing any units,.
2. If Applicant elects to proceed with the Project and pay an in lieu fee, Applicant shall pay the estimated in lieu fee of \$228,070.30 as provided for in the BMR Ordinance and Guidelines. Notwithstanding the proceeding, nothing in this Agreement shall obligate Applicant to proceed with the Project. The applicable in lieu fee is that which is in effect on the date the payment is made. The in lieu fee will be calculated as set forth in the table below; however, the applicable fee for the Project will be based upon the amount of square footage within Group A and Group B at the time of payment. The estimated in lieu fee is provided below.

Table 1: BMR Requirements and Applicant Proposal			
	Fee per square foot	Square feet	Component fees
Existing Building - Office	\$16.15	65,952	(\$1,065,124.80)
Existing Building - Non-Office	\$8.76	0	\$0.00
Proposed Building - Office	\$16.15	74,754	\$1,207,277.10
Proposed Building - Non-Office	\$8.76	9,808	\$85,918.08
BMR In-Lieu Fee Option			\$228,070.30

The in lieu fee may be paid at any time after approval of this Agreement by the Planning Commission. If for any reason, a building permit is not issued within a reasonable time after Applicant's payment of the in lieu fee, upon request by Applicant, City shall promptly refund the in lieu fee, without interest, in which case the building permit shall not be issued until payment of the in lieu fee is again made at the rate applicable at the time of payment.

3. If the Applicant elects to proceed with the Project, the Applicant shall pay the in lieu fee, partner with other applicants to deliver the equivalent of at least

- 0.7 units toward the creation of an off-site unit, or pay the in lieu fee prior to final sign-off of the building permit.
4. This Agreement shall be binding on and inure to the benefit of the parties hereto and their successors and assigns. Each party may assign this Agreement, subject to the reasonable consent of the other party, and the assignment must be in writing.
 5. If any legal action is commenced to interpret or enforce this Agreement or to collect damages as a result of any breach of this Agreement, the prevailing party shall be entitled to recover all reasonable attorney's fees and costs incurred in such action from the other party.
 6. This Agreement shall be governed by and construed in accordance with the laws of the State of California and the venue for any action shall be the County of San Mateo.
 7. The terms of this Agreement may not be modified or amended except by an instrument in writing executed by all of the parties hereto.
 8. This Agreement supersedes any prior agreements, negotiations, and communications, oral or written, and contains the entire agreement between the parties as to the subject matter hereof.
 9. Any and all obligations or responsibilities of Applicant under this Agreement shall terminate upon the payment of the required fee.
 10. To the extent there is any conflict between the terms and provisions of the Guidelines and the terms and provisions of this Agreement, the terms and provisions of this Agreement shall prevail.

IN WITNESS WHEREOF, the parties hereto have executed this Agreement as of the day and year first written above.

CITY OF MENLO PARK

Tarlton Properties Inc.

By: _____
City Manager

By: _____
Its: