



REGULAR MEETING **AMENDED** AGENDA

Date: 10/16/2017
Time: 7:00 p.m.
City Council Chambers
701 Laurel St., Menlo Park, CA 94025

This agenda is amended to modify item E1

A. Call To Order

B. Roll Call

C. Reports and Announcements

Under "Reports and Announcements," staff and Commission members may communicate general information of interest regarding matters within the jurisdiction of the Commission. No Commission discussion or action can occur on any of the presented items.

D. Public Comment

Under "Public Comment," the public may address the Commission on any subject not listed on the agenda, and items listed under Consent Calendar. Each speaker may address the Commission once under Public Comment for a limit of three minutes. Please clearly state your name and address or political jurisdiction in which you live. The Commission cannot act on items not listed on the agenda and, therefore, the Commission cannot respond to non-agenda issues brought up under Public Comment other than to provide general information.

E. Consent Calendar

- E1. Approval of minutes from the September 11, 2017, Planning Commission meeting. ([Attachment](#))

F. Regular Business

- F1. Review of Substantial Conformance Determination/David Ruth/350 Sharon Park Drive:
Review of staff determination of substantial conformance for exterior modifications to 18 apartment buildings and a clubhouse located at 350 Sharon Park Drive located in the R-3-A-Z zoning district. ([Attachment](#))

G. Public Hearing

- G1. Conditional Development Permit Amendment, Development Agreement Amendment, and Environmental Review/Hibiscus Properties, LLC/301-309 Constitution Drive:
Request for an amendment to a Conditional Development Permit (CDP) approved in November 2016 to modify the location, design, and footprint of Building 22 (Phase 2) of the Facebook

Campus Expansion Project, construct an eight-story parking structure, allow for the retention of Building 305 during construction of Building 22, add an electrical vehicle charging facility for intra-campus trams, shuttles, and buses, and incorporate additional landscaping once Building 305 is removed. Building 22 would continue to meet the minimum setback requirements of the CDP; however, the building mass and footprint, along with the proposed garage, would be shifted toward the north of the site along the Bayfront Expressway frontage and the location and design of the potential connection between Buildings 21 and 22 would be modified. No changes to the hotel are proposed at this time, and the hotel would be reviewed through a separate future architectural design review, as set forth in the CDP. The proposed modifications would continue to comply with the minimum parking ratio, floor area ratio, building coverage requirements, and maximum height limits of the previously approved CDP, with the exception of specific elements (skylights and maintenance platforms) of the modified Building 22 that would exceed the 75-foot height limit for a total potential height of 97 feet. The CDP would also allow for the fire smoke ventilation fans on the roof to temporarily exceed the noise limitation of the Zoning Ordinance during weekly routine testing. The increase in building height and the extent of the proposed changes to the site plan and conditions within the CDP require an amendment to the previously approved CDP. The proposed CDP amendment would further refine the conditions of approval and set the applicable development standards for the proposed revised project. The proposed modifications to the Development Agreement would be limited to changes to ensure consistency with timing of benefits associated with the previously approved project and additional funds for city services. The project site is located in the O (Office) zoning district. The City Council certified the Environmental Impact Report (EIR) as part of its approval of the project in November 2016. The proposed revisions were analyzed for consistency with the certified EIR. That analysis found that the proposed revised project did not result in new impacts or an increase in the severity of previously identified impacts. As a result an Addendum to the certified EIR has been prepared. Copies of the Facebook Campus Expansion Project EIR Addendum are available for review at the City offices. ([Staff Report #17-064-PC](#))

H. Informational Items

H1. Future Planning Commission Meeting Schedule – The upcoming Planning Commission meetings are listed here, for reference. No action will be taken on the meeting schedule, although individual Commissioners may notify staff of planned absences.

- Regular Meeting: October 23, 2017 (canceled)
- Regular Meeting: November 6, 2017
- Regular Meeting: November 13, 2017

I. Adjournment

Agendas are posted in accordance with Government Code Section 54954.2(a) or Section 54956. Members of the public can view electronic agendas and staff reports by accessing the City website at www.menlopark.org and can receive e-mail notification of agenda and staff report postings by subscribing to the "Notify Me" service at menlopark.org/notifyme. Agendas and staff reports may also be obtained by contacting the Planning Division at 650-330-6702. (Posted: 10/11/17)

At every Regular Meeting of the Commission, in addition to the Public Comment period where the public shall have the right to address the Commission on any matters of public interest not listed on the agenda, members of the public have the right to directly address the Commission on any item listed on the agenda at a time designated by the Chair, either

before or during the Commission's consideration of the item.

At every Special Meeting of the Commission, members of the public have the right to directly address the Commission on any item listed on the agenda at a time designated by the Chair, either before or during consideration of the item.

Any writing that is distributed to a majority of the Commission by any person in connection with an agenda item is a public record (subject to any exemption under the Public Records Act) and is available for inspection at the City Clerk's Office, 701 Laurel St., Menlo Park, CA 94025 during regular business hours.

Persons with disabilities, who require auxiliary aids or services in attending or participating in Commission meetings, may call the City Clerk's Office at 650-330-6620.



REGULAR MEETING MINUTES - DRAFT

Date: 9/11/2017
Time: 7:00 p.m.
City Council Chambers
701 Laurel St., Menlo Park, CA 94025

A. Call To Order

Chair Drew Combs called the meeting to order at 7:00 p.m.

B. Roll Call

Present: Andrew Barnes, Drew Combs (Chair), Susan Goodhue, Larry Kahle (Vice Chair), John Onken, Henry Riggs

Absent: Katherine Strehl

Staff: Sunny Chao, Assistant Planner; Deanna Chow, Principal Planner; Angela Obeso, Senior Transportation Engineer; Nikki Nagayo, Assistant Public Works Director

C. Reports and Announcements

Principal Planner Deanna Chow said the City Council considered the 2131 Sand Hill Road, Stanford Annexation and Office Project last week, and continued it, tentatively to its September 26, 2017 meeting. She said the City Council tentatively at the same meeting would consider the 500 El Camino Real Mixed-Use Project.

D. Public Comment

There was none.

E. Consent Calendar

E1. Approval of minutes from the August 14, 2017, Planning Commission meeting. ([Attachment](#))

ACTION: Motion and second (John Onken/Susan Goodhue) to approve the minutes; passes 6-0 with Commissioner Strehl absent.

F. Public Hearing

F1. Use Permit/Leila Osseiran/1074 Del Norte Avenue:
Request for a use permit to partially demolish, remodel, and add a second story addition to an existing nonconforming single-story, single-family residence on a substandard lot with respect to lot width in the R-1-U (Single-Family Urban Residential) zoning district. The proposed work would exceed 50 percent of the existing replacement value in a 12-month period. The proposal would

also exceed 50 percent of the existing floor area and is considered equivalent to a new structure. The project was previously reviewed and continued at the Planning Commission meeting of May 22, 2017. ([Staff Report #17-057-PC](#))

Staff Comment: Assistant Planner Sunny Chao said there were no updates to the written staff report.

Applicant Presentation: Andreas Hofmann, property owner, said the existing structure was built in 1954 and was a 1700 square foot home with garage on an approximately 8700 square foot lot. He said the proposed renovation would increase the footprint expanding the master bedroom, relocating the kitchen to the covered patio area, and adding a second story with two bedrooms and two bathrooms. He said the Commission had provided detailed comments when they considered the project at its May 22, 2017 meeting, and he would address each one. He said in the original design they had a 12-foot wall connecting the first to the second floor which was now completely revised by relocating the second floor addition back and to the left. He said they decided to use stucco with stone accents as opposed to using redwood siding. He said the second comment regarded the roof lines and balance. He said the hip roof was used throughout the entire design now with just a flat roof over the entrance door. He said another concern was about the lack of connectivity between the shed roof over the garage and the elevation and termination of the hip roof on the right side elevation. He said all of that had been addressed with the second floor centered with a very traditional roof that went around the sides of the house. He said the fourth comment was to redesign the addition over the garage as it was disproportionate compared to the rest of the house. He said they addressed that by moving the second floor addition to the left and centered and no longer massing to one side. He said a fifth comment was to go over the whole design to make sure it was balanced and to work out the details so it would fit within the neighborhood. He said they believed that they had addressed all of the comments.

Commissioner Larry Kahle said the initial proposal had redwood siding and he did not think the Commission wanted to have that eliminated. He asked if the applicant would be willing to add some redwood siding to the design. Mr. Hofmann said they wanted to give the house a modern feel. He said they experimented with different siding and decided on stone accents to frame the entrance and somewhat above as that aligned with their desired style and also with some of the work done in the neighborhood.

Chair Combs opened the public hearing.

- Cathy Tokic, Menlo Park, expressed her appreciation for the kindness and thoughtfulness of the applicants, her neighbors, noting they were active in the community. She said the homes in the neighborhood were built post-World War II and constructed well but were very similar in style. She said the neighborhood now was charmingly eclectic with home styles that included Tudor, Mediterranean, contemporary, Spanish, ranch, and cottage. She said the subject property proposal was tastefully designed to blend with the surrounding area. She expressed support for the project noting the letter she read was signed also by her husband David. She noted she had two other letters. She said in the first one from Ramsey Najal he wrote that he and his wife April lived at 24 Oakwood Place and supported the proposed project. She said the second letter was from Janelle Gee and Ryan Sandoval who lived directly across the street from the subject property and they also were in support of the project.

Chair Combs closed the public hearing.

Commission Comment: Commissioner Onken thanked the applicant for taking the Commission comments seriously and making significant changes. He said he could support the project.

Commissioner Goodhue said she appreciated the applicants' efforts noting she had voted against the continuance for redesign as she thought the Commission was leaning toward designing from the dais. She said she thought the applicant had responded well to Commission comments.

Commissioner Kahle said he appreciated the changes made, especially the relocation of the second floor to address massing concerns and the change in window materials from vinyl to wood. He said his two concerns were the siding as he did not think it read as contemporary and that the stucco with some limited stone accents seemed overwhelming. He said he would like to see some redwood from the original proposal. He said his second concern was the height of the second floor which would benefit from having eight foot ceilings rather than nine foot ceilings. He said otherwise he thought it was much improved from the original proposal.

Commissioner Henry Riggs thanked the applicant for his efforts, noting that the lot was certainly not a small lot although it was substandard in lot width. He noted two palms and a yucca tree in the front yard and asked if the applicant was interested in removing those as it might be advantageous to get that request made at the Commission level.

Principal Planner Chow said the Planning Commission could provide guidance about trees but any removals would have to be done through the heritage tree removal process.

Mr. Hofmann said both of the palm trees had overgrown their location.

Commissioner Riggs asked about the finish materials on the right side elevation that seemed to have a window seat. Walid Nazzal, project designer, said the material was stucco.

Commissioner Riggs moved to approve as recommended in the staff report with an added recommendation of support should the property owner want to remove any of the two palm trees and/or yucca tree in the front yard. Commissioner Goodhue seconded the motion.

Chair Combs said he recalled that he thought the Commission had been very detailed in its comments and he appreciated the applicants' efforts to address those.

ACTION: Motion and second (Riggs/Goodhue) to approve as recommended in the staff report with following modification; passes 6-0 with Commissioner Strehl absent.

1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Approve the use permit subject to the following **standard** conditions:

- a. Development of the project shall be substantially in conformance with the plans prepared by Walid Nazzal and Associates consisting of 12 plan sheets, dated received September 5, 2017, and approved by the Planning Commission on September 11, 2017, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
- b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.
- c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
- d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
- e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division.
- f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits.
- g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.

I. The Commission supports removal of any of the two palm trees and one yucca tree in the front yard.

- F2. Use Permit/Srinath Narayanan/1005 Almanor Avenue:
Request for a use permit to demolish a one-story, single-family residence with a detached garage and to construct a new two-story, single-family residence with an attached garage and a basement on a substandard lot with regards to lot width in the R-1-U (Single-Family Urban Residential) zoning district. As part of the proposed development, one heritage London plane tree in the left corner of the rear yard is proposed for removal. ([Staff Report #17-058-PC](#))

Staff Comment: Assistant Planner Chao said the project architect informed staff today that the basement would perfectly align with the wall of the first floor. She said in the staff report she had noted that approximately 30 square feet was outside the wall of the first floor and counted toward Floor Area Limit (FAL) even though the total FAL was under the allowable maximum. She said with this alignment the project was substantially conforming.

Questions of Staff: Commissioner Riggs asked if the excavation for the light well was within the building wall. Assistant Planner Chao said light wells were not included in FAL calculations. Commissioner Riggs said he was thinking about the neighbor's concern regarding excavation near the property line. Assistant Planner Chao said there were three light wells with one on each side and one at the rear. Commissioner Riggs said the change would put the light wells closer to the property lines on the sides. Assistant Planner Chao suggested referring to the project architect.

Applicant Presentation: Greg Miller, project designer, said the plans currently showed the basement coming forward a bit on the right front of the house toward the large oak tree. He said with the revised plan that an area of the basement would be pulled back to align with the first floor footprint, noting that was a mechanical room and did not need to be as large as proposed originally. He said pulling away from the heritage oak tree was good. He said this would not affect the light wells and those would not get any closer to the side or rear property lines.

Mr. Miller said the staff report was very thorough. He said they tried to design a house that would be a good addition to the neighborhood. He said the lot width was 64 feet in a zone requiring 65 feet width which meant the project needed a use permit. He said they tried to keep all other standards below maximum allowable. He said the building was 28-feet in height but under the daylight plane limits. He said the second floor was setback from the front and sides and windows were kept minimal and small both in number and size on the sides to maintain privacy for the neighbors. He said regarding excavation that sheet SB1 was done by an engineer to show that excavation would not pose danger to the neighbors. He said the applicant's family was present as well.

Commissioner Kahle noted the coastal oak tree, #3, in the front was rather close to the house and where a 10-foot deep basement was being proposed. He said he did not see anything in the arborist report regarding that and asked if staff was concerned with the tree being that close to a deep basement. Mr. Miller said that the arborist report had specific recommendations regarding the tree preservation and protection. He said the tree was a fantastic element in the lot and they would do everything to protect it. Commissioner Kahle said on the shoring plan the shoring pier, the stitch piers, did not wrap around the front. Mr. Miller said they did not need to wrap around the front. He said they could use them in the front of the tree if that was what they needed to do to keep the excavation to match the arborist report. He said generally the shoring was on the side as this was a small lot and they could not do a 45-degree cut.

Commissioner Kahle said he did not think the plans matched the left side elevation. He said on the floor plan there was a large window in the bedroom, no window in the closet and one in the shower and that was different from the elevation. Mr. Miller said there would be two windows and the plan was accurate. He said they would not do a high window in the closet. Commissioner Kahle confirmed with Mr. Miller that the one bedroom window would be smaller.

Commissioner Riggs said Mr. Miller had said the windows were small and minimized. He said typically first floor windows were below the fence height or at the fence height, and typically not an issue. He said on the right side neither the office nor the rear room had any windows on the first floor, and asked Mr. Miller if they really wanted to do that. Mr. Miller said there was not much to look at noting the side yard was short and there was a fence. He said they were getting enough light from the front and back where more light would come into the house. He said in the dining room he wanted to use the entire wall for shelves and cabinets. He said for the office they could

easily add a window there but was not sure it was needed. Commissioner Riggs said a neighbor had expressed concern that there was sandy soil related to excavating. He asked what the soils report had found. Mr. Miller said he had not studied the soils report carefully but recalled there was some sand at some point. Commissioner Riggs said that sandy clay and sandy loam were quite different. He asked why they were removing the heritage plane tree from the back of the lot. Mr. Miller said it was a heritage tree by virtue of its size but it was not attractive and dropped a lot of debris. He said when they removed the garage and started grading there was concern that the tree would not survive. He said it was not 100% necessary to remove the tree but they thought it would make for a better lot and they would replace it with several other trees.

Commissioner Riggs left the dais and brought up a slide of the plane tree. He said in his opinion it was a rather well formed plane tree at the intersection of four lots. He said in removing it there would be a hole in the landscaping. He said it would provide shade for two lots from the south sun during the summer and above all it was a heritage tree. He said reasons for the heritage tree ordinance were heat gain, oxygen regeneration, shade and aesthetics. He said he could not understand removing a heritage tree that was very close to an existing structure, which was actually being removed. He said excavation for slab on grade should not be much of an impact on the tree.

Mr. Miller said primarily it was the tree appearance and its mess why they wanted to remove it but they did not have to and would consult with their arborist regarding its preservation and protection during demolition.

Chair Combs said the City Arborist had given tentative approval of the heritage tree removal permit and asked what that would mean if the project were approved by the Planning Commission. Assistant Planner Chao said that tentative approval would become approval if the project were approved by the Planning Commission. Chair Combs confirmed with Ms. Chao that the City Arborist's perspective was that this was a tree that should or could be removed. He clarified with Ms. Chao that the tree removal request would not be reviewed by the Environmental Quality Committee.

Commissioner Andrew Barnes asked if the City Arborist's finding of support for heritage tree removal was based on a tree's health. Principal Planner Chow said the City Arborist looked at different things related to heritage tree removal including the tree health. She said in the case of development there was a consideration of design constraint and appropriateness. She said if the Planning Commission did not approve a project the tree removal permit might not necessarily be granted. Commissioner Barnes asked if the tree was perfectly healthy. Principal Planner Chow referred to the included arborist's report, which said the tree's health was fair.

Chair Combs opened the public hearing.

- Christine Couch, Menlo Park, said her home was across the street from the subject property. She said she had no objections to the project but had some requests for information. She asked regarding the basement excavation if they ran into groundwater what would happen to it; if there was a build time limit in Menlo Park; and how construction traffic and congestion were regulated.

Chair Combs closed the public hearing.

Commission Comment: Chair Combs asked about the speaker's question about groundwater being found during excavation. Principal Planner Chow said during the building permit process engineering, building and planning reviewed the plans and that inspections would occur during construction. She said she did not know if there was a particular concern. Ms. Couch spoke from the audience. Chair Combs clarified the question was whether Menlo Park had regulations for the handling of groundwater if encountered during excavation and whether it would be recycled or disposed. Principal Planner Chow said she did not have specifics on the dewatering of a construction site.

Chair Combs asked regarding build time limit if there was a construction time line that projects had to follow, and if it was not followed, whether there were penalties. Principal Planner Chow said there was no specific time limit but there needed to be an active building permit. She said if it became inactive reapplication would need to be made.

Chair Combs said the final question was about construction traffic and congestion, and whether there was a requirement for a construction traffic plan. Principal Planner Chow said the City's ordinance limited construction hours to Monday through Friday. She said regarding a construction staging plan staff could work with the applicant to provide something that was incorporated into the plans to ensure the construction vehicles would be on the project site and limit the amount of parking in the public right of way.

Chair Combs said he hoped that the speaker's questions had been answered and suggested that she reach out to City staff if more information was needed.

Commissioner Kahle said the project's geotechnical engineer would address any water table issues. He said as far as he knew the City did not have any regulations regarding dewatering. He said the plane tree in question was located in a six-foot public utility easement and per the arborist's report was trimmed to clear overhead utility lines, which was another consideration. He said his concern had been the oak in the front and with the work conducted per the arborist's recommendations that was addressed. He said the house was attractive and well designed, and he thought it would fit the neighborhood very well. He said the rendering showed a tan house but the applicant's letter said it would have a white board and batten siding which he thought would be attractive. He said a minor concern he had was with the balcony in front as it looked nice but he was not sure it would ever be used. He said the other concern was the height as it was right at the maximum allowable. He said the home had 10-foot ceilings on the first floor and nine foot ceilings on the second story and he would love to see that reduced or at least removing a foot if the first floor was two feet off the ground.

Commissioner Riggs said overall the proposal was an attractive house in a popular style and appeared to be well detailed and well done. He said it was sensitive to the neighbors. He said it appeared that the 28-foot height was only at a ridge about six feet in length. He moved to make the findings and approve the use permit with the exception he would not like the Commission to approve the plane tree approval. He said without any more information on why the tree could not be trimmed and preserved and weigh-in from the three other neighbors whose lots would be impacted by the tree's removal that he could not approve with that condition.

Chair Combs asked if separating the project approval and tree removal was possible. Principal Planner said the two could be separated and if the Planning Commission did not support the tree removal, staff would pass that information to the City Arborist.

Commissioner Onken seconded the motion with the intent of retention of the heritage plane tree in the rear. He said the design passed many of the litmus tests the Commission looks out for and he greatly appreciated keeping the live oak in the front as it was a far more important tree. He said he hoped the owner, the arborist and contractor would do everything possible to save the oak tree.

ACTION: Motion and second (Riggs/Onken) to approve as recommended in the staff report with the following modification; passes 6-0 with Commissioner Strehl absent.

1. Make a finding that the project is categorically exempt under Class 3 (Section 15303, "New Construction or Conversion of Small Structures") of the current California Environmental Quality Act (CEQA) Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Approve the use permit subject to the following **standard** conditions:
 - a. Development of the project shall be substantially in conformance with the plans prepared by Greg Miller Designs consisting of 12 plan sheets, dated received August 31, 2017, and approved by the Planning Commission on September 11, 2017, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
 - b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.
 - c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
 - d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
 - e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division.
 - f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits.

- g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the recommendations in the arborist report by McClenahan Consulting, LLC dated December 17, 2016 (revised May 1, 2017).
- h. ***The London Plane Heritage Tree Removal Permit is not supported by the Planning Commission.***

G. Regular Business

- G1. Ravenswood Avenue Railroad Crossing Study Presentation:
The Ravenswood Avenue Railroad Crossing Study is evaluating the feasibility of replacing the existing at-grade crossing of the Caltrain tracks within the City of Menlo Park with a prioritization at Ravenswood Avenue. The project team will be presenting the project status and requesting input to determine a preferred alternative, to answer questions and to receive feedback. ([Staff Report #17-059-PC](#))

Staff Comment: Principal Planner Chow said that Angela Obeso, Senior Transportation Engineer with the City would make the presentation.

Chair Combs said that after the presentation the public comment period would open prior to Commission questions, comments and discussions.

Staff Presentation: Senior Transportation Engineer Angela Obeso, project manager, introduced Ety Mercurio, project manager for the consultant team of AECOM. She said that they would be presenting project alternatives with a request for the Commission's recommendations on a preferred alternative to take to the City Council. She presented a PowerPoint reviewing briefly earlier studies of the Caltrain Ravenswood crossing done in 2003, the use of a San Mateo County Transportation Authority grant to consider the feasibility of replacing the existing at-grade crossing of Caltrain tracks within the City with the prioritization of the Ravenswood Avenue crossing; Alternative A which would leave the crossing as it is and Alternative B was a hybrid option that raised the train tracks some at Ravenswood Avenue. She said two other options were looked at for the previous feasibility studies and those were trench alternatives that would bring the rail elevation underground. She said those were found to be infeasible within the City limits because of constraints of different rail curvature and grade slope of the rail such that a full trench was not achievable. She said drainage east-west and in the tunnel would be complex and construction would require a long window incurring high costs. She said another option looked at in previous studies was a viaduct or raised track. She said similar to the trench option the constraints of the geometry of the train tracks with required slopes to maintain would not allow the complete elevation needed to have the required clearance over all four of the city's crossings. She said again this would require a complicated, lengthy and expensive construction process.

Ms. Obeso said they took the most feasible alternatives of the previous studies now called the underpass and the hybrid. She said they met with the City's Rail Subcommittee and City Council and received direction from them on how to proceed. She said they also had a series of meetings with Caltrain and the Santa Clara County Transportation Authority and those helped in determining what a feasible scope should look like. She said in late 2015 the City did a request for proposals for consultants to help with the project study report. She said a contract was awarded to AECOM in March 2016.

Ms. Obeso said with the hiring of the consultant to help with the study they immediately began with community outreach noting that since then they have hosted three public meetings, hosted a rail information meeting open to the public, and conducted ongoing one-on-one meetings with all the potentially impacted property owners along the entire rail corridor. She said also they have visited with City Commissions to provide status updates and have visited with the City Council twice. She said after tonight they would make a presentation to the City's Complete Streets Commission and then go to the City council on October 10 summarizing both what they have heard to date, at this evening's meeting and the September 13 Complete Streets Commission meeting, and ask Council to give direction on a preferred alternative so staff could get started with funding and working on environmental and design.

Ms. Obeso said with the feedback to date there had been reoccurring themes and concerns expressed by the community. She said that people would generally like more grade separations as it would increase east-west connectivity and improve delays there. She said another theme was to minimize the height of the railroad as people were concerned with the visual impact of the higher railroad. She said another was to improve pedestrian and bicycle access particularly east to west and improve the connectivity particularly at the intersection of Alma Street and Ravenswood Avenue. She said at the dais were printouts of emails received over the past couple of days to the City Council's email box that the Commissioners might not have seen yet

Ms. Obeso said in December they had presented three different alternatives to the Commission. She said one was an underpass and the other two were variations of a hybrid. She said related to why they were bringing only two of the alternatives forward that Alternative A, the underpass, was looking just at grade separation at Ravenswood Avenue, leaving the train tracks at their existing elevation and bringing Ravenswood Avenue under the train tracks. She said with that alternative there was no proposal to do any changes at Oak Grove, Glenwood and Encinal Avenues. She said Alternative B was a version of the hybrid that would potentially grade separate Ravenswood and Oak Grove Avenues. She said this would have a steeper slope and a higher elevation of rail. She said Alternative C was a hybrid that looked at three grade crossings at Ravenswood, Oak Grove, and Glenwood Avenues that would have generally a flatter rail elevation through town compared to Alternative B. She said based on the feedback they had received wanting more grade separations and less rail height it pointed them towards Alternative C and focusing more time on it as opposed to Alternative B. She said when they took this to City Council in April it directed them to move forward with Alternatives A and C.

Etty Mercurio, AECOM, project consultant, showed a 3-D animation of Alternative A and Alternative C. She said with Alternative A that access to Alma and Merrill Streets and Alma Lane from Ravenswood Avenue would be lost. She said Alternative C would maintain full access from Alma Street to Ravenswood Avenue and Alma Lane, and Merrill Street would connect into Ravenswood Avenue. She said both Alternatives separated the rail and vehicular traffic improving safety, reducing vehicle delays, and creating better connectivity between the east and west sides of Menlo Park. She said Alternative A would increase traffic on Laurel Street with some diversions of traffic onto Oak Grove and Glenwood Avenues. She said the traffic impact on Laurel Street with Alternative A would be mitigated by adding a right hand turn pocket onto Laurel Street to keep levels of service and traffic circulation at levels that were acceptable. She noted that in the years 2020 to 2040 with Alternative C that the City might well want to negotiate with Atherton to signalize the intersections of Laurel Street and Glenwood Avenue as well as at Middlefield Road and Glenwood Avenue.

Ms. Mercurio said a Shoofly was basically a detour for the railroad and was basically the same for Alternatives A and C. He said these were proposed toward the west closer to the commercial properties rather than closer to the residential properties. She showed graphics of the proposed Shoofly.

Ms. Mercurio said construction staging for either Alternative A or C was about getting the project ready to move the railroad over to its Shoofly. She said for Alternative A, the first stages would be relocating utilities, installing shoring to support the excavation, excavating down the roadway and installing temporary bridges underneath the Shoofly itself and constructing the Shoofly. She said upon completion of those two stages, the railroad traffic would be moved to the Shoofly after which roadway traffic would be moved to the north side and work would then begin on the south side of the roadway, completing the bridges for the railroad and for Alma Street. She said then they railroad would be moved onto its permanent alignment after which the Shoofly was removed and the roadway finished to open it up for full traffic. She said duration for Alternative A would be three to four years. She said staging for Alternative C was very similar to the staging plan for Alternative A except it would occur at three locations. She said duration for Alternative C would be four to five years.

Ms. Obeso said to determine preference for Alternative A or C they reviewed all of the comments they received from the public and stake holders. She said the items they kept hearing became the comparison factors. She directed the Commission's attention to the slide labeled *Alternatives Matrix*. She said this comparison showed with Alternative A that the City would get more moderate improvement with more moderate impact. She said with Alternative C the City would get a greater benefit and level of improvement but also a greater level of impacts. She said at the last community meeting on June 7, 2017 they presented this same information. She said they did breakout tables and asked attendees to vote for either Alternative A or C and why. She said about 85% of the 55 people who attended the meeting preferred Alternative C because it provided more grade separations, increased east-west connectivity for all modes, and achieved some of the goals they wanted to see. She said those who supported Alternative A said it had the highest volume of vehicles, pedestrians and bicyclists at Ravenswood Avenue and it had a lower construction cost. She said there was also continued interest in the trench and viaduct alternatives. She said they asked the community about construction considerations and they heard that the community generally would be open to longer working days such as 8 a.m. to 8 p.m. to help construction move more quickly. She said also there was support for some weekend and night work but not every weekend nor every night. She said one other item they heard was that whichever alternative was chosen it was critical to maintain bicycle and pedestrian access at all the crossings during construction.

Ms. Obeso said throughout the process they held a number of one-on-one meetings with all of the potentially impacted property and business owners. She said those property owners who had minimal potential impacts to their access generally preferred Alternative C and those property and business owners with major effects specifically on Oak Grove and Glenwood Avenues tended to prefer Alternative A. She said regarding construction impacts that these owners wanted assurance that access would be maintained to their properties for their residences, their customers, and their clients.

Ms. Obeso said they wanted the Commission's feedback and a recommendation on the preferred alternative. She said on Wednesday they would present to the Complete Streets Commission to

get their feedback and recommendation, and on October 10 they would present to the City Council with a summary of the commissions' feedback and asking for their direction on a preferred alternative.

Chair Combs opened public comment.

- Steve Schmidt, Menlo Park, said he thought Alternative C was preferable. He said that the best solution would be to separate the rail grade all through Menlo Park, which would mean all four grade crossings including Encinal Avenue. He said he was not convinced that it was infeasible for a viaduct line through Menlo Park going to grade at Atherton. He said the study was deficient in not analyzing all four grade crossings. He recommended to opening the process to include an Alternative D that looked at separating all four grade crossings.
- Adam Tobin said he and his wife owned a home on Mills Court, the back of which bordered the train track. He said they supported Alternative C or D as had just been suggested. He said their main concern currently was with noise and horns and quality of life. He said with Alternative A that traffic might get worse at the other crossings and that the noise would not be abated in terms of the horns. He said they were very much proponents of a quiet zone and considered that if Atherton could do so, Menlo Park could also do it. He said he appreciated the information about the Shoofly and found generally that Alternative C addressed their concerns.
- Steve Van Pelt, Menlo Park, said he wanted a plan to do one crossing at a time without a Shoofly. He said the impetus for that was that potentially there could be seven more under crossings when discussions of the Dumbarton corridor begin. He said the under passing at 5th Avenue was done without a Shoofly track. He said he did not know how expensive a Shoofly track would be but it would be very inconvenient. He confirmed with Chair Combs that the Commission would make a recommendation to City Council. He said he had submitted two letters to discuss his proposals in great detail and that he did not know if those were being considered or not. He said one letter was submitted in June this year and the other in 2014. He said he was not able to find his letters in any of the information related to this project and asked if he should resubmit them. Chair Combs said he should resubmit to Council and/or Commission and if the letters did not show up to call staff.

Chair Combs closed the public comment period.

Commission Comment: Commissioner Barnes said he would like to see some type of modeling of Alternative C that would show what it would look and feel like noting that the 3-D video did not provide that understanding.

Ms. Obeso noted a photo being provided by Commissioner Riggs of the San Carlos train berm that extended through most of that City as seen from the north or bayside. Commissioner Riggs said relative to Menlo Park this would be a similar view from Alma and/or Laurel Streets. He said the housing viewed behind the berm was two-story which he noted for sense of scale. He said this showed what an elevated track and berm would do to the visual perception and connection between one side of town and the other side. He said the area was landscaped but there was only so much that could be expected of landscaping for such a structure.

Chair Combs suggested staff continue responding to Commissioner Barnes' question as the photo provided by Commissioner Riggs was not part of staff's presentation. Ms. Obeso said she thought

looking at other examples was a good way to get real world view of what the different grade separations would look like. She said the cities of San Carlos and Belmont were examples of what a hybrid grade separation looked like and particularly at the stations. She presented some photos taken by the project team of other station areas with hybrid crossings and said that people could drive or take the train to see what those looked like.

Ms. Mercurio said they had done a similar grade separation study for the City of Burlingame. She said in the 3-D animations they took stills at eye level to give people a sense of when standing at a certain location what the project would look like.

Commissioner Barnes said he could visualize each individual underpass but he did not know what the experience, the look and feel, would be in the spaces in between for local residents with basically a 10-foot wall. Ms. Obeso said still photos could be created from the 3-D models shown this evening. She said at the community meeting they had virtual reality goggles at some locations generated from the same 3-D model. She asked if that was something that would be helpful to bring to City Council. Commissioner Barnes said his primary concern was the split this would create in Menlo Park and not being able to get a sense of the look and feel of that. He said with the scope and major expense of this project it was important for people to have a real sense of what it would be like when completed.

Ms. Obeso said in the area along Alma Street of Burgess Park and the Recreation Center, they took still photos and then put in a model of the elevation of the train running through. She said those were on the website but they could bring those back and share directly with people. She said www.menlopark.org/ravenswood had all of the reports and presentations done as well as the videos and animations. She said also if Commissioner Barnes could identify points of concern that they could work up some still photos and modeling to provide more information.

Commissioner Barnes asked about the data considered for the analysis in the Alternatives Matrix. Ms. Obeso said they did analysis for some of the items such as traffic for which they did counts and traffic models. She said they looked at where traffic would go with Alternatives A and C and how that would improve the delays and different traffic operations standards. Noting Commissioner Barnes' question about noise, she said that was a very complex issue for which they needed to bring in a noise specialist. She said that would be done as part of the environmental study. She said what they did was look at similar case studies where tracks were raised and how that affected noise. She said in general with grade separation that the horns, gates and bells were being removed and those created the noise that was most complained about. She said Caltrain would be electrifying, which would be less noisy than engines, and that would occur before the City's project.

Commissioner Barnes asked if Ms. Obeso would describe the analysis for each item on the matrix.

Ms. Obeso said for the item "Reduce Potential Rail/Vehicle Conflict" that they looked at the number of trains and how often they crossed at each location. She said Caltrain had done studies which provided information on how much delay grade crossings created now. She said in removing crossings they looked at how much potential decrease in delay was created.

Commissioner Barnes asked what the data was for each crossing so he had some sense of which of the two alternatives was optimal.

Ms. Obeso said for “Reducing Potential Rail/Vehicle Conflict” and “Improving East-West Connectivity” that the number of conflicts per crossing per day would be very similar as the crossings were very close. She said they knew roughly the number of vehicles that traveled on each road every day. She said regarding East-West Connectivity they have the traffic volumes for each of the four crossings so they knew right now how many vehicles crossed through there every day. She said also they projected the anticipated growth between now and when this project might potentially be constructed. She said if they were to do Alternative A and grade separated at Ravenswood that drivers who currently take Glenwood Avenue might be more likely to use Ravenswood Avenue. She said their traffic model numbers took that into account. She said they did not bring the detailed traffic studies this evening but if that information was wanted they could pull it out and provide.

Commissioner Barnes said an important data point for him to recommend one of the alternatives over the other was the level of effort in relationship to benefit. He said he hoped for more specificity for each item compared.

Commissioner Onken asked about Caltrain and its Joint Power Authority and whether they were on board with the potential project. Ms. Obeso said that Caltrain was a partner and their Joint Powers Board owned the right-of-way and operate the Caltrain itself. Commissioner Onken asked if it were possible that the Caltrain might reject a viaduct if the City decided to built that because it was building high speed trains and the project would go away for another 10 years. Ms. Obeso said if the City was committed and pushing toward a solution that Caltrain wanted grade separations as those improved their operations and facility. She said Caltrain would look more toward the City for funding and staff was already seeking funding. Commissioner Onken asked about electrification and pole installation and how this project would mesh with that. Ms. Obeso said that Caltrain would be a partner throughout the entire process and would review everything regarding the project. Ms. Mercurio said that by the time this project went to construction they were assuming that electrification would have occurred and would have to be considered in the design and the construction costs. She said if they had to do a Shoofly or whatever other construction would mean that the poles and all would have to be relocated temporarily to keep the commuter rails moving forward. Commissioner Onken asked about the Middle Avenue underpass and what would happen with that with either of these two alternatives. Ms. Obeso said Alternatives A and C would not alter the Middle Avenue crossing. She said she understood that the idea was for Middle Avenue to go under the tracks, although that was not yet confirmed. She said they had team members working on both projects. She said it was expected that the Middle Avenue crossing would get constructed first and the Ravenswood project team would watch closely how that moved forward and would accommodate what was done at Middle Avenue. Commissioner Onken said the alignment for Alternative C would be different as it would climb up approaching Middle Avenue. Ms. Obeso said it would provide more clearance and would be better. Commissioner Onken said he expected the stand of Monterey pines in the middle of Ravenswood Avenue west of El Camino Real would have to be removed. He said the Shoofly looked like it went right through the City’s original train station. He asked if as part of the \$400 million for that project alternative if the historic building would be relocated for preservation and then reinstated. Ms. Obeso said they were accounting for protecting the historical building.

Commissioner Kahle said Alternative C had a new signalized intersection at Alma Street. He asked in Alternative A if there was anything that stopped traffic on Alma Street or was it continuous. Ms. Mercurio said at this time they were assuming continuous. She said they would have a signalized intersection at El Camino Real. Commissioner Kahle confirmed that with Alternative A there was no

stopping on Ravenswood Avenue from El Camino Real until Laurel Street. He said the consultant mentioned they were working with the City of Burlingame on grade separation. He asked if grade separation was done city by city and how Menlo Park compared, and whether it became a hodgepodge of up and down along the tracks. Ms. Obeso said both in Burlingame and Menlo Park projects were moving forward because the community wanted to see improvement. She said Caltrain was a partner for every project along their corridor and looked at all the different projects including the up and down of the rail and making sure it met all the constraints of their rail operation. Commissioner Kahle asked if the heights of the Holly Street and Ralston Avenue train rail were higher or lower than what was proposed in Alternative C. Ms. Mercurio said she thought San Carlos and Holly Street would be very similar to Alternative C, which would have a partial excavation and a partial raise. Commissioner Kahle said the photo Commissioner Riggs showed looked much higher than 10 feet. Commissioner Riggs said the photo was taken from the parking lot of Pep Boys in San Carlos. Ms. Obeso said they could look at that element but Ms. Mercurio and she both agreed that it looked like it was 15-feet high at that location rather than 10-foot high at Holly Street.

Commissioner Goodhue said relating back to Commissioner Barnes' comments that she went to the website and looked at the videos and animations to get a better sense of the schematics. She said she feared that when people looked at those that they would not have a full appreciation of what the wall would look like especially for Alternative C given the height. She said Alternative A was much less of a fortress dividing the two parts of the City but there was a fortress effect because Alma Street was cut off. She said she thought it would be very useful as raised in the public comments for people to have some visuals of what it would look like coming from the west side of the tracks going to the Library. She said under Alternative A one could no longer make a right on Alma Street. She said about someone going to the Arrillaga Sports Complex with Alternative A as there was no right onto Alma Street from Ravenswood Avenue. She said she was very curious what the experience would be with Alternative A whether driving, walking or riding a bicycle. She asked with either Alternative A or C whether bicyclists would travel with vehicular traffic or on pedestrian walks. She said the underpass at Embarcadero at Town and County was constrained with a bridge and bicyclists travel with pedestrians.

Ms. Obeso said in all the locations and alternatives they made sure to accommodate pedestrians and bicyclists. She said this was at a conceptual stage but Alternative A would have shoulders so bicyclists comfortable traveling with vehicles would have room to ride there. She said that the sidewalk area would be wider and at a more moderate grade to meet ADA requirements which would allow bicyclists not comfortable riding with vehicles to travel on that path.

Commissioner Goodhue said that her anecdotal experience of waiting for the train at any of these crossings except Ravenswood Avenue was basically like waiting for a traffic light to change. She asked what the wait time was. Ms. Obeso said it was in the studies done by Caltrain which she did not have this evening. She said they could look that up and provide that information. Commissioner Goodhue said she really wanted to know what they were getting in exchange for a visual barrier throughout the City and whether that was worth doing more grade crossings. She asked what city along the Caltrain corridor was most analogous to the City's current crossing conditions or what city had addressed such analogous conditions.

Ms. Mercurio said San Mateo has four existing grade separations all in a row that would completely be replaced and those were an elevated track through residential for four crossings, which needed to have greater clearance.

Commissioner Riggs said he appreciated the comments made by Commissioners Barnes and Goodhue. He said he shared concerns that people were looking at stations and specific underpasses and not looking at the considerable length in between those potential underpasses. He asked staff to put the image of the San Carlos berm on the screen. He said he took the photo to assess whether a berm running through a town created a separation. He said Ravenswood Avenue was part of the Civic Center and there were activities on all four sides of the crossing. He said to lower Ravenswood even with Alternative C was too close to the 5th Avenue underpass to be part of the City's fairly close knit downtown. He said Encinal Avenue was a key way to get between El Camino Real and Middlefield Road, to get between West Menlo and Atherton, from Lorelei to downtown, to get to Encinal School and a traffic light at Middlefield Road. He said Alma Street in the images looked more like Old County Road in Redwood City, San Carlos and Belmont. He said while Alternative C was better than Alternative A it was still a wall. He said they had been told the slopes would not calculate for a viaduct between Encinal Avenue and Atherton Creek but last week he saw calculations by Adrian Brandt, who looked at the Caltrain grades and their requirements, the starting point and the ending point, and found that a via duct could actually be done and was worth a second look. He said he heard comments that the construction of a viaduct took longer to do and was more expensive than Alternative A or C. He said that surprised him as Bart chose to do a viaduct in Oakland that goes through business and residential areas. He said he did not see how doing a viaduct could take longer and cost more than construction requiring a Shoofly and buying right-of-way. He said the viaduct option deserved more attention. He said 10 years ago Palo Alto, Menlo Park, and Atherton held a three day workshop to ask citizens without the issue of cost and construction questions what was the preferred Caltrain grade crossing - partially at grade or above, partially below in a trench, down in a tunnel or up in a viaduct. He said the majority were strongly in favor of a tunnel. He noted that the two alternatives given to the Commission to look at for recommendation of a preference to the City Council was as mentioned by Commissioner Goodhue earlier a false choice. He said 80% of the emails to the City Council asked that the City look at other alternatives and it was the same with the emails received by the Commission. He said he thought the Commission's message to Council was that choices had been narrowed to two alternatives and were not what was needed, and that the City should look at doing a tunnel or a viaduct built above the existing Caltrain track that would not create a wall in between. He said Menlo Park should decide what its two best choices were and push forward with those and not start with what Caltrain or what Atherton preferred. He asked if the Chair would entertain a motion. Chair Combs said not at this time as he received more requests for people to provide public comment. He said although he had closed public comment he wanted to make sure that everyone who came to speak on this matter had a chance to speak.

Chair Combs reopened public comment.

- Stephen Kerman said in the comparison of Alternatives A and C there was an item for noise mitigation related to the sounds of the gates, bells and horns, which either of the two alternatives would reduce by eliminating grade crossings. He said another item should be considered as he believed elevating the train would cause the sound of the wheels on the rails to carry much further, and the train would be much more audible and at much more distances even after electrification. He said that should be an item in the comparison.
- John Kadwany, Menlo Park, said he supported the direction the Commission comments were leading. He said he had not spoken earlier as the survey results indicated that 85% of people supported Alternative C which intimidated him. He said he supported the critical comments on

the two choices being made available. He said there was not a sufficient appreciation by others and everyone what the wall would look like for those choices. He said words were not sufficient and pictures were needed to understand the bicycle and pedestrian routes and all of the elements of the live-experience. He said the data quality provided was nonexistent and alternative choices had too much subjectivity and policy judgment baked into too much superficiality based on color coded answers for what would likely be a half a billion dollar project. He supported Commissioner Riggs' idea to expand the decision space of alternatives and consider those that affected the desired outcomes. He said there would be a trade off in that some things in terms of traffic would be improved. He said that it was not possible to build the way out of many traffic problems. He said he was not saying not to do a project but recommending looking at how much they were willing to forego of how the City looked and felt versus providing better traffic for 15% of what were local drivers and 85% of regional drivers. He said he could not support the big divide of the alternatives presented unless the divisiveness was removed perhaps through use of a viaduct. He said environmentally if there was a coherent ecological niche chopping it in half was one of the worst things that could be done.

- William Pflaum, Menlo Park, asked if there was coordination with Palo Alto on the crossing on Alma Street that comes out of Palo Alto, what the elevation of that might be and if it would require changes in the two alternatives presented.
- Wendy Shindler, Menlo Park, said she had to concur that they did not have all the choices they needed and she was worried about the two choices being offered. She said she would like to see more data about going underground and why it would not work. She asked what the cost of leaving Encinal Avenue out of the choices in time, safety and money was. She said she would like the aqueduct option looked at. She thought that decisions had been made already that were not supportable and she could not even picture what these would look like leaving Encinal Avenue out of the project.

Chair Combs closed the public comment noting for the second time. He clarified that public comment had been moved before extensive Commission questions and comments to allow people to speak earlier. He said what comments the Commission were making stimulated more persons to speak. He said in the future he might have two public comment periods – one for persons needing to leave early and a second near the regular position in the meeting for persons who were inspired by Commission questions and discussion.

Commission Comment: Chair Combs asked staff to respond to Mr. Pflaum's question about coordination with Palo Alto and possible changes to the train elevation at Alma Street there and if the elevation from Palo Alto into Menlo Park was considered in these proposals.

Ms. Obeso said throughout the project staff had met with Palo Alto and Atherton staff both to coordinate on rail corridor issues but also specifically on this project. She said last week she met with the Atherton Public Works Director and next week she would meet with staff at Palo Alto. She said her understanding of the project that Palo Alto was looking at right now was a study that was they were fairly early into, and that they were focused on their southern crossings. She said they had no information that Palo Alto was planning or considering making any changes to the northern end of Palo Alto crossings, which was where its elevation could influence Menlo Park's elevation. She said the City's study was assuming that the creek and the city limits, at Palo Alto's crossings, and downtown station were to remain as existing.

Commissioner Onken asked Principal Planner Chow if CEQA would be done on this project. Principal Planner Chow said Ms. Obeso had information on the next steps but a project moving forward would be subject to CEQA. Ms. Mercurio said it would be subject to CEQA and with any federal funds it would also be subject to National Environmental Protection Agency (NEPA). Commissioner Onken said that impacts being referred to from the alternatives would be reviewed through the environmental review process. He said there had been some confusion that was clarified today that Oak Grove and Glenwood Avenues would not be closed off under Alternative A. He said what was the situation with Encinal Avenue under Alternative C and if it would be abandoned. Ms. Obeso said the Encinal Avenue crossing under both Alternatives A and C as presented now would remain as existing.

Commissioner Riggs asked if it was possible to have at grade crossing for high speed rail. Ms. Obeso said that was a question for high speed rail and she did not have an update as to where they were.

Nikki Nagaya, Assistant Public Works Director, said what they have learned from high speed rail was that the operating speeds they were proposing were allowable at at-grade tracks. She said they could confirm that information. She said with two railroad lines there was an outstanding question on whether a third track would necessitate grade separation. She said there was not a full answer yet from the California Public Utilities Commission. She said earlier this summer high speed rail was moving forward with alternatives that would not entail a third track in Menlo Park and were only looking at two track alternatives through Menlo Park.

Commissioner Kahle asked if the reason Encinal Avenue was not included in the alternatives was because it was too close to Atherton. Ms. Obeso said some comments received were a desire for grade separation there and other comments received indicated a preference for no grade separation there. She said they looked at the traffic volumes for that crossing and it was by far the lowest volume of all of the City's existing crossings. She said the need being less there, staff was directed to move forward on Alternative C with the other three crossings.

Commissioner Barnes asked about bicycle access under Alternative A from Alma Street through Ravenswood Avenue intersection and traveling to downtown Menlo Avenue. Ms. Obeso showed a visual slide of Alternative A showing that Alma Street was a bridge at the same elevation as existing and the same as the train with Ravenswood Avenue going underneath. She said traveling from Willow Road there would be a bicycle and pedestrian connection to get between Alma Street and Ravenswood Avenue but there would not be a vehicle connection. She said that they did not have the specifics yet but there would be some sort of ramp system for bicyclists/pedestrians to get from either the Ravenswood Avenue elevation or Alma Street elevation to the train station. She said that same access would likely be the way for bicyclists/pedestrians to cross El Camino Real to the downtown. Commissioner Barnes asked where the last place on Alma Street would be to turn off at grade into Burgess Park or at what point Alma Street started to rise. Commissioner Riggs asked to see what was determined to be slide 36. Ms. Obeso said Alma Street would be at the same elevation it was today under Alternative A for Burgess Park, the parking lots and Library and other buildings off Alma Street. She said the ramp shown would get one from the Library (on bike/foot) down to Ravenswood Avenue. Ms. Nagaya said if a person was bicycling down Alma Street and trying to get downtown that they could use the wonderful new bicycle lanes on Oak Grove Avenue.

Commissioner Barnes said what he was hearing from the community was if the intersection of Alma Street and Ravenswood Avenue was differentiated at different grades that it would be difficult to navigate. He said it sounded like from a bicyclist perspective that he could use Oak Grove Avenue or the access at Alma Street and Ravenswood Avenue to get downtown under Alternative A. Ms. Obeso said it was critical to maintain good bicycle/pedestrian connection at that intersection as it was used by so many bicyclists and pedestrians now. Commissioner Barnes confirmed that if he was driving on Alma Street that the first opportunity to go left and go downtown would be at Oak Grove Avenue. He also confirmed if he was driving from downtown along Menlo Avenue to Ravenswood Avenue that he would not be able to make a right on Alma Street anymore and would have to make a right at Laurel Street. He asked about Laurel Street's capacity to handle increased traffic volume. Ms. Obeso said they looked at that several years ago. She said when they cut off the right turn access into and out of the Library area that traffic at the intersection at Laurel Street and Ravenswood Avenue greatly expanded. She said based on that and from the traffic model done there was a need for more capacity at that intersection which was why they were recommending a mitigation should Alternative A move forward to have a right-turn pocket to handle extra queuing. Commissioner Barnes said also he thought many drivers were trying to get to Willow Road as fast as they could to get to Hwy. 101 or the Dumbarton Bridge. He said the right-turn pocket would speed up traffic getting onto Laurel Street but volume on Laurel Street would increase. He asked about the increase in volume with that on Laurel Street.

Ms. Mercurio said if Alternative A was done without the Laurel Street right-turn pocket that there would be an F level of service or stand still traffic. She said with the pocket it would be D level of service but traffic would be moving. She said the delays for travel time in the peak p.m. hours would go from 510 seconds to 285 seconds so delays were cut in half during peak p.m. traffic with the addition of the right-turn pocket.

Commissioner Barnes said in terms of the appropriateness of Laurel Street to handle that increased volume it appeared there would be delays. He asked if there had been discussion as to whether Laurel Street was an appropriate road to handle that increased traffic volume. Ms. Obeso said that Laurel Street was part of the traffic model they looked at but they did not focus on it for this presentation as it did not have any issues when they ran it with the new configuration of Alternative A. She said they expected Laurel Street to handle the volume and it was getting the traffic through the Ravenswood Avenue and Laurel Street intersection that had challenges.

Commissioner Barnes asked about electrification and schedule frequencies in terms of Menlo Park only doing the Ravenswood Avenue grade separation or all the crossings. He asked if electrification would impact timing or speed or anything else. Ms. Obeso said she could not speak for Caltrain but it would give them the ability to have more reliable service. She said it was in its best interest because this helped with operations by reducing the potential for collisions, which could cause backups in their whole system. Commissioner Barnes said it had been indicated that Caltrain would prefer that as many cities as possible do grade separations and confirmed that mitigating accidents was of great interest to Caltrain. He asked about funding for the project. Ms. Obeso said for this study the City had a SMCTA grant. She said when there was a preferred alternative the City would be in a better position to compete for funds. She said there were county, federal and other organization and agency funds available. She said they could get on lists but not compete without a preferred alternative. She noted for Commissioner Barnes that she expected funding to be from a variety of sources and she said they did not have a feel as to a preference for large or small grants. Ms. Nagaya said that the City would be willing to seek out every possible funding source. She said San Bruno used Measure A funds from the County for their work that was

recently completed and funds from the Rehabilitation Act and stimulus packages from 2008 and 2009 as they had a project fully designed and ready to begin construction. She said San Mateo also had a significant amount of County Measure A funds and High Speed Rail Authority funds to construct their next grade separation project.

Commissioner Onken said that 85% of the community voiced support of one alternative was important for the Council to hear but he did not think that specifically should guide what the Commission's recommendation to Council was. He said discussions about grade separations and how to manage a busy train line through the City had been ongoing for a long time. He said several years ago there was a very tragic accident at the Ravenswood Avenue crossing raising the question of what to do at crossing to an almost emergency level. He said that heightened the sense of urgency was less now, and he thought the question could be approached very pragmatically. He suggested looking at what the purposes of grade separations were, what would really happen to the businesses located along the rail corridor, whether the City really wanted four years of disruption, and what the gain of the Shoofly would be as it would create a very difficult situation for some time. He said another option was to do nothing. He said he could see lots of benefits for the undercrossing at Ravenswood Avenue, noting the increased speed was desirable, Alma Street going straight through was desirable, and that he biked on Alma Street each day so he would be partial to that. He said he thought it would be much like the Jefferson Avenue undercrossing in Redwood City. He said he thought it was a manageable project that he thought would work. He said there was concern about losing the center of the City. He said there were many centers in the City and one was being created at Oak Grove Avenue with the Station 1300 project. He said there were good things for the City to hold onto rather than to wipe them out with a huge berm. He said the impact of disconnecting Alma Street from Ravenswood Street would really reduce the cut through traffic that everyone in the Willows complained about. He said he thought there were a number of small things that were a huge benefit from doing the Ravenswood Avenue grade separation. He said he could not see the benefits of the other crossings and further chopping up the City. He said he supported Alternative A over C if that were the City's only choices.

Commissioner Kahle suggested remembering the Middle Avenue underpass related back to Commissioner Barnes hypothetical bike ride to Amici's and downtown from Alma Street. He said he was surprised at 85% support of Alternative C and wondered if that was due to a misconception that Alternative A would close Oak Grove and Glenwood Avenues. He said he went to the community meeting on June 7 and found that helpful. He said it seemed that the path was already down to Alternatives A and C. He said he was surprised there was still talk about trenching and tunneling and elevating paths. He said if those were even choices he thought the trench or the tunnel option would be difficult but much preferable over an elevated track due to additional noise from that and its visual blight. He said he was leaning strongly toward Alternative A. He said he was deeply concerned about the wall or the berm. He said what was shown in the photo provided by Commissioner Riggs was probably taller than what it would be for Menlo Park but once power poles were added for electrification and lights it would look fairly tall. He said his major concern was that Alternative C would divide the City. He said two additional benefits of Alternative A was a strong bicycle connection on the north-south direction and a year less of construction. He said he had the sense the Commission was leaning toward Alternative A as its recommendation and he fully supported.

Chair Combs said one of his concerns was that they had these two options because for every other option ever considered a decision had been made not to pursue those. He said if they

recommended that they did not like either of the options presented that could reexamination of other options and a never ending cycle of discussion. He said that was not to say that the alternatives presented were desirable in terms of cost benefits analysis. He said with Alternative A there would be lots of concrete and dead space. He said other Commissioners had pointed out that the core area of the City would become desolate looking or freeway looking. He said he did not know if the benefit was worth the cost both related to funding and the cost to the residents' psyche creating a split. He said that he was skeptical whether this was something imminently needed and worth spending substantial amounts of money on for benefits that did not outweigh the costs. He said as promised he would return to Commissioner Riggs who started to make a motion before the public comment period was reopened.

Commissioner Riggs said they heard from public speakers tonight and through emails, and through Commissioners Barnes and Goodhue's comments earlier in the meeting concerns about the two alternatives. He said he did not completely buy into Lee Duboc's description that the choice between Alternatives A and C was bad and terrible but he thought both alternatives were bad. He said to advance that much money and disruption for a couple of bad alternatives did not feel right to him. He said his motion was that Council be requested to consider another option, primarily a viaduct, and address the engineering with a little more skeptical eye with the help perhaps of Adrian Brandt's observations. He said the recommendation would be to look at what most would benefit Menlo Park and not look at what would fit into Caltrain's engineers' preferences as to slope at Atherton. He said the City should also keep the tunnel as an option. He said the latter's huge downside was its expense but it would have hardly any other downsides. He said he wanted to keep the tunnel for rail on the table. He said if they did a viaduct it would be a compromise for the sake of money. He said that his motion was for the options of a tunnel or viaduct to be the preferred alternatives and not Alternatives A and C.

Chair Combs said the motion on the table was to recommend to City Council that they prioritize a viaduct option but keep a tunnel option on the table related to the Caltrain Union Pacific right of way through the City.

Commissioner Onken said one of the speakers asked the question about quiet zones in Atherton and why the City did not have one. He said his experience living near the tracks in Atherton was that they wanted a quiet zone but it was not enforceable as the train engineers were liable if they were to hit a car or object and had not blown their horns. He said it was outside Atherton's jurisdiction to have a quiet zone and train engineers blow the horns just as much at Atherton crossings as in Menlo Park. He said Alternative C would remove horn noise from three crossings.

Commissioner Kahle asked if the motion was to support a viaduct. Chair Combs said the motion was to support an option that was not one of the options presented to the Commission but which was within the Commission's purview to recommend as an Alternative "X." Commissioner Kahle said he would not be able to support a viaduct as an option.

Chair Combs noted that the motion died for lack of a second.

Commissioner Riggs asked whether Ms. Obeso had said related to Alternative A that Alma Street would remain at grade and a driver could turn into the Library parking lot from it. He said Alma Street ran along the tracks. He said he recalled that Ravenswood Avenue would only be lowered 10 feet which meant that Alma Street would have an eight foot clearance underneath so a five to 10% rise on Alma Street would have to begin somewhere before the Library parking lot, and

probably as far back as the skate park. He said Alma Street was going to be a challenge as an uphill ride for bicyclists and to construct. Ms. Obeso said with Alternative A that Alma Street and the train tracks did not change from their existing elevations. She said Ravenswood Avenue would go completely under the train tracks and excavated at approximately 22 feet.

Commissioner Kahle moved to request that the City Council remain open to other options but the recommendation of the Planning Commission was to support Alternative A primarily because it avoided a barrier or wall between the east and west sides of the City. Chair Combs said the motion was to recommend Alternative A. He said he understood the desire for other options but the Commission had only been presented with Alternatives A and C. Commissioner Kahle said there was a lot of support for other options and he did not know if those were realistic but he wanted to at least have it as part of the Commission's action. Chair Combs said the motion was a recommendation for Alternative A with recognition that there were supporters of other options not presented and those could be viable.

Commissioner Barnes said they had discussion tonight as it related to lack of clarity on what a viaduct would cost or why it was not feasible. He asked if Commissioner Kahle would consider attaching to his motion a request for further clarification for additional materials to provide analysis of the options that did not make it to the preferred stage whether a viaduct or overhead and why not, and related to Alternative C to provide simulations of what that berm would be like experientially in the City as that information had not really been provided. Commissioner Kahle accepted Commissioner Barnes' added request and recommendation.

Chair Combs said the motion was to recommend Alternative A. He asked if Commissioner Barnes was offering an addendum and a second or just an addendum. Commissioner Barnes said he was offering a second with an addendum. Chair Combs said there was a first and second with an addendum acceptable to the maker of the first that additional information be provided with reasoning, any logic, and cost estimates around alternatives that were not further explored, and secondly some better idea of what the actual impact of the berm in Alternative C would be to the City.

Commissioner Riggs asked if they expected this information to come back to the Commission. Chair Combs said the expectation was it to be provided to the Council and would be provided to the public.

Commissioner Onken said he did not want to dilute or confuse but perhaps they could just make recommendations and not vote up or down. Chair Combs said at this point there was a motion and second. He said Commissioner Onken had a fair point but his understanding was staff would like to see a full vote of the Commission on what its recommendation was on one of the presented alternatives.

Commissioner Goodhue asked if staff could repeat back what the proposed motion was at this point.

Principal Planner Chow said the motion was to recommend to City Council to support Alternative A primarily to avoid an east-west barrier or berm but to be open to consider other options with additional analysis to identify potential cost implications for those other options such as the viaduct and to provide photo simulations of what the berm experience would be at grade level.

Chair Comb called for the vote.

ACTION: Motion and second (Kahle/Barnes) to recommend to City Council to support Alternative A primarily to avoid an east-west barrier or berm but to be open to consider other options with additional analysis to identify potential cost implications for those other options such as the viaduct and to provide photo simulations of what the berm experience would be at grade level; passes 4-2 with Commissioners Barnes, Goodhue, Kahle and Onken supporting the motion, Commissioners Combs and Riggs opposing the motion and Commissioner Strehl absent.

H. Informational Items

H1. Future Planning Commission Meeting Schedule

Principal Planner Chow said on the agenda there was a future Planning Commission meeting date error showing that the second meeting date in October was October 30 but should be corrected to October 23. She said the agenda was corrected in its posting on the website.

- Regular Meeting: September 25, 2017
- Regular Meeting: October 16, 2017
- Regular Meeting: October 23, 2017

I. Adjournment

Chair Combs adjourned the meeting at 10:18 p.m.

Staff Liaison: Deanna Chow, Principal Planner

Recording Secretary: Brenda Bennett



MEMORANDUM

Date: 10/6/2017
To: Planning Commission
From: Kaitie Meador, Associate Planner
Re: 350 Sharon Park Drive – Determination of Substantial Conformance

For all applications that involve the construction or alteration of structures (e.g., Architectural Control and Use Permit), a standard condition of approval is applied requiring the subsequent development to be in substantial conformance with the approved plans. Staff has come across a case where the proposed modifications warrant notification of the Planning Commission, although staff believes they are in substantial conformance with the original approval. Any Planning Commissioner may request that the item be added to the agenda of the next Planning Commission meeting for further discussion of the item.

Background

On September 12, 2016, the Planning Commission approved an architectural control application at 350 Sharon Park Drive for exterior modifications of eighteen existing apartment buildings, one existing clubhouse and three accessory buildings in the R-3-A-X (Garden Apartment, Conditional Development) zoning district. The proposed exterior modifications included replacing balcony railings, siding, patio screens, modifying the exterior color scheme, new landscaping and improvements to the site amenities. The project also included the removal of 39 heritage trees. The Planning Commission staff report and minutes are available through the links provided below.

Staff report

<http://menlopark.org/DocumentCenter/View/11478>

Minutes

http://menlopark.org/AgendaCenter/ViewFile/Minutes/_09122016-2805

On March 2, 2017, a substantial conformance memorandum was sent to the Planning Commission to review exterior changes to the approved elevations. The primary changes included modifications to the exterior building materials and adjustments to the window and door orientations and sizes. A Planning Commissioner requested to discuss the item at the March 13, 2017 Planning Commission meeting. However, no discussion on the item occurred at the meeting and the revisions were approved. The substantial conformance memo is available through the link provided below.

Substantial conformance memo

<http://www.menlopark.org/DocumentCenter/View/15727>

Proposed Revisions

The applicant is now requesting additional exterior changes to the approved elevations. The applicant is requesting the proposed changes due to recent testing,

which revealed asbestos in the exterior stucco. The proposed modifications are required to minimize the amount of hazardous material abatement on the site. The primary changes are listed below.

Apartment Building Type I - IV

- Stair Towers: The existing sloped roof structures over the stairs would remain where flat roof structures were approved.
- Stair Screens: The stair screens on building type I would extend to the existing roofline. The stair screens on building type II would decrease. The proposed roof screen would extend to the top of the third floor instead of to the roofline.
- Stucco: The existing textured stucco would remain where smooth coat stucco with score lines was approved. The stucco paint colors would remain unchanged from the approved colors.
- Windows Type: The proposed windows would be vinyl replacement windows where the approved windows were new aluminum clad vinyl windows. The trim color would be consistent with the approved color.
- Window Placement: Four windows would be removed at the entries of four interior units on building type IV.

Clubhouse Building

- Siding Material: The horizontal fiber-reinforced siding on the front elevation and under the eave overhang would be changed to cedar siding. The color would be consistent with the approved color.
- Siding Colors: The color of the horizontal siding on the rear elevation would be revised to match the color of the vertical siding.
- Doors: The design of the door on the west elevation would be changed from a glass double swing door to a solid door with a single side lite.
- Roof Guardrail: The guardrail for the bridge that connects the parking garage to the roof would be a cable railing. The cable railing would be consistent with the railings on the apartment buildings.

Accessory Buildings

- Doors: The doors on the front elevation of the accessory buildings would remain roll-up overhead doors with a slight decrease in the overall size of the doors.

The applicant's project description letter and project plans depicting the approved elevations after the first substantial conformance memo and proposed configurations are included as Attachments A and B, respectively.

Staff Review

Staff has determined that the changes to the project plans are in substantial conformance with the Planning Commission's previous architectural control recommendation, based on the following:

- The overall integrity of the architectural design would be maintained with the proposed exterior changes.

- The proposed changes would be compatible with the approved contemporary architectural style, and the exterior finishes would be consistent throughout the buildings on the complex.
- Architectural interest would be maintained on the building facades by the updated color scheme, new windows and doors, and updated exterior building materials.
- The project modifications meet all development regulations in the R-3-A-X zoning district and Conditional Development Permit.

Planning Commission Review

If any member of the Commission would like to discuss the changes to the plans described above at the October 16, 2017 Planning Commission meeting, please notify staff no later than **5:00 p.m. on Tuesday, October 10, 2017**. If staff does not receive a request from a Planning Commissioner, there will be no further review and the City will proceed with processing the described modifications as part of the building permit application. If any member of the Commission makes such a request, the item would be placed on the October 16, 2017 agenda as a regular business item to give the full Commission the opportunity to determine whether or not the changes meet the intent of the original approval. No additional materials beyond what is contained in this memorandum would be prepared for the agenda item.

If you have questions about the project or process, please contact Kaitie Meador at kmmeador@menlopark.org.

Attachments

- A. Project Description Letter
- B. Revised Plans (includes the approved and proposed plans)

Residential Buildings:

Stucco Demolition

Recent testing confirmed asbestos in the exterior stucco, so in an effort to minimize the potential impact to residents and expedite the renovation, the amount of stucco removal was reduced. As a result, the following scope of work was reduced to minimize hazardous material abatement procedures on site:

- **Stair slats to extend to existing roofline (Building type 1)** - the project scope was revised to leave all stair tower roofs as existing
- **Existing stair tower roof to remain (Building Type 2)** - the project scope was revised to leave all stair tower roofs as existing
- **Stair slats revised to extend to the top of the 3rd floor, and no longer extend into the attic (Building type 2)** - the project scope was revised to open only the stairwell floors of the building type 2 stair towers and no longer demolish and add slats into the existing attic space.
- **Stucco score lines removed** – To avoid cutting into hazardous material, made friable from saw cutting score joints into the stucco, we will maintain the existing joints.
- **Existing Stucco to remain and be painted** – To get proper adhesion to apply smooth stucco skim coat, the existing surface would require sandblasting, exposing the potential hazardous material. As such, the scope has been reduced to prepare and paint the existing stucco to match the proposed color scheme and design.
- **Window specification revised to vinyl retrofit** – The proposed window required demolition of stucco to 18” around the existing window opening. To minimize the potential exposure, the window specification is revised to a vinyl retrofit in lieu of the original nail on aluminum. The look of the proposed window will appear similar to the proposed window.

Building Openings

Recent revisions to the unit layout effected the following openings:

- **Window to be removed as a result of new floorplan layout. Door to remain (Building type 4)** - As a result of a floorplan revision to the 3x2 units, we have removed a window near the entryway of the building type 4 interior units.

Clubhouse:

Material/Specifications

The following material changes and specification revisions have been made to the clubhouse:

- **Entry door revised** - Revised for a grander entry
- **Bridge railing revised to cable rail** - Bridge from parking garage to mechanical roof revised to match the balcony railing
- **Drinking fountains added** - Per plan check comments, we have added water fountains to the exterior of the clubhouse.
- **Material at wing wall changed from fiber reinforced siding to cedar** - We have revised the material on the exterior wing wall to match the interior ceiling of the clubhouse for consistency
- **Siding on backside of clubhouse to match vertical siding** - The horizontal siding on the mechanical room fence and back side of the clubhouse is to be the dark fiber reinforced siding so that the mechanical fence “disappears” as it will match the main building siding
- **Siding width change on back of clubhouse** - The width of the vertical siding that wraps the back of the clubhouse, to the stucco side has been reduced
- **Roof walkway matt added to roof for equipment access** - TPO walkway mat has been added to the roof for access from the bridge to the equipment room

Pavilion:

Material/Specifications

The following material changes and specification revisions have been made to the Pavilion buildings:

Door specification changed on the pavilion buildings - The door we have specified is slightly smaller than the original shown

Community Development: Substantial Conformance Memo

09/11/2017



Sharon Green Apartments
350 Sharon Park Drive
Menlo Park, CA

Table of Contents

Building Type I

1. West Elevation
2. North Elevation
3. East Elevation
4. South Elevation

Building Type II

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7. East Elevation
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9. Section

Building Type III

10. West Elevation
11. North Elevation
12. East Elevation
13. South Elevation

Building Type IV

14. West Elevation
15. North Elevation
16. East Elevation
17. South Elevation

Clubhouse

18. West Elevation
19. North Elevation
20. East Elevation
21. South Elevation
22. Roof Plan

Pavilions

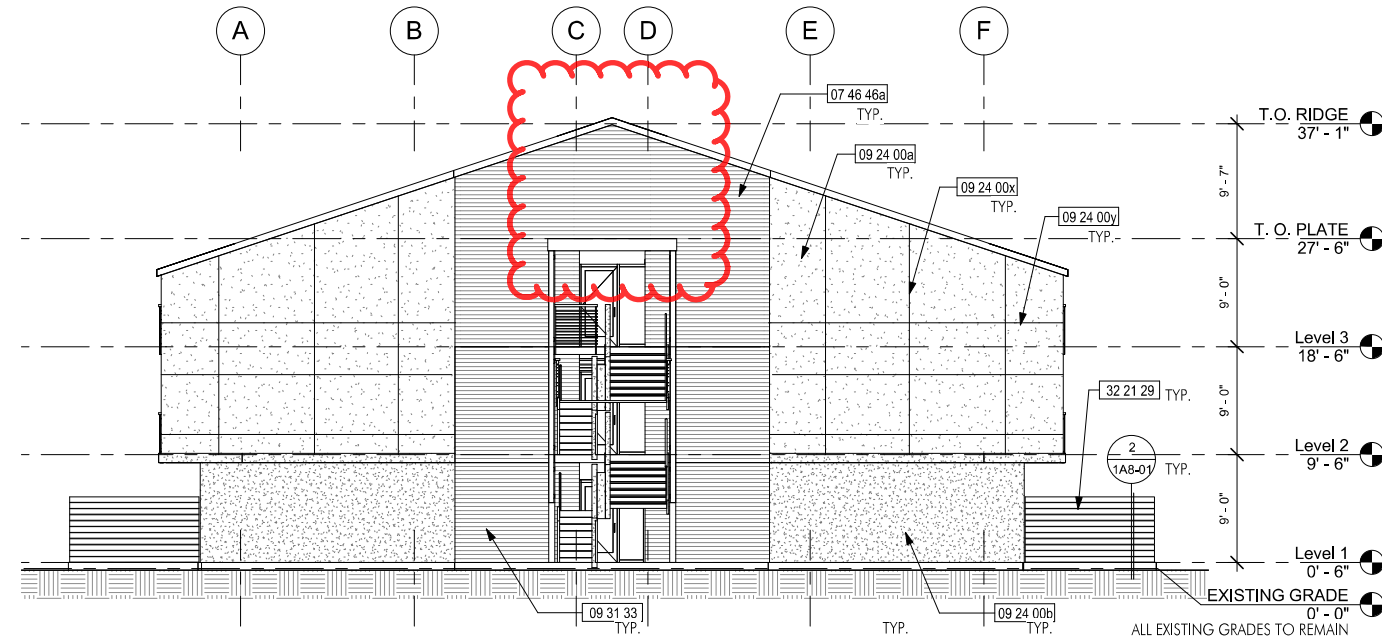
23. Pavilion 1
24. Pavilion 2
25. Pavilion 3

Planning Submittal: Materials Board

New Colors & Materials - Apartments

New Colors & Materials - Clubhouse



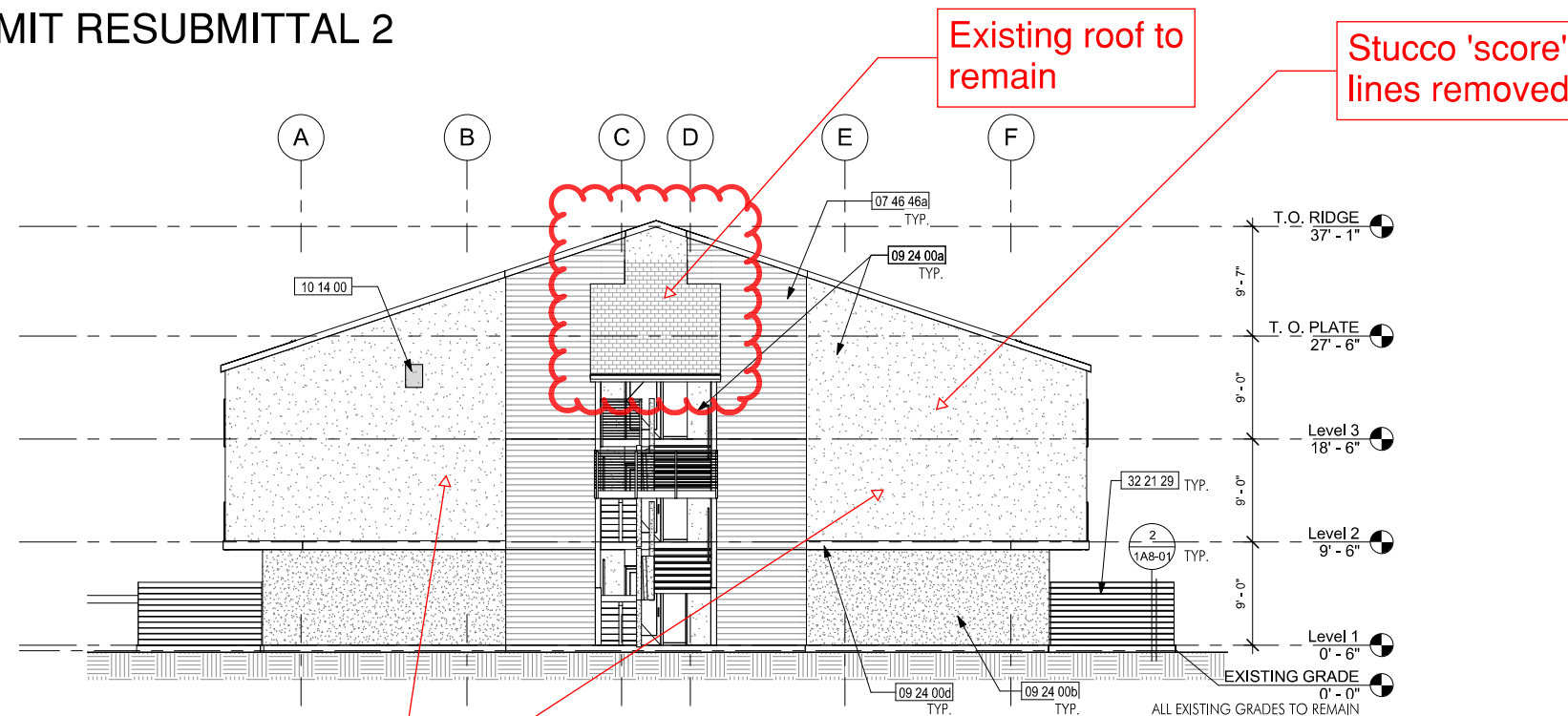


WEST ELEVATION
1/8" = 1'-0"

KEY NOTES

02 01 80b	(E) MECH. WELL TO REMAIN
05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00	EXTERIOR WOOD FASCIA, PAINTED
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4"
08 32 13a	ALUM. CLAD VINYL SLIDING DOOR - GIENOW DESIGN SERIES
08 51 13	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
08 91 00	STAIR SCREEN WALL - RESYSTA SLATS #FHC006035144, OVER STEEL SUBSTRUCTURE
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00b	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00d	PORTLAND CEMENT PLASTER SKIM COAT OVER (N) TRIM, OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
09 31 33	STONE TILING. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.

PERMIT RESUBMITTAL 2

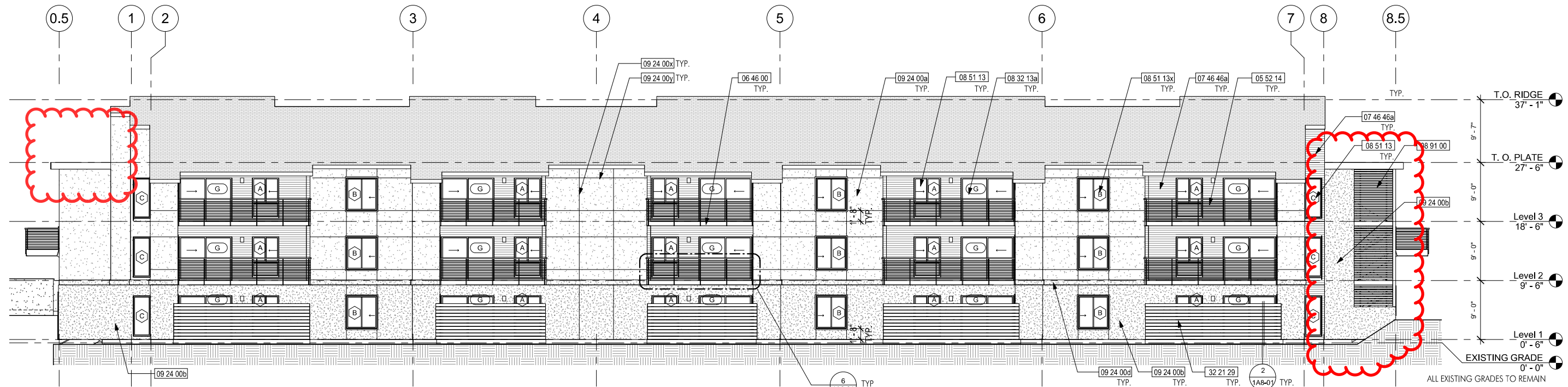


WEST ELEVATION
1/8" = 1'-0"

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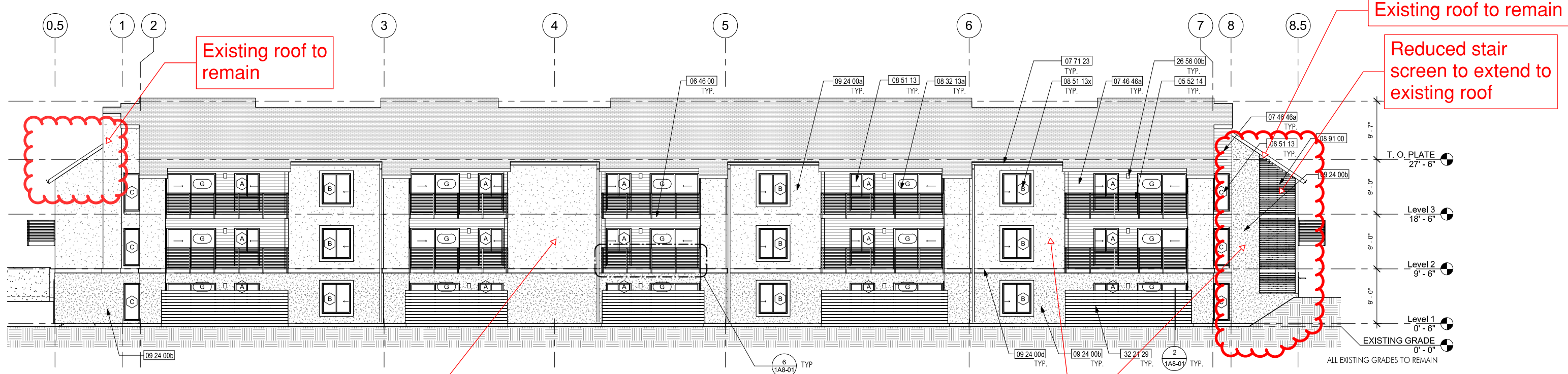
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05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
08 91 00	STAIR SCREEN WALL - RESYSTA SLATS #FHC006035144 COLOR: C64, OVER STEEL SUBSTRUCTURE
09 24 00a	PAINT (E) STUCCO - COLOR 1
09 24 00b	PAINT (E) STUCCO - COLOR 2
09 24 00d	(N) WOOD TRIM PTD W/ CONT ALUM FLASHING, OVER (E) STUCCO
10 14 00	BLDG ADDRESS SIGNAGE
26 56 00b	EXTERIOR LIGHTING FIXTURE, LOCATED BETWEEN DOOR & WINDOW - SEE ELECTRICAL DRAWINGS
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS, STAIN FINISH.





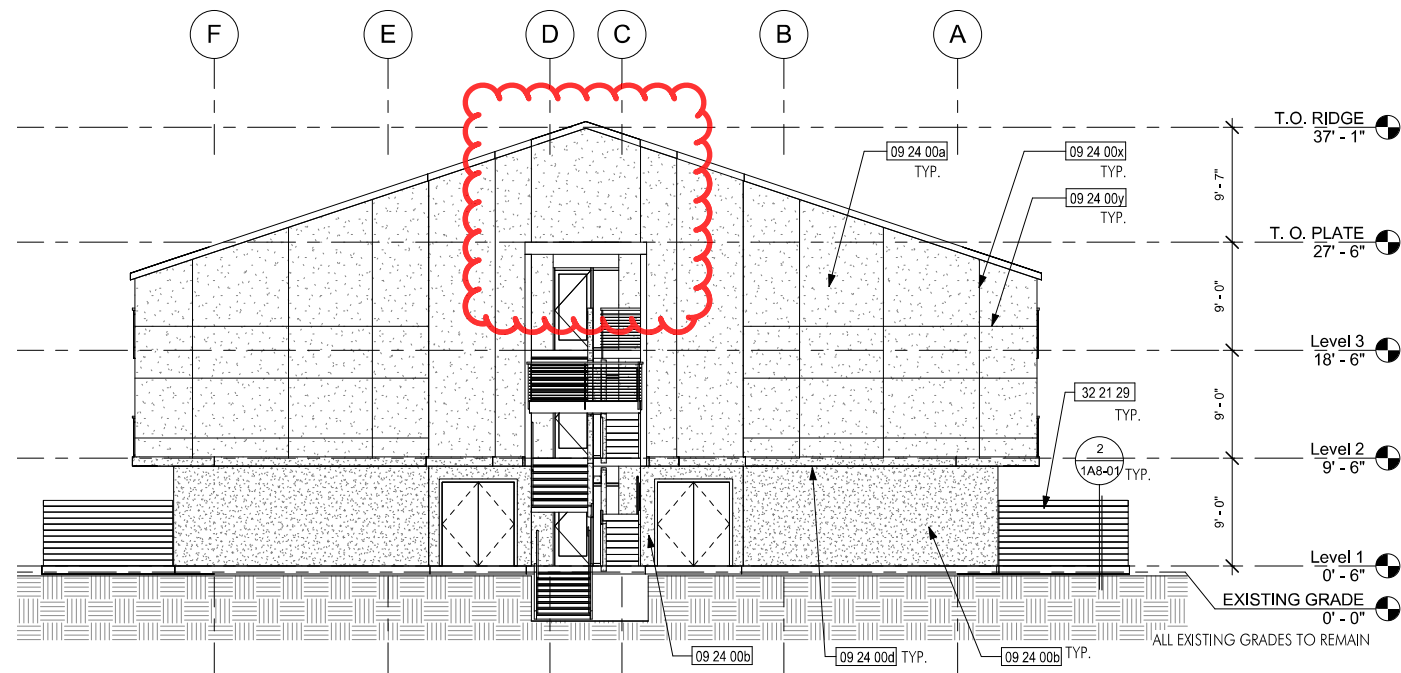
NORTH ELEVATION
1/8" = 1'-0"

PERMIT RESUBMITTAL 2



NORTH ELEVATION
1/8" = 1'-0"





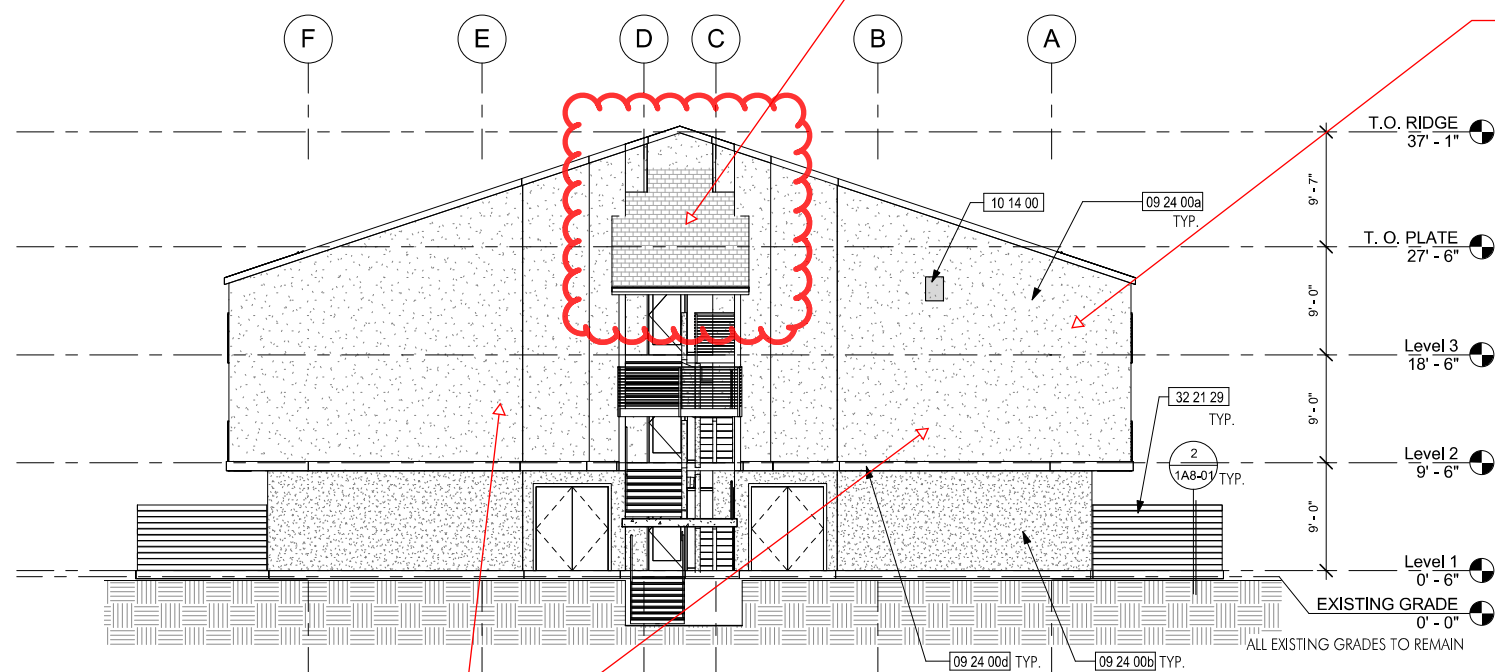
EAST ELEVATION

1/8" = 1'-0"

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PERMIT RESUBMITTAL 2



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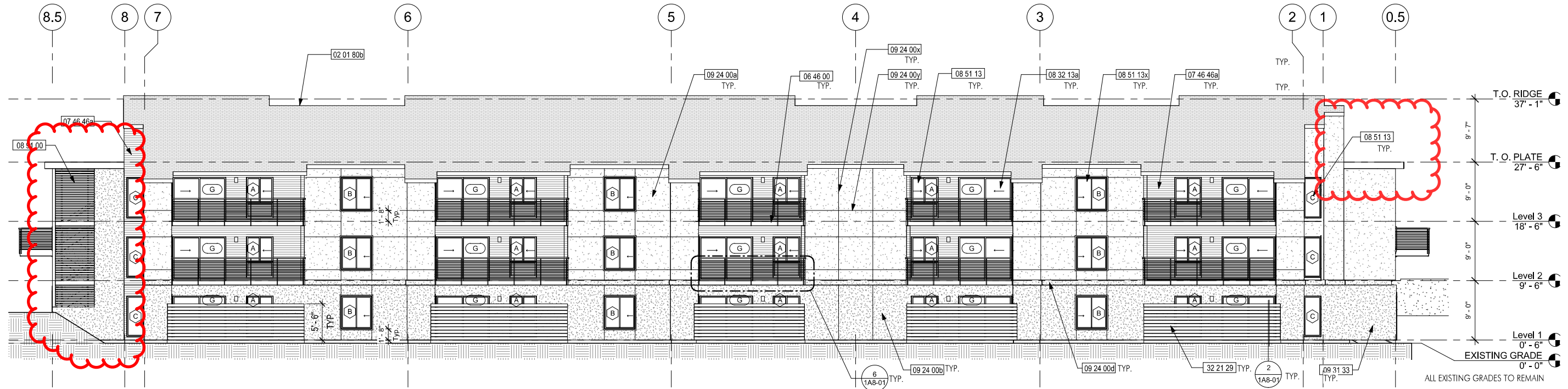
Existing stucco to remain and receive new paint

Existing roof to remain

Stucco 'score' lines removed

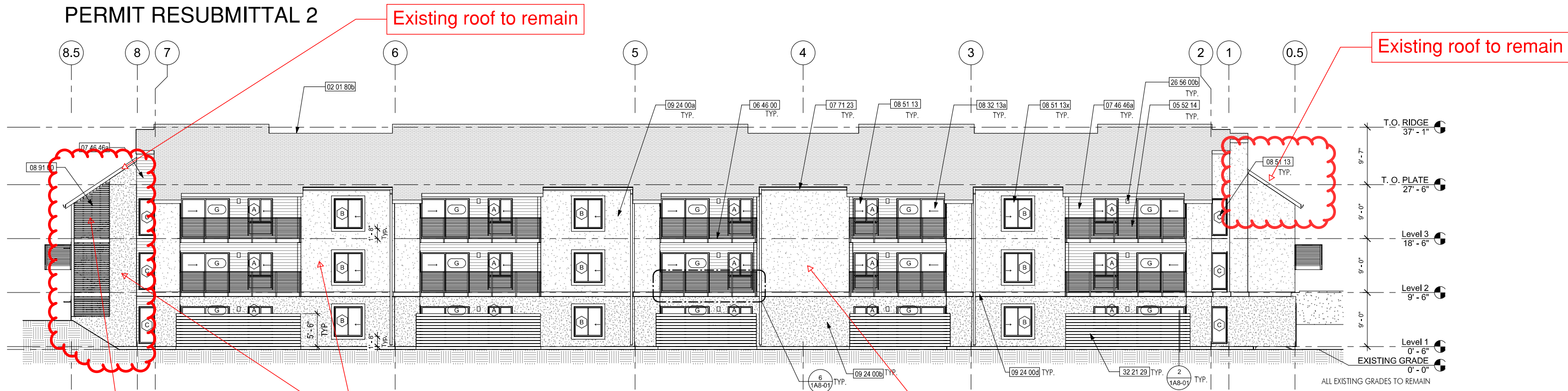
Window & door product is changed





SOUTH ELEVATION
1/8" = 1'-0"

PERMIT RESUBMITTAL 2



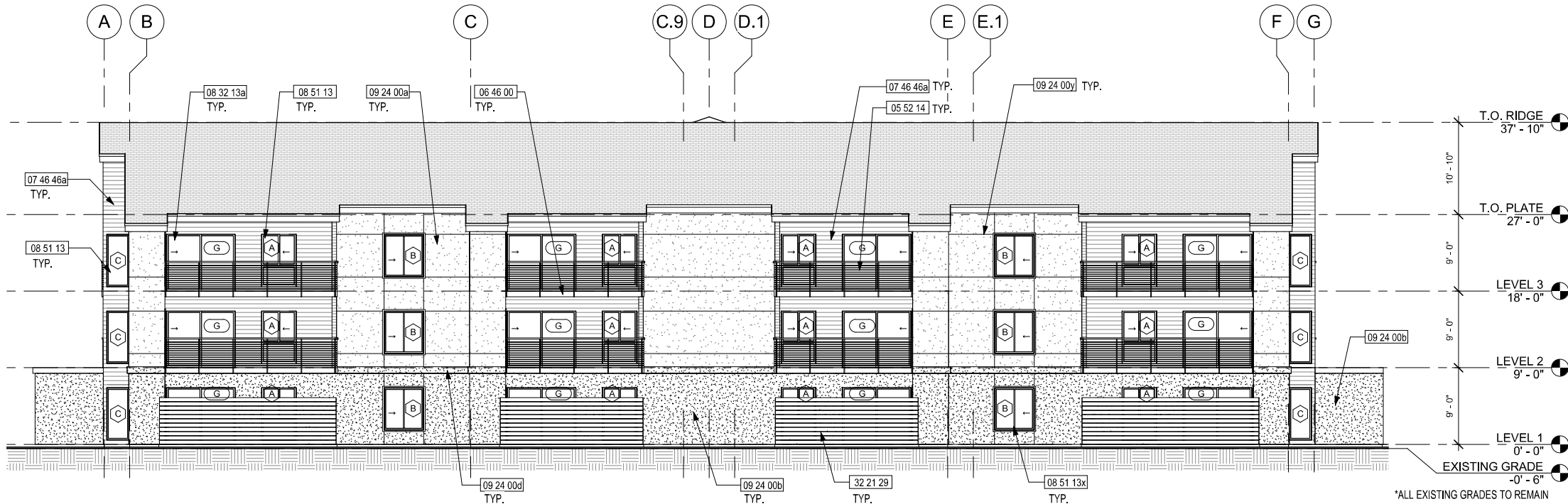
SOUTH ELEVATION
1/8" = 1'-0"

Reduced stair screen to extend to existing roof

Existing stucco to remain and receive new paint

Stucco 'score' lines removed



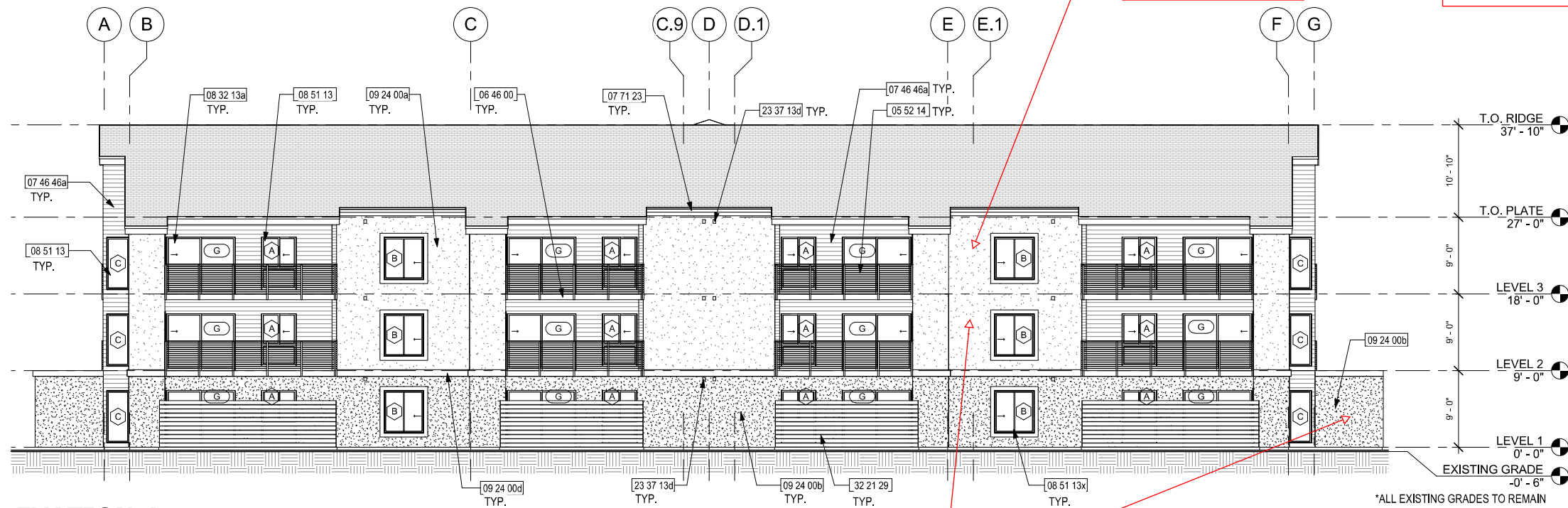


KEYNOTES

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09 24 00b	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00d	PORTLAND CEMENT PLASTER SKIM COAT OVER (N) TRIM, OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
09 31 33	STONE TILING. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.

WEST ELEVATION
1/8" = 1'-0"

PERMIT RESUBMITTAL 2

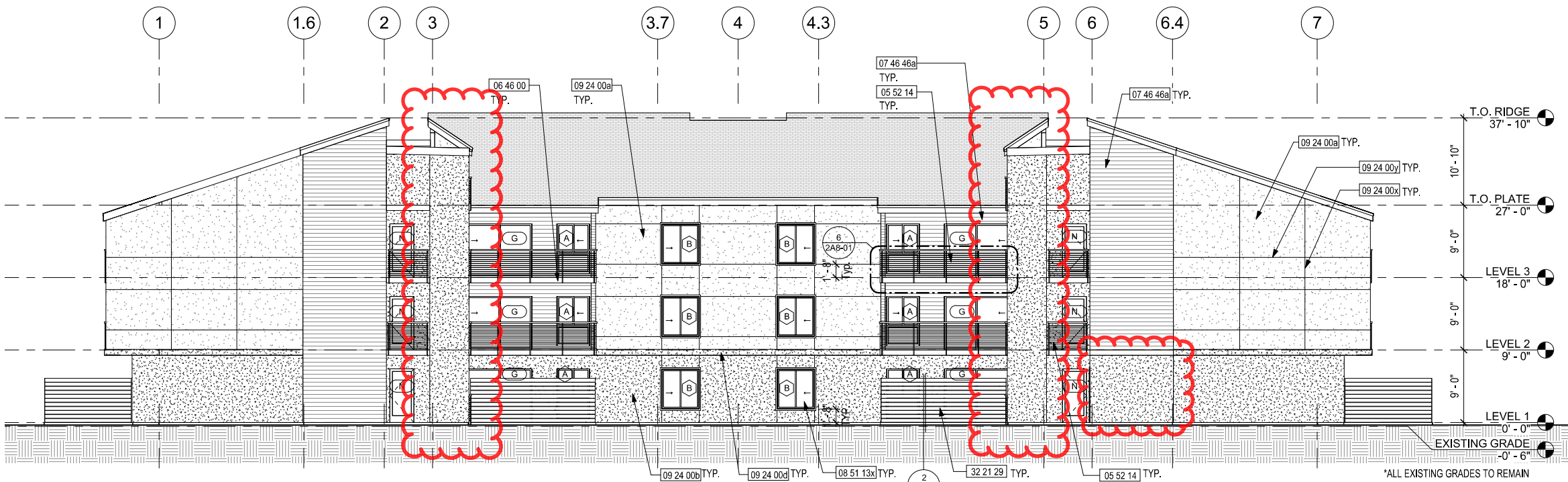


KEY NOTES

05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 71 23	REINSTALLED GUTTER & RVL PAINTED
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
09 24 00b	PAINT (E) STUCCO - COLOR 2
09 24 00d	(N) WOOD TRIM PTD W/ CONT ALUM FLASHING, OVER (E) STUCCO
23 37 13d	DRYER EXHAUST VENT CAP
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.

WEST ELEVATION
1/8" = 1'-0"



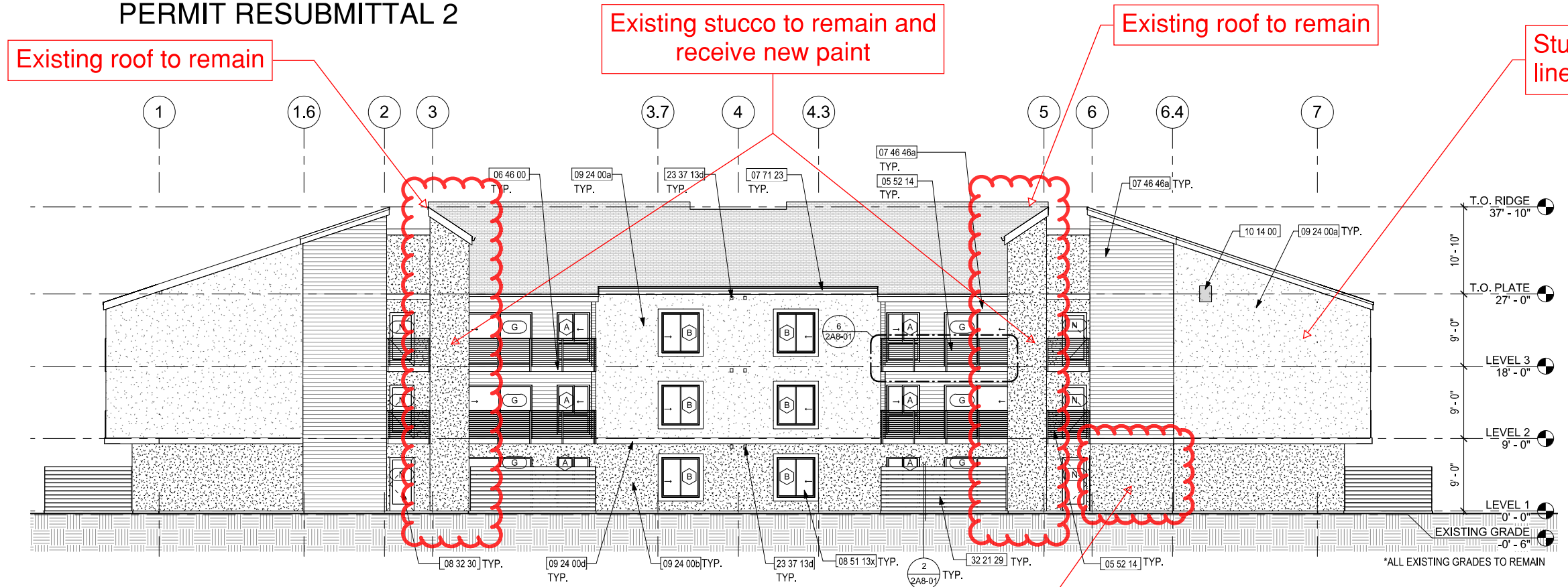


NORTH ELEVATION
1/8" = 1'-0"

KEYNOTES

02 01 80e	(E) ROOF TO REMAIN
05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00	EXTERIOR WOOD FASCIA, PAINTED
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4"
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00b	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00d	PORTLAND CEMENT PLASTER SKIM COAT OVER (N) TRIM, OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
09 31 33	STONE TILING. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.

PERMIT RESUBMITTAL 2

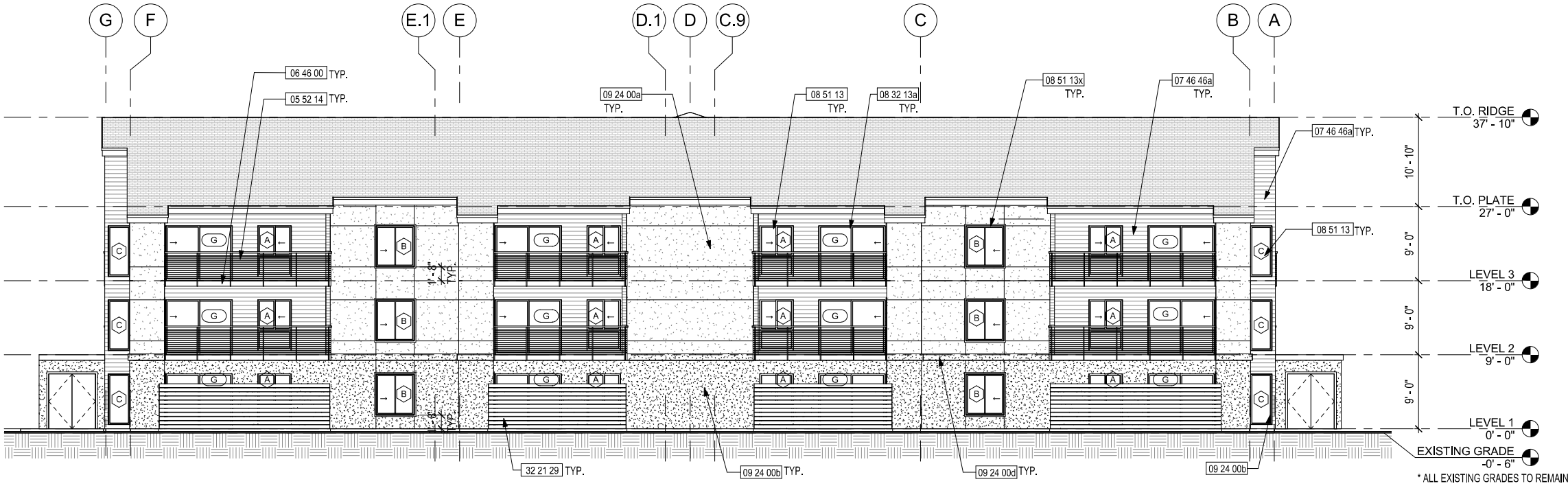


NORTH ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
09 24 00b	PAINT (E) STUCCO - COLOR 2
09 24 00d	(N) WOOD TRIM PTD W/ CONT ALUM FLASHING, OVER (E) STUCCO
09 31 33	STONE VENEER TILE. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12
10 14 00	BLDG ADDRESS SIGNAGE
23 37 13d	DRYER EXHAUST VENT CAP
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.
32 31 30	(N) EXTERIOR WESTERN RED CEDAR 1X6 O/ 1X FURRING, O/ (E) STUCCO. STAIN FINISH



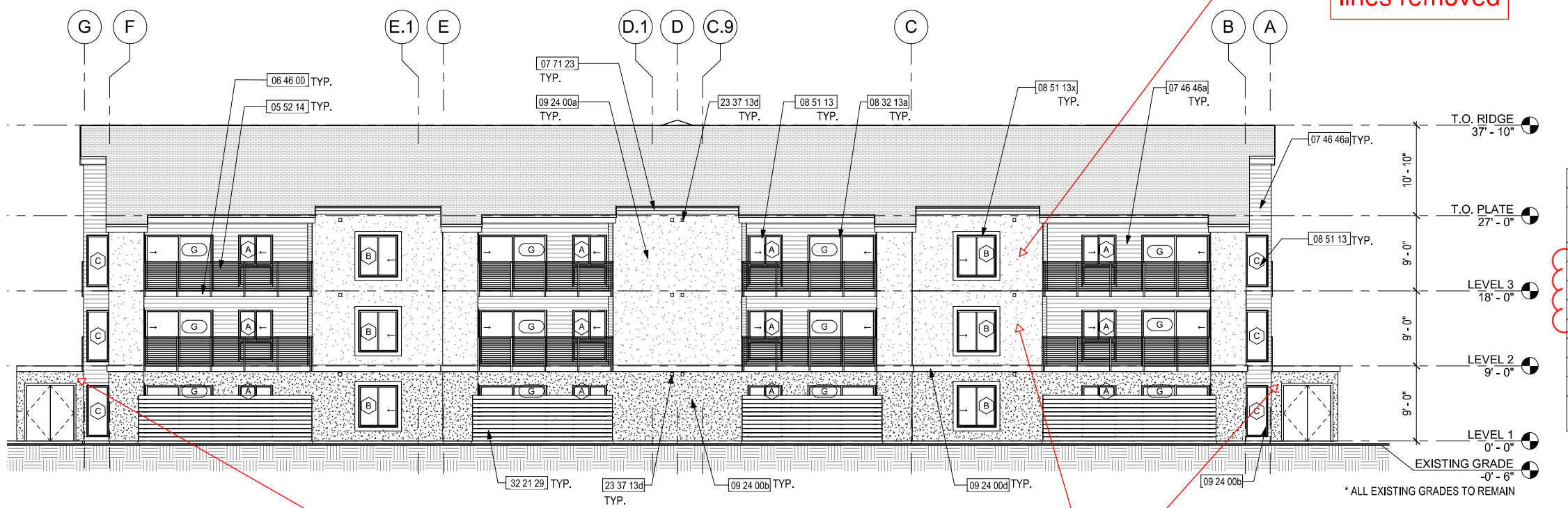


KEYNOTES

02 01 80e	(E) ROOF TO REMAIN
05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00	EXTERIOR WOOD FASCIA, PAINTED
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4"
08 32 13a	ALUM. CLAD VINYL SLIDING DOOR - GIENOW DESIGN SERIES
08 51 13	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00b	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00d	PORTLAND CEMENT PLASTER SKIM COAT OVER (N) TRIM, OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
09 31 33	STONE TILING. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.

EAST ELEVATION
1/8" = 1'-0"

PERMIT



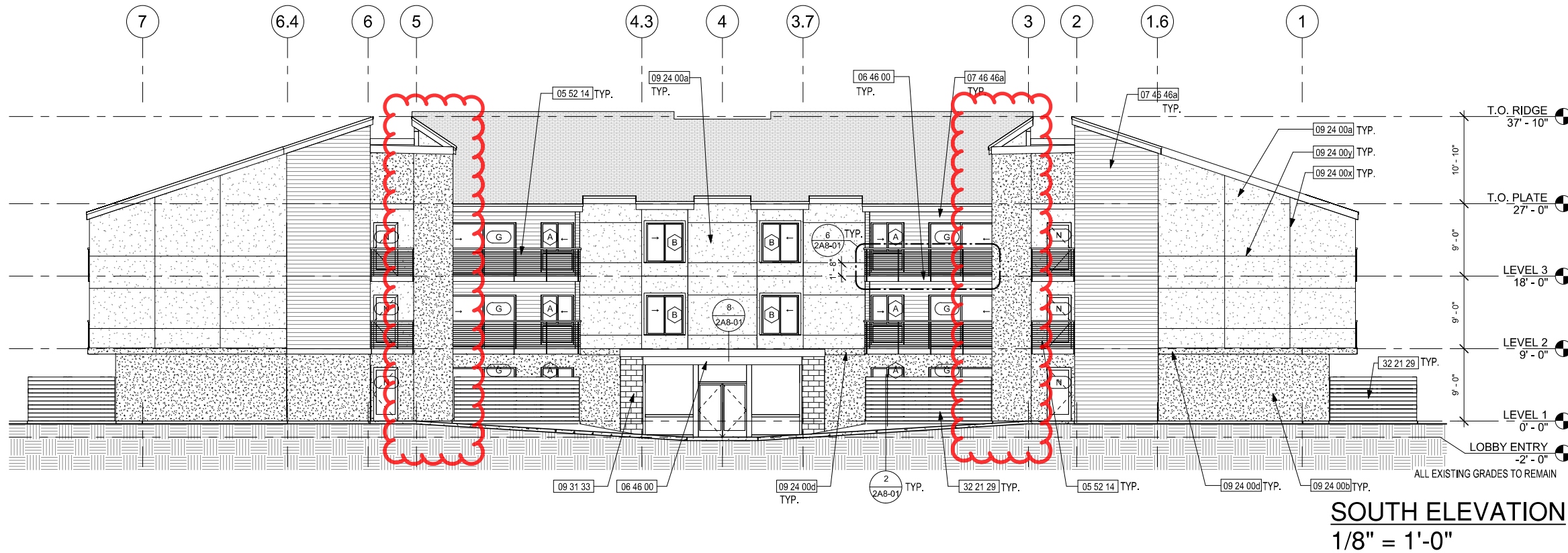
KEY NOTES

05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 71 23	REINSTALLED CUTTER & RWL PAINTED
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
09 24 00b	PAINT (E) STUCCO - COLOR 2
09 24 00d	(N) WOOD TRIM PTD W/ CONT ALUM FLASHING, OVER (E) STUCCO
23 37 13d	DRYER EXHAUST VENT CAP
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.

Existing stucco to remain
and receive new paint

EAST ELEVATION
1/8" = 1'-0"

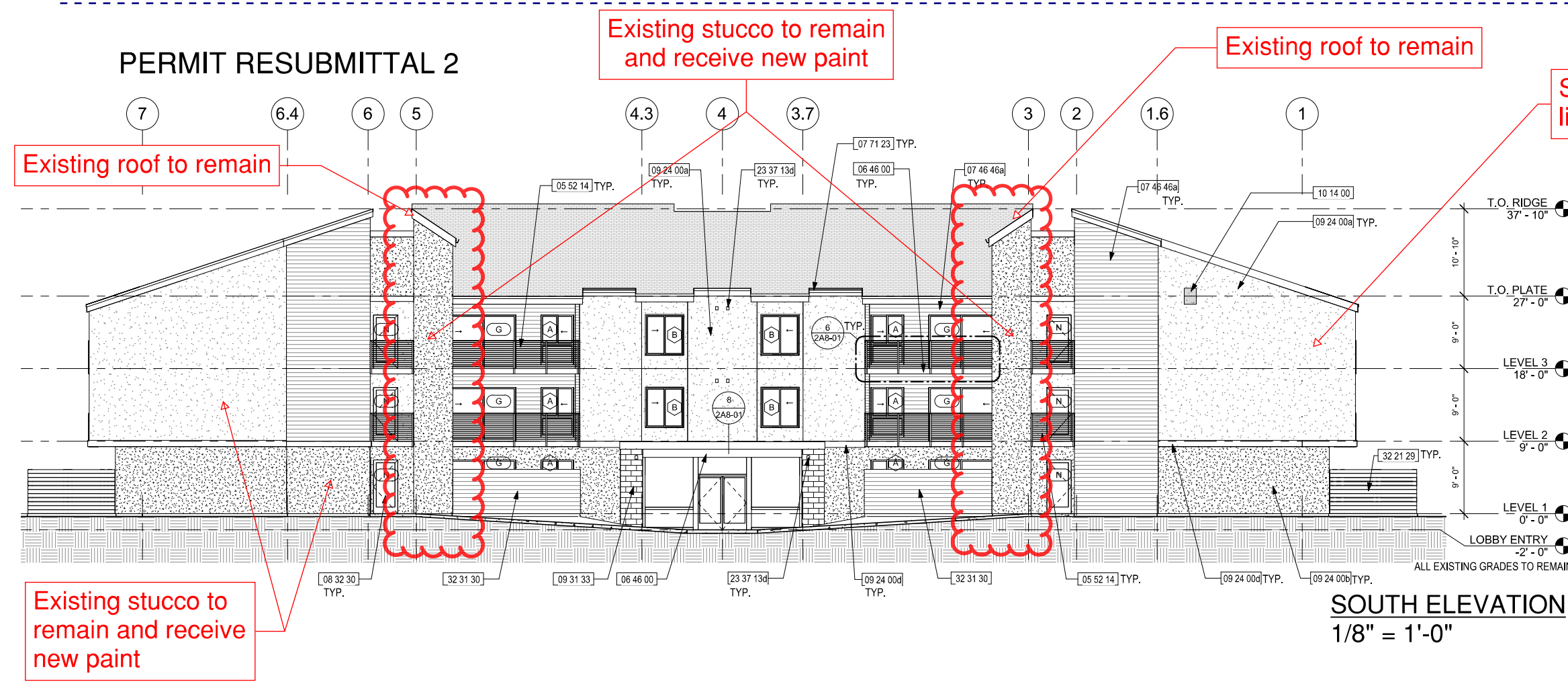




KEYNOTES

02 01 80e	(E) ROOF TO REMAIN
05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00	EXTERIOR WOOD FASCIA, PAINTED
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4"
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00b	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00d	PORTLAND CEMENT PLASTER SKIM COAT OVER (N) TRIM, OVER (E) STUCCO - SMOOTH FINISH COLOR 2
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
09 31 33	STONE TILING. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.

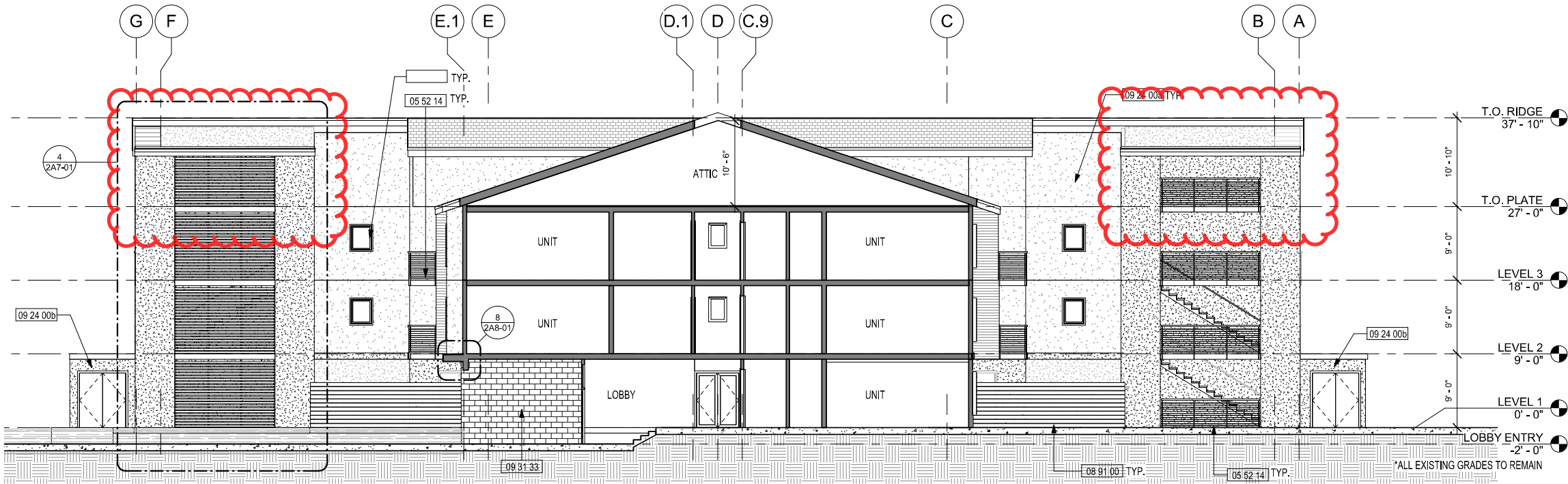
PERMIT RESUBMITTAL 2



KEY NOTES

05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
09 24 00b	PAINT (E) STUCCO - COLOR 2
09 24 00d	(N) WOOD TRIM PTD W/ CONT ALUM FLASHING, OVER (E) STUCCO
09 31 33	STONE VENEER TILE. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12
10 14 00	BLDG ADDRESS SIGNAGE
23 37 13d	DRYER EXHAUST VENT CAP
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.
32 31 30	(N) EXTERIOR WESTERN RED CEDAR 1X6 O/ 1X FURRING, O/ (E) STUCCO. STAIN FINISH





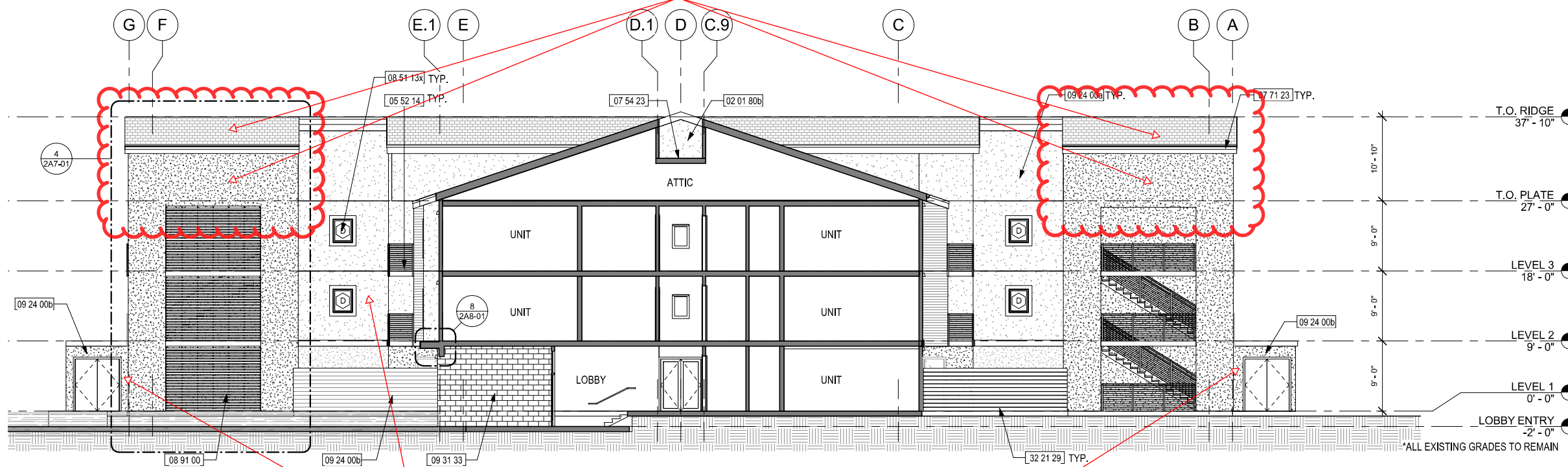
KEYNOTES

02 01 80b	(E) MECH. WELL TO REMAIN
02 01 80c	(E) ATTIC TO REMAIN
02 01 80e	(E) ROOF TO REMAIN
05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
07 18 13	PEDESTRIAN TRAFFIC COATING OVER (E) DECK
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
08 91 00	STAIR SCREEN WALL - RESYSTA SLATS #FHC006035144, OVER STEEL SUBSTRUCTURE
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 31 33	STONE TILING. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12

SECTION
1/8" = 1'-0"

PERMIT RESUBMITTAL 2

Existing roof & wall to remain



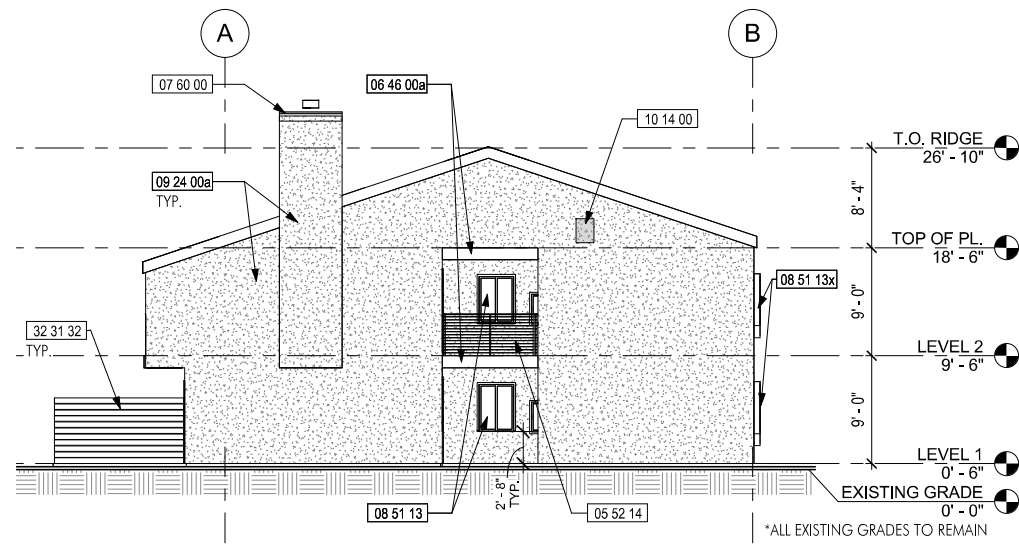
KEY NOTES

02 01 80b	(E) MECH. WELL TO REMAIN
02 01 80e	(E) ROOF TO REMAIN; FLAT ROOF TO RECEIVE (N) TPO
05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
07 18 13	PEDESTRIAN TRAFFIC COATING, PLI-DECK SYSTEMS ESR-2097, OVER (E) DECK
07 54 23	THERMOPLASTIC-POLYOLEFIN (TPO) ROOFING, GAF UL ER1306-01
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
08 91 00	STAIR SCREEN WALL - RESYSTA SLATS #FHC006035144 COLOR: C64, OVER STEEL SUBSTRUCTURE
09 24 00a	PAINT (E) STUCCO - COLOR 1
09 24 00b	PAINT (E) STUCCO - COLOR 2
09 31 33	STONE VENEER TILE. ADHERE VENEER IN ACCORDANCE W/ THE MANUF. INSTRUCTIONS TO COMPLY W/CRC R703.12
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN, (E) WOOD POSTS, STAIN FINISH.

Existing stucco to remain
and receive new paint

SECTION
1/8" = 1'-0"



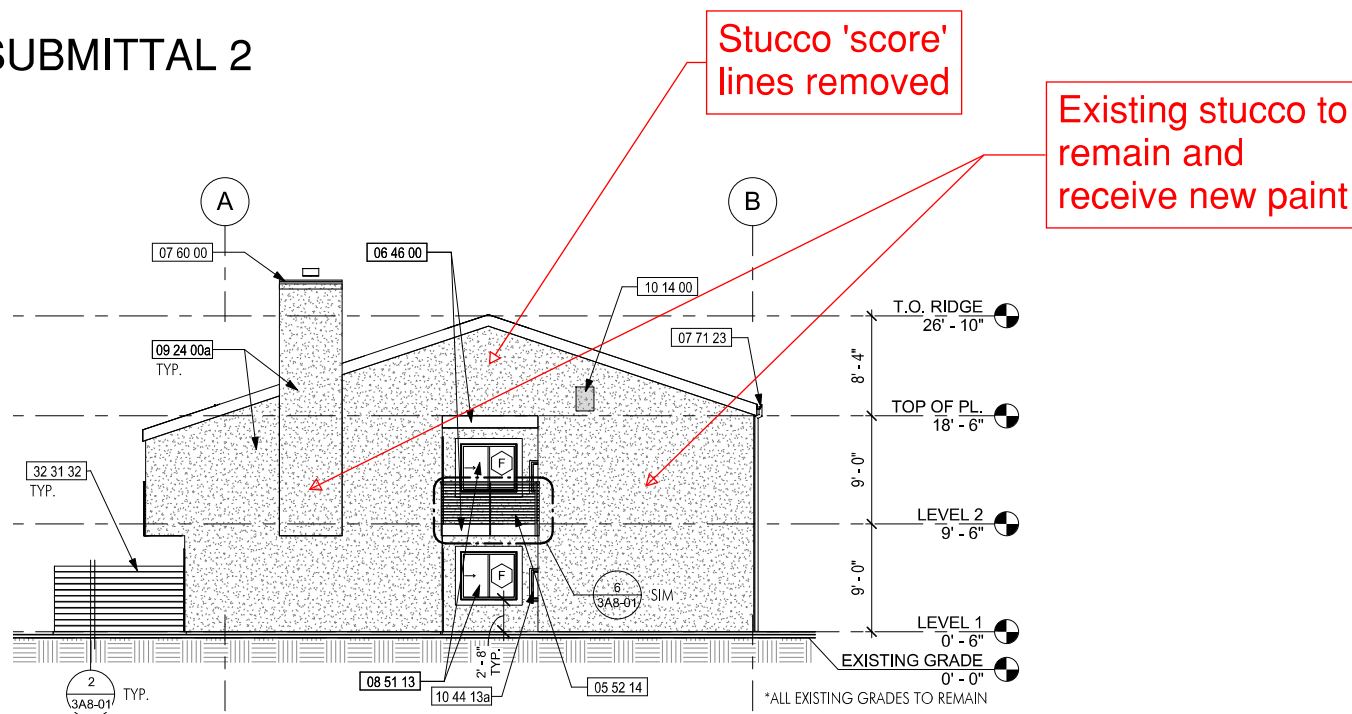


WEST ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00a	
06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
08 32 13a	ALUM. CLAD VINYL SLIDING DOOR - GIENOW DESIGN SERIES
08 51 13	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
10 14 00	BLDG ADDRESS SIGNAGE
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING

PERMIT RESUBMITTAL 2

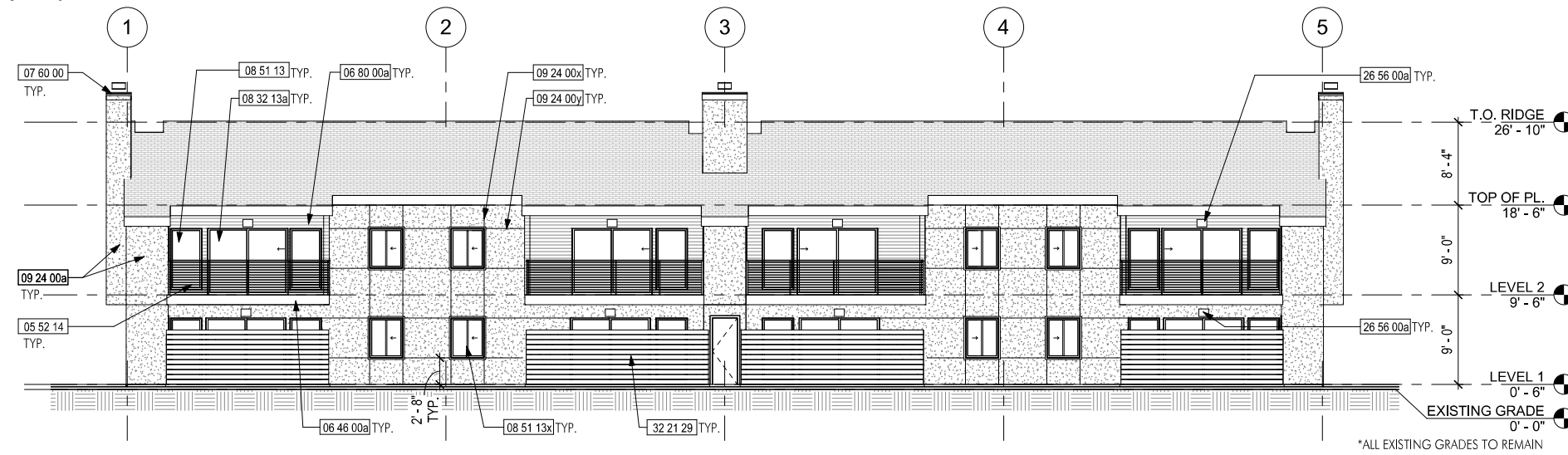


WEST ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
10 14 00	BLDG ADDRESS SIGNAGE
10 44 13a	(E) FIRE EXTINGUISHER CABINET
23 37 13d	DRYER EXHAUST VENT CAP
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING



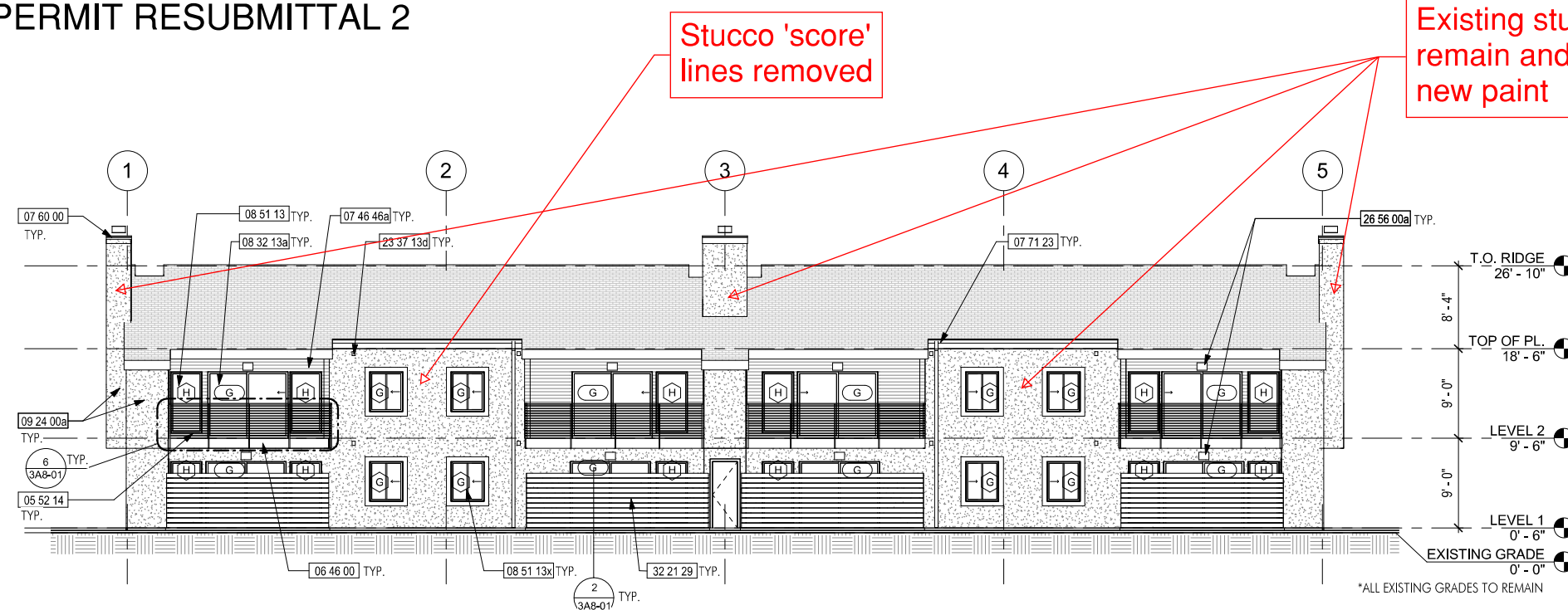


NORTH ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00a	
06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
08 32 13a	ALUM. CLAD VINYL SLIDING DOOR - GIENOW DESIGN SERIES
08 51 13	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
10 14 00	BLDG ADDRESS SIGNAGE
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING

PERMIT RESUBMITTAL 2

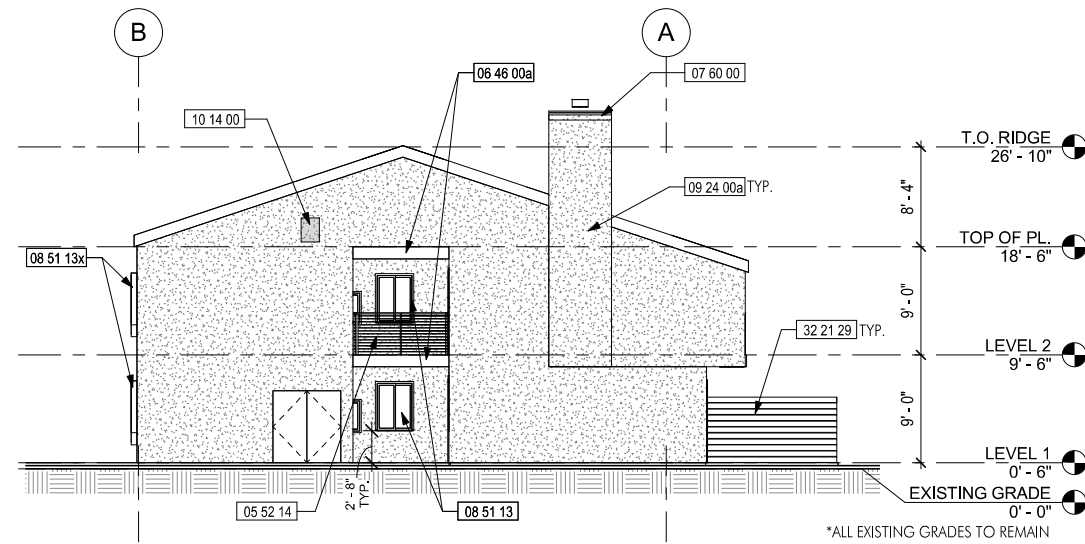


NORTH ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
10 14 00	BLDG ADDRESS SIGNAGE
10 44 13a	(E) FIRE EXTINGUISHER CABINET
23 37 13d	DRYER EXHAUST VENT CAP
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING



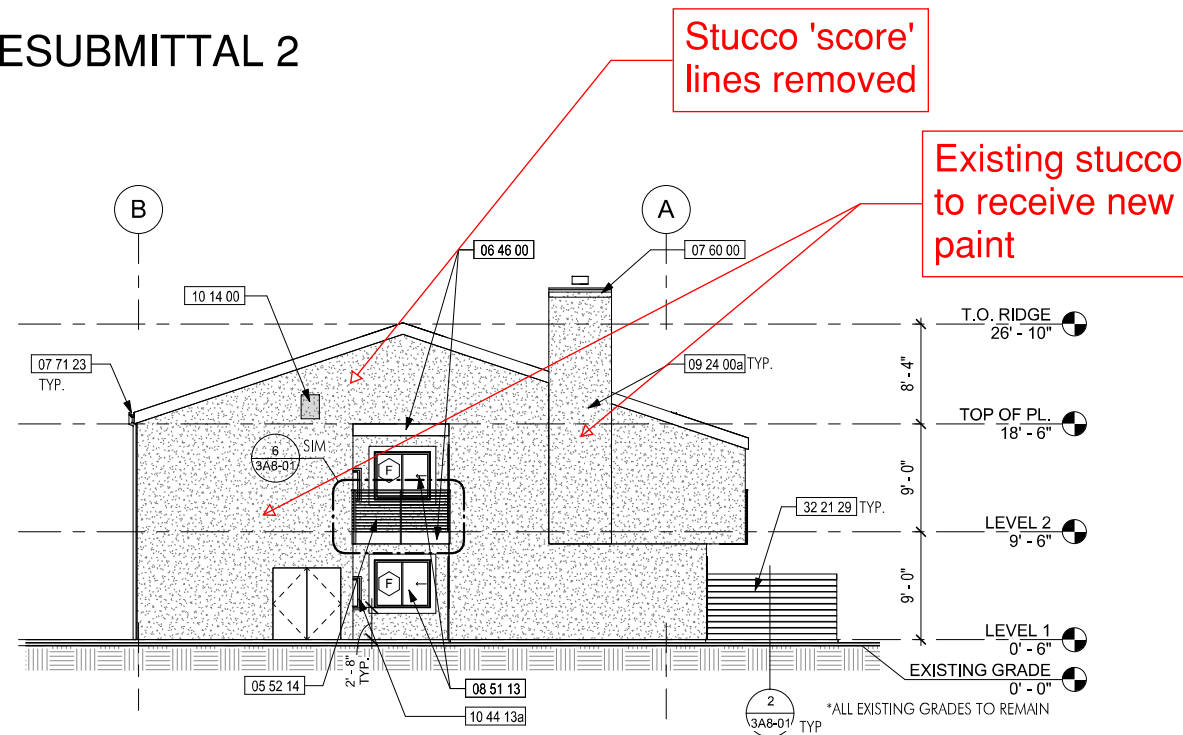


EAST ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00a	
06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
08 32 13a	ALUM. CLAD VINYL SLIDING DOOR - GIENOW DESIGN SERIES
08 51 13	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
10 14 00	BLDG ADDRESS SIGNAGE
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING

PERMIT RESUBMITTAL 2



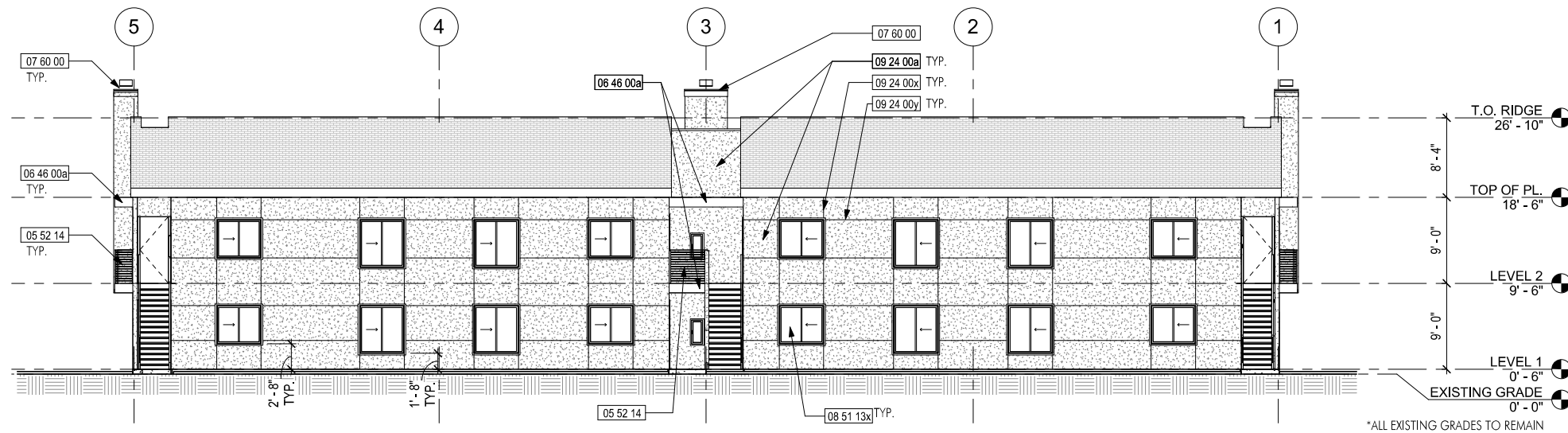
EAST ELEVATION
1/8" = 1'-0"

Window & door product is changed

KEY NOTES

05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
10 14 00	BLDG ADDRESS SIGNAGE
10 44 13a	(E) FIRE EXTINGUISHER CABINET
23 37 13d	DRYER EXHAUST VENT CAP
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING



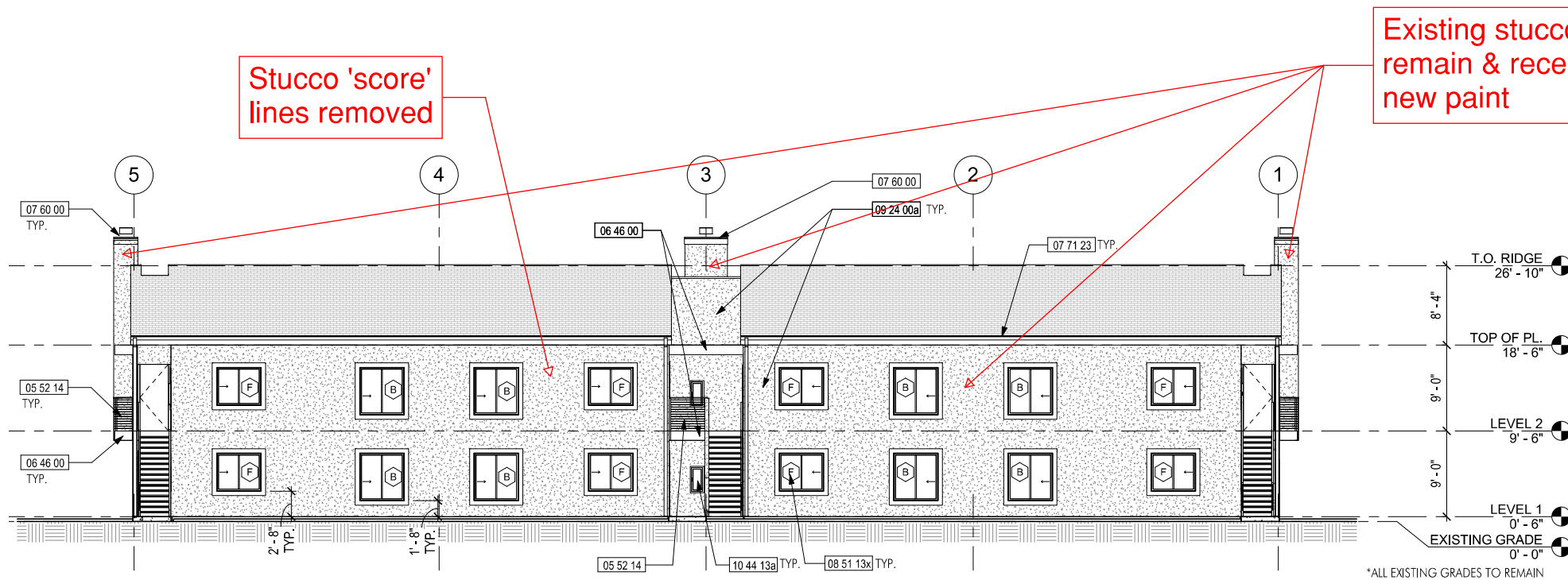


SOUTH ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
08 32 13a	ALUM. CLAD VINYL SLIDING DOOR - GIENOW DESIGN SERIES
08 51 13	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
10 14 00	BLDG ADDRESS SIGNAGE
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING

PERMIT RESUBMITTAL 2

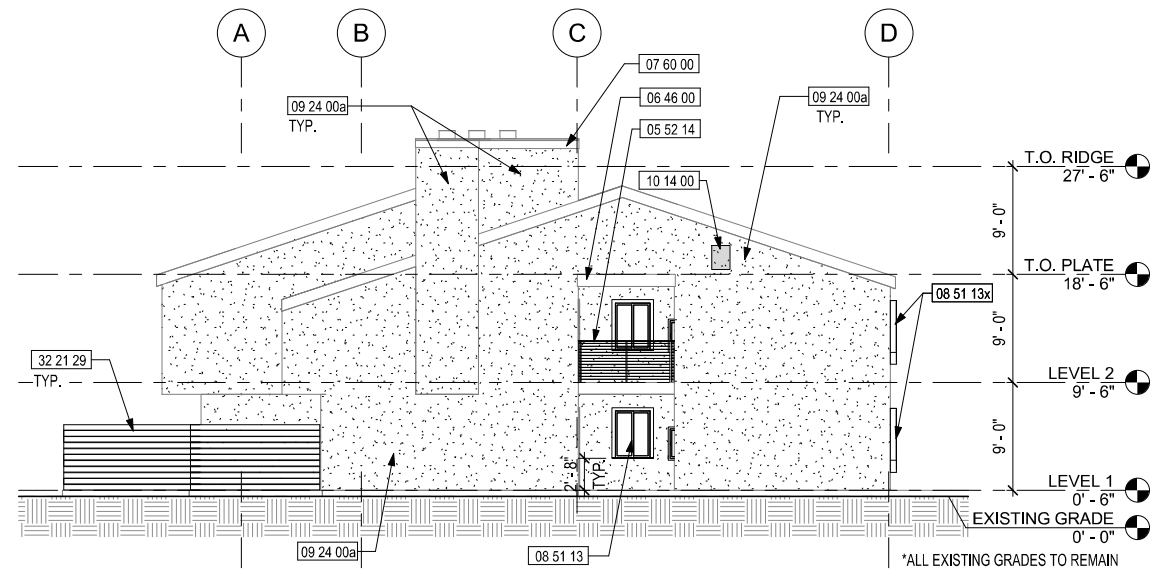


SOUTH ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	CABLE RAILING W/ 2" X 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
10 14 00	BLDG ADDRESS SIGNAGE
10 44 13a	(E) FIRE EXTINGUISHER CABINET
23 37 13d	DRYER EXHAUST VENT CAP
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING





WEST ELEVATION
1/8" = 1'-0"

KEY NOTES

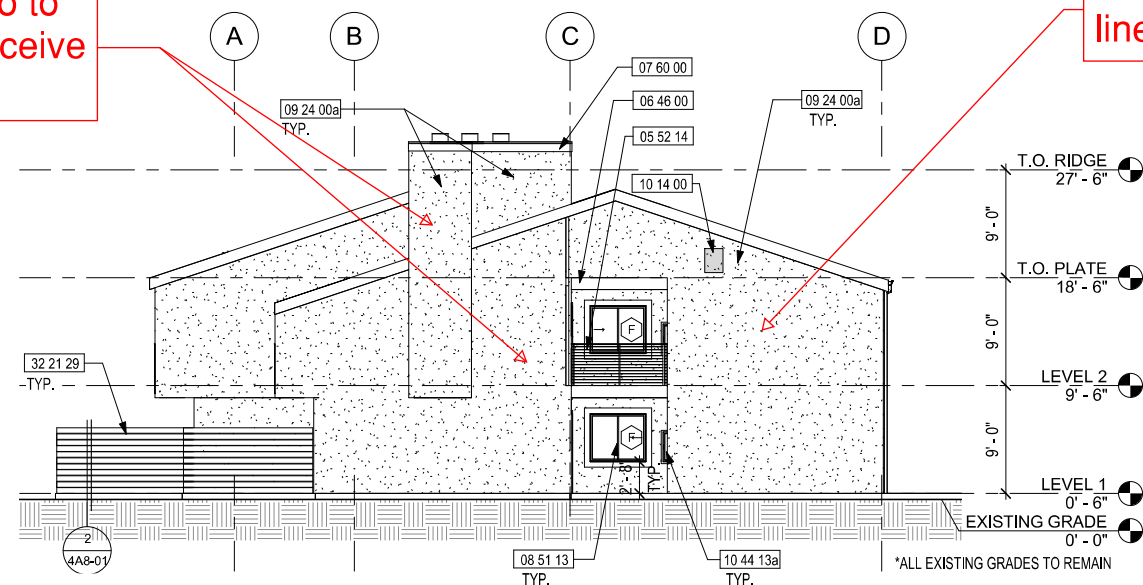
05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00	EXTERIOR WOOD FASCIA, PAINTED
06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
08 32 13a	ALUM. CLAD VINYL SLIDING DOOR - GIENOW DESIGN SERIES
08 51 13	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
10 14 00	BLDG ADDRESS SIGNAGE
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.

PERMIT RESUBMITTAL 2

Existing stucco to remain and receive new paint

Stucco 'score' lines removed

Window & door product is changed

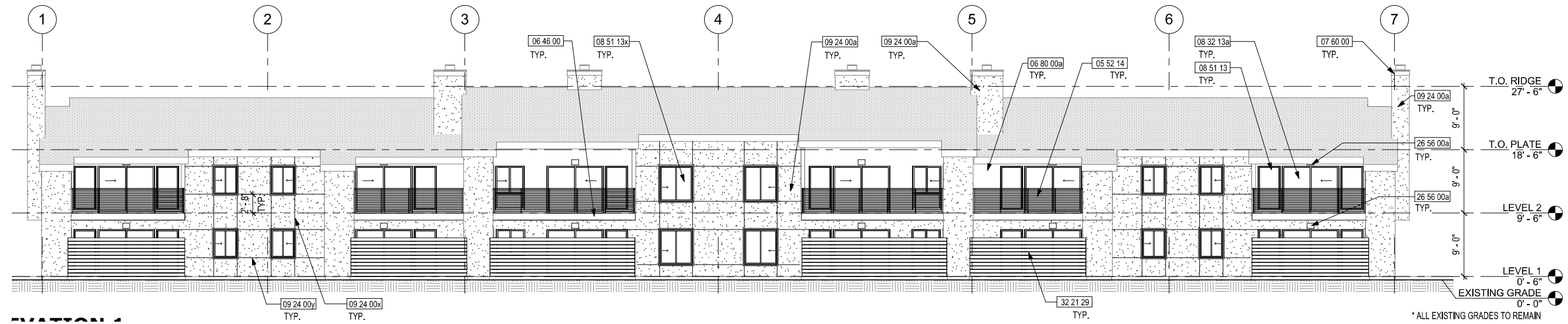


WEST ELEVATION
1/8" = 1'-0"

KEY NOTES

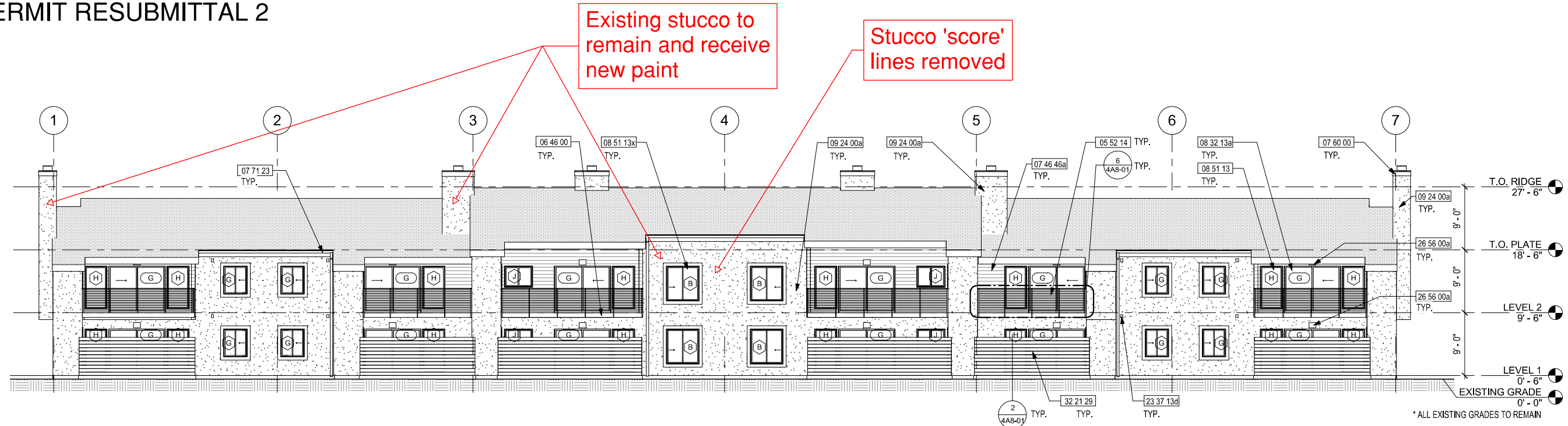
05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 14 23a	REPLACE REMOVED WOOD ENTRY DOOR IN KIND
08 14 23b	INFILL WINDOW OPENING & FINISH TO MATCH ADJACENT STUCCO
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO - COLOR 1
10 14 00	BLDG ADDRESS SIGNAGE
10 44 13a	(E) FIRE EXTINGUISHER CABINET
23 37 13d	DRYER EXHAUST VENT CAP
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.





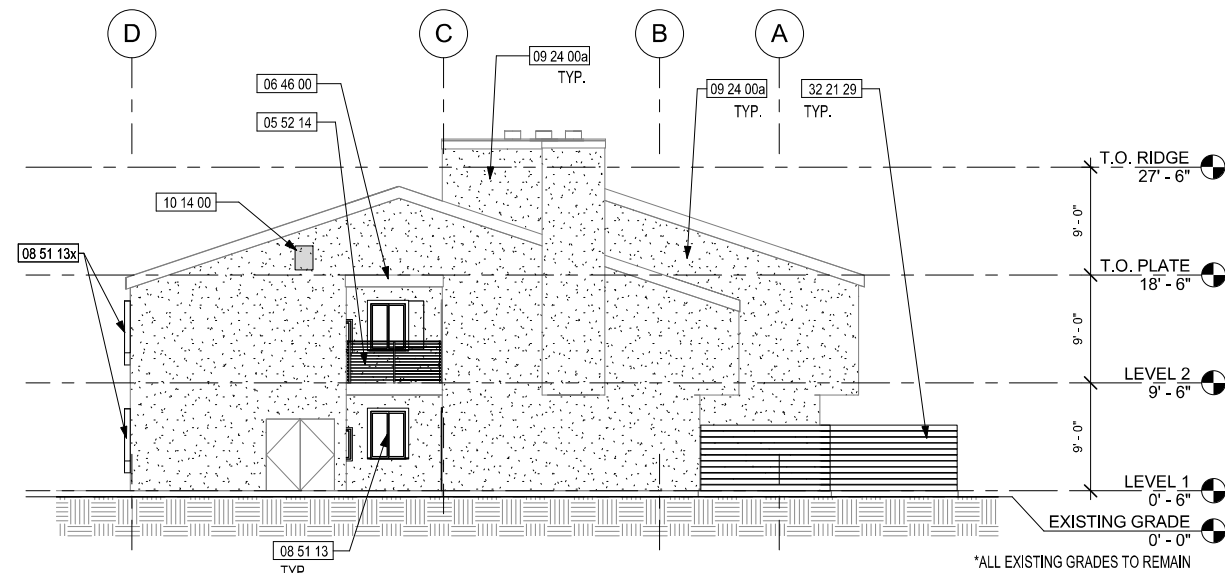
NORTH ELEVATION
1/8" = 1'-0"

PERMIT RESUBMITTAL 2



NORTH ELEVATION
1/8" = 1'-0"



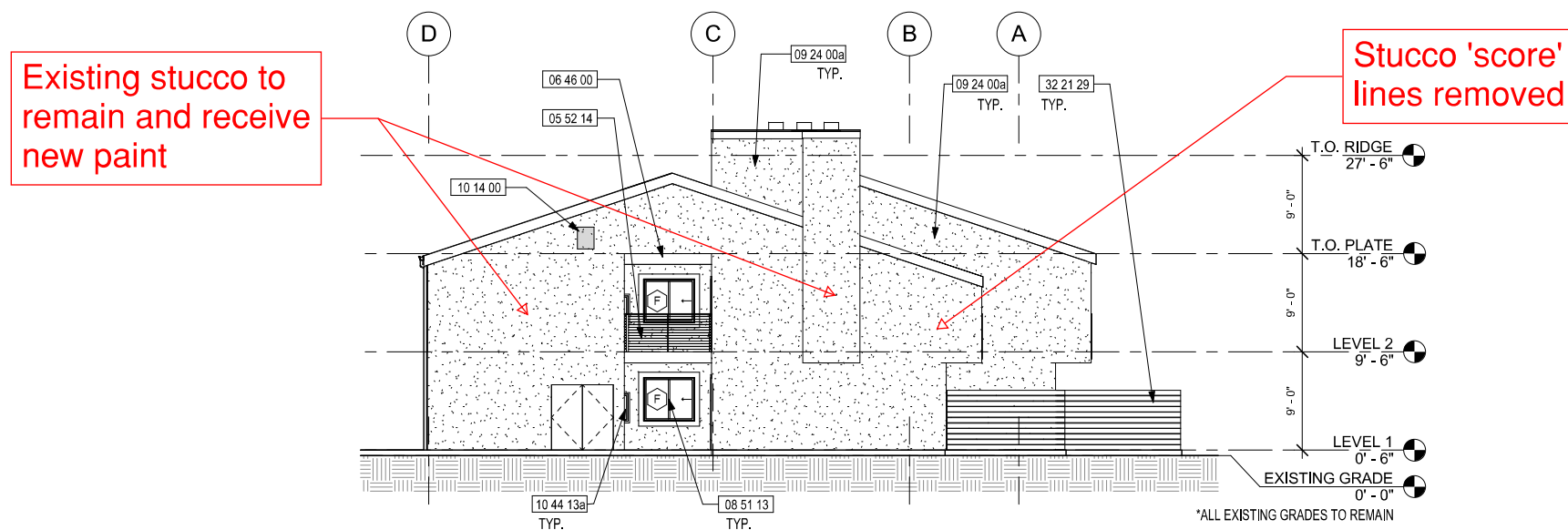


EAST ELEVATION
1/8" = 1'-0"

KEY NOTES

05 52 14	ALUM. CABLE RAILING W/ 5/8" X 5/8" RESYSTA #AHF008018144 TOP RAIL & TS STANCHION
06 46 00	EXTERIOR WOOD FASCIA, PAINTED
06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
08 32 13a	ALUM. CLAD VINYL SLIDING DOOR - GIENOW DESIGN SERIES
08 51 13	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES
08 51 13x	ALUM. CLAD VINYL WINDOWS - GIENOW DESIGN SERIES, W/ CUSTOM SHADOW BOX.
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
09 24 00x	PORTLAND CEMENT PLASTER SCORE JOINT VERTICAL
09 24 00y	PORTLAND CEMENT PLASTER SCORE JOINT HORIZONTAL
10 14 00	BLDG ADDRESS SIGNAGE
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS.

PERMIT RESUBMITTAL 2



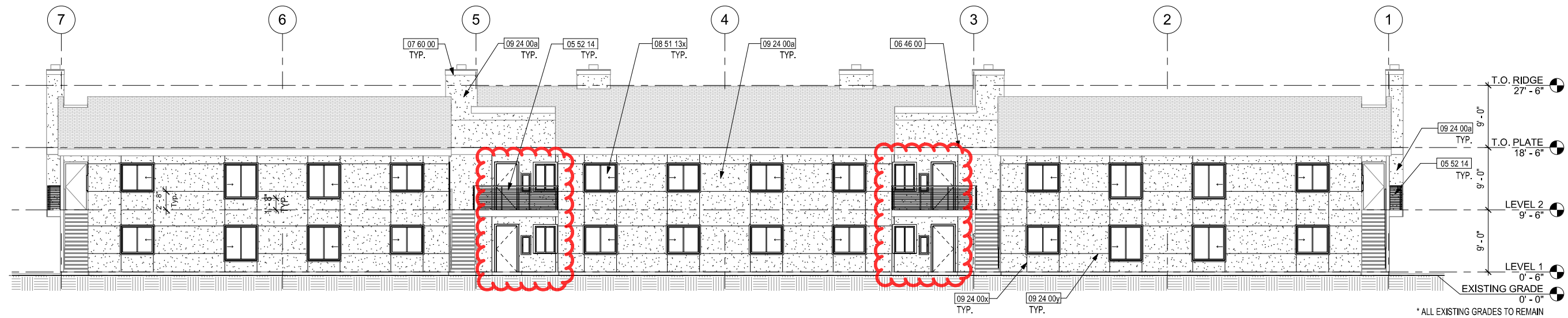
EAST ELEVATION
1/8" = 1'-0"

Window & door product is changed

KEY NOTES

05 52 14	CABLE RAILING W/ 2" x 1" RESYSTA #GSP022011144 TOP RAIL & TS STANCHION
06 46 00	PAINT (E) WOOD JOIST FASCIA
07 46 46a	FIBER-CEMENT SIDING - WOODTONE RUSTIC SERIES LAP SIDING, 6-1/4", COLOR: SUMMER WHEAT
07 60 00	SHT. MTL. CHIMNEY CAP - GSM, PAINTED
07 71 23	REINSTALLED GUTTER & RWL, PAINTED
08 14 23a	REPLACE REMOVED WOOD ENTRY DOOR IN KIND
08 14 23b	INFILL WINDOW OPENING & FINISH TO MATCH ADJACENT STUCCO
08 32 13a	REPLACEMENT VINYL SLIDING DOOR BY PLYGEM
08 51 13	REPLACEMENT VINYL WINDOW BY PLYGEM
08 51 13x	REPLACEMENT VINYL WINDOW BY PLYGEM, W/ EXT WOOD TRIM, PTD
09 24 00a	PAINT (E) STUCCO COLOR 1
10 14 00	BLDG ADDRESS SIGNAGE
10 44 13a	(E) FIRE EXTINGUISHER CABINET
23 37 13d	DRYER EXHAUST VENT CAP
26 56 00a	EXTERIOR LIGHTING FIXTURE, LOCATED ABOVE SLIDING DOOR
32 21 29	(N) EXTERIOR WESTERN RED CEDAR PRIVACY SCREEN. (E) WOOD POSTS. STAIN FINISH.

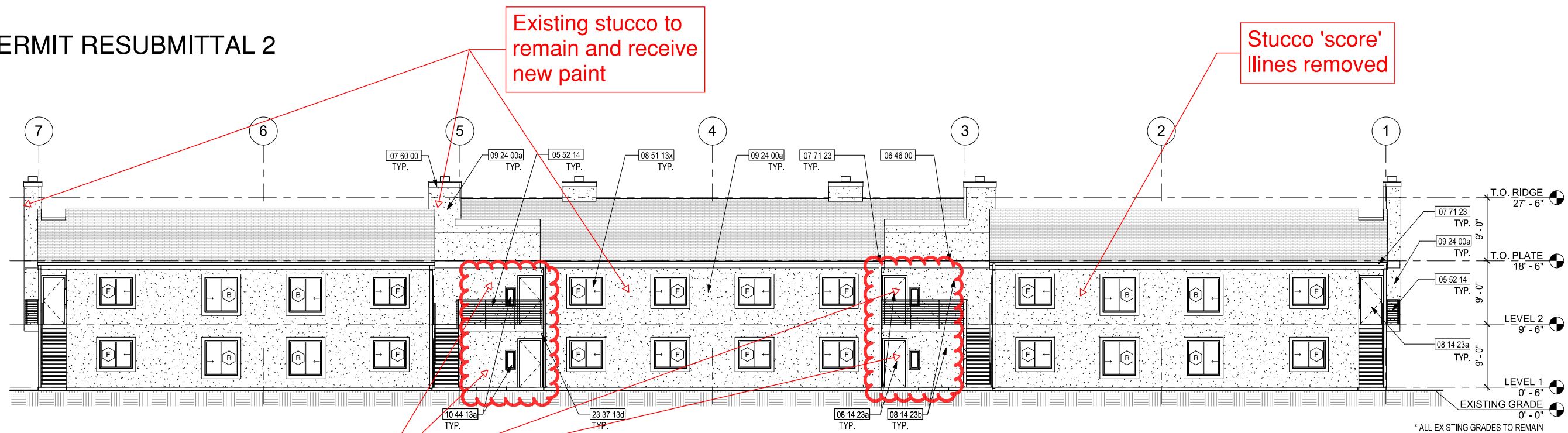




SOUTH ELEVATION

1/8" = 1'-0"

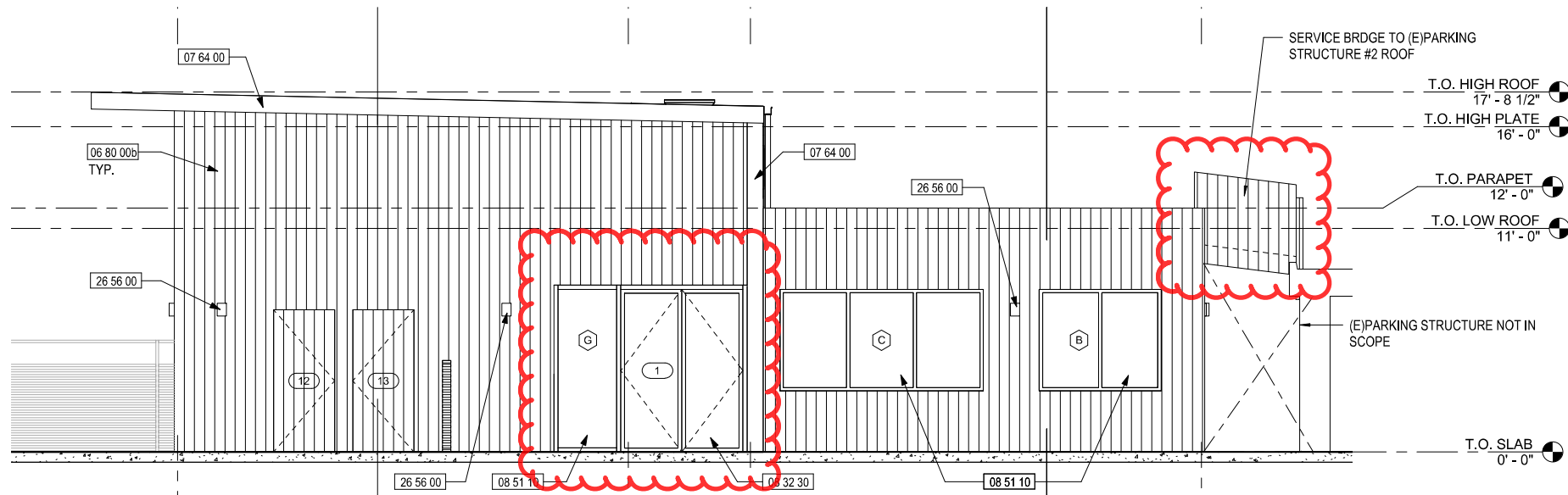
PERMIT RESUBMITTAL 2



SOUTH ELEVATION

1/8" = 1'-0"



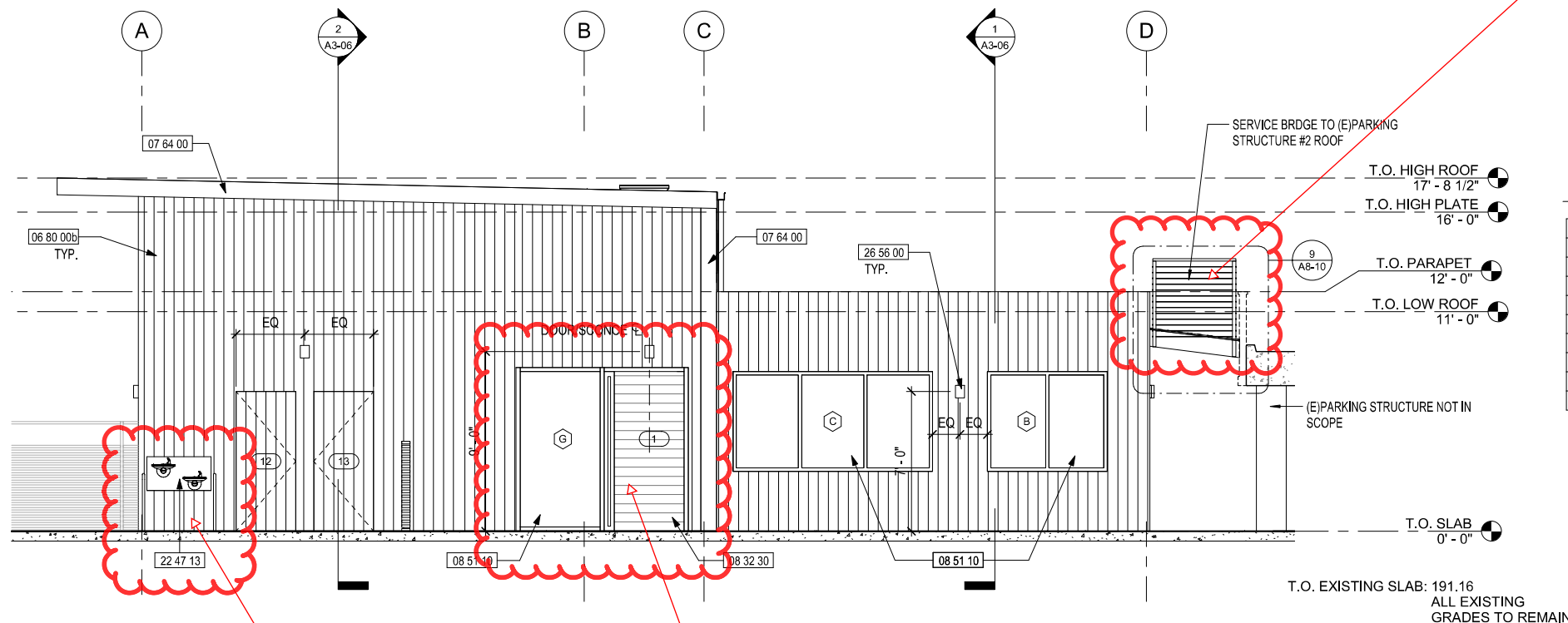


WEST ELEVATION
1/8" = 1'-0"

KEYNOTES

06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144
07 64 00	BRAKE METAL CLADDING
08 32 30	ALUM. HINGED DOOR - WESTERN WINDOW SYSTEMS 900 SERIES
08 51 10	ALUM. WINDOW - WESTERN WINDOW SYSTEMS 600 SERIES
08 91 19	FIXED LOUVER VENT
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING

PERMIT RESUBMITTAL 2



WEST ELEVATION
1/8" = 1'-0"

Guardrail changed to cable railing

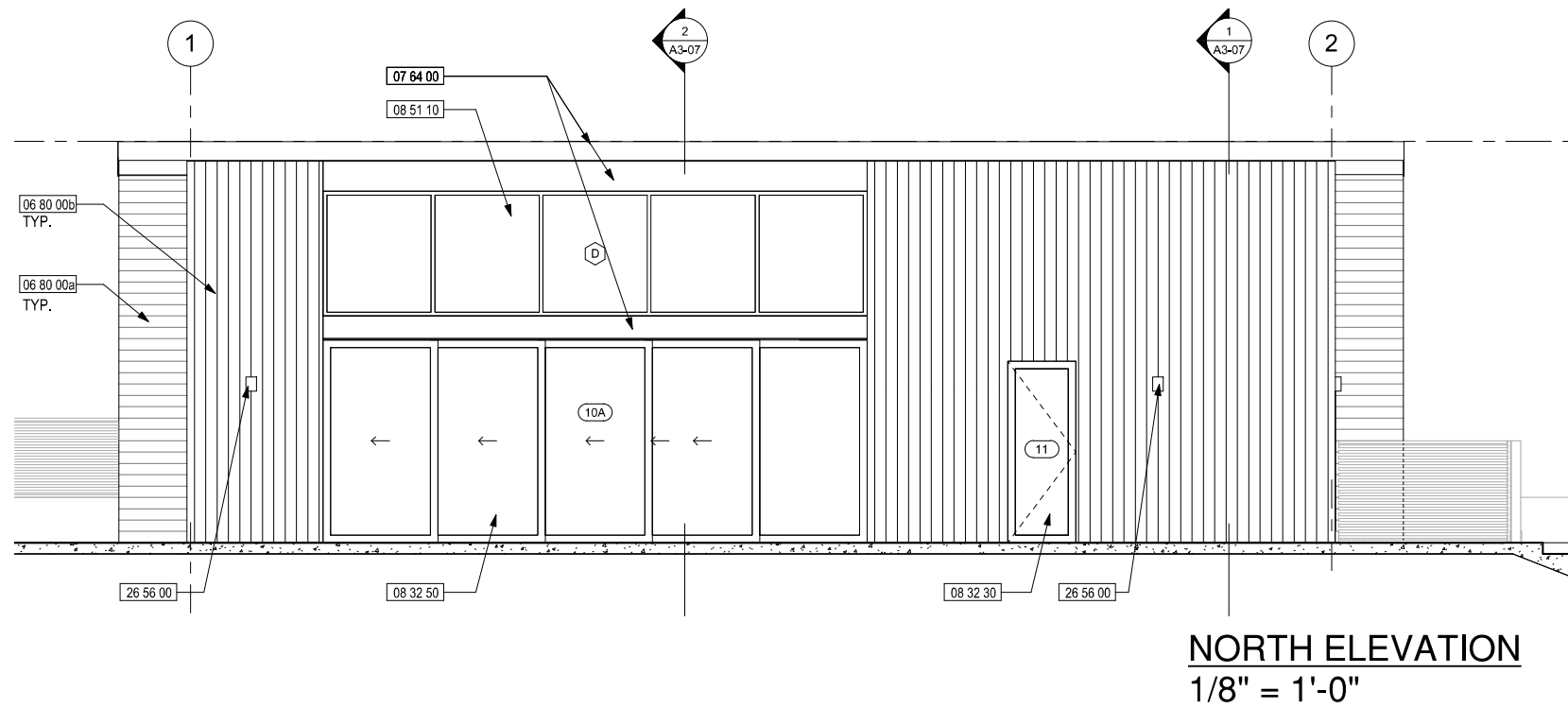
Drinking fountain added during plan check review

Revised entry door design

KEY NOTES

06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C. COLOR: #1078
06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 64 00	BRAKE METAL CLADDING
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 51 10	ALUM. WINDOW - CASCADE ALUMINUM
08 91 19	FIXED LOUVER VENT
09 24 00a	PAINT (E) STUCCO - COLOR 1
22 47 13	(N) HIGH-LOW DRINKING FOUNTAIN - SPD, PROVIDE 1 DRINKING FOUNTAIN GRAB BAR EACH SIDE - BOBRICK 819298 OR EQUAL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING

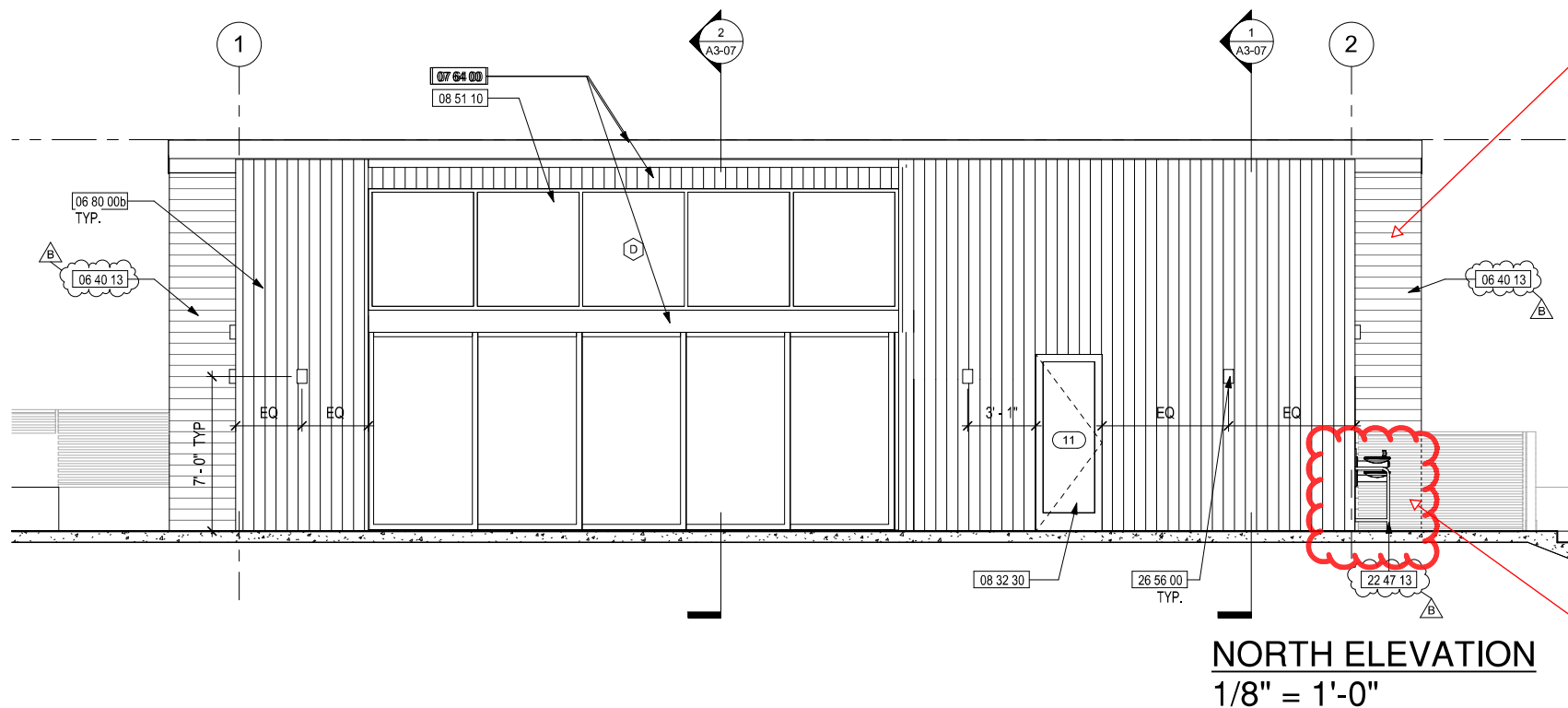




KEYNOTES

06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144
07 64 00	BRAKE METAL CLADDING
08 32 30	ALUM. HINGED DOOR - WESTERN WINDOW SYSTEMS 900 SERIES
08 32 50	ALUM. MULTI-PANEL SLIDING DOORS - WESTERN WINDOW SYSTEMS 600 SERIES
08 51 10	ALUM. WINDOW - WESTERN WINDOW SYSTEMS 600 SERIES
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS

PERMIT RESUBMITTAL 2



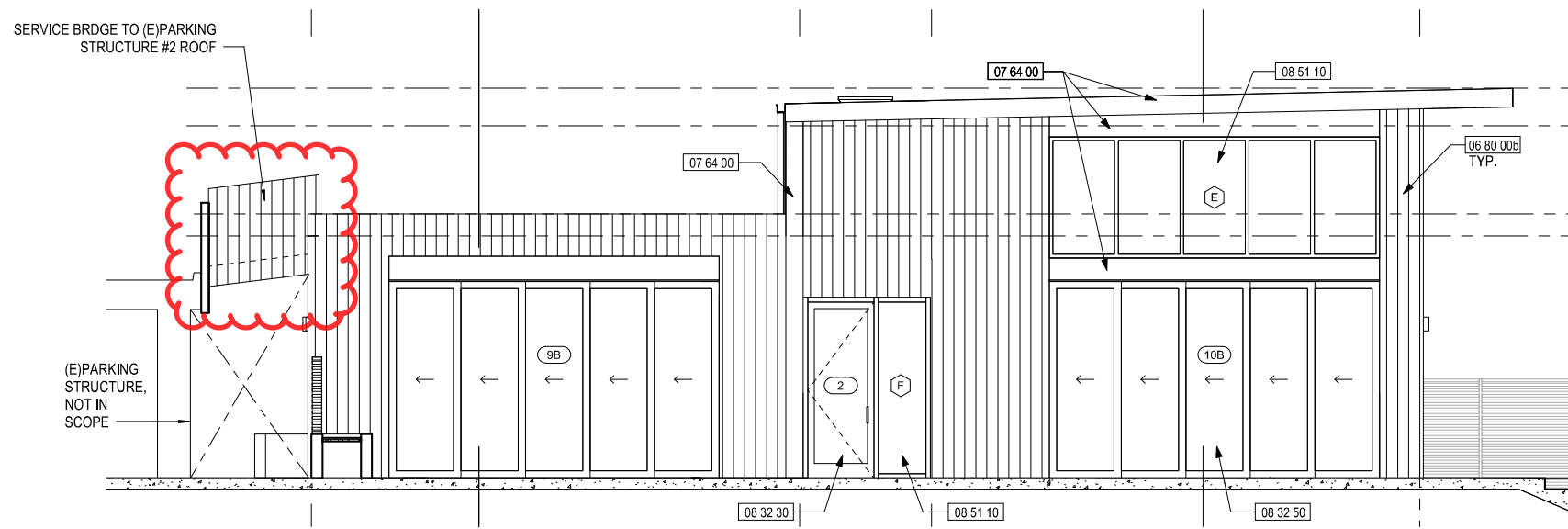
Material changed to T&G cedar. Color to be similar to previous fiber-reinforced siding

KEY NOTES

06 40 13	1X6 T&G - WESTERN RED CEDAR PER SPEC
06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 64 00	BRAKE METAL CLADDING
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 51 10	ALUM. WINDOW - CASCADE ALUMINUM
22 47 13	(N) HIGH-LOW DRINKING FOUNTAIN - SPD. PROVIDE 1 DRINKING FOUNTAIN GRAB BAR EACH SIDE - BOBRICK 819298 OR EQUAL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS

Drinking fountain added during plan check review



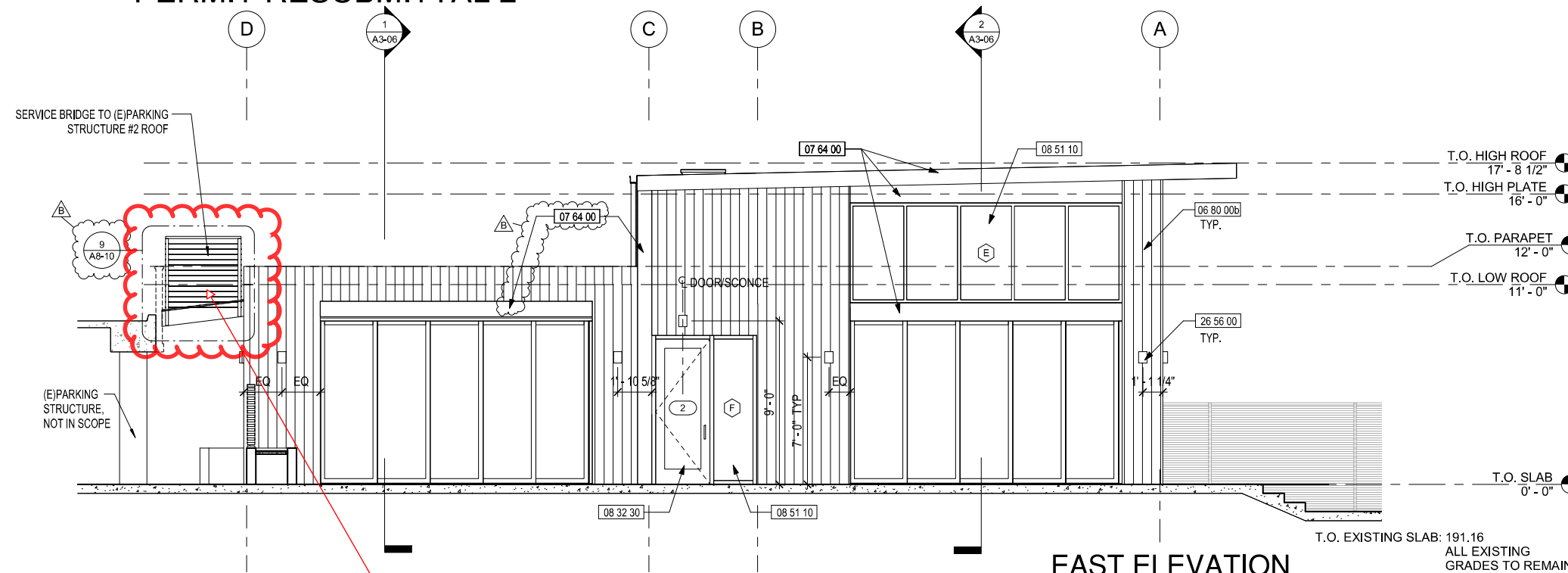


EAST ELEVATION
1/8" = 1'-0"

KEYNOTES

06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144
07 64 00	BRAKE METAL CLADDING
08 32 30	ALUM. HINGED DOOR - WESTERN WINDOW SYSTEMS 900 SERIES
08 32 50	ALUM. MULTI-PANEL SLIDING DOORS - WESTERN WINDOW SYSTEMS 600 SERIES
08 51 10	ALUM. WINDOW - WESTERN WINDOW SYSTEMS 600 SERIES
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS

PERMIT RESUBMITTAL 2



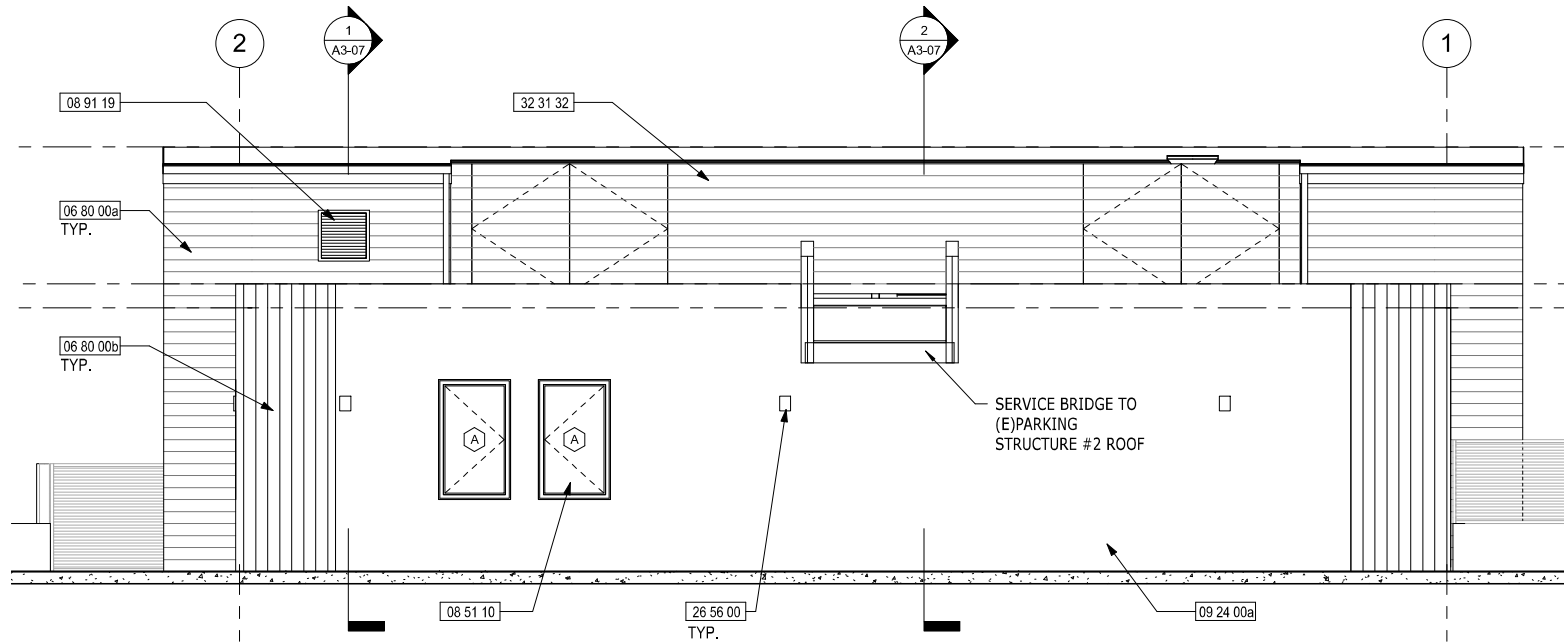
EAST ELEVATION
1/8" = 1'-0"

KEY NOTES

06 40 13	1X6 T&G - WESTERN RED CEDAR PER SPEC
06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 64 00	BRAKE METAL CLADDING
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 51 10	ALUM. WINDOW - CASCADE ALUMINUM
22 47 13	(N) HIGH-LOW DRINKING FOUNTAIN - SPD. PROVIDE 1 DRINKING FOUNTAIN GRAB BAR EACH SIDE - BOBRICK 819298 OR EQUAL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS

Guardrail changed to cable railing



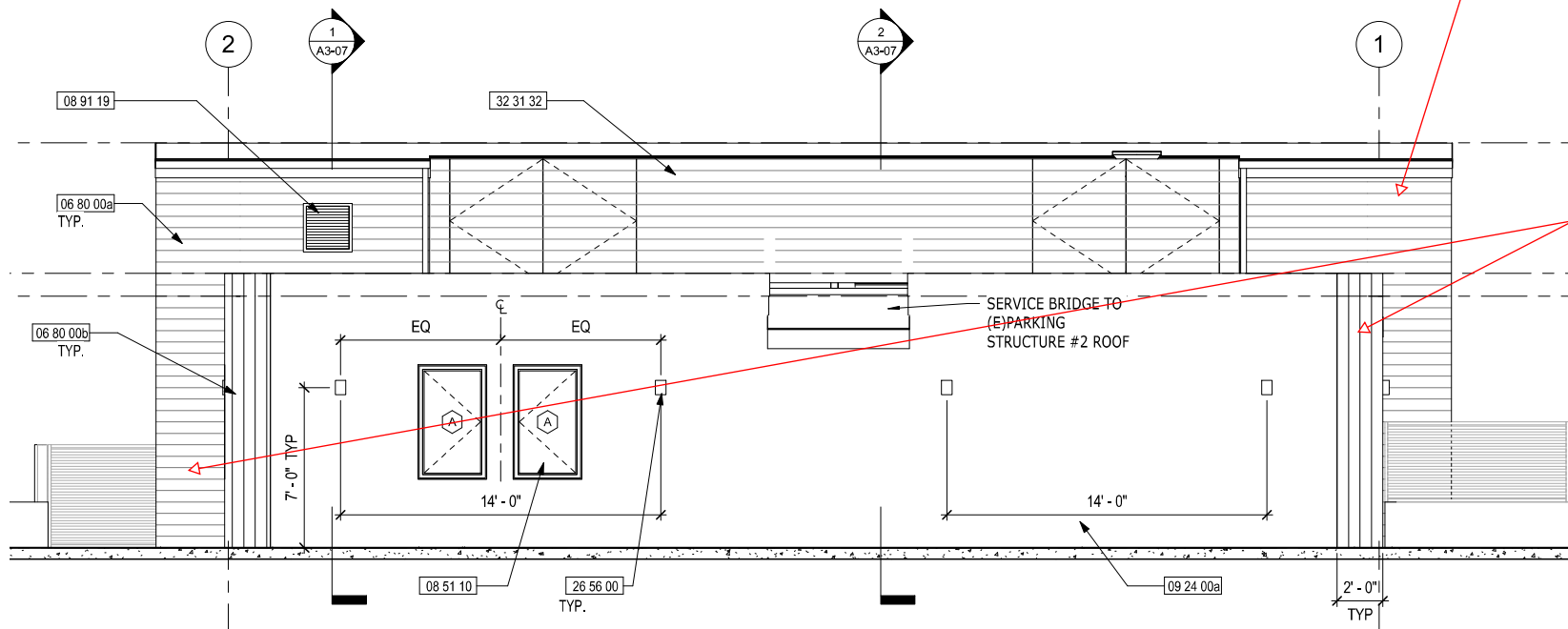


SOUTH ELEVATION
1/8" = 1'-0"

KEYNOTES

06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C.
06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144
07 64 00	BRAKE METAL CLADDING
08 32 30	ALUM. HINGED DOOR - WESTERN WINDOW SYSTEMS 900 SERIES
08 51 10	ALUM. WINDOW - WESTERN WINDOW SYSTEMS 600 SERIES
08 91 19	FIXED LOUVER VENT
09 24 00a	PORTLAND CEMENT PLASTER SKIM COAT OVER (E) STUCCO - SMOOTH FINISH COLOR 1
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING

PERMIT RESUBMITTAL 2



SOUTH ELEVATION
1/8" = 1'-0"

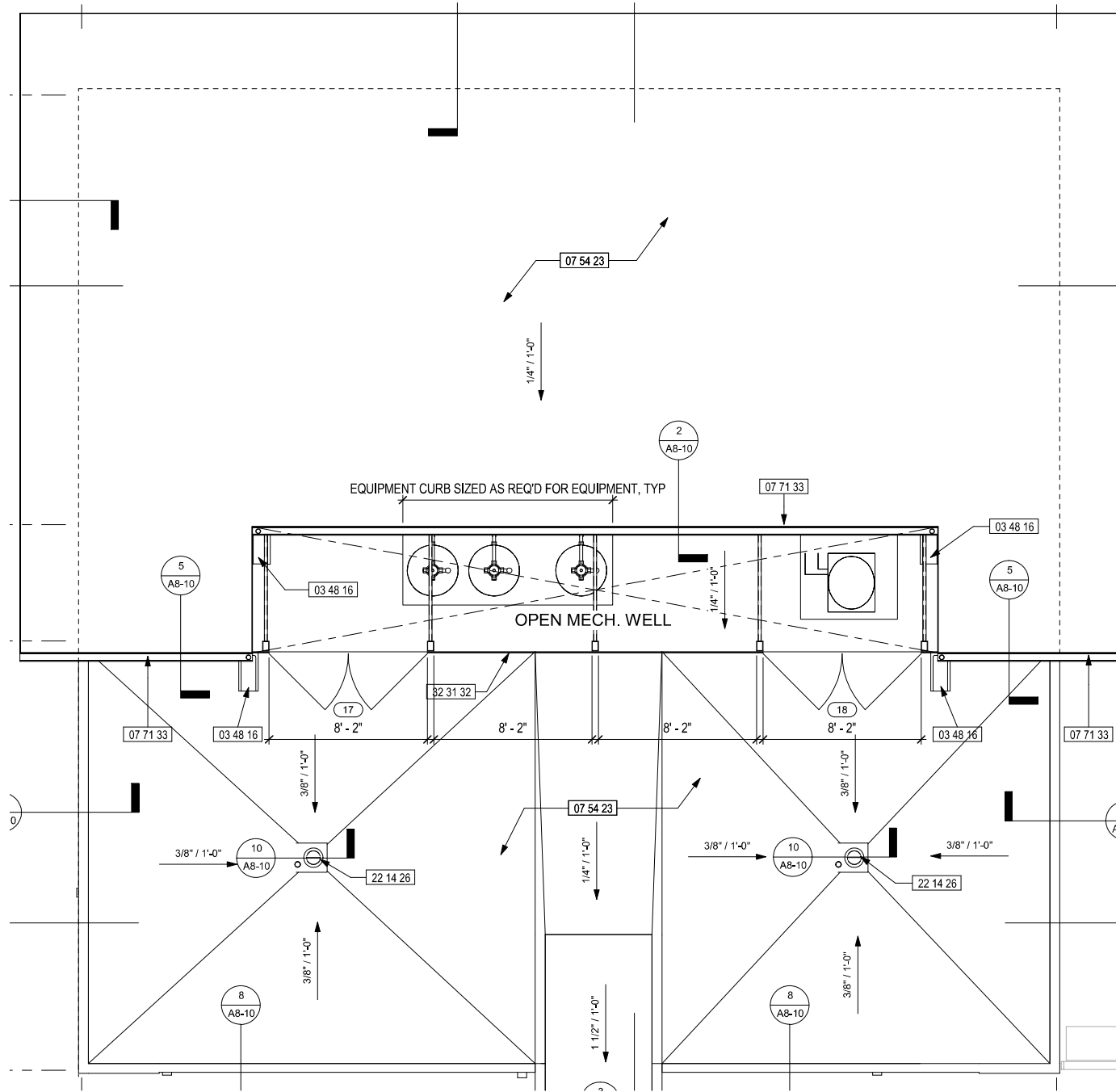
Horizontal siding color changed to match vertical siding

Siding dimension width reduced, see above

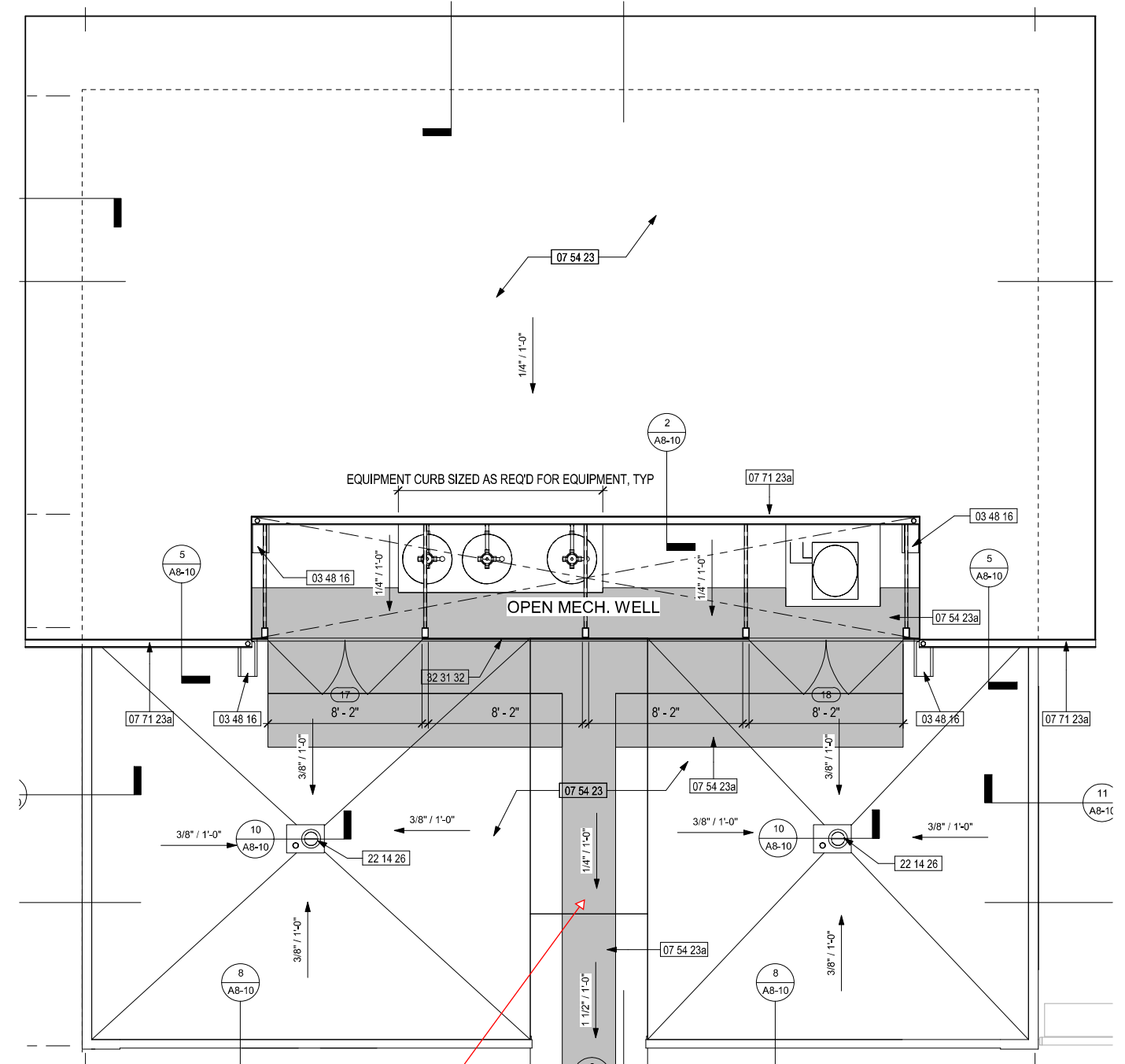
KEY NOTES

06 80 00a	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING #SFB006060144 OVER 3/4" VERTICAL BATTEN @ 16" O.C. COLOR: #1078
06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 64 00	BRAKE METAL CLADDING
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 51 10	ALUM. WINDOW - CASCADE ALUMINUM
08 91 19	FIXED LOUVER VENT
09 24 00a	PAINT (E) STUCCO - COLOR 1
22 47 13	(N) HIGH-LOW DRINKING FOUNTAIN - SPD, PROVIDE 1 DRINKING FOUNTAIN GRAB BAR EACH SIDE - BOBRICK 819298 OR EQUAL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS
32 31 32	FIBER-REINFORCED COMPOSITE FENCE & GATE - RESYSTA 6", TO MATCH HORIZONTAL SIDING





ROOF PLAN
1/8" = 1'-0"



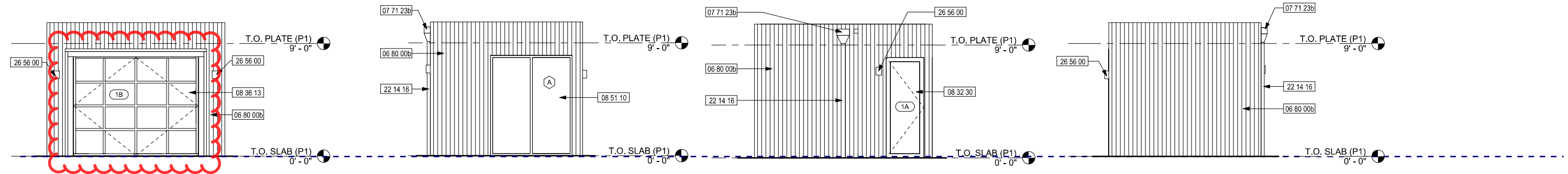
TPO walkway mat
added, shown shaded

ROOF PLAN
1/8" = 1'-0"



KEY NOTES

06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 71 23b	GSM COLLECTOR HEAD WITH FACTORY FINISH
08 32 30	ALUM. HINGED DOOR - KAWNEER SERIES AA 350
08 36 13	OVERHEAD DOOR - CROWN 50/50 HYDRAULIC LIFT
08 51 10	ALUM. WINDOW - WESTERN WINDOW SYSTEMS 600 SERIES
22 14 16	RAINWATER LEADER - TIE INTO EXISTING STORM DRAIN SYSTEM - COORDINATE WITH CIVIL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS



FRONT ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"

REAR ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"

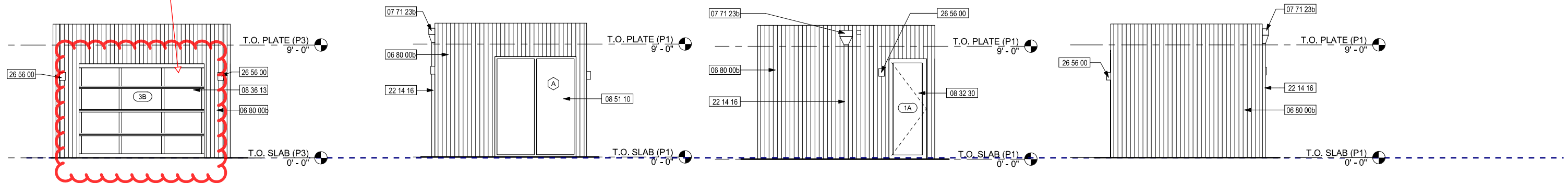
PERMIT RESUBMITTAL 2

KEY NOTES

06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 71 23b	GSM COLLECTOR HEAD WITH FACTORY FINISH
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 36 13	OVERHEAD DOOR - AMARR FULL VIEW SECTIONAL ROLL-UP DOOR
08 51 10	ALUM. WINDOW - CASCADE ALUMINUM
22 14 16	RAINWATER LEADER - TIE INTO EXISTING STORM DRAIN SYSTEM - COORDINATE WITH CIVIL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS

Door type changed to roll-up overhead, width & height slightly reduced

*Remaining elevations unaltered



FRONT ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"

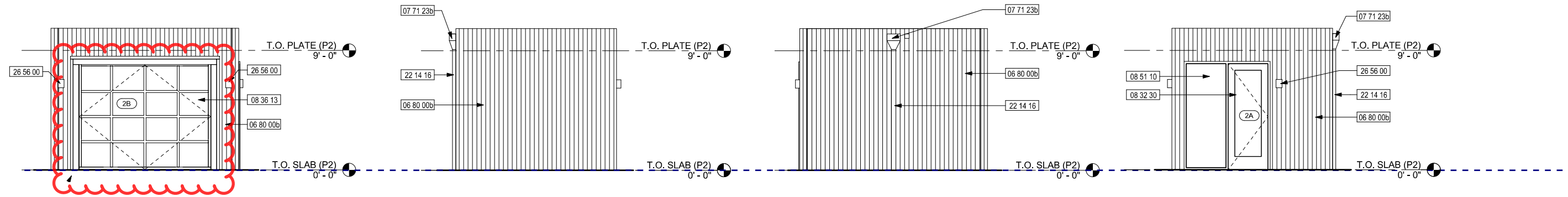
REAR ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"



KEY NOTES

06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 71 23b	GSM COLLECTOR HEAD WITH FACTORY FINISH
08 32 30	ALUM. HINGED DOOR - KAWNEER SERIES AA 350
08 36 13	OVERHEAD DOOR - CROWN 50/50 HYDRAULIC LIFT
08 51 10	ALUM. WINDOW - WESTERN WINDOW SYSTEMS 600 SERIES
22 14 16	RAINWATER LEADER - TIE INTO EXISTING STORM DRAIN SYSTEM - COORDINATE WITH CIVIL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS



FRONT ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"

REAR ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"

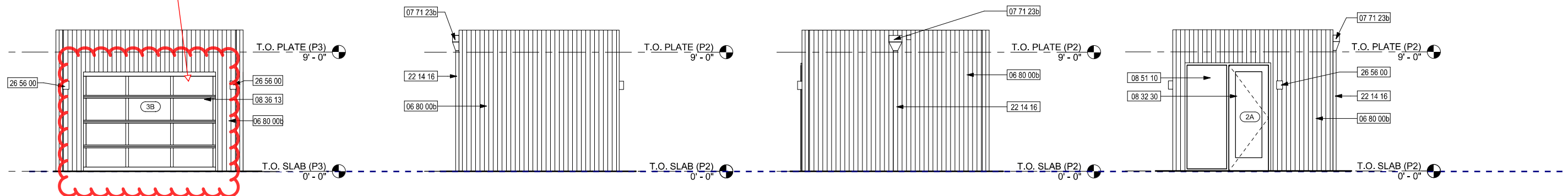
PERMIT RESUBMITTAL 2

KEY NOTES

06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 71 23b	GSM COLLECTOR HEAD WITH FACTORY FINISH
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 36 13	OVERHEAD DOOR - AMARR FULL VIEW SECTIONAL ROLL-UP DOOR
08 51 10	ALUM. WINDOW - CASCADE ALUMINUM
22 14 16	RAINWATER LEADER - TIE INTO EXISTING STORM DRAIN SYSTEM - COORDINATE WITH CIVIL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS

Door type changed to roll-up overhead, width & height slightly reduced

*Remaining elevations unaltered



FRONT ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"

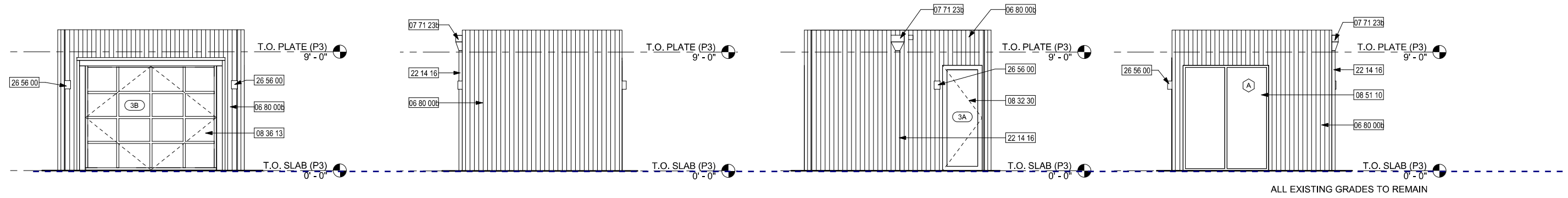
REAR ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"



KEY NOTES

06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 71 23b	GSM COLLECTOR HEAD WITH FACTORY FINISH
08 32 30	ALUM. HINGED DOOR - KAWNEER SERIES AA 350
08 36 13	OVERHEAD DOOR - CROWN 50/50 HYDRAULIC LIFT
08 51 10	ALUM. WINDOW - WESTERN WINDOW SYSTEMS 600 SERIES
22 14 16	RAINWATER LEADER - TIE INTO EXISTING STORM DRAIN SYSTEM - COORDINATE WITH CIVIL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS



FRONT ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"

REAR ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"

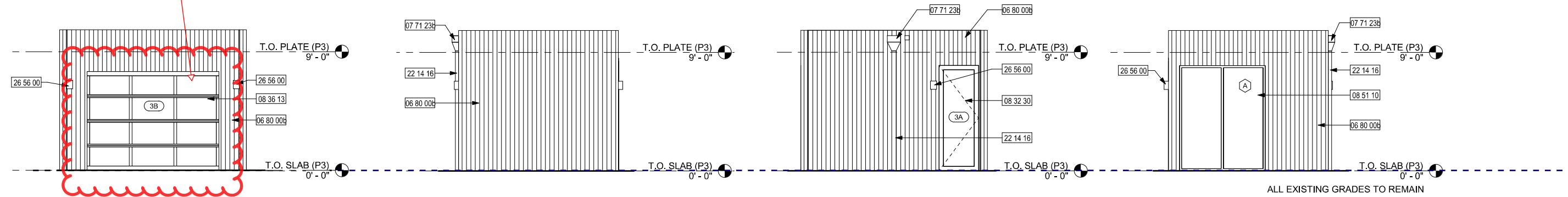
PERMIT RESUBMITTAL 2

KEY NOTES

06 80 00b	FIBER-REINFORCED HYBRID SIDING - RESYSTA 6" SIDING, VERTICAL, #SFB006060144, COLOR: #1078
07 71 23b	GSM COLLECTOR HEAD WITH FACTORY FINISH
08 32 30	ALUM. HINGED DOOR - CASCADE ALUMINUM
08 36 13	OVERHEAD DOOR - AMARR FULL VIEW SECTIONAL ROLL-UP DOOR
08 51 10	ALUM. WINDOW - CASCADE ALUMINUM
22 14 16	RAINWATER LEADER - TIE INTO EXISTING STORM DRAIN SYSTEM - COORDINATE WITH CIVIL
26 56 00	EXTERIOR LIGHTING FIXTURE, SEE ELECTRICAL DRAWINGS

Door type changed to roll-up overhead, width & height slightly reduced

*Remaining elevations unaltered



FRONT ELEVATION
1/8" = 1'-0"

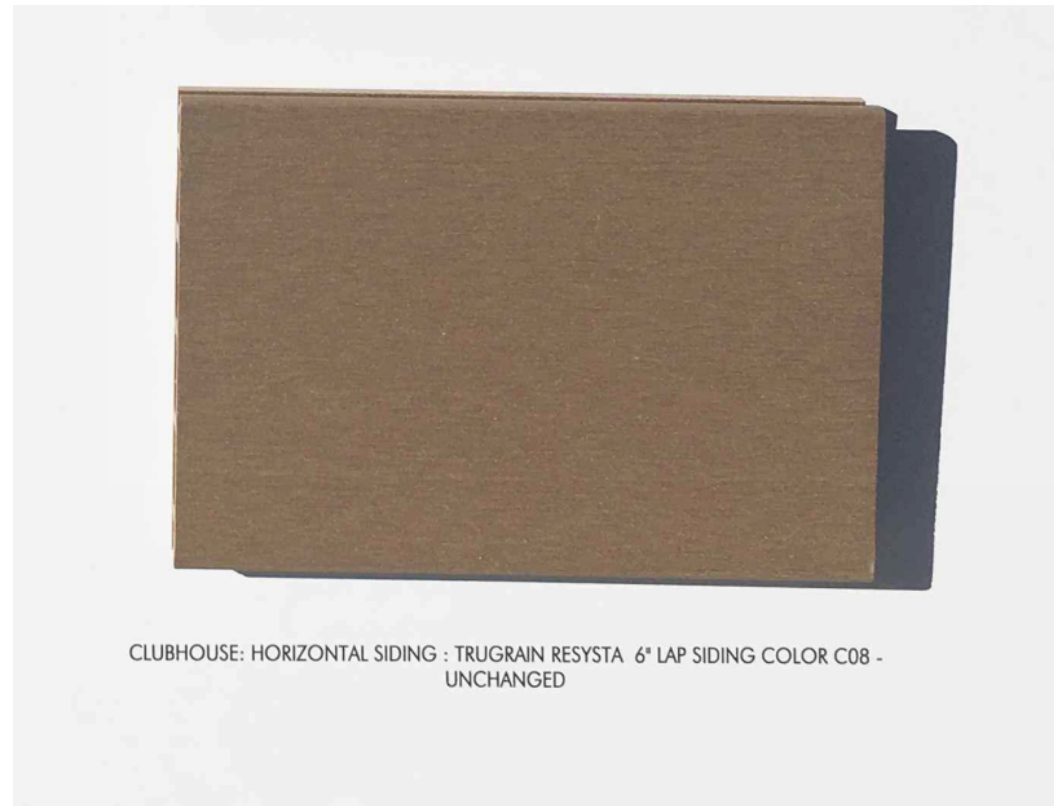
SIDE ELEVATION
1/8" = 1'-0"

REAR ELEVATION
1/8" = 1'-0"

SIDE ELEVATION
1/8" = 1'-0"



Sample



Installed



Original

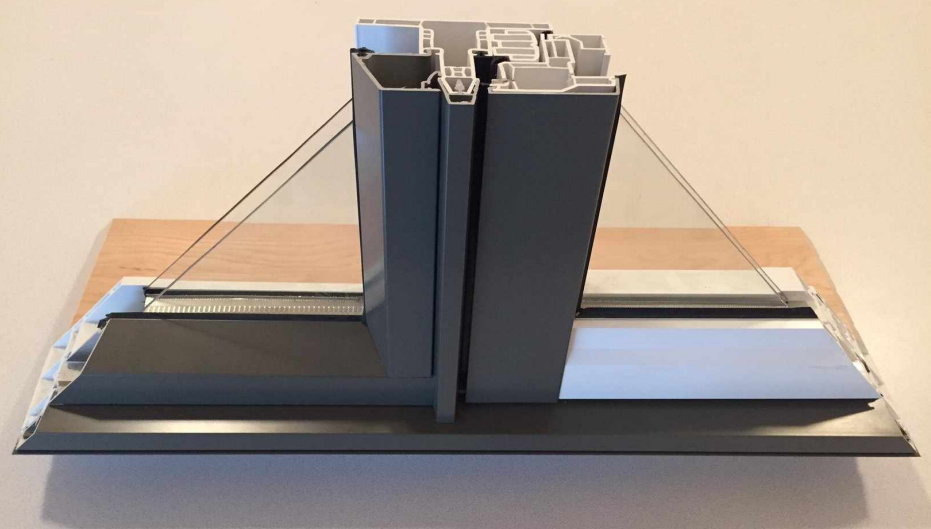


Revised



Sample

Installed



Original



Revised



STAFF REPORT

Planning Commission

Meeting Date: 10/16/2017

Staff Report Number: 17-064-PC

Regular Business: **Conditional Development Permit Amendment, Development Agreement Amendment, and Environmental Review/Hibiscus Properties, LLC/301-309 Constitution Drive**

Recommendation

Staff recommends that the Planning Commission review and provide a recommendation that the City Council make the necessary findings and take action for approval of the proposed Revised Facebook Campus Expansion Project (referred to herein as the proposed project) located at 301-309 Constitution Drive as outlined in Attachment A. The Planning Commission should provide a recommendation to the City Council on the following entitlements and components of the proposed project:

- **Environmental Review** that analyzed the proposed project for consistency with the Facebook Campus Expansion Project Environmental Impact Report (EIR) that was certified in November 2016.
- **Second Amended and Restated Conditional Development Permit** to modify the Amended and Restated Conditional Development Permit, which was approved in November 2016, including revisions to the building footprint for Building 22, modifications to several of the development standards for Building 22, construction of an eight-story parking garage (seven stories above finished grade and one level below the finished grade), retention of Building 305 during construction of Building 22, the addition of an electrical vehicle charging facility for intra-campus trams, shuttles, and buses, and incorporation of additional landscaping once Building 305 is removed.
- **Amendment to the Development Agreement** to ensure consistency with the timing of benefits associated with the approved project and the provision of additional funds for city services.

The environmental analysis determined that the proposed project would not result in new impacts or an increase in the severity of previously identified impacts. As a result, an addendum to the certified EIR has been prepared for the proposed project. No action is required on the addendum, but the document should be used in the Planning Commission's evaluation of the proposed project. The Facebook Campus Expansion Project EIR Addendum is available on the City-maintained project page:

<http://menlopark.org/1012/Environmental-Impact-Report>

Copies of the addendum are also available for review at the Community Development Department, the Main Branch of the Library, and the Belle Haven Branch Library.

Further, since a Fiscal Impact Analysis (FIA) was prepared for the approved project, an addendum to the FIA was prepared by BAE to inform decision makers and the public of the potential changes to the fiscal

impacts of the proposed project. The addendum to the FIA is included as Attachment I. If the Planning Commission recommends that the City Council approve the proposed project, then staff would prepare resolutions for the actions listed above in advance of the City Council meeting on the project.

Policy Issues

The proposed Second Amended and Restated Conditional Development Permit requires the Planning Commission and City Council to consider the merits of the proposed project, including consistency with the City's General Plan, Municipal Code, and other adopted policies and programs. The Commission and Council will also need to consider the proposed development standards and revised project phasing in the requested Second Amended and Restated Conditional Development Permit. As part of the project review, the Commission and Council will also need to evaluate the associated modifications to the Development Agreement for consistency with the approved project's anticipated timeline for delivery of the public benefits as well as added benefits proposed by the applicant. The Planning Commission is the recommending body to the City Council on the proposed project.

Background

Site location

Using Bayfront Expressway in an east to west orientation at the project site and Willow Road in north to south orientation, the project site extends from the corner of Chilco Street and Bayfront Expressway east toward Building 20, located at 1 Facebook Way, near the intersection of Willow Road and Bayfront Expressway. Collectively the project site (301-309 Constitution Drive), Building 20 (1 Facebook Way), and Building 23 (300 Constitution Drive) are referred to as the Facebook West Campus. Chilco Street wraps around the western side and a portion of the southern side of the property. Bayfront Expressway is adjacent to the campus on the north and south of the former salt ponds that are the subject of a forthcoming restoration project. To the west are commercial and industrial uses within the O (Office) zoning district, including the Facebook occupied buildings at 180-200 Jefferson Drive. To the south, across the Dumbarton Rail Corridor and Chilco Street, are the Onetta Harris Community Center and Menlo Park Senior Center, Beechwood School, Menlo Park Fire Protection District Station 77, single-family residences (R-1-U zoning district), and single-family residences in the Hamilton Park housing development (R-3-X zoning district). A location map identifying the entire Facebook West Campus is included as Attachment B.

Project History

In March 2015, Hibiscus Properties, LLC, a wholly owned subsidiary of Facebook, Inc., submitted an application for the redevelopment of the former TE Connectivity Campus (301-309 Constitution Drive) known as the Facebook Campus Expansion Project. Prior to the submittal of the application for comprehensive redevelopment of the TE Campus, in December 2014, the Planning Commission approved a use permit to convert an existing approximately 180,108 square foot warehouse and distribution building to offices and ancillary employee amenities, now referred to as Building 23. Building 23 is located at 300 Constitution Drive, near the Constitution Drive entrance to the site along Chilco Street. Construction for Building 23 is complete and Facebook currently occupies the building. The City Council approved the entitlements and agreements for Building 20 in March 2013. Building 20 is located on the former 312-314

Constitution Drive portion of the West Campus.

In November 2016, the City Council approved the land use entitlements and Development Agreement and certified the EIR for the Facebook Campus Expansion Project (referred to herein as the approved project), which encompassed the remainder of the TE Campus commonly referred to as 301-309 Constitution Drive. The approved project included two new office buildings (Buildings 21 and 22) encompassing approximately 962,400 square feet and a 200-room limited service hotel of approximately 174,800 square feet. The approved project resulted in an increase of 121,300 square feet of gross floor area at the project site and the total build out (including Building 23) would result in approximately 1.3 million square feet of gross floor area. Since Building 23 was previously approved (300 Constitution Drive) it is not considered part of the approved project; however, for purposes of calculating FAR, building coverage, etc. Building 23 is included in the site calculations.

After project approval, the City Council approved the rezoning of the property from M-2(X) (General Industrial, Conditional Development) to O (Office) as part of its adoption of the ConnectMenlo General Plan and M-2 Area Zoning Ordinance update. However, as identified in the Development Agreement, the Conditional Development Permit (CDP) and all other land use entitlements for the Facebook Campus Expansion Project continue to regulate the development at the site. In December 2016, the City issued the permit for construction of Building 21. At this time, Building 21 is currently under construction and completion is anticipated in mid-2018.

In February 2017, Facebook submitted an application for a CDP amendment, Development Agreement amendment, and associated environmental review for modifications to the approved project. The Planning Commission reviewed the proposed project at a study session in May 2017 and provided guidance and feedback on the proposal to the applicant team and City staff. The Planning Commission's direction and the applicant's response are discussed later in this report.

Analysis

Project description

The approved project was expected to be constructed in multiple phases. Demolition of Buildings 307-309 and the subsequent construction of Building 21 would be completed in the first phase. Construction of the first phase was anticipated to begin in fall 2016. The demolition of the remaining buildings on site (buildings 301-306 and the chemical transfer facility or "CTF") and construction of Building 22 and the hotel would be constructed in the second phase. Construction of the second phase was anticipated to begin in early 2018. While Building 22 was anticipated to be constructed before the hotel, the two buildings could have been constructed in either order. At this time, the first phase of construction is underway.

Based on the approved project plans, the buildings at 301-306 Constitution Drive and the CTF building would need to be demolished to allow for the construction of Building 22 (Phase 2). Therefore, TE Connectivity (TE), would need to completely vacate the site to allow for the construction of Building 22. Facebook has stated that its current lease agreement with TE extends to September 2022, with options for TE to leave prior to the end of the current lease agreement. At the time of the City Council's approval of

the approved project in November 2016, the applicant anticipated TE vacating all of its buildings prior to the end date of its lease agreements. However, since then it has become apparent that TE would not vacate on the anticipated timeline and could be in Building 305 through September 2022 (based on its current lease). As a result, Facebook evaluated options to modify the project site plan to allow for Building 305 to continue to be located on site and occupied by TE, while allowing for the concurrent construction and occupancy of Building 22. Under the approved project's site plan, if TE and Building 305 remain on site, construction of Building 22 could not begin until after the lease expires in September 2022 and TE vacates the site.

Therefore, on February 7, 2017, Hibiscus Properties, LLC on behalf of Facebook, submitted an application to amend the CDP for the Facebook Campus Expansion Project and commence the associated environmental review. The applicant's project description letter is included in Attachment C and the project plans are included in Attachment D. The proposed project includes the following modifications to the Amended and Restated Conditional Development Permit:

- Modify the design of Building 22 to encompass a four-story building of approximately 449,500 square feet of gross floor area (maximum approved under previous CDP) with a reduced building footprint;
- Relocate the surface parking beneath Building 22 into a stand-alone multistory parking garage structure (seven stories above finished grade and one level below the finished grade);
- Increase the maximum height of Building 22 to approximately 87 feet from 75 feet to allow for specific elements of the building (skylights), with an allowance for skylight maintenance platforms, to extend approximately seven to 10 feet above the skylight for a total maximum height of 97 feet;
- Construct a new electric vehicle charging facility (encompassing approximately 97,500 square feet of site area) for Facebook's potential future fleet of electric shuttle buses and trams after demolition of Building 305;
- Increase the amount of private landscaped open space by approximately five acres and increase the size of the publicly accessible open space to 2.2 acres after the demolition of Building 305;
- Retain Building 305 and associated manufacturing operations in an interim phase prior to TE's lease expiration; and
- Modify the timeline for delivery of the full extent of the publicly accessible open space to accommodate the retention of Building 305 in the interim phase.

Section 6 (Modifications) of the Previously Approved Project defines the review process for modifications to the approved project. The review process includes four distinct scenarios based on the extent of the proposed project revisions:

Section	Title	Acting Body
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6.1.1	Substantially Consistent Modifications	Planning Division Staff
6.1.2	Minor Modifications	Planning Division Staff with notification to Planning Commission and subject to Commissioner request for additional review
6.1.3	Major Modifications	Planning Commission
6.1.4	Design Review	Planning Commission; Limited to review of architectural review of Building 22 and Hotel, provided project plans consistent with CDP
6.1.5	CDP Amendments	City Council, with review and recommendation by Planning Commission

Section 6.1.5 (Conditional Development Permit Amendments) states that a CDP amendment is required for the following modifications:

- The relaxation of development standards outlined in Section 2 (of the CDP);
- Material changes to the uses identified in Section 3 (of the CDP); or
- Material modifications to the conditions of approval identified in Sections 7, 9, 10, 11, 12, 13, 14, and 15 (of the CDP).

The proposed revisions to the project would result in material modifications to the conditions of approval, modifications of allowed uses (parking garage structure and on-site electrical vehicle recharging facility and vehicle storage), and the relaxation of development standards. Therefore, staff determined that the proposed revisions require a CDP amendment. In addition to amending the CDP, the Development Agreement (DA) for the project would also need to be revised to ensure that the DA is consistent with the amended CDP and to ensure that the expected timing for economic benefits negotiated with the approved project are realized.

The proposed project would comply with the maximum floor area ratio (FAR), maximum building coverage, minimum parking spaces, and minimum setbacks of the approved project. The proposed project would reduce the overall building coverage. In addition, the proposed project would increase the landscaping on-site with the reduced footprint of Building 22. The proposed project would comply with the FAR limits of the previously approved Amended and Restated Conditional Development Permit. However, the proposed project would result in an increase in height for specific features of Building 22. The table below identifies the development standards of the approved project and the proposed modifications.

Conditional Development Permit Amendment (Development Standards Comparison Table)

Development Standard (Conditional Development Permit)	Approved Project (Buildings 21, 22, 23, and Hotel)	Proposed Project (Buildings 21, 22, 23, and Hotel)
Front Setback	Min 40 feet	Min 40 feet
Side Setback	Min 40 feet	Min 40 feet
Rear Setback	Min 40 feet	Min 40 feet
Lot Coverage	55 percent	<i>44 percent*</i>
Floor Area Ratio (FAR)	45 percent Offices; 55 percent Other Uses (Hotel)	45 percent Offices; 55 percent Other Uses (Hotel)
Height	75 feet	<i>87 feet for Building 22* (Skylight)</i> <i>97 feet for Building 22* (Skylight Maintenance Platform)</i>
Parking	3,533 spaces	3,533 spaces; 3,288 spaces in interim (does not include parking for the hotel or existing spaces associated with Building 305)

*Bold italics represent changes of the proposed from the approved project.

The proposed project would allow Building 22 to move forward while TE continued to operate within Building 305. Since the hotel could not be built prior to demolition of Building 305 (which could remain occupied until September 2022), the revised project would likely extend the timeline for delivery of the hotel. As part of the proposed project, the applicant has committed to demolishing Building 305 within one year from when TE vacates the site and also has agreed to not renew or extend TE’s current lease to ensure that TE vacates the site by September 2022 at the latest. The proposed project would create an additional phase, referred to as the interim phase. The interim phase would include the demolition of Building 301-304, 306 and the CTF, allow for the retention of Building 305, and include the construction of Building 22. The interim phase of the proposed project includes the completion of the bicycle and pedestrian bridge and the partial completion of the publicly accessible open space. The final phase would include the demolition of Building 305, the construction of the hotel and the electrical vehicle charging facility, and the completion of the expanded extent of the publicly accessible open space. Since Building 305 would remain on site in the interim phase, the proposed amended and restated CDP includes clarifications that the Trip Cap applies to all existing building on site, in addition to the proposed buildings.

The approved project included a commitment from the applicant to utilize Environmental Protection Agency Tier 4 emissions compliant engines for all off-road diesel engines. However, during construction of Building 21 it was determined that there are a limited number of Tier 4 pile rigs available on the west coast. Therefore, the applicant proposed retrofitting Tier 2 and Tier 3 compliant pile rigs and submitted documentation to the City that the retrofitted rigs would be consistent with Tier 4 compliant emissions standards. This was done as a substantially consistent modification after showing that there would be no additional emissions not analyzed as part of the environmental review for the approved project. Anticipating that this will continue to be an issue, the applicant has requested to memorialize this approach in Item 9.39 of the Second Amended and Restated CDP as part of the proposed project.

Planning Commission Study Session

At its meeting on May 22, 2017, the Planning Commission received a presentation from the applicant team, took public comment on the proposed project, asked clarifying questions of the applicant team, and provided comments on the proposal to the applicant and staff. The minutes for the May 22 Planning Commission meeting are available at the following link:

http://menlopark.org/AgendaCenter/ViewFile/Minutes/_05222017-2927

In general, the Planning Commission was supportive of the architectural design for Building 22, but raised concerns about the height, massing, and exterior facade of the parking garage structure. Further, the Planning Commission asked that the applicant evaluate options for reducing the prominence of the roof mounted equipment enclosures and suggested that the applicant refine the publicly accessible open space to encourage visitors to utilize the entire extent of the open space. In addition to the Planning Commission's comments on the project, one member of the public spoke in favor of the proposed revised design.

The applicant submitted a letter outlining the applicant team's revisions to the project based on the Planning Commission's guidance at the study session. The letter is included as Attachment E. The applicant states that the following changes, which are described in more detail throughout the report, were made to the proposed design program in response to the Planning Commission's comments:

- Parking structure foundation was lowered by approximately six feet, which resulted in the safety railing being within the 75 foot height limit of the Amended and Restated Conditional Development Permit;
- The solar photovoltaics above the parking garage were set back to reduce the appearance of an additional story;
- Green walls on parking structure façades were removed, resulting in more natural light within the structure and a more transparent design;
- Increased landscape plantings in front of the parking structure (north and south facades) including large trees;
- Added a limited number of façade planters to the façades of the parking garage to provide screening; and
- Refined the publicly accessible open space design to enhance opportunities for passive recreation and create a more inviting space.

The City received a comment letter from the Citizens to Complete the Refuge on the proposed project. That comment letter was provided to the Planning Commission as part of its study session and included questions about the applicability of Mitigation Measure BIO-1 from the ConnectMenlo General Plan Update. While mitigation measures from the General Plan Update are not applicable to the approved project or the proposed project, the applicant has agreed to voluntarily comply with the requirements, as applicable, in BIO-1 of the ConnectMenlo EIR and that requirement is included in Item 9.48 of the proposed Second Amended and Restated CDP.

Revised Site Plan

As outlined earlier in this report, the approved project would have been constructed in two phases. Phase

1, which is currently under construction, includes the construction of Building 21. In addition, Facebook is required to apply to Caltrans and other applicable reviewing agencies for the appropriate permits for the bicycle and pedestrian bridge over Bayfront Expressway during Phase 1. Phase 2 included the construction of Building 22 and the hotel, along with the bicycle and pedestrian bridge and publicly accessible open space. Under the approved project, the bicycle and pedestrian bridge and publicly accessible open space would be required to be completed prior to occupancy of Building 22. With the proposed project, the bicycle and pedestrian bridge would continue to be required to be completed prior to occupancy of Building 22; however, the full extent of the publicly accessible open space would not be completed prior to occupancy of Building 22, but rather would be completed in two phases: the interim phase prior to occupancy of Building 22 and the final phase within 18 months of the date TE vacates the site. The publicly accessible open space is discussed in more detail later in the report.

The proposed site plan modifications would shift the footprint of Building 22 north of Building 305 toward Bayfront Expressway. The proposed building would still be set back 95 feet from Bayfront Expressway. The proposed Building 22, like Buildings 21 and 20, would be oriented east-to-west along Bayfront Expressway. The applicant is proposing to relocate the at-grade parking into a multistory parking garage structure that would be located to the west of Building 22 and along Bayfront Expressway. The parking garage would accommodate approximately 1,736 parking spaces for Building 22 and Building 23. The interim and final site plans would accommodate the minimum required parking spaces, consistent with the Amended and Restated CDP. The parking garage would be accessed from the intersection at Chilco Street and Constitution Drive. The parking associated with Buildings 22 and 23 and the future hotel would be accessed solely from the entrance at Constitution Drive, which is consistent with the approved project. In the interim phase, the site plan would continue to include Building 305; however, after demolition of the building, the electrical vehicle charging facility and additional landscaping would be incorporated into the site plan. The additional landscaping would include approximately five acres for employee use, including pedestrian pathways connecting the buildings, and the additional acreage associated with the publicly accessible open space.

The hotel would remain located in the northwest corner of the site, near the corner of Chilco Street and Bayfront Expressway. The applicant is not proposing any changes to the hotel at this time and the hotel would be subject to a future architectural control review by the Planning Commission. The hotel could not be constructed until demolition of Building 305, due to the floor area ratio (FAR) limits on the property. Upon demolition of Building 305, approximately five acres of private landscaping and open space for employees and a new electric vehicle charging facility for buses, shuttles, and trams would be constructed in the general area of Building 305. The electric vehicle charging facility would encompass approximately 97,500 square feet of site area and could include solar photovoltaic canopies. While the design of electric vehicle charging facility is preliminary, it is anticipated that it could accommodate up to approximately 66 shuttle buses and up to approximately 12 trams. However, the actual ratio of buses, shuttles, and trams is not known at this time and would be further refined through the review process for the electric vehicle charging facility. It is also anticipated that shuttle buses would charge between 10:00 a.m. and 3:00 p.m. as well as between 8:00 p.m. and 5:00 a.m. Trams would likely charge at the facility overnight from 6:00 p.m. to 6:00 a.m. The electrical vehicle charging facility and associated landscaping would require a separate architectural control review by the Planning Commission as outlined in Section 6.1.4 of the proposed Second Amended and Restated CDP.

Design and Materials

The proposed modifications would result in a smaller footprint for Building 22 with four levels instead of a single large floor plate with a smaller mezzanine. Building 22 would contain the same amount of gross floor area as the approved project. Per the approved project, Building 22 would have been designed similarly to Buildings 20 and 21 with a large main level, mezzanine space, usable area on the roof deck level, and at-grade parking below the main level of the building. The proposed building would be designed around an enclosed atrium that would extend lengthwise through the building, creating an internal open space, allowing for natural light within the building. The atrium would be enclosed in a glass skylight that would run the length of the building. The proposed Building 22 would include reduced floor to ceiling heights, which would allow the majority of the building to comply with the 75 foot height limit of the Amended and Restated Conditional Development Permit, with the exception of the skylight over the atrium that would extend beyond the maximum height limit. The skylight also has two access platforms for maintenance that would further extend beyond the total height of the skylights. The increase in height is discussed later in the report. The skylight maintenance access platforms would be galvanized steel painted grey to match the skylight. In contrast to Buildings 20 and 21, the revised Building 22 would have an at-grade first floor instead of being constructed on a podium over surface parking. Since the proposed project would include an occupied ground floor instead of open parking, the applicant has voluntarily included a vapor barrier in the construction of Building 22 to ensure no volatile organic compounds (VOCs) from the soil would enter the building, even though sampling didn't identify unsafe levels of VOCs within the footprint of Building 22.

The building massing would be broken up with multiple step backs and landscaping on the exterior terraces, specifically along the southern façade of the building. Building 22 would generally be clad in glass curtain walls with ceramic frit glazing, insulated metal panels, and standing seam metal panels. The insulated and standing seam metal panels would include both grey and white panels, similar to Buildings 20 and 21. The skylight enclosure would be composed of grey metal panels. The glass curtain walls would have grey painted aluminum mullions. The entryways on all levels would contain wood panel decking. The materials and colors for Building 22 would be comparable to those utilized on Building 20 and 21. The exterior staircases along the building's facades would have two-inch welded wire mesh for the railings. Multiple exterior staircases would be located on the north and south facades and could help break up the massing and provide visual interest. There would also be exterior staircases on the east and west facades but given the size of those elevations the number of staircases would be limited in comparison to the other elevations. Building 22 would be connected to Building 21 through an open air bridge and also would be connected to the proposed parking garage through open air bridges on each floor (three bridges total).

There would be three canopy structures along the south side of the revised Building 22. One canopy (the west canopy) would be located adjacent to the southwest corner of Building 22 adjacent to the dining facility and the large conference/meeting room. The west canopy would be constructed in the first phase. Two additional canopies would be located across the EVA from Building 22 and would be constructed after demolition of Building 305. The center canopy would contain a seating area and the east canopy would be connected to an amphitheater. This portion of the site would be for employees only. The canopies would be heavy timber structures and utilize corrugated metal for the roofs and count towards the site's building coverage. The west canopy would be approximately 30 feet in height and the center and

east canopies would be 24 and 23 feet in height.

A color and materials board will be distributed at the Planning Commission meeting.

Roof Mounted Equipment, Emergency Generators, and Screening

The zoning ordinance permits mechanical equipment to be located on the roof of a building provided that such equipment shall be screened from view as observed at an eye level horizontal to the top of the roof-mounted equipment. The proposed screening for the rooftop mechanical equipment would be composed of two layers of half-inch metal mesh panels, separated by approximately 14 inches from one another. The dual panels would generally obscure the mechanical equipment behind the screen, but would not be technically compliant with the Zoning Ordinance requirement for roof mounted equipment screening. The Planning Commission previously raised concerns about the bulk and visibility of the roof-mounted equipment. The applicant states that the design of Building 22 contains a significant amount of glazing on the façades and white metal panels, which give the building a “lighter” feel. As such, the applicant is requesting to utilize the metal mesh for the mechanical equipment screening, stating that opaque metal panels would result in more visually prominent elements being placed on the roof of the building, which could increase the potential visual impact of the building. Therefore, the applicant is requesting to use the CDP to modify the screening requirements for the proposed project.

The mechanical equipment enclosures would be approximately 20 feet in height along the southern façade of the building and up to approximately 26 feet in height along the northern façade of the building (the Bayfront Expressway façade). The height of the screening does not count towards the maximum height of the building. Due to the size of the mechanical equipment required for the building and the associated enclosures, fully opaque screening could potentially be more visually impactful than metal mesh. Therefore, in this specific building design, staff believes a deviation from the zoning ordinance requirement would be appropriate.

The at-grade generator is proposed to be screened with a curved concrete wall clad in corrugated metal along the south side of the generator. The other sides of the generator would be enclosed in wire mesh. The generator would be located at grade, along the southern façade of the parking garage. In addition to the proposed concrete and corrugated metal screening wall, the generator would be further screened by redwood trees and other landscaping, and would not be visible from the public right-of-way or from neighboring properties given the location on the site.

The applicant has also proposed to locate a smoke ventilation system on the roof of Building 22. The proposed system would include exhaust fans that would be used to expel the smoke from the building in the event of a fire. The applicant has indicated that the exhaust fans would need to be tested on a schedule determined by the Fire Marshal. The proposed exhaust fans, associated with the smoke ventilation system, would exceed the noise limitations of the zoning ordinance for roof mounted equipment, which is 50 decibels at 50 feet. However, the exhaust fans would comply with the noise ordinance limitations at the nearest residential property line, which is 60 decibels during the daytime hours and 50 decibels during the nighttime hours. The applicant is requesting to permit the exhaust fans to exceed the zoning ordinance noise limitations for roof-mounted equipment during routine testing through the Second Amended and Restated CDP. The proposed exhaust fans would be required to be tested

Monday through Friday between the hours of 8:00 a.m. to 6:00 p.m. and would be required to comply with the noise ordinance requirements of Chapter 8:06 of the Municipal Code, per the proposed amendments to the CDP. These requirements are consistent with the noise limitations and testing schedule requirements for the emergency generators enumerated in the Second Amended and Restated CDP.

Site Parking

The approved site plan includes 3,533 parking spaces for Buildings 21, 22, 23 and the hotel in surface parking lots. The proposed office buildings and hotel in the approved project would be built on podiums over surface parking. In the revised project, Building 22 would include an at-grade first floor, which would displace parking. The proposed revisions would include a multi-story parking structure to accommodate the loss of surface parking below Building 22. Since, the approved project did not include a multi-story parking garage structure, a CDP amendment is necessary to relocate the parking from at-grade surface lots to a parking garage structure. The parking structure would be constructed concurrent with Building 22 and contain all required parking for Building 22, as well as parking for Building 23.

The parking garage structure would be approximately 72 feet, four inches in height to the top of the railing, which is within the maximum permitted height of 75 feet set by the CDP for all buildings on site. The parking garage includes a solar photovoltaic canopy, which is exempt from the height limit. At its study session on the proposed project, the Planning Commission raised concerns regarding the height and massing of the proposed garage. The applicant states in its response letter that there are a number of site constraints that limit the placement of the parking garage, including the low water table (which affects the depth of excavation), Building 305's lease line, the PG&E tower easement, the required EVAs for the project, the required number of parking spaces for Buildings 22 and 23 (per the approved CDP), and the building code setback requirement from Building 22. However, the applicant has made several modifications to address the Planning Commission's concerns that resulted in a height that would be below the 75 foot height limit, a simpler and more open design, a reduction in the extent of the rooftop solar PV (now set back 12 feet from edges of the structure), which reduces the associated massing of the PV, and the elimination of the green walls on the facades and mesh screening on the top parking level. In lieu of a green wall on the façade of the building, the planting plan around the parking garage has been revised to include additional landscape screening such as redwoods and poplars. The poplars would be used on the north canopy since those trees are not considered preferred perches for raptors that might use the trees to prey on the plover in the marshes across Bayfront Expressway. Plantings along the other facades would predominately be composed of redwood trees to reduce the visual massing of the parking garage.

The proposed parking structure would be concrete and include a high strength galvanized cable rail system. Select bays within the parking garage would include planters on the outside of the building to allow for a limited amount of vegetation to help add visual interest and articulation to the façade of the parking garage structure, however, the amount of vegetation on the facades has been significantly reduced. The garage structure would be connected to Building 22 with pedestrian bridges on three levels.

Increase in Height

As part of the proposed project, the applicant initially requested to increase the maximum height for the parking garage structure to approximately 83 feet (to the top of the mesh screen at the roof deck level) and Building 22 to approximately 87 feet from the previously approved maximum height of 75 feet. As stated previously, since the Planning Commission study session, the applicant has revised the height of the garage to comply with the 75 foot height limit of the CDP. Building 22 would be designed to comply with the maximum height limit of 75 feet, with the exception of the skylight over the inner atrium. The proposed skylight requires a CDP amendment to allow for the maximum height of the building to extend to 87 feet. However, the roof deck level of Building 22 would be 69.5 feet in height, which is below the maximum permitted height of 75 feet. In addition, the applicant has indicated that a skylight maintenance platform would be needed to allow for skylight maintenance. The platform would be located on a track above the skylight and would extend approximately 10 feet above the skylight to the top of the safety railing, for a total height of 97 feet. Per the zoning ordinance, roof mounted equipment and the associated screening is exempt from the height limit; however the maintenance platform is not exempt.

Trees, landscaping, and publicly accessible open space

The approved project included the removal of all 770 trees on the site, 274 of which are heritage trees. The proposed project would continue to remove the approved heritage trees and plant the minimum required number of replacement trees, per the CDP. The approved project would include the planting of a minimum of 423 heritage tree replacements. For the first phase, currently under construction, 48 heritage trees were removed and a minimum of 70 heritage tree replacements are approved to be planted. However, the approved landscape plan for the first phase of the approved project includes a significant number of additional trees that would meet the heritage tree replacement requirements. The remaining phases associated with the proposed project would remove 226 heritage trees and plant a minimum of 353 heritage tree replacements. As part of the proposed project, the City evaluated the tree disposition plans, proposed heritage tree removals, and arborist's evaluation of the health and impacts of the trees to determine if the revised site plan contains any feasible opportunities to preserve heritage trees that warrant preservation. That evaluation determined that there are no heritage trees suitable for preservation as part of the proposed project. The applicant submitted an updated arborist report (Attachment F) which documents the current health condition of the trees. The tree disposition plans in the plan set identify the updated reasons for removal. The City Arborist has reviewed the revised arborist report and supporting documents and has provided tentative approval of the removal of the trees associated with Building 22's construction phase. Tree removals associated with the hotel phase would be further evaluated for suitability for preservation, in accordance with the heritage tree removal procedures outlined in the CDP, during the architectural review for the hotel. If the City Arborist and the Planning Division determine that any heritage trees currently approved for removal during the hotel phase are viable candidates for preservation, then staff will direct the applicant to submit a heritage tree preservation plan. This process is enumerated in Item 9.9 of the proposed Second Amended and Restated CDP.

In addition to the heritage tree removals and replacements, the revised site plan would increase the amount of landscaping at the site from the approved project. The applicant has stated that the proposed project would increase the landscaping and open space at the site by five acres after demolition of Building 305. The increased landscaping and open space would be for employee use and not available to the public. However, the approximately 2-acre publicly accessible open space that would be constructed

between Buildings 21 and 22 would be increased slightly in size. In the interim phase, Building 305 would limit the ability of the applicant to construct the full extent of the public open space, resulting in an approximately 1.6 acre publicly accessible open space. During the interim phase, the public plaza would be constructed along with some of the bio-treatment areas and landscaping; however, some additional pathways and landscaping would not be able to be constructed until after demolition of Building 305 and the surrounding parking area. Therefore, the full 2-acres of the publicly accessible open space would not be constructed prior to occupancy of Building 22. The phasing is outlined in Section 12 of the proposed Second Amended and Restated CDP. In addition to the increase in size of the publicly accessible open space, the design of the open space has been further refined and the updated design includes more usable area for the public. The public open space would continue to be comprised of passive landscaping, a public plaza, landscaped bio-treatment areas (BTAs), and walking paths linking up to the bicycle and pedestrian bridge over Bayfront Expressway.

Conditional Development Permit Amendment

The proposed project requires an amendment to the Amended and Restated CDP. The proposed changes to the CDP generally include the following:

- Modifications to the project description and uses to allow an electrical vehicle charging facility, the retention of Building 305 in an interim phase, and the parking garage structure;
- Increase the height of Building 22 from 75 feet to 87 feet for the skylight and allow up to an additional 10 feet for two skylight maintenance access platforms for a total height of 97 feet;
- Modify the roof mounted equipment screening requirement to permit overlapped metal mesh with a maximum half-inch penetration instead of a solid screen;
- Allow smoke ventilation exhaust fans to exceed the Zoning Ordinance requirements for noise generated by roof mounted equipment during routine testing, provided that the exhaust fans comply with the noise ordinance requirement at the nearest residential property line;
- Specific conditions requiring demolition of Building 305 and commitment that the current lease with TE is not extended;
- Establish the timeline for demolition of Building 305;
- Voluntarily include Mitigation Measure BIO-1 from the ConnectMenlo General Plan Update into the Facebook Campus Expansion Project;
- Voluntarily install a vapor barrier below Building 22 to limit potential exposure of occupants to volatile organic compounds (VOCs);
- Update the Trip Cap associated with the amended and restated CDP to clarify that the site wide trip cap applies to all interim uses, such as Building 305.
- Clarify that the use of modified Tier 2 and Tier 3 engines for pile rigs, if Tier 4 pile rigs are not available, is equivalent to the Tier 4 emissions standards requirements.
- Identify that the publicly accessible open space would be constructed in two phases with the first phase (interim) required to be completed prior to occupancy of Building 22 and the second phase (final) within 18 months of the date the current lease with TE Building 305 terminates or expires.

In addition to the items listed above, the proposed amendments to the CDP contain a number of minor modifications for consistency and to enable the major changes outlined above. The draft of the proposed Second Amended and Restated CDP is included in Attachment G.

Development Agreement Amendments

The following changes would be made to the Development Agreement (DA):

- Timing of the in-lieu Transient Occupancy Tax (TOT) payment;
- Timing of the removal of the cap for Utility Users Tax (UUT); and
- Additional revenue (\$9 million) for city services that benefit the safety of the community.

Changes to the DA would be required to ensure consistency with the timing of the delivery of the negotiated public benefits (TOT and UUT) of the approved project, as envisioned through the previous negotiation process between the City and the applicant. The third bullet above includes a specific offer from Facebook to the City to fund city services that benefit the safety of the local community as a public benefit of the proposed project, which would be included in the modified DA. The payment shall be made in five equal installments over five years. At this time, the amount of the payment is identified as \$9 million in the draft amendment to the DA; however, the City and Facebook are engaged in ongoing discussions to determine the final amount of revenue that would be provided to the City. That amount would be determined prior to the City Council's review of the amendment to the DA.

The proposed modifications would be made through an amendment to the DA. The Term Sheet for the amendment was reviewed by the City Council as an informational item at its meeting on July 18, 2017. The draft proposed amendment to the DA is included in Attachment H.

The City prepared an addendum to the fiscal impact analysis (FIA) to assess the potential fiscal implications of the revised timeline for delivery of the hotel. While the addendum to the FIA did not assess the fiscal implications of the amendment to the DA, the FIA does provide decision makers with information on the economic implications of the proposed project. The FIA analyzed two scenarios for the construction and operation of the hotel based on the likely dates that TE would vacate the site and the permitting process for the hotel could commence. Scenario One analyzed the fiscal implications for the project if TE vacates in September 2022 and Scenario Two analyzed the fiscal implications if TE elects to terminate its lease early in September 2020. The addendum to the FIA (Attachment I) determined that the net fiscal impact at the end of the forecast period (year 2035) would be \$2.2 Million for the both the approved project and proposed project. However, the proposed project would not have a positive fiscal impact until 2024 (Scenario 1) or 2022 (Scenario 2) where the approved project's fiscal impact turned positive in 2019 with the completion of the hotel. However, the net present value of the proposed project would be \$11.4 million (Scenario 1) and \$13.8 million (Scenario 2), compared to \$17.4 million for the approved project. Therefore, the proposed project would result in lesser economic benefits to the City.

As such, staff determined that the DA would need to be modified to ensure the delivery of the expected economic benefits of the project on the anticipated timeline used to negotiate the DA for which was informed by the FIA for the approved project. Therefore, the amendments to the DA, as stated above, would ensure that the TOT guarantee would commence on a timeline more consistent with the approved project. This TOT guarantee would incentivize construction of the hotel and at a minimum, ensure that the City received the minimum \$1.25 million TOT guarantee upon the construction of Building 22. To ensure the timeline for the negotiated economic benefits of the approved project are realized consistent with the

approved DA, the guarantee commencement date for the TOT in lieu fee and the removal of the UUT cap has been revised. The guarantee commencement date has been modified to commence upon July 1 of the second full City fiscal year following the earlier of the TE Vacation Date, or the date the City issues the first building permit for Building 22. The DA for the approved calculated the Guarantee Commencement Date from the date when TE vacates the site, which was assumed to be prior to issuance of a building permit for Building 22 (or before early 2018 when construction was anticipated to commence). The proposed amendments to the DA would result in fiscal benefits to the City being realized consistent with the negotiated terms of the DA for the approved project.

Correspondence

Staff has not received any items of correspondence on the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

An EIR was prepared for the approved project. When revisions are proposed to a project after an EIR has been certified, an agency must determine whether an addendum or a supplemental EIR is the appropriate document to analyze the potential impacts of the revised project. Per CEQA Guidelines Section 15162(a), a supplemental EIR is required if:

- 1) Substantial changes are proposed in the project, which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- 2) Substantial changes occur with respect to the circumstances under which the project is undertaken, which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- 3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete, shows any of the following:
 - (A) The project will have one or more significant effects not discussed in the previous EIR;
 - (B) Significant effects previously examined will be substantially more severe than shown in the previous EIR;
 - (C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
 - (D) Mitigation measures or alternatives, which are considerably different from those analyzed in the previous EIR, would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

If none of the above conditions apply, then an addendum is the appropriate environmental document to analyze a revised project. For purposes of the proposed project, an addendum is the appropriate environmental document because at full build out, the proposed project would maintain the uses identified in the approved project, would include the same gross square footage as the approved project (approximately 1,137,200 square feet of gross floor area), would employ the same number of people (approximately 6,550 employees), and would have the same minimum number of parking spaces (3,533 spaces). Further, the Trip Cap for the approved project would apply to the proposed project. Based on these considerations, no new significant impacts or increases in the severity of previously identified significant impacts are expected to result from the proposed project, thereby rendering a supplemental EIR unnecessary. The interim condition would also not result in new significant impacts or the identification of considerably different mitigation measures or new measures that were not identified in the certified EIR. Furthermore, the EIR was certified on November 1, 2016. In the last year, there have been no substantial changes with respect to background conditions that would suggest that the circumstances under which the proposed project would be undertaken would be substantially different from those assumed or described in the certified EIR. Finally, although the City adopted a new general plan after approval of the approved project, approval of the City's new general plan would not result in any new significant impacts under the proposed project or new or considerably different mitigation measures compared to those of the approved project. Accordingly, as described further in this document, an addendum is the appropriate mechanism for CEQA review of the proposed project. The Facebook Campus Expansion EIR Addendum is available for review at the Community Development Department, the main branch of the Menlo Park Library, the Belle Haven branch of the Menlo Park Library, and online at the following link.

<http://menlopark.org/1012/Environmental-Impact-Report>

Hard copies of the Addendum were provided to the Planning Commission in advance of the public hearing for their review and consideration.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a quarter mile radius of the subject property.

Attachments

- A. Recommended Actions
- B. Location Map
- C. Project Plans
- D. Project Description Letter
- E. Response Letter from Applicant to Planning Commission
- F. Updated Arborist Report and Tree Survey Data
- G. Draft Second Amended and Restated CDP
- H. Draft Amendment to the Development Agreement
- I. Fiscal Impact Analysis for Proposed Revised Project

The previously approved CDP and DA are available for review on the City-maintained project page at the following link: <http://menlopark.org/995/Facebook-Campus-Expansion-Project>.

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

Color and Materials Board

Report prepared by:
Kyle Perata, Senior Planner

Report reviewed by:
Deanna Chow, Principal Planner

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**RECOMMENDED ACTIONS FOR PLANNING COMMISSION –
Revised Facebook Campus Expansion Project
(301-309 Constitution Drive)**

Environmental Review

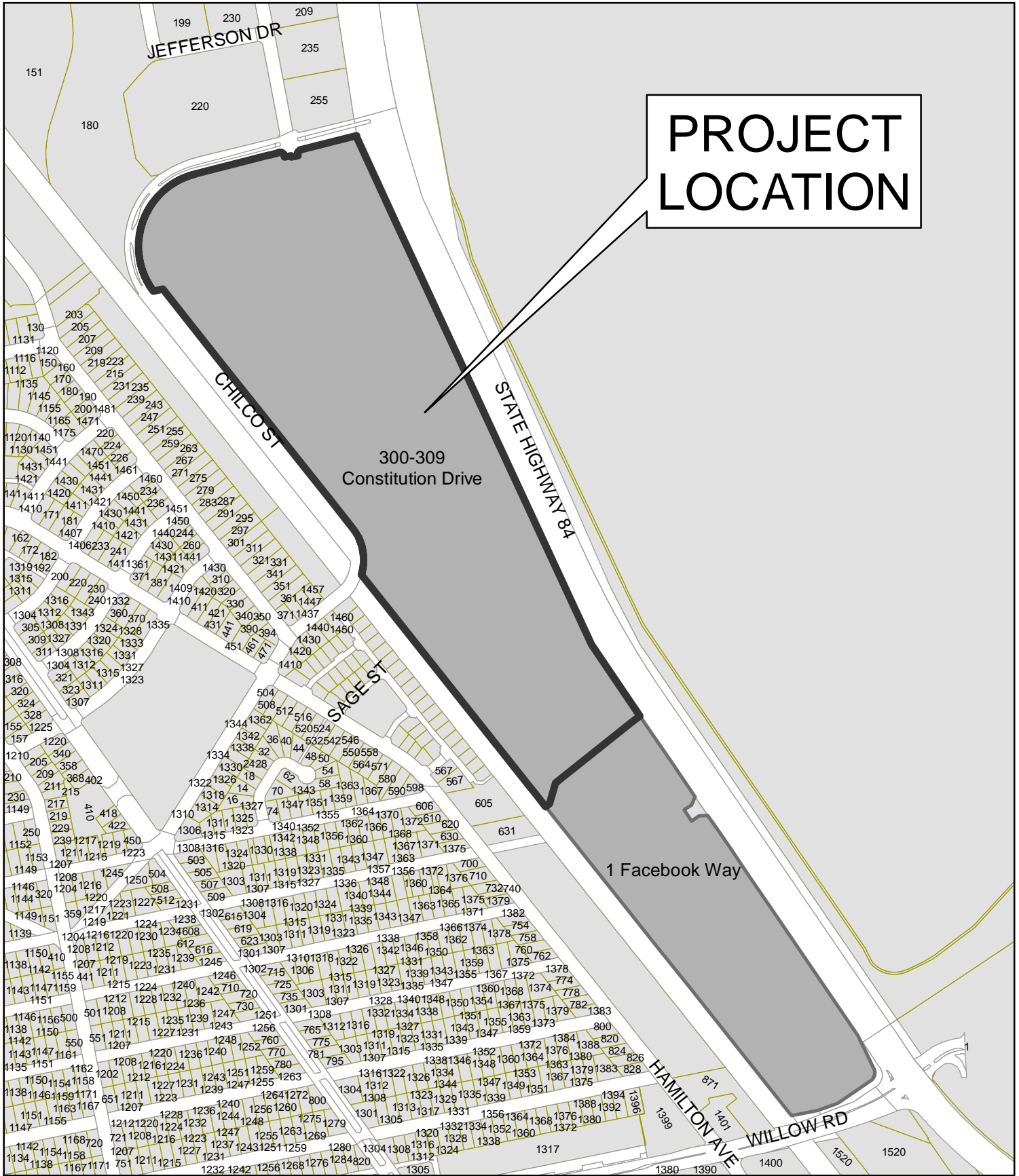
1. Recommend that the City Council find that it has considered the Addendum and that the proposed project is consistent with the certified EIR for the Approved Project as analyzed in the Facebook Campus Expansion Project EIR Addendum.

Conditional Development Permit

2. Recommend that the City Council Approve the Second Amended and Restated Conditional Development Permit for the Facebook Campus Expansion Project located at 300-309 Constitution Drive and 1 Facebook Way (Building 20) (Attachment G).

Development Agreement

3. Recommend that the City Council Approve the Amendment to the Development Agreement for 301-309 Constitution Drive (Facebook Campus Expansion Project). (Attachment H)



**PROJECT
LOCATION**

300-309
Constitution Drive

1 Facebook Way

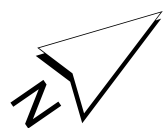



CITY OF MENLO PARK

LOCATION MAP

301-309 CONSTITUTION DRIVE & 1 FACEBOOK WAY

DRAWN: TAS CHECKED: KTP DATE: 10/16/17 SCALE: 1" = 300' SHEET: 1





FACEBOOK CAMPUS EXPANSION
BUILDING 22 DESIGN REVIEW &
REQUEST FOR CDP AMENDMENT
PROJECT 2015-007
PREPARED BY GEHRY PARTNERS
SEPTEMBER 28, 2017

FACEBOOK CAMPUS EXPANSION

*Prepared By:
Gehry Partners, LLP*

***BUILDING 22 DESIGN REVIEW &
REQUEST FOR CDP AMENDMENT***
SEPTEMBER 28, 2017

	Previously Approved Project	MPK 22 Design Review & CDP Amendment Interim Phase	MPK 22 Design Review & CDP Amendment Final Phase
SITE AREA TOTAL (SF)	3,503,608	* 3,504,041	* 3,504,041
MPK20 Site AREA (SF)	963,680	959,565	959,565
TE SITE AREA (SF)	2,539,928	2,544,476	2,544,476
Building Coverage (SF)	1,838,665	1,720,497	1,522,342
Building Coverage (%)	52%	49.10 %	43.45 %
MPK20 (GFA)	433,555	433,555	433,555
MPK21 (GFA)	512,900	512,900	512,900
MPK22 (GFA)	449,500	449,500	449,500
MPK23 (GFA)	180,108	180,108	180,108
BLDG 305 (GFA)		289,718	
Hotel (GFA)	174,800		174,800
Total Office (GFA)	1,576,063	1,576,063	1,576,063
Office FAR	0.45	0.45	0.45
Total Mixed Use (GFA)	1,750,863	1,865,781	1,750,863
Mixed Use FAR	0.50	0.53	0.50
Parking Stalls			
MPK20	1,446	1,446	1,446
MPK21	1,476	1,495	1,495
MPK22	1,294	** 0	** 0
MPK23	518	** 57	** 57
Parking Garage		1,736	1,736
Hotel	245		245
Total Parking Stalls	4,979	4,734	4,979

Notes:

* Increase in square footage for the site is caused by new survey from Kier and Wright. The site area of the previously approved project has been based on a survey from BKF. For more information please review Sheets A0-02 to A0-04.
 ** Required minimum parking for Building 22 and 23 is incorporated into the garage.

GEHRY PARTNERS, LLP
 ARCHITECT
 1000 WEST 10TH STREET
 SAN ANGELO, TEXAS 76901
 (817) 841-1000

FACEBOOK
 OWNER
 1 ZION WAY
 MENLO PARK, CALIFORNIA 94025

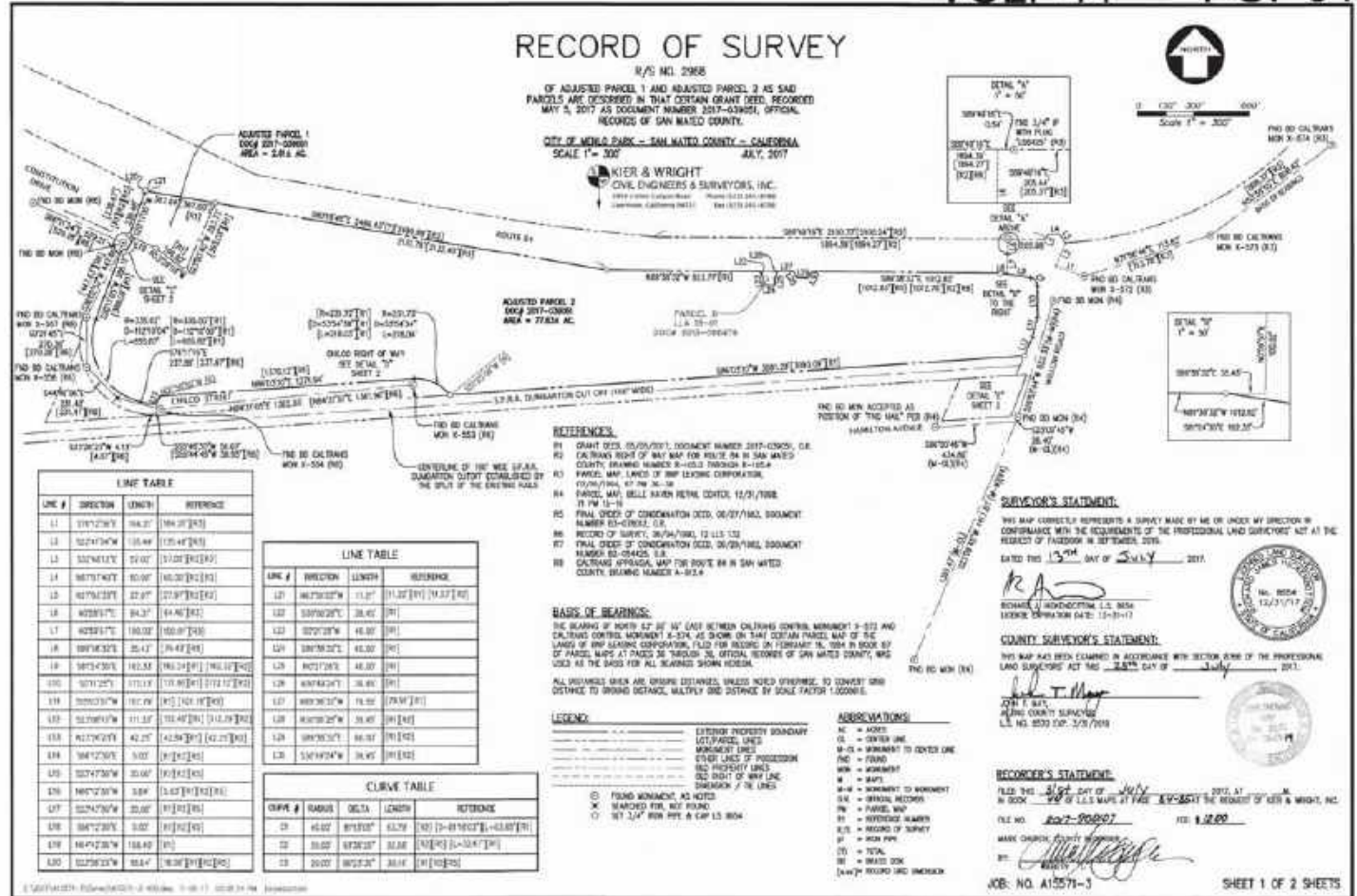
DATE: 09/28/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PROJECT NUMBER
 PROJECT NAME
 PROJECT DATE

PROJECT NUMBER
 PROJECT NAME
 PROJECT DATE
 SEPTMBER 28, 2017
 1001 WEST 10TH, S.F., CA
 (415) 435-1000

PROJECT NUMBER
 PROJECT NAME
 PROJECT DATE
 A0-01



KIER & WRIGHT
CIVIL ENGINEERS & SURVEYORS, INC.
3999 FORTY-NINTH AVENUE SUITE 200
SAN MATEO, CALIFORNIA 94403-1211
PHONE (650) 241-6100
FAX (650) 241-6100

FACEBOOK
OWNER
1 HUNTER WAY
MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROJECT DATA
Record Of Survey
Kier & Wright

PROJECT NUMBER: 2017-00807
DATE: 8.2.2017

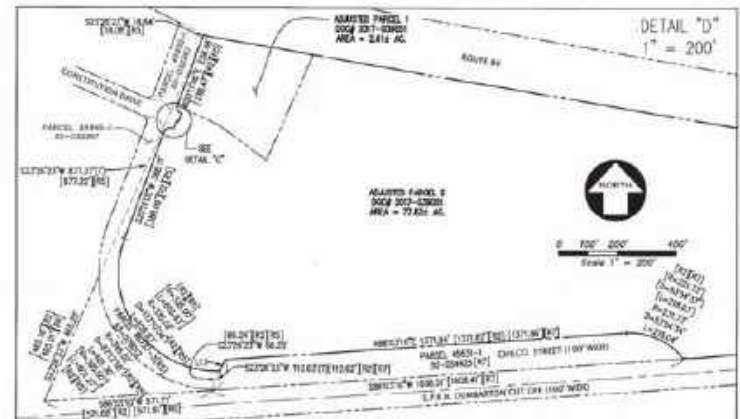
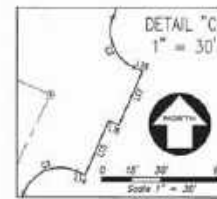
SEPTEMBER 28, 2017
157 WEST 22ND, S.F. 94115
PHONE: (415) 778-1171

SHEET NUMBER: A0-02

RECORD OF SURVEY

R/S NO. 2958
 OF ADJUSTED PARCEL 1 AND ADJUSTED PARCEL 2 AS SAID
 PARCELS ARE DESCRIBED IN THAT CERTAIN GRANT DEED, RECORDED
 MAY 5, 2017 AS DOCUMENT NUMBER 2017-039051, OFFICIAL
 RECORDS OF SAN MATEO COUNTY.
 CITY OF MENLO PARK - SAN MATEO COUNTY - CALIFORNIA
 SCALE 1" = 300' JULY, 2017

KIER & WRIGHT
 CIVIL ENGINEERS & SURVEYORS, INC.
 2953 Collier Canyon Road, Menlo Park, CA 94025
 650.329.4100 Fax: 650.329.4099



LINE #	DIRECTION	LENGTH	REFERENCE
11	S07°25'29"W	64.87	[18A] [14]
12	S22°41'14"W	132.89	[18A] [14]
13	S27°40'17"E	87.07	[11] [14] [14]
14	N07°01'43"E	80.07	[10] [14] [14]
15	N07°01'43"E	87.97	[22] [14] [14]
16	N05°01'17"E	84.37	[16] [14] [14]
17	N05°01'17"E	100.07	[10] [14] [14]
18	S05°01'17"E	85.47	[16] [14] [14]
19	S05°01'17"E	85.37	[10] [14] [14]
20	S05°01'17"E	112.17	[19] [14] [14]
21	S05°01'17"E	81.79	[10] [14] [14]
22	S22°41'14"W	113.17	[19] [14] [14]
23	N07°01'43"E	82.37	[10] [14] [14]
24	S05°01'17"E	83.07	[10] [14] [14]
25	S27°40'17"E	83.07	[10] [14] [14]
26	N07°01'43"E	83.87	[10] [14] [14]
27	S07°25'29"W	83.67	[10] [14] [14]
28	S05°01'17"E	83.07	[10] [14] [14]
29	N05°01'17"E	83.07	[10] [14] [14]
30	N05°01'17"E	83.07	[10] [14] [14]

LINE #	DIRECTION	LENGTH	REFERENCE
31	N07°01'43"E	11.37	[11] [14] [14]
32	S07°01'17"E	46.97	[11]
33	S05°01'17"E	46.97	[11]
34	N07°01'43"E	46.97	[11]
35	N07°01'43"E	38.46	[11]
36	N05°01'17"E	38.46	[11]
37	N05°01'17"E	38.46	[11]
38	N05°01'17"E	38.46	[11]
39	S05°01'17"E	38.46	[11]
40	S05°01'17"E	38.46	[11]

CURVE #	RADIUS	DELTA	LENGTH	REFERENCE
01	45.00'	87.120°	61.79'	[11] [14] [14] [14]
02	36.00'	87.020°	33.88'	[11] [14] [14]
03	28.00'	87.230°	33.16'	[11] [14] [14]

JOB NO. A15571-3 SHEET 2 OF 2 SHEETS

KIER & WRIGHT
 CIVIL ENGINEERS & SURVEYORS, INC.
 2953 Collier Canyon Road, Menlo Park, CA 94025
 650.329.4100 Fax: 650.329.4099

DATE PLOTTED

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PROJECT DATA
 Record Of Survey
 Kier & Wright

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 28, 2017
 PLOT SHEET NO. 4-5
 SHEET TOTAL 47 OF 50

SHEET NUMBER
 A0-03

KIER & WRIGHT
 Surveyors
 1000 4th Avenue
 Suite 100
 San Francisco, CA 94107
 Telephone: 415.398.3600
 Fax: 415.398.3601
 Email: k&w@kierwright.com

FACEBOOK
 OWNER
 1 Hacker Way
 Menlo Park, CA 94025

PROJECT NUMBER
 2015-007

DATE
 09/08/17

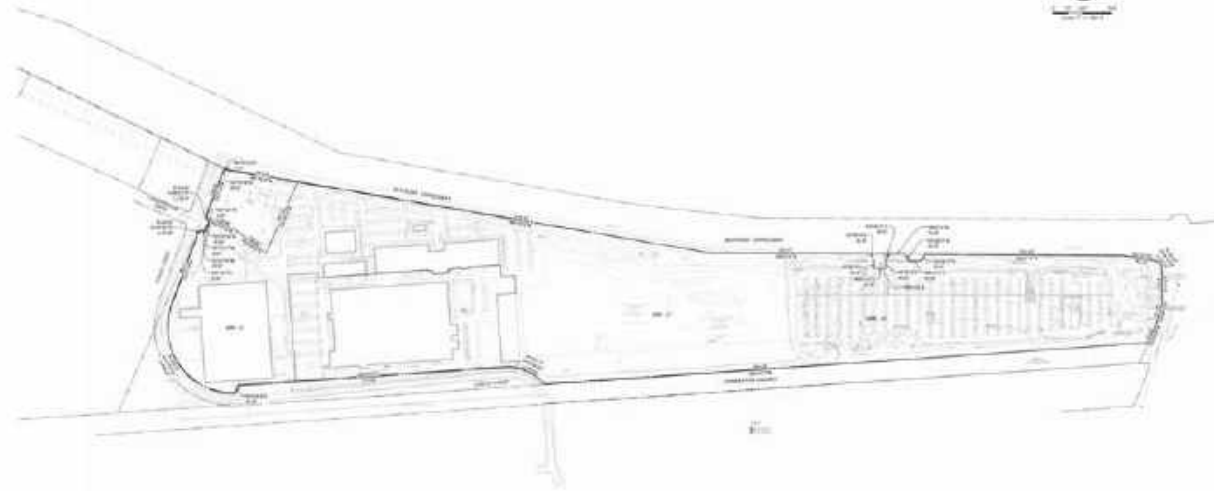
PROJECT NAME
 Facebook MFK 21

EXHIBIT

DATE
 09/08/17

BY
 [Signature]

CHECKED BY
 [Signature]



BOUNDARY TABLE		
BOUNDARY ADJACENT TO	BOUNDARY TYPE	BOUNDARY AREA (SQ FT)
BY KIER & WRIGHT SURVEY	EXISTING SURVEY	10,135,000
BY KIER & WRIGHT SURVEY	EXISTING SURVEY	10,135,000
BY KIER & WRIGHT SURVEY	EXISTING SURVEY	10,135,000
TOTAL BOUNDARY AREA = 30,405,000 SQ FT		

NOTE: THESE SURVEY RESULTS ARE SUBJECT TO REVISIONS. THESE RESULTS SHALL BE VALID ONLY FOR THE PURPOSES OF THIS PROJECT AND SHOULD NOT BE USED FOR ANY OTHER PURPOSES.

DATE	DESCRIPTION

KIER & WRIGHT
 SURVEYORS
 1000 4th Avenue
 Suite 100
 San Francisco, CA 94107
 Telephone: 415.398.3600
 Fax: 415.398.3601
 Email: k&w@kierwright.com

FACEBOOK
 OWNER
 1 Hacker Way
 Menlo Park, CA 94025

DRAWN BY

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PROJECT DATA
 BKF VS Kier & Wright Survey
 Kier & Wright

PROJECT NUMBER
 2015-007

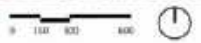
DATE
 09/08/17

BY
 [Signature]

CHECKED BY
 [Signature]

SHEET NUMBER
A0-04

DATE PLOTTED
 09/08/17



GEHRY PARTNERS, LLP
 ARCHITECT
 2000 BAYVIEW DRIVE
 LOS ANGELES, CALIFORNIA 90024
 (310) 360-1000

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

DATE: 09/28/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

AERIAL REGIONAL
 SITE VIEW

PROJECT NUMBER
 2016-007
 DATE
 SEPTEMBER 28, 2017
 SHEET NUMBER AND TITLE
 REGIONAL SITE VIEW (17' x 30')

SHEET NUMBER
A0-05
 © Gehry Partners, LLP

REFERENCE TABLE

	Building Cov. (SF)	Building Cov. (%)	Site Area (SF)	FAR	GFA (SF) *	Parking Stalls
Building 20	526,668		959,565	0.45	433,555	1,446
TE Site			2,544,476	0.45	1,142,508	
TE Site + Hotel			2,544,476	0.52	1,317,308	
Building 21	503,691				512,900	1,495
Building 22	145,307				449,500	0
Building 23	196,666				180,108	57
Hotel	44,020				174,800	245
Parking Structure	76,025					1,736
Pedestrian Bridge	7,440					
Pavillions	13,369					
Guard Shacks	496					
MPK21 & MPK22 Bridge	7,287					
Electrical Substation	1,373					
TOTAL	1,522,342	44%	3,504,041		1,750,863	4,979

Note:
 * GFA of respective Guard Shacks are included in GFA of Building 21, 22, 23 & Hotel.

GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BOULVARD STREET
 330 MARKET, EMPLOYER SUITE
 3300 14TH AVENUE

FACEBOOK
 OWNER
 1000 MARKET STREET
 SUITE 1000, SAN FRANCISCO, CA 94102

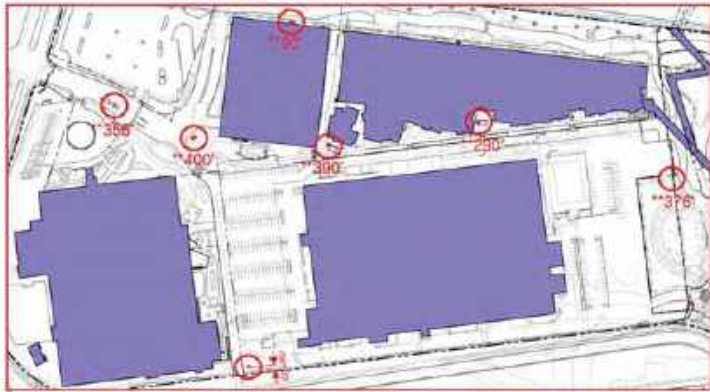
DATE: 09/20/12

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
FACEBOOK CAMPUS EXPANSION + WEST CAMPUS REFERENCE TABLE

PROJECT NUMBER
 2010-007
 DATE
 SEPTEMBER 20, 2012
 SHEET NUMBER AND TITLE
 ORIGINAL SHEET NO. 117 OF 127

A0-06



TOTAL SITE AREA
 3,504,041 SF
 COVERAGE AREA (PROPOSED)
 1,720,407 SF
 PERCENTAGE OF SITE COVERAGE = 49.10%

LOCATION OF GUARD SHACKS INTERIM PHASE
 Note:
 * 496 SF includes all Guard Shacks shown above.
 **All Guard Shacks are at a minimum of 20 feet from exterior property lines. The closest distance of the guard shacks to the exterior property lines are approximately 20' - 400'. Number in feet below Guard Shack location indicates approximate distance to nearest exterior property line.



GEHRY PARTNERS, LLP
 ARCHITECT
 1000 R STREET, SUITE 1000
 WASHINGTON, DC 20004
 (202) 462-2000

FACEBOOK
 OWNER
 1600 AVENUE K
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

**BUILDING COVERAGE
 CALCULATION DIAGRAM
 INTERIM PHASE**

PROJECT NUMBER
 2015-007
 DATE
 11-000
 SEPTEMBER 26, 2017
 DATE PLOTTED: 09/26/17
 DRAWING NO.: 01 - 01

SHEET NUMBER
A0-07
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[Dashed Box] TOTAL SITE AREA
 3,504,041 SF
 [Solid Blue Box] COVERAGE AREA (PROPOSED)
 1,522,342 SF
 PERCENTAGE OF SITE COVERAGE = 43.45%

LOCATION OF GUARD SHACKS FINAL PHASE
 Note:
 * 496 SF includes all Guard Shacks shown above.
 ** All Guard Shacks are at a minimum of 20 feet from exterior property lines. The closest distance of the guard shacks to the exterior property lines are approximately 20' - 400'. Number in feet below Guard Shack location indicates approximate distance to nearest exterior property line.



GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BULLFINCH STREET
 LOS ANGELES, CALIFORNIA 90015
 (213) 455-0000

FACEBOOK
 OWNER
 1600 AVENUE OF THE STARS
 MENLO PARK, CALIFORNIA 94025

SCALE: 1/8" = 1'-0"

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

BUILDING COVERAGE
CALCULATION DIAGRAM
FINAL PHASE

PROJECT NUMBER
 2015-007
 SCALE
 1"=500'
 DATE
 SEPTEMBER 28, 2015
 DRAWN BY
 JEFFREY W. WILSON
 CHECKED BY
 JEFFREY W. WILSON

SHEET NUMBER
A0-08

Program Areas by Building (approx. sf)							
BUILDING	Office	Support Rms	Amenities	Event Space	Hotel	Circulation, Walls, Structure, Stairs, etc.	GFA
MPK 21	195,900	50,400	60,165	31,100	0	175,307	512,872
MPK 22	211,392	39,606	21,820	21,626	0	155,042	449,486
HOTEL	1,800	11,500	13,700	0	61,700	86,100	174,800

Level Areas by Building (approx. sf)					
BUILDING	Ground	Level 1	Level 1 Mezz	Roof	GFA
MPK 21	16,444	389,140	81,509	25,779	512,872

Level Areas by Building (approx. sf)						
BUILDING	LEVEL 01	LEVEL 02	LEVEL 03	LEVEL 04	ROOF LEVEL	GFA
MPK 22	135,446 SF	111,559 SF	107,514 SF	96,796 SF	171 SF	449,486

BUILDING	Ground	Podium	Level 3	Level 4	Level 5	Level 6	Level 7	GFA
HOTEL	13,700	39,400	22,300	25,000	25,000	25,000	24,400	174,800

NOTE:

1. THE PROGRAM INFORMATION CONTAINED IN THESE TABLES ARE DRAFT APPROXIMATIONS AS THEY STAND AT THIS POINT IN TIME. THE PROGRAM INFORMATION WILL CONTINUE TO BE REFINED AS THE DESIGN OF THE BUILDINGS EVOLVE.

SUPPORT ROOMS:

Support Rooms include Electrical & Machine Rooms, Shipping & Receiving Facilities, Storage Room, Security, Bicycle Storage, Restrooms, IT Rooms, Showers, Lockers.

AMENITIES:

Amenities include Cafeteria, Private Dining Rooms, Cafes, Microkitchens, Mother's/Wellness Room, Meditation Rooms

GEHRY PARTNERS, LLP
ARCHITECT
600 BAYVIEW DRIVE
SAN ANTONIO, TEXAS 78209
(214) 461-3000

FACEBOOK
OWNER
10000 WALKER
HOUSTON, TEXAS 77036

DISCIPLINE

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROGRAM AREAS BLDG 21,
BLDG 22, BLDG 23 & HOTEL

PROJECT NUMBER
2016-007
DATE

SEPTEMBER 26, 2017
1617 0007 000 6.1.1
DRAWN: 0007 000 017.2.00

SHEET NUMBER

A0-20

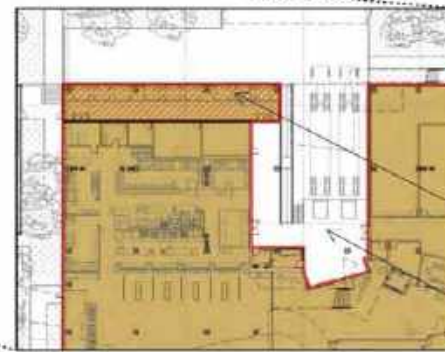
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LEVEL 2 GROSS AREA 112,810 SF
 EXCLUSIONS 1,251 SF
 SUBTOTAL 111,559 SF



LEVEL 1 GROSS AREA 138,026 SF
 EXCLUSIONS 2,580 SF
 SUBTOTAL 135,446 SF



- LEGEND**
- BUILDING ENCLOSURE
 - EXTERIOR TERRACE
 - SECURITY STATIONS
 - ENCLOSURE AREA
 - EXCLUSION AREA

TRASH AND RECYCLING AREA

LOADING DOCK IS NOT ENCLOSED

GLAZING

	LEVEL 01	LEVEL 02	LEVEL 03	LEVEL 04	ROOF LEVEL	TOTAL
NEW EXISTING GLAZED AREA	1,062,019 SF	111,230 SF	309,296 SF	46,115 SF	4,377 SF	1,533,037 SF
EXCLUSIONS FROM						
NON-OCCUPABLE TRANSPARENT WALLS*	3,822 SF	380 SF	1,017 SF	434 SF	0 SF	5,653 SF
GLAZED ROOFING SYSTEMS	0 SF	0 SF	0 SF	0 SF	4,086 SF	4,086 SF
SCREENING WALLS**	0 SF	0 SF	0 SF	0 SF	4,086 SF	4,086 SF
GLAZED - HVAC PLUMBING**	0 SF	679 SF	1,125 SF	885 SF	0 SF	2,689 SF
EXCLUSIONS FOR TRASH & RECYCLING**	3,359 SF	0 SF	0 SF	0 SF	0 SF	3,359 SF
NON-OCCUPABLE TRANSPARENT - OTHER GLAZES**	0 SF	0 SF	0 SF	0 SF	0 SF	0 SF
TOTAL GLAZING EXCLUSIONS	7,181 SF	1,059 SF	2,142 SF	1,319 SF	4,086 SF	15,789 SF
GLAZING CALCULATION						
NEW TOTAL GLAZED AREA - TOTAL GLAZING EXCLUSIONS	1,054,838 SF	110,171 SF	307,154 SF	45,681 SF	377 SF	1,518,041 SF

- NOTES:**
1. EXISTING GLAZED AREA (EXISTING GLAZED AREA) AND NEW GLAZED AREA (NEW GLAZED AREA) ARE IDENTIFIED BY COLOR.
 2. EXCLUSIONS FROM GLAZED AREA ARE IDENTIFIED BY COLOR AND HAVE HAD THEIR ACCESS, UNDEVELOPED AIR AND WINDOWS OR SCREENING, AND NO SCREENING.
 3. EXCLUSIONS FROM GLAZED AREA (GLAZED AREA) WITH NON-GENERATING EQUIPMENT WITH A GENERATOR NOT TO EXCEED 15% OF GFA. AREA TOTALS LISTED ABOVE HAVE BEEN PROPORTIONALLY REDUCED TO AVOID THE UNREASONABLE EXCLUSION OF 15% OF GFA.
 4. EXCLUSIONS FROM GLAZED AREA (GLAZED AREA) ARE IDENTIFIED BY COLOR AND HAVE HAD THEIR ACCESS, UNDEVELOPED AIR AND WINDOWS OR SCREENING, AND NO SCREENING.
 5. EXCLUSIONS FROM GLAZED AREA (GLAZED AREA) ARE IDENTIFIED BY COLOR AND HAVE HAD THEIR ACCESS, UNDEVELOPED AIR AND WINDOWS OR SCREENING, AND NO SCREENING.
 6. TRASH AND RECYCLING AREA IS EXCLUDED FROM GLAZED AREA PER CITY OF DENVER PARKING ORDINANCE 18.34.020 C.3.
 7. NON-OCCUPABLE TRANSPARENT GLAZES (GLAZES) CALCULATION INCLUDES AREAS WITH HEIGHTS OF 10 FEET OR 4 FEET THAT DO NOT HAVE ENCLOSURE UNDER EXISTING TO EXCEED 15.34.020 C.3.1. NONE OF THESE NON-OCCUPABLE GLAZES WILL BE IDENTIFIED ON UNDEVELOPED GFA. THESE AREAS ARE IDENTIFIED AS NON-OCCUPABLE SPACE ABOVE GLAZED AREA WITH UNDEVELOPED WALLS, NO FLOORS AND UNDEVELOPED CEILING, WITH LIMITED ACCESS, UNDEVELOPED AIR, NO WINDOWS OR SCREENING, AND NO SCREENING.

GEHRY PARTNERS, LLP
 ARCHITECT
 1000 MARKET STREET
 SUITE 4000, SAN FRANCISCO, CA 94102
 (415) 774-1000

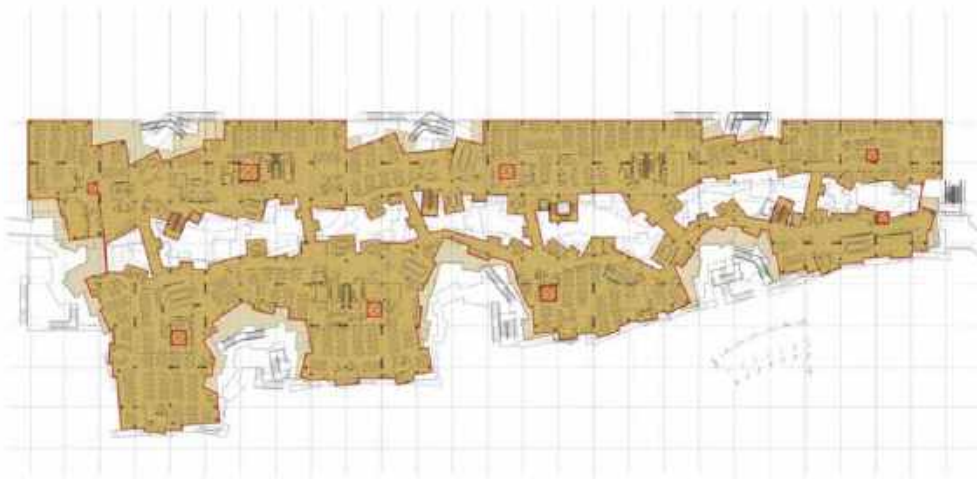
FACEBOOK
 OWNER
 1700 BAY STREET
 SAN FRANCISCO, CALIFORNIA 94133

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

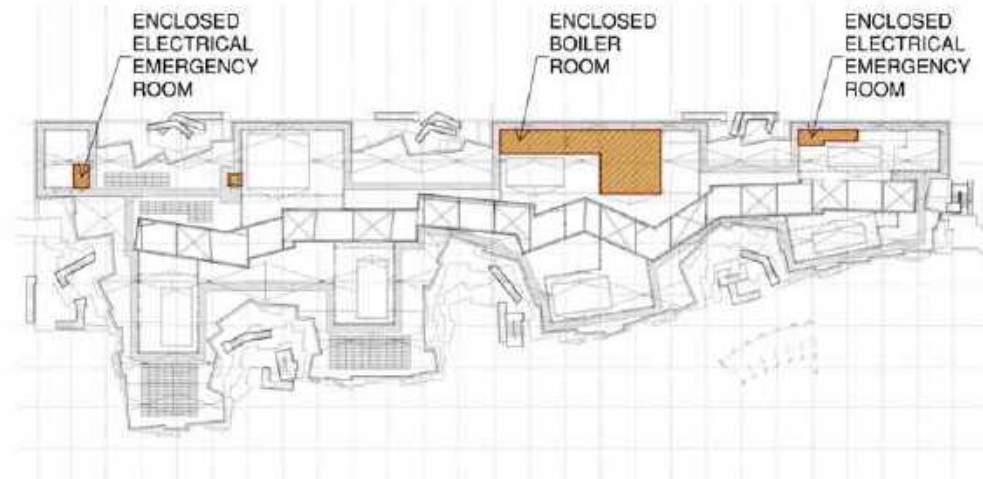
MPK22 SQUARE FOOT
 DIAGRAMS

PROJECT NUMBER
 2016.007
 DATE
 SEPTEMBER 26, 2017
 1/17 9:07 AM, 8/11
 SHEET 002 OF 17

SHEET NUMBER
 A0-21
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LEVEL 4 GROSS AREA 96,315 SF
 EXCLUSIONS 1,519 SF
 SUBTOTAL 94,796 SF



ROOF LEVEL GROSS AREA 4,771 SF
 EXCLUSIONS 4,600 SF
 SUBTOTAL 171 SF



LEVEL 3 GROSS AREA 109,294 SF
 EXCLUSIONS 1,780 SF
 SUBTOTAL 107,514 SF

LEGEND

- BUILDING ENCLOSURE
- EXTERIOR TERRACE
- SECURITY STATIONS
- ENCLOSURE AREA
- EXCLUSION AREA

- BUILDING ENCLOSURE
- EXTERIOR TERRACE
- SECURITY STATIONS
- ENCLOSURE AREA

GEHRY PARTNERS, LLP
 ARCHITECT
 800 BOYD STREET
 100 SAN FRANCISCO, CALIFORNIA 94102
 (415) 761-3000

FACEBOOK
 OWNER
 1600 CALIFORNIA AVENUE
 MENLO PARK, CALIFORNIA 94025

DATE: 09/20/11

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

MPK22 SQUARE FOOT
 DIAGRAMS

PROJECT NUMBER
 2010-007

DATE

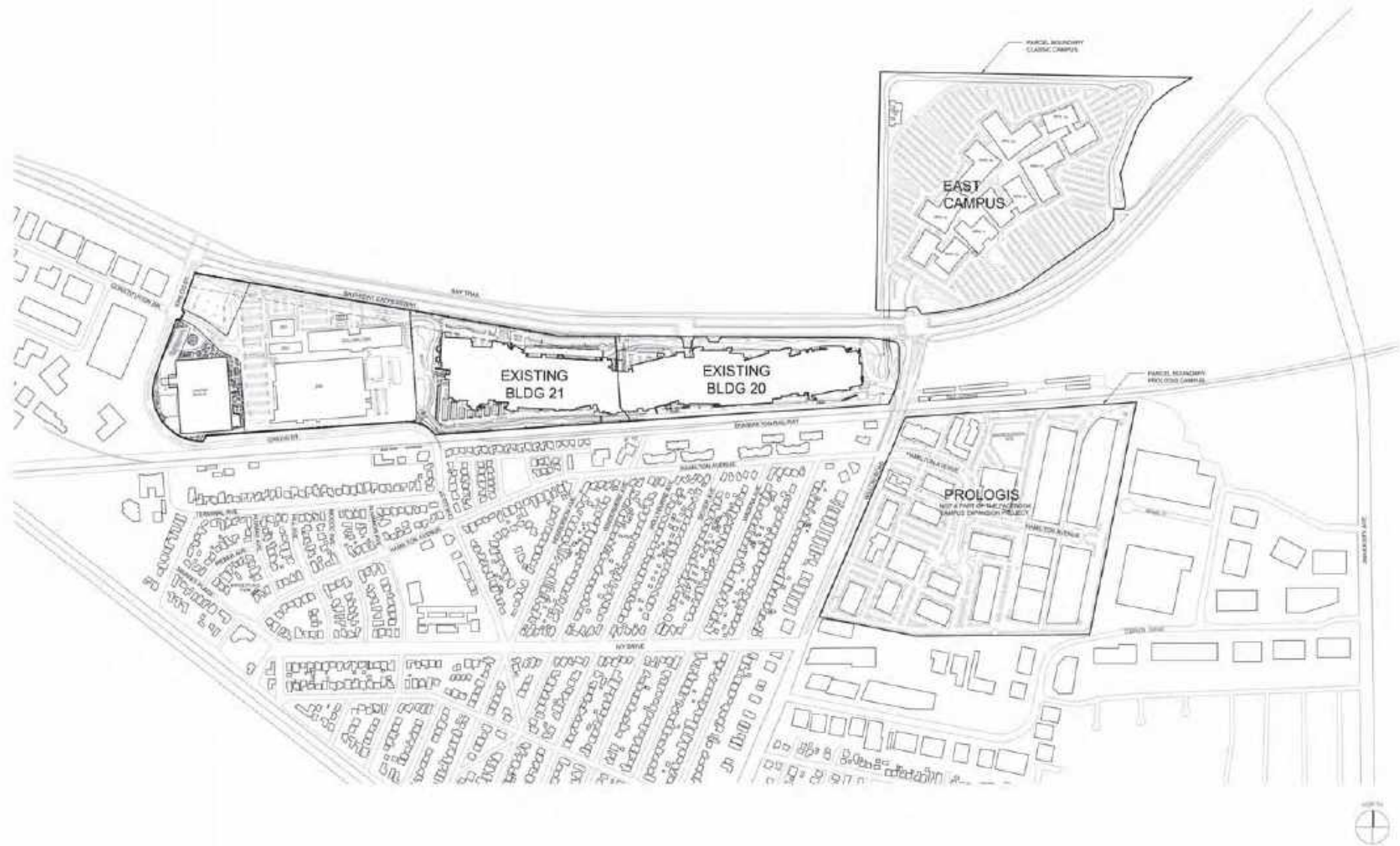
SEPTEMBER 20, 2011

1/4" = 1'-0" (VERTICAL)
 1/8" = 1'-0" (HORIZONTAL)

SHEET NUMBER

A0-22

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 ARCHITECT
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 SUITE 1000, ANN ARBOR, MI 48106
 (734) 761-1000

FACEBOOK
 OWNER
 1600 AVENUE B
 MENLO PARK, CALIFORNIA 94025

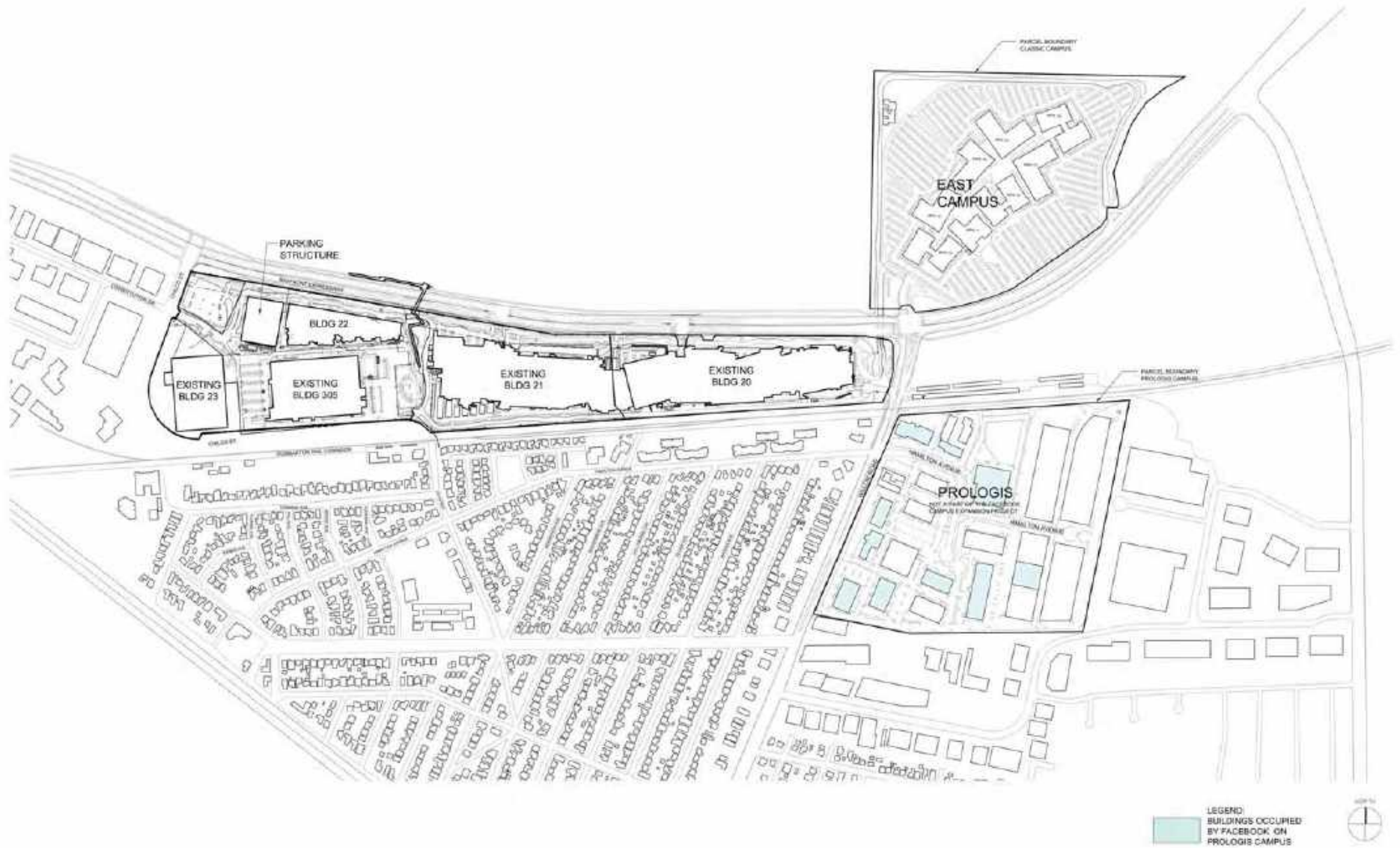
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

EXISTING REGIONAL PLAN

PROJECT NUMBER
 2015-027
 SCALE
 1" = 300'

 DATE
 SEPTEMBER 20, 2017
 THIS SHEET IS ONE OF 15
 SHEETS SHOWN ON 20" x 30"

SHEET NUMBER
A1-01
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 ARCHITECT
 300 W. WALTON STREET
 CHICAGO, ILLINOIS 60610
 (312) 467-8000

FACEBOOK
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 1601 AVENUE B
 MENLO PARK, CALIFORNIA 94025

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 300-309 Constitution Drive

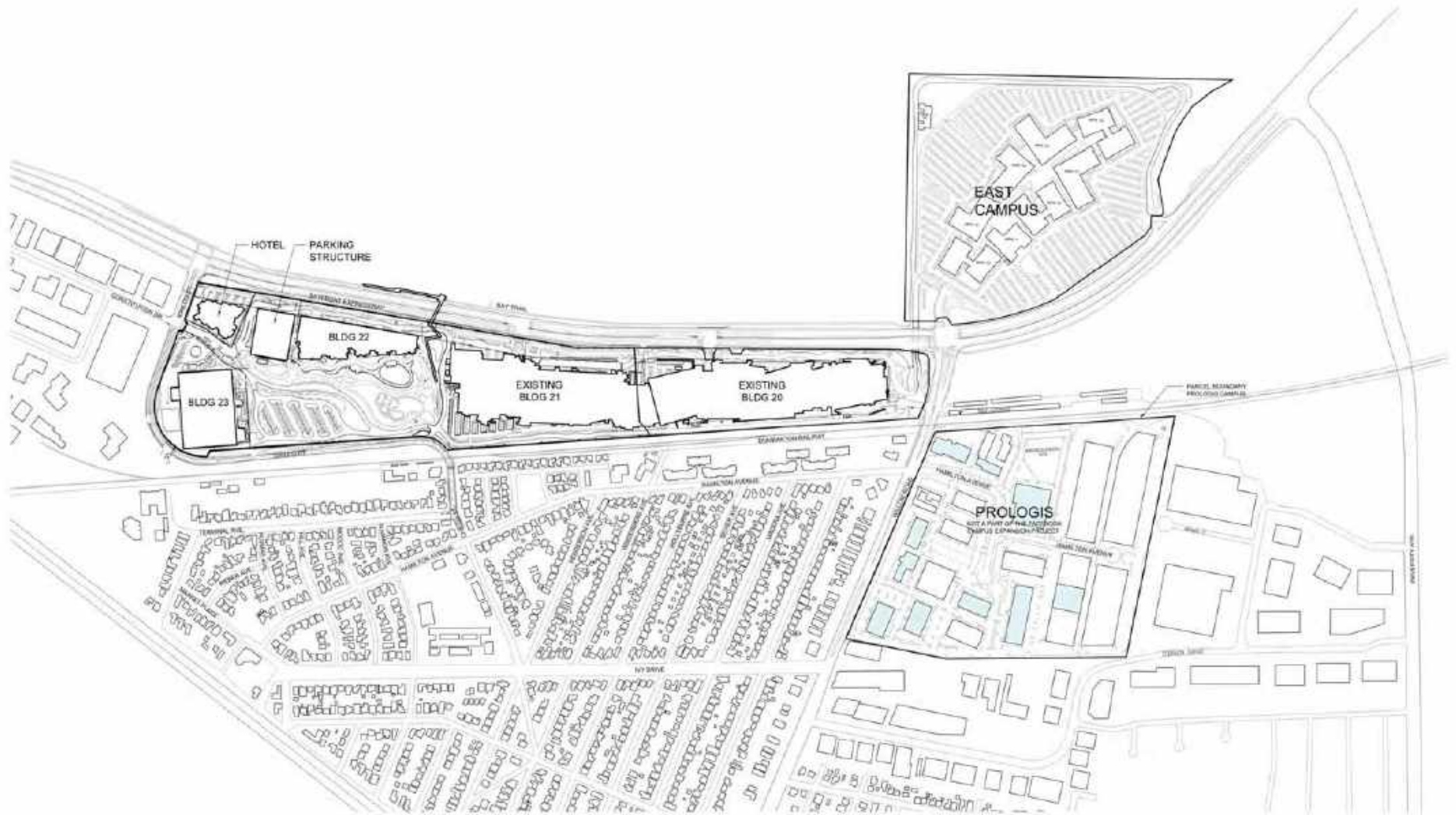
PROPOSED REGIONAL PLAN
 INTERIM PHASE

PROJECT NUMBER
 2015-077
 0004
 17" x 36"

DATE
 SEPTEMBER 28, 2017

SCALE
 PLAN: 1/8" = 1'-0"
 ELEVATION: 1/8" = 1'-0"

A1-02



NOTE:
PROPOSED HOTEL IS SHOWN
FOR ILLUSTRATIVE PURPOSE ONLY

LEGEND
BUILDINGS OCCUPIED
BY FACEBOOK OR
PROLOGIS CAMPUS



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 ARCHITECT
 300 W. BROAD ST. #1000
 LOS ANGELES, CALIFORNIA 90071
 (213) 487-8800

FACEBOOK
 OWNER
 1600 AVENUE OF THE STARS
 MENLO PARK, CALIFORNIA 94025

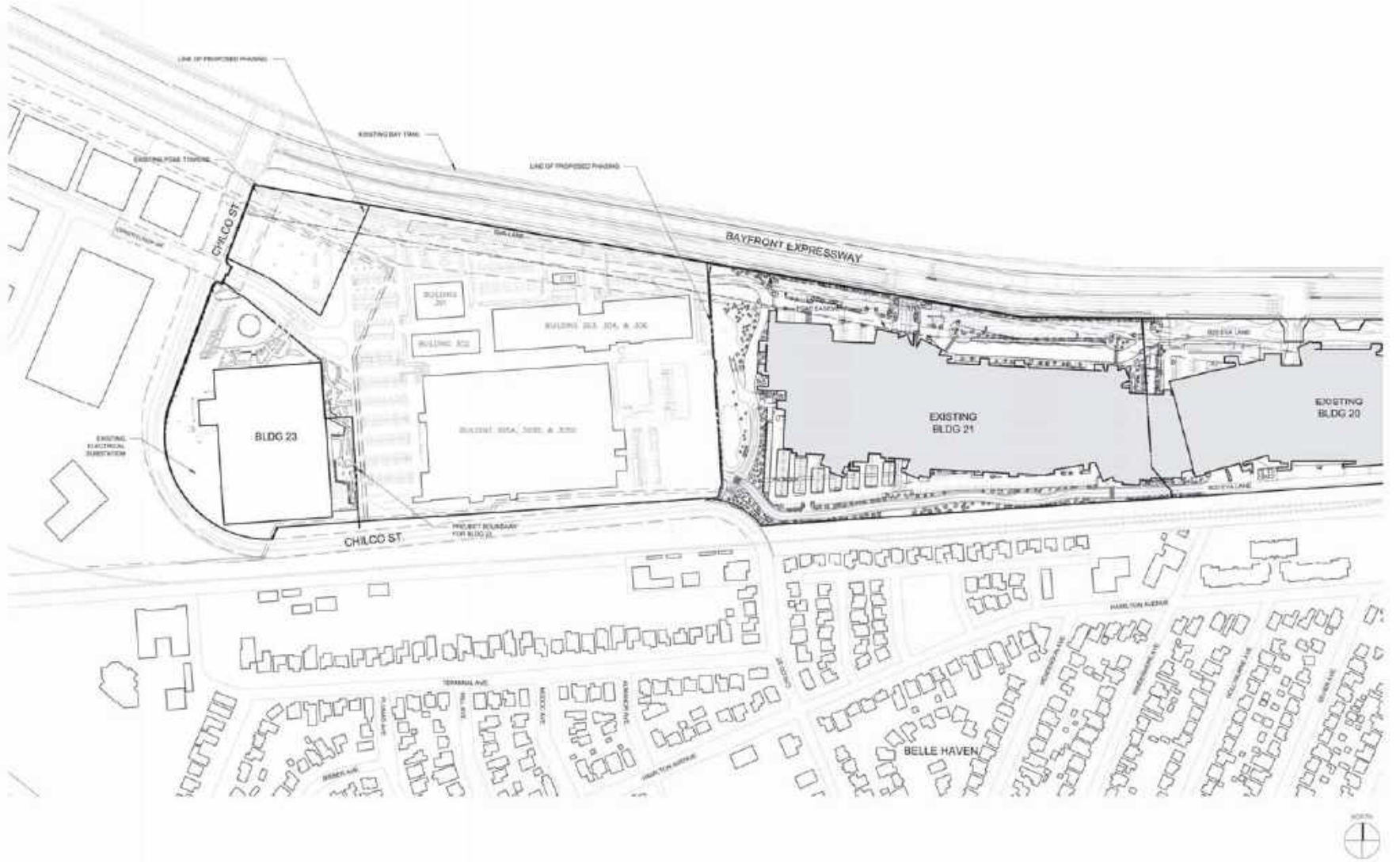
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
PROPOSED REGIONAL PLAN
FINAL PHASE

PROJECT NUMBER
 2015-027
 SCALE
 1" = 300'

 DATE
 SEPTEMBER 26, 2017
 PLAN SHEET NO. 8 OF 15
 SHEET SHEET NO. 34 OF 34

SHEET NUMBER
A1-03
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GEHRY PARTNERS, LLP
 ARCHITECT
 1001 BROADWAY, SUITE 1100
 NEW YORK, NY 10018
 TEL: 212 677 2000
 FAX: 212 677 2001
 WWW.GEHRY.COM

OWNER

FACEBOOK
 OWNER
 1601 AVENUE OF THE STARS
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
EXISTING SITE PLAN

PROJECT NUMBER
 2015-007
 SHEET
 174-100
 DATE
 SEPTEMBER 26, 2015
 DRAWN BY
 JAY WANG, AIA
 CHECKED BY
 JAY WANG, AIA

SHEET NUMBER
A1-11
 DRAWN BY
 JAY WANG, AIA
 CHECKED BY
 JAY WANG, AIA



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 ARCHITECT
 2000 BROADWAY STREET
 LOS ANGELES, CALIFORNIA 90071
 (213) 487-3800

FACEBOOK
 OWNER
 10000 AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
**PROPOSED SITE PLAN
 INTERIM PHASE**

PROJECT NUMBER
 2015-027
 SCALE
 1" = 100'
 DATE
 SEPTEMBER 26, 2017
 PLAN SHEET NO. 08 OF 15
 SHEET NO. 08 OF 34

SHEET NUMBER
A1-12



NOTE: PROPOSED HOTEL IS SHOWN FOR ILLUSTRATIVE PURPOSE ONLY



<p>GEHRY PARTNERS, LLP ARCHITECT 200 BROADWAY, SUITE 1100 NEW YORK, NY 10038 212 675 1000</p> <p>FACEBOOK OWNER 1601 AVENUE AVENUE MENLO PARK, CALIFORNIA 94025</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>PROPOSED SITE PLAN FINAL PHASE</p>	<p>PROJECT NUMBER 2015-027 DATE 11-1-15</p> <p>SEPTEMBER 24, 2015 DATE PLOTTED: 11-15-15 DRAWING NO.: 347-347</p>	<p>SHEET NUMBER A1-13</p> <p>© 2015 GHP, LP</p>
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 ARCHITECT
 200 W. BROADWAY STREET
 LOS ANGELES, CALIFORNIA 90071
 (213) 480-0000

FACEBOOK
 OWNER
 16015 3RD STREET
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

DATE
ENLARGED SITE PLAN
 INTERIM PHASE

PROJECT NUMBER
 2015-007
 DATE
 11-19-15

SEPTEMBER 20, 2017
 DATE PLOTTED: 09/20/17 10:15 AM
 DRAWING NUMBER: 2015-007-A1-14

SHEET NUMBER
A1-14



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 LOS ANGELES, CALIFORNIA 90006
 (213) 480-8800

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

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 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

ENLARGED SITE PLAN
 FINAL PHASE

PROJECT NUMBER
 2015-027
 DATE
 11-19-15

SEPTEMBER 20, 2017
 THIS SHEET IS ONE OF 15
 SHEETS, SHEET NO. 20 OF 34

SHEET NUMBER
A1-15



GEHRY PARTNERS, LLP
ARCHITECT
1001 8th Street
1001 8th Street, San Francisco, CA 94103
415.774.1000

FACEBOOK
OWNER
1 Hacker Way
Menlo Park, California 94025

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PREVIOUSLY PROPOSED
SITE PLAN

PROJECT NUMBER
2018-007
DATE
SEPTEMBER 26, 2017
SHEET NUMBER
117 of 117

SHEET NUMBER
A1-16



GERHY PARTNERS, LLP
 ARCHITECT
 1000 BARRIE STREET
 SUITE 1000, OLYMPIA, WASH
 98513-1000

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

CONSULTANTS

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

BUILDING 22
FLOOR PLAN LEVEL 01

PROJECT NUMBER
 2010-007
 SCALE
 1/32" = 1'-0"
 DATE
 SEPTEMBER 26, 2011
 DRAWN AND CHECKED BY
 JEFFREY W. WILSON, ARCHITECT

SHEET NUMBER
A2-01
 DRAWN BY
 JEFFREY W. WILSON, ARCHITECT



LEGEND:
 ATRIUM
 TERRACE

1 LEVEL 2
 112' x 110'

GEHRY PARTNERS, LLP
 ARCHITECT
 400 MARINE STREET
 LOS ANGELES, CALIFORNIA 90015
 (213) 480-1800

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

BUILDING 22
 FLOOR PLAN LEVEL 02

PROJECT NUMBER
 2015-007
 SCALE
 1/32" = 1'-0"
 DATE
 SEPTEMBER 26, 2017
 DRAWN BY
 J. S. J. / J. S. J.
 CHECKED BY
 J. S. J. / J. S. J.

Sheet Number
 A2-02



1 LEVEL 3
112'0" x 112'0"

LEGEND:
■ ATRIUM
■ TERRACE

GEHRY PARTNERS, LLP
 ARCHITECT
 400 MARKET STREET
 LOS ANGELES, CALIFORNIA 90012
 (213) 480-1800

FACEBOOK
 OWNER
 1600 AVENUE OF THE STARS
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

BUILDING 22
 LEVEL 03 FLOOR PLAN

PROJECT NUMBER
 2015-007
 SCALE
 1/32" = 1'-0"
 DATE
 SEPTEMBER 26, 2017
 PLAN SHEET 3 OF 11
 DRAWING SHEET 6 OF 24

SHEET NUMBER
 A2-03



1 LEVEL 4
1132'-7" x 1132'-7"

LEGEND:
■ ATRIUM
■ TERRACE

GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BAYVIEW STREET
 17TH FLOOR, SUITE 1000
 SAN FRANCISCO, CA 94133
 (415) 774-2000

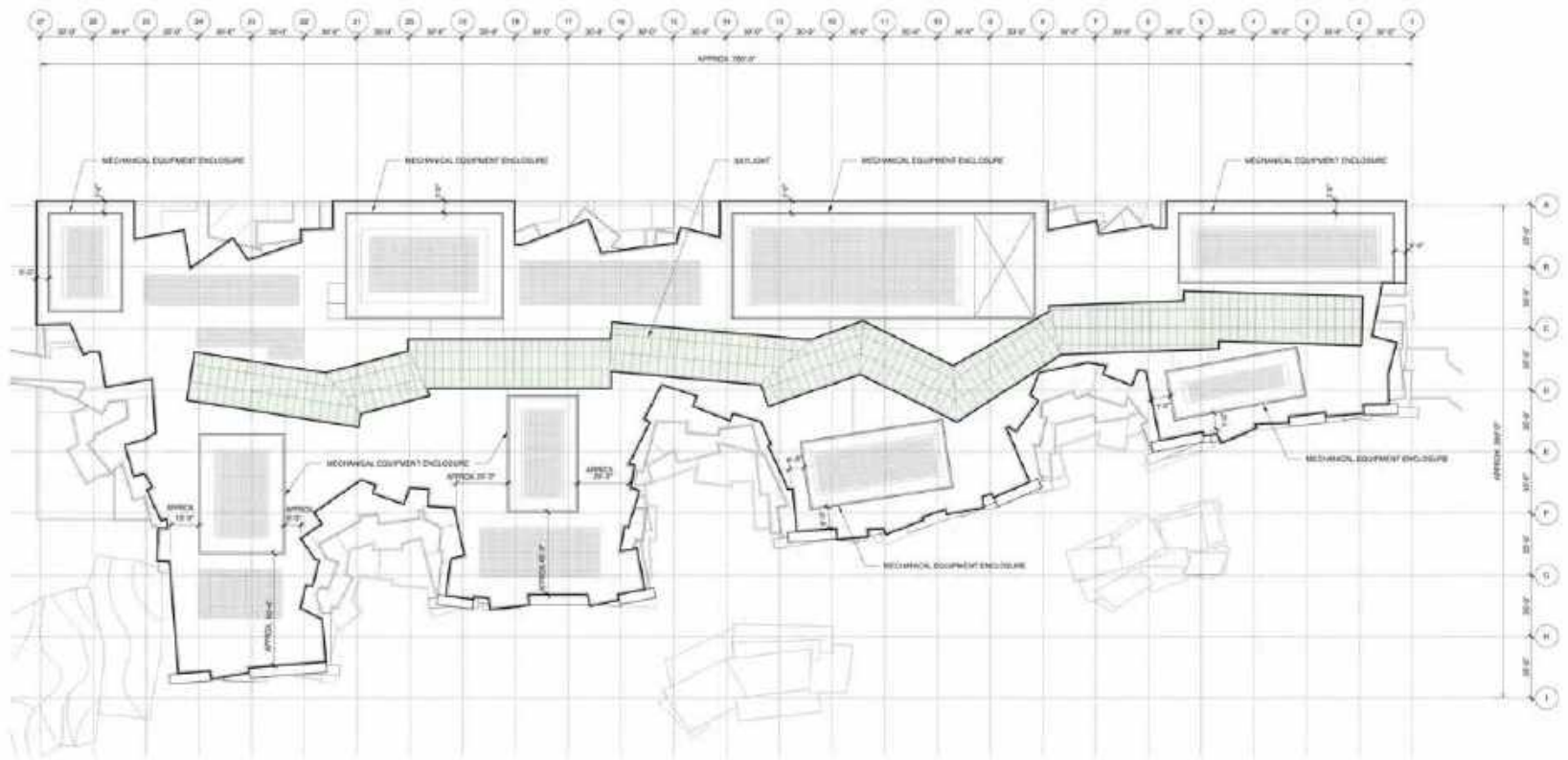
FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

DATE
BUILDING 22
LEVEL 04 FLOOR PLAN

PROJECT NUMBER
 2015-007
 SCALE
 1/32" = 1'-0"
 DATE
 SEPTEMBER 26, 2015
 PLAN SHEET 04 OF 10
 DESIGN SHEET 04 OF 04

SHEET NUMBER
A2-04
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1 ROOF
1/16" = 1'-0"

GEHRY PARTNERS, LLP
 ARCHITECT
 400 BAYVIEW STREET
 LOS ANGELES, CALIFORNIA 90004
 (213) 480-8800

FACEBOOK
 OWNER
 1600 AVENUE
 MENLO PARK, CALIFORNIA 94025

DATE PLOTTED

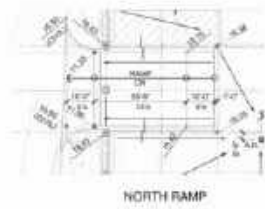
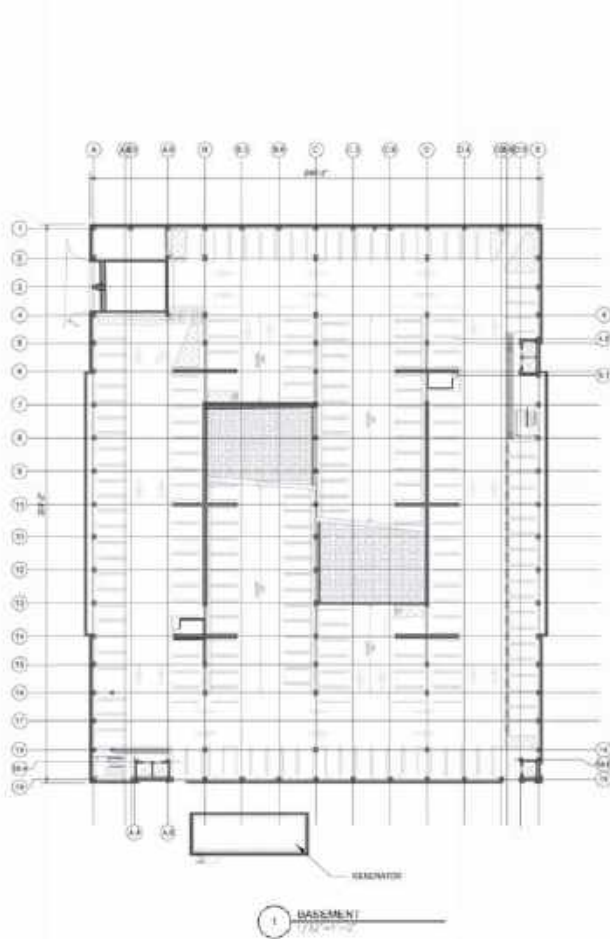
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

DATE
BUILDING 22
ROOF PLAN

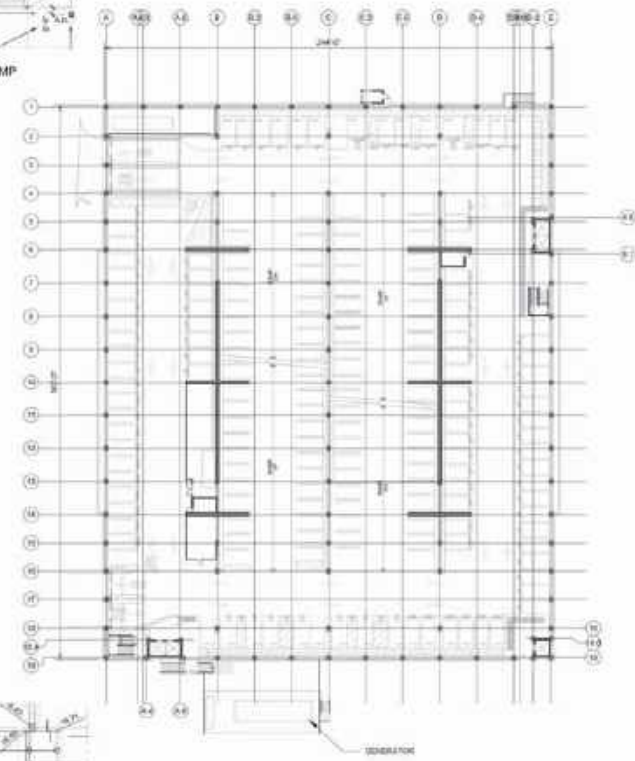
PROJECT NUMBER
 2015-007
 SCALE
 1/16" = 1'-0"

 DATE PLOTTED
 SEPTEMBER 26, 2017
 PLOT SHEET SIZE: 18" x 24"
 SHEET NUMBER: 047 of 047

SHEET NUMBER
A2-05
 SHEET NUMBER, 047



NORTH RAMP



SOUTH RAMP

2 LEVEL 1
1200'-0" x 1100'-0"

WATRY DESIGN, INC.
ARCHITECT
2000 CALIFORNIA ST., SUITE 200
SAN JOSE, CALIFORNIA 95128
408.255.7400

FACEBOOK
OWNER
10000 AVENUE
REDWOOD CITY, CALIFORNIA 94063

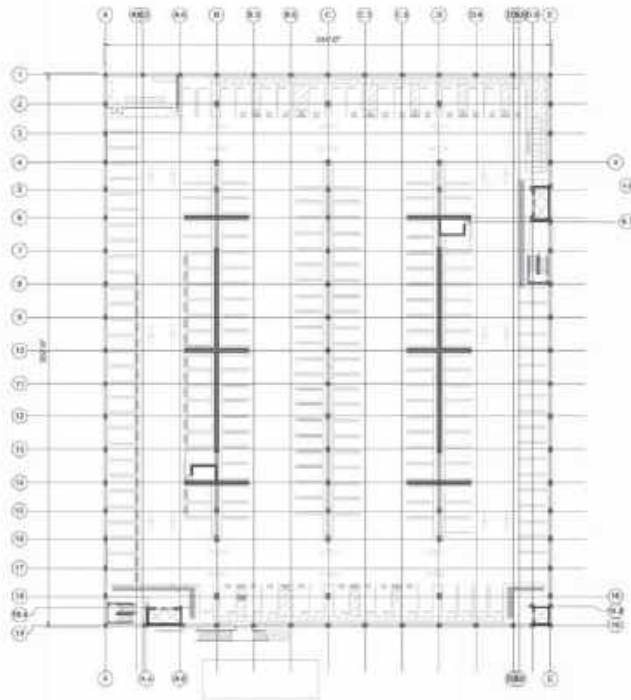
DATE PLOTTED

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

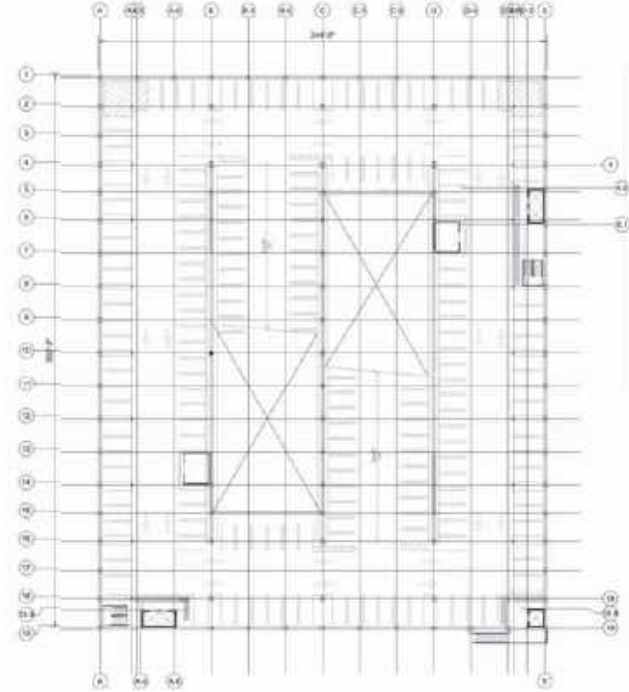
FACEBOOK
BASEMENT & LEVEL 1
PARKING STRUCTURE

PROJECT NUMBER
2016-007
DATE
1/22/17 11:07
DATE PLOTTED
SEPTEMBER 26, 2017
SHEET NO. 5/11
TOTAL SHEETS 27 OF 37

SHEET NUMBER
A2-10
WATRY DESIGN, LP



1 TYP. FLOOR 2-6
1/32" = 1'-0"



2 LEVEL 7
1/32" = 1'-0"

WATRY DESIGN, INC.
ARCHITECT
2000 CANTON ST., SUITE 200
SAN JOSE, CALIFORNIA 95128
408.255.7400

DATE/DATE

FACEBOOK
OWNER
1 FARRIS WAY
MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

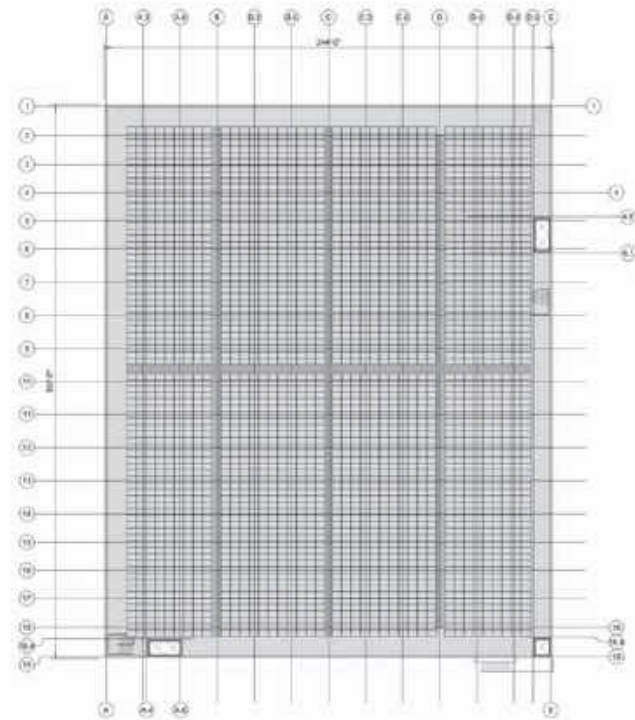
LEVEL 2 - 7
PARKING STRUCTURE

PROJECT NUMBER
2016-007
DATE
1/22/16 1:00"
REVISIONS
SEPTEMBER 28, 2017
REV. SHEET NO. 011
DESIGN SHEET NO. 011 + 20"

PROJECT NUMBER

A2-11

WATRY DESIGN, INC.



1 PV ROOF
17.32' WT = 4"

WATRY DESIGN, INC.
ARCHITECT
2000 CANTONMENT BL. #200
SAN JOSE, CALIFORNIA 95128
408.255.7400

FACEBOOK
OWNER
1 FARRIS BLVD.
MENLO PARK, CALIFORNIA 94025

DATE: 08/21/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

ROOF
PARKING STRUCTURE

PROJECT NUMBER
2016-007
DATE
1/22/17
SCALE
1/2" = 1'-0"
REVISED
SEPTEMBER 28, 2017
BY: [unreadable] R-11
CHECKED: [unreadable] R-11
DRAWN: [unreadable] R-11

SHEET NUMBER
A2-12
WATRY DESIGN, LP

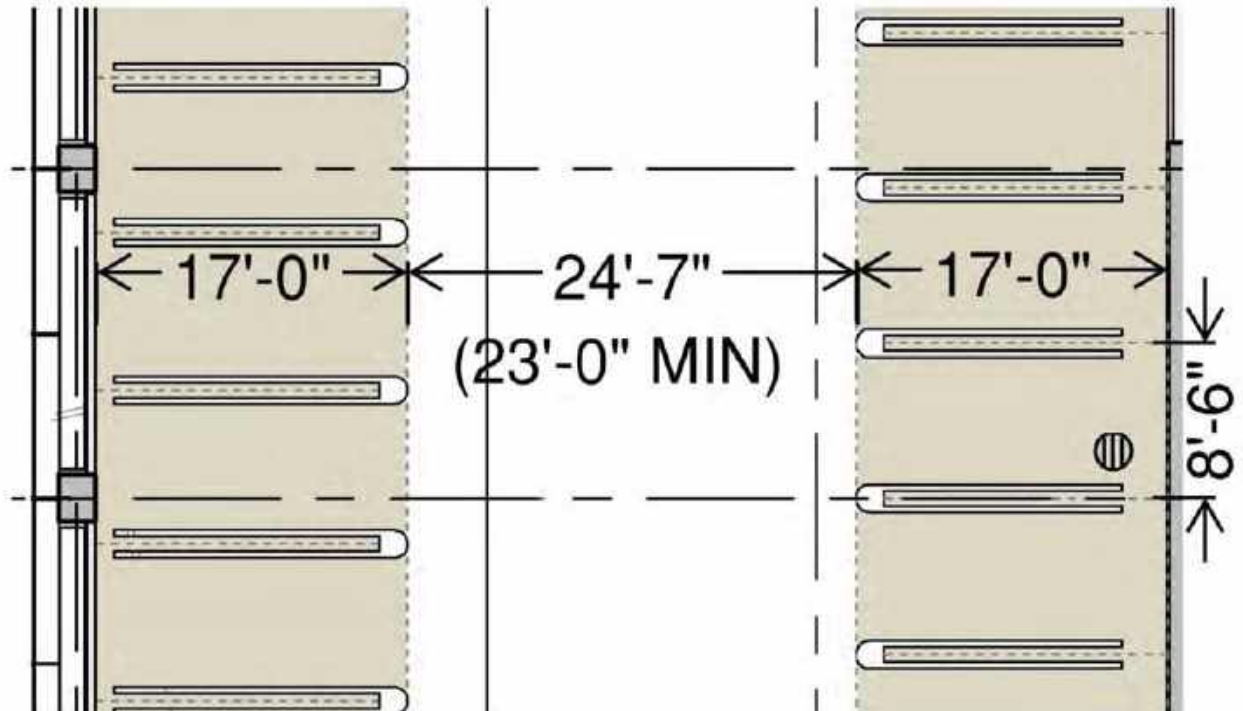
PARKING STALL SUMMATION CHART

LEVEL	TYPE											TOTAL	MOTORCYCLE (3'-0" x 6'-0")	AREA	SQ. FT. PER STALL
	DAY 1 INSTALLED														
	LEVEL 3 EV														
	ACCESSIBLE (8'-0" x 18'-0")	VAN ACCESSIBLE (12'-0" x 18'-0")	STANDARD	15FEV CARPOOL (8'-0" x 17'-0")	FAST CHARGER EV (8'-0" x 17'-0")	STANDARD EV (8'-0" x 17'-0")	ACCESSIBLE EV (8'-0" x 15'-0")	VAN ACCESSIBLE EV (12'-0" x 18'-0")	AMBULATORY EV (8'-0" x 15'-0")	FUTURE PROVISION (EV)					
SEVENTH LEVEL			191									191		82,300	329
SIXTH LEVEL			238									238		73,750	313
FIFTH LEVEL			230									230		73,750	319
FOURTH LEVEL			230									230		73,750	308
THIRD LEVEL			234									234		73,750	315
SECOND LEVEL	17	3	142						6	43		214	11	73,750	344
GROUND LEVEL	5	2	28		20	40	4	2	4	10		185	65	73,750	388
BASEMENT LEVEL			185	19								204		84,750	335
TOTAL	22	5	1963	19	20	40	4	2	4	60		1730	26	573,295	329

DESCRIPTION	REQUIRED	PROVIDED - DAY 1	FUTURE PLANNED	PHASE 2 GOAL TOTAL
TOTAL ADA & ADA VAN	27	27		
DESIGNATED VEHICLE PARKING (3%)	139	139		
EV (8%)	104	70	+35	126
ACCESSIBLE EV	4	4		
ACCESSIBLE VAN EV	2	2		
AMBULATORY EV	4	4		
MOTORCYCLE (1.5%)	26	26		

NOTES:

- DESIGNATED PARKING INCLUDES EV NUMBER
- PER FACEBOOK, EV INSTALLATION WILL BE AS FOLLOWS:
 - DAY 1 INFRASTRUCTURE SHOULD INCLUDE:
 - 10 DC FAST CHARGERS = 20 SPACES
 - 35 LEVEL 3 CHARGERS = 50 SPACES
 - TOTAL = 70 SPACES
 - SPACE & INFRASTRUCTURE SHOULD BE PLANNED TO SCALE UP TO:
 - 20 DC FAST CHARGERS = 40 SPACES
 - 48 LEVEL 2 CHARGERS = 80 SPACES
 - TOTAL = 120 SPACES
- DESIGNATED AS CLEAN AIR/VANPOOL/CARPOOL AT DAY 1 AND SHOULD BE MODIFIED TO EVCS ONCE INSTALLED
- * THE NUMBER OF ACCESSIBLE EVCS PROVIDED IS BASED ON THE TOTAL PHASE 2 GOAL OF 120 STALLS & NOT THE ACTUAL INSTALLED/PROVIDED DAY ONE ONLY.
- ** EIR STALL COUNT REQUIRED = 1736 STALLS PROVIDED = 1748 STALLS



WATRY DESIGN, INC.
ARCHITECT
1000 15TH AVENUE, SUITE 1000
DENVER, COLORADO 80202
(303) 733-1000

FACEBOOK
OWNER
1000 15TH AVENUE, SUITE 1000
DENVER, COLORADO 80202

PREVIOUSLY SUBMITTED

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

SCALE
DETAIL OF TYPICAL
PARKING SPACE

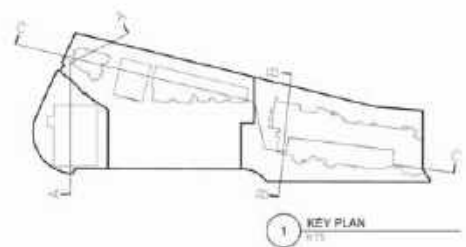
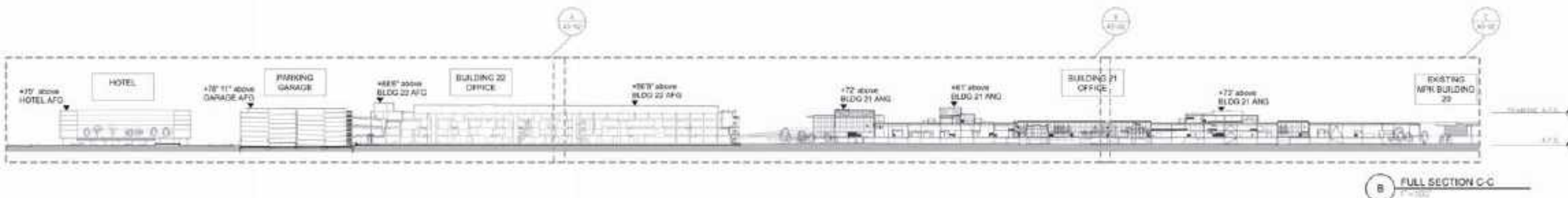
PROJECT NUMBER
2019-007

SEPTEMBER 08, 2017
1001 15TH AVENUE, SUITE 1000
DENVER, COLORADO 80202

SHEET NUMBER

A2-13

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NOTES

1. PROPOSED HOTEL IS SHOWN FOR ILLUSTRATIVE PURPOSES ONLY.
2. MPA OCCUPIES FLOOR AREA BELOW HEIGHT LIMIT.
3. A.F.S. = FINISHED FLOOR GRADE
4. A.N.G. = AVERAGE NATURAL GRADE

GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BAYVIEW BLVD
 SUITE 1000, OAKLAND, CA 94612
 (415) 778-1000

FACEBOOK
 OWNER
 16000 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

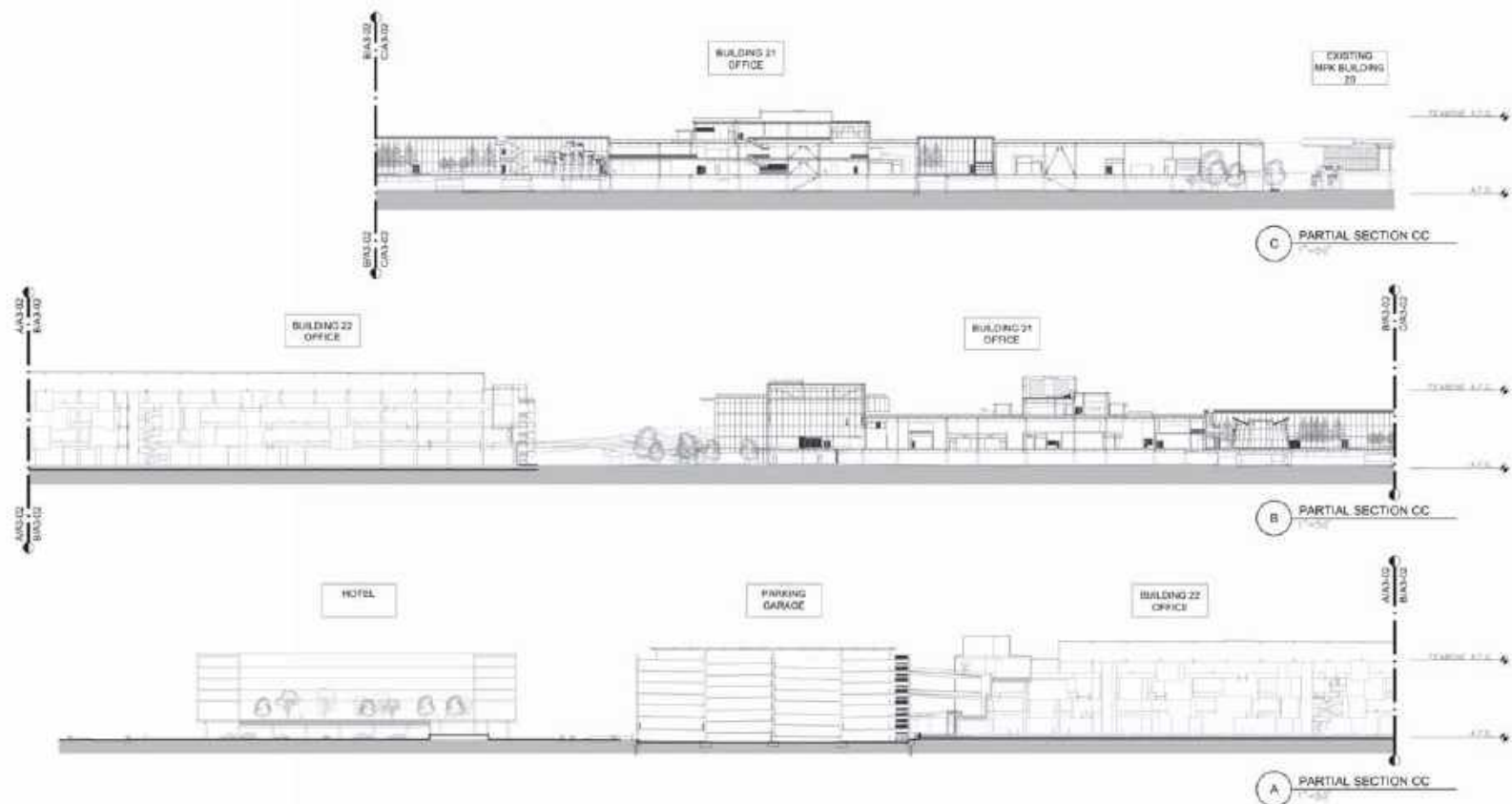
BUILDING 21, 22 & HOTEL
 SITE SECTIONS

PROJECT NUMBER
 2015-007
 SHEET NUMBER
 AS NOTED

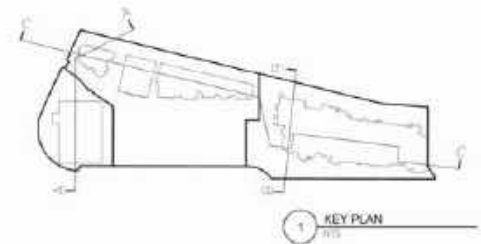
SEPTEMBER 24, 2015
 DATE PLOTTED: 09/24/15
 DRAWING SCALE: 1/8" = 1'-0"

A3-01

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- NOTES**
1. PROPOSED HOTEL IS SHOWN FOR ILLUSTRATIVE PURPOSES ONLY
 2. MKR OCCUPY FLOOR AREA SHALL HEIGHT LIMIT
 3. A.P.G. = AVERAGE FRESH GRADE
 4. A.N.G. = AVERAGE NATURAL GRADE



GEHRY PARTNERS, LLP
 ARCHITECT
 3000 BOULEVARD STREET
 LOS ANGELES, CALIFORNIA 90008
 310.798.3000

FACEBOOK
 OWNER
 15000 BOULEVARD
 MENLO PARK, CALIFORNIA 94025

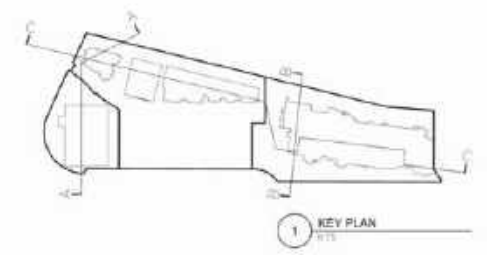
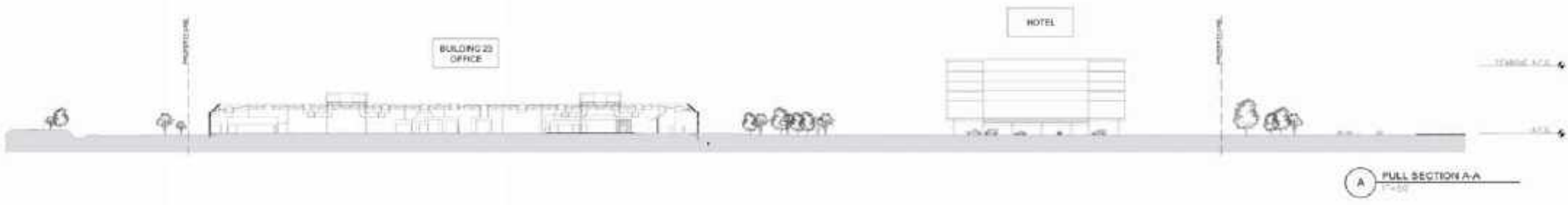
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

BUILDING 21, 22 & HOTEL
SITE SECTION

PROJECT NUMBER
 2018-007
 SHEET
 AS NOTED

SEPTEMBER 28, 2017
 10:15 AM '17
 10:15 AM '17

SHEET NUMBER
A3-02



NOTES

1. PROPOSED HOTEL IS SHOWN FOR ILLUSTRATIVE PURPOSES ONLY.
2. MAX OCCUPANCY FLOOR AREA BEGINS HEIGHT LIMIT.
3. A.F.G. = AVERAGE FINISH GRADE.
4. A.N.G. = AVERAGE NATURAL GRADE.

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 SUITE 1000, OAKLAND, CA 94612
 (415) 778-1000

FACEBOOK
 OWNER
 16000 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

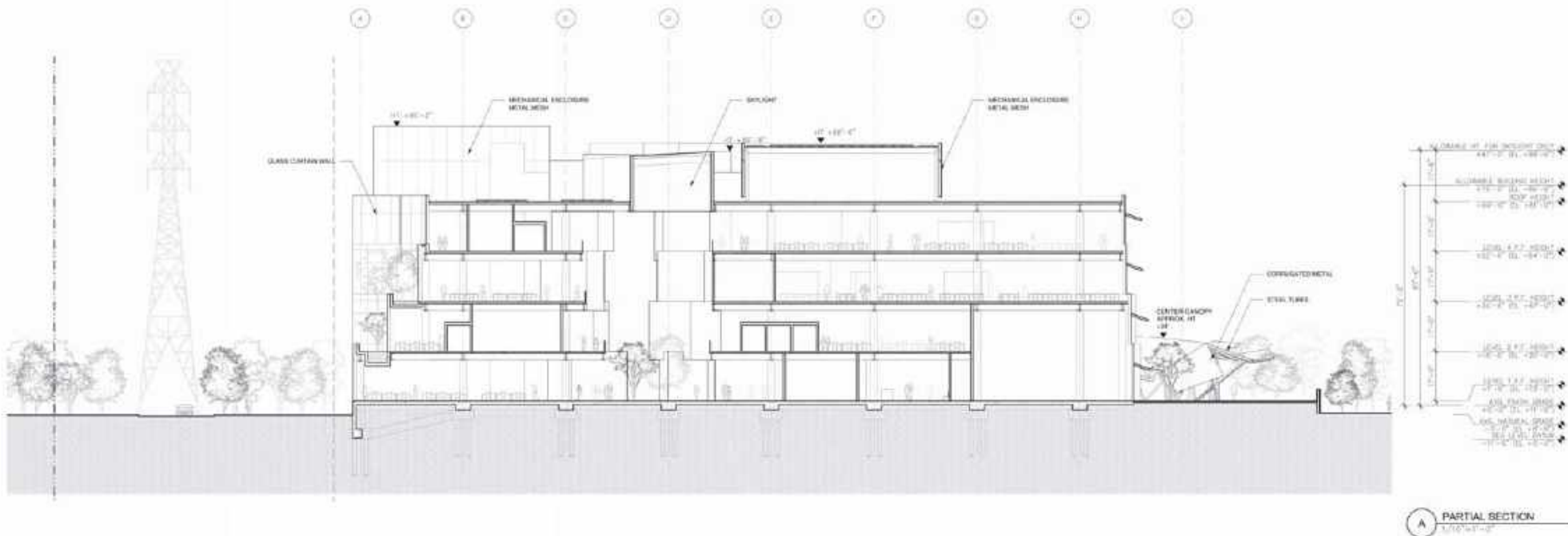
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
BUILDING 21,23 & HOTEL
SITE SECTIONS

PROJECT NUMBER
2015-007
 DATE
 AS NOTED

SEPTEMBER 26, 2017
 THIS SHEET SIZE: 18" X 24"
 DRAWING SHEET NO.: 24' x 36'

SHEET NUMBER
A3-03



GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BAYVIEW BLVD
 SUITE 1000, SAN FRANCISCO, CA 94134
 (415) 774-2000

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

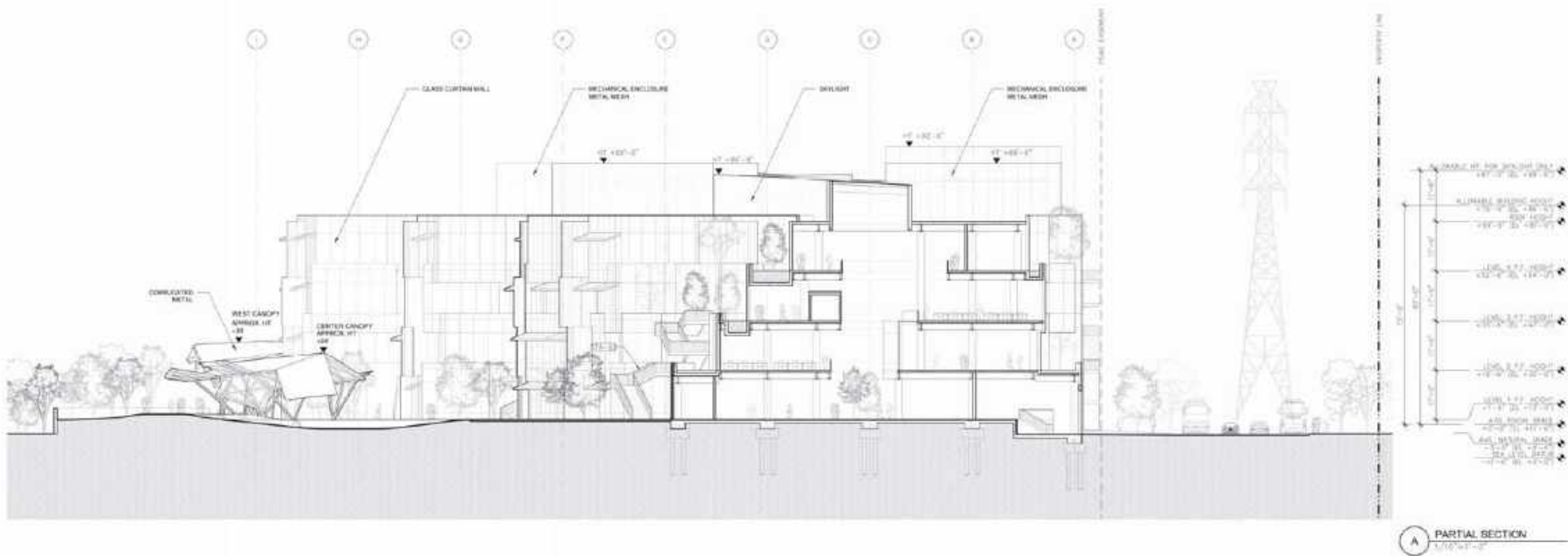
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

BUILDING 22
 ENLARGED SECTION

PROJECT NUMBER
 2015-007
 DATE
 AS NOTED

A3-04

SEPTEMBER 28 2015
 11:14 AM



GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BAYVIEW BLVD.
 SUITE 1000, CANTONMENT, CA 94010
 415.433.1000

FACEBOOK
 OWNER
 16000 AVENUE
 REDWOOD CITY, CALIFORNIA 94063

09/03/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

BUILDING 22
ENLARGED SECTIONS

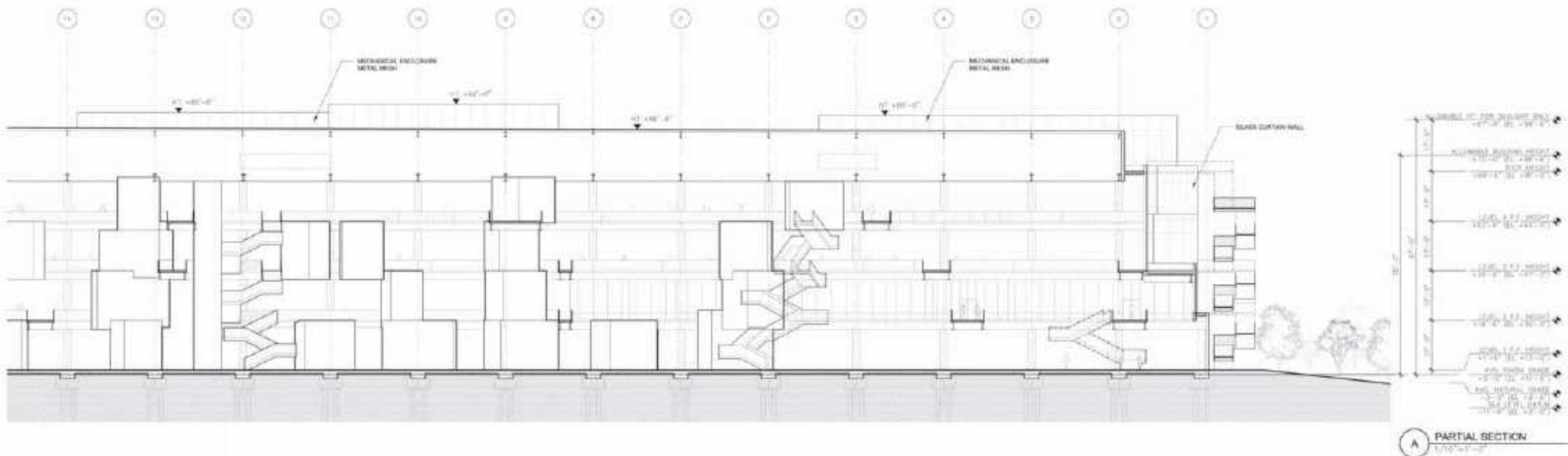
PROJECT NUMBER
 2015-027
 SHEET
 AS NOTED

SEPTEMBER 28, 2017
 DATE PLOTTED: 9/28/17
 DRAWING SHEET NO.: 24 OF 24

SHEET NUMBER

A3-05

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A PARTIAL SECTION
1/16"=1'-0"

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 ARCHITECT
 1000 BAYVIEW BLVD
 SUITE 1000, OAKLAND, CA 94612
 (415) 779-1000

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

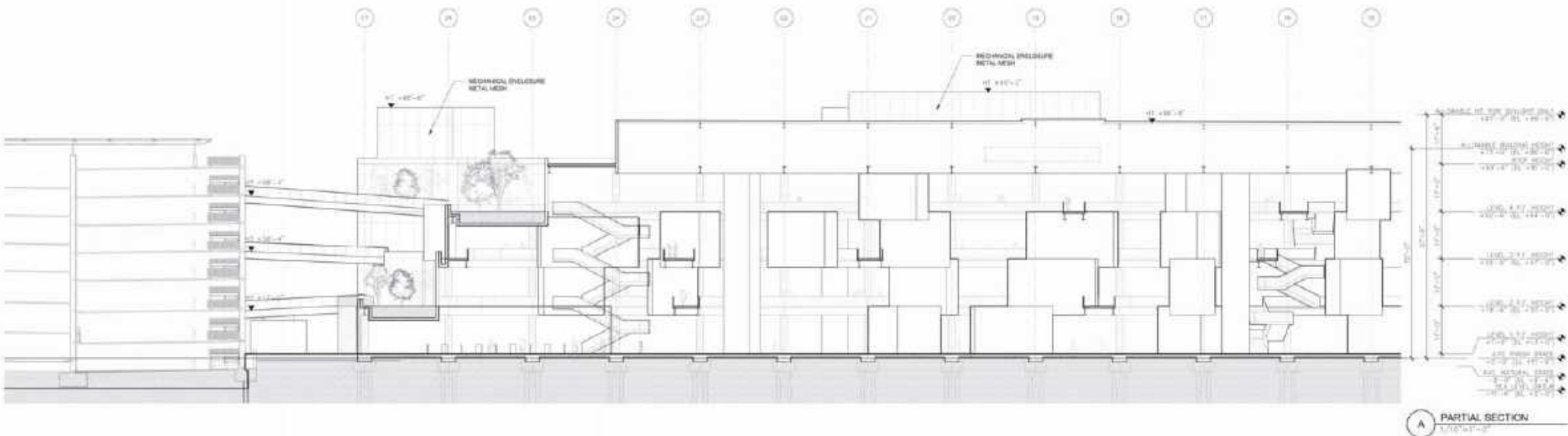
BUILDING 22
 ENLARGED SECTIONS

PROJECT NUMBER
 2015-027
 DATE
 AS NOTED

SHEET NUMBER
A3-06

SEPTEMBER 28, 2017
 DATE PLOTTED: 09/28/17
 DRAWING SHEET NO.: 04 - 06

GHP/MLP/MLP



A PARTIAL SECTION
1/16"=1'-0"

GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BAYVIEW BLVD
 SUITE 1000, SAN FRANCISCO, CA 94134
 415.774.2000
 WWW.GEHRYPARTNERS.COM

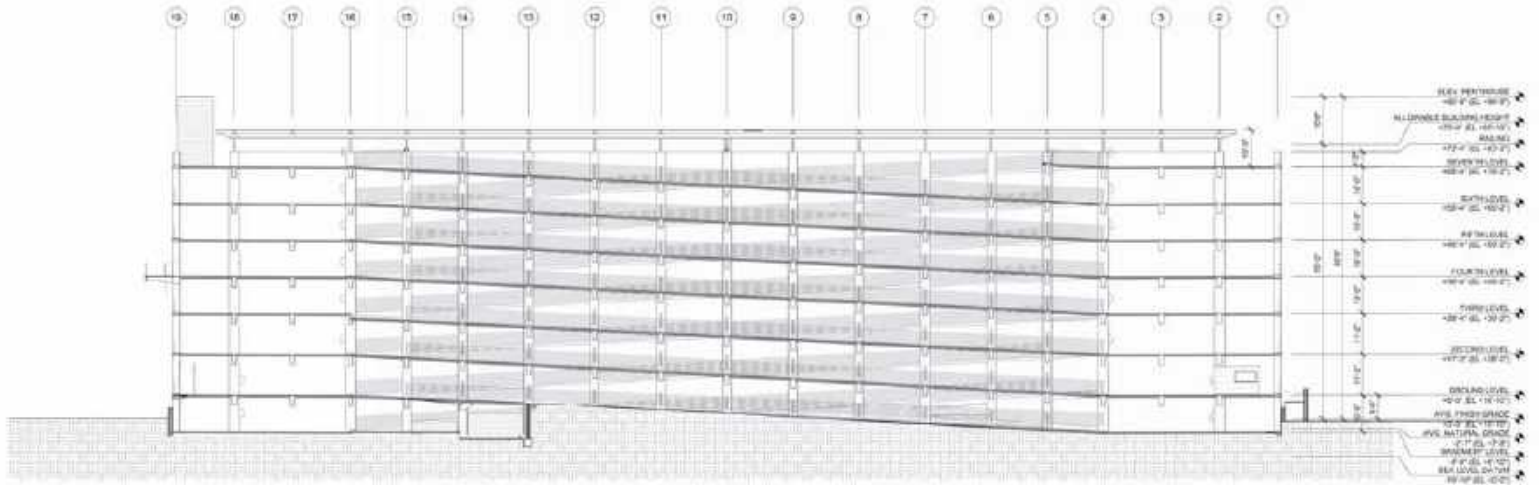
09/20/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

BUILDING 22
 ENLARGED SECTIONS

PROJECT NUMBER
 2015-027
 DATE
 AS NOTED
 SEPTEMBER 28, 2017
 FILED UNDER 916.14
 APPROX. SHEET NO. 24 OF 34

SHEET NUMBER
 A3-07
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A SECTION
1/30'-1'-0"

WATRY DESIGN, INC.
ARCHITECT
2000 34th Street, Suite 400
San Jose, California 95131
408.262.7500

FACEBOOK
OWNER
1 Hacker Way
Menlo Park, California 94025

09/24/15 (1)

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

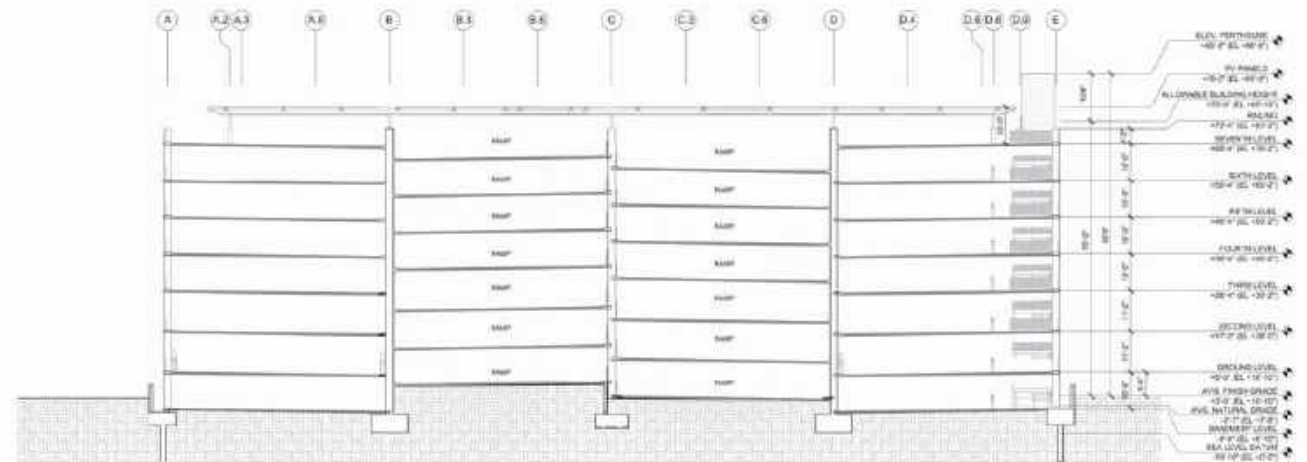
PARKING STRUCTURE
LONGITUDINAL SECTION

PROJECT NUMBER
2015-007
DATE
AS NOTED

SHEET NUMBER
A3-08

SEPTEMBER 28, 2015
PLotted: 2015-09-15
CHECKED: 2015-09-15

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A SECTION
1/30"=1'-0"

WATRY DESIGN, INC.
ARCHITECT
2000 34 STREET, SUITE 200
SAN ANTONIO, TEXAS 78207
PHONE: 214-343-7500
FAX: 214-343-7501

FACEBOOK
OWNER
1000 BAYVIEW BLVD.
MENLO PARK, CALIFORNIA 94025

09/24/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

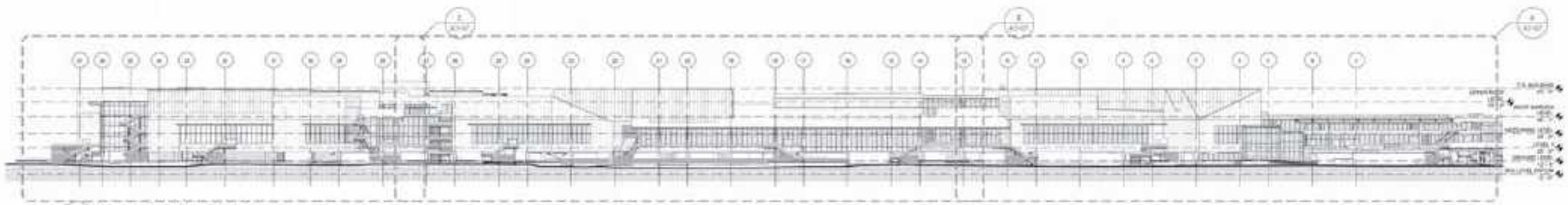
PARKING STRUCTURE
TRANSVERSE SECTIONS

PROJECT NUMBER
2015-007
DATE
AS NOTED
SEPTEMBER 28, 2017
PLANS SHEET NO. 16 OF 16
ISSUED SHEET NO. 24 OF 24

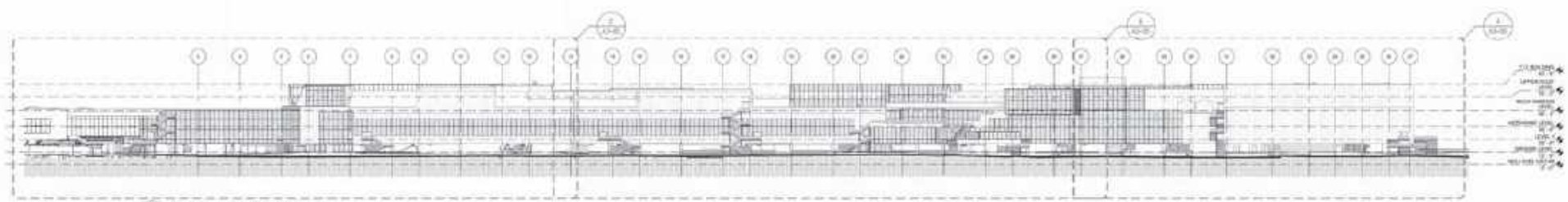
SHEET NUMBER
A3-09
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NOTES:

1. BUILDING ELEVATIONS ARE PRELIMINARY APPROVAL
2. THESE ELEVATIONS ARE MEANT FOR CONCEPT ONLY AND ARE NOT PART OF THE PROJECT



B SOUTH ELEVATION KEY PLAN
SCALE: 1" = 50'



A NORTH ELEVATION KEY PLAN
SCALE: 1" = 50'

GEHRY PARTNERS, LLP
ARCHITECT
1001 BROADWAY, SUITE 111
NEW YORK, NY 10018
212 675-1000

FACEBOOK
OWNER
1600 ZEEB ROAD
MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

MPK21
NORTH & SOUTH
BUILDING ELEVATIONS

PROJECT NUMBER
2015-007

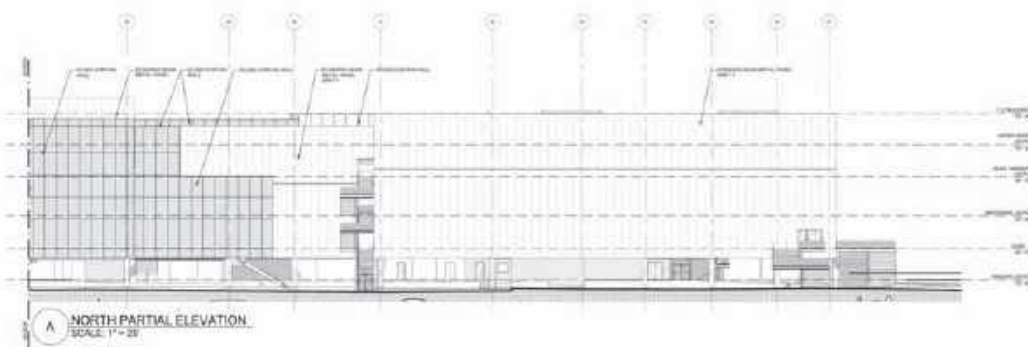
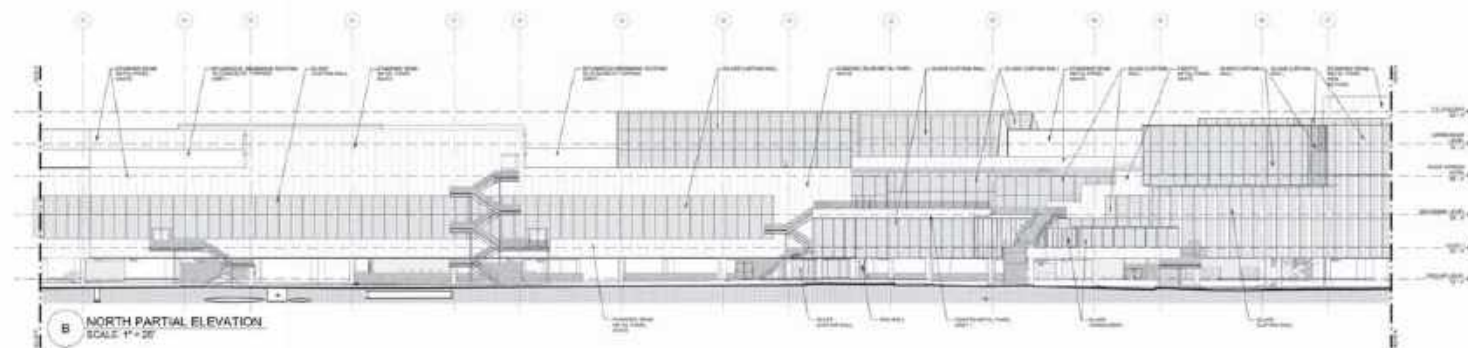
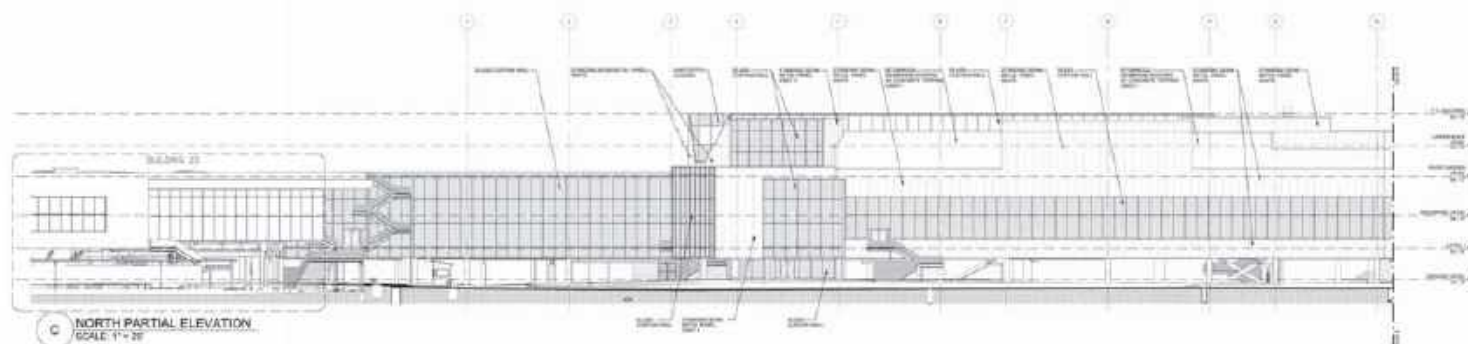
DATE
11-1-15

SEPTEMBER 28, 2015

JULY 2015 REV. 01-15
ISSUED SHEET 01-15 OF 15

SHEET NUMBER
A3-10

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NOTES:
 1. BUILDING ELEVATIONS ARE PREVIOUSLY APPROVED
 2. THESE ELEVATIONS ARE SHOWN FOR CONTEXT ONLY AND ARE NOT PART OF THE PROJECT.

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 ARCHITECT
 1001 CALLE DE SERRA
 SAN FRANCISCO, CALIFORNIA 94104
 (415) 774-2000

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

MPK21
 NORTH
 BUILDING ELEVATIONS

PROJECT NUMBER
 2015-007

DATE
 11.4.20

SEPTEMBER 28 2017

DATE PREPARED BY
 JEFFREY W. WILSON

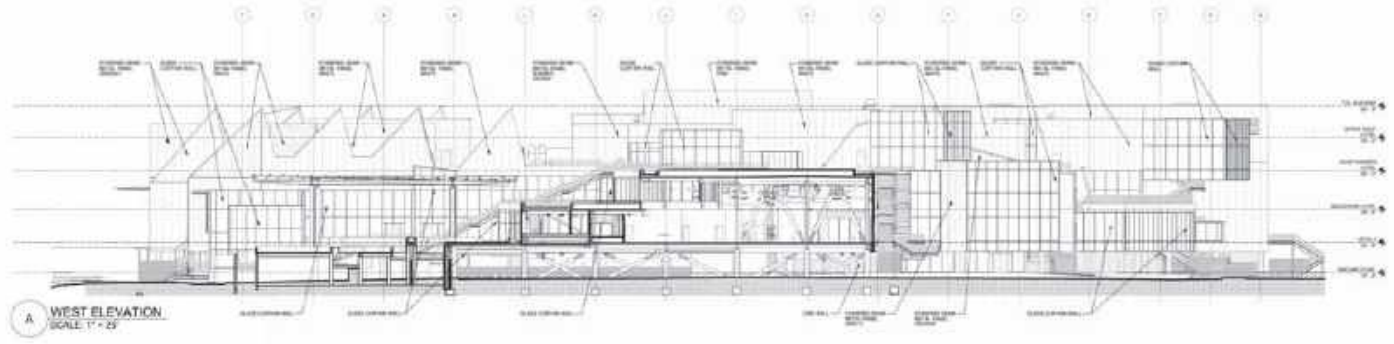
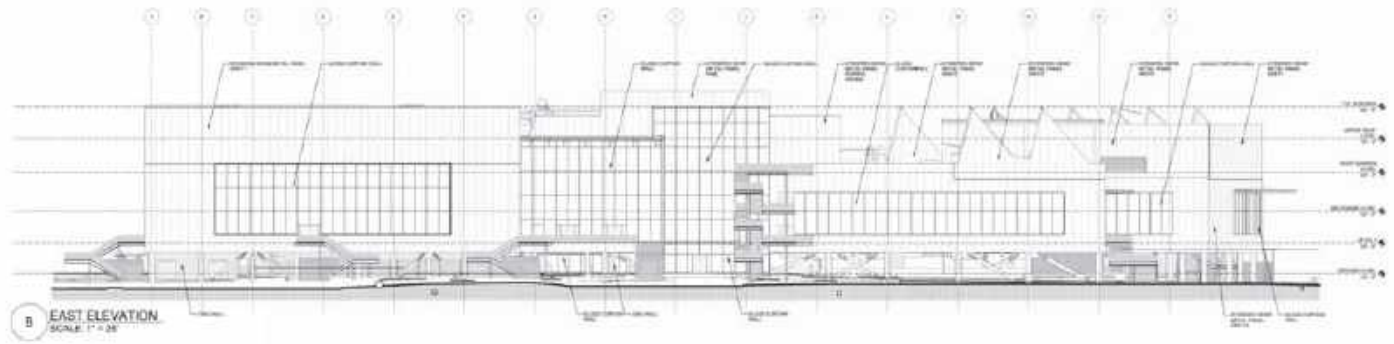
DATE SHEET PREPARED BY
 JEFFREY W. WILSON

A3-11

GEHRY PARTNERS, LLP

NOTES:

- 1. BUILDING ELEVATIONS ARE PREVIOUSLY APPROVED
- 2. EXISTING BUILDING CONSTRUCTION AND INTERIOR FINISH ARE NOT PART OF THE PROJECT



GEHRY PARTNERS, LLP
ARCHITECT
1001 BROADWAY STREET
SAN FRANCISCO, CALIFORNIA 94103
(415) 774-2000

FACEBOOK
OWNER
1600 AVENUE AVENUE
MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

MPK21
EAST & WEST
BUILDING ELEVATIONS

PROJECT NUMBER
2015-007

SCALE
1" = 20'

SEPTEMBER 28, 2015

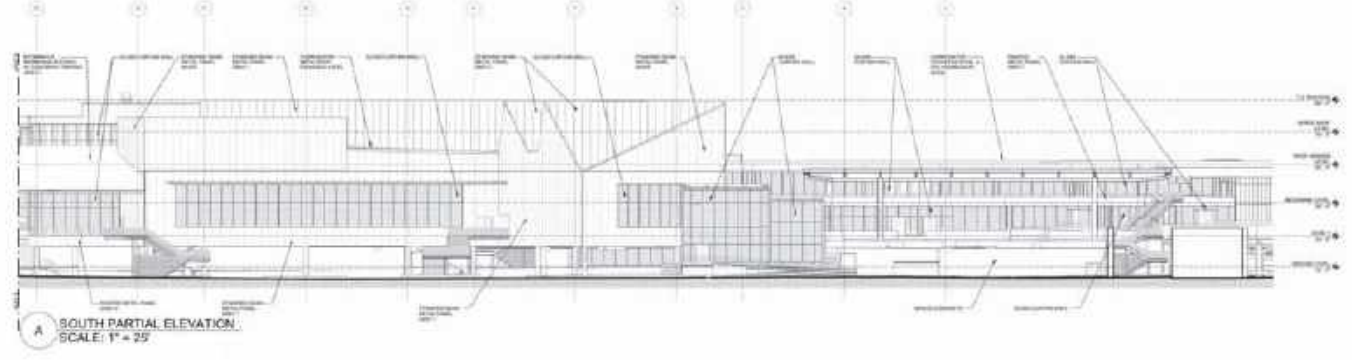
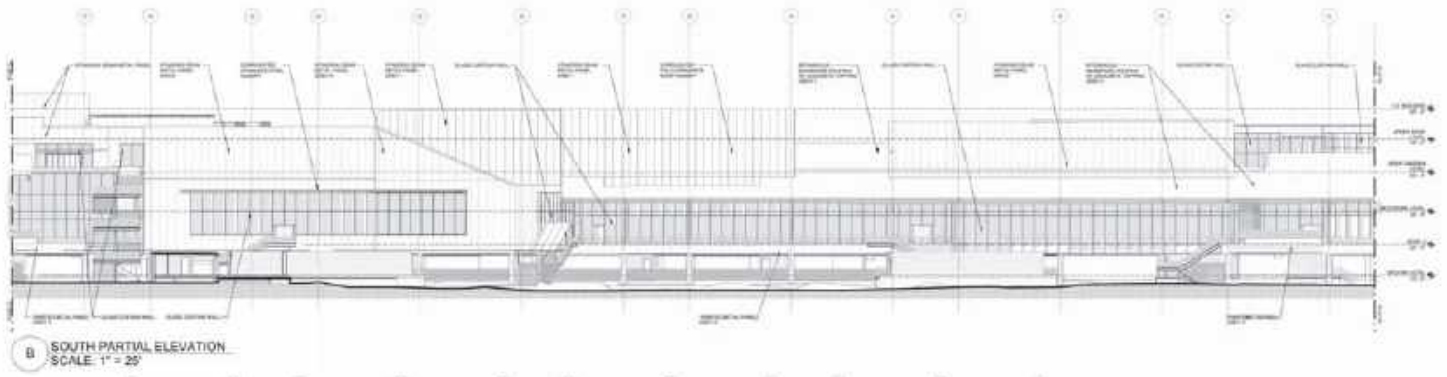
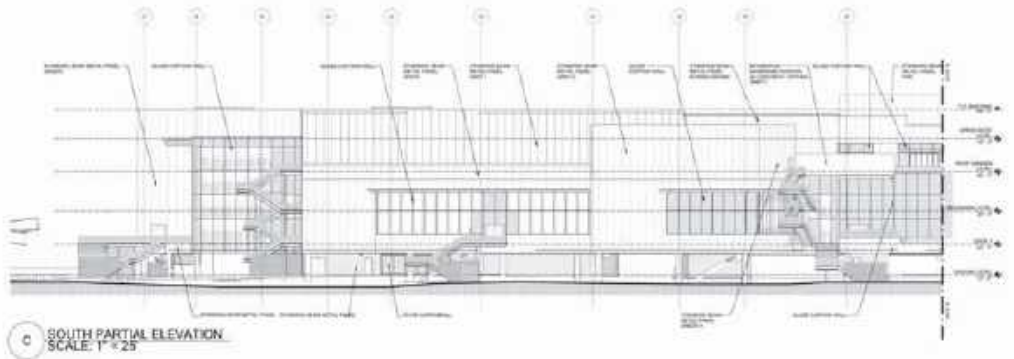
DATE PLOTTED: 9/15/15
DRAWING SHEET NO.: 24' x 36'

A3-12

GEHRY PARTNERS, LP

NOTES:

- 1. BUILDING ELEVATIONS ARE PREPARED BY ARCHITECT
- 2. THESE ELEVATIONS ARE 0-COUNT FOR CONSTRUCTION AND ARE NOT PART OF THE PROJECT



GEHRY PARTNERS, LLP
ARCHITECT
1001 BROADWAY, SUITE 111
NEW YORK, NY 10018
212 675-1000

FACEBOOK
OWNER
1601 AVENUE OF THE STARS
MENLO PARK, CALIFORNIA 94025

STRUCTURES

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

MPK21
SOUTH
BUILDING ELEVATIONS

PROJECT NUMBER
2015-007

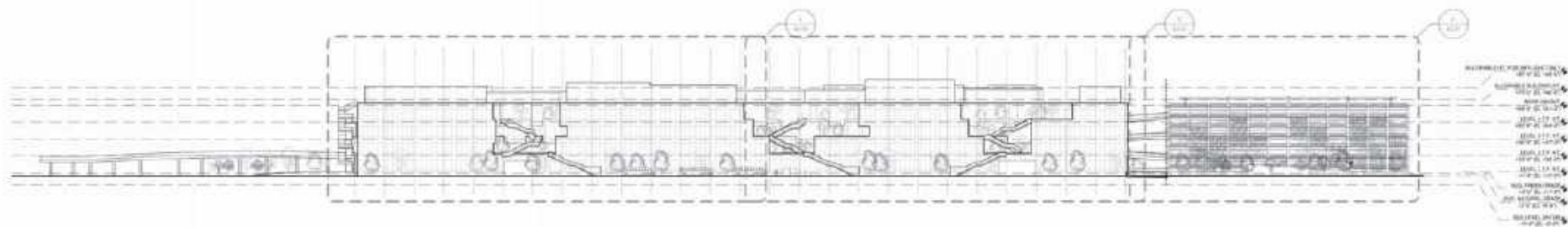
DATE
11.4.15

SEPTEMBER 28, 2017

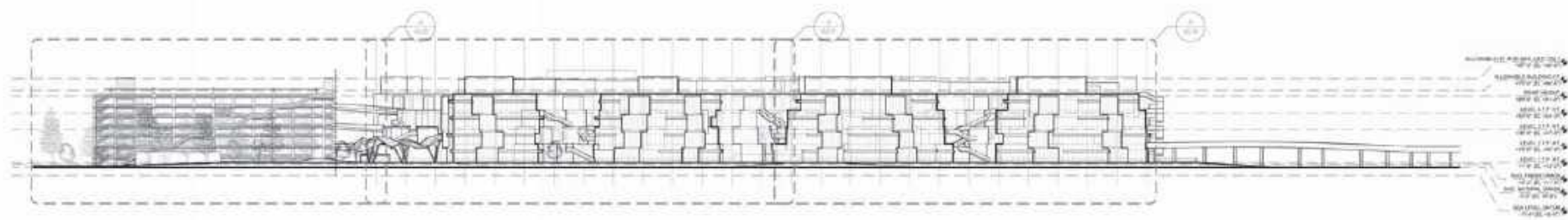
ISSUE NUMBER
02/15

A3-13

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B NORTH ELEVATION KEY PLAN
SCALE 1/4"=30'



A SOUTH ELEVATION KEY PLAN
SCALE 1/4"=30'

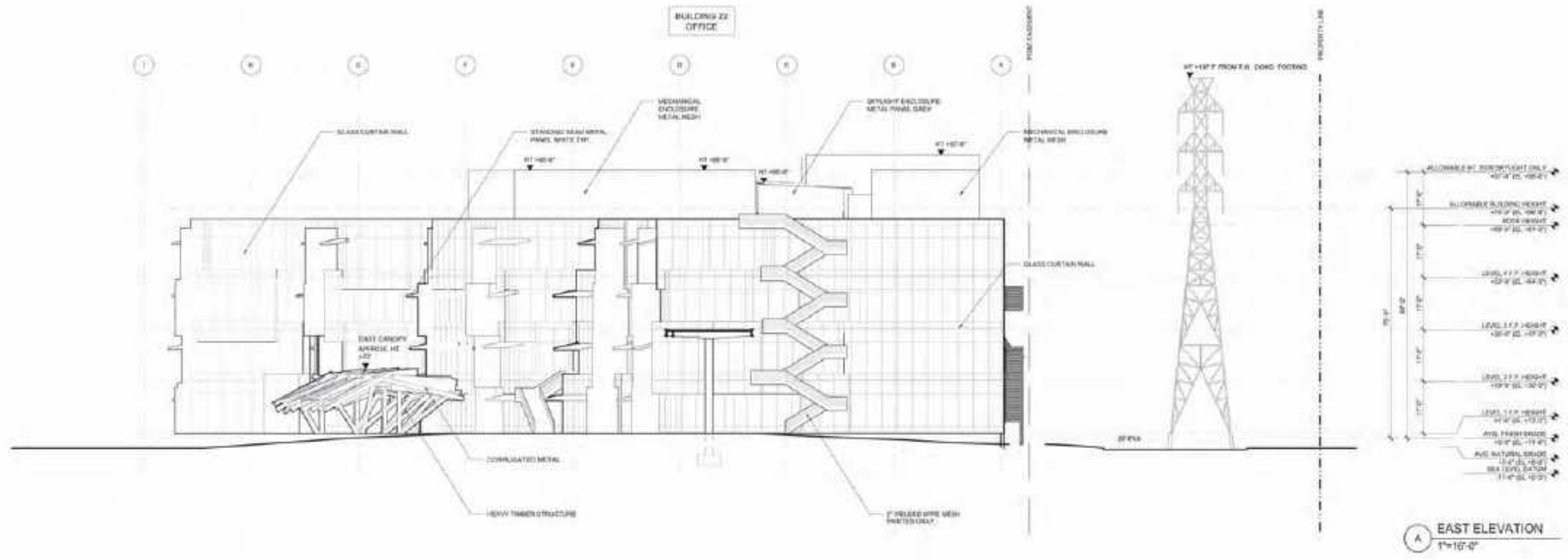
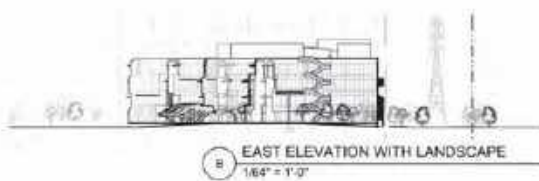
GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BAYVIEW BLVD
 SUITE 1000, OAKLAND, CA 94612
 (415) 779-1000
FACEBOOK
 OWNER
 1600 4TH AVE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

MPK22
 NORTH AND SOUTH
 BUILDING ELEVATIONS

PROJECT NUMBER
 2015-027
 DATE
 11-10-15
 SEPTEMBER 28 2015
 1/4"=30' (SEE 15-15)
 DRAWING SHEET NO. 24 OF 24

SHEET NUMBER
A3-14
 © GEHRY PARTNERS, LLP



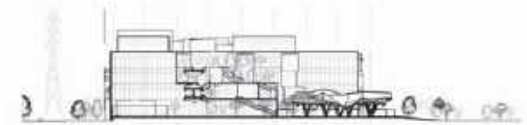
ALLOWABLE SIGHTING ONLY	+52'-0" (EL. +68'-2")
ALLOWABLE BUILDING HEIGHT	+33'-0" (EL. +49'-2")
ROOF HEIGHT	+48'-0" (EL. +64'-2")
LEVEL 1 F.F. HEIGHT	+33'-0" (EL. +49'-2")
LEVEL 2 F.F. HEIGHT	+38'-0" (EL. +54'-2")
LEVEL 3 F.F. HEIGHT	+43'-0" (EL. +59'-2")
LEVEL 4 F.F. HEIGHT	+48'-0" (EL. +64'-2")
AVG. FINISH GRADE	+30'-0" (EL. +46'-0")
AVG. FINISH GRADE	+35'-0" (EL. +51'-0")
SEA LEVEL	0'-0" (EL. 0'-0")

A EAST ELEVATION
1"=16'-0"

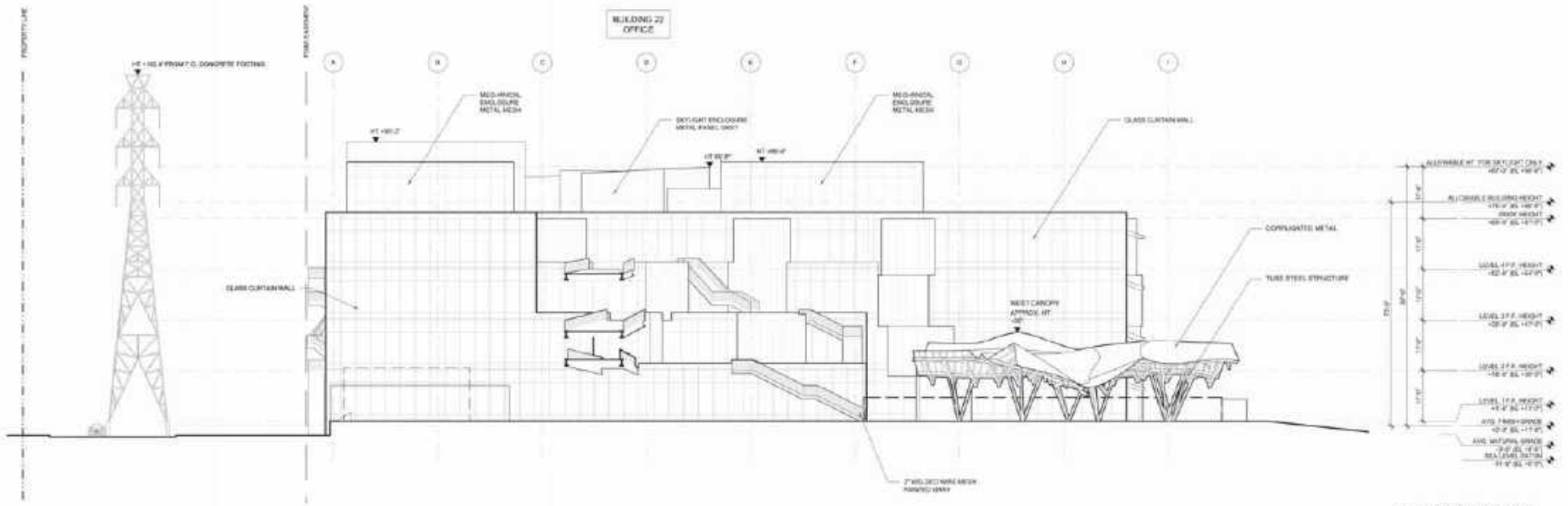
NOTES:

1. UNOCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
2. LANDSCAPE REACHED FOR CLARITY AS DIRECTED BY CITY OF MALDEN

<p>GEHRY PARTNERS, LLP ARCHITECT 400 BRIDGE STREET CAMBRIDGE, MASSACHUSETTS 02142 (617) 452-1200</p> <p>FACEBOOK OWNER 1 FRONT STREET MALDEN, MASSACHUSETTS 02148</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>MPK22 EAST BUILDING ELEVATION</p>	<p>PROJECT NUMBER 2015-037 DATE 1/13/15 SCALE 1/8" = 1'-0"</p>	<p>PROJECT NUMBER A3-15 DATE PLOTTED SEPTEMBER 28, 2015 PLOT SCALE 1/8" = 1'-0"</p>
---	--	---	--	--



B WEST ELEVATION WITH LANDSCAPE
1/8" = 1'-0"



A WEST ELEVATION
1/16" = 1'-0"

- NOTES:**
1. UNOCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
 2. LANDSCAPE PROVIDED FOR CLARITY AS DIRECTED BY CITY OF WASHINGTON

GEHRY PARTNERS, LLP
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ONE BATTERY PLACE
NEW YORK, NY 10004
TEL: 212 675-1000
WWW.GEHRYPARTNERS.COM

FACEBOOK
OWNER
1601 AVENUE K
COLUMBIA, MD 21046

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

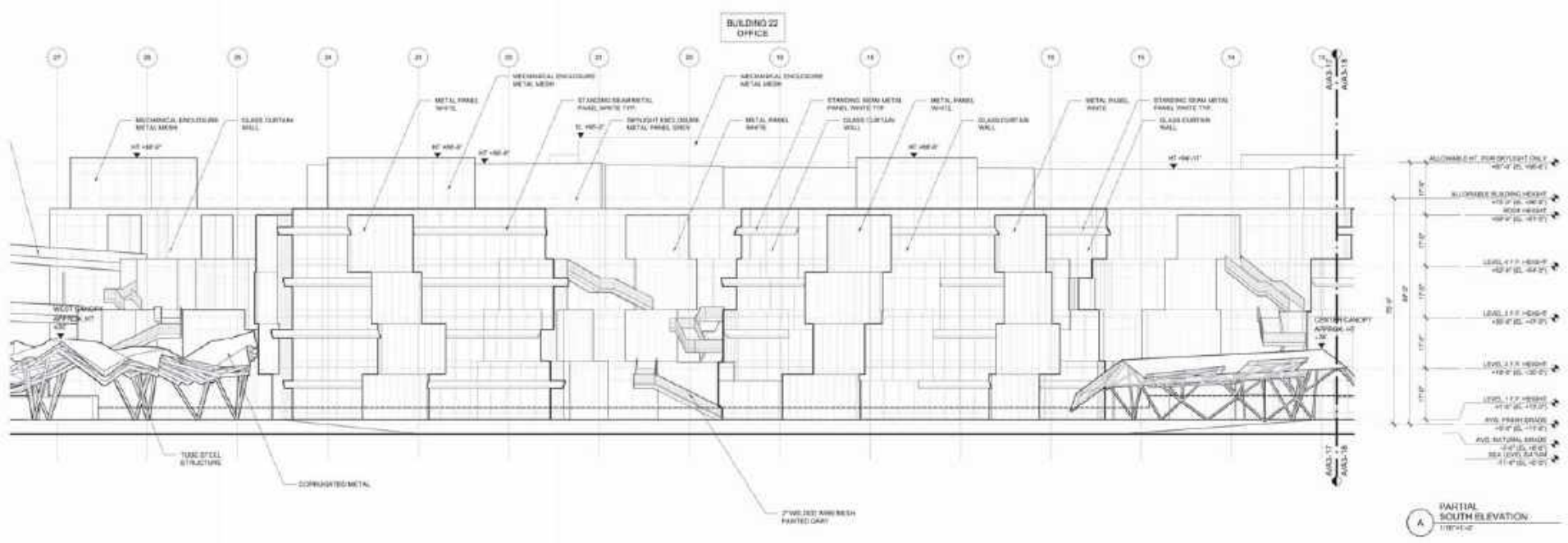
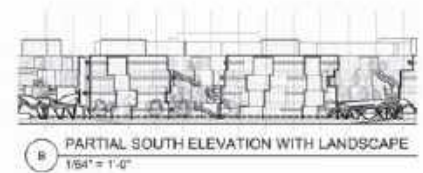
MPK22
WEST BUILDING ELEVATION

PROJECT NUMBER
2015-007
SCALE
1/16" = 1'-0"

DATE
SEPTEMBER 28, 2015
DRAWN BY
GUY HOFFMAN, AIA
CHECKED BY
GUY HOFFMAN, AIA

SHEET NUMBER
A3-16

© GUY HOFFMAN, AIA



- NOTES:**
1. UNOCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
 2. LANDSCAPE REMOVED FOR CLARITY AS DIRECTED BY CITY OF BERKELEY

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 ARCHITECT
 1400 BAYVIEW DRIVE
 SUITE 2000, SAN FRANCISCO, CA 94134
 (415) 774-2000

FACEBOOK
 OWNER
 1600 BAY STREET, SUITE 1000
 SAN FRANCISCO, CA 94133

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

154
MPK22
SOUTH
BUILDING ELEVATIONS

PROJECT NUMBER
 2018-007
 SCALE
 1/8" = 1'-0"

DATE
 SEPTEMBER 28, 2017

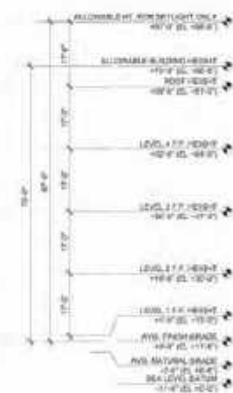
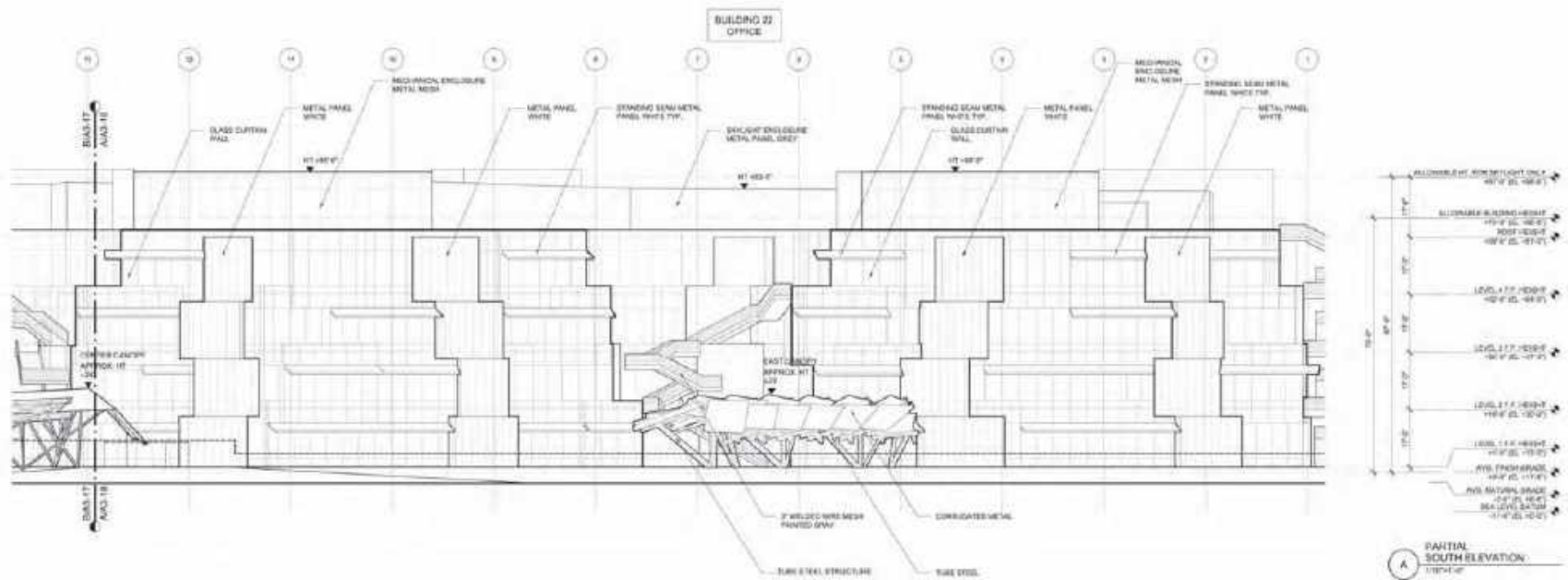
DESIGNED BY
 TONY BOUTON, S.F.A.
 DRAWN BY
 JEFFREY W. CHAN, S.F.A.

SHEET NUMBER
A3-17

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B PARTIAL SOUTH ELEVATION WITH LANDSCAPE
1/8" = 1'-0"



A PARTIAL SOUTH ELEVATION
1/8" = 1'-0"

NOTES:
 1. UNOCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
 2. LANDSCAPE REACHED FOR CLARITY AS DIRECTED BY CITY OF MILWAUKEE

GENRY PARTNERS, LLP
 ARCHITECT
 1000 WISCONSIN STREET
 SUITE 2000, MILWAUKEE, WISCONSIN 53233
 TEL: 414.224.4444
 WWW.GENRY.COM

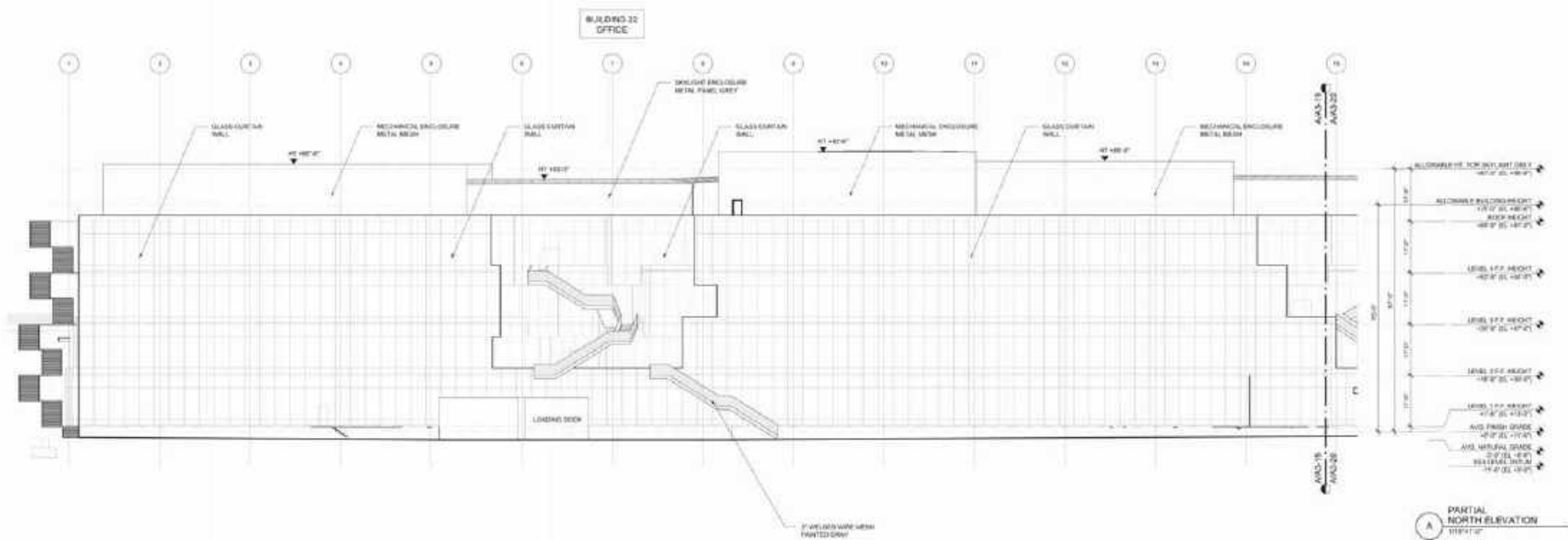
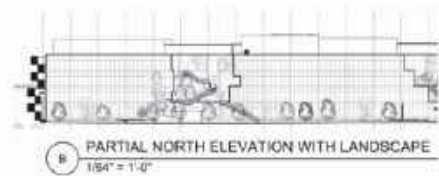
FACEBOOK
 OWNER
 1601 RIVER ROAD
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

MPK22
 SOUTH
 BUILDING ELEVATIONS

PROJECT NUMBER
 2016-007
 SCALE
 1/8" = 1'-0"
 DATE
 SEPTEMBER 28, 2017
 DRAWN BY: J. L. L. & S. L.
 CHECKED BY: J. L. L. & S. L.

SHEET NUMBER
 A3-18
 DESIGN PARTNER: J.P.



GEHRY PARTNERS LLP
 ARCHITECT
 1000 BAYVIEW STREET
 15TH FLOOR, SUITE 1500
 SAN FRANCISCO, CA 94133
 TEL: 415.774.2000
 WWW.GHPARTNERS.COM

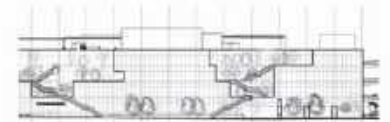
FACEBOOK
 OWNER
 1600 CALIFORNIA AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

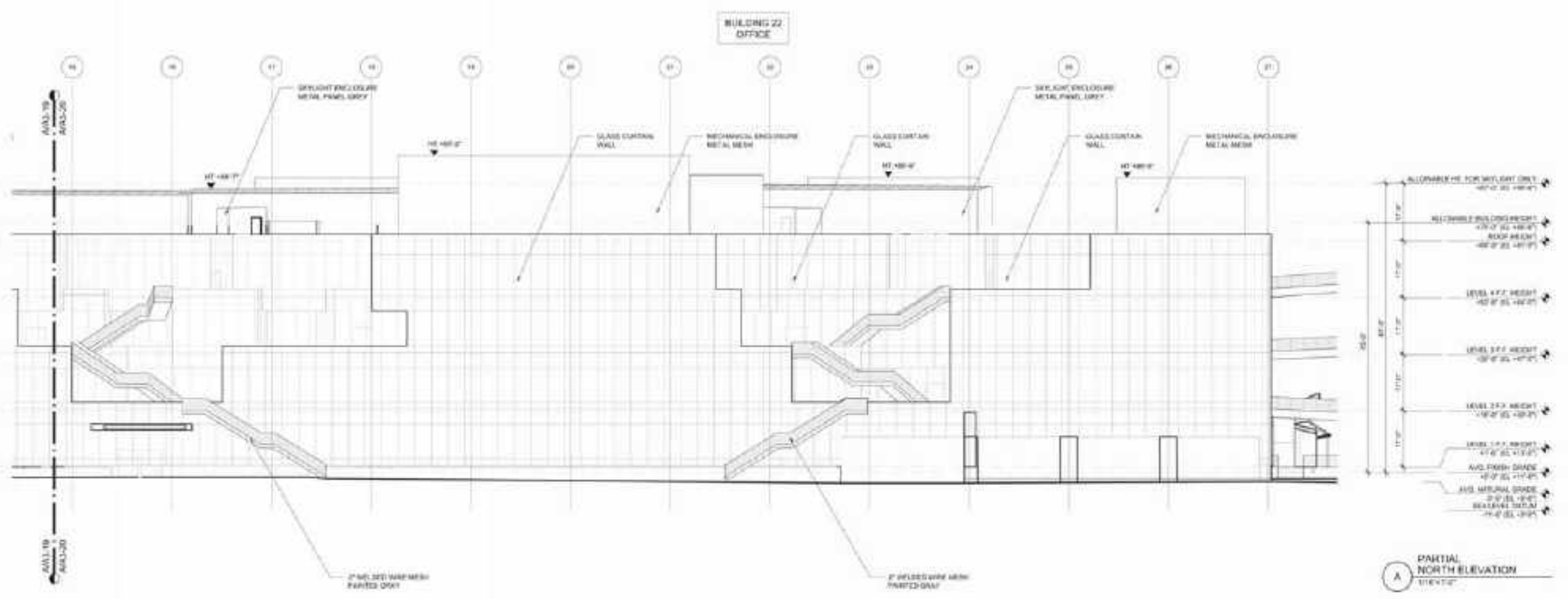
THE
MPK22
NORTH
BUILDING ELEVATIONS

PROJECT NUMBER
 2015-007
 SCALE
 1/16" = 1'-0"
 DATE
 SEPTEMBER 28, 2017
 FILE NUMBER
 15-007-001-113
 SHEET NUMBER
 24 OF 24

A3-19
 © GEHRY PARTNERS, LP



B PARTIAL NORTH ELEVATION WITH LANDSCAPE
1/8" = 1'-0"



A PARTIAL NORTH ELEVATION
1/8" = 1'-0"

NOTES:
 1. NO OCCUPIED FLOOR AREA ABOVE HEIGHT LIMIT
 2. LANDSCAPE PROVIDED FOR CLARITY AS DIRECTED BY CITY OF BERKELEY

GENRY PARTNERS, LLP
 ARCHITECT
 1001 BAYVIEW STREET
 SAN FRANCISCO, CALIFORNIA 94133
 (415) 774-2000

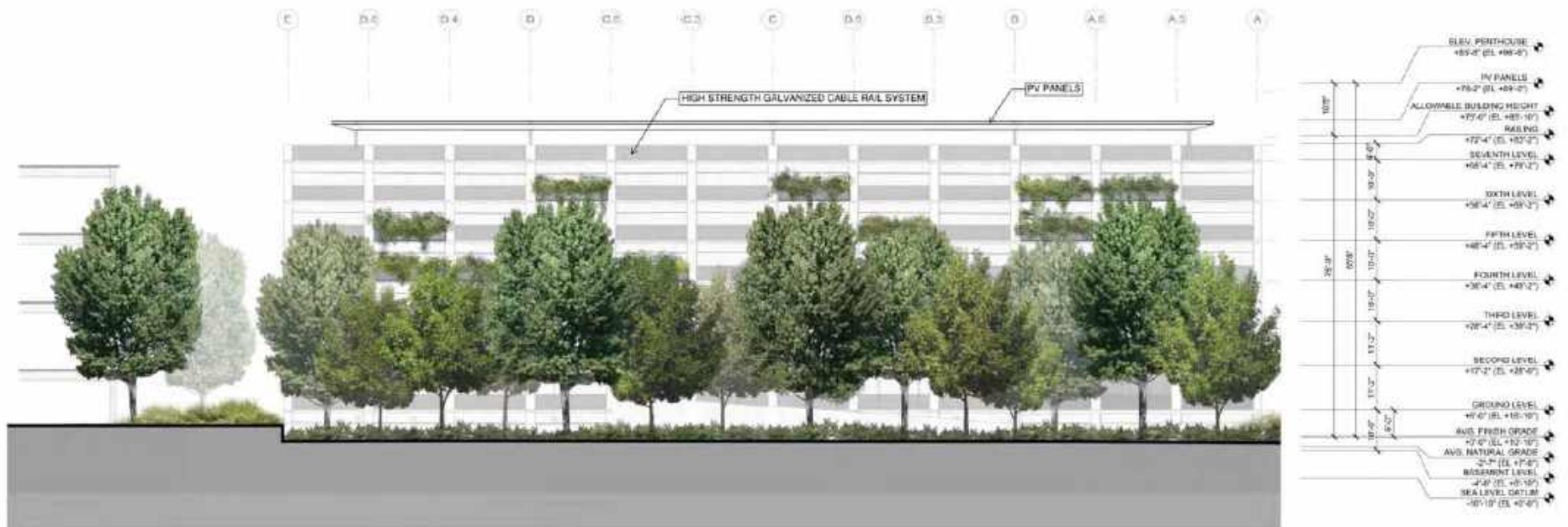
FACEBOOK
 OWNER
 13000 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

MPK22
 NORTH
 BUILDING ELEVATIONS

PROJECT NUMBER
 2016-002
 DATE
 1/18/17 = 1'-0"
 SEPTEMBER 28, 2017
 SHEET NUMBER: 217 of 217

SHEET NUMBER
A3-20



WATRY DESIGN, INC.
 ARCHITECT
 2000 W. 10TH AVENUE
 SUITE 200, CALIFORNIA, CA 91601
 (415) 399-7100

FACEBOOK
 OWNER
 1 WATSON WAY
 MENLO PARK, CALIFORNIA 94025

DATE PLOTTED

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
PARKING STRUCTURE
NORTH ELEVATION

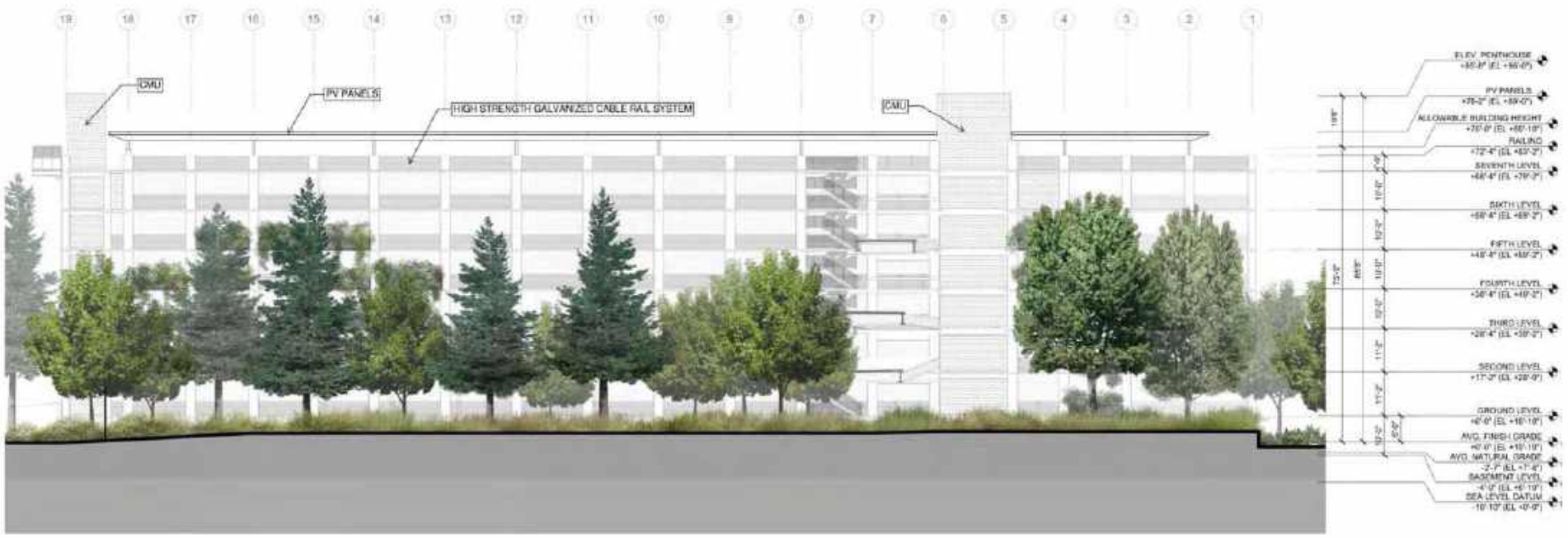
PROJECT NUMBER
 2016-007
 SCALE
 AS NOTED

DATE PLOTTED
 SEPTEMBER 28, 2017
 PLOT SCALE
 1/4" = 1'-0"

HEET NUMBER

A3-21

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WATRY DESIGN, INC.
 ARCHITECT
 2000 W. TRACY BL. #200
 SAN JOSE, CALIFORNIA 95128
 (408) 255-1500

FACEBOOK
 OWNER
 1 WATSON WAY
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
PARKING STRUCTURE
EAST ELEVATION

PROJECT NUMBER
 2016-007
 SCALE
 AS NOTED
 DATE
 SEPTEMBER 28, 2017
 DRAWN BY
 JAVIER SOTO, B.S.
 CHECKED BY
 JAVIER SOTO, B.S.

SHEET NUMBER
A3-22
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WATRY DESIGN, INC.
 ARCHITECT
 200 DAVENPORT BLVD.
 3RD FLOOR, SUITE 301
 FORT WORTH, TX 76102

FACEBOOK
 OWNER
 17000 HWY 121
 AUSTIN, TEXAS 78758

DATE PLOTTED

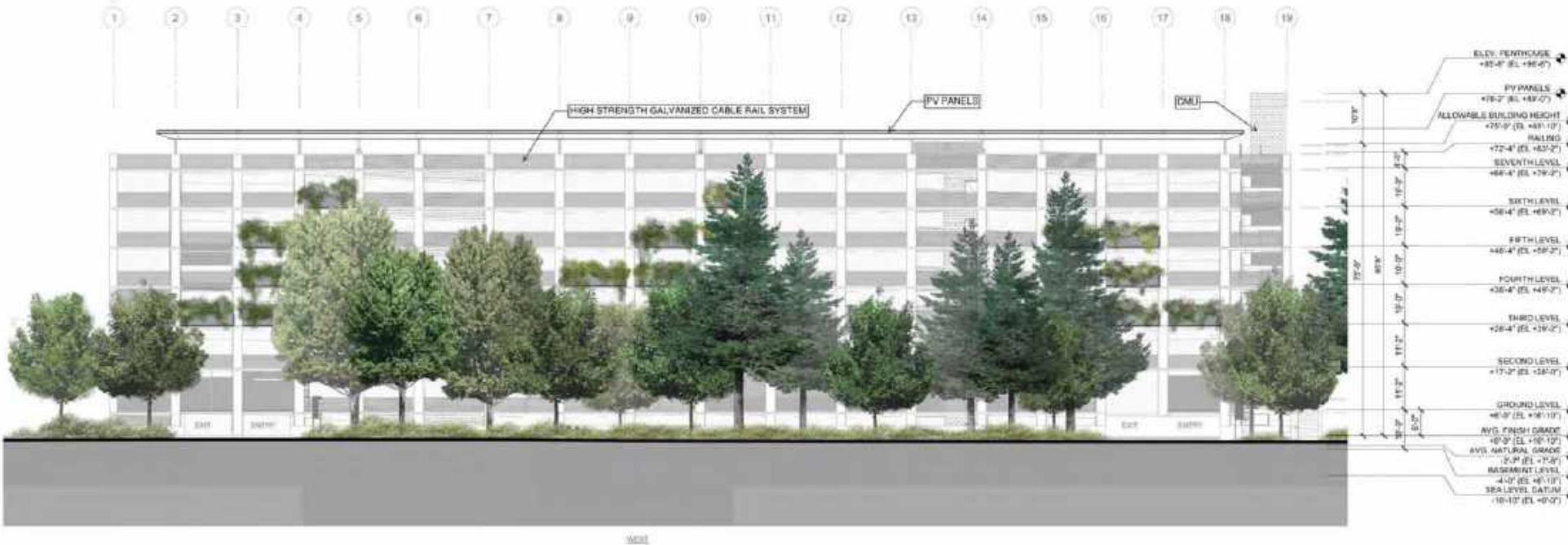
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PARKING STRUCTURE
 SOUTH ELEVATION

PROJECT NUMBER
 2016-007
 SCALE
 AS NOTED

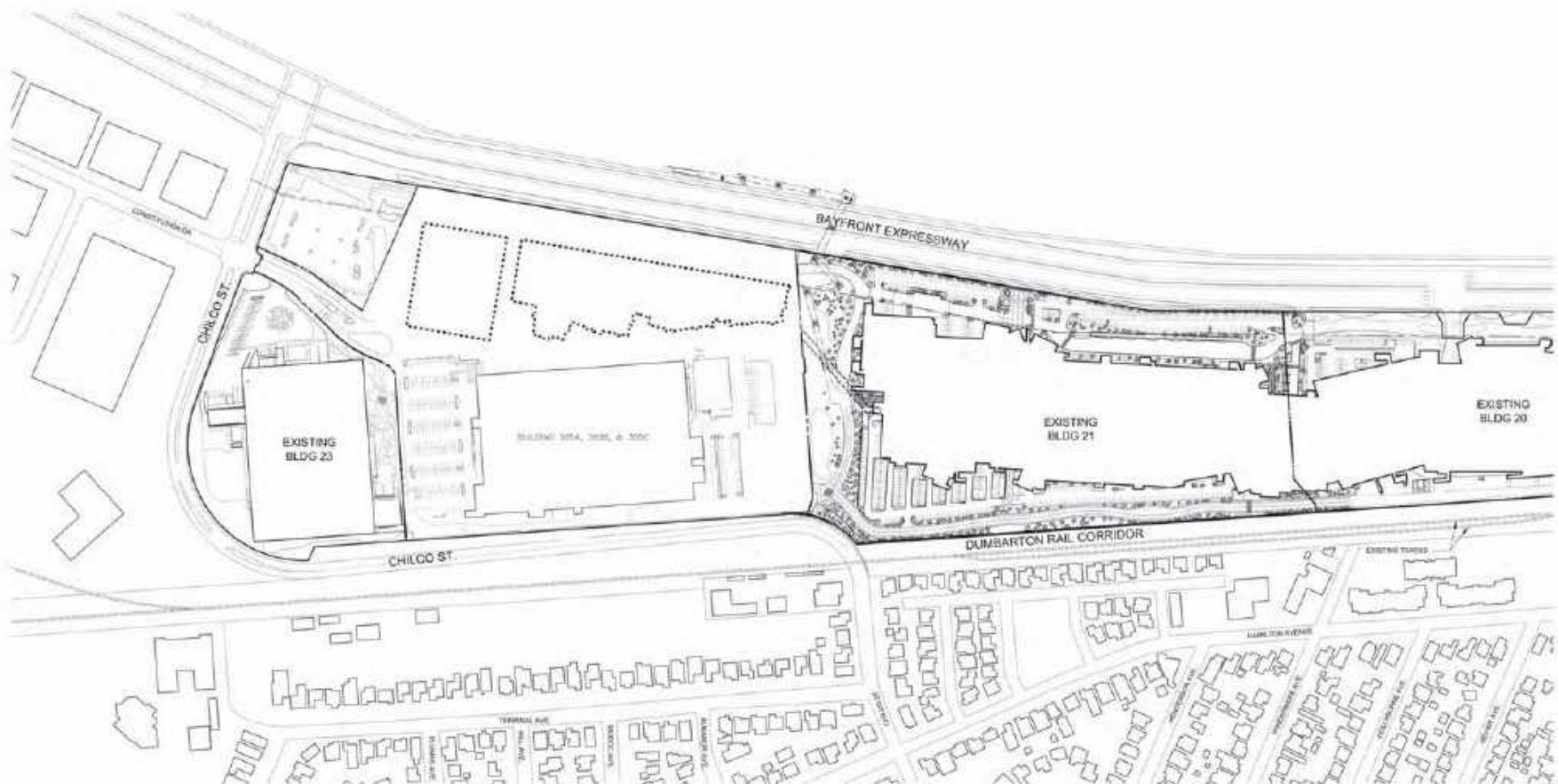
DATE PLOTTED
 SEPTEMBER 28, 2017
 PLOT NUMBER
 24" x 36" (1:12)

A3-23



WEST

<p>WATRY DESIGN, INC. ARCHITECT 2000 DEER CREEK BLVD. SAN JOSE, CALIFORNIA 95131 408.255.1500</p> <p>FACEBOOK OWNER 1 WATSON WAY MENLO PARK, CALIFORNIA 94025</p>	<p>PROJECT NAME</p> <p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p>PARKING STRUCTURE WEST ELEVATION</p>	<p>PROJECT NUMBER</p> <p>2016-007</p> <p>SCALE</p> <p>AS NOTED</p> <p>DATE</p> <p>SEPTEMBER 28, 2017</p> <p>DATE PREPARED</p> <p>BY: [Name] / [Title]</p> <p>CHECKED: [Name] / [Title]</p>	<p>SHEET NUMBER</p> <p>A3-24</p>
---	--	--	--	---



KEY

	NUMBER OF SPACES
1 BUILDING 21	1,495
2 BUILDING 23	57
3 TOTAL	1,552



GEHRY PARTNERS LLP
 ARCHITECT
 1000 WILSON AVENUE
 SUITE 1000
 PITTSBURGH, PA 15222
 TEL: 412.326.1000
 FAX: 412.326.1001
 WWW.GEHRY.COM

FACEBOOK
 OWNER
 1600 AVENUE K
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
PARKING PHASING 1
 MPK21 BUILT
 MPK22 UNDER CONSTRUCTION

PROJECT NUMBER
 2015-077
 SHEET
 11 OF 180
 DATE
 SEPTEMBER 28, 2017
 DRAWN BY
 J. W. WILSON
 CHECKED BY
 J. W. WILSON

SHEET NUMBER
A4-01
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KEY

	NUMBER OF SPACES
1 BUILDING 21	1,495
2 BUILDING 22	0
3 BUILDING 23	57
4 PARKING GARAGE	1,736
TOTAL	3,288



GEHRY PARTNERS LLP
 ARCHITECT
 100 W. WASHINGTON STREET
 LOS ANGELES, CALIFORNIA 90012
 (213) 480-1000

FACEBOOK
 OWNER
 1600 AVENUE B
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PARKING PHASING 2
 INTERIM PHASE
 HOTEL UNDER CONSTRUCTION

PROJECT NUMBER
 2015-077
 SHEET
 17 OF 18X

SEPTEMBER 28, 2017
 THIS SHEET IS IN THE
 DESIGN DEVELOPMENT STAGE

SHEET NUMBER
A4-02



KEY

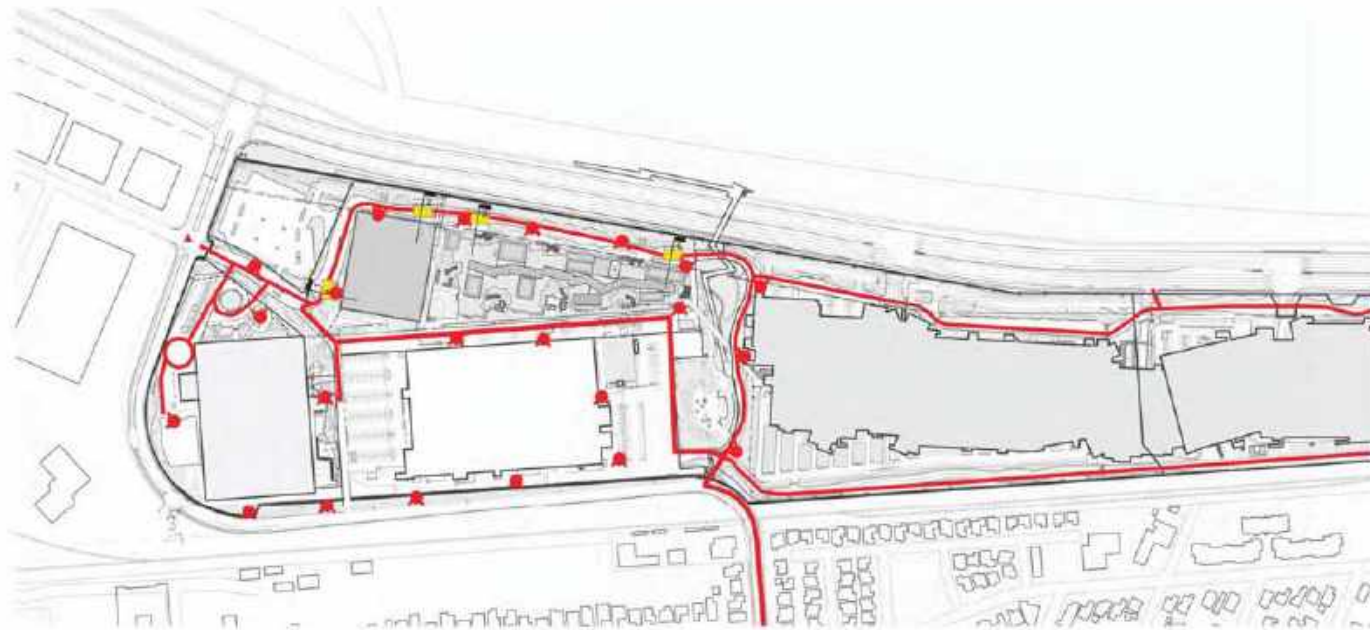
	NUMBER OF SPACES
1 BUILDING 21	1,495
2 BUILDING 22	0
3 BUILDING 23	57
4 PARKING GARAGE	1,730
5 HOTEL	245
TOTAL	3,533

REQUIRED MINIMUM PARKING FOR BUILDING 22 AND 23 IS INCORPORATED INTO PARKING GARAGE.

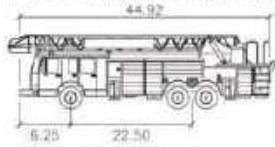
GEHRY PARTNERS, LLP
 ARCHITECT
 1000 W. BROADWAY, SUITE 1000
 DENVER, CO 80202
 TEL: 303.733.4400
FACEBOOK
 OWNER
 1000 W. BROADWAY, SUITE 1000
 DENVER, CO 80202

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PROJECT NUMBER
2015-077
 DATE
 11-18-15
 SHEET NUMBER
A4-03
 DATE PLOTTED
 9/28/17 10:15 AM
 PLOT SCALE
 1" = 30'



- LEGEND**
- BUILDING ROOF
 - FIRE TRUCK
 - FIRE ACCESS - 26'-0" MINIMUM WIDTH
 - FIRE STAGING AREA - 30' X 60'
 - FIRE HYDRANT



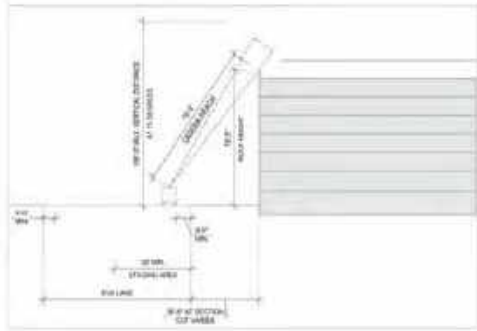
MP Fire Truck feet

Width	: 8.25
Track	: 8.25
Lock to Lock Time	: 6.0
Steering Angle	: 25.4

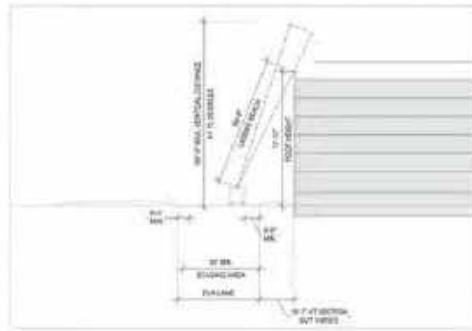
Note: Additional fire hydrants can be added during final design phase



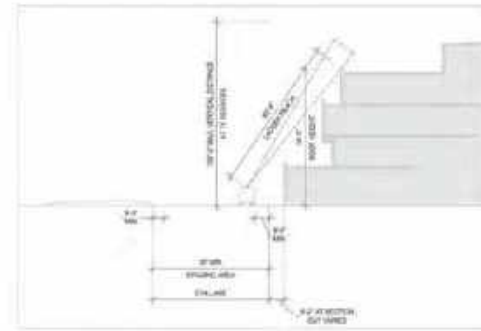
<p>GEHRY PARTNERS, LLP ARCHITECT</p> <p>FACEBOOK OWNER</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>INTERIM PHASE FIRE ACCESS PLAN</p>	<p>PROJECT NUMBER 2015-007</p> <p>DATE AS NOTED</p> <p>SEPTEMBER 28, 2017</p>	<p>PROJECT NUMBER A5-01</p>
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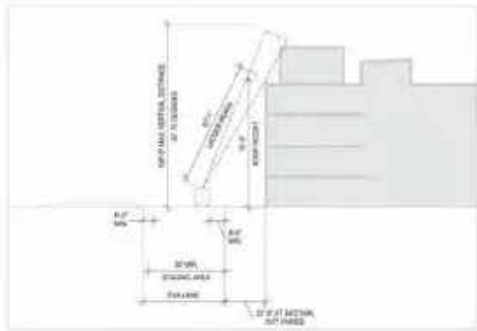
1 SECTION AT FD STAGING AREA 1
SCALE: 1/32" = 1'-0"



2 SECTION AT FD STAGING AREA 2
SCALE: 1/32" = 1'-0"



3 SECTION AT FD STAGING AREA 3
SCALE: 1/32" = 1'-0"



4 SECTION AT FD STAGING AREA 4
SCALE: 1/8" = 1'-0"



5 MP FIRE TRUCK
SCALE: 1/8" = 1'-0"

GEHRY PARTNERS, LLP
ARCHITECT
1000 AVENUE OF THE STARS
SUITE 4000
WASHINGTON, DC 20007

FACEBOOK
OWNER
1000 AVENUE OF THE STARS
SUITE 4000
WASHINGTON, DC 20007

CONSULTANTS

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

1100
INTERIM PHASE LADDER
ACCESS SECTIONS

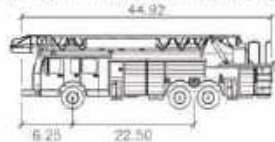
PROJECT NUMBER
2015-002
DATE
AS NOTED
SEPTEMBER 01, 2015
PROJECT ARCHITECT: G.P.A.
DRAWING SHEET NO.: 2P-A-20

SHEET NUMBER
A5-02



LEGEND

- BUILDING ROOF
- FIRE TRUCK
- FIRE ACCESS - 26'-0" MINIMUM WIDTH
- FIRE STAGING AREA - 30' X 60'
- FIRE HYDRANT



MP Fire Truck	feet
Width	: 8.25
Track	: 8.25
Lock to Lock Time	: 6.0
Steering Angle	: 25.4

Note: Additional fire hydrants can be added during final design phase



GEHRY PARTNERS, LLP
ARCHITECT
1000 AVENUE OF THE STARS
SUITE 1000
WASHINGTON, DC 20004

FACEBOOK
OWNER
1700 AVENUE OF THE STARS
SUITE 1000
WASHINGTON, DC 20004

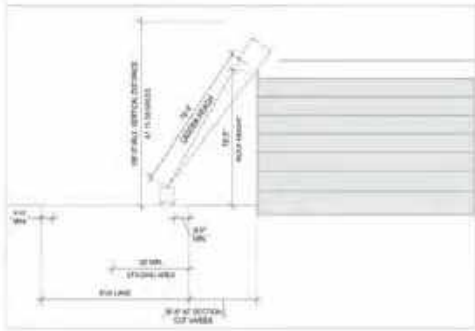
Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

FINAL PHASE FIRE ACCESS
PLAN

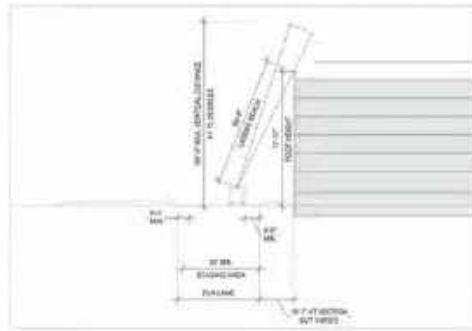
PROJECT NUMBER
2015-007
DATE
AS NOTED

SEPTEMBER 24, 2015
THIS SHEET IS A PART OF A SET.
CHECK SHEET 01 OF 2 SET.

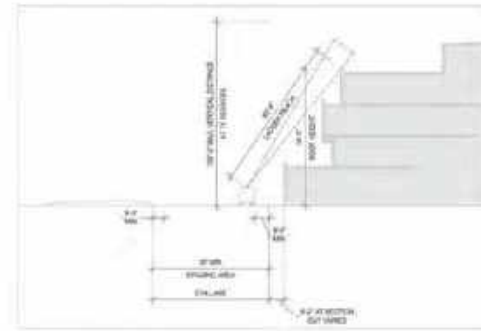
SHEET NUMBER
A5-03



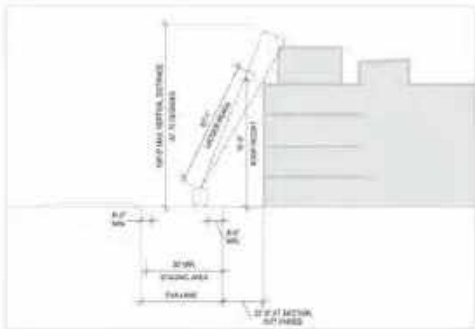
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SCALE: 1/32" = 1'-0"



2 SECTION AT FD STAGING AREA 2
SCALE: 1/32" = 1'-0"



3 SECTION AT FD STAGING AREA 3
SCALE: 1/32" = 1'-0"



4 SECTION AT FD STAGING AREA 4
SCALE: 1/32" = 1'-0"



5 MP FIRE TRUCK
SCALE: 1/8" = 1'-0"

FINAL PHASE LADDER ACCESS SECTIONS | A5-04

GEHRY PARTNERS, LLP
ARCHITECT
1000 AVENUE OF THE STARS
SUITE 1000
WASHINGTON, DC 20004
202-462-6000

FACEBOOK
OWNER
1601 AVENUE OF THE STARS
SUITE 1000
WASHINGTON, DC 20004

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

FINAL PHASE LADDER
ACCESS SECTIONS

PROJECT NUMBER
2015-007
DATE
AS NOTED

SEPTEMBER 28, 2017
DATE PLOTTED: 9-13-17
DRAWING SHEET 001 OF 307

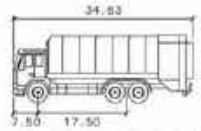
SHEET NUMBER

A5-04

©2017 Facebook, Inc.

LEGEND

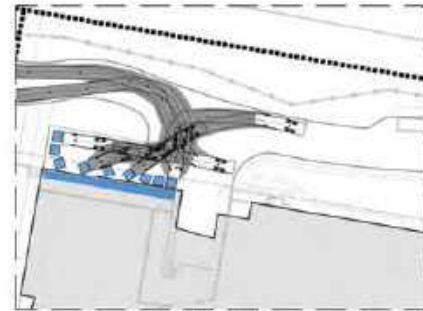
- SITE SECURITY CONTROL STATION
- S BUILDING SECURITY CONTROL STATION
- TRASH COLLECTION



GARBAGE TRUCK FACEBOOK

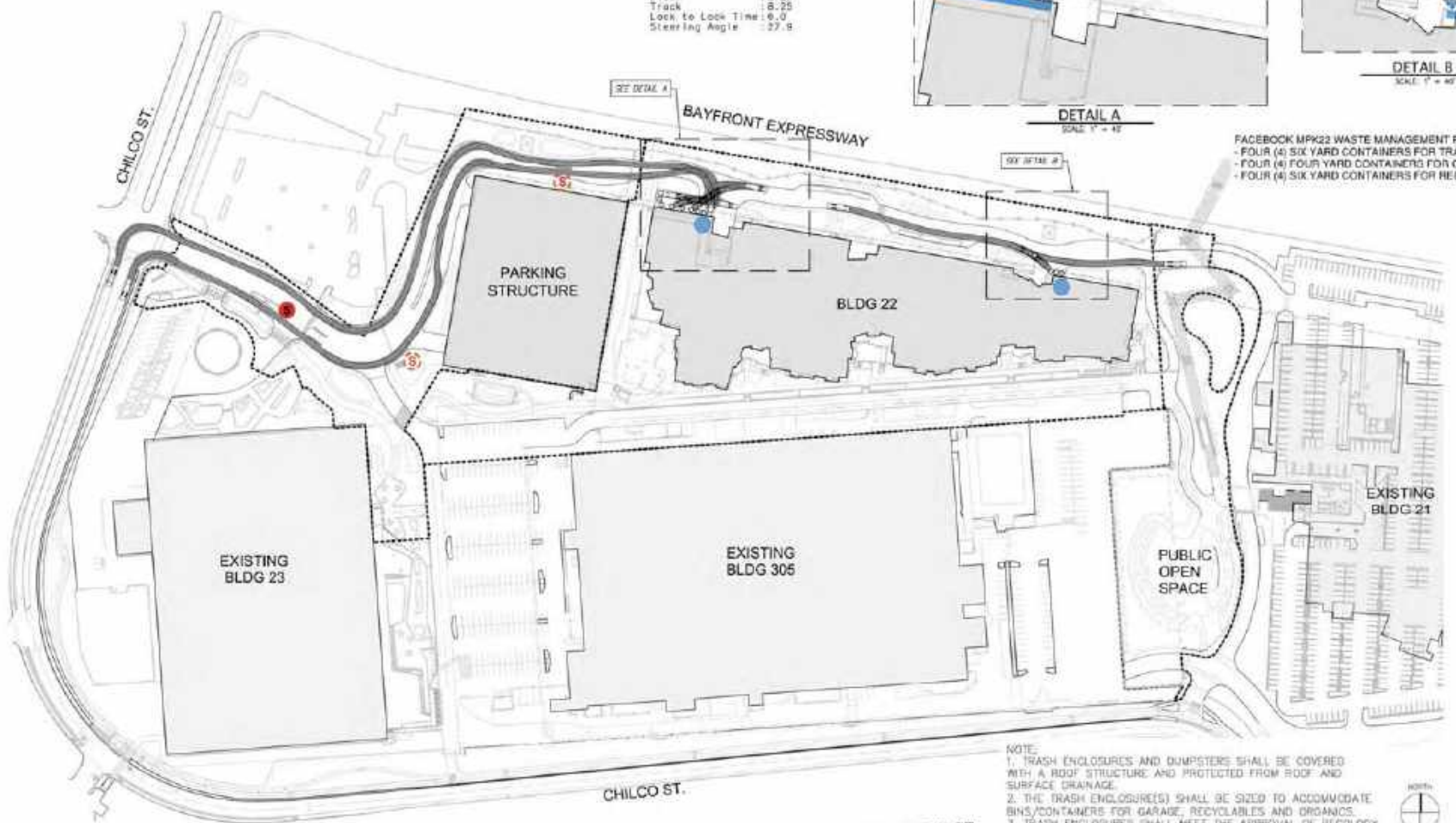
feet

Width 34.00
 Track 7.80
 Lock to Lock Time 17.50
 Steering Angle 27.8



FACEBOOK MPK22 WASTE MANAGEMENT REQUIREMENTS:

- FOUR (4) SIX YARD CONTAINERS FOR TRASH
- FOUR (4) FOUR YARD CONTAINERS FOR COMPOST
- FOUR (4) SIX YARD CONTAINERS FOR RECYCLING



NOTE:

1. TRASH ENCLOSURES AND DUMPSTERS SHALL BE COVERED WITH A ROOF STRUCTURE AND PROTECTED FROM ROOF AND SURFACE DRAINAGE.
2. THE TRASH ENCLOSURE(S) SHALL BE SIZED TO ACCOMMODATE BINS/CONTAINERS FOR GARAGE, RECYCLABLES AND ORGANICS.
3. TRASH ENCLOSURES SHALL MEET THE APPROVAL OF RECYCLO AND ENVIRONMENTAL PROGRAMS MANAGER OF THE CITY OF MENLO PARK.



KIER & WRIGHT
 CIVIL ENGINEER
 2500 STELLER CANYON ROAD
 BERKELEY, CALIFORNIA 94704, USA
 TEL: 415-558-8700
 FAX: 415-558-8700

FACEBOOK
 OWNER
 170000 001
 MENLO PARK, CALIFORNIA 94025

CONSULTANTS

PERKINS+WILL ENGINEERS
 1000 WILSON AVENUE
 CHICAGO, ILLINOIS 60601
 TEL: 312-279-2000
 FAX: 312-279-2000

PRC
 4000 CALIFORNIA AVENUE
 SUITE 200
 SAN FRANCISCO, CALIFORNIA 94133
 TEL: 415-774-1200
 FAX: 415-774-1200

CMG
 LANDSCAPE CONSULTANTS
 1000 AVENUE 46, SUITE 100
 SAN FRANCISCO, CALIFORNIA 94112
 TEL: 415-774-1200
 FAX: 415-774-1200

EXPOINTER
 2500 STELLER CANYON ROAD
 BERKELEY, CALIFORNIA 94704
 TEL: 415-558-8700
 FAX: 415-558-8700

BRIGHTWORKS
 400 CALIFORNIA AVENUE
 SUITE 200
 SAN FRANCISCO, CALIFORNIA 94133
 TEL: 415-774-1200
 FAX: 415-774-1200

EVOSBY/ATOMIC
 2500 STELLER CANYON ROAD
 BERKELEY, CALIFORNIA 94704
 TEL: 415-558-8700
 FAX: 415-558-8700

MKA
 4000 CALIFORNIA AVENUE
 SUITE 200
 SAN FRANCISCO, CALIFORNIA 94133
 TEL: 415-774-1200
 FAX: 415-774-1200

NEWSON/BRONX
 2500 STELLER CANYON ROAD
 BERKELEY, CALIFORNIA 94704
 TEL: 415-558-8700
 FAX: 415-558-8700

IRAS DESIGN GROUP
 4000 CALIFORNIA AVENUE
 SUITE 200
 SAN FRANCISCO, CALIFORNIA 94133
 TEL: 415-774-1200
 FAX: 415-774-1200

LEITCH WATERS
 2500 STELLER CANYON ROAD
 BERKELEY, CALIFORNIA 94704
 TEL: 415-558-8700
 FAX: 415-558-8700

DOOR + HARDWARE CONSULT.
 4000 CALIFORNIA AVENUE
 SUITE 200
 SAN FRANCISCO, CALIFORNIA 94133
 TEL: 415-774-1200
 FAX: 415-774-1200

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PROPOSED INTERIM GARBAGE TRUCK

PROJECT NUMBER: 2018-007
 DATE: 11-14-18

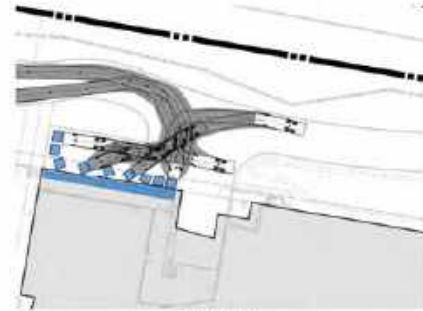
ISSUE NUMBER: A5-10

SEPTEMBER 28, 2017

DATE PLOTTED: 10/14/18
 PLOTTER: HP DesignJet T1100

LEGEND

- SITE SECURITY CONTROL STATION
- Ⓢ BUILDING SECURITY CONTROL STATION
- TRASH COLLECTION



- FACEBOOK MPK22 WASTE MANAGEMENT REQUIREMENTS:**
- FOUR (4) SIX YARD CONTAINERS FOR TRASH
 - FOUR (4) FOUR YARD CONTAINERS FOR COMPOST
 - FOUR (4) SIX YARD CONTAINERS FOR RECYCLING



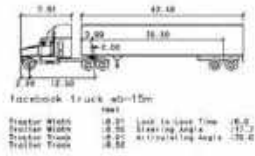
- NOTE:**
1. TRASH ENCLOSURES AND DUMPSTERS SHALL BE COVERED WITH A ROOF STRUCTURE AND PROTECTED FROM ROOF AND SURFACE DRAINAGE.
 2. THE TRASH ENCLOSURE(S) SHALL BE SIZED TO ACCOMMODATE BINS/CONTAINERS FOR GARAGE, RECYCLABLES AND ORGANICS.
 3. TRASH ENCLOSURES SHALL MEET THE APPROVAL OF RECYCLOGY AND ENVIRONMENTAL PROGRAMS MANAGER OF THE CITY OF MENLO PARK.



<p>KIER & WRIGHT CIVIL ENGINEER 3545 STELLER DRIVE SUITE 100 MENLO PARK, CALIFORNIA 94025 TEL: 650-594-8758 FAX: 650-594-8759</p> <p>FACEBOOK OWNER 3000 AVENUE MENLO PARK, CALIFORNIA 94025</p>	<p>POWELL & GIBSON ENGINEERS STRUCTURAL ENGINEER 1000 SHAW BLVD SAN FRANCISCO, CA 94111 TEL: 415-398-4700 FAX: 415-398-4700</p> <p>RNE ARCHITECTURAL 427 GARDEN STREET, SUITE 100 SAN FRANCISCO, CA 94142 TEL: 415-774-1100</p> <p>CMG LANDSCAPE CONSULTANT 1000 AVENUE 401 SAN FRANCISCO, CA 94111 TEL: 415-774-1100</p>	<p>SAPORANT 2FA ARCHITECTURAL 1000 CALIFORNIA BLVD SAN ANGELO, TX 76901 TEL: 254-791-4700</p> <p>BRIGHTWORKS 440 TOWNSEND CENTER DRIVE 410 BAYVIEW BLVD, #700 SAN FRANCISCO, CA 94133 TEL: 415-774-1100</p>	<p>LYONS&WATSON LANDSCAPE CONSULTANT 200 CALIFORNIA BLVD SAN FRANCISCO, CA 94111 TEL: 415-774-1100</p> <p>MKA 450 TOWNSEND CENTER DRIVE 410 BAYVIEW BLVD, #700 SAN FRANCISCO, CA 94133 TEL: 415-774-1100</p>	<p>NEWSON&WONG GEOTECH CONSULTANT 2000 BAYVIEW BLVD, SUITE 200 SAN FRANCISCO, CA 94133 TEL: 415-774-1100</p> <p>IRAS DESIGN GROUP 450 TOWNSEND CENTER DRIVE 410 BAYVIEW BLVD, #700 SAN FRANCISCO, CA 94133 TEL: 415-774-1100</p>	<p>LENCH WATERS BUILDING PERFORMANCE CONSULTANT 2115 FERRIS BLVD, SUITE 11 SAN ANGELO, TX 76901 TEL: 254-791-4700</p> <p>DOOR & MIRRORWARE CONSULT. ARCHITECTURAL CONSULTANT 2000 BAYVIEW BLVD, #200 SAN FRANCISCO, CA 94133 TEL: 415-774-1100</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>PROPOSED FINAL GARBAGE TRUCK</p>	<p>PROJECT NUMBER 2018-007 SCALE 1" = 40' SEPTEMBER 28, 2017 DATE PLOTTED: 9/27/17 10:27 AM</p>	<p>SHEET NUMBER A5-11</p>
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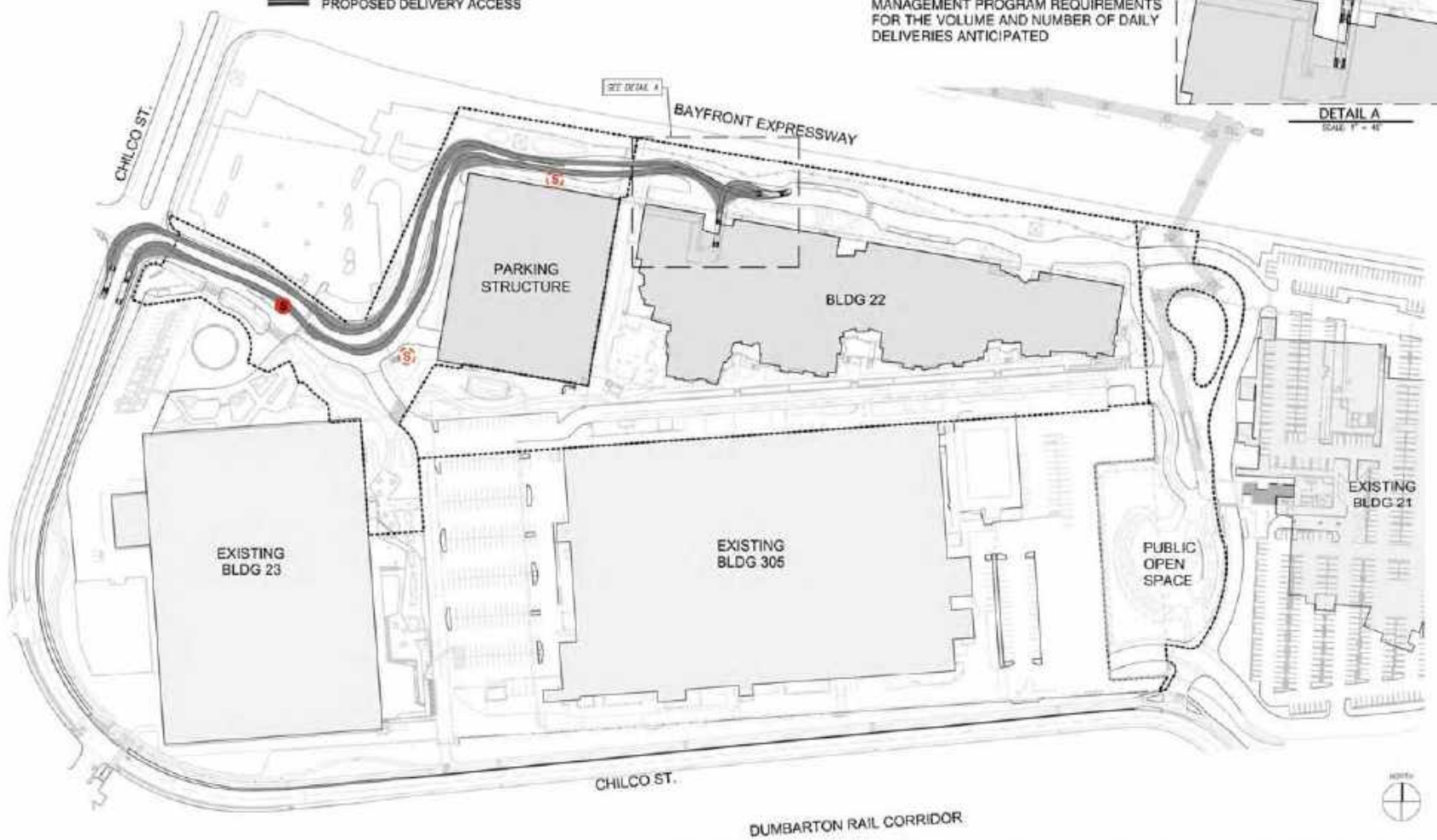
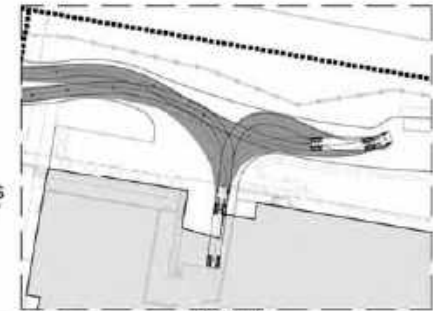
LEGEND

- SITE SECURITY CONTROL STATION
- S BUILDING SECURITY CONTROL STATION
- TRASH COLLECTION
- PROPOSED DELIVERY ACCESS



NOTE:

1. WB-15 USED FOR TURN ANALYSIS FOR SERVICE TO NORTH DOCK
2. LOADING DOCK LOCATIONS AND CAPACITIES ARE PROVIDED PER FACEBOOK FACILITIES MANAGEMENT PROGRAM REQUIREMENTS FOR THE VOLUME AND NUMBER OF DAILY DELIVERIES ANTICIPATED



KIER & WRIGHT
CIVIL ENGINEER
2501 STELLER CANYON ROAD
COSTA MESA, CALIFORNIA 92626 USA
TEL: 415-556-8758
FAX: 949-266-8758

FACEBOOK
OWNER
1600 ZEPHYRUS AVENUE
MENLO PARK, CALIFORNIA 94025

CONSULTANTS

PORTULANUS&SIEBER ENGINEERS
STRUCTURAL ENGINEER
400 West St.
San Francisco, CA 94111
Tel: 415-774-1700

P&E
ARCHITECTURAL
4177 CALIFORNIA STREET, SUITE 2200
SAN FRANCISCO, CA 94116
Tel: 415-774-1700

CM&I
LANDSCAPE ARCHITECT
1000 AND ST. #112
SAN FRANCISCO, CA 94103
Tel: 415-774-1700

EXPOSITION
2501 STELLER CANYON ROAD
COSTA MESA, CALIFORNIA 92626 USA
TEL: 415-556-8758

BRIGHTWORKS
400 CALIFORNIA STREET, SUITE 2200
SAN FRANCISCO, CA 94116
Tel: 415-774-1700

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COSTA MESA, CALIFORNIA 92626 USA
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M&A
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SAN FRANCISCO, CA 94116
Tel: 415-774-1700

NEWSON&WONG
GEOTECH CONSULTANT
2000 Mission Blvd., Suite 201
San Francisco, CA 94115
Tel: 415-774-1700

IRAS DESIGN GROUP
3000 CALIFORNIA STREET, SUITE 2200
SAN FRANCISCO, CA 94116
Tel: 415-774-1700

LEITCH MATES
BUILDING PERFORMANCE CONSULTANT
2114 Francisco Street, Suite 4
San Rafael, CA 94901
Tel: 415-451-0000

DOOR & HARDWARE CONSULT.
KARLSON CONSULTANTS
2000 Mission Blvd., Suite 201
San Francisco, CA 94115
Tel: 415-774-1700

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROPOSED INTERIM
LOADING DOCK DELIVERY PLAN

PROJECT NUMBER
2018-007

DATE
11.4.18

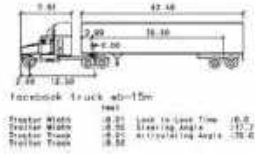
SEPTEMBER 28, 2017
REVISED BY: S.L.
ISSUED BY: S.L. 24" x 36"

SHEET NUMBER
A5-12

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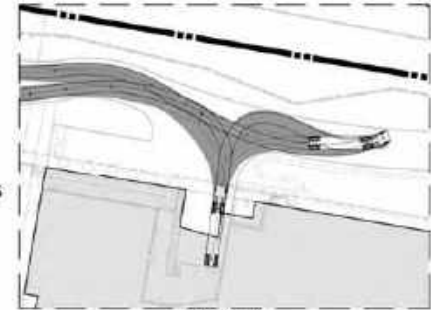
LEGEND

- SITE SECURITY CONTROL STATION
- S BUILDING SECURITY CONTROL STATION
- TRASH COLLECTION
- PROPOSED DELIVERY ACCESS



NOTE:

1. WB-15 USED FOR TURN ANALYSIS FOR SERVICE TO NORTH DOCK
2. LOADING DOCK LOCATIONS AND CAPACITIES ARE PROVIDED PER FACEBOOK FACILITIES MANAGEMENT PROGRAM REQUIREMENTS FOR THE VOLUME AND NUMBER OF DAILY DELIVERIES ANTICIPATED



KIER & WRIGHT
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 2541 STELLER CIRCLE SUITE 100
 COSTA MESA, CALIFORNIA 92626
 TEL: 949-446-8700
 FAX: 949-446-8709
FACEBOOK
 OWNER
 144000 800
 MENLO PARK, CALIFORNIA 94025

CONSULTANTS
PERKINS+WILSON ENGINEERS
 ARCHITECTURAL CONSULTANT
 400 West 10th St.
 San Francisco, CA 94111
 TEL: 415-774-2500

ENG
 ARCHITECTURAL
 427 GARDNER STREET SUITE 100
 SAN FRANCISCO, CA 94102
 TEL: 415-774-2500

CMG
 LANDSCAPE CONSULTANT
 1800 ANDERSON BLVD.
 SAN ANTONIO, TX 78214
 TEL: 214-343-2500

EXPOVENET
 254 WEST 10TH STREET
 SAN FRANCISCO, CA 94111
 TEL: 415-774-2500

RIGHTWORKS
 400 MARKET ST. 4TH FLOOR
 SAN FRANCISCO, CA 94102
 TEL: 415-774-2500

LYONS/SHAW/COOPER
 LANDSCAPE CONSULTANT
 200 CALIFORNIA ST.
 SAN FRANCISCO, CA 94102
 TEL: 415-774-2500

NEWSON/SHOYAN
 GEOTECH CONSULTANT
 2000 BAYVIEW BLVD. SUITE 300
 SAN RAFAEL, CA 94903
 TEL: 415-774-2500

LEITCH WATERS
 BUILDING PERFORMANCE CONSULTANT
 2310 FRANCISCO BLVD. SUITE 11
 SAN RAFAEL, CA 94903
 TEL: 415-774-2500

DOOR & HERSHMAN CONSULTANTS
 ARCHITECTURAL CONSULTANT
 2000 BAYVIEW BLVD. SUITE 300
 SAN RAFAEL, CA 94903
 TEL: 415-774-2500

Facebook Campus Expansion

Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive

PROPOSED FINAL LOADING DOCK DELIVERY PLAN

PROJECT NUMBER: 2019-007
 SHEET: 11 of 40
 DATE: SEPTEMBER 28, 2019
 DRAWING SCALE: 1/8" = 1'-0"

SHEET NUMBER: **A5-13**

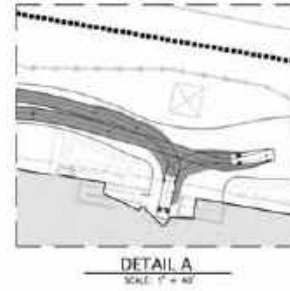
LEGEND

- SITE SECURITY CONTROL STATION
- S BUILDING SECURITY CONTROL STATION
- TRASH COLLECTION
- PROPOSED DELIVERY ACCESS



NOTE:

1. FB TRUCK USED FOR TURN ANALYSIS FOR SERVICE TO NORTH DOCK
2. LOADING DOCK LOCATIONS AND CAPACITIES ARE PROVIDED PER FACEBOOK FACILITIES MANAGEMENT PROGRAM REQUIREMENTS FOR THE VOLUME AND NUMBER OF DAILY DELIVERIES ANTICIPATED.



KIER & WRIGHT
 CIVIL ENGINEER
 2541 STELLER CANYON ROAD
 COSTA MESA, CALIFORNIA 92626
 TEL: 415-556-8758
 FAX: 949-266-8758

FORSLUIS/LEIBER ENGINEERS
 CIVIL ENGINEER
 1401 STEEL DRIVE
 COSTA MESA, CA 92626
 TEL: 714-952-4700
 FAX: 714-952-4700

ONE
 LANDMARK CONSULTING
 4000 AVENUE 48, SUITE 100
 DALLAS, TEXAS 75246
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 FAX: 214-635-1111

EXPOSITION
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 COSTA MESA, CALIFORNIA 92626
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 FAX: 949-266-8758

BRIGHTWORKS
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 FAX: 214-635-1111

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 FAX: 949-266-8758

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

**PROPOSED INTERIM
 LOADING DOCK DELIVERY PLAN**

PROJECT NUMBER
 2015-007
 DATE
 11.4.15
 SEPTEMBER 28, 2015
 2541 STELLER CANYON ROAD, COSTA MESA, CA 92626
 2541 STELLER CANYON ROAD, COSTA MESA, CA 92626

SHEET NUMBER
A5-14

LEGEND

- SITE SECURITY CONTROL STATION
- Ⓢ BUILDING SECURITY CONTROL STATION
- TRASH COLLECTION
- PROPOSED DELIVERY ACCESS



NOTE:

1. FB TRUCK USED FOR TURN ANALYSIS FOR SERVICE TO NORTH DOCK
2. LOADING DOCK LOCATIONS AND CAPACITIES ARE PROVIDED PER FACEBOOK FACILITIES MANAGEMENT PROGRAM REQUIREMENTS FOR THE VOLUME AND NUMBER OF DAILY DELIVERIES ANTICIPATED.



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FAX: 949-446-8758

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OWNER
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MIAMI, FLORIDA 33187

STRUCTURE

PERKINS+WILSON ENGINEERS
ARCHITECTURAL CONSULTANT
100 Pine St.
San Francisco, CA 94111
TEL: 415-774-1200

RME
ARCHITECTURAL
427 LARKSPUR DRIVE, SUITE 100
LOS ANGELES, CA 90024
TEL: 310-441-1000

CMG
LANDSCAPE CONSULTANT
1000 4TH ST., SUITE 400
SAN FRANCISCO, CA 94103
TEL: 415-774-1200

EXPO/VENT
200 BAYVIEW DRIVE SUITE 100
SAN FRANCISCO, CA 94133
TEL: 415-774-1200

BRIGHTWORKS
400 TOWNSEND STREET SUITE 100
SAN FRANCISCO, CA 94103
TEL: 415-774-1200

LYONS/PLATTNER
LANDSCAPE CONSULTANT
200 CALIFORNIA ST.
SAN FRANCISCO, CA 94102
TEL: 415-774-1200

MKA
ARCHITECTURAL CONSULTANT
2700 J. AVENUE SUITE 100
SAN FRANCISCO, CA 94115
TEL: 415-774-1200

NEWSON/SHOAF
ARCHITECTURAL CONSULTANT
2000 MARKET STREET SUITE 100
SAN FRANCISCO, CA 94114
TEL: 415-774-1200

IRAS DESIGN GROUP
ARCHITECTURAL CONSULTANT
411 MARKET STREET SUITE 7
SAN FRANCISCO, CA 94102
TEL: 415-774-1200

LEITCH WATERS
BUILDING PERFORMANCE CONSULTANT
2010 FERRIS BLVD SUITE 11
SAN ANTONIO, TX 78247
TEL: 214-342-0200

DOOR & WINDOW CONSULT
ARCHITECTURAL CONSULTANT
2000 MARKET STREET SUITE 100
SAN FRANCISCO, CA 94114
TEL: 415-774-1200

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROPOSED FINAL
LOADING DOCK DELIVERY PLAN

PROJECT NUMBER
2015-007

SCALE
1" = 40'

SEPTEMBER 30, 2015
REVISED 09/23/15 6:14
ISSUES 04/27/15 04' x 30'

SHEET NUMBER
A5-15



- VIEW LEGEND**
- 1. HILL AVE
 - 2. MODOC AVE
 - 3. CHILCO ST
 - 4. HAMILTON PARK
 - 5. BCCD PUBLIC SHORELINE TRAIL
 - 6. BAY TRAIL
 - 7. BEDWELL BAYFRONT PARK

GEHRY PARTNERS, LLP
 ARCHITECT
 1000 BAYVIEW STREET
 442 SAN DIEGO, CALIFORNIA 92101
 (619) 444-1000

FACEBOOK
 OWNER
 1 HAVEN WAY
 WILMINGTON, CALIFORNIA 90744

09/25/2017

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PHOTO SIMULATION:
 AERIAL REGIONAL SITE
 VIEW LOCATION

PROJECT NUMBER
 2015-0017

DATE
 SEPTEMBER 28, 2017

SCALE
 1:62,500 (1" = 512')

PROJECT NUMBER
 A6-00

DESIGN PARTNER, LP



EXISTING



PROPOSED

GEHRY PARTNERS, LLP
 ARCHITECT
 2000 BAYVIEW DRIVE
 LOS ANGELES, CALIFORNIA 90024
 (310) 467-3888

CONSULTANTS

FACEBOOK
 OWNER
 16000 WAY
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

P&A
INTERIM PHASE PHOTO
SIMULATION: HILL AVE
VIEW 1

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 28, 2017
 10.7 SHEET NO. 6 OF 15
 DRAWING SHEET NO. 217 OF 307

SHEET NUMBER
A6-01
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PROPOSED

GEHRY PARTNERS, LLP
 ARCHITECT
 1000 AVENUE OF THE STARS
 SUITE 1000
 WASHINGTON, DC 20004
 TEL: 202-690-0000
 WWW: GHPARTNERS.COM

DATE/TIME

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

FINAL PHASE PHOTO
 SIMULATION: HILL AVE
 VIEW 1

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 28, 2017
 FILE NAME
 2015-007-01-01-01

SHEET NUMBER
A6-02
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 1000 BAY STREET, SUITE 200
 SAN FRANCISCO, CALIFORNIA 94111
 (415) 774-2900

DATE: 09/28/16

FACEBOOK
 OWNER
 1700 BAY STREET, SUITE 200
 SAN FRANCISCO, CALIFORNIA 94111

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

INTERIM PHASE PHOTO
 SIMULATION: MODOC AVE
 VIEW 2

PROJECT NUMBER
 2016-007
 DATE
 SEPTEMBER 28, 2016
 SHEET NUMBER: A-1.1
 DRAWING SCALE: 1/4" = 1'-0"

SHEET NUMBER
 A6-03

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 1001 30TH STREET
 LOS ANGELES, CALIFORNIA 90008
 (310) 467-1000

FACEBOOK
 OWNER
 16000 WAY
 REDWOOD CITY, CALIFORNIA 94061

DATE/TIME

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

FINAL PHASE PHOTO
 SIMULATION: MODOC AVE
 VIEW 2

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 28, 2017
 1001 30TH STREET, 6TH FLOOR
 LOS ANGELES, CALIFORNIA 90008

SHEET NUMBER
 A6-04

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 1001 BAYVIEW STREET
 (312) 846-1111, (415) 499-8888
 (415) 491-3888

FACEBOOK
 OWNER
 1600 AVENUE OF THE STARS
 MENLO PARK, CALIFORNIA 94025

DATE/TIME

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

INTERIM PHASE PHOTO
 SIMULATION: CHILCO
 STREET VIEW 3

PROJECT NUMBER
 2016-007
 DATE
 SEPTEMBER 28, 2017
 10:17 AM PST (08:11)
 ORIGINAL SHEET NO.: 117 & 121

SHEET NUMBER
 A6-05
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 1000 BAYVIEW STREET
 LOS ANGELES, CALIFORNIA 90024
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CONSULTANT

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TYPED
FINAL PHASE PHOTO
SIMULATION: CHILCO
STREET VIEW 3

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 28, 2017
 (PLOT SHEET 002) A14
 (SHEET 001) 31" x 36"

SHEET NUMBER
A6-06
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GEHRY PARTNERS, LLP
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 1201 BAYVIEW STREET
 LOS ANGELES, CALIFORNIA 90006
 (310) 249-1000

FACEBOOK
 OWNER
 1 PALMER WAY
 MENLO PARK, CALIFORNIA 94025

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 300-309 Constitution Drive

PHOTO
 INTERIM PHASE PHOTO
 SIMULATION: HAMILTON
 PARK VIEW 4

PROJECT NUMBER
 2015-0317
 DATE
 SEPTEMBER 28, 2015
 10:17 AM PST (PDT)
 MODEL SCALE 1/8" = 1' 0"

SHEET NUMBER
 A6-07
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GEHRY PARTNERS, LLP
ARCHITECT
1000 BROADWAY
NEW YORK, NY 10018
TEL: 212 677 2300

FACEBOOK
OWNER
1601 AVENUE AVENUE
MENLO PARK, CALIFORNIA 94025

CONSULTANTS

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

FINAL PHASE PHOTO
SIMULATION: HAMILTON
PARK VIEW 4

PROJECT NUMBER
2016-007
DATE
SEPTEMBER 28, 2017
1601 AVENUE AVENUE, N.Y.
DRAWING SCALE: 1/4" = 1'-0"

DRAWING NUMBER
A6-08
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 1001 BAYVIEW STREET
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 (312) 462-1214

FACEBOOK
 OWNER
 10000 BAY
 WHEELER, CALIFORNIA 94612

UNRECORDED

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 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

Y&J
**INTERIM PHASE PHOTO
 SIMULATION: BCDC PUBLIC
 SHORELINE TRAIL VIEW 5**

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 10, 2017
 SHEET NUMBER
 1 OF 1
 DRAWING SHEET SIZE: 36" x 48"

SHEET NUMBER
A6-09
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 ARCHITECT
 1224 BENTLEY STREET
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 FACEBOOK
 OWNER
 1 HUNTER WAY
 BOSTON, MASSACHUSETTS 02108

DATE: 09/23/15

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

FINAL PHASE PHOTO
 SIMULATION: BCDC PUBLIC
 SHORELINE TRAIL VIEW 5

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 23, 2015
 1447 WEST 50TH, #116
 BOSTON, MASSACHUSETTS 02116

SHEET NUMBER
 A6-10
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 ARCHITECT
 2001 BAYVIEW SQUARE
 3225 AVENUE 108, CALIFORNIA 94065
 (415) 455-2000

CONTRACT#

FACEBOOK
 OWNER
 1601 CALIFORNIA AVENUE
 MENLO PARK, CALIFORNIA 94025

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 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

P&A
**INTERIM PHASE PHOTO
 SIMULATION: BAY TRAIL
 VIEW 6**

PROJECT NUMBER
 2015-007
 TITLE
 SEPTEMBER 28, 2017
 16.7 SHEET NO. 6 OF 16
 (SHEET SIZE: 24" x 36")

SHEET NUMBER
A6-11
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 1000 BOULDER AVENUE
 100 SAN FRANCISCO, CALIFORNIA 94103
 (415) 774-2000

CONSULTANTS

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

FINAL PHASE PHOTO
 SIMULATION: BAY TRAIL
 VIEW 6

PROJECT NUMBER
 2016-007
 DATE
 SEPTEMBER 28, 2017
 1417 MARKET STREET, SUITE 200
 SAN FRANCISCO, CALIFORNIA 94103

SHEET NUMBER
A6-12
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GENRY PARTNERS, LLP
 ARCHITECT
 1000 BAYVIEW STREET
 SUITE 2000, SAN FRANCISCO, CA 94133
 (415) 774-2000

CLIENT

FACEBOOK
 OWNER
 1600 CALIFORNIA AVENUE
 MENLO PARK, CALIFORNIA 94025

PROJECT
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PHASE
**INTERIM PHASE PHOTO
 SIMULATION: BEDWELL
 BAYFRONT PARK VIEW 7**

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 28, 2017
 CITY: SAN FRANCISCO, CA
 DRAWING SHEET NO.: 24' x 36'

SHEET NUMBER
A6-13
 GENRY PARTNERS, LLP



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GEHRY PARTNERS, LLP
 ARCHITECT
 1201 BAYVIEW STREET
 LOS ANGELES, CALIFORNIA 90001
 (213) 746-1000

CONSULTANTS

FACEBOOK
 OWNER
 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

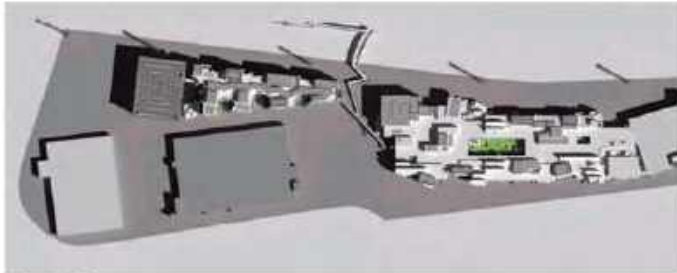
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

PLS
FINAL PHASE PHOTO
SIMULATION: BEDWELL
BAYFRONT PARK VIEW 7

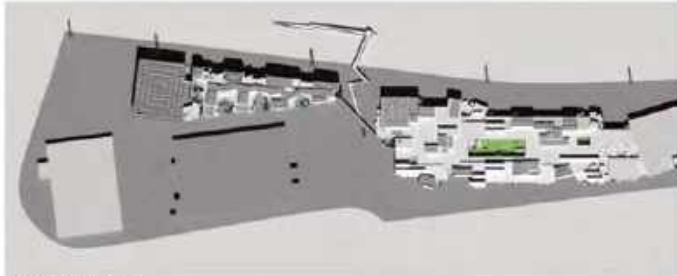
PROJECT NUMBER
 2015-0012
 DATE
 SEPTEMBER 28, 2017
 1447 SHEET NO. 8 OF 8
 DESIGN SCALE: 1/4" = 10'

SHEET NUMBER
A6-14
 ©2017 BEDWELL, LP

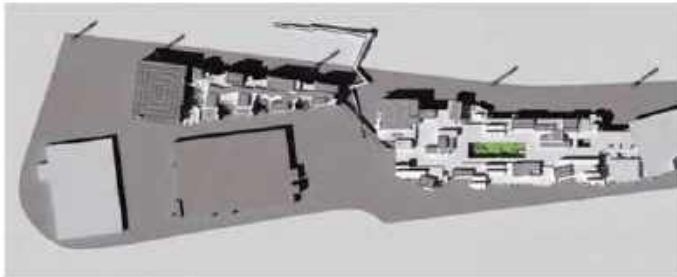
Spring Equinox (March 20) Shadows



March 20, 9 am

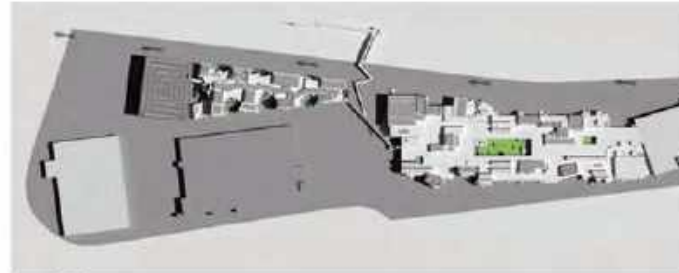


March 20, 12 pm

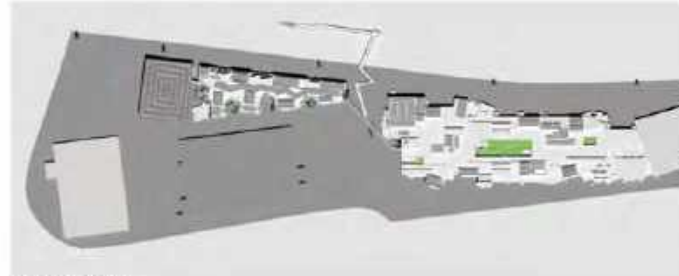


March 20, 3 pm

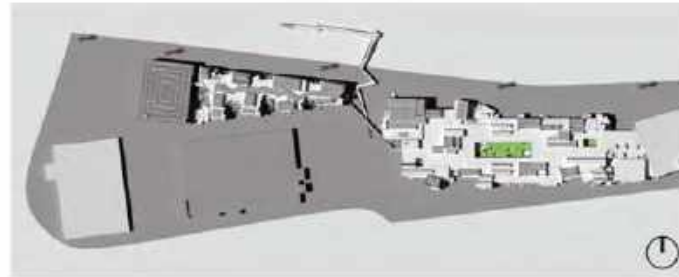
Summer Solstice (June 21) Shadows



June 21, 9 am



June 21, 12 pm



June 21, 3 pm

GEHRY PARTNERS, LLP
 ARCHITECT
 1001 BOULDER STREET
 LOS ANGELES, CALIFORNIA 90006
 (213) 480-1000

FACEBOOK
 OWNER
 7 HUNTER ST.
 SOUTHWEST, SALT LAKE 84102

CONSULTANT

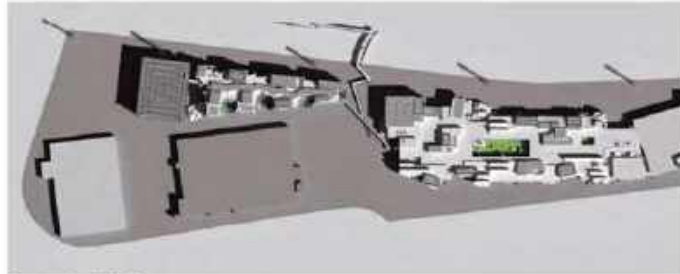
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TS&J
 INTERIM PHASE SHADOW
 DIAGRAMS

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 28, 2015
 PLOT SHEET NO. A-13
 DRAWING SHEET NO. A7-01

SHEET NUMBER
 A7-01
 GEARH PARTNERS, LLP

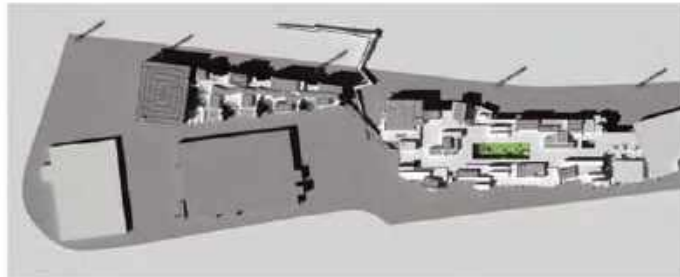
Fall Equinox (September 23) Shadows



September 23, 9 am

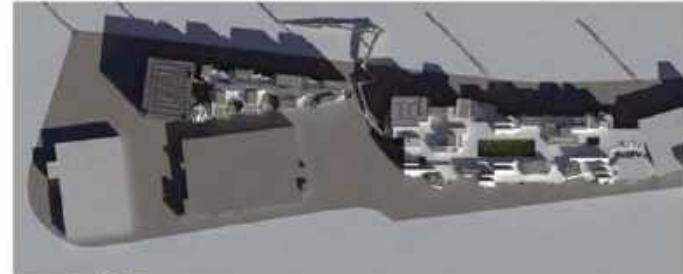


September 23, 12 pm



September 23, 3 pm

Winter Solstice (December 22) Shadows



December 22, 9 am



December 22, 12 pm



December 22, 3 pm

GEHRY PARTNERS, LLP
 ARCHITECT
 1200 BOULVARD STREET
 LOS ANGELES, CALIFORNIA 90001
 (310) 760-1000

FACEBOOK
 OWNER
 1 HAWAII WAY
 MENLO PARK, CALIFORNIA 94025

CONSULTANTS

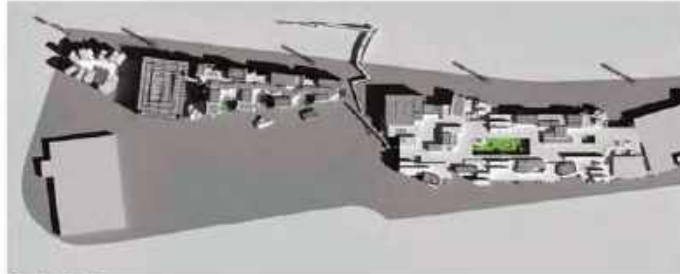
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

YES
 INTERIM PHASE SHADOW
 DIAGRAMS

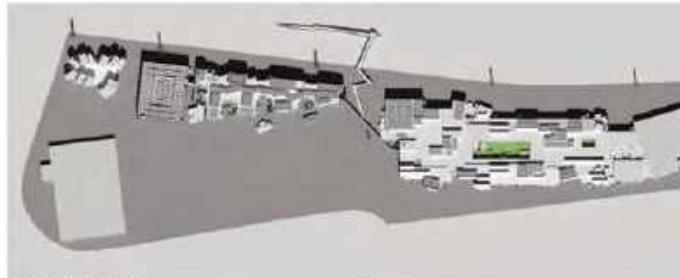
PROJECT NUMBER
 2015-0312
 DATE
 SEPTEMBER 30, 2017
 1007 900TH AVE. S.E.
 BELLEVUE, WA 98008 312' x 81'

SHEET NUMBER
 A7-02
 ©Gehry Partners, LP

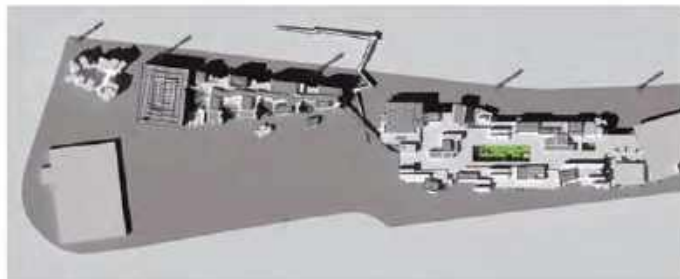
Spring Equinox (March 20) Shadows



March 20, 9 am

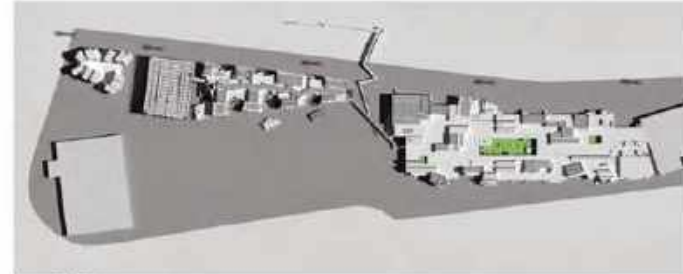


March 20, 12 pm

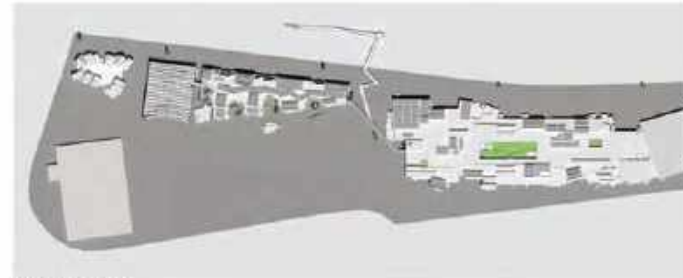


March 20, 3 pm

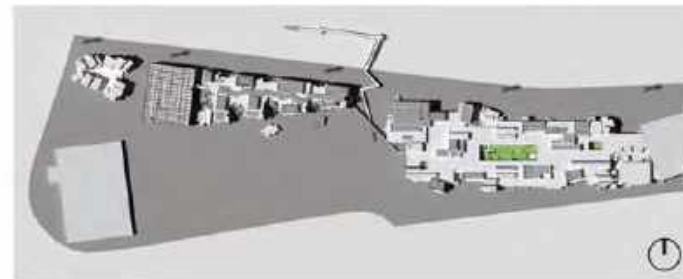
Summer Solstice (June 21) Shadows



June 21, 9 am



June 21, 12 pm



June 21, 3 pm

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CONSULTANTS

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

FINAL PHASE SHADOW
DIAGRAMS

PROJECT NUMBER
2016-007

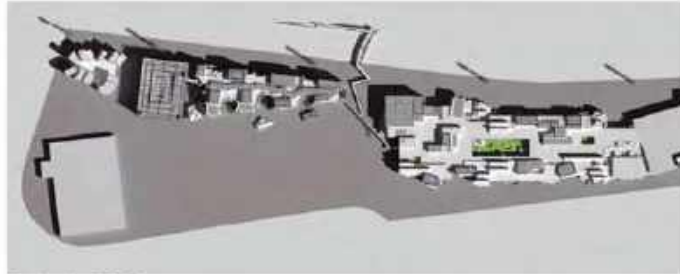
DATE
SEPTEMBER 28, 2017

SCALE
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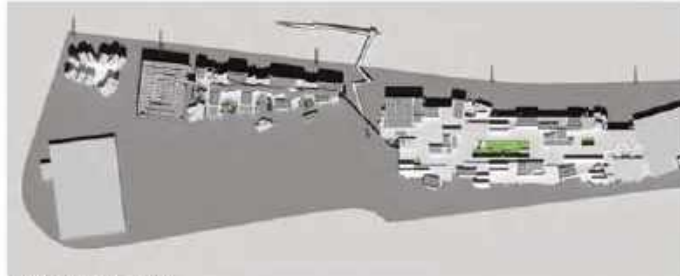
SHEET NUMBER
A7-03

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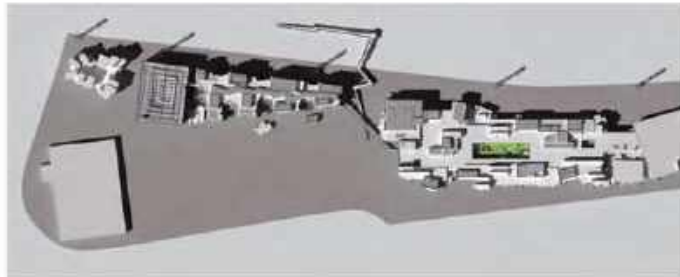
Fall Equinox (September 23) Shadows



September 23, 9 am

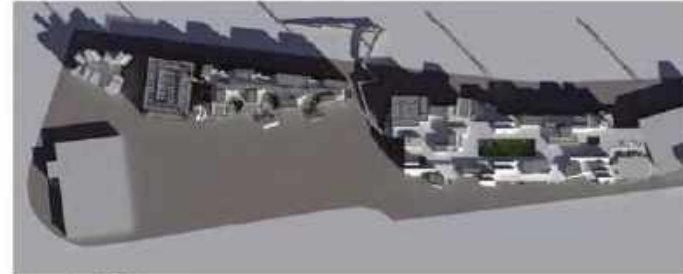


September 23, 12 pm



September 23, 3 pm

Winter Solstice (December 22) Shadows



December 22, 9 am



December 22, 12 pm



December 22, 3 pm

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FINAL PHASE SHADOW
 DIAGRAMS

PROJECT NUMBER
 2015-007
 DATE
 SEPTEMBER 28, 2017
 SHEET NUMBER: 6 OF 15
 (Overall SHEET SIZE: 24" x 36")

SHEET NUMBER
A7-04
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BUILDING 22 RENDERINGS

A8



FACEBOOK CAMPUS AERIAL VIEW FROM EAST



FACEBOOK CAMPUS AERIAL VIEW FROM WEST



VIEW FROM CHILCO STREET/ PUBLIC OPEN SPACE



VIEW FROM BAYFRONT EXPRESS WAY



FACEBOOK BUILDING 22 VIEW FROM SOUTH

EXAMPLES OF USE OF MESH AT OTHER GEHRY BUILDINGS AS SIGNATURE ELEMENTS

NOTE:

In previous Gehry projects metal mesh was not intended for visual screening.

The metal mesh size in these projects is larger than 1/2" metal mesh size proposed for the roof screening of Facebook Building 22 .

A9



EDGEMAR RETAIL COMPLEX



SANTA MONICA PLACE PARKING GARAGE



SANTA MONICA PLACE PARKING GARAGE



CABRILLO MARINE AQUARIUM



GEHRY RESIDENCE

PHOTOS OF
MOCK-UP
FOR PROPOSED
ROOF SCREENING

A10

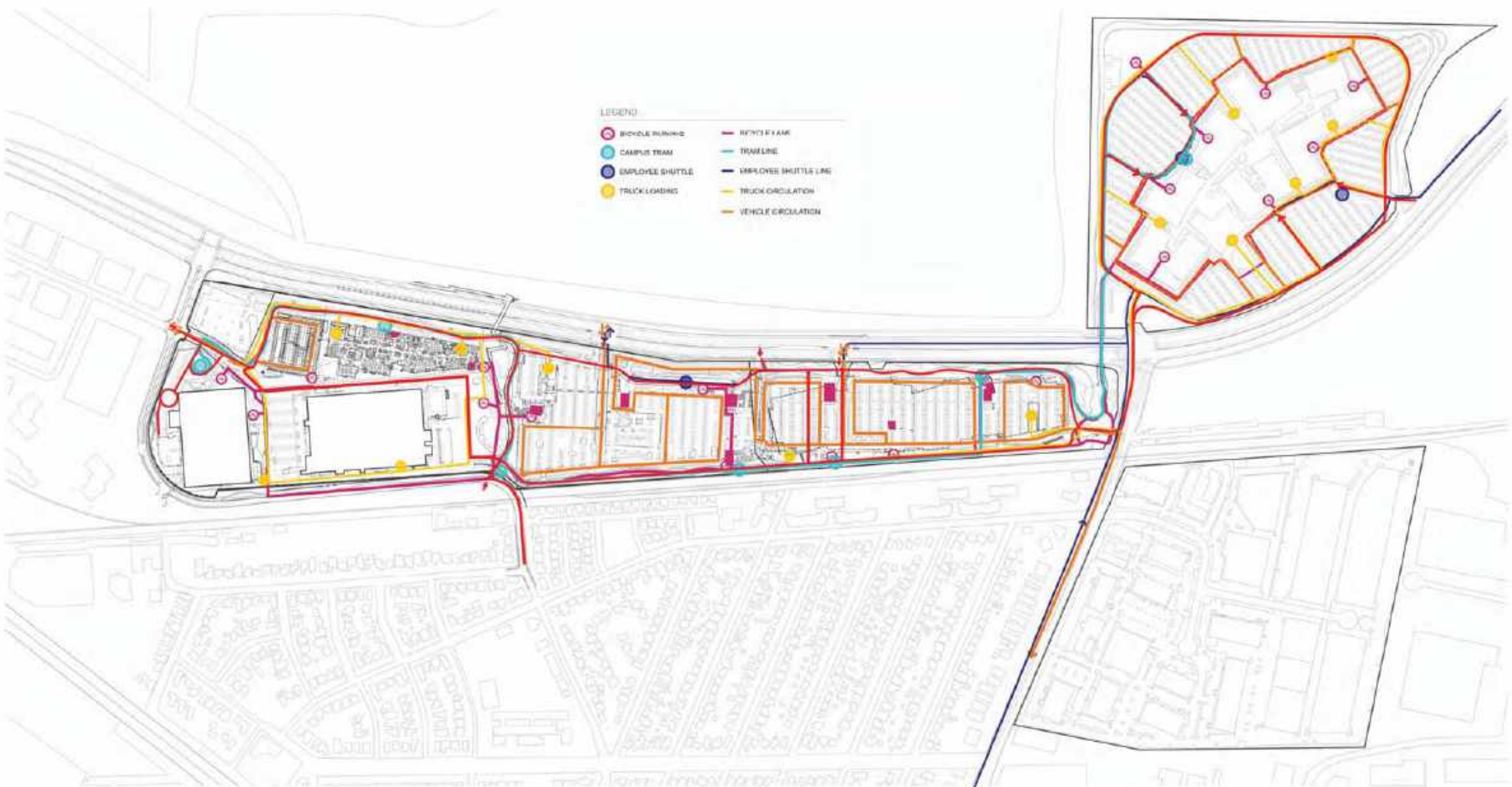


VISUAL MOCK UP FOR MECHANICAL SCREENING FACEBOOK BUILDING 22



VISUAL MOCK UP FOR MECHANICAL SCREENING FACEBOOK BUILDING 22

TRAFFIC



- LEGEND
- BICYCLE ROUTING
 - CAMPUS TRAM
 - EMPLOYEE SHUTTLE
 - TRUCK LOADING
 - REFUSE YARD
 - TRAIL LINE
 - EMPLOYEE SHUTTLE LINE
 - TRUCK CIRCULATION
 - VEHICLE CIRCULATION



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TYPE
INTERIM OVERLAY

PROJECT NUMBER
 2015-007

SHEET
 AS NOTED

DATE
 SEPTEMBER 28, 2017

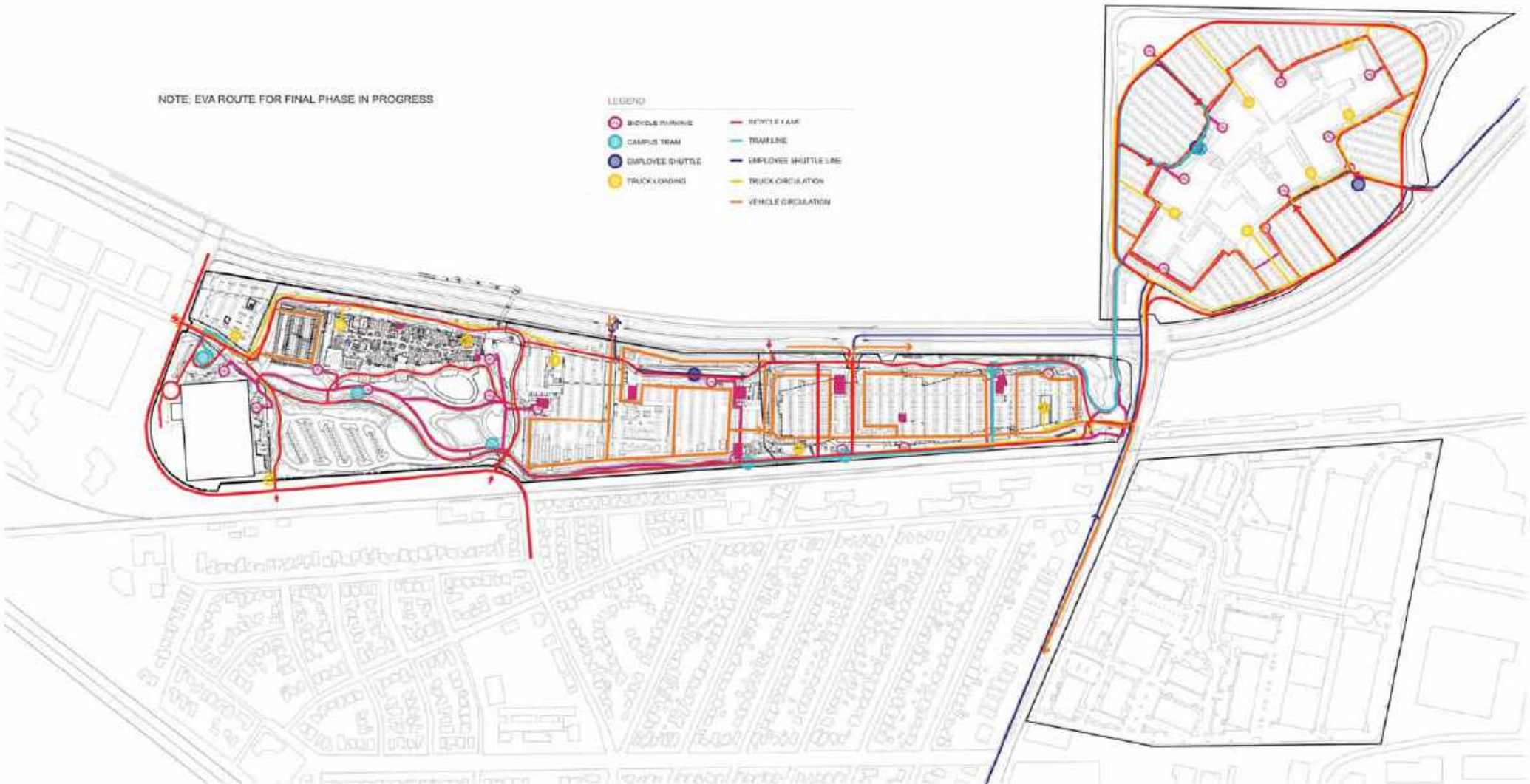
DATE PLOTTED: 10/11/17
 DRAWING SHEET: 01P & 02P

SHEET NUMBER
T1-01

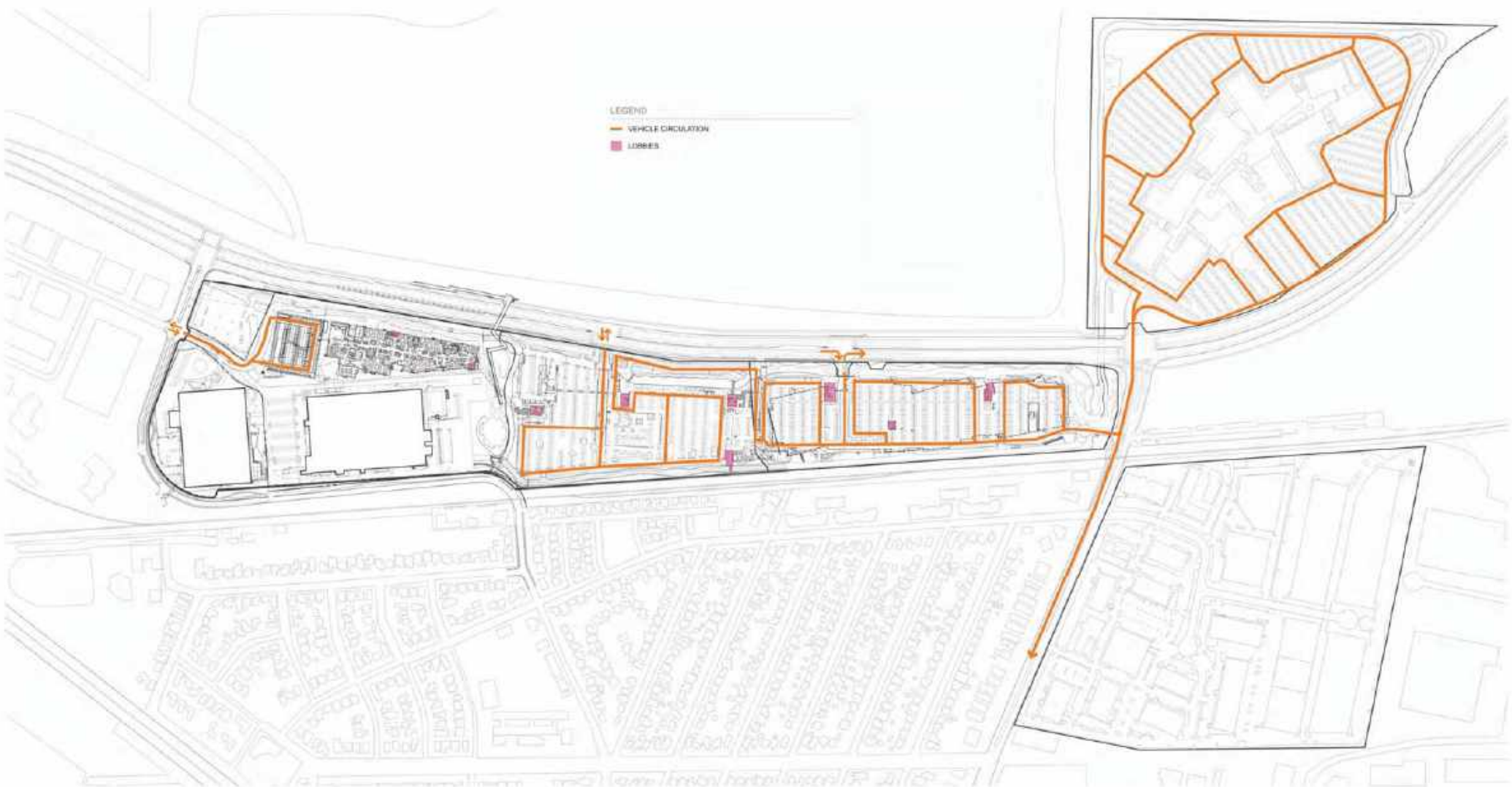
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NOTE: EVA ROUTE FOR FINAL PHASE IN PROGRESS

- LEGEND
- BICYCLE PARKING
 - CAMPUS TRAM
 - EMPLOYEE SHUTTLE
 - TRUCK LOADING
 - BICYCLE LANE
 - TRAM LANE
 - EMPLOYEE SHUTTLE LANE
 - TRUCK CIRCULATION
 - VEHICLE CIRCULATION



<p>GEHRY PARTNERS, LLP ARCHITECT 1000 BRIDGE STREET 175 WOOD STREET, SUITE 200 SAN FRANCISCO, CA 94103</p> <p>FACEBOOK OWNER 1 FRONT ST MENLO PARK, CALIFORNIA 94025</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>FINAL OVERLAY</p>	<p>PROJECT NUMBER 2015-007</p> <p>DATE AS NOTED</p> <p>SEPTEMBER 28, 2017</p> <p>DATE PLOTTED: 9/15 DRAWING SHEET 017 OF 27</p>	<p>SHEET NUMBER T1-02</p> <p>GEHRY PARTNERS, LP</p>
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LEGEND
 — VEHICLE CIRCULATION
 ■ LOBBIES



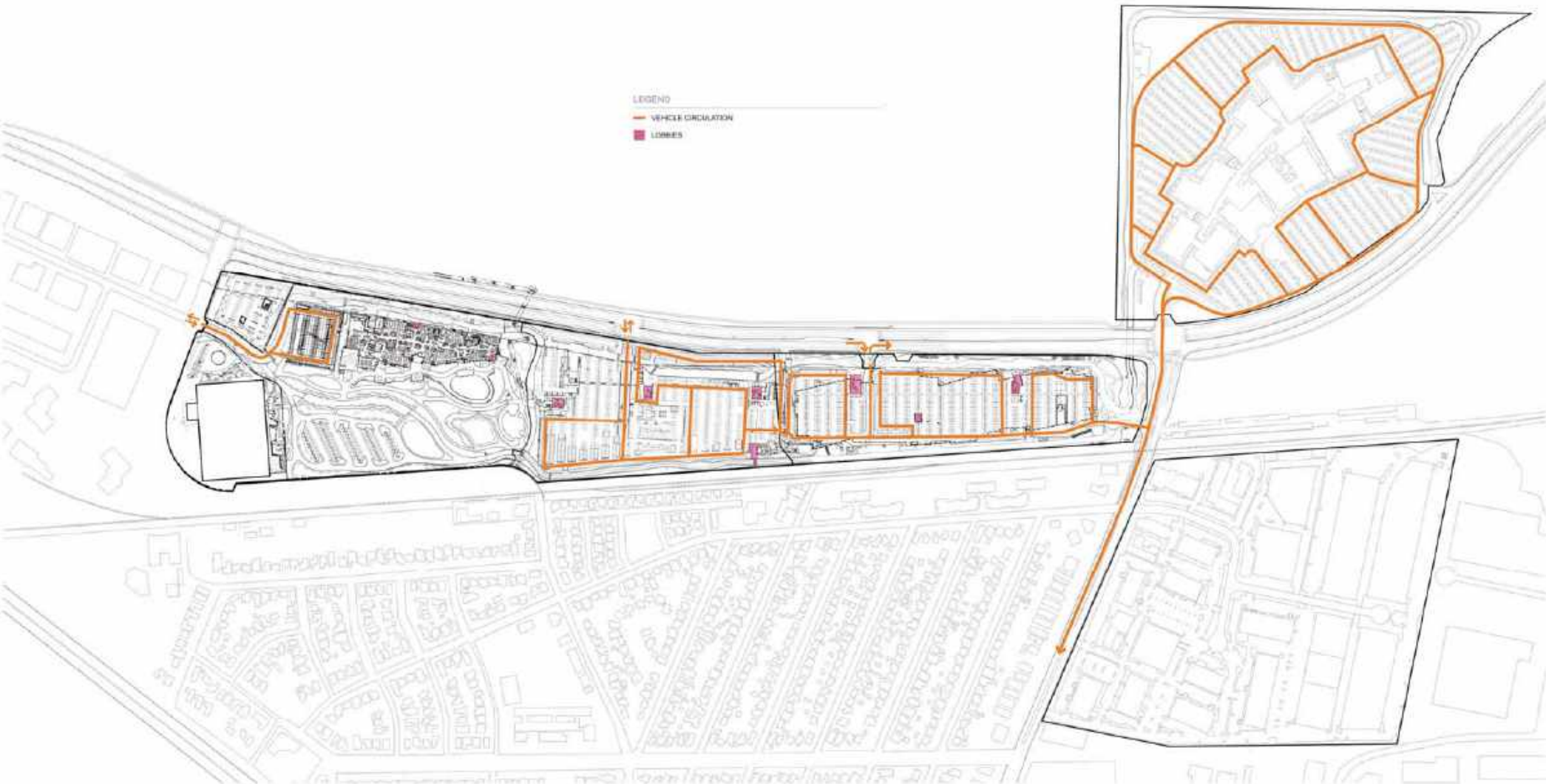
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 (212) 675-1000
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 1 FRONT ST.
 NEW YORK, NY 10038

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TYPE
 INTERIM INTERNAL VEHICLE
 ACCESS

PROJECT NUMBER
 2019-001
 TITLE
 AS NOTED
 DATE
 SEPTEMBER 28, 2017
 DRAWING SHEET NO. 27 OF 37

SHEET NUMBER
 T1-03
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LEGEND
 — VEHICLE CIRCULATION
 ■ Lobbies



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DATE: 09/18/17

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THIS
 FINAL INTERNAL VEHICLE
 ACCESS

PROJECT NUMBER
 2016-007
 DATE
 AS NOTED
 SEPTEMBER 18, 2017
 DATE LAST MOD. BY
 (DATE, USER ID, OF + 2)

SHEET NUMBER
 T1-04
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LEGEND

- BICYCLE PARKING
- BICYCLE LANE
- LOBBIES



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DATE: 09/28/17

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TITLE
**INTERIM INTERNAL BICYCLE
 CIRCULATION**

PROJECT NUMBER
 2015-007
 SHEET AS NOTED
 SEPTEMBER 28 2017
 10:41 AM EDT
 SHEET SIZE: 24" x 36"

SHEET NUMBER
T1-05
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- BICYCLE PARKING
- LOBBIES
- BICYCLE LANE



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TITLE
**FINAL INTERNAL BICYCLE
 CIRCULATION**

PROJECT NUMBER
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 SHEET NUMBER
 AS NOTED
 DATE
 SEPTEMBER 28, 2017
 DRAWN BY
 JAVIER GARCIA, AIA
 CHECKED BY
 JAVIER GARCIA, AIA

SHEET NUMBER
T1-06
 DRAWN BY
 JAVIER GARCIA, AIA



LEGEND
 ● EMPLOYEE SHUTTLE — EMPLOYEE SHUTTLE LINE
 ■ LOBBIES



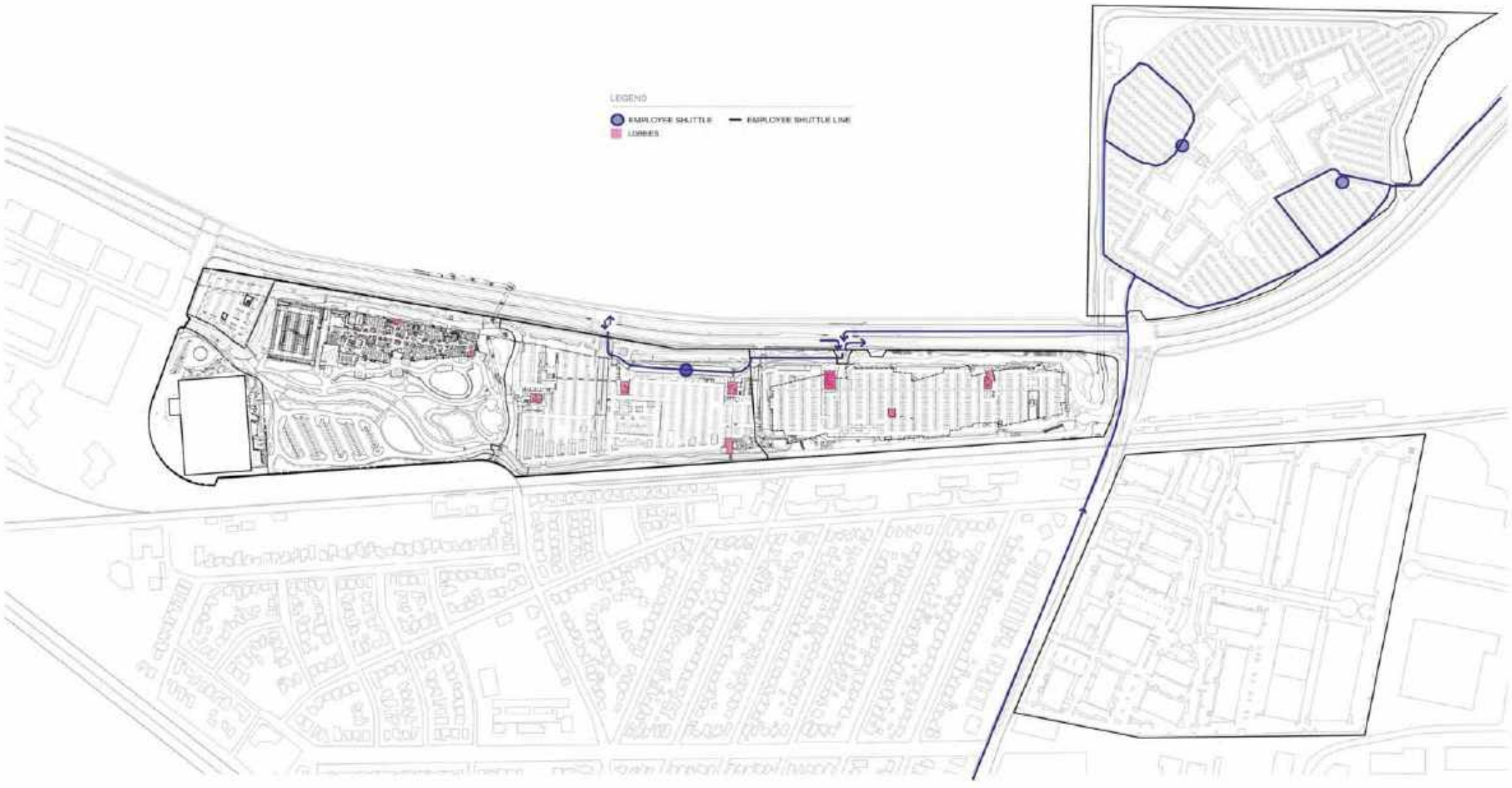
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 (212) 675-1000
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 OWNER
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DATE
 INTERIM EMPLOYEE SHUTTLE
 BUS

PROJECT NUMBER
 2019-007
 DATE AS NOTED
 SEPTEMBER 28, 2017
 THIS SHEET NO. 5/14
 ORIGINAL SHEET NO. 27 + 28

SHEET NUMBER
 T1-07
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LEGEND

- EMPLOYEE SHUTTLE
- EMPLOYEE SHUTTLE LINE
- Lobbies



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TITLE
**FINAL EMPLOYEE SHUTTLE
 BUS**

PROJECT NUMBER
 2015-007
 SHEET AS NOTED
 SEPTEMBER 28, 2017
 DATE PLOTTED: 9/27/17
 DRAWN: GUYE, DUNN, WANG

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T1-08
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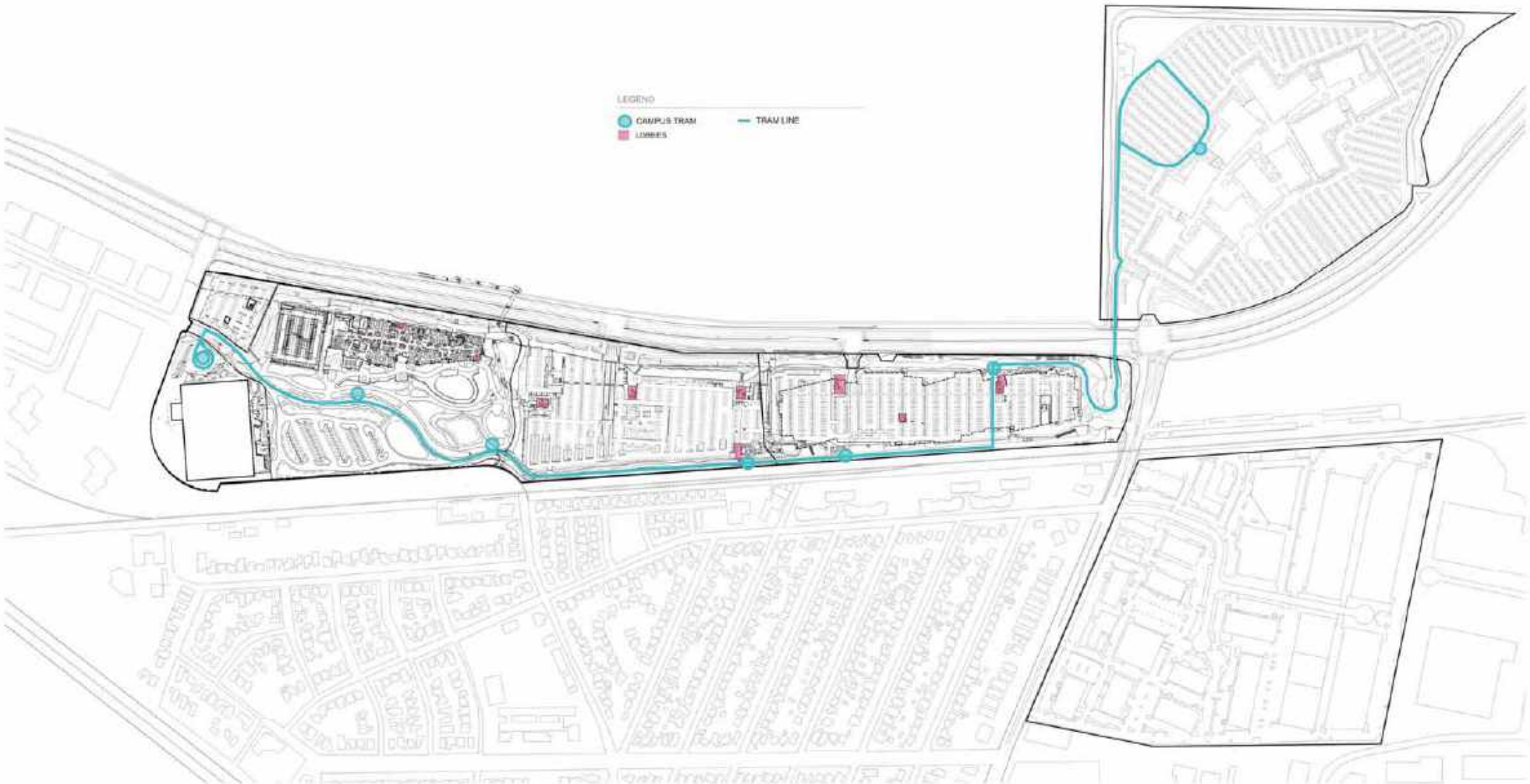
DATE: 09/18/19

Facebook Campus Expansion
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TITLE
**INTERIM EMPLOYEE INTRA
 CAMPUS TRAM**

PROJECT NUMBER
 2019-007
 DATE AS NOTED
 SEPTEMBER 18, 2019
 SHEET SIZE: 30" x 42"

SHEET NUMBER
T1-09
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LEGEND
 ● CAMPUS TRAM
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 — TRAM LINE



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FINAL EMPLOYEE INTRA
 CAMPUS TRAM

PROJECT NUMBER
 2015-007
 DATE
 AS NOTED

SEPTEMBER 28, 2017
 DATE PLOTTED: 9/27/17
 DRAWING SHEET NO.: 02' x 34'

SHEET NUMBER
 T1-10

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LEGEND
 ● TRUCK LOADING
 ■ LOBBIES
 — TRUCK CIRCULATION



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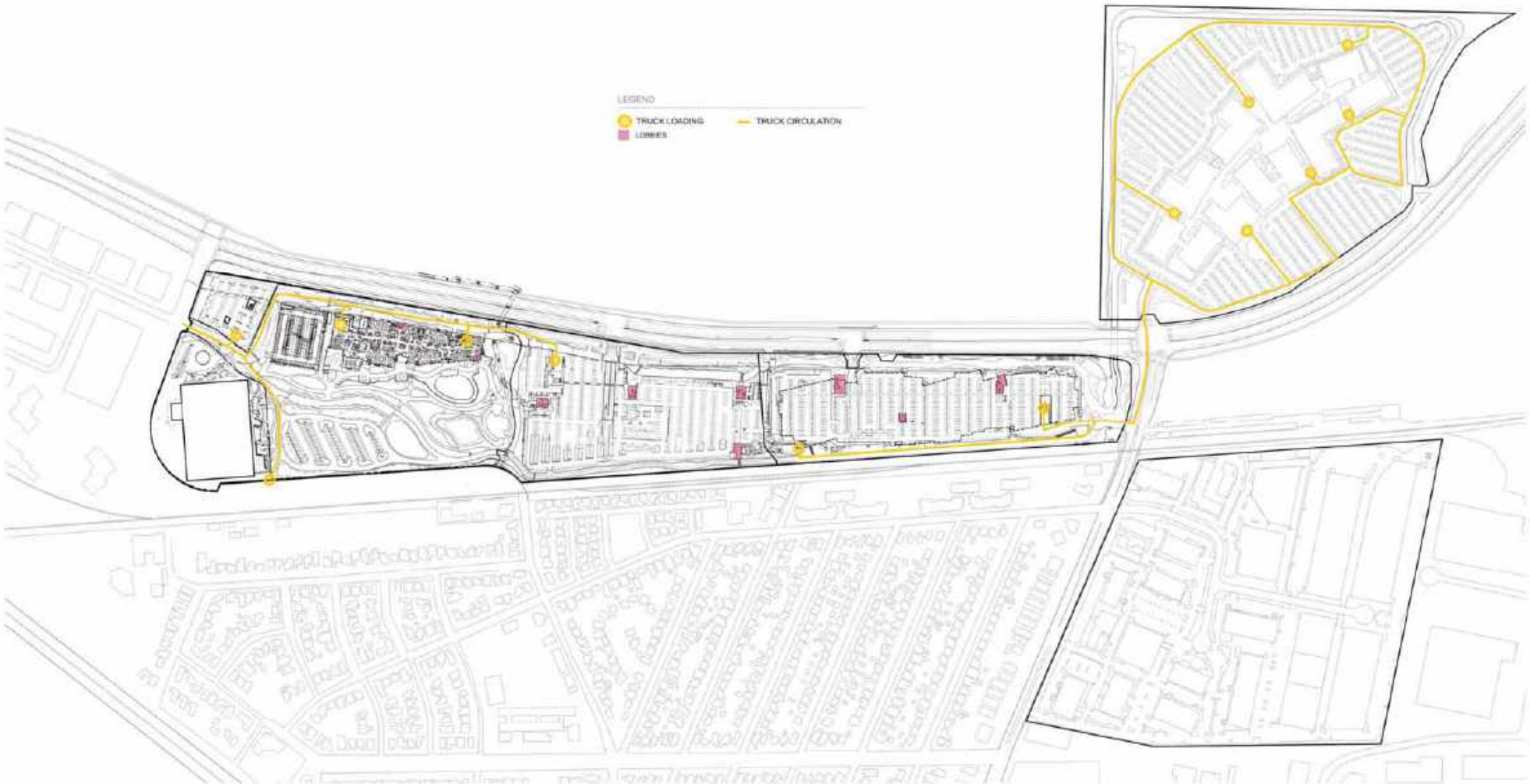
DATE

Facebook Campus Expansion
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TYPE
 INTERIM TRUCK ACCESS

PROJECT NUMBER
 2015-007
 DATE
 AS NOTED
 SEPTEMBER 28, 2017
 10:17 AM EDT, 15.15
 2017 09 28 10:17 AM EDT

SHEET NUMBER
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LEGEND

- TRUCK LOADING
- LOBBIES
- TRUCK CIRCULATION



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DESIGNER

Facebook Campus Expansion
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TITLE
FINAL TRUCK ACCESS

PROJECT NUMBER
 2015-007
 DATE AS NOTED
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 DATE PLOTTED: 9/15
 DRAWING SHEET: 01P - 30

SHEET NUMBER
T1-12
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LEGEND
 — STAIR AND
 ■ LOBBIES



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TYPE
 INTERIM EMERGENCY
 VEHICLE ACCESS

PROJECT NUMBER
 2019-002
 SHEET
 AS NOTED

SEPTEMBER 28, 2017
 DATE PREPARED BY: [illegible]
 DRAWING SHEET NO.: [illegible]

SHEET NUMBER
 T1-13

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NOTE: EVA ROUTE FOR FINAL PHASE IN PROGRESS

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- Lobbies



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**FINAL EMERGENCY VEHICLE
 ACCESS**

PROJECT NUMBER
 2019-007
 DATE AS NOTED
 SEPTEMBER 18, 2019
 DATE PLOTTED: 9/19
 SHEET NUMBER: 07 of 30

SHEET NUMBER
T1-14
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PROJECT NUMBER
 2015-007
 TITLE
 AS NOTED
 DATE
 SEPTEMBER 30, 2017
 DRAWN BY
 JAY WELLS, ET AL.
 CHECKED BY
 JAY WELLS, ET AL.

SHEET NUMBER
T1-15
 TOTAL SHEETS
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TITLE
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FINAL REGIONAL PEDESTRIAN
ACCESS

PROJECT NUMBER
2015-007
DATE
AS NOTED
SEPTEMBER 28, 2017
DATE AND TIME OF THE
PUBLIC MEETING: 2P - 5P

SHEET NUMBER
T1-17
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LANDSCAPE

FACEBOOK CAMPUS EXPANSION - EIR Amendment
LANDSCAPE DESCRIPTION - MPK 22 Office - Interim Phase

The interim landscape improvements include the landscape areas associated with the proposed parking structure and MPK 22 Office building prior to the removal of Building 305. Proposed landscape improvements would significantly increase open space and landscape areas, provide public access and connectivity between the Belle Haven community and the Bay Trail, improve biking and pedestrian circulation, create new campus social spaces and provide the 1st phase of the public access area and park. The Site and Landscape improvements include bicycle and pedestrian paths, fire/emergency vehicle access, bus and shuttle access and loading zones, trash disposal areas, and landscape improvements surrounding the building.

The interim phase will include completion of the 1.4 acre public access area located between Building 305 and MPK 21. The open space will include a mix of uses and will provide a safe pedestrian route to Bayfront Expressway and the Bay Trail. The interim improvements will include a public plaza and event space will anchor the southeast corner of the site, marking the entry to the park at the intersection of Chico Street and the Dumbarton Railroad corridor. The plaza will be a focal point for the park and is envisioned as a flexible space with a mix of paving, seating, and shaded landscape areas. The plaza will include lighting, site furnishings, and power and water service for events. The public access area will provide connections to the multi-use bicycle and pedestrian bridge from the park over Bayfront Expressway to the Bay Trail. Wayfinding signage will be provided to direct public and campus access and circulation.

The landscape surrounding the parking structure and MPK 22 will be similar to the Site Level landscape improvements at MPK 20 and 21 with low landforms, understory plantings, and extensive planting of trees. Low landforms up to 4-feet above existing grades will be planted with native and adapted understory plants. The landforms will provide a buffer along both the north edge of the site with groupings of trees and understory plantings that provide scale and screen the parking structure and office building from Bayfront Expressway. Stormwater treatment areas will be integrated within the landscape and will receive stormwater runoff diverted from impervious surfaces associated with roof, terraces and site level impervious areas. They will be planted with a wide variety of native species with a focus on habitat and stormwater treatment functions.

An outdoor dining terrace and plaza will be located at the southwest corner of the office building adjacent to the interior dining area and event/training spaces. The terrace will include a large shade canopy and planting areas along with moveable furniture and a security station and gate. A series of irregular terraces and landscape areas punctuate both the north and south elevations of the building providing outdoor areas at each floor of the office building. The terraces create an organic rhythm that provides scale and texture to the building by bringing the landscape from ground level up the elevation of the building.

The landscape on the south side of the building will be a temporary landscape with limited improvements and will be finalized as part of the final site construction when the Building 305 site becomes available. The design includes interim egress paths and landscape improvements. Permanent landscape improvements will be provided in locations that will remain in place as part of the final construction. An interim EVA will be located on the south edge of the terrace within the corridor between MPK 22 and Building 305. The EVA will also serve as the primary east-west bicycle circulation route between MPK 21, 22, 23 and Facebook offices to the west.

The site improvements will require the removal of most trees within the project site. Select trees will be preserved or relocated where merited and feasible along the edges of the property. New trees will be planted in sizes and quantities adequate to meet tree replacement requirements consistent with the City of Menlo Park Heritage Tree Ordinance and the City Arboret.

The perimeter of the property and the boundaries of the publicly accessible open space area will be secured with an 8-foot chain-link fence with security stations at each entry to monitor and secure access to the campus. Monument and wayfinding signage will be provided at each of the vehicular entry points consistent with the City of Menlo Park Signage Ordinance. Additional pedestrian wayfinding and rules signage will be located throughout the campus and within the open space area.

FACEBOOK CAMPUS EXPANSION - EIR Amendment
LANDSCAPE DESCRIPTION – Final Phase

The final phase landscape improvements include the expansion of the public access and park area, completion of the campus landscape areas south side of MPK 22 and the addition of a shuttle bus charging and parking area east of MPK 22. The improvements would significantly increase the campus and public open space and landscape areas, expand public access and connectivity, improve biking and pedestrian circulation, and create new campus social spaces. The site and landscape improvements include bicycle and pedestrian paths, fire/emergency vehicle access, bus and shuttle access and loading zones, trash disposal areas, and extensive plantings.

The final phase landscape improvements include expansion and completion of the 2 acre public access area. The expanded open space will include a mix of uses and will maintain connections to the multi-use bicycle and pedestrian bridge from the park over Bayfront Expressway to the Bay Trail. The final improvements will also retain the public plaza and event space (completed as part of the MPK 22 Office phase) at southeast corner of the site, marking the entry to the park at the intersection of Chilco Street and the Dumbarton Railroad corridor. The plaza will be a focal point for the park and is envisioned as a flexible space with a mix of paving, seating, and shaded landscape areas. The plaza will include lighting, site furnishings, and power and water service for events. The public access area will be expanded to the west with the addition of a generous lawn area that will provide space for passive recreation and gatherings. The lawn will be ringed by a path and seating areas and surrounded by shade trees. The interim stormwater treatment areas west of the plaza will be relocated to the west edge of the public access area and integrated with a series of paths and seating areas. The stormwater gardens will be integrated with the adjacent landscape campus areas and will provide screening of the security fence that separates the public access area from the campus spaces.

The completion of the landscape improvements on the south side of the building would include a new EVA route and pedestrian promenade linking the east lobby of MPK 22 with the central entry and dining terrace at the southwest corner of the building. The south edge of the promenade would be punctuated by a series of terraces and shade pavilions. An informal amphitheater and lawn area would be located at the southeast corner of MPK 22 providing space for passive recreation, campus, events, and gatherings. A second shade pavilion located at the middle of the promenade would provide an outdoor seating area along with a secure entry point to MPK 22. MPK 22 and the south terrace area would be separated from the landscape areas to the south by a curvilinear retaining wall designed to accommodate the difference in elevation between existing site grades (EI 8.09.0) and the building finish floor elevation (EI 13.0). The retaining wall would be a total of 8-feet in height and would include a 42-inch guardrail. The exterior (south) side of the wall would be clad with corrugated stainless steel panels and screened by landforms and plantings. The outdoor dining terrace and plaza will be located at the southwest corner of the office building adjacent to the interior dining area will

be retained and the landscape along the south edge of the parking structure will be expanded to include the east-west pedestrian and bicycle route.

To the south of the terrace a new bicycle and pedestrian route connecting MPK 21, 22, and 23 would be provided. The campus drive and EVA route from MPK 21 would be extended on a curvilinear alignment to the northwest to provide a connection to the Constitution Drive campus entry. A new shuttle and tram drop-off area would be located at the middle of the drive to provide access to MPK 21, 22, and 23. The campus drive would be integrated with the surrounding landscape to create the feeling of a parkway. Stormwater treatment areas will be integrated within the landscape and will receive stormwater runoff diverted from impervious surfaces associated with site impervious surfaces. They will be planted with a wide variety of native species with a focus on habitat and stormwater treatment functions. The shuttle bus parking and electric charging area located south of the campus drive/EVA and east of MPK 23 will be screened by large trees located along the Chilco Street frontage and will include large islands flanking each bay of parking.

The site improvements will require the removal of most trees within the project site. Select trees will be preserved or relocated where merited and feasible along the edges of the property. New trees will be planted in sizes and quantities adequate to meet tree replacement requirements consistent with the City of Menlo Park Heritage Tree Ordinance and the City Arborist.

The perimeter of the property and the boundaries of the publicly accessible open space area will be secured with an 8-foot chain-link fence with security stations at each entry to monitor and secure access to the campus. Monument and wayfinding signage will be provided at each of the vehicular entry points consistent with the City of Menlo Park Signage Ordinance. Additional pedestrian wayfinding and rules signage will be located throughout the campus and within the open space area.



CMG
 LANDSCAPE ARCHITECTS
 454 FORT ST. SAN FRANCISCO, CA 94107
 (415) 440-1510

FACEBOOK
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 1600 AVENUE AVENUE
 MENLO PARK, CALIFORNIA 94025

DATE: 07/17/2017

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
Proposed Landscape
Illustrative Plan
Interim Condition

PROJECT NUMBER
 2015-007
 DATE
 JULY 17, 2017
 SCALE
 1/4" = 100'-0" (A, B, C)
 SHEET NUMBER
 217 OF 307

SHEET NUMBER
L0-02
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 444 F STREET, SW, WASHINGTON, DC 20004
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DATE: 07/17/2017

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TITLE
Proposed Landscape
Illustrative Plan
Final Condition

PROJECT NUMBER
 2015-007
 DATE
 July 17, 2017
 DRAWN: [unreadable] R. [unreadable]
 CHECKED: [unreadable] J.P. [unreadable]

SHEET NUMBER
L0-03
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GENERAL KEY	
LINE TYPES	
	Trees to be removed
	BBIS Line Line
	MPK 21 Limit of Work (LOW)
	TE Site Property Line
NOTE: All proposed tree removals are required as a result of construction impacts.	

2016 APPROVED PROJECT				
BUILDING 301-309 SUMMARY -- PER FALL 2016 APPROVED EIR				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
549		GOOD health	2:1	258
611		FAIR-POOR health	1:1	61
13		DEAD health	1:1	59
274 TOTAL				423 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE				
496		See tree survey data	NA	NA

BUILDING 307-309 SUMMARY (MPK 21 SITE) -- ALREADY REMOVED PER FALL 2016				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
22		GOOD health	2:1	44
18		FAIR-POOR health	1:1	18
8		DEAD health	1:1	8
48 TOTAL				70 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE				
307		See tree survey data	NA	NA

2017 PROPOSED REVISED PROJECT				
BUILDING 301-306 SUMMARY -- EXCLUDING MPK 21 REMOVED TREES				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
127		GOOD health	2:1	254
48		FAIR-POOR health	1:1	48
51		DEAD health	1:1	51
226 TOTAL				353 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE				
189		See tree survey data	NA	NA

MPK 22 INTERIM PHASE				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
85		GOOD health	2:1	172
29		FAIR-POOR health	1:1	29
13		DEAD health	1:1	13
148 TOTAL				234 TOTAL

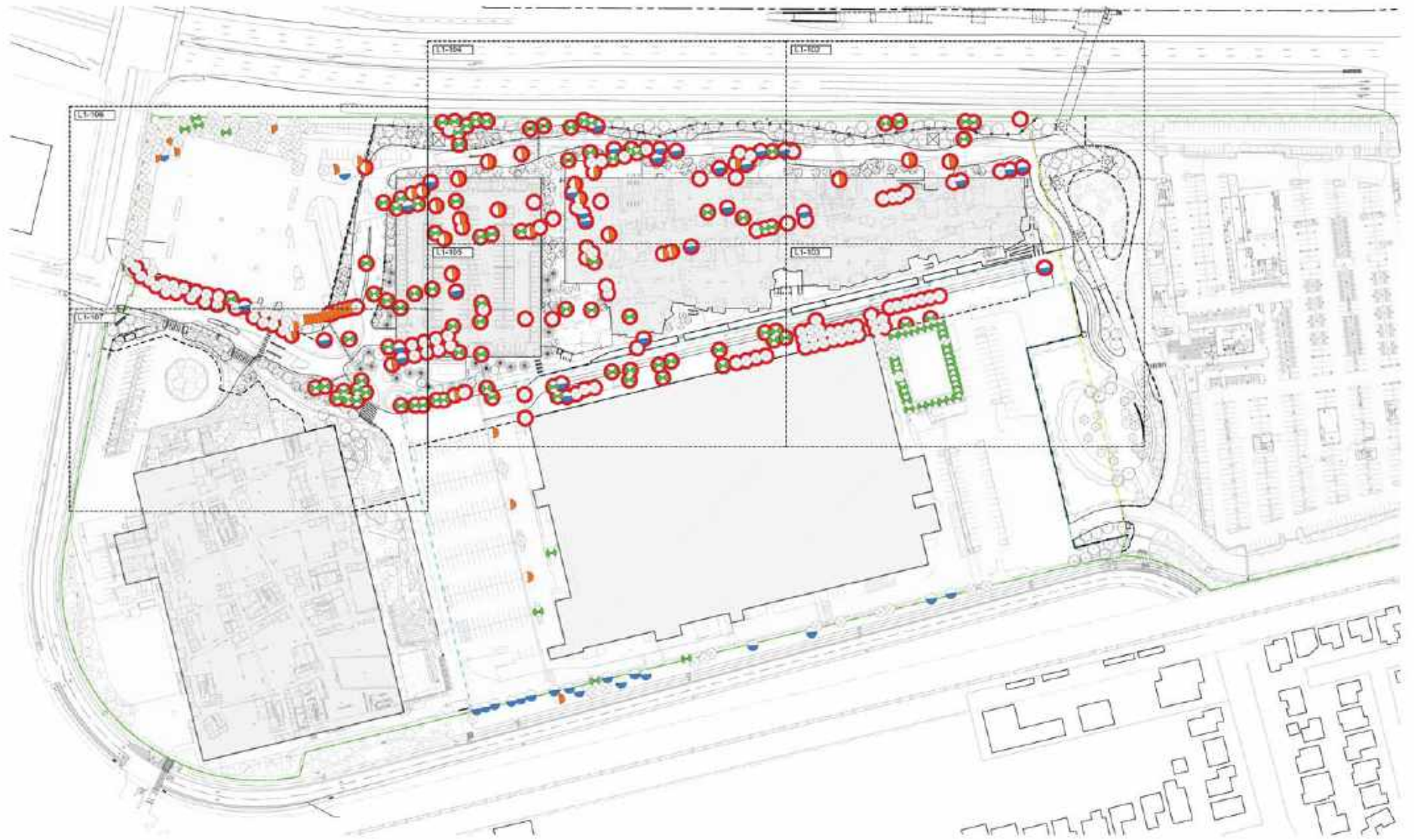
Tree Removal & Required Replacement Values: NON-HERITAGE				
100		See tree survey data	NA	NA

MPK 22 FINAL PHASE				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
37		GOOD health	2:1	74
17		FAIR-POOR health	1:1	17
4		DEAD health	1:1	4
58 TOTAL				95 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE				
23		See tree survey data	NA	NA

2017 PROPOSED REVISED PROJECT, CONTINUED				
MPK 22 Hotel Site				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
4		GOOD health	2:1	8
2		FAIR-POOR health	1:1	2
14		DEAD health	1:1	14
20 TOTAL				24 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE				
65		See tree survey data	NA	NA



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 LANDSCAPE ARCHITECTS
 400 BROAD ST., SAN FRANCISCO, CA 94103
 (415) 440-2020
 FACEBOOK
 OWNER
 1 FORTRESS WAY, CULVER CITY, CALIFORNIA 90230

DATE: 09/20/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
 HERITAGE TREE DISPOSITION
 OVERALL SITE PLAN

Interim Condition

PROJECT NUMBER
 2015-027
 SCALE
 1" = 25' 0"
 20-September-2017
 THIS SHEET IS ONE
 OF THE TOTAL SET OF 24 SHEETS

SHEET NUMBER
 L1-101
 DESIGN PARTNER: GFL



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LANDSCAPE ARCHITECTS
400 WEST 10TH, SAN FRANCISCO, CA 94103
(415) 440-0000

FACEBOOK
OWNER
1 FORTRESS WAY, MENLO PARK, CALIFORNIA 94025

DATE: 10/26/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROJECT: HERITAGE TREE DISPOSITION
ENLARGEMENT PLAN

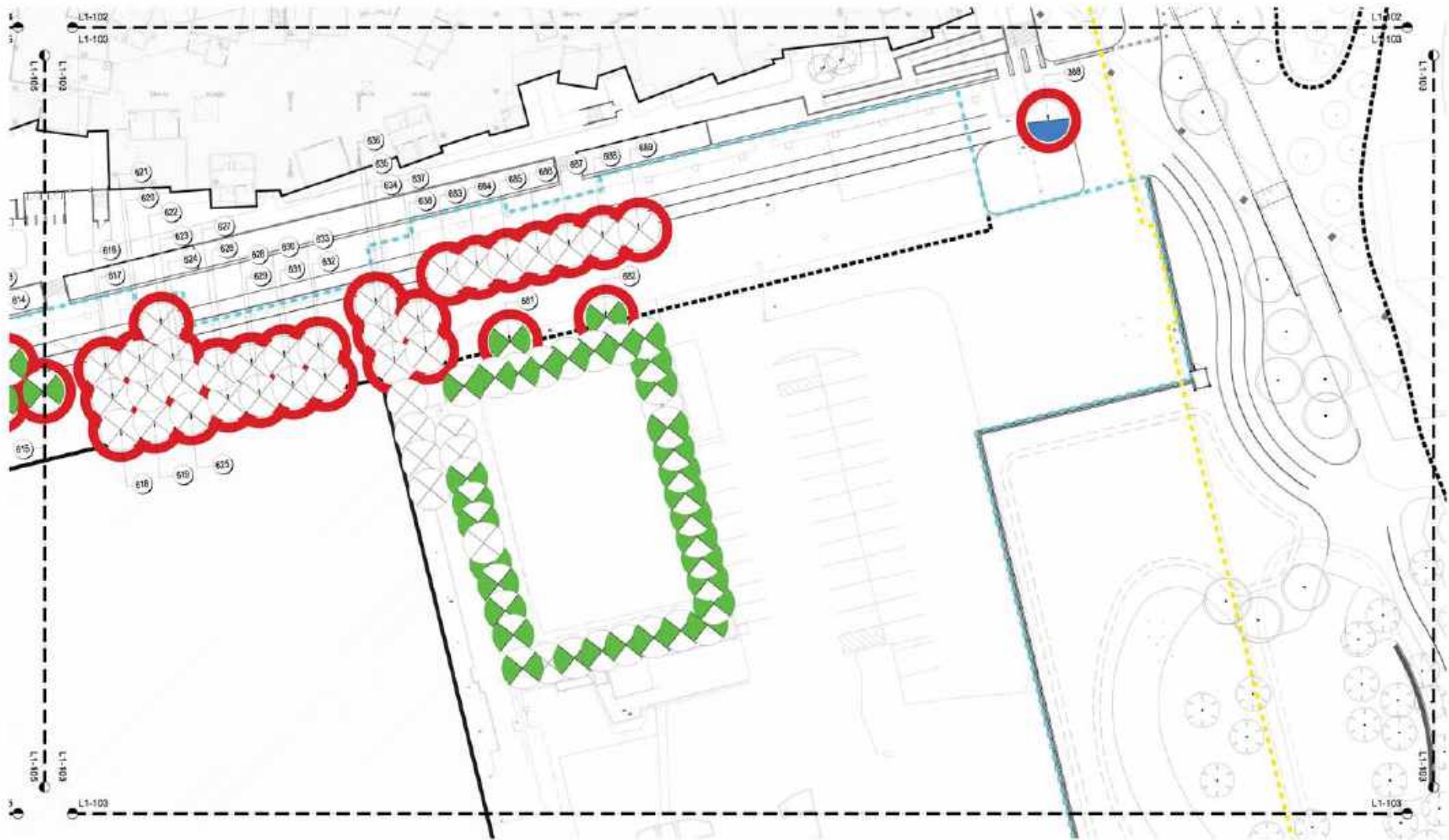
Interim Condition

PROJECT NUMBER: 2015-007
SCALE: 1" = 20' 0"
DATE: 26-December 2017
DRAWN BY: [Name]
CHECKED BY: [Name]

SHEET NUMBER

L1-102

DATE: 10/26/17



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400 WEST 10TH, SAN FRANCISCO, CA 94103
(415) 440-2020

FACEBOOK
OWNER
1700 BAY STREET, MENLO PARK, CALIFORNIA 94025

DATE: 10/26/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROJECT NAME
HERITAGE TREE DISPOSITION
ENLARGEMENT PLAN

Interim Condition

PROJECT NUMBER
2015-027
SCALE
1" = 20' 0"

26-December 2017
THIS SHEET IS ONE OF 10 SHEETS IN SET: 24 - 26"

SHEET NUMBER
L1-103

DATE PLOTTED: 10/26/17



CMG
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FACEBOOK
 OWNER
 1 FORTRESS WAY, MENLO PARK, CALIFORNIA 94025

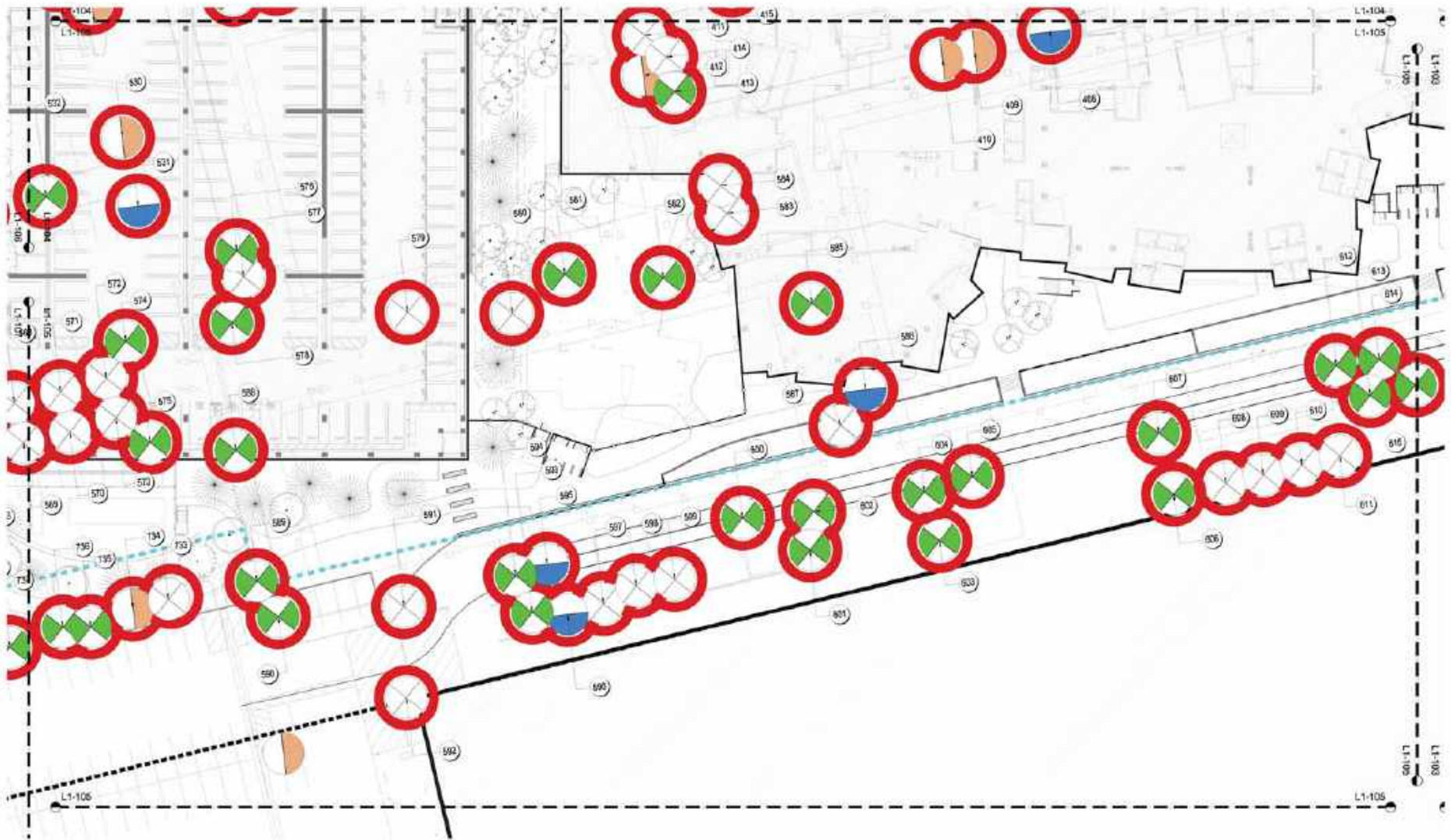
DATE: 10/26/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
HERITAGE TREE DISPOSITION
ENLARGEMENT PLAN
 Interim Condition

PROJECT NUMBER
 2016-007
 SCALE
 1" = 20' 0"
 DATE
 26-December 2017
 THIS SHEET NO. 116
 TOTAL SHEET NO. 24 + 24*

SHEET NUMBER
L1-104
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FACEBOOK
OWNER
1 FORTRESS WAY
MENLO PARK, CALIFORNIA 94025

DATE: 10/26/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TYPE
HERITAGE TREE DISPOSITION
ENLARGEMENT PLAN

Interim Condition

PROJECT NUMBER
2015-027

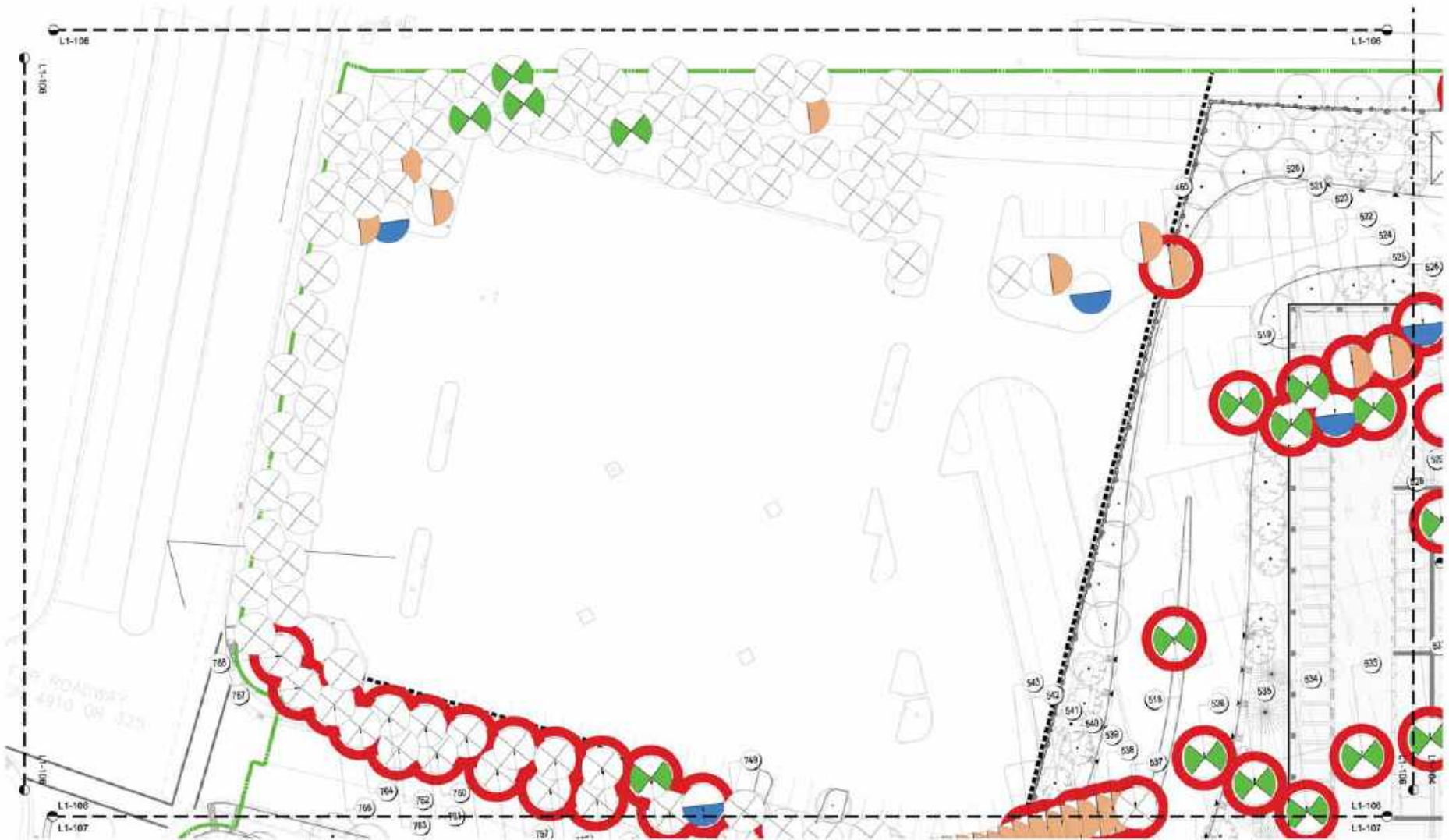
SCALE
1" = 20' 0"

26-December 2017
THIS SHEET IS ONE OF 16
OTHER SHEETS: 24 + 25*

SHEET NUMBER

L1-105

DATE PLOTTED: 11/1/17



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400 BRANT ST., SAN FRANCISCO, CA 94103
(415) 440-2020

FACEBOOK
OWNER
1700 BAY ST., MENLO PARK, CALIFORNIA 94025

DATE: 10/26/17

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300-309 Constitution Drive

PROJECT NAME
HERITAGE TREE DISPOSITION
ENLARGEMENT PLAN

Interim Condition

PROJECT NUMBER
2015-027
SCALE
1" = 20' 0"
DATE
26-December-2017
PLANT SYMBOLS: N/A
DRAWING SHEET NO.: 24' x 36'

SHEET NUMBER

L1-106

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400 WEST 10, SAN FRANCISCO, CA 94111
(415) 440-2020

FACEBOOK
OWNER
1 FORTRESS WAY, CUPERTINO, CA 95014

DATE: 10/26/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

752
HERITAGE TREE DISPOSITION
ENLARGEMENT PLAN

Interim Condition

PROJECT NUMBER
2015-027

SCALE
1" = 20' 0"

26-December 2017
THIS SHEET IS A
PART OF SET 24 - 26'

SHEET NUMBER

L1-107

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SBCA TREE CONSULTING

1529 Park Avenue, Coventry, CT 06238
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Website: www.sbcacorp.com

Steve Baskin, Consulting Arborist
NY State Certified Arborist #222
ISPC Certified Arborist #214
ISA Certification Expires 09/30/2019
E-mail: sbaskin@sbcacorp.com

Shelly Baskin, Consulting Arborist
NY State Certified Arborist #78144
ISA Tree Risk Assessment Qualified
E-mail: sbaskin@sbcacorp.com

Date: March 28, 2016

To: Wayne (Wayne) Davis

Subject: Tree Survey

Location: 300-309 Constitution Drive

Assignment: Assess tree safety for top and canopy of trees located at site and City trees along 300th Ave.

City of Meriden Park Ordinance

Conditions of Heritage Tree:

- Any tree having a trunk with a circumference at 4.5 inches diameter at 10 inches or more measured at diameter above buttresses.
- Any oak tree native to California, with a circumference of 24 inches or more measured at 4.5 inches diameter or greater.
- Any tree or group of trees specifically designated by the City Council for protection because of its historical significance, special character or ornamental quality.
- Any tree with a diameter measured at the point where the trunk divides, with a circumference of 4.5 inches diameter at 10 inches or more, with a measured root system that extends to the 100 feet or more, which is exempt from the ordinance.

Summary:

- SUMMARY:** The tree survey recorded information on seven hundred seventy (770) trees located on the grounds of 300-309 Constitution Drive and along the west end of 300th Ave. Metal number tags were attached to all trees. Data was taken on Tree Size, Health and Structural Condition, Suitability for Retention, and Potential Risk.
- Two hundred seventy-four (274) trees surveyed qualify as "Heritage Trees".
- Thirty-four (34) different species were noted in the survey. The species most represented on-site include London Plane, Platanus or Mammoth, with one hundred twenty-two (122) specimens.

<http://www.meriden.org/300thAve.html>

300-309 Constitution Drive Tree Survey 3/28/16
045 1 of 4

removed. One (1) specimen, representing only one (1) tree, was noted. However, this plant remains with only right (R), and Silver Sucker Gum (S) specimens taken from left (L) specimens.

- Twenty-two (22) trees surveyed were dead, most are London Plane located along the southern property line. One (1) specimen is due for "trimming".
- Trees given a "Poor" suitability for retaining are located near health facility and parking lot adjacent to building, and for poor soil growing conditions and soil with poor tree placement. Soil conditions are concerned being of the root cause of poor performance.

Summary of Tree Species

Table on following page provides information on the tree species surveyed and the number of trees in Heritage Trees, with suitability for retention and potential risk. The survey data is provided in Appendix E.

Species	Common Name	Amount	Percent of Total	Suitability for Retention	Risk
1	Acacia	1	0.1	0	0
2	Acacia saligna	1	0.1	0	0
3	Acacia saligna	1	0.1	0	0
4	Azadirachta indica	1	0.1	0	0
5	Celastrus scandens	1	0.1	0	0
6	Clusia rosea	1	0.1	0	0
7	Clusia rosea	1	0.1	0	0
8	Clusia rosea	1	0.1	0	0
9	Clusia rosea	1	0.1	0	0
10	Clusia rosea	1	0.1	0	0
11	Clusia rosea	1	0.1	0	0
12	Clusia rosea	1	0.1	0	0
13	Clusia rosea	1	0.1	0	0
14	Clusia rosea	1	0.1	0	0
15	Clusia rosea	1	0.1	0	0
16	Clusia rosea	1	0.1	0	0
17	Clusia rosea	1	0.1	0	0
18	Clusia rosea	1	0.1	0	0
19	Clusia rosea	1	0.1	0	0
20	Clusia rosea	1	0.1	0	0
21	Clusia rosea	1	0.1	0	0
22	Clusia rosea	1	0.1	0	0
23	Clusia rosea	1	0.1	0	0
24	Clusia rosea	1	0.1	0	0
25	Clusia rosea	1	0.1	0	0
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31	Clusia rosea	1	0.1	0	0
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33	Clusia rosea	1	0.1	0	0
34	Clusia rosea	1	0.1	0	0
35	Clusia rosea	1	0.1	0	0
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37	Clusia rosea	1	0.1	0	0
38	Clusia rosea	1	0.1	0	0
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41	Clusia rosea	1	0.1	0	0
42	Clusia rosea	1	0.1	0	0
43	Clusia rosea	1	0.1	0	0
44	Clusia rosea	1	0.1	0	0
45	Clusia rosea	1	0.1	0	0
46	Clusia rosea	1	0.1	0	0
47	Clusia rosea	1	0.1	0	0
48	Clusia rosea	1	0.1	0	0
49	Clusia rosea	1	0.1	0	0
50	Clusia rosea	1	0.1	0	0

SBCA Tree Consulting
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Website: www.sbcacorp.com

300-309 Constitution Drive Tree Survey 3/28/16
045 1 of 4

Species	Common Name	Amount	Percent of Total	Suitability for Retention	Risk
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2	Acacia saligna	1	0.1	0	0
3	Acacia saligna	1	0.1	0	0
4	Azadirachta indica	1	0.1	0	0
5	Celastrus scandens	1	0.1	0	0
6	Clusia rosea	1	0.1	0	0
7	Clusia rosea	1	0.1	0	0
8	Clusia rosea	1	0.1	0	0
9	Clusia rosea	1	0.1	0	0
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50	Clusia rosea	1	0.1	0	0

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300-309 Constitution Drive Tree Survey 3/28/16
045 1 of 4

Species	Common Name	Amount	Percent of Total	Suitability for Retention	Risk
1	Acacia	1	0.1	0	0
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3	Acacia saligna	1	0.1	0	0
4	Azadirachta indica	1	0.1	0	0
5	Celastrus scandens	1	0.1	0	0
6	Clusia rosea	1	0.1	0	0
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48	Clusia rosea	1	0.1	0	0
49	Clusia rosea	1	0.1	0	0
50	Clusia rosea	1	0.1	0	0

End Report

Appendix

- 1. Tree Survey Data

Submitted by:
Steve Baskin

Steve Baskin, Consulting Arborist
NY State Certified Arborist #222
ISA Tree Risk Assessment Qualified
ISA Certification Expires 09/30/2019
E-mail: sbaskin@sbcacorp.com

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LANDSCAPE ARCHITECTS
485 West 25th Street, Fremont, CA 94538
Tel: (510) 466-9300

FACEBOOK
OWNER
17000000000000000000

PROJECT NAME
2015-007
GOLF
170

PROJECT NAME
2015-007
GOLF
170

PROJECT NAME
2015-007
GOLF
170

PROJECT NAME
2015-007
GOLF
170

PROJECT NAME
2015-007
GOLF
170

TREE LEGEND	
SYMBOL	DESCRIPTION
	GOOD health
	FAIR POOR health
	POOR health
	Non-heritage tree
	Tree with GOOD suitability for retention
	Tree with FAIR suitability for retention
NOTES	
<p>1) See table overview of existing heritage and non-heritage trees with the proposed plans for MPK 21 & MPK 22.</p> <p>2) SCA Arborist reviewed the site on 19 October 2016 to review all existing trees located along the Bayfront Drive way frontage of the T1 site. The table was to determine which trees may be suitable for retention in the new landscape to avoid screening to the site and existing POA towers. Note the original survey was conducted in early 2015.</p> <p>3) In all other cases for all trees found on site trees (POA) trees surveyed can be found in accompanying TREE SURVEY DATA, 23-04-2016. The last column provides comments from the recent inspection. Significant changes in other structural condition of health have been yellow light green on the survey data.</p> <p>4) Trees most suitable for retention are highlighted in green and given a ranking of 'GOOD'.</p> <p>5) Trees that could be suitable for retention are highlighted in blue and given a ranking of 'FAIR'.</p> <p>6) All existing heritage and non-heritage trees of 'GOOD' or 'FAIR' suitability for retention that fall with in the MPK 21 project site have already been removed under the MPK 21 Grading Permit.</p> <p>7) All existing heritage and non-heritage trees of 'GOOD' or 'FAIR' suitability for retention that fall with in the MPK 22 project site could potentially be kept for screening as they fall within proposed landscaped areas. They will be determined at the time of further analysis and approval.</p> <p>8) The trees of 'GOOD' or 'FAIR' suitability for retention at this location, are scheduled to be cut and also street 13-171.</p> <p>9) For all other trees of 'GOOD', 'NOT SUITABLE' or 'POOR' suitability for retention, see accompanying TREE SURVEY DATA, 23-04-2016.</p> <p>10) SCA report includes further information as a list of trees suitable for retention, see accompanying APPENDIX REPORT, 16-04-2016.</p> <p>11) See Tree Disposition Plans, Data, and Approval Report sheets L1-100 through L1-136.</p>	
SUITABILITY KEY	
<p>CHANGE: RESTRICTED AND HEALTHY (with) Final indicator error correction (March 2015) with the second letter after "tree" indicating recent assessment where there is a high level of change in other health or structural condition.</p> <p>GOOD SUITABILITY (green) - indicates trees in good condition that could be retained if kept in situ.</p> <p>FAIR SUITABILITY (blue) - indicates trees in fair condition that could possibly be retained if kept in situ.</p>	

MPK 22: FAIR suitability for retention									
Tag #	Species	DIM	Height	Structure	Health	Suitability for Retention	Notes	Total Trees	Total Heritage
383	Pinus radiata	24	15	G	F	F-F	Close to 4072, loose RF, CB	1	1
384	Pinus radiata	8	28	G	F	F-G	Small up, JB, SF	1	0
385	Pinus radiata	21	45	G	F	F-G	Close to 4143, CB, structure, large pruning wounds	1	1
441	Pinus radiata	20.5	15	F	F	F	Close to tree, CB	1	1
459	Pinus radiata	15	10	G	F-F	F	Significant loss, large pruning wounds, Crowded	1	1
462	Pinus radiata	28.5	15	G	F-F	G	CB, loss	1	1
Total MPK 22								6	5
MPK 22: GOOD suitability for retention									
Tag #	Species	DIM	Height	Structure	Health	Suitability for Retention	Notes	Total Trees	Total Heritage
387	Pinus radiata	20	20	F-F	F	F	Off centre, CB, loose, CB	1	1
440	Pinus radiata	20	48	F-G	F	G	CB, DM	1	1
457	Pinus radiata	39 @ 2'	45	C	G	G	CB	1	1
458	Pinus radiata	35.5	18	F-G	F	F	Crowded, DM	1	1
464	Pinus radiata	23.5 @ base	45	G	F-G	G	Large pruning wounds, close tree	1	1
Total MPK 22								5	5
Hotel: FAIR suitability for retention									
Tag #	Species	DIM	Height	Structure	Health	Suitability for Retention	Notes	Total Trees	Total Heritage
470	Alnus incana	8.5	10	A-G	G	G	Attraction CB, high voltage power lines	1	0
479	Alnus incana	12.5	40	F-G	G	F	Loss	1	0
481	Alnus incana	15	40	F	G	F		1	0
500	Alnus incana	12.5	40	F	F-G	F		1	0
507	Alnus incana	17	40	F	G	F		1	0
508	Acrotydon confertiflorus	9.5	15	G	F	F-F	Loss over parking lot, structure damage	1	0
511	Alnus incana	18	45	F	F	F		1	0
Total MPK 22								7	1





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 LANDSCAPE ARCHITECTS
 400 WEST 10th, SAN FRANCISCO, CA 94103
 (415) 440-2000

FACEBOOK
 OWNER
 1 FORTRESS BLVD., MENLO PARK, CALIFORNIA 94025

DATE: 10/20/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
**HERITAGE TREE POTENTIAL
 PRESERVATION PLAN**

Interim Condition

PROJECT NUMBER
 2015-007
 SCALE
 1" = 75' 0"
 DATE
 26-December 2017
 DRAWN BY
 JESSICA WILSON
 CHECKED BY
 JESSICA WILSON

SHEET NUMBER
L1-121
 SHEET TOTAL
 121 OF 121

HERITAGE TREE CONSULTING
1844 14th Street, Suite 200, San Francisco, CA 94103
Phone: (415) 398-3000
Fax: (415) 398-3000
Website: www.htrc.com

Steve Rabinovich, Consulting Arborist
MS in Forest Management (2010)
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E-mail: steve@htrc.com

Marty Rabinovich, Consulting Arborist
MS in Forest Management (2010)
C-107 Certified Urban Forester (2010)
E-MAIL: marty@htrc.com

Date: October 21, 2017
To: Steve@cmg.com
Subject: Facebook Building Expansion, Tree Removal and Retention Review
Location: 1750 North Broadway (along Redwood Damaging)
Assignment: A review was conducted to assess the health and retention potential of the Facebook property to determine which trees may be suitable for retention in the new building use.

Introduction
The original tree survey was conducted in early 2015. The most recent transaction was conducted to determine which trees along the Redwood Damaging property could be retained, and which trees would be removed. The survey was conducted in early 2017.

Inspection Notes
Inspection notes can be found on the accompanying field report sheets. The best solution provides comments from the survey perspective. Care columns or ground sheet has noted comments. Lightly colored changes in which structural condition of health have been noted highlighted in the survey data. Trees shown suitable for retention are highlighted in green.

Changes Made
The results of the most significant changes to the data survey were resulting updates to the health and death of a number of trees. The death and retention status for trees in the survey data were updated if the tree showed evidence of water stress or other factors. Trees that are considered to be having significant water stress are highlighted in yellow.

Tree Loss Potential
The loss potential for the most significant trees was assessed to have a high potential for loss due to the amount of water stress and other factors.

Tree Retention Potential
The loss potential for the most significant trees was assessed to have a high potential for loss due to the amount of water stress and other factors.

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Phone: (415) 398-3000 Fax: (415) 398-3000 Website: www.htrc.com

10/21/17
1 of 9

Tree Health Data
Data collected on 10/21/17 from a tree survey conducted along the north portion of the Facebook property that is bordering the Redwood Damaging. The suitability ratings are listed below:

- 100%** (17/10/17): Ten (10) trees were assigned a Good suitability rating. These trees included the tree 100% (17/10/17) and 100% (17/10/17) and 100% (17/10/17).
- 75%** (10/10/17): Seven (7) trees were assigned a Fair suitability rating. This rating is for trees that are in a marginal condition but could be retained with an arborist and/or health mitigation.
- 50%** (10/10/17): Three (3) trees were assigned a Poor rating. This indicates that the tree is in poor condition and that arborist recommendations for retention, to determine which trees could be retained.
- 25%** (10/10/17): Three (3) trees were assigned a Not Suitable rating, meaning that these trees are not suitable for retention due to health or other structural factors.
- 0%** (0/10/17): No trees were assigned a Not Suitable rating.

Retention of Specific and Problematic Trees
100% (17/10/17): One of the most trees in the survey, 100% (17/10/17) is a large and prominent tree.

100% (17/10/17): This tree is a large tree that is in good health and is a prominent tree.

75% (10/10/17): These trees are in fair health and are in good health.

75% (10/10/17): These trees are in fair health and are in good health.

75% (10/10/17): These trees are in fair health and are in good health.

50% (10/10/17): These trees are in poor health and are in poor health.

50% (10/10/17): These trees are in poor health and are in poor health.

25% (10/10/17): These trees are not suitable for retention due to health or other structural factors.

0% (0/10/17): These trees are not suitable for retention due to health or other structural factors.

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Heritage Tree Preservation for Site Screening
Suitability Analysis - MPK 22 Project Update

10/21/17

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Heritage Tree Preservation for Site Screening
Suitability Analysis - MPK 22 Project Update

10/21/17

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0% (0/10/17): These trees are not suitable for retention due to health or other structural factors.

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FACEBOOK
OWNER
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Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

**HERITAGE TREE POTENTIAL
PRESERVATION
ARBORIST REPORT & MEMO**

Interim Condition

PROJECT: KLASER
2015-007
SCALE: 1" = 20' 0"
DATE: 26-December-2017
DRAWN: 2017-10-17 10:15
REVISED: 2017-10-17 10:15

L1-122

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Tag #	Species	DBH	Height	Structure	Health	Suitability for Reference	Notes	16-04 2016 Inspection Notes
324	<i>Pinus helgolandica</i>	17	30	G	G	G	lean, fine tree	GOOD SUITABILITY
325	<i>Pinus helgolandica</i>	27.5	50	F	F	F	Good tree, well light tree	GOOD SUITABILITY
326	<i>Pinus helgolandica</i>	23	25	F	G	F	FL flower tree	GOOD SUITABILITY
327	<i>Pinus helgolandica</i>	33.5	40	F	G	F	Powerline, lower planting	POOR SUITABILITY, HEALTH
328	<i>Pinus helgolandica</i>	22	30	F	F	F	LDN	POOR SUITABILITY, HEALTH
329	<i>Pinus helgolandica</i>	26.5	35	G	G	G	Good tree, well light	POOR SUITABILITY
330	<i>Pinus helgolandica</i>	18	30	G	F	F	LDN	POOR SUITABILITY, HEALTH
331	<i>Pinus helgolandica</i>	25	60	F	G	F	LDN	FAIR SUITABILITY
332	<i>Acacia melanocorymba</i>	22	20	G	G	F		GOOD
333	<i>Quercus agrifolia</i>	8	30	G	G	G	Substrate for vegetation, New tree	POOR SUITABILITY, HEALTH
334	<i>Acacia melanocorymba</i>	8	30	F	G	F		CODE
335	<i>Quercus agrifolia</i>	8	35	G	G	G	Substrate for vegetation, New tree	GOOD
336	<i>Myoporum laetifolium</i>	5.5	25	F	F	F	Minor dead	NOT SUITABILITY, MINOR DEAD
337	<i>Myoporum laetifolium</i>	7.5	25	G	F	F		NOT SUITABLE, HEALTH
342	<i>Acacia melanocorymba</i>	21	28	F	F	F		POOR SUITABILITY, LEAF FELL
343	<i>Acacia melanocorymba</i>	1.8	46	F	F	F	Minor dead	POOR SUITABILITY, LEAF FELL
344	<i>Acacia melanocorymba</i>	2.8	49	F	F	F	lean	POOR SUITABILITY, LEAF FELL
345	<i>Acacia melanocorymba</i>	3.2	40	F	F	F		POOR SUITABILITY, LEAF FELL
346	<i>Acacia melanocorymba</i>	3.8	38	G	F	F	CB tag	GOOD
347	<i>Acacia melanocorymba</i>	1.1	35	F	F	F	lean	POOR SUITABILITY, LEAF FELL
348	<i>Acacia melanocorymba</i>	8	23	F	F	F	CHL, well light	POOR SUITABILITY, LEAF FELL
349	<i>Acacia melanocorymba</i>	23.5	40	G	F	F		POOR SUITABILITY, LEAF FELL
350	<i>Acacia melanocorymba</i>	23.5	40	F	F	F		POOR SUITABILITY, LEAF FELL
351	<i>Acacia melanocorymba</i>	25.5	40	F	F	F	LDN	POOR SUITABILITY, LEAF FELL
352	<i>Pinus helgolandica</i>	17	45	F	F	F	Minor dead, old tree	NOT SUITABLE, MINOR DEAD
353	<i>Pinus helgolandica</i>	22	40	G	G	G	CB, Substrate	GOOD SUITABILITY
354	<i>Pinus helgolandica</i>	15	40	G	G	G	lean, CB, Substrate	GOOD SUITABILITY
355	<i>Pinus helgolandica</i>	23.5	30	G	G	G	lean	FAIR SUITABILITY, LEAF
356	<i>Acacia melanocorymba</i>	11, 13	30	F	F	F	lean	POOR SUITABILITY, LEAF FELL
357	<i>Acacia melanocorymba</i>	22.5	40	F	F	F	CHL, W	POOR SUITABILITY, LEAF FELL
358	<i>Acacia melanocorymba</i>	12	40	F	F	F	H	POOR SUITABILITY, LEAF FELL
359	<i>Acacia melanocorymba</i>	24.5	45	F	F	F	CD	POOR SUITABILITY, LEAF FELL
360	<i>Myoporum laetifolium</i>	4	30	F	F	F	Minor dead	NOT SUITABLE, HEALTH
361	<i>Acacia melanocorymba</i>	27.5	30	F	F	F	Minor dead	POOR SUITABILITY, HEALTH
362	<i>Acacia melanocorymba</i>	18	45	F	F	F		POOR SUITABILITY, LEAF FELL
363	<i>Acacia melanocorymba</i>	17	30	F	F	F	PP	POOR SUITABILITY, LEAF FELL
364	<i>Acacia melanocorymba</i>	25.5	30	F	F	F	Significant dead, broken branches	POOR SUITABILITY, LEAF FELL
365	<i>Acacia melanocorymba</i>	23	40	F	F	F	PP	POOR SUITABILITY, LEAF FELL
383	<i>Pinus helgolandica</i>	24	25	G	F	F	CH tag K73, lean, PP, CD	FAIR SUITABILITY

Tag #	Species	DBH	Height	Structure	Health	Suitability for Reference	Notes	16-04 2016 Inspection Notes
384	<i>Pinus helgolandica</i>	8	30	G	F	F	Seedling, DB SP	FAIR SUITABILITY
385	<i>Pinus helgolandica</i>	23	45	G	F	F	CH tag K50, CD, DBL, CHL, LEAF, POWERLINE	FAIR SUITABILITY
386	<i>Pinus helgolandica</i>	18.5	25	G	F	F	In canopy of other, CD, H, lean	POOR SUITABILITY
387	<i>Pinus helgolandica</i>	33	25	F	F	F	CH tag K, lean, CD	POOR SUITABILITY
388	<i>Pinus helgolandica</i>	23	30	F	F	F	CH tag K, CD, DB, SP	POOR SUITABILITY, STRUCTURE
417	<i>Pinus helgolandica</i>	24.5	25	G	F	F	CH tag K54, Significant lean, LEAF, POWERLINE	NOT SUITABLE, STRUCTURE
418	<i>Pinus helgolandica</i>	31	30	G	G	G	Significant lean, CD	POOR SUITABILITY, STRUCTURE
419	<i>Pinus helgolandica</i>	23.5	45	G	F	F	CHL, CD	POOR SUITABILITY, STRUCTURE
440	<i>Pinus helgolandica</i>	33	40	F	F	F	CH, CD	POOR SUITABILITY
441	<i>Pinus helgolandica</i>	30.5	25	F	F	F	Callus on tree, CD	FAIR SUITABILITY
442	<i>Pinus helgolandica</i>	23.5	40	G	F	F	CHL, large pruning wounds	POOR SUITABILITY
457	<i>Pinus helgolandica</i>	19	45	G	G	G	CD	GOOD SUITABILITY
458	<i>Pinus helgolandica</i>	24.5	40	F	F	F	CHL, CD	GOOD SUITABILITY
459	<i>Pinus helgolandica</i>	19	30	G	F	F	Significant lean, large pruning wounds, CHL, CD	FAIR SUITABILITY
460	<i>Pinus helgolandica</i>	22	30	G	F	F	CH tag K55, CD, lean, large pruning wounds	POOR SUITABILITY, STRUCTURE
461	<i>Pinus helgolandica</i>	24.5	25	G	F	F	CH tag K56, lean	NOT SUITABLE, LEAF
462	<i>Pinus helgolandica</i>	36.5	25	G	F	F	CD, lean	FAIR SUITABILITY
463	<i>Pinus helgolandica</i>	18	25	F	F	F	Unpruned wounds, Crowded, Significant lean	POOR SUITABILITY, STRUCTURE
464	<i>Pinus helgolandica</i>	23.5	40	G	F	F	Large pruning wounds, New tree	GOOD SUITABILITY
469	<i>Pinus helgolandica</i>	5	25	F	F	F	Significant lean, Crowded, H, lean, large pruning wounds	POOR SUITABILITY, HEALTH
470	<i>Pinus helgolandica</i>	8.5	22	F	F	F	Minor dead, CD, high voltage power lines	FAIR SUITABILITY
471	<i>Pinus radiata</i>	13	30	F	F	F	F, F, lean F	NOT SUITABLE, HEALTH
472	<i>Pinus radiata</i>	11	30	F	F	F		POOR SUITABILITY, STRUCTURE
473	<i>Pinus radiata</i>	10	25	F	F	F	Minor dead	NOT SUITABLE, LEAF & HEALTH
474	<i>Pinus radiata</i>	1	40	F	F	F	lean, CD	NOT SUITABLE, STRUCTURE & HEALTH
475	<i>Pinus radiata</i>	12	40	F	F	F	DB	NOT SUITABLE, HEALTH
476	<i>Pinus radiata</i>	4	25	F	F	F	lean	POOR SUITABILITY, INCREASED LEAF HEALTH
477	<i>Pinus radiata</i>	4	25	F	F	F	CD	POOR SUITABILITY
478	<i>Pinus radiata</i>	5.5	30	F	F	F	large pruning wounds	POOR SUITABILITY
479	<i>Pinus radiata</i>	12.5	40	F	F	F	lean	FAIR SUITABILITY
480	<i>Pinus radiata</i>	12.5	40	F	F	F	lean	NOT SUITABLE, APPEARS TO BE LEAFY
481	<i>Pinus radiata</i>	14	40	F	F	F		FAIR SUITABILITY
482	<i>Pinus radiata</i>	5.5	45	F	F	F	lean, power supply	NOT SUITABLE, HEALTH
483	<i>Pinus radiata</i>	4.5	35	F	F	F	lean	NOT SUITABLE, HEALTH & STRUCTURE
484	<i>Pinus radiata</i>	18	40	F	F	F	Minor dead	NOT SUITABLE, STRUCTURE
485	<i>Myoporum laetifolium</i>	17	30	F	F	F	Minor dead, Termite, Minor dead	NOT SUITABLE
486	<i>Pinus radiata</i>	10	40	F	F	F	DB	FAIR SUITABILITY
487	<i>Myoporum laetifolium</i>	13	30	F	F	F	Termite, CD	NOT SUITABLE

Tag #	Species	DBH	Height	Structure	Health	Suitability for Reference	Notes	16-04 2016 Inspection Notes
488	<i>Myoporum laetifolium</i>	14	20	F	F	F	CD, Termite	NOT SUITABLE
489	<i>Myoporum laetifolium</i>	5.5	20	F	F	F	Termite	NOT SUITABLE
490	<i>Myoporum laetifolium</i>	1.2	25	F	F	F	Termite	NOT SUITABLE
491	<i>Myoporum laetifolium</i>	3.5	25	F	F	F	Termite	NOT SUITABLE
492	<i>Myoporum laetifolium</i>	4	20	F	F	F	Termite, H	NOT SUITABLE
493	<i>Pinus helgolandica</i>	18	30	G	F	F	Significant lean, CD tag	NOT SUITABLE
494	<i>Pinus radiata</i>	11	40	F	F	F	lean	POOR SUITABILITY
495	<i>Pinus helgolandica</i>	23	30	F	F	F	Significant lean, CD tag	NOT SUITABLE, SB DEGREE LEAF
496	<i>Pinus helgolandica</i>	7	20	F	F	F	large pruning wounds	NOT SUITABLE, POOR HEALTH
497	<i>Pinus radiata</i>	12	40	F	F	F	Termite	NOT SUITABLE, MINOR DEAD
498	<i>Pinus radiata</i>	11	40	F	F	F	Termite	GOOD
499	<i>Pinus helgolandica</i>	18	20	F	F	F	Minor dead	NOT SUITABLE
500	<i>Pinus radiata</i>	12.5	40	F	F	F		FAIR
501	<i>Pinus helgolandica</i>	4	20	F	F	F		POOR SUITABILITY
502	<i>Pinus helgolandica</i>	17	40	F	F	F	lean	NOT SUITABLE, TERMITE
503	<i>Pinus helgolandica</i>	4.5	20	F	F	F		NOT SUITABLE
504	<i>Pinus radiata</i>	17.5	45	F	F	F	CH tag K57, CD, lean, F	NOT SUITABLE, SB
505	<i>Pinus radiata</i>	13	25	F	F	F	In canopy, Crowded, CD	GOOD
506	<i>Pinus radiata</i>	14	40	F	F	F	Termite	GOOD
507	<i>Pinus radiata</i>	17	40	F	F	F		FAIR
508	<i>Acacia melanocorymba</i>	9.5	25	G	F	F	lean over parking lot, vehicle damage	FAIR W/ (IN FEELING)
509	<i>Pinus radiata</i>	4	20	F	F	F		NOT SUITABLE
510	<i>Myoporum laetifolium</i>	21.5	25	F	F	F	Minor dead	GOOD
511	<i>Pinus radiata</i>	14	40	F	F	F		FAIR
512	<i>Pinus radiata</i>	38	20	F	F	F	Termite	NOT SUITABLE
513	<i>Myoporum laetifolium</i>	23.5	20	F	F	F	CH tag K73, CD, Termite	NOT SUITABLE
514	<i>Pinus radiata</i>	17	25	F	F	F	CH tag K58, lean, H for high voltage power lines	GOOD
515	<i>Myoporum laetifolium</i>	12	25	F	F	F	Termite, lean, High voltage power lines	NOT SUITABLE
516	<i>Pinus radiata</i>	16	45	F	F	F	large pruning wounds, CD, High voltage power lines	GOOD

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OWNER
17000 1st St, San Francisco, CA 94103

DATE: 10/16/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

HERITAGE TREE POTENTIAL
PRESERVATION DATA

Interim Condition

PROJECT NUMBER
2016-007
SCALE
1" = 20'-0"

28 September 2017
1411 18th Ave, S.F., CA
94116 (415) 774-8770

PROJECT NUMBER
L1-123

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SITE AREAS	
AREA	DESCRIPTION
South Landscape	The INTERIM design of the landscape on the south side of the building will be a temporary landscape with limited improvements and will be modified and finalized as part of the FINAL site construction when the TC site becomes available. The design includes interim egress paths. Permanent landscape improvements will be provided in locations that will remain in place as part of the FINAL construction, with INTERIM improvements in areas that will be modified by the FINAL construction. The terrain will be set at the finish floor of the 1st level at approximately elevation 33.00.
South EVA/ Bike and Pedestrian Route	An INTERIM EVA will be located on the south edge of the terrace within the corridor between MPK 22 and Building 305. The EVA will also serve as the primary east-west bicycle circulation route between MPK 21, 22, 23 and Facebook offices to the west. The EVA is envisioned as a simple asphalt concrete path with markings and limited repair work to the Building 305 courtyard and landscape areas.
Southwest Dining Terrace	The southwest dining terrace will provide a large outdoor dining space and associated shade structure. The terrace will also serve as building entry/exit with a secure entry point. The terrace will include a mix of hardscape (80%) and planting areas (20%). Trees will be located around the shade structure in select locations.
Building Entries	The East Lobby (1) will provide secondary access for bicyclists and pedestrians circulating from MPK 21. The North Lobby (2) will serve as the primary building entry for Campus Shuttle and Trains during the INTERIM condition. North and west entry landscape areas will include a mix of hardscape (70%) and planting areas (30%). Building access, seating areas and bike parking will be included at these locations. The site and landscape improvements in this area will be consistent with MPK 20 and 23 entry areas.
Public Open Space & Access Area	The public open space and access area located west of the EVA between MPK 21 and 22 will include a generous plaza area and provide a flexible and durable surface for community and campus events and gatherings. The INTERIM open space improvements will provide 3.4 acres of the designated 2-acre public access area and its program and design will be consistent with the CDP requirements. Public pedestrian and bicycle circulation to the multi-use bridge is also provided as part of the INTERIM open space improvements.
General Perimeter & Parking Area Landscape	General landscape improvements will be made in areas surrounding the building. The landscape will be similar to the West Campus Site Level landscape improvements at MPK 20 and 21 with low landforms, bio-retention areas, understory plantings, and trees.

PAVING SCHEDULE		
SYMBOL	TYPE	DESCRIPTION
	Asphalt Paving -At all bike & pedestrian pathways	2-inch asphalt concrete, 4-inch Class 2 aggregate base
	Asphalt Paving -EVA (H20 loading 75,000 lbd)	Asphalt Concrete, 3-inch asphalt, 18-inch aggregate base
	Asphalt Unit Paver -Southwest Dining Terrace	3-inch asphalt paver, 6-inch Class 2 aggregate base
	CIP Concrete -North Entry & Building egress paths	Black concrete with medium texture finish, tuckered joints. 6-inch aggregate base course
	Concrete Unit Paver -at Park Plaza	Mix of 4x6, 6x6, 6x6, 8x8 inch precast concrete unit pavers, 1" thick units designed for H20 vehicle loading, sand set on 18-inch Class 2 base. Include 4x4 galvanized steel angle edge anchored to 8x8 inch CIP concrete curb at all edges.
PAVING		
NOTES		
<ol style="list-style-type: none"> The extent of potential site-level gathering and landscape areas are subject to modification. Site level site lighting will be designed to address dark skies with low level lighting where applicable and not out of the path of egress and emergency lighting. All pedestrian paths and egress routes shall meet CBC, Title 24, & ADA Accessibility requirements. 		
LINE TYPES		
SYMBOL	TYPE	
	Public Open Space Interim Phase: 1.4 Acres	
	Public Open Space Interim Phase: Additional 0.2 Acres at B-S EVA	
	RGS Lease Line	
	MPK 21 Interim Limit of Work (L2W)	
	MPK 22 Interim Limit of Work (L2W)	
	TC Site Property Line	

SITE ELEMENTS	
TYPE	DESCRIPTION
Bioretention Area -At perimeter landscape areas	Stormwater treatment area
Bioretention Area -At South terrace landscape edge	Stormwater treatment area: Provide concrete walls to retain slope on East & West sides of BTA's at slope
Stair -At South terrace landscape edge	CIP Concrete stair with metal handrail
Ramp -At East Lobby	CIP Concrete ramp with metal handrail
Bench -At Plaza	Manufactured park bench with steel frame, wood seat, & wood back -Assume BS
Custom Park Bench -At Plaza	Manufactured steel frame with wicker seat & back -Assume 60 LF
Temporary Fire Ramp -At South EVA	Asphalt paving with retaining wall
SITE SECURITY	
TYPE	DESCRIPTION
General Site Security Improvements	<p>The landscape design shall be coordinated with the following site security components:</p> <ol style="list-style-type: none"> Public Access Area (over-crack cased): 8ft / vinyl coated chain-link / 1" mesh / black Site Perimeter Fence and vehicle barrier @ south property line (Crack tested): 8ft / vinyl coated chain-link / 1" mesh / black Vehicle Access Control Bollards Pedestrian Access points/Gates Guard Stations

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FACEBOOK
OWNER
1700001 001
MOUNTAIN VIEW, CALIFORNIA 94039

DATE: 10/26/2017

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROPOSED LANDSCAPE
MATERIALS & SITE ELEMENTS
SCHEDULE
Interim Condition

PROJECT NUMBER
2015-007
SCALE
1" = 20'-0"

26-December 2017
1717 0001 001 010
ISSUED: 2017 001 24 - 26"

SHEET NUMBER

L1-200

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 FACEBOOK
 OWNER
 1 FORTRESS WAY, COLUMBIA, MARYLAND

DATE: 10/20/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
 PROPOSED LANDSCAPE
 PLAN
 Interim Condition

PROJECT NUMBER
 2015-027
 SCALE
 1" = 25' 0"
 26-December 2017
 THIS SHEET NO. 115
 TOTAL SHEETS 24 + 24*

SHEET NUMBER
 L1-202
 DESIGN PARTNER: LP



INTERIM PUBLIC OPEN SPACE LEGEND	
SYMBOL	DESCRIPTION
	Public Open Space Interim Phase: 2.4 Acres
	Public Open Space Interim Phase: Additional 0.2 Acres at N-S Park
	EOS Lease Line
	MPX 23 Limit of Work (LOW)
	MPX 22 Interim Limit of Work (LOW)
	FE Site Property Line
	MPX 21
	MPX 22
	Building 101
	Chico Street
	Chico Entry: 1) Public bike and pedestrian access point to public open space and Bayfront Bridge 2) Fire access
	Bayfront Bridge
	Plaza
	3-way guide bike lane connecting Chico Street to Bayfront Bridge
	Lawn
	Parking Area
	Restoration Area

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LANDSCAPE ARCHITECTS
400 South 10th Street, Suite 200
CITY OF DENVER

FACEBOOK
OWNER
1700 15th Street, Suite 1000
DENVER, COLORADO 80202

DATE: 10/20/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TITLE
PUBLIC OPEN SPACE
ENLARGEMENT PLAN

Interim Condition

PROJECT NUMBER
2015-007
SCALE
1" = 20' 0"
DATE
26-December 2017
DRAWN BY: J. S. S.
CHECKED BY: J. S. S.

SHEET NUMBER
L1-300
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GENERAL KEY	
LINE TYPES	
	Trees to be removed
	BEES Line
	MPK 21 Limit of Work (LOW)
	TE Site Property Line
NOTE: All proposed tree removals are required as a result of construction impacts.	

2016 APPROVED PROJECT				
BUILDING 301-309 SUMMARY -- PER FALL 2016 APPROVED EIR				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
549		GOOD health	2:1	258
60		FAIR-POOR health	1:1	60
19		DEAD health	1:1	19
274 TOTAL				423 TOTAL
Tree Removal & Required Replacement Values: NON-HERITAGE				
496		See tree survey data	NA	NA

BUILDING 307-309 SUMMARY (MPK 21 SITE) -- ALREADY REMOVED PER FALL 2016				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
22		GOOD health	2:1	44
18		FAIR-POOR health	1:1	18
8		DEAD health	1:1	8
48 TOTAL				70 TOTAL
Tree Removal & Required Replacement Values: NON-HERITAGE				
307		See tree survey data	NA	NA

2017 PROPOSED REVISED PROJECT				
BUILDING 301-306 SUMMARY -- EXCLUDING MPK 21 REMOVED TREES				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
127		GOOD health	2:1	254
48		FAIR-POOR health	1:1	48
51		DEAD health	1:1	51
226 TOTAL				353 TOTAL
Tree Removal & Required Replacement Values: NON-HERITAGE				
189		See tree survey data	NA	NA

MPK 22 INTERIM PHASE				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
85		GOOD health	2:1	172
29		FAIR-POOR health	1:1	29
18		GOOD health	1:1	18
148 TOTAL				234 TOTAL
Tree Removal & Required Replacement Values: NON-HERITAGE				
100		See tree survey data	NA	NA

MPK 22 FINAL PHASE				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
37		GOOD health	2:1	74
17		FAIR-POOR health	1:1	17
4		DEAD health	1:1	4
58 TOTAL				95 TOTAL
Tree Removal & Required Replacement Values: NON-HERITAGE				
23		See tree survey data	NA	NA

2017 PROPOSED REVISED PROJECT, CONTINUED				
MPK 22 Hotel Site				
REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
4		GOOD health	2:1	8
2		FAIR-POOR health	1:1	2
14		DEAD health	1:1	14
20 TOTAL				24 TOTAL
Tree Removal & Required Replacement Values: NON-HERITAGE				
65		See tree survey data	NA	NA

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FACEBOOK
OWNER
1700 West 14th, San Francisco, CA 94115

DATE: 10/27/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROJECT NAME
HERITAGE TREE DISPOSITION
SCHEDULE

Final Condition

PROJECT NUMBER
2016-007
SCALE
1/8" = 1'-0"

26-September 2017
THIS SHEET IS ONE OF 10 SHEETS IN SET 24-100

SHEET NUMBER
L2-100

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DATE: 10/20/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
HERITAGE TREE DISPOSITION
OVERALL SITE PLAN

Final Condition

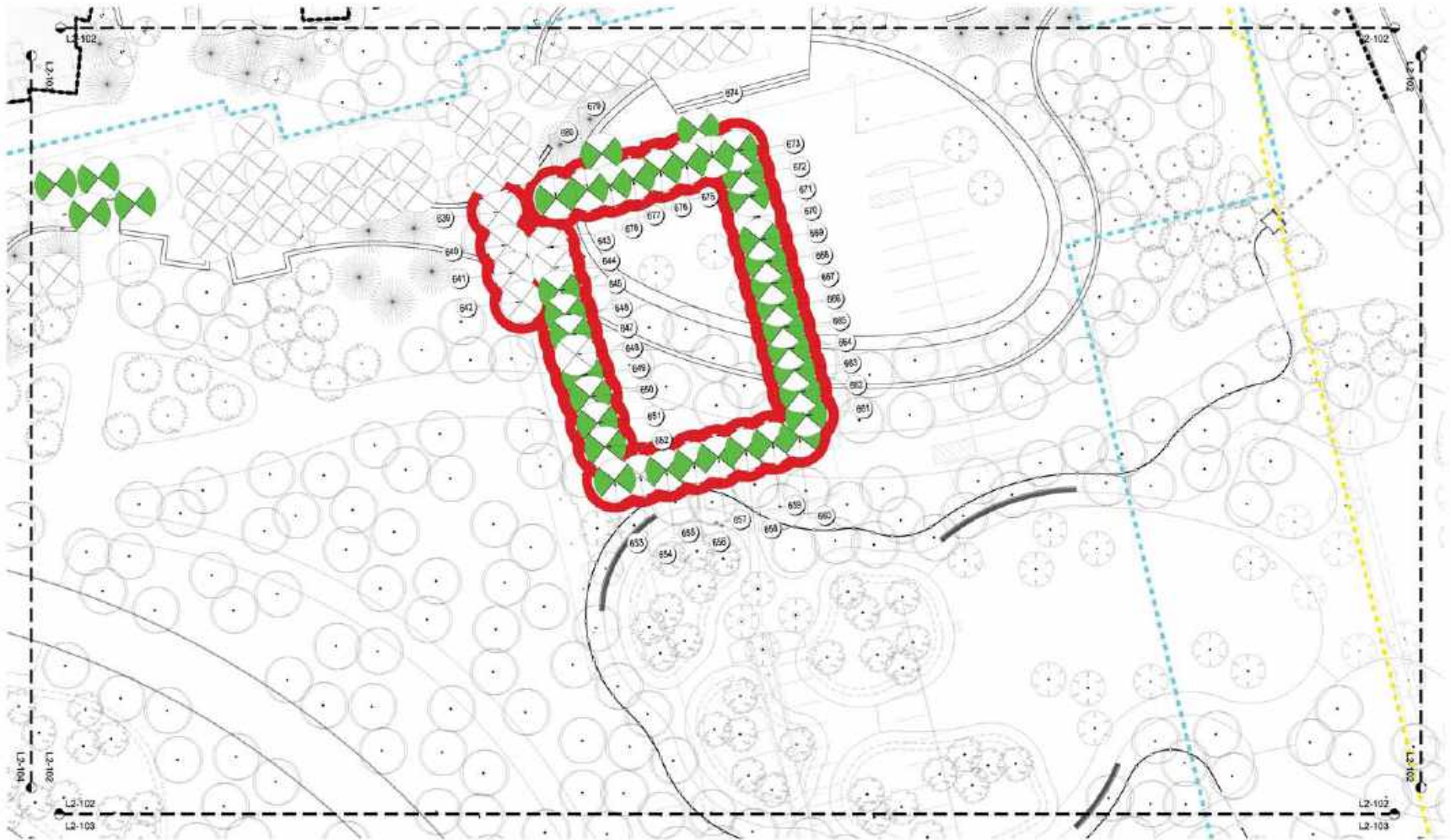
PROJECT NUMBER
 2015-027
 SCALE
 1" = 25' 0"

26-December 2017
 THIS SHEET IS ONE OF 16
 SHEETS TOTAL: 16 OF 16

DATE: 10/20/17

L2-101

DATE: 10/20/17



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1700 West 15th, Menlo Park, California 94023

DATE: 10/26/17

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300-309 Constitution Drive

PROJECT NAME
HERITAGE TREE DISPOSITION
ENLARGEMENT PLAN

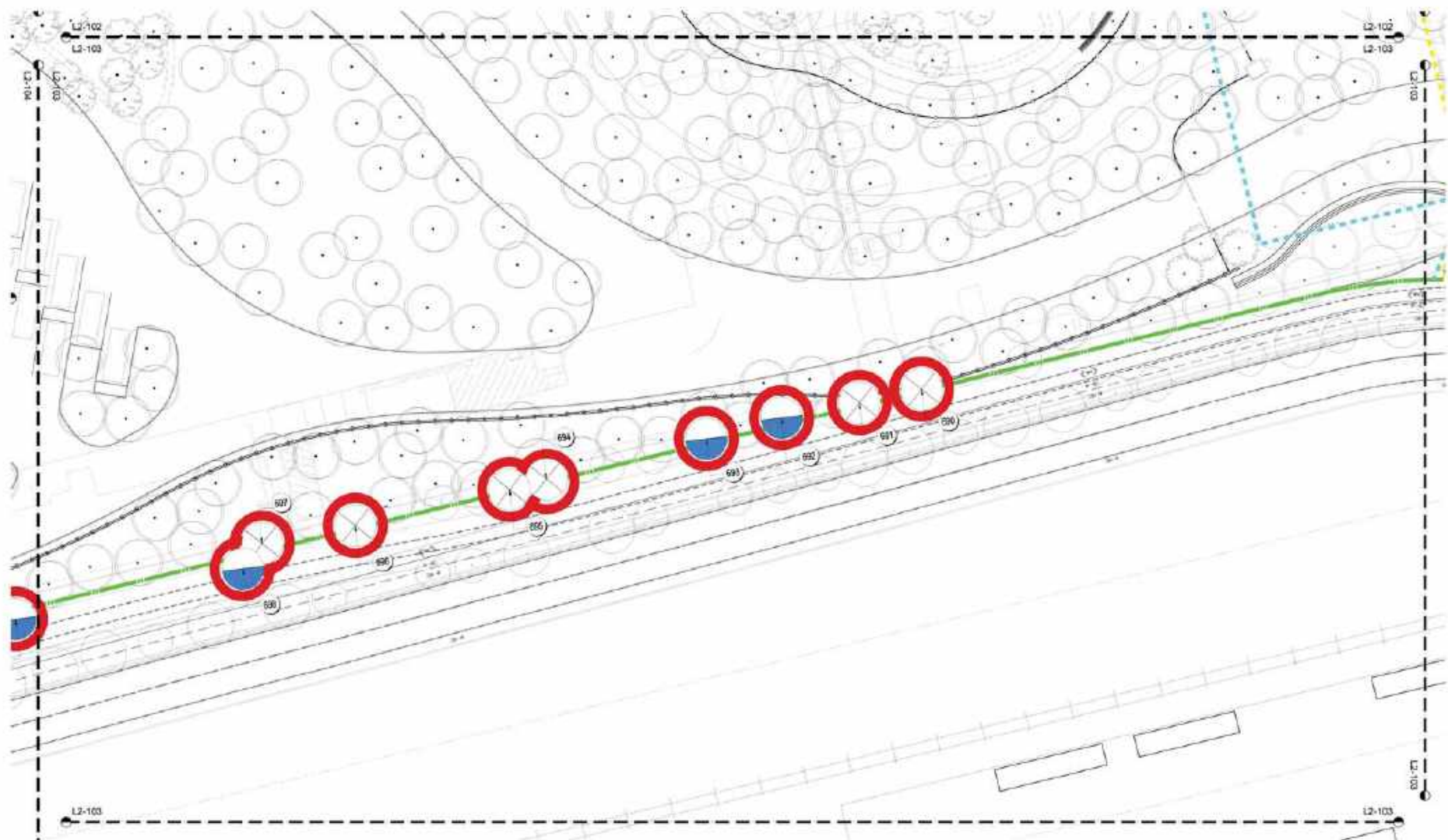
Final Condition

PROJECT NUMBER
2015-007
SCALE
1" = 20' 0"
DATE
26-December 2017
DRAWN BY
MELISSA WOOD, M.S.
CHECKED BY
JAMES WOOD, M.S.

SHEET NUMBER

L2-102

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FACEBOOK
 OWNER
 1700 WEST 15th, SAN FRANCISCO, CALIFORNIA 94116

DATE: 10/17/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
**HERITAGE TREE DISPOSITION
 ENLARGEMENT PLAN**

Final Condition

PROJECT NUMBER
 2015-027
 SCALE
 1" = 20' 0"
 DATE
 26-September 2017
 DRAWN BY
 JAMES W. WILSON, AIA
 CHECKED BY
 JAMES W. WILSON, AIA

SHEET NUMBER
L2-103
 SHEET TOTALS
 1 of 1



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DATE: 10/26/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
 HERITAGE TREE DISPOSITION
 ENLARGEMENT PLAN

Final Condition

PROJECT NUMBER
 2015-007
 SCALE
 1" = 20' 0"
 DATE
 26-September 2017
 DRAWN BY
 M.S.
 CHECKED BY
 J.C.

SHEET NUMBER

L2-104

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DATE: 10/26/17

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 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
 HERITAGE TREE DISPOSITION
 ENLARGEMENT PLAN

Final Condition

PROJECT NUMBER
 2015-027
 SCALE
 1" = 20' 0"
 DATE
 26-December 2017
 THIS SHEET NO. 115
 SHEET TOTAL NO. 24 + 24*

SHEET NUMBER
 L2-105
 DESIGN PARTNER: CMG



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OWNER
1 FORTRESS WAY
MENLO PARK, CALIFORNIA 94025

DATE: 10/26/17

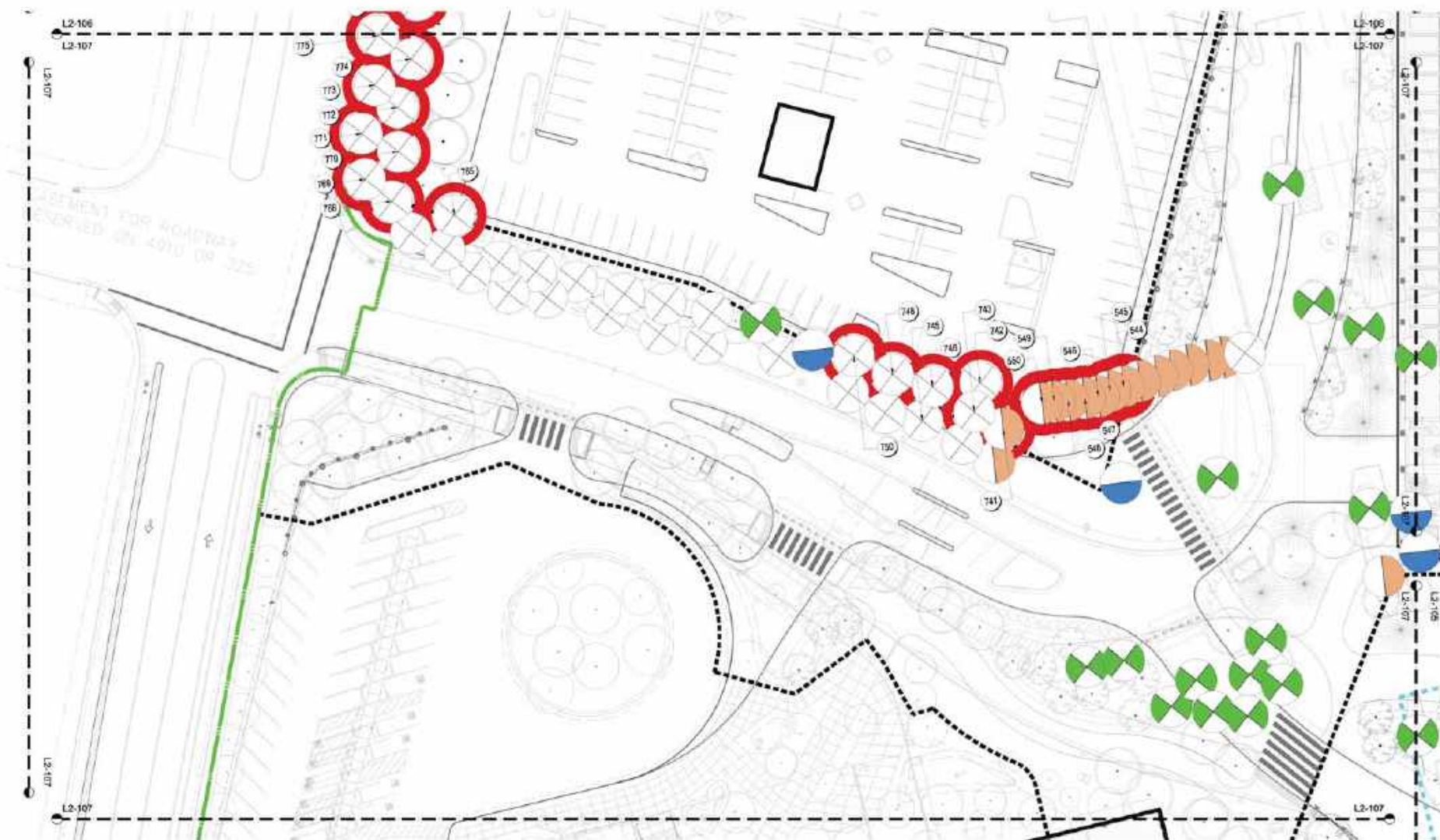
Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROJECT: HERITAGE TREE DISPOSITION
ENLARGEMENT PLAN

Final Condition

PROJECT NUMBER: 2015-027
SCALE: 1" = 20' 0"
DATE: 26-December 2017
THIS SHEET IS ONE OF 10 SHEETS TOTAL: 24 x 36"

SHEET NUMBER: L2-106
DESIGNED BY: [Name]



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 FACEBOOK
 OWNER
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DATE: 10/26/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
 HERITAGE TREE DISPOSITION
 ENLARGEMENT PLAN
 Final Condition

PROJECT NUMBER
 2015-027
 SCALE
 1" = 20' 0"
 26-September 2017
 THIS SHEET NO. 115
 SHEET 0227 OF 24 + 24"

SHEET NUMBER
 L2-107
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SBCA TREE CONSULTING

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Fax (209) 947-5083
Website: www.sbcacorp.com

Shari Baskin, Consulting Arborist
MS ISA Certified Arborist #222
SBCA Certified Urban Forester #134
ISA Conservation Specialist #99 82669
E-mail: shari@sbcacorp.com

Shari Baskin, Consulting Arborist
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SBCA Certified Urban Forester #134
ISA Conservation Specialist #99 82669
E-mail: shari@sbcacorp.com

Date: March 22, 2017
To: Shari Baskin, SBCA
Subject: Tree Survey
Location: 300-309 Constitution Drive

Assignment: Assess tree condition and quality of trees located on site, and City trees along Edison Ave.

City of Mendocino Districts

Conditions of Heritage Tree:

- 1. Any tree having a trunk with a circumference at 4.5 inches diameter at 4.5 inches or more measured at 6 inches above the ground.
- 2. Any oak tree native to California, with a circumference of 24 inches or more measured at 4.5 inches diameter or greater.
- 3. Any tree or group of trees specifically designated by the City Council for protection because of its historical significance, special character or scientific interest.
- 4. Any tree with more than one trunk measured at the point where the trunks divide, with a circumference of 4.5 inches diameter at 4.5 inches or more, with a maximum of one inch diameter between 12 feet or height, which is exempt from the ordinance.

Summary:

- **SURVEY RESULTS:** The tree survey recorded information on seven hundred seventy (770) trees located on the grounds of 300-309 Constitution Drive and along the west end of Edison St. Meter number tags were attached to all trees. Data was taken on Tree Size, Health and Structural Condition, Suitability for Retention, and Potential Uses.
- **Tree Condition:** Seven hundred seventy (770) trees surveyed qualify as "Heritage Trees".
- **Tree Size (DBH):** Efficient samples were noted in the survey. The greatest DBH measured on-site includes London Plane (Platanus x Magnolia) with one hundred twenty-two (122) centimeters.

http://www.mendocino.ca.gov/heritage-trees

300-309 Constitution Drive Tree Survey
Date: 3/22/17

Surveyed: Seven (7) trees located on the grounds of 300-309 Constitution Drive and along the west end of Edison Street with only eight (8) and Silver Sucker Gum (Magnolia adnematocarpa) fifty-four (54) specimens.

- Twenty-two (22) trees surveyed were dead, most on London Plane located along the southern property line. One (1) specimen is due for "trimming".
- Trees given a "Poor" suitability for retention are located near health facility and existing parking lot/entrance, and for poor post-pruning/offer site area with poor tree placement. Good conditions are concerned timing and the root cause of poor performance.

Summary of Tree Species

Table on following page provides information on the tree species surveyed and the number qualifying as Heritage Trees, with suitability for retention and potential uses. The survey data is provided in Appendix E.

Species	Common Name	Amount	Percent of Total	Suitability for Retention	Notes	
1	Amelanchier	1	0.1	0	0	
2	Azalea	1	0.1	0	0	Plants ignored
3	Banksia	3	0.4	0	0	Site location
4	Blackberry	1	0.1	0	0	Location along sidewalk
5	Blackberry	1	0.1	0	0	Location along sidewalk
6	Blackberry	1	0.1	0	0	Location along sidewalk
7	Blackberry	1	0.1	0	0	Location along sidewalk
8	Blackberry	1	0.1	0	0	Location along sidewalk
9	Blackberry	1	0.1	0	0	Location along sidewalk
10	Blackberry	1	0.1	0	0	Location along sidewalk
11	Blackberry	1	0.1	0	0	Location along sidewalk
12	Blackberry	1	0.1	0	0	Location along sidewalk
13	Blackberry	1	0.1	0	0	Location along sidewalk
14	Blackberry	1	0.1	0	0	Location along sidewalk
15	Blackberry	1	0.1	0	0	Location along sidewalk
16	Blackberry	1	0.1	0	0	Location along sidewalk
17	Blackberry	1	0.1	0	0	Location along sidewalk
18	Blackberry	1	0.1	0	0	Location along sidewalk
19	Blackberry	1	0.1	0	0	Location along sidewalk
20	Blackberry	1	0.1	0	0	Location along sidewalk

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Phone: (209) 947-5078
Fax: (209) 947-5083
Website: www.sbcacorp.com

300-309 Constitution Drive Tree Survey
Date: 3/22/17

Species	Common Name	Amount	Percent of Total	Suitability for Retention	Notes	
22	Blackberry	1	0.1	0	0	Location along sidewalk
23	Blackberry	1	0.1	0	0	Location along sidewalk
24	Blackberry	1	0.1	0	0	Location along sidewalk
25	Blackberry	1	0.1	0	0	Location along sidewalk
26	Blackberry	1	0.1	0	0	Location along sidewalk
27	Blackberry	1	0.1	0	0	Location along sidewalk
28	Blackberry	1	0.1	0	0	Location along sidewalk
29	Blackberry	1	0.1	0	0	Location along sidewalk
30	Blackberry	1	0.1	0	0	Location along sidewalk
31	Blackberry	1	0.1	0	0	Location along sidewalk
32	Blackberry	1	0.1	0	0	Location along sidewalk
33	Blackberry	1	0.1	0	0	Location along sidewalk
34	Blackberry	1	0.1	0	0	Location along sidewalk
35	Blackberry	1	0.1	0	0	Location along sidewalk
36	Blackberry	1	0.1	0	0	Location along sidewalk
37	Blackberry	1	0.1	0	0	Location along sidewalk
38	Blackberry	1	0.1	0	0	Location along sidewalk
39	Blackberry	1	0.1	0	0	Location along sidewalk
40	Blackberry	1	0.1	0	0	Location along sidewalk
41	Blackberry	1	0.1	0	0	Location along sidewalk
42	Blackberry	1	0.1	0	0	Location along sidewalk
43	Blackberry	1	0.1	0	0	Location along sidewalk
44	Blackberry	1	0.1	0	0	Location along sidewalk
45	Blackberry	1	0.1	0	0	Location along sidewalk
46	Blackberry	1	0.1	0	0	Location along sidewalk
47	Blackberry	1	0.1	0	0	Location along sidewalk
48	Blackberry	1	0.1	0	0	Location along sidewalk
49	Blackberry	1	0.1	0	0	Location along sidewalk
50	Blackberry	1	0.1	0	0	Location along sidewalk

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Fax: (209) 947-5083
Website: www.sbcacorp.com

300-309 Constitution Drive Tree Survey
Date: 3/22/17

Species	Common Name	Amount	Percent of Total	Suitability for Retention	Notes	
22	Blackberry	1	0.1	0	0	Location along sidewalk
23	Blackberry	1	0.1	0	0	Location along sidewalk
24	Blackberry	1	0.1	0	0	Location along sidewalk
25	Blackberry	1	0.1	0	0	Location along sidewalk
26	Blackberry	1	0.1	0	0	Location along sidewalk
27	Blackberry	1	0.1	0	0	Location along sidewalk
28	Blackberry	1	0.1	0	0	Location along sidewalk
29	Blackberry	1	0.1	0	0	Location along sidewalk
30	Blackberry	1	0.1	0	0	Location along sidewalk
31	Blackberry	1	0.1	0	0	Location along sidewalk
32	Blackberry	1	0.1	0	0	Location along sidewalk
33	Blackberry	1	0.1	0	0	Location along sidewalk
34	Blackberry	1	0.1	0	0	Location along sidewalk
35	Blackberry	1	0.1	0	0	Location along sidewalk
36	Blackberry	1	0.1	0	0	Location along sidewalk
37	Blackberry	1	0.1	0	0	Location along sidewalk
38	Blackberry	1	0.1	0	0	Location along sidewalk
39	Blackberry	1	0.1	0	0	Location along sidewalk
40	Blackberry	1	0.1	0	0	Location along sidewalk
41	Blackberry	1	0.1	0	0	Location along sidewalk
42	Blackberry	1	0.1	0	0	Location along sidewalk
43	Blackberry	1	0.1	0	0	Location along sidewalk
44	Blackberry	1	0.1	0	0	Location along sidewalk
45	Blackberry	1	0.1	0	0	Location along sidewalk
46	Blackberry	1	0.1	0	0	Location along sidewalk
47	Blackberry	1	0.1	0	0	Location along sidewalk
48	Blackberry	1	0.1	0	0	Location along sidewalk
49	Blackberry	1	0.1	0	0	Location along sidewalk
50	Blackberry	1	0.1	0	0	Location along sidewalk

End Report

Appendix

1. Tree Survey Data

Submitted by:
Shari Baskin

Shari Baskin, Consulting Arborist
MS ISA Certified Arborist #222
SBCA Certified Urban Forester #134
ISA Conservation Specialist #99 82669
E-mail: shari@sbcacorp.com

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(503) 460-7025

FACEBOOK
OWNER
17000 15th Street, Emeryville, CA 94608

DATE: 3/22/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROJECT: ARBORIST
2015-2017
SCALE: 1" = 20' 0"

ARBORIST REPORT
March 2016

MPK 21, Interim, & Final Phases

PROJECT: ARBORIST
2015-2017
SCALE: 1" = 20' 0"

26-December 2017
1717 84th Street, N.E.
Bellevue, WA 98008-3277

SHEET NUMBER
L2-108
Baker Partnership LP

SITE AREAS	
AREA	DESCRIPTION
South Terrace Landscape	Includes relocation of the INTERIM FYN and modification of the landscape area on the south side of the building to create a series of outdoor rooms and an amphitheater/event lawn and site security improvements. The terrace will be set at the finish floor of the 1st level at approximately elevation 13.00 and will include a wall and guardrail at the edge of the terrace to provide an initial security perimeter between the outdoor areas and the larger park area to the South. Two large shade structures are provided at the central entry and amphitheater.
Campus Drive & Transit Center	The campus drive will be extended to complete a loop on the south side of MPK 22 and will include a transit center to accommodate shuttle and train drop-off/pick-up and bike parking areas.
East Lobby	The TE site truck route on the east side of MPK 22 will be removed and the East Lobby will be reconfigured to create a more generous entry area and modified EvA access.
General Park, Perimeter and Parking Area Landscapes	General landscape improvements in areas surrounding bus changing station and the . The landscape will be similar to the West Campus Site Level landscape improvements at MPK 20 and 21 with low landforms, bio-retention areas, understory plantings, and trees. General requirements are outlined below.
Public Open Space & Access Area	The Public Access area will be expanded west to include the 2.1 acre area reserved in the CDP. The improvements will include a stormwater treatment area a walking path and seating areas and will be enclosed with an 8-foot security fence. New bicycle and pedestrian routes and associated landscape areas will connect MPK 21, 22 and 23.
Union Street Phase 4B Improvements	Separation of pedestrian and bicycle paths and landscape improvements within the street right of way.

PAVING SCHEDULE		
SYMBOL	TYPE	DESCRIPTION
	Asphalt Paving -At all bike & pedestrian pathways	2" inch asphalt concrete, 6-inch Class 2 aggregate base
	Asphalt Paving -EVA (H20 loading 75,000 lbf)	Asphalt Concrete, 3-inch asphalt, 18-inch aggregate base
	Asphalt Unit Paver -Southwest Dining Terrace	3-inch asphalt paver, 6-inch Class 2 aggregate base
	C/P Concrete -At building egress paths	Flow concrete with medium texture finish, tuckered joints. 6-inch aggregate base course
	Concrete Unit Paver -at Park Plaza	8x8 of 4x6, 6x6, 6x6, 8x8 inch precast concrete unit pavers, 1" thick units designed for 100 vehicle loading, sand set on 18 inch Class 2 base. Include 4x4 galvanized steel angle edge anchored to 8x8 inch C/P concrete curb at all edges.
PAVING		
NOTES		
<ol style="list-style-type: none"> The extent of potential site level parking and landscape areas are subject to modification. Site level site lighting will be designed to address dark views with low level lighting where applicable and follow-off the path of egress and emergency lighting. All pedestrian paths and egress routes shall meet CBC, Title 24, & ADA Accessibility requirements. 		
LINE TYPES		
SYMBOL	TYPE	
	Public Open Space Interim Phase: LA Acre	
	Public Open Space Interim Phase: Additional 0.2 Acres at N-3/EVA	
	BNSF Lease Line	
	MPK 21 Limit of Work (LOW)	
	MPK 22 Interim Limit of Work (LOW)	
	TE Site Property Line	

SITE ELEMENTS	
TYPE	DESCRIPTION
Bioretention Area -At perimeter landscape areas	Stormwater treatment area
Seat Wall Type 1 -At South Terrace Outdoor Rooms	<ol style="list-style-type: none"> Length per drawing Custom, Reclaimed timber bench base: 16-inches high x 74-inches wide Reclaimed timber bench back: 12-inches high by 12-inches wide milled to a tapered section.
Seat Wall Type 2 -At South Terrace Amphitheater	<ol style="list-style-type: none"> 1500 ft 3-foot wide, 2-foot depth Recycled concrete units - approximate 8-foot in length Sandblast finish Set on aggregate base with setting bed.
SITE SECURITY	
TYPE	DESCRIPTION
General Site Security Improvements	<p>The landscape design shall be coordinated with the following site security components:</p> <ol style="list-style-type: none"> Public Access Area (Open mesh rated): 8 1/2" vinyl coated chain link / 1" mesh / Black Site Perimeter Fencing and vehicle barrier @ South property line (Crash-rated): 8 1/2" vinyl coated chain link / 1" mesh / Black Vehicle Access Control Bollards Pedestrian Access points/Gates Guard Stations
South Terrace Edge	<p>The south terrace is secured by a crash-rated perimeter wall:</p> <ol style="list-style-type: none"> C/P Concrete Wall: 8" C/P concrete wall with corrugated steel attached to face Fire Access Gates: Custom Steel Frames, with non climbable mesh infill panels.

CWG
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OWNER
17000 N. 17th St., Colorado Springs, CO

DATE: 10/27/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROPOSED LANDSCAPE
MATERIALS & SITE ELEMENTS
SCHEDULE

Final Condition

PROJECT NUMBER
2015-007
SCALE
1" = 20'-0"
26-December 2017
THIS SHEET IS ONE OF 10
ISSUED UNDER THE 24' x 36'

SHEET NUMBER
L2-200
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UNDERSTORY PLANTING SCHEDULE			
SYMBOL	SIZE	%	SPACING
Coastal Sage Scrub Low water use, native & adaptive shrubs, perennial, vines, and groundcovers planted upon undulating topography. The mix will be approximately even/50% and naturalistic in character.			
1 Gal	25%	12' o.c.	
1 Gal	50%	24' o.c.	
5 Gal	25%	36' o.c.	
<i>Arctostaphylos sp.</i> (Manzanita) <i>Artemisia californica</i> (California Sage) <i>Baccharis pilularis</i> 'Emerald Carpet' (Coyote Bush) <i>Conoclinium 'Ivory Star'</i> (Ivory Star Conoclinium) <i>Conoclinium Lower Coulter'</i> (Lower Coulter Conoclinium) <i>Elysiacum carsoni</i> 'Calefax' (California Fuchsia) <i>Engelmann glaucus</i> 'Wayne Redstart' (Wayne Redstart Daisy) <i>Halenia arbutifolia</i> (Toyon) <i>Rosaedia pulchra</i> (Purple Needle Grass) <i>Pentstemon gemmifidus</i> (General's Northern Beardtongue) <i>Rhamnus californica</i> (Calforniana) <i>Santalum clavatum</i> 'Alice Chiversing' (Sage)			
Riparian/ETA A mix of native riparian and adapted species selected for bank-stabilization and soil moisture levels including eroded bank-stabilization and low riparian site along the dry stream.			
1 Gal	50%	12' o.c.	
5 Gal	25%	36' o.c.	
<i>Achillea millefolium</i> (Common Yarrow) <i>Carex proserpinacoides</i> (California Flew Sage) <i>Amnis affinis</i> (Common Rush) <i>Mimulus guttatus</i> (Monkey Flower)			
Lawn High traffic use, low water use			
Soil	100%	-	
<i>Desert Tall Fescue</i> (Soil) (Lawn)			
South Terrace Low water use, native & adaptive shrubs, perennial, vines, and groundcovers planted upon undulating topography. The mix will be approximately even/50% and naturalistic in character.			
1 Gal	30%	12' o.c.	
1 Gal	20%	24' o.c.	
5 Gal	20%	36' o.c.	
5 Gal	20%	48' o.c.	
15 Gal	10%	60' o.c.	
<i>Artemisia californica</i> (California Sage) <i>Baccharis pilularis</i> 'Emerald Carpet' (Coyote Bush) <i>Rhamnus californica</i> (Calforniana) <i>Santalum clavatum</i> 'Alice Chiversing' (Sage)			
Perennial Garden			
1 Gal	25%	12' o.c.	
1 Gal	25%	24' o.c.	
Dead PM	50%	12'-18' o.c.	
<i>Achillea 'Ternacetyl'</i> (Hybrid Yarrow) <i>Euphorbia lambei</i> (Tree Euphorbia) <i>Pentstemon 'Winged'</i> (Winged Pentstemon) <i>Pheasant 'Dwarf'</i> (Hybrid Blue Zelandia Flax) <i>Saxifraga rosacea</i> 'Lemon Queen' (Saxifrage Lavender Queen)			

TREE PLANTING SCHEDULE				
SYMBOL	BOX SIZE	QUANTITY	% OF MIX	SPACING
Mixed Oak Woodland				
1	36" Dia	17	03%	18'-40'
	48" Dia	168	30%	
	60" Dia	173	32%	
	72" Dia	107	19%	
	84" Dia	73	13%	
	96" Dia	17	03%	
Total		562		
<i>Acer macrophylla</i> (Big Leaf Maple)				
<i>Aspalathus californica</i> (California Dudleya)				
<i>Halepistis arbutifolia</i> (Toyon)				
<i>Laportea juncea</i> (Dudleya)				
<i>Quercus agrifolia</i> (Coast Live Oak)				
<i>Quercus ilex</i> (Holly Oak)				
<i>Quercus virginiana</i> (Southern Live Oak)				
ETA / Stormwater				
1	24" Dia	102	100%	18'-20'
Total		102		
<i>Alnus incana</i> (White Alder)				
<i>Myrica californica</i> (Pacific Yew Myrica)				
<i>Populus fremontii</i> (Fremont Poplar)				
<i>Salix sp.</i> (Native Willow)				
Redwood				
1	48" Dia	8	15%	18'-22'
	60" Dia	14	24%	
	72" Dia	13	23%	
	84" Dia	14	24%	
	96" Dia	8	15%	
Total		57		
<i>Sequoia sempervirens</i> (Redwood)				
South Terrace - Flowering				
1	36" Dia	17	30%	18'-20'
	48" Dia	29	35%	
	60" Dia	10	18%	
	72" Dia	8	10%	
Total		64		
<i>Cercis canadensis 'Imperial'</i> (Flowering Redbud)				
<i>Cornus Edulis White</i> (Dogwood)				
<i>Magnolia x aztecangama</i> (Sweet Magnolia)				
Seasonal Groves				
1	36" Dia	4	9%	18'-20'
	48" Dia	32	35%	
	60" Dia	28	32%	
	72" Dia	24	27%	
Total		88		
<i>Gingko biloba</i> (Gingko)				
<i>Keteleeria pinnatifida</i> (Golden Bell Tree)				
<i>Platanus chinensis</i> (Chinese Platane)				
Public Open Space - Elm/Zelkova				
1	72" Dia	11	49%	18'-40'
	84" Dia	12	51%	
Total		23		
<i>Zelkova serrata 'Italia'</i> (Zelkova)				
<i>Ulmus parvifolia</i> (Common Elm)				
Total FINAL Site Level Trees		950		

UNDERSTORY PLANTING NOTES	
1.	GENERAL DESIGN INTENT: PROVIDE A DENSELY PLANTED AND ATTRACTIVE LANDSCAPE THAT IS NATURALISTIC AND ECOLOGICALLY PREFERRED LANDSCAPE.
2.	FINAL PLANT SELECTION AND LAYOUT SHALL BE REVIEWED BASED ON THE FOLLOWING SELECTION CRITERIA: A) SOILS AND HORTICULTURAL SUITABILITY B) SALT AND WIND TOLERANCE C) WATER EFFICIENT LANDSCAPE ORDINANCE (WELO) REQUIREMENTS D) AESTHETIC QUALITY E) ECOLOGICAL VALUE/HABITAT AND ENVIRONMENTAL CONSIDERATIONS, INCLUDING ADJACENT SALT FLAT HABITAT AND RAPTOR CONSIDERATIONS F) SUN & SHADE
TREE PLANTING NOTES	
1.	All site trees shall have sub-storage per typical planting details unless otherwise noted by the Landscape Architect.
HERITAGE TREE REPLACEMENT NOTES	
1.	Minimum box size for heritage tree replacement: 24" dia
2.	Due to planting of this project, some trees that are planted in the interim phase will require removal due to proposed development in the Final phase. These quantities are listed in the schedule below.
3.	Existing trees for screening: Trees along the Eastport edge of the MP#22 project site were analyzed to determine whether any could possibly be retained for screening of the site & existing POSE towers. Fifteen (15) trees (heritage & non-heritage) were identified as possible candidates for retention based on current health and condition. The existing root locations were marked on the proposed plans to show where conflicts with the proposed road exist. See sheet L-100. These fifteen (15) trees fall within proposed landscape areas where the site will be less disturbed during construction. Final determination for retention will be evaluated as the project develops.
4.	Heritage tree removal plans will be submitted with individual project Grading Permit applications.
5.	As design for the project evolves, there may be adjustments to the proposed tree counts. Proposed tree plans will be submitted with individual project documentation as part of the Core & Shell Permit applications.
TREE SELECTION CRITERIA	
The majority of species specified for the project are broad-leafy trees that generally do not exceed 10 feet in height, particularly in urban landscape conditions. This is particularly true of trees located along the North edge of the property adjacent to Bayfront Drive and adjacent to the salt flat habitat.	
The retained trees located to the South of the garage and office building are approximately 500 feet from the adjacent salt flats. As they are planted at site level, the line of site and distance to the salt flats reduces the likelihood of vapor production from this location.	
It is important to note that the same requirement was applied to both MP#20 & 22 projects. As part of the permit review and approval process, Scott Tarril, a Senior Ornithologist with Mt. Harvey & Associates, reviewed the project plans and species and provided a memo regarding compliance with the above requirements. The points for MP# 22 interim and final phases are necessary the same and with respect to the redwood trees discussed above, none of the proposed species are large, upright or perennial trees that would provide a riparian patch.	
REQUIRED HERITAGE TREE REPLACEMENTS	
PHASE	QUANTITY
Interim	234
Final	110
Total	353
PROPOSED HERITAGE TREE REPLACEMENTS	
PHASE	QUANTITY
Interim	104
Final	90
Total Interim + Final Site Level Trees	1324

IRRIGATION NOTES	
The landscape irrigation system shall be a fully automated water efficient system and designed to comply with the City of Menlo Park Water Efficient Landscape Ordinance. The system shall be integrated with the on-site wastewater treatment system while allowing for potential cut-over to a City public supply in the event that the wastewater treatment system goes off-line for an extended period. The system shall be a 2 zone system. The system shall be controlled by baseline central controllers which shall be connected to the central control building, and to the internet via a cellular connection. The system shall include the following:	
<ol style="list-style-type: none"> 1. All piping and components shall be purple pipe and compliant for use of recycled water. 2. Primary irrigation for all planting areas will be sub-surface drip or bubbler based irrigation, with air tubes at 1/4 inches in diameter. 3. Secondary irrigation at select planting areas. 4. Spray or mist irrigation in large grass or turf areas. 5. Minimum run times shall be provided in each planting zone/micro climate. 6. Provide spot emitters at every 500 feet of mainline. 7. Provide a fertigation system. 8. Locate controllers on mechanical rooms or enclosures. 9. The system will be wired based on hydro zoning and plant type to minimize irrigation use. 10. Location of valves and other components shall be concealed and to reduce visual impact. 11. Subsurface irrigation drip lines are generally spaced 1-2' below soil surface to achieve greatest saturation in soil. 	
WELO NOTES	
1. THE PLANTING DESIGN COMPLIES WITH THE CRITERIA FOR THE WATER CONSERVATION IN LANDSCAPING ORDINANCE AND HAS BEEN APPROVED FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE AND IRRIGATION DESIGN.	
7. ALL LANDSCAPE PLANTING AREAS SHALL INCLUDE A 3 INCH MINIMUM LAYER OF MULCH PER SPECIFICATIONS.	
SUB SOIL PREPARATION NOTES	
Soil testing for horticultural purposes shall be completed during the Design Development Phase to test both soil grade and topical conditions for horticulture suitability and development of the site with plant. The site soils plan will include a soil quality control program for testing and verification of amended and import soils. Planting soil mix specifications shall be met to the maximum, conditioning, importation of topsoil or a combination of trees.	
Sub Soil Preparation: Excavate and remove 24 inches soil planting areas.	
SOIL PREPARATION NOTES	
1. Project Landscape Architect to provide C&S files for staking of tree pits/continuous trenches.	
2. Provide penetration test at each tree in areas noted on plan.	
3. Provide dry well submit per detail as required by test results and specifications.	
GRADING NOTES	
1. The perimeter landscape will include a series of berms, terraces and bio-retention areas up to 2' in height and 2' in depth relative to existing grades. Landscape grading will conform with grading requirements and final elevations as defined by the Civil Engineer.	
2. Provide final grading of all areas disturbed by construction. Provide erosion control during plant establishment.	
DRAINAGE & STORMWATER TREATMENT NOTES	
1. Provide underdrainage system for all trees. Connect to storm drain.	
2. Provide landscape area drainage and bio-retention inlets, clean outs and overflow drains at each bio-retention area. Storm drain systems to be developed as design progresses.	
3. Provide liner and 18-inch course of drain rock in all bio-retention areas.	
4. Provide 4" underdrain line at the center of each bio-retention area. The line shall run the length of the area and include clean-outs at 75 feet on center.	
5. Provide clean outs and overflow drains at each bio-retention area.	

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(415) 440-1000

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OWNER
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MENLO PARK, CALIFORNIA 94025

PROJECT NAME
PROJECT NUMBER
DATE
DRAWING NUMBER

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

PROPOSED LANDSCAPE PLANTING, IRRIGATION, GRADING, DRAINAGE, & SOILS SCHEDULE Final Condition

PROJECT NUMBER: 2015-027
SCALE: 1" = 20'-0"
DATE: 26-December-2017
PROJECT LOCATION: 300-309 CONSTITUTION DRIVE, MENLO PARK, CA 94025

REV: 027-001

PROJECT NUMBER: L2-201

DATE: 26-December-2017



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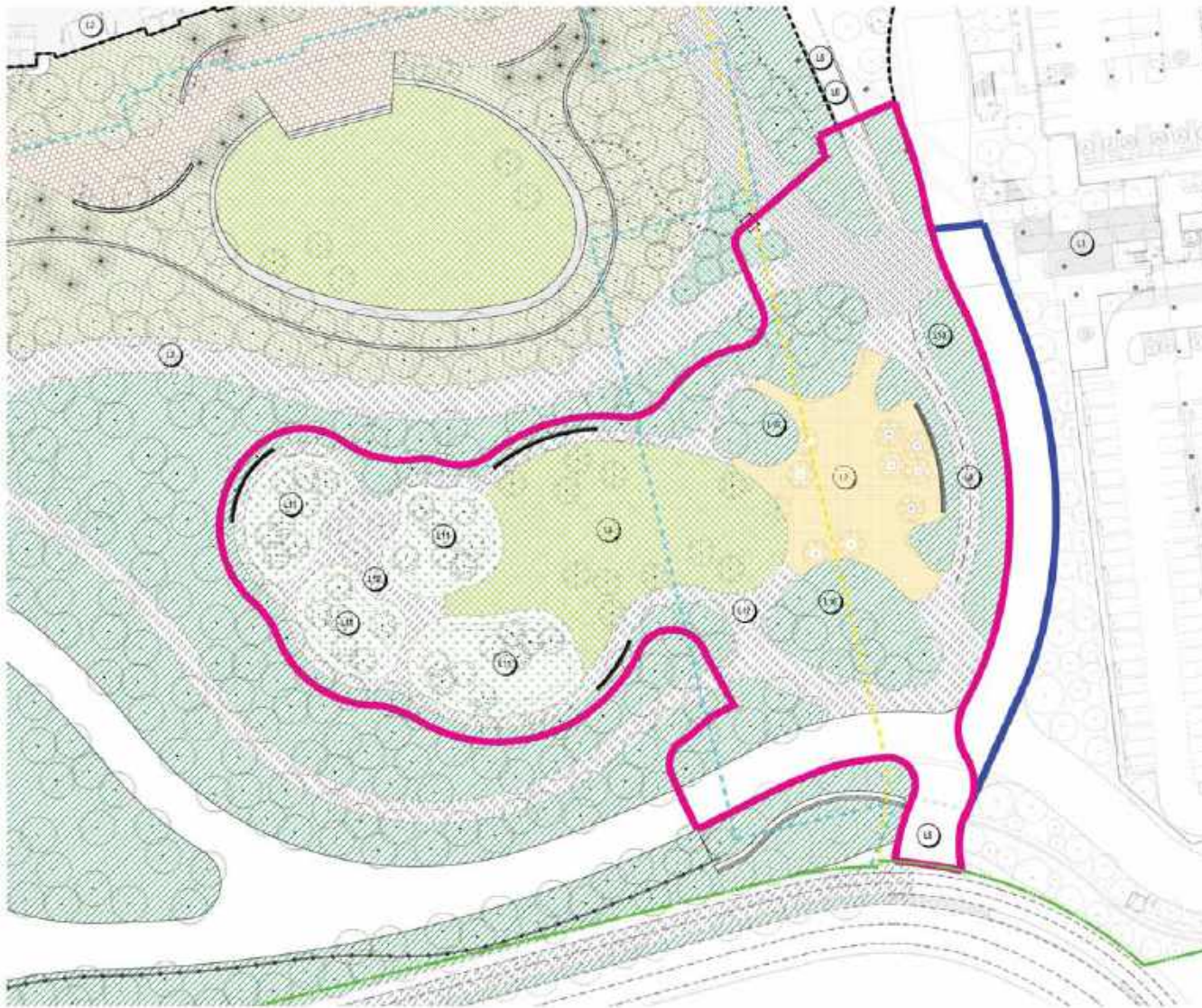
DATE: 10/26/17

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
 PROPOSED LANDSCAPE
 PLAN
 Final Condition

PROJECT NUMBER
 2015-027
 SCALE
 1" = 25' 0"
 26-December 2017
 THIS DRAWING IS A
 PART OF SET NO. 24-28

SHEET NUMBER
 L2-202
 DESIGN PARTNER OF



FINAL PUBLIC OPEN SPACE LEGEND	
SYMBOL	DESCRIPTION
	Public Open Space Final Phase: 2.0 Acres
	Public Open Space Final Phase: Additional 0.3 Acres at 8-5 EYA
	EOS Lease Line
	M/W 23 Limit of Work (LOW)
	M/W 22 Interim Limit of Work (LOW)
	E Site Property Line
	M/W 21
	M/W 22
	Building 10A (Renovated)
	Chico Street
	Chico Entry (Installed per M/W 23) 1) Public bike and pedestrian access point to public open space and Bayfront Bridge 2) Fire access
	Bayfront Bridge (Installed per Interim Phase)
	Plaza (Installed per Interim Phase)
	3-way guide bike lane connecting Chico Street to Bayfront Bridge (Installed per Interim Phase)
	Lawn
	Planting Area
	Bioretention Area
	Walking path

CMG
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400 South St., San Francisco, CA 94107
415.774.0000

FACEBOOK
OWNER
1700 Market Street, San Francisco, CA 94103

DATE: 10/20/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TYPE:
PUBLIC OPEN SPACE
ENLARGEMENT PLAN

Final Condition

PROJECT NUMBER
2015-007
SCALE
1" = 20' 0"
DATE
26-December 2017
DRAWN BY: J. S. S.
CHECKED BY: J. S. S.

SHEET NUMBER
L2.300
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FLOOD_ZONE

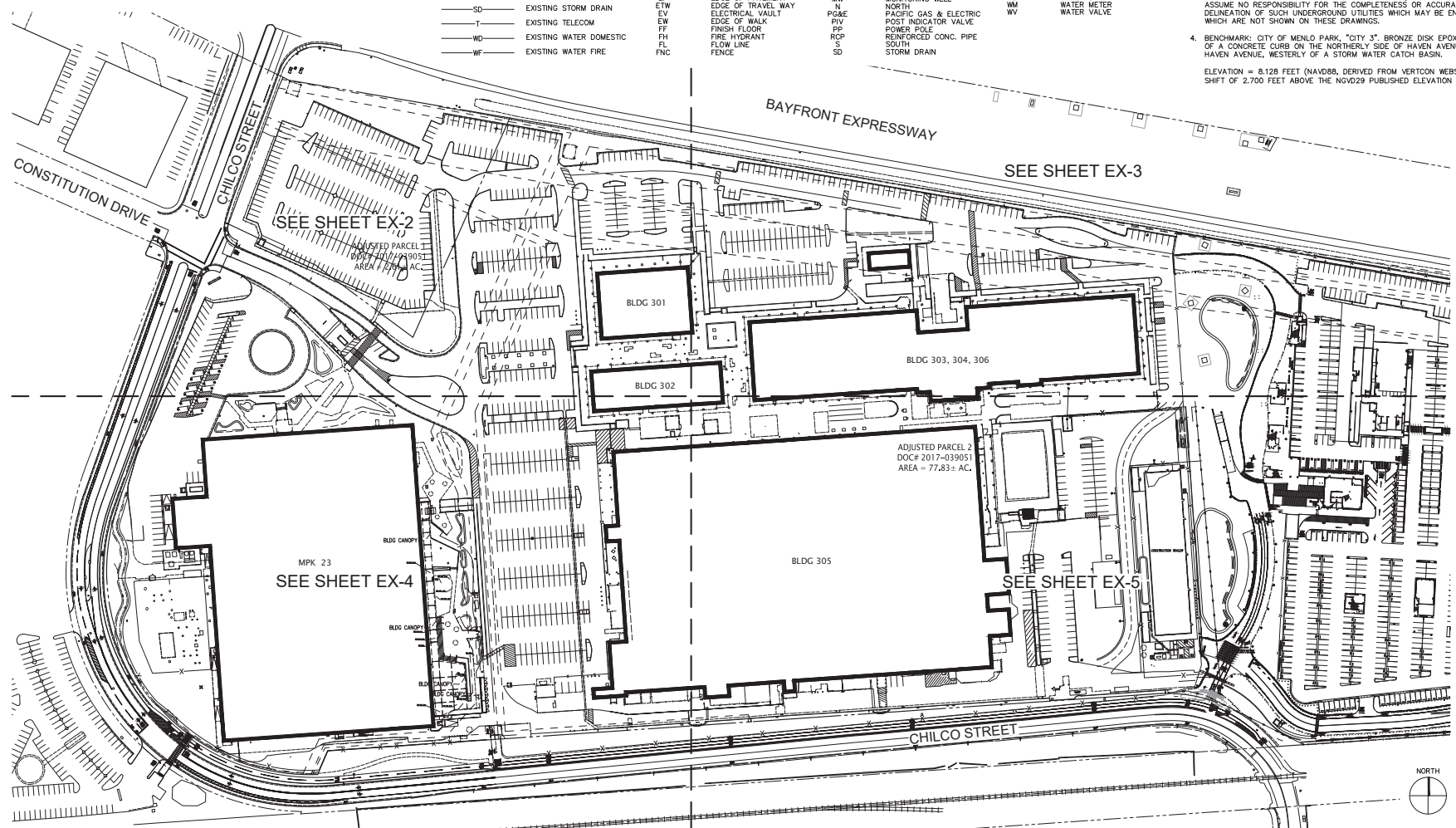
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 BASE FLOOD ELEVATION IS 10.3 FEET
 *PER FLOOD INSURANCE STUDY BY FEMA, OCTOBER 16, 2012 (NAVD 88)

LEGEND

---	PROPERTY LINE	AC	ASPHALT CONCRETE	FS	FIRE SERVICE	SDJ	STORM DRAIN JUNCTION BOX
---	EASEMENT	AD	AREA DRAIN	GM	GAS METER	SDM	STORM DRAIN MANHOLE
---	EXISTING COMMUNICATIONS	AI	AREA INLET	GV	GUY ANCHOR	SE	SOUTH EAST
---	EXISTING ELECTRICAL	BFP	BACK FLOW PREVENTER	GV	GAS VALVE	SL	STREET LIGHT
---	EXISTING FENCE	BOL	BOLLARD	GW	GUY WIRE	SS	SANITARY SEWER
---	EXISTING GAS	BW	BACK OF WALK	HCR	HANDICAP RAMP	SSC	SANITARY SEWER CLEAN OUT
---	EXISTING FIBER OPTIC	CB	CATCH BASIN	IB	IRRIGATION BOX	SSMH	SANITARY SEWER MANHOLE
---	EXISTING STORM DRAIN	CLF	CHAIN LINK FENCE	IRV	INVERT	SW	SOUTH WEST
---	EXISTING TELECOM	CLF	CLEANOUT	IRK	IRRIGATION LINE	TB	TELEPHONE BOX
---	EXISTING WATER DOMESTIC	COMM	COMMUNICATIONS	JB	JUNCTION BOX	TFC	TOP FACE OF CURB
---	EXISTING WATER FIRE	CONC	CONCRETE	JP	JOINT POWER POLE	TMH	TELEPHONE MANHOLE
---		DI	DRAIN INLET	LG	LIP OF GUTTER	TUB	UNKNOWN UTILITY BOX
---		E	EAST	LL	LANE LINE	VCP	VITRIFIED CLAY PIPE
---		EB	ELECTRIC BOX	MH	MANHOLE	W	WEST
---		EP	EDGE OF PAVEMENT	IRK	IRRIGATION LINE	WB	WATER BOX
---		ETW	EDGE OF TRAVEL WAY	MW	MONITORING WELL	WM	WATER METER
---		EV	ELECTRICAL VAULT	N	NORTH	WV	WATER VALVE
---		EW	EDGE OF WALK	PG&E	PACIFIC GAS & ELECTRIC		
---		FF	FINISH FLOOR	N	NORTH		
---		FL	FIRE HYDRANT	PP	POST INDICATOR VALVE		
---		FH	FLOW LINE	RCP	REINFORCED CONC. PIPE		
---		FNC	FENCE	S	SOUTH		
---				SD	STORM DRAIN		

NOTES

1. THE BOUNDARY, EASEMENT, AND OTHER ENCUMBRANCES SHOWN ON THIS DRAWING ARE BASED SOLELY UPON INFORMATION CONTAINED IN ALTA/ACSM LAND TITLE SURVEY PREPARED BY BFL.
2. ALL DISTANCES AND ELEVATIONS SHOWN HEREON ARE IN FEET AND DECIMALS THEREOF.
3. THE TYPES, LOCATIONS, SIZES AND/OR DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS TOPOGRAPHIC SURVEY WERE OBTAINED FROM SOURCES OF VARYING RELIABILITY. THE CONTRACTOR IS CAUTIONED THAT ONLY ACTUAL EXCAVATION WILL REVEAL THE TYPES, EXTENT, SIZES, LOCATIONS AND DEPTHS OF SUCH UNDERGROUND UTILITIES. (A REASONABLE EFFORT HAS BEEN MADE TO LOCATE AND DELINEATE ALL KNOWN UNDERGROUND UTILITIES). HOWEVER, THE ENGINEER CAN ASSUME NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF ITS DELINEATION OF SUCH UNDERGROUND UTILITIES WHICH MAY BE ENCOUNTERED, BUT WHICH ARE NOT SHOWN ON THESE DRAWINGS.
4. BENCHMARK: CITY OF MENLO PARK, "CITY 3" BRONZE DISK EPOXIED INTO THE TOP OF A CONCRETE CURB ON THE NORTHERLY SIDE OF HAVEN AVENUE, AT #3585 HAVEN AVENUE, WESTERLY OF A STORM WATER CATCH BASIN.
 ELEVATION = 8.128 FEET (NAVD88, DERIVED FROM VERTCON WEBSITE WITH A DATUM SHIFT OF 2.700 FEET ABOVE THE NGVD29 PUBLISHED ELEVATION OF 5.428)



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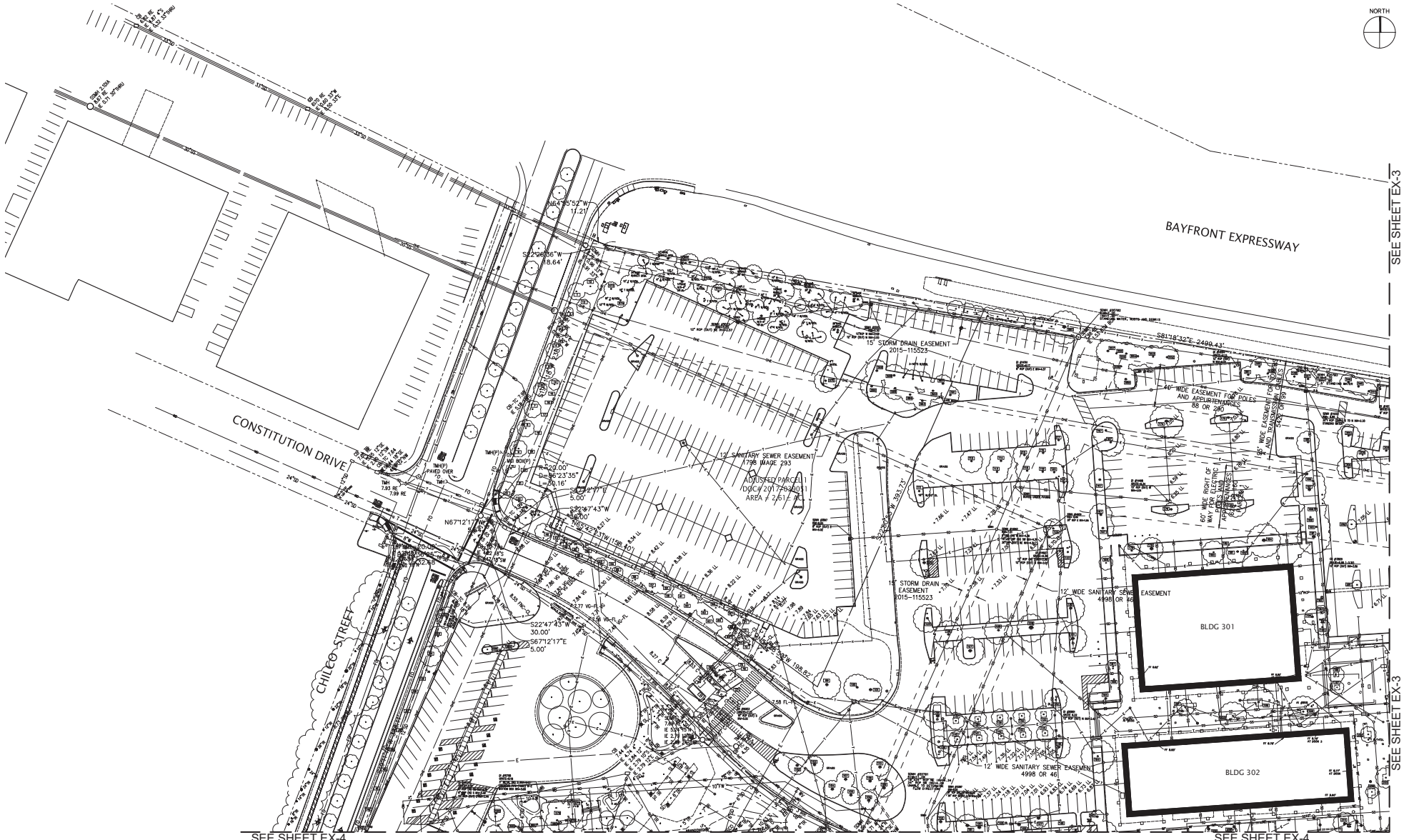
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
EXISTING CONDITIONS

PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
EX-1

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SEE SHEET EX-4

SEE SHEET EX-4

SEE SHEET EX-3

SEE SHEET EX-3

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 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
EXISTING CONDITIONS

PROJECT NUMBER
 2015-007
 SCALE
 1" = 40'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
EX-2
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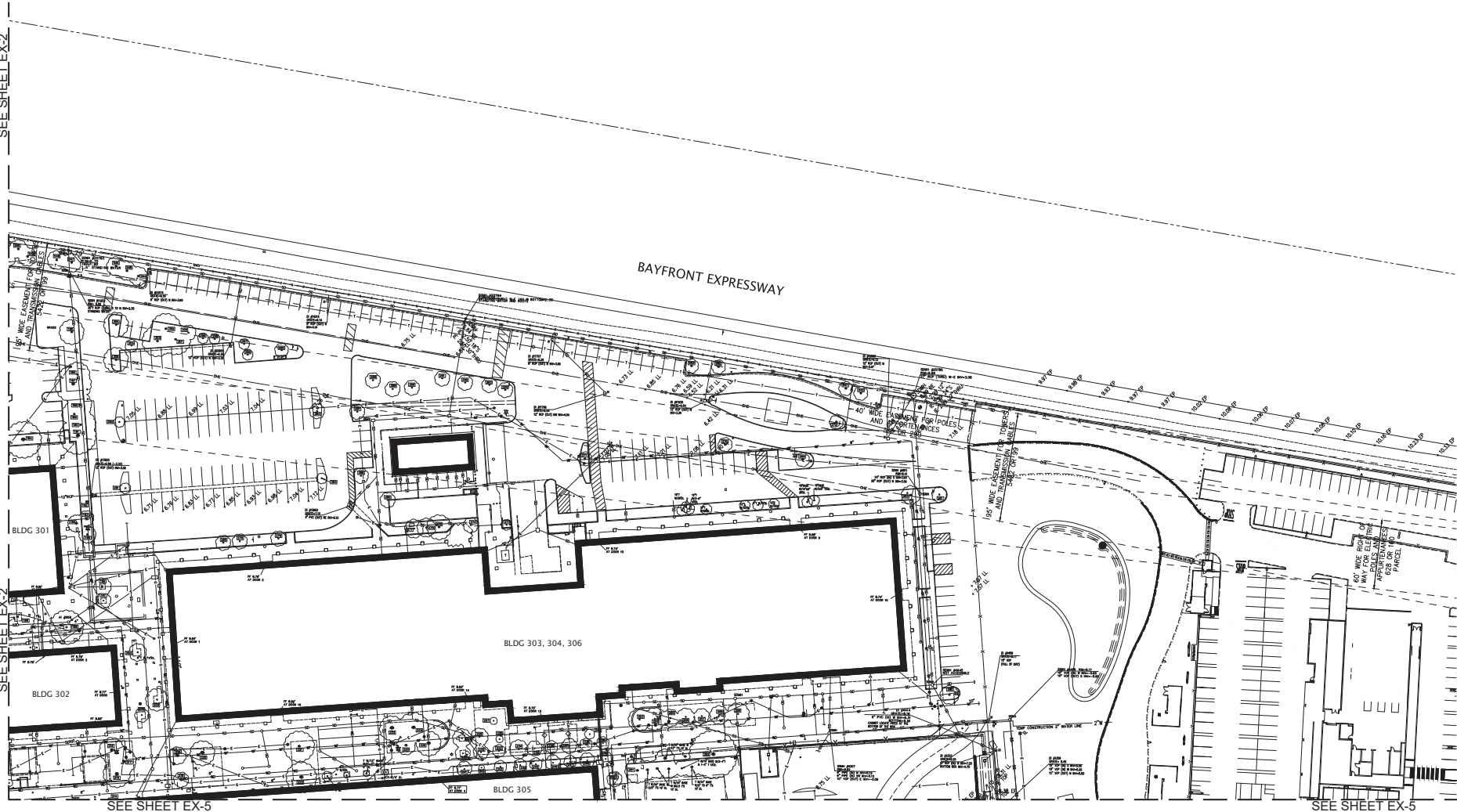


SEE SHEET EX-2

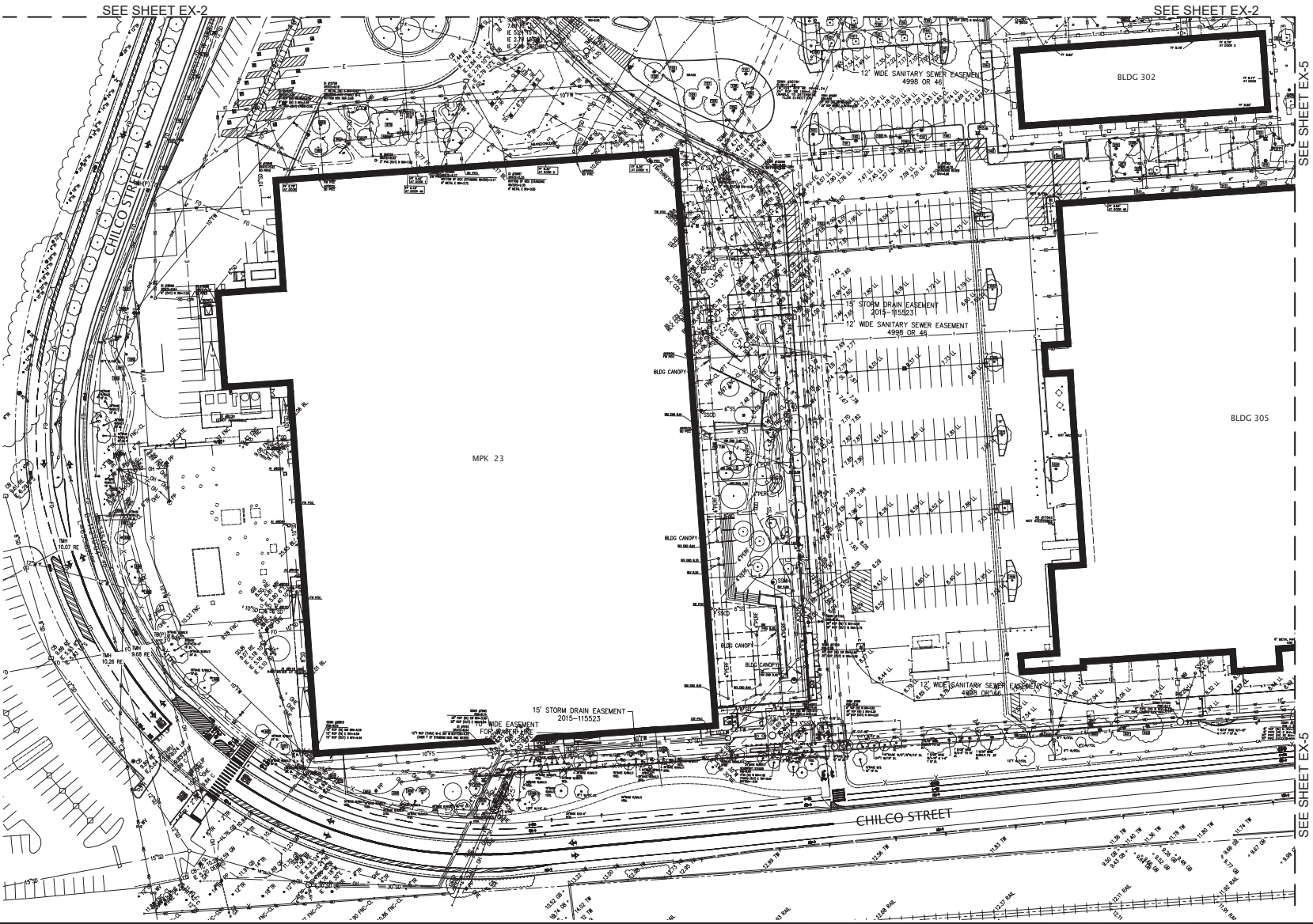
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SEE SHEET EX-5

SEE SHEET EX-5



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SEE SHEET EX-2

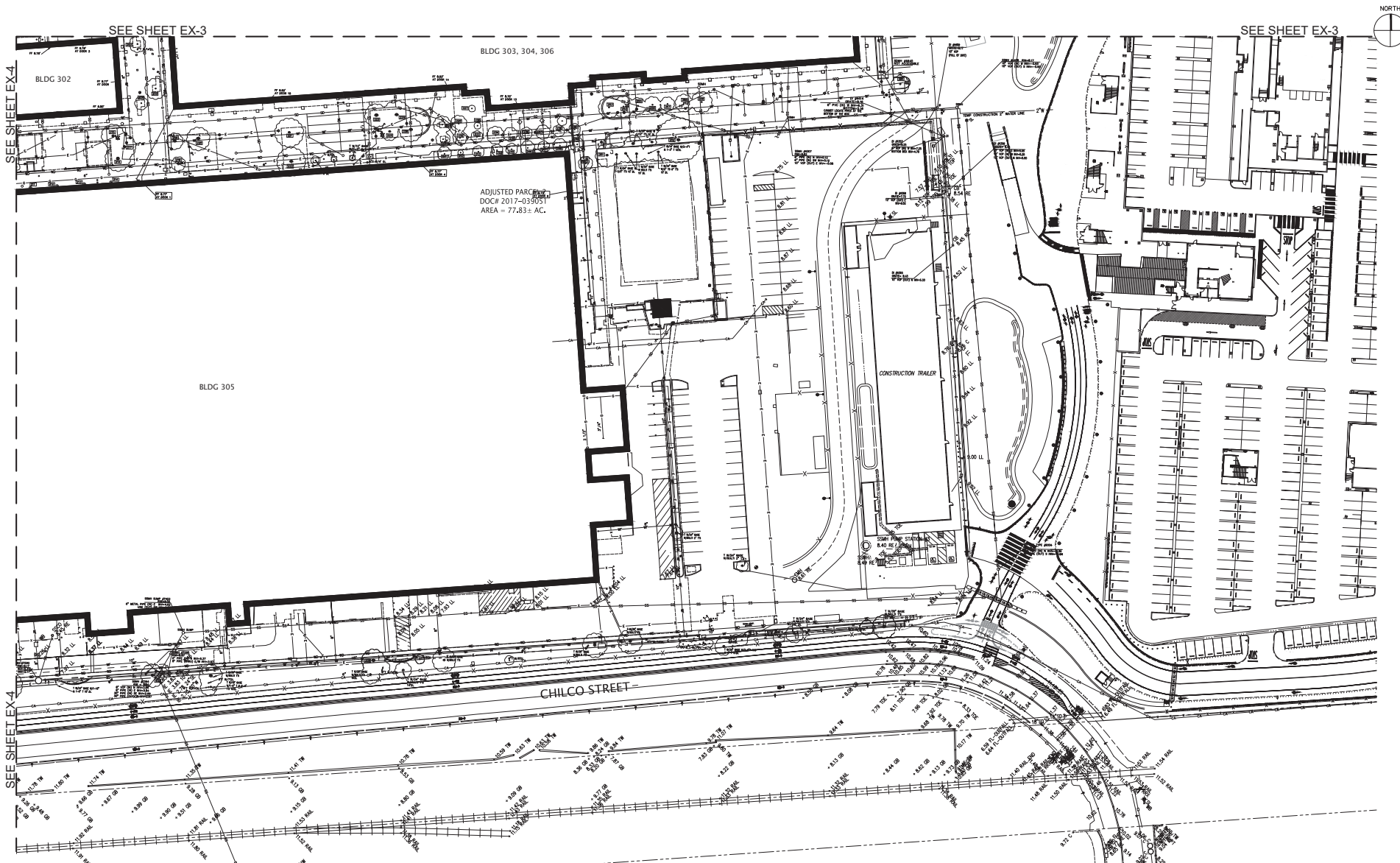
SEE SHEET EX-2



SEE SHEET EX-5

SEE SHEET EX-5

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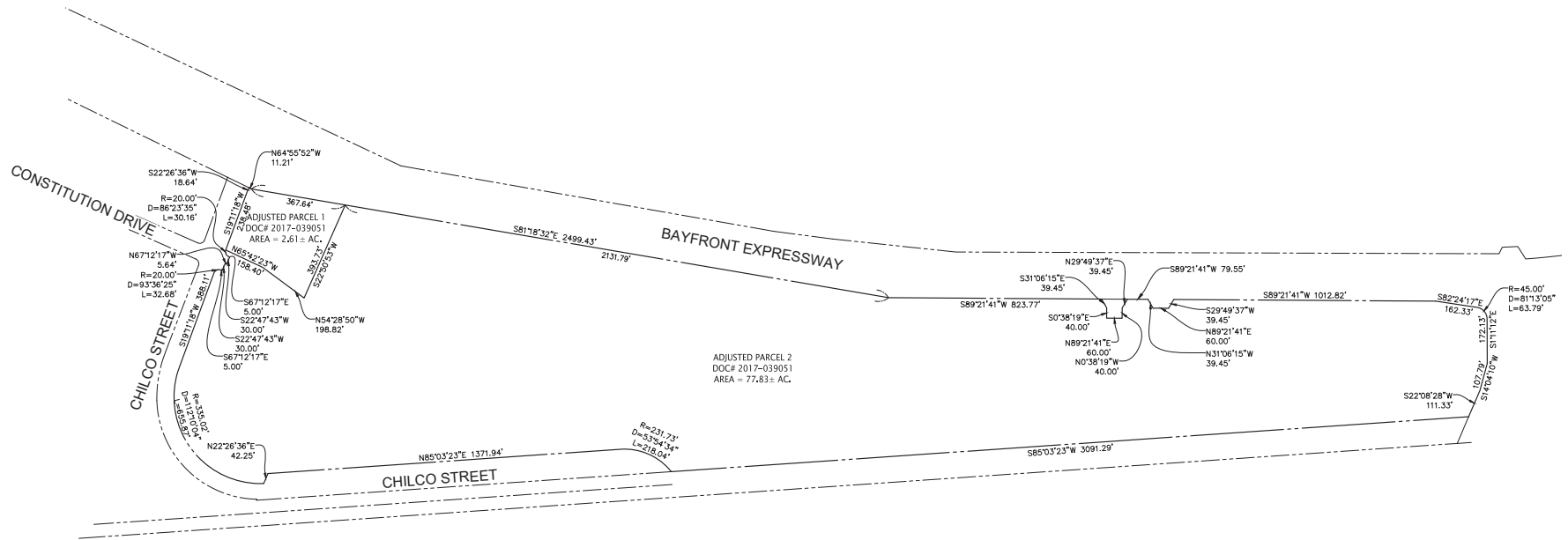
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 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

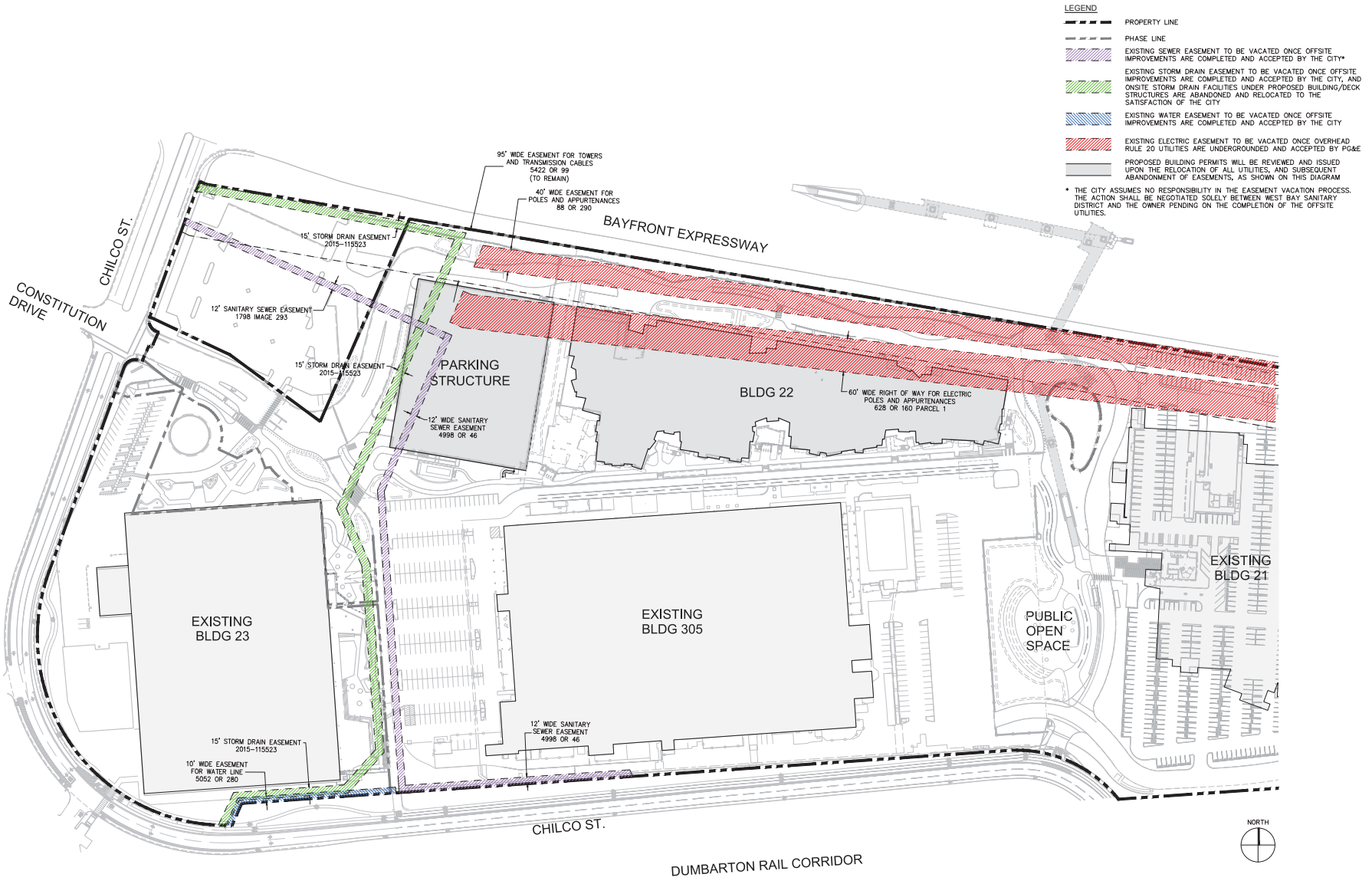
TITLE
EXISTING CONDITIONS

PROJECT NUMBER
 2015-007
 SCALE
 1" = 40'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
EX-5
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Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
EXISTING EASEMENTS
INTERIM PHASE

PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

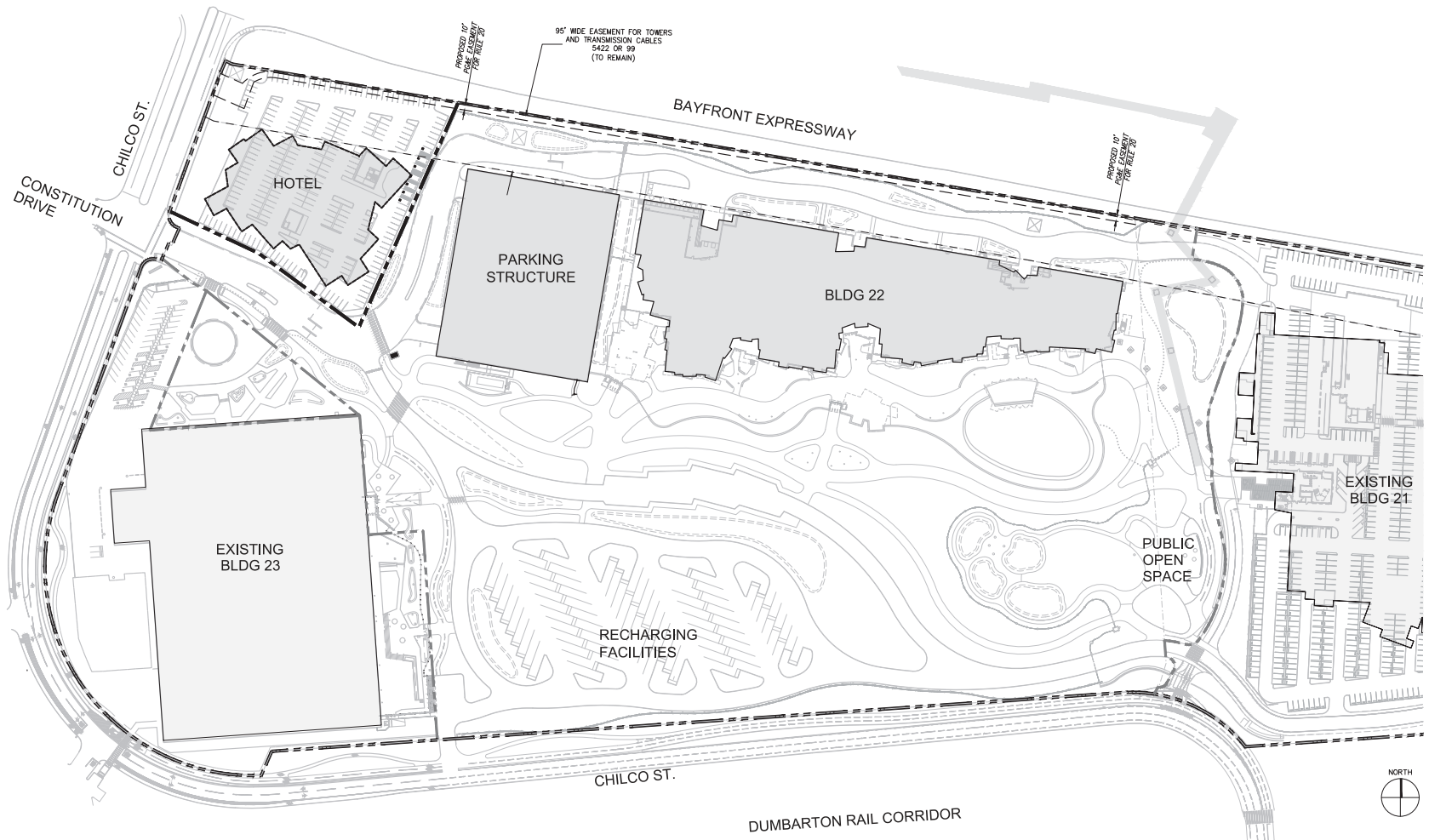
SHEET NUMBER
ESMT-1

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LEGEND

- PROPERTY LINE
- PHASE LINE

NOTE:
NO EXISTING EASEMENTS TO BE VACATED DURING THIS PHASE



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 FAX: 925-245-8798
FACEBOOK
 OWNER
 1 HANCOCK WAY
 MENLO PARK, CALIFORNIA 94025

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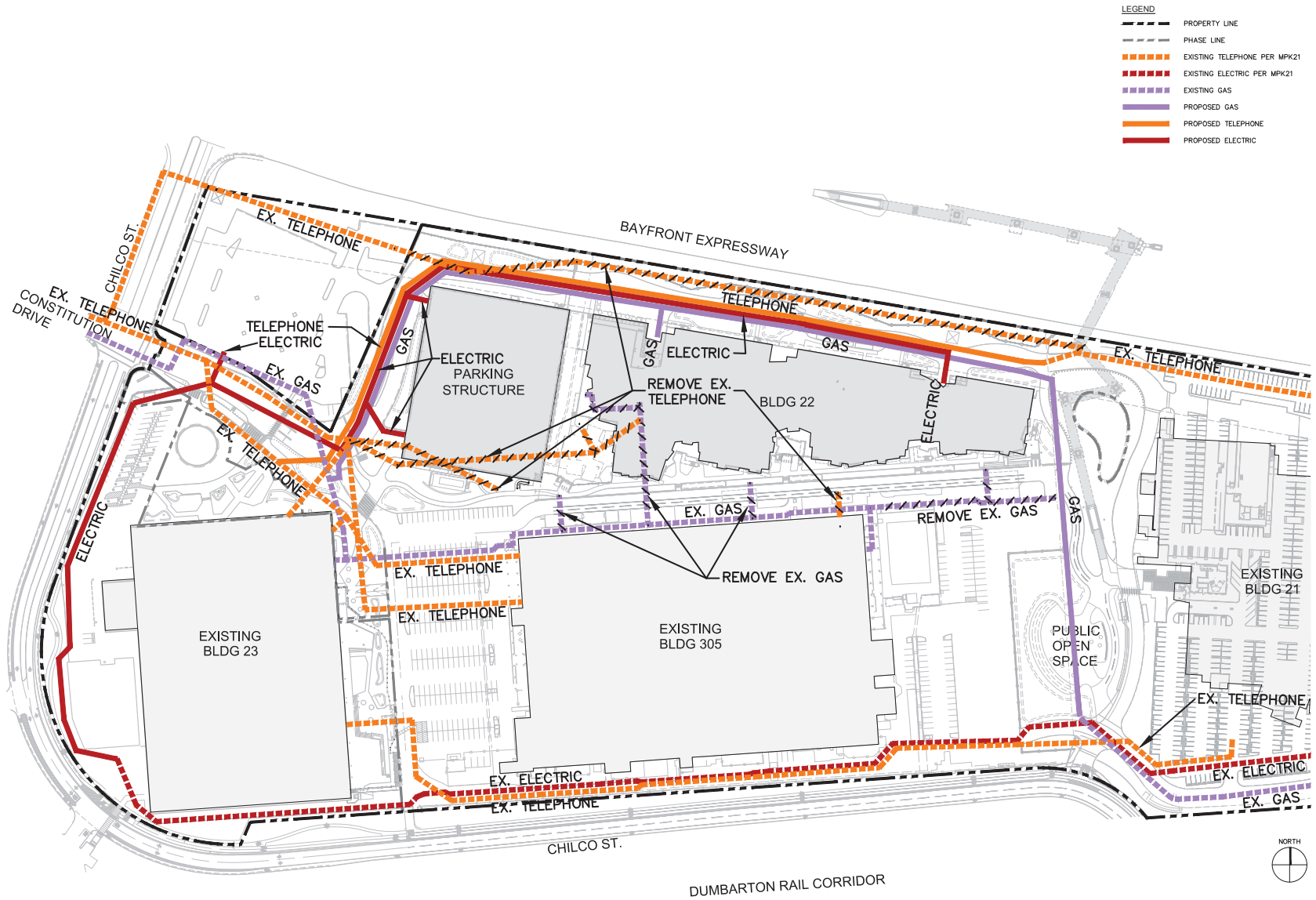
TITLE
EXISTING EASEMENTS
FINAL PHASE

PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

ESMT-2

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 MENLO PARK, CALIFORNIA 94025

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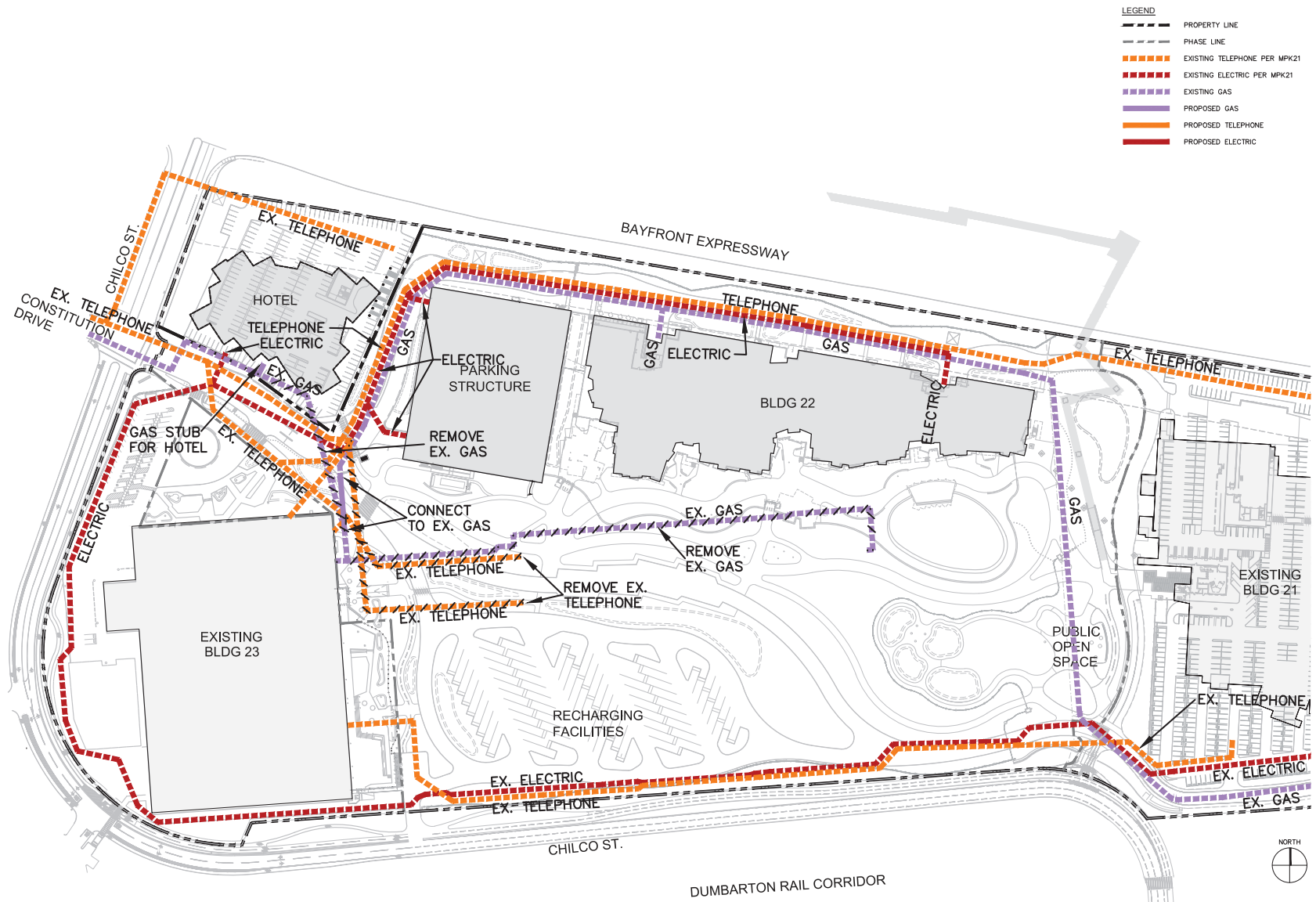
Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
**PROPOSED INTERIM
 DRY UTILITIES**

PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
DRY-1

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 OWNER
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CONSULTANTS

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PROPOSED FINAL DRY UTILITIES

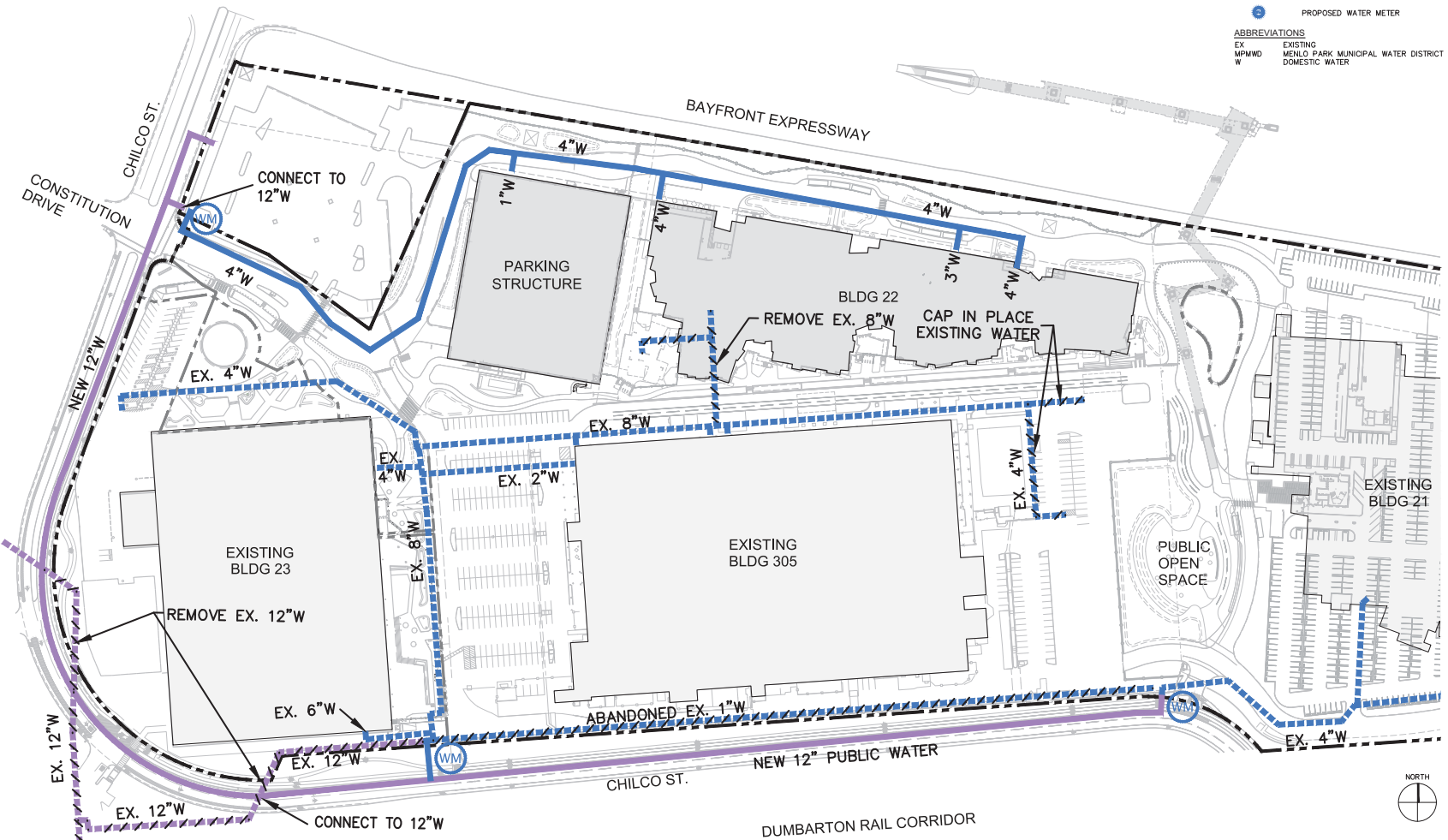
PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
DRY-2
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NOTE:
OFFSITE IMPROVEMENTS SHOWN ON CHILCO STREET ARE PENDING THE APPROVAL OF THE CITY'S PUBLIC WORKS DEPARTMENT.

- LEGEND**
- PROPERTY LINE
 - - - PHASE LINE
 - EXISTING PRIVATE WATER SYSTEM
 - EXISTING PUBLIC WATER SYSTEM (MPMWD)
 - ⊕ EXISTING WATER METER
 - PROPOSED PUBLIC WATER SYSTEM
 - PROPOSED PRIVATE WATER SYSTEM
 - ⊕ PROPOSED WATER METER

- ABBREVIATIONS**
- EX EXISTING
 - MPMWD MENLO PARK MUNICIPAL WATER DISTRICT
 - W DOMESTIC WATER



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OWNER
1 HANDBY WAY
MENLO PARK, CALIFORNIA 94025

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PROPOSED INTERIM DOMESTIC WATER

PROJECT NUMBER
2015-007
SCALE
1" = 80'
9-28-2017
11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

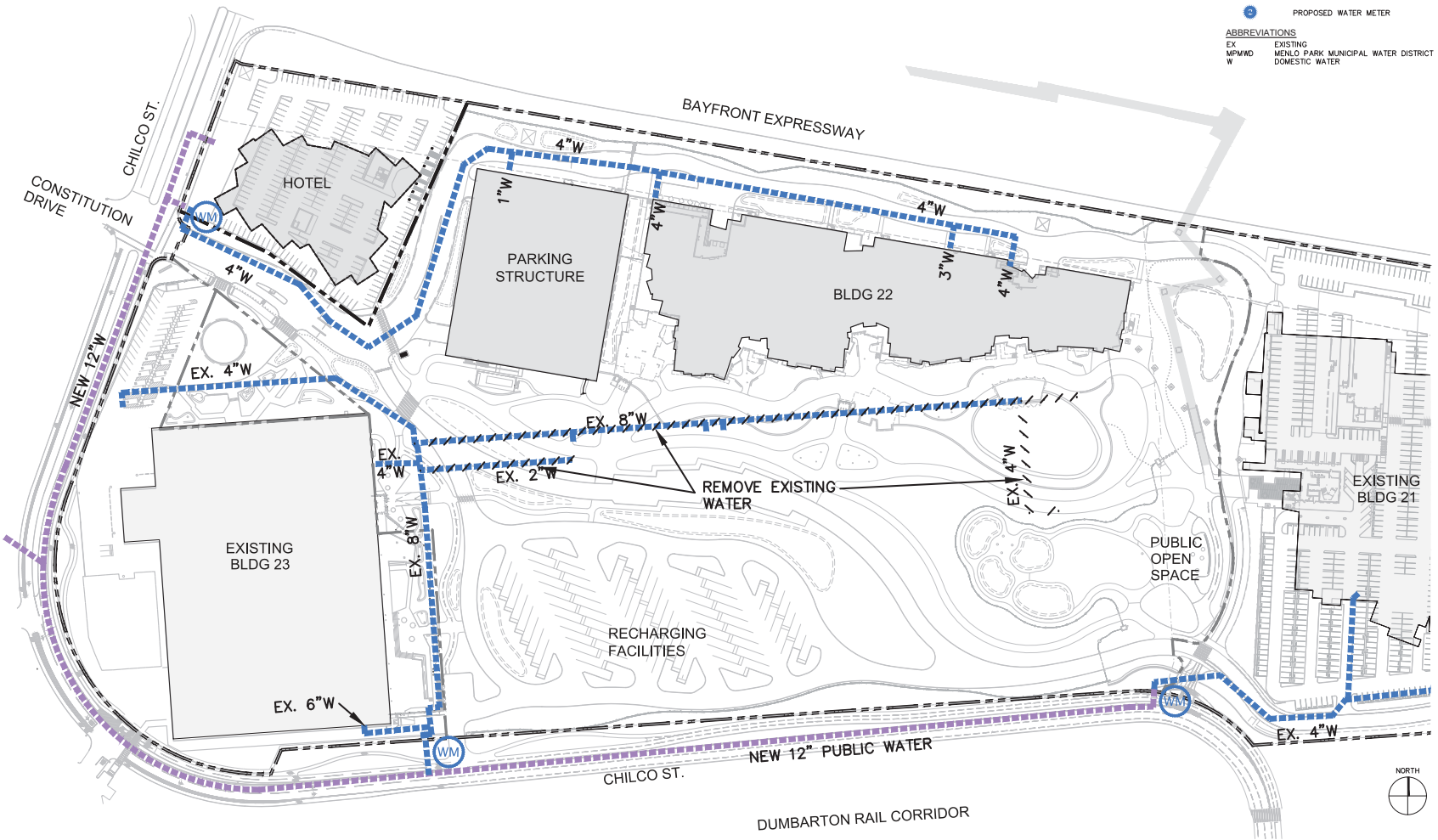
SHEET NUMBER
DW-1

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NOTE:
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- LEGEND**
- PROPERTY LINE
 - - - PHASE LINE
 - EXISTING PRIVATE WATER SYSTEM
 - EXISTING PUBLIC WATER SYSTEM (MPMWD)
 - ⊕ EXISTING WATER METER
 - PROPOSED PUBLIC WATER SYSTEM
 - PROPOSED PRIVATE WATER SYSTEM
 - ⊕ PROPOSED WATER METER

- ABBREVIATIONS**
- EX EXISTING
 - MPMWD MENLO PARK MUNICIPAL WATER DISTRICT
 - W DOMESTIC WATER



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MENLO PARK, CALIFORNIA 94025

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Facebook Building 21, 22 & Hotel Site
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PROPOSED FINAL DOMESTIC WATER

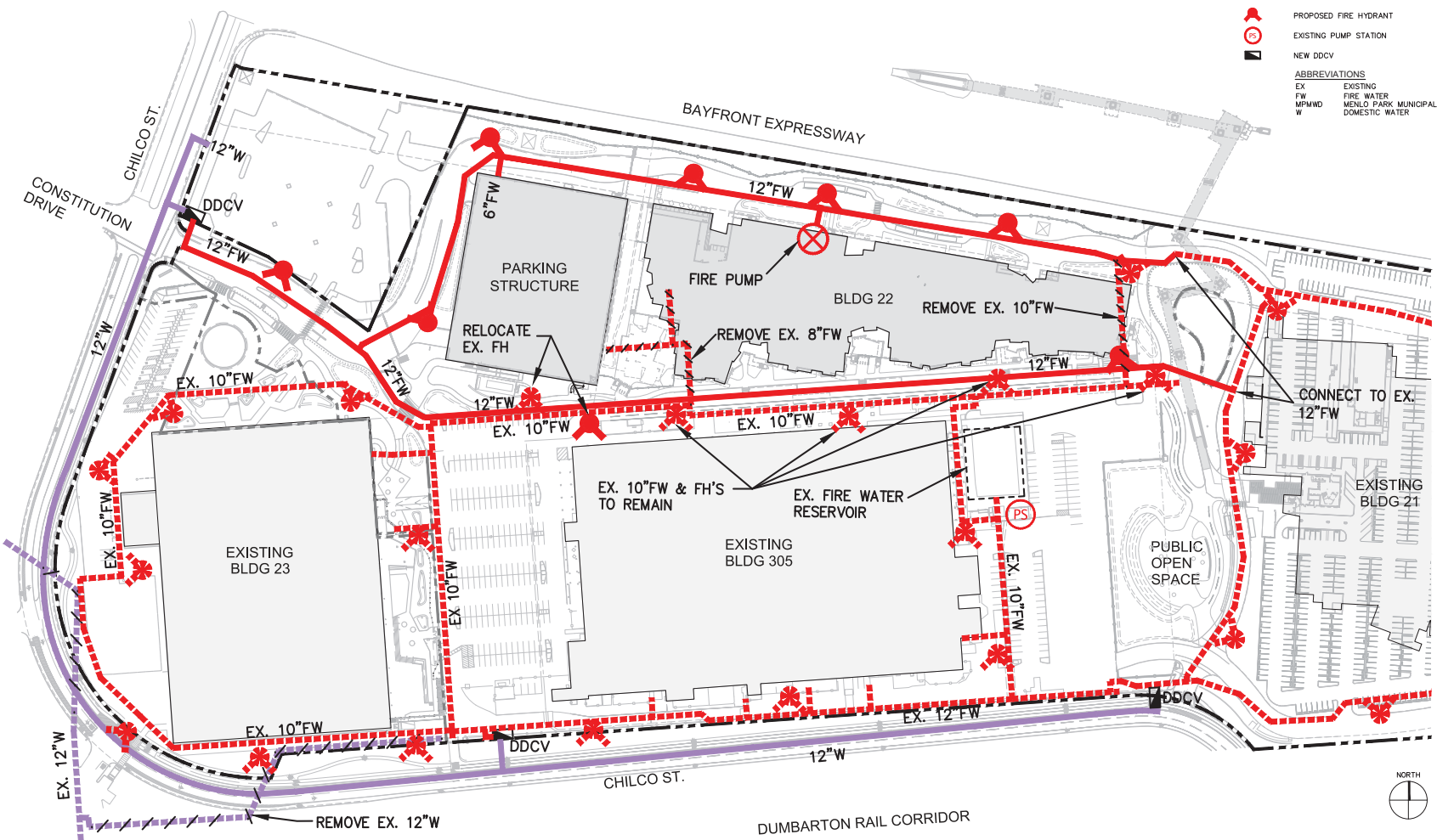
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2015-007
SCALE
1" = 80'
9-28-2017
11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
DW-2

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NOTES:
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 ADDITIONAL FIRE HYDRANTS MAY BE INSTALLED AS REQUIRED DURING FINAL DESIGN

- LEGEND**
- PROPERTY LINE
 - PHASE LINE
 - - - EXISTING PRIVATE FIRE WATER
 - - - EXISTING PUBLIC FIRE WATER (MPMWD)
 - ⊗ EXISTING FIRE HYDRANT
 - ⊗ PROPOSED PRIVATE FIRE WATER
 - ⊗ PROPOSED PUBLIC FIRE WATER
 - ⊗ PROPOSED FIRE HYDRANT
 - ⊗ EXISTING PUMP STATION
 - ⊗ NEW DDCV
- ABBREVIATIONS**
- EX EXISTING
 - FW FIRE WATER
 - MPMWD MENLO PARK MUNICIPAL WATER DISTRICT
 - W DOMESTIC WATER



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**PROPOSED INTERIM
 FIRE WATER**

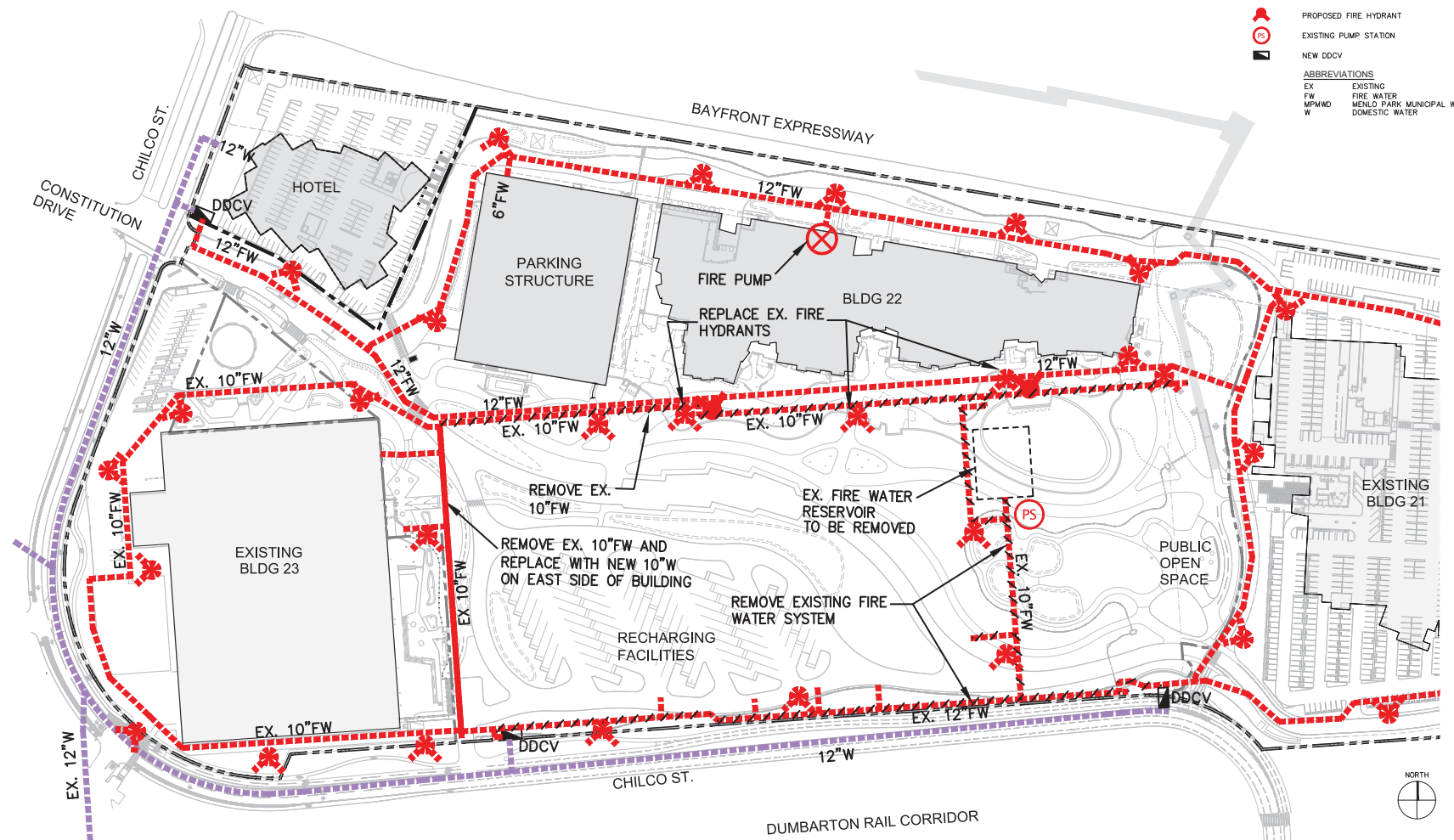
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 SCALE
 1" = 80'
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 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
FW-1



NOTES:
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 ADDITIONAL FIRE HYDRANTS MAY BE INSTALLED AS REQUIRED DURING FINAL DESIGN

- LEGEND**
- PROPERTY LINE
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 - EXISTING PRIVATE FIRE WATER
 - EXISTING PUBLIC FIRE WATER (MPMWD)
 - EXISTING FIRE HYDRANT
 - PROPOSED PRIVATE FIRE WATER
 - PROPOSED PUBLIC FIRE WATER
 - PROPOSED FIRE HYDRANT
 - EXISTING PUMP STATION
 - NEW DDCV
- ABBREVIATIONS**
- EX EXISTING
 - FW FIRE WATER
 - MPMWD MENLO PARK MUNICIPAL WATER DISTRICT
 - W DOMESTIC WATER



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PROPOSED FINAL FIRE WATER

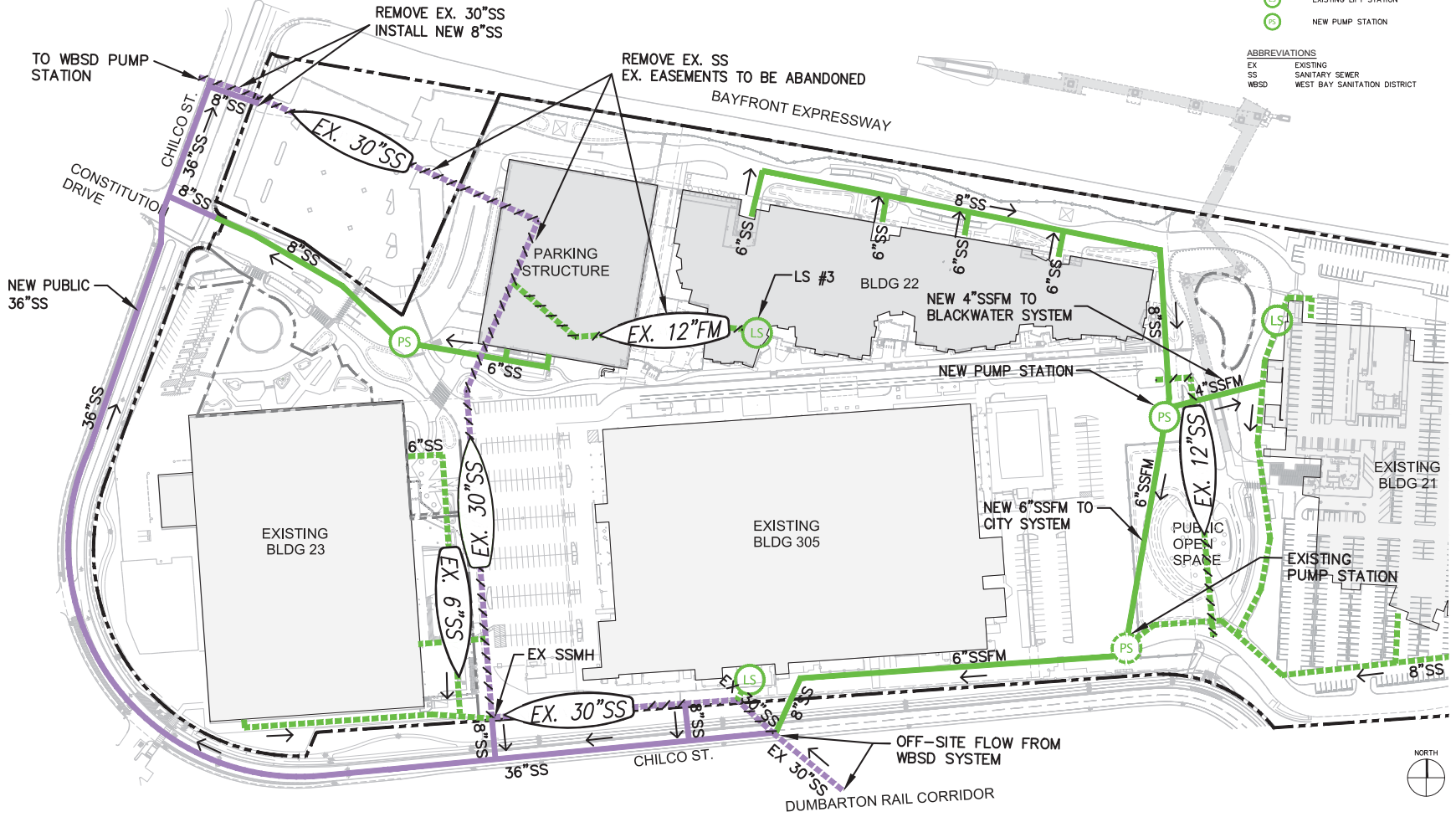
PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
FW-2
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NOTE:
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- LEGEND**
- PROPERTY LINE
 - - - PHASE LINE
 - EXISTING PRIVATE SANITARY SEWER
 - EXISTING PUBLIC SANITARY SEWER (WBSD)
 - PROPOSED PUBLIC SANITARY SEWER
 - PROPOSED PRIVATE SANITARY SEWER
 - LS EXISTING LIFT STATION
 - PS NEW PUMP STATION

- ABBREVIATIONS**
- EX EXISTING
 - SS SANITARY SEWER
 - WBSD WEST BAY SANITATION DISTRICT



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**PROPOSED INTERIM
SANITARY SEWER**

PROJECT NUMBER
2015-007
SCALE
1" = 80'
9-28-2017
11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

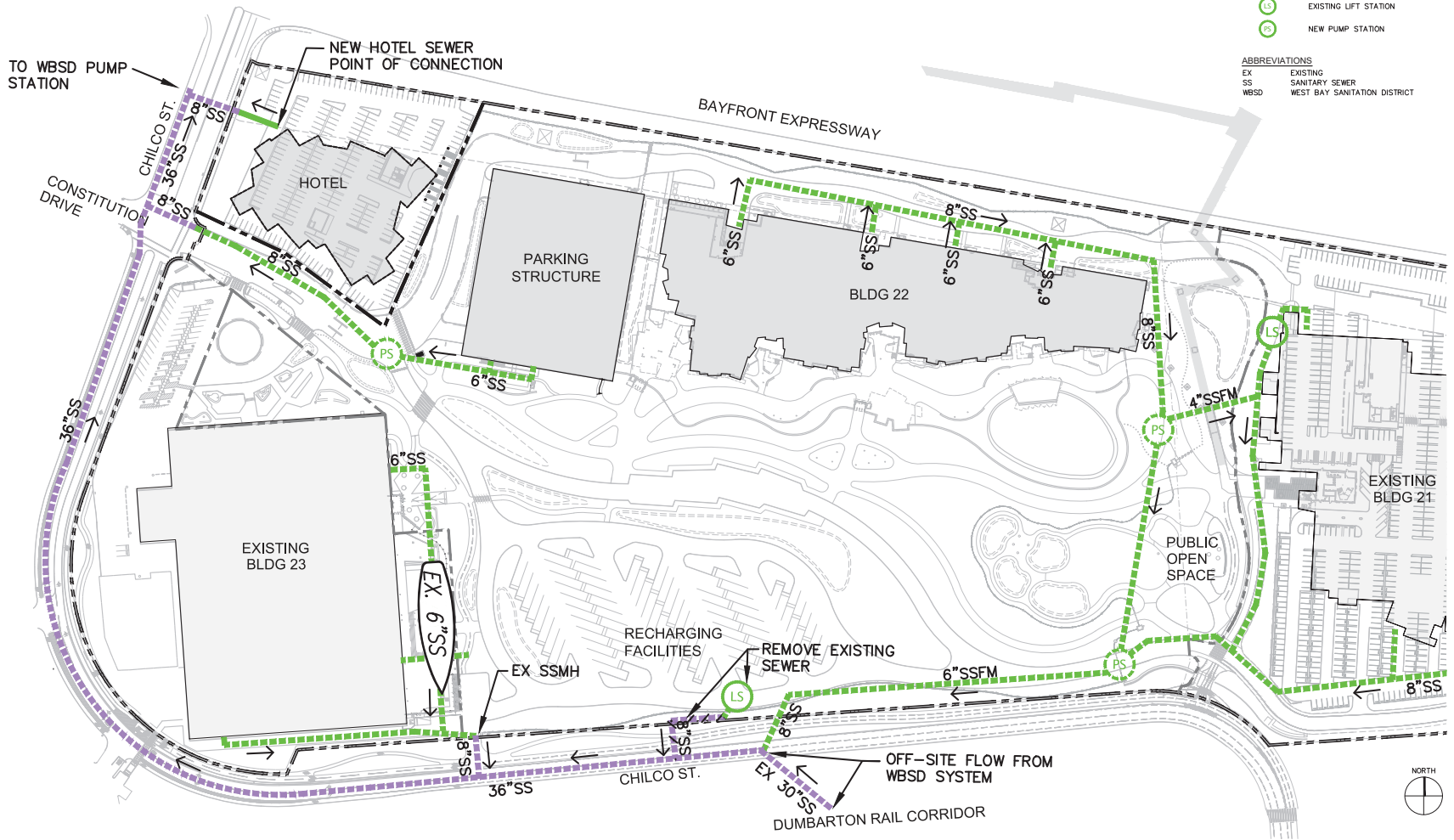
SS-1

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NOTE:
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THE CITY'S PUBLIC WORKS DEPARTMENT.

- LEGEND**
- PROPERTY LINE
 - - - PHASE LINE
 - EXISTING PRIVATE SANITARY SEWER
 - EXISTING PUBLIC SANITARY SEWER (WBSD)
 - PROPOSED PUBLIC SANITARY SEWER
 - PROPOSED PRIVATE SANITARY SEWER
 - LS EXISTING LIFT STATION
 - PS NEW PUMP STATION

- ABBREVIATIONS**
- EX EXISTING
 - SS SANITARY SEWER
 - WBSD WEST BAY SANITATION DISTRICT



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**PROPOSED FINAL
SANITARY SEWER**

PROJECT NUMBER
2015-007
SCALE
1" = 80'
9-28-2017
11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

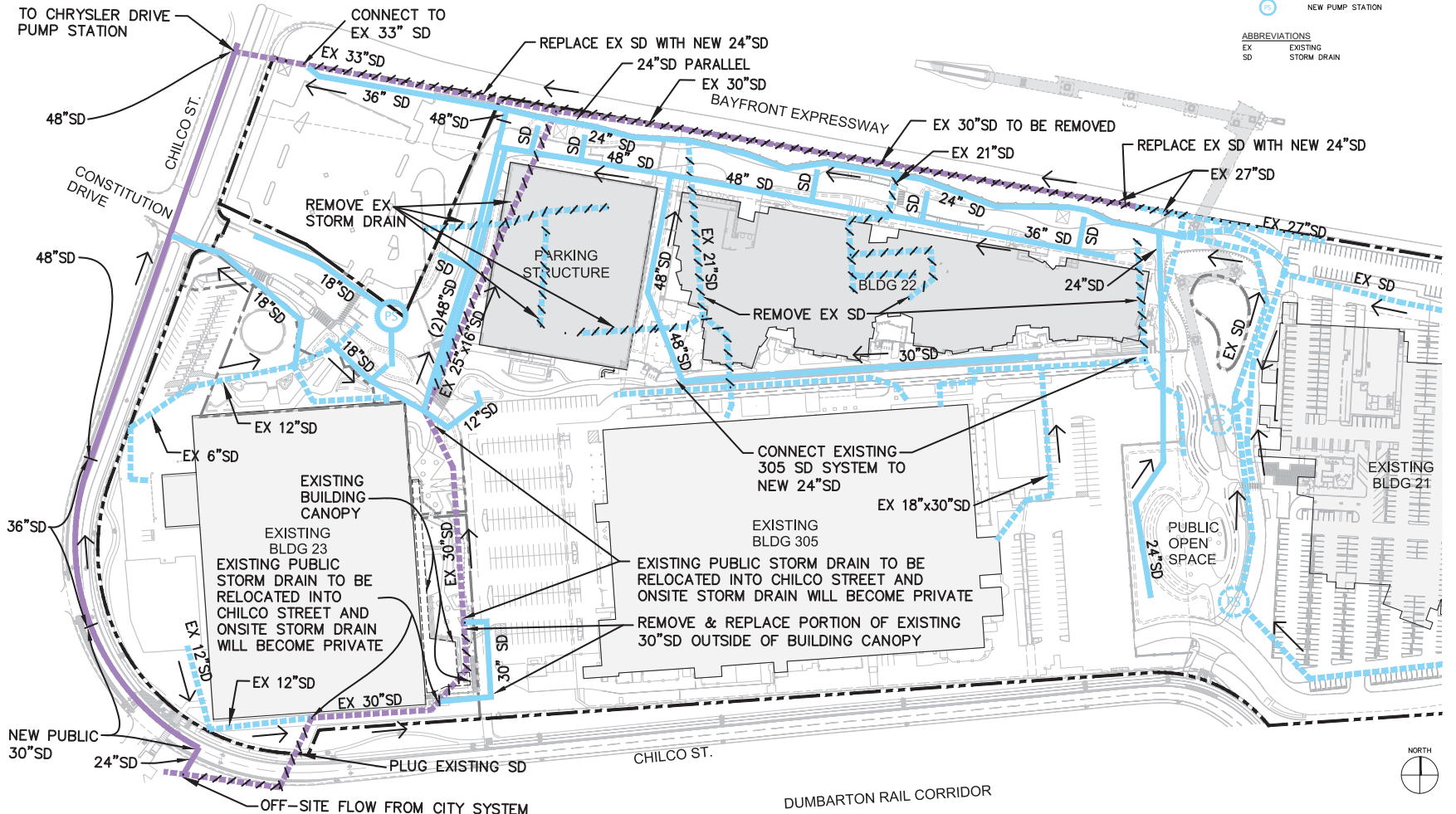
SS-2

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NOTE: OFFSITE IMPROVEMENTS SHOWN ON CHILCO STREET ARE PENDING THE APPROVAL OF THE CITY'S PUBLIC WORKS DEPARTMENT.

- LEGEND**
- PROPERTY LINE
 - - - PHASE LINE
 - EXISTING PRIVATE STORM DRAIN
 - EXISTING PUBLIC STORM DRAIN (CITY OF MENLO PARK)
 - PROPOSED PUBLIC STORM DRAIN (CITY OF MENLO PARK)
 - PROPOSED PRIVATE STORM DRAIN
 - ← FLOW DIRECTION
 - ⊙ NEW PUMP STATION

- ABBREVIATIONS**
- EX EXISTING
 - SD STORM DRAIN

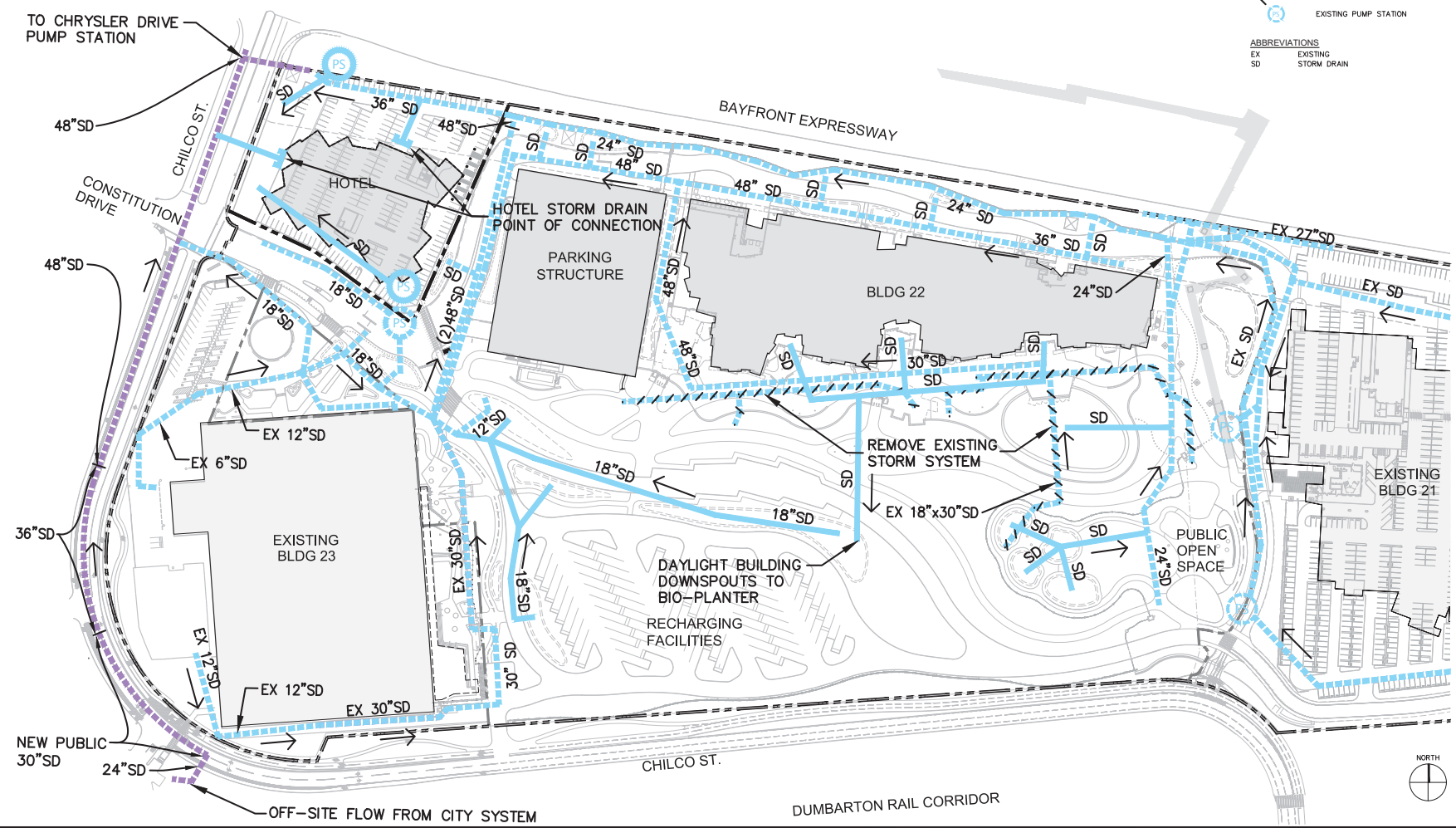


<p>CIER & WRIGHT CIVIL ENGINEER 2850 COLLIER CANYON ROAD LIVERMORE, CALIFORNIA 94551 USA TEL: 925-245-8788 FAX: 925-245-8798 FACEBOOK OWNER 1 HANDBY WAY MENLO PARK, CALIFORNIA 94025</p>	<p>CONSULTANTS</p> <p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p>PROPOSED INTERIM STORM DRAIN</p>	<p>PROJECT NUMBER 2015-007 SCALE 1" = 80' 9-28-2017 11x17 SHEET SIZE: N.T.S. ORIGINAL SHEET SIZE: 24" x 36"</p>	<p>SHEET NUMBER</p> <p>SD-1</p> <p>© SDPI PARTNERS, LLP</p>
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NOTE:
OFFSITE IMPROVEMENTS SHOWN ON CHILCO
STREET ARE PENDING THE APPROVAL OF
THE CITY'S PUBLIC WORKS DEPARTMENT.

- LEGEND**
- PROPERTY LINE
 - - - PHASE LINE
 - EXISTING PRIVATE STORM DRAIN
 - EXISTING PUBLIC STORM DRAIN (CITY OF MENLO PARK)
 - PROPOSED PUBLIC STORM DRAIN (CITY OF MENLO PARK)
 - PROPOSED PRIVATE STORM DRAIN
 - ← FLOW DIRECTION
 - ⊙ EXISTING PUMP STATION

- ABBREVIATIONS**
- EX EXISTING
 - SD STORM DRAIN



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FAX: 925-245-8786
FACEBOOK
OWNER
1 HANDBY WAY
MENLO PARK, CALIFORNIA 94025

CONSULTANTS

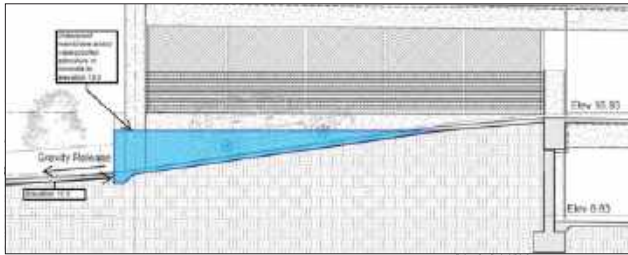
Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TITLE
**PROPOSED FINAL
STORM DRAIN**

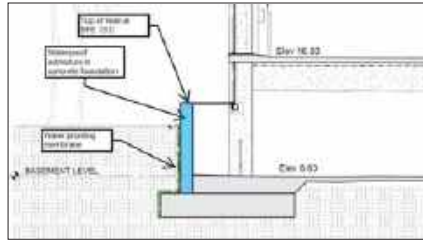
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2015-007
SCALE
1" = 80'
9-28-2017
11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
SD-2

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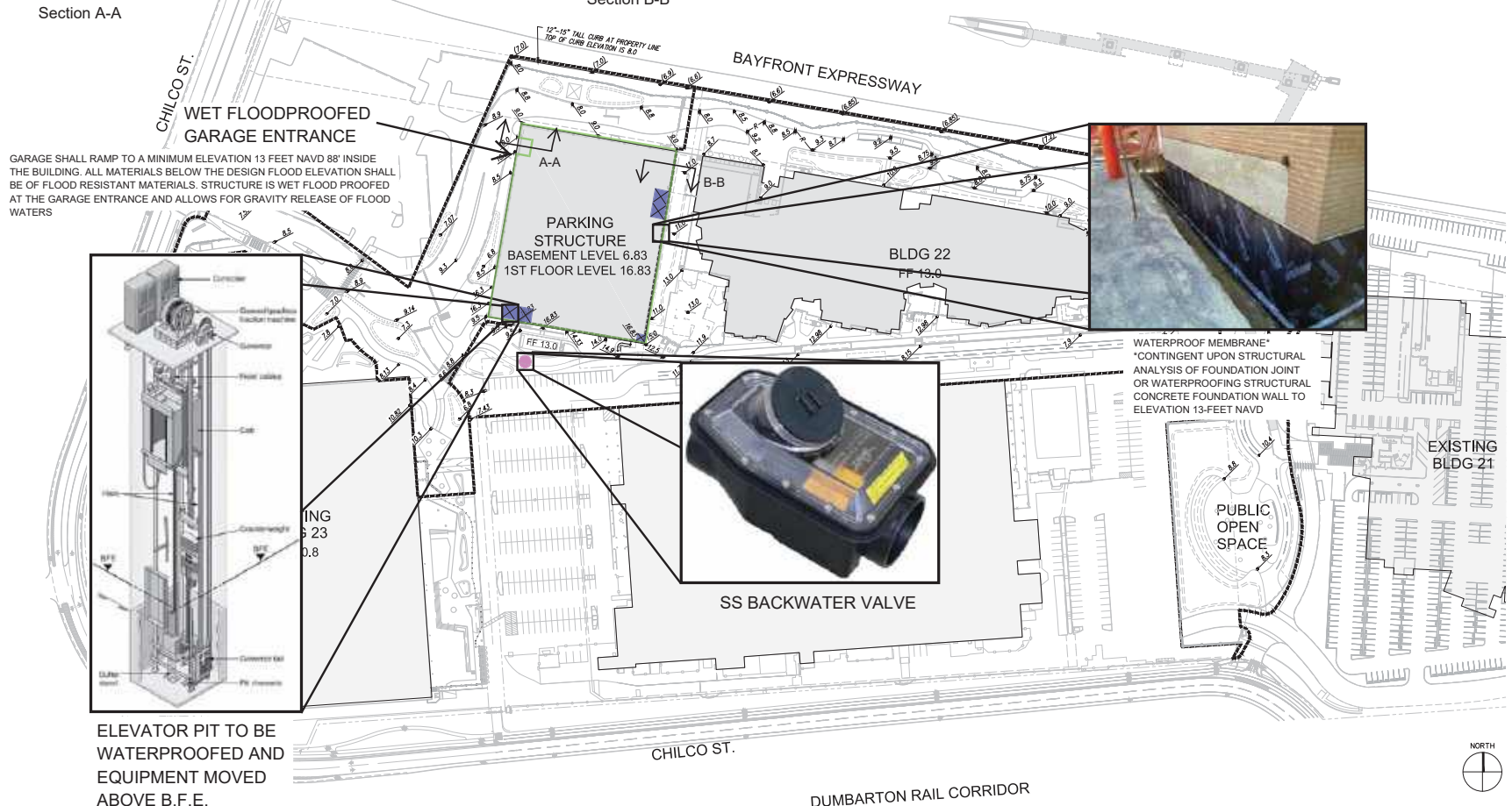
Section A-A



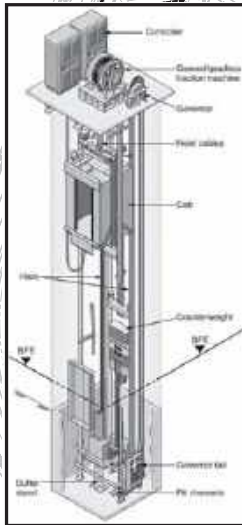
Section B-B

Key	Location	Product Type	Number of Locations
■	Elevators	Follow FEMA TB-4	5
●	Sanitary Sewer Line	Backwater Valve	1
—	Wall below flood elevation level	Waterproof Membrane Sealant	Perimeter
N/A	Utility Penetrations in Foundation Slab	N/A - Build Penetrations Above Flood Level	Perimeter

NOTE: ALL MATERIALS BELOW THE BFE (13 FEET NAVD 88) MUST BE CONSTRUCTED OF A FLOOD RESISTANT MATERIAL (CONCRETE, PRESSURE TREATED WOOD, ETC.)



GARAGE SHALL RAMP TO A MINIMUM ELEVATION 13 FEET NAVD 88' INSIDE THE BUILDING. ALL MATERIALS BELOW THE DESIGN FLOOD ELEVATION SHALL BE OF FLOOD RESISTANT MATERIALS. STRUCTURE IS WET FLOOD PROOFED AT THE GARAGE ENTRANCE AND ALLOWS FOR GRAVITY RELEASE OF FLOOD WATERS



ELEVATOR PIT TO BE WATERPROOFED AND EQUIPMENT MOVED ABOVE B.F.E.



WATERPROOF MEMBRANE*
*CONTINGENT UPON STRUCTURAL ANALYSIS OF FOUNDATION JOINT OR WATERPROOFING STRUCTURAL CONCRETE FOUNDATION WALL TO ELEVATION 13-FEET NAVD



SS BACKWATER VALVE

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San Francisco, CA 94111
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PAE
LANDSCAPE CONSULTANT
500 2nd St., #215
San Francisco, CA 94104
TEL: 415-544-7500

EXPONENT
LIFE SAFETY CONSULTANT
2401 McCord Ave.
Los Angeles, CA 90066
TEL: 310-754-2700

BRIGHTWORKS
SUSTAINABILITY CONSULTANT
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San Francisco, CA 94105
TEL: 415-392-6850

L'OBSERVATOIRE
LIGHTING CONSULTANT
288 Lafayette St.
New York, NY 10012
TEL: 212-525-4463

NEWSON BROWN
ACOUSTIC CONSULTANT
2001 Waverly Blvd., Suite 301
Santa Monica, CA 90403
TEL: 310-829-6343

LERCH BATES
BUILDING MAINTENANCE CONSULTANT
2171 Francisco Blvd., East, Suite H
San Rafael, CA 94901
TEL: 415-432-0750

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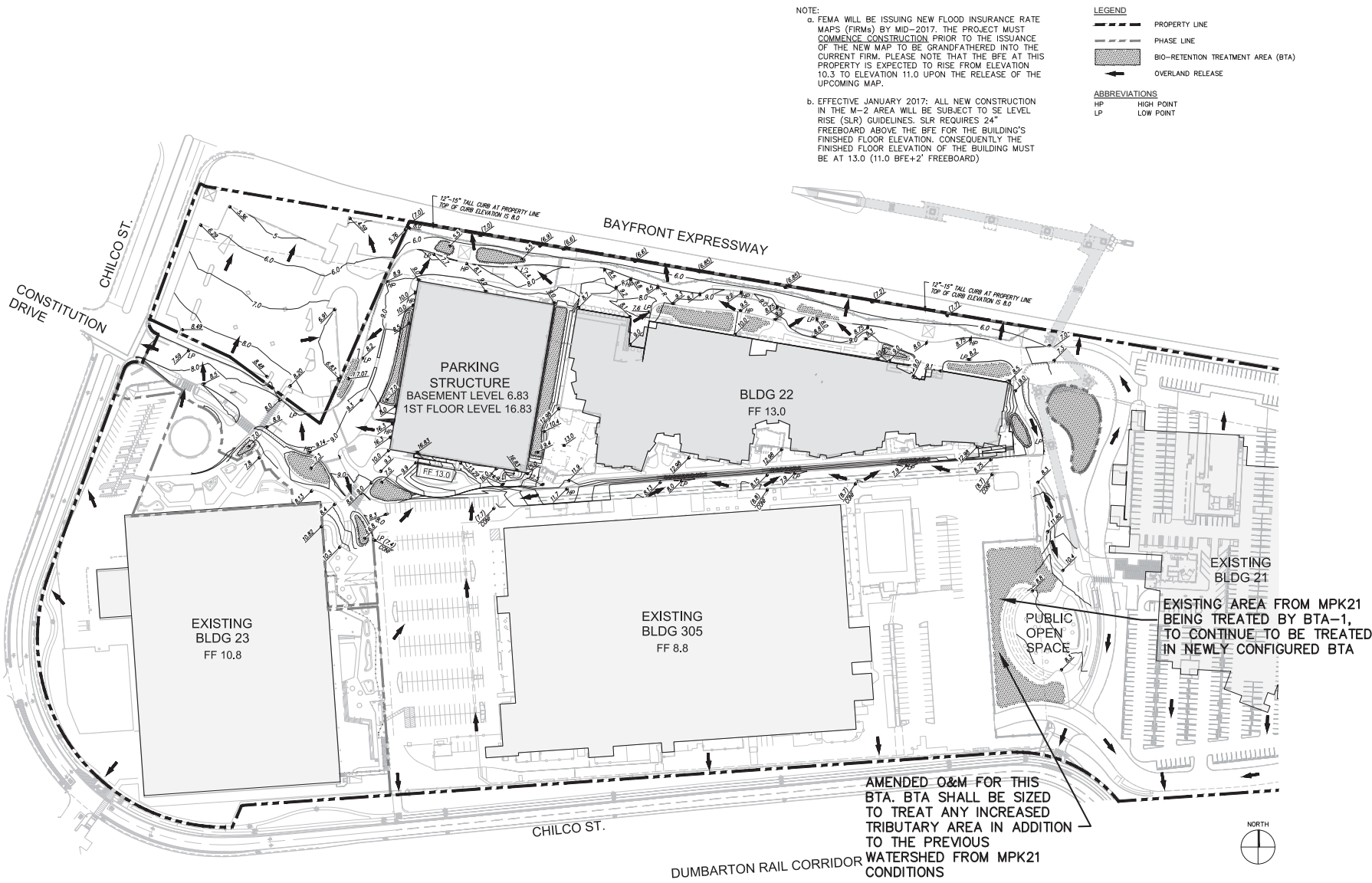
FEMA PLAN - INTERIM GRADING

PROJECT NUMBER
2015-007
SCALE
1" = 80'

SHEET NUMBER
GP-3

11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

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NOTE:

a. FEMA WILL BE ISSUING NEW FLOOD INSURANCE RATE MAPS (FIRMS) BY MID-2017. THE PROJECT MUST COMMENCE CONSTRUCTION PRIOR TO THE ISSUANCE OF THE NEW MAP TO BE GRANDFATHERED INTO THE CURRENT FIRM. PLEASE NOTE THAT THE BFE AT THIS PROPERTY IS EXPECTED TO RISE FROM ELEVATION 10.3 TO ELEVATION 11.0 UPON THE RELEASE OF THE UPCOMING MAP.

b. EFFECTIVE JANUARY 2017: ALL NEW CONSTRUCTION IN THE M-2 AREA WILL BE SUBJECT TO SE LEVEL RISE (SLR) GUIDELINES. SLR REQUIRES 24" FREEBOARD ABOVE THE BFE FOR THE BUILDING'S FINISHED FLOOR ELEVATION. CONSEQUENTLY THE FINISHED FLOOR ELEVATION OF THE BUILDING MUST BE AT 13.0 (11.0 BFE+2' FREEBOARD)

- LEGEND**
- PROPERTY LINE
 - - - PHASE LINE
 - ▨ BIO-RETENTION TREATMENT AREA (BTA)
 - OVERLAND RELEASE
- ABBREVIATIONS**
- HP HIGH POINT
 - LP LOW POINT

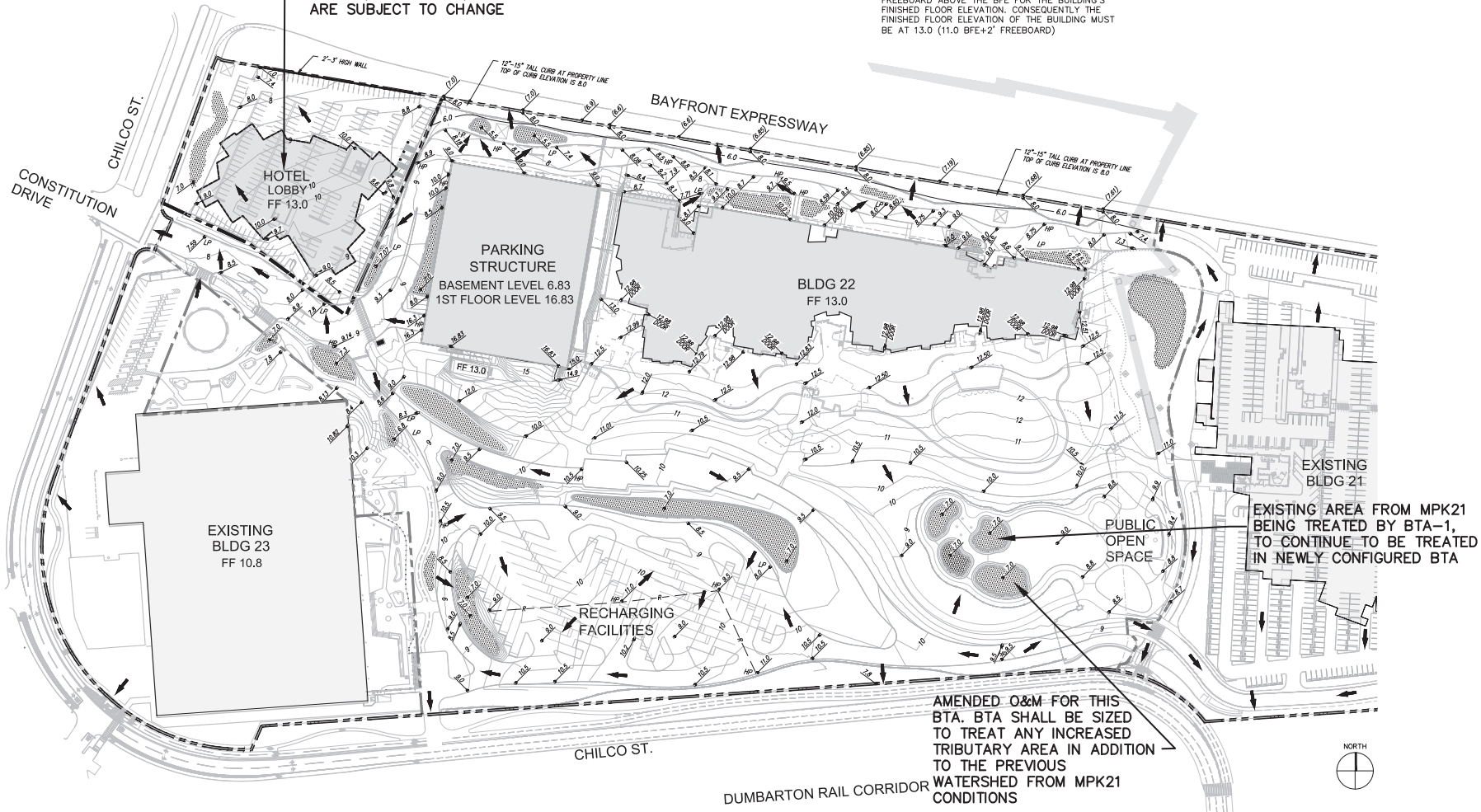
AMENDED O&M FOR THIS BTA. BTA SHALL BE SIZED TO TREAT ANY INCREASED TRIBUTARY AREA IN ADDITION TO THE PREVIOUS WATERSHED FROM MPK21 CONDITIONS

<p>CONTRACTORS</p> <p>KIER & WRIGHT CIVIL ENGINEER 2850 COLLIER CANYON ROAD LIVERMORE, CALIFORNIA 94551 USA TEL: 925-245-8788 FAX: 925-245-8788</p> <p>FACEBOOK OWNER 1 HANOVER WAY MENLO PARK, CALIFORNIA 94025</p>	<p>TITLE</p> <p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p>PROPOSED INTERIM GRADING</p>	<p>PROJECT NUMBER</p> <p>2015-007</p> <p>SCALE</p> <p>1" = 80'</p> <p>9-28-2017</p> <p>11x17 SHEET SIZE: N.T.S. ORIGINAL SHEET SIZE: 24" x 36"</p>	<p>SHEET NUMBER</p> <p>GP-1</p> <p>© 2017 KIER PARTNERS, LLP</p>
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THE GRADING SHOWN ON THE HOTEL SITE IS FOR 'ILLUSTRATIVE PURPOSES' ONLY. THE HOTEL IS STILL UNDER DESIGN AND THE DESIGN SHOWN ON THIS PLAN IS NOT INTENDED TO BE FINAL AS THE DESIGN, LAYOUT, HOTEL SIZE, AND PAVEMENT ELEVATIONS ALL ARE SUBJECT TO CHANGE

NOTE:
 a. FEMA WILL BE ISSUING NEW FLOOD INSURANCE RATE MAPS (FIRMs) BY MID-2017. THE PROJECT MUST COMMENCE CONSTRUCTION PRIOR TO THE ISSUANCE OF THE NEW MAP TO BE GRANDFATHERED INTO THE CURRENT FIRM. PLEASE NOTE THAT THE BFE AT THIS PROPERTY IS EXPECTED TO RISE FROM ELEVATION 10.3 TO ELEVATION 11.0 UPON THE RELEASE OF THE UPCOMING MAP.
 b. EFFECTIVE JANUARY 2017: ALL NEW CONSTRUCTION IN THE M-2 AREA WILL BE SUBJECT TO SE LEVEL RISE (SLR) GUIDELINES. SLR REQUIRES 24" FREEBOARD ABOVE THE BFE FOR THE BUILDING'S FINISHED FLOOR ELEVATION. CONSEQUENTLY THE FINISHED FLOOR ELEVATION OF THE BUILDING MUST BE AT 13.0 (11.0 BFE+2' FREEBOARD)

LEGEND
 - - - - - PROPERTY LINE
 - - - - - PHASE LINE
 [Hatched Area] BIO-RETENTION TREATMENT AREA (BTA)
 [Arrow] OVERLAND RELEASE
 ABBREVIATIONS
 HP HIGH POINT
 LP LOW POINT



AMENDED O&M FOR THIS BTA. BTA SHALL BE SIZED TO TREAT ANY INCREASED TRIBUTARY AREA IN ADDITION TO THE PREVIOUS WATERSHED FROM MPK21 CONDITIONS

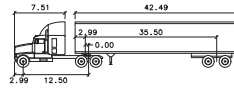
CONSULTANTS
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 LIVERMORE, CALIFORNIA 94551 USA
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 MENLO PARK, CALIFORNIA 94025

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TITLE
 PROPOSED FINAL
 GRADING PLAN

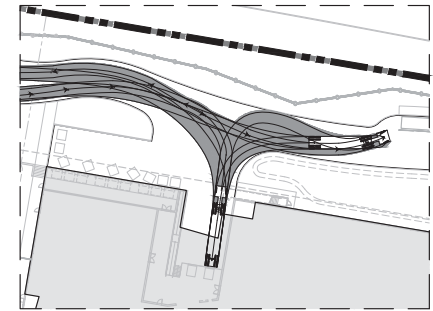
PROJECT NUMBER
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SHEET NUMBER
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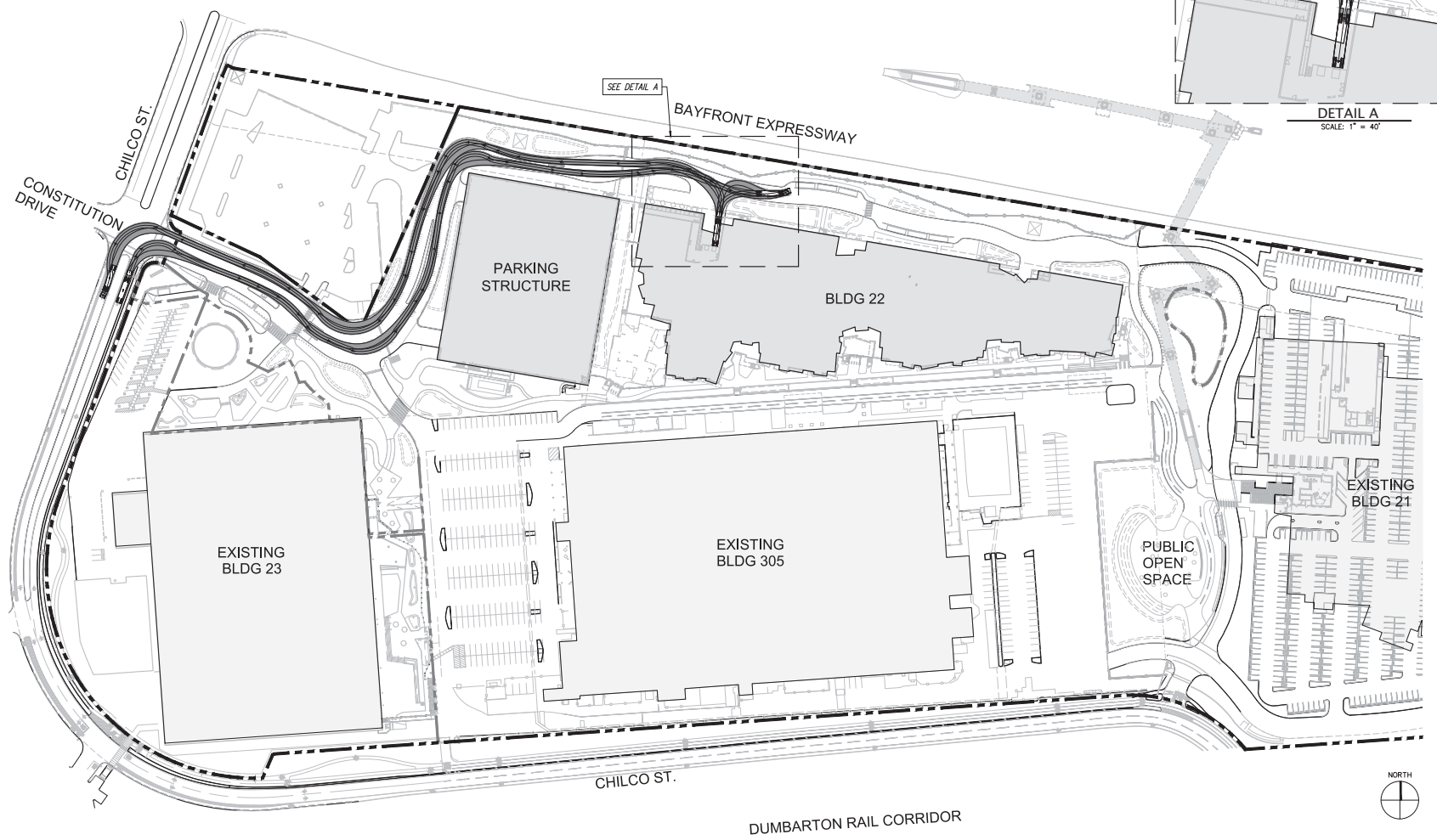


facebook truck wb-15m
feet

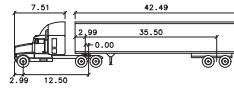
Tractor Width	:8.01	Lock to Lock Time	:8.0
Trailer Width	:8.50	Steering Angle	:15.7
Tractor Track	:8.01	Articulating Angle	:70.0
Trailer Track	:8.50		



DETAIL A
SCALE: 1" = 40'

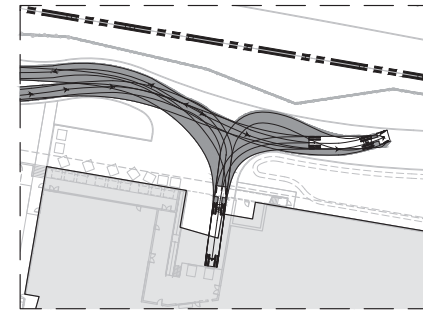


<p>CONSULTANTS</p> <p>KIER & WRIGHT CIVIL ENGINEER 2850 COLLIER CANYON ROAD LIVERMORE, CALIFORNIA 94551 USA TEL: 925-245-8788 FAX: 925-245-8788</p> <p>FACEBOOK OWNER 1 HANOVER WAY MENLO PARK, CALIFORNIA 94025</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p>PROPOSED INTERIM LOADING DOCK DELIVERY PLAN</p>	<p>PROJECT NUMBER 2015-007</p> <p>SCALE 1" = 80'</p> <p>9-28-2017</p> <p>11x17 SHEET SIZE: N.T.S. ORIGINAL SHEET SIZE: 24" x 36"</p>	<p>SHEET NUMBER</p> <p>B-1</p> <p>© 2017 KIER PARTNERS, LLP</p>
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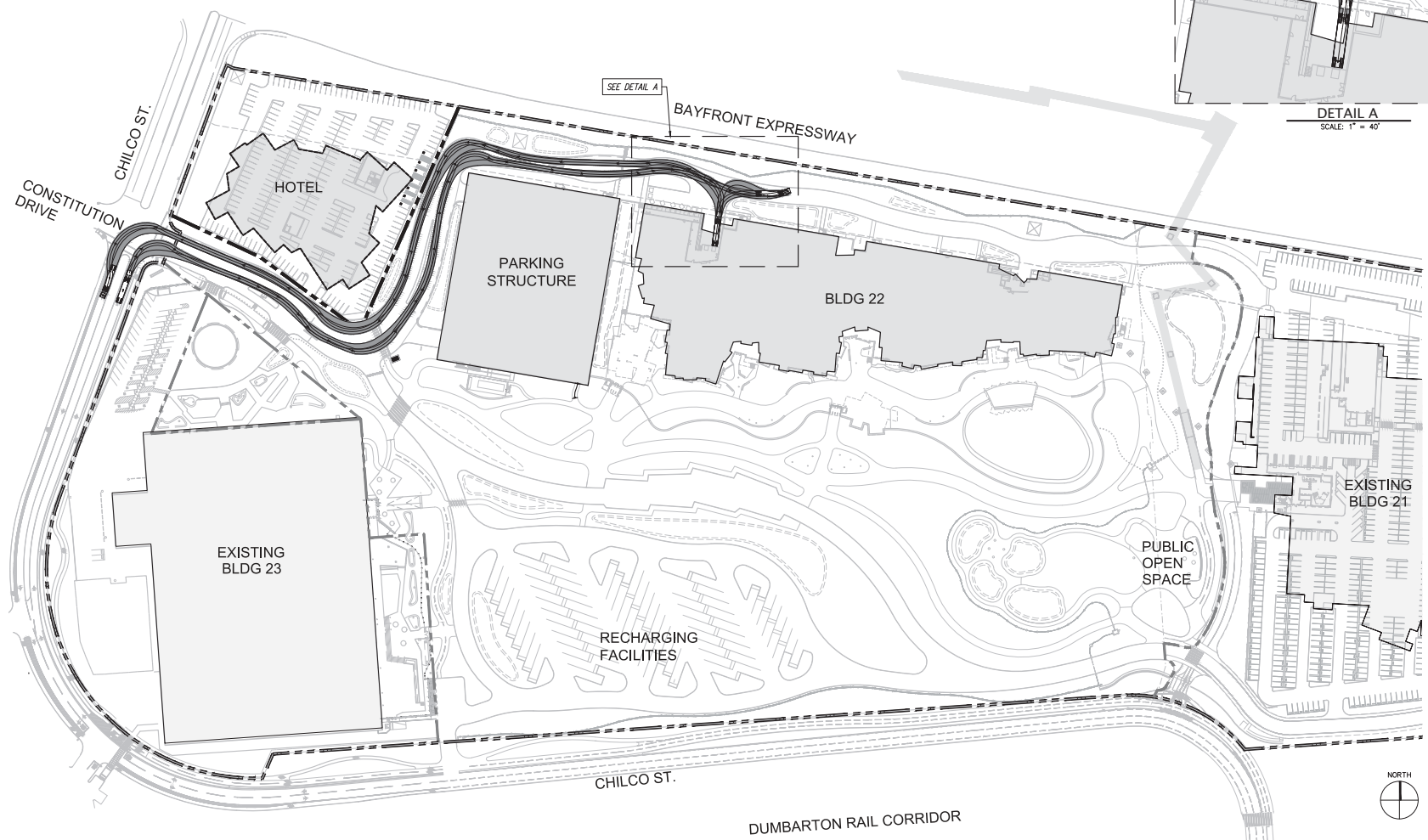


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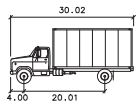
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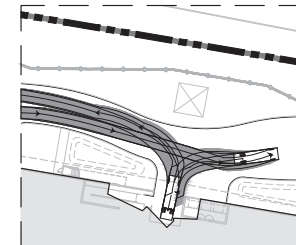
DETAIL A
SCALE: 1" = 40'



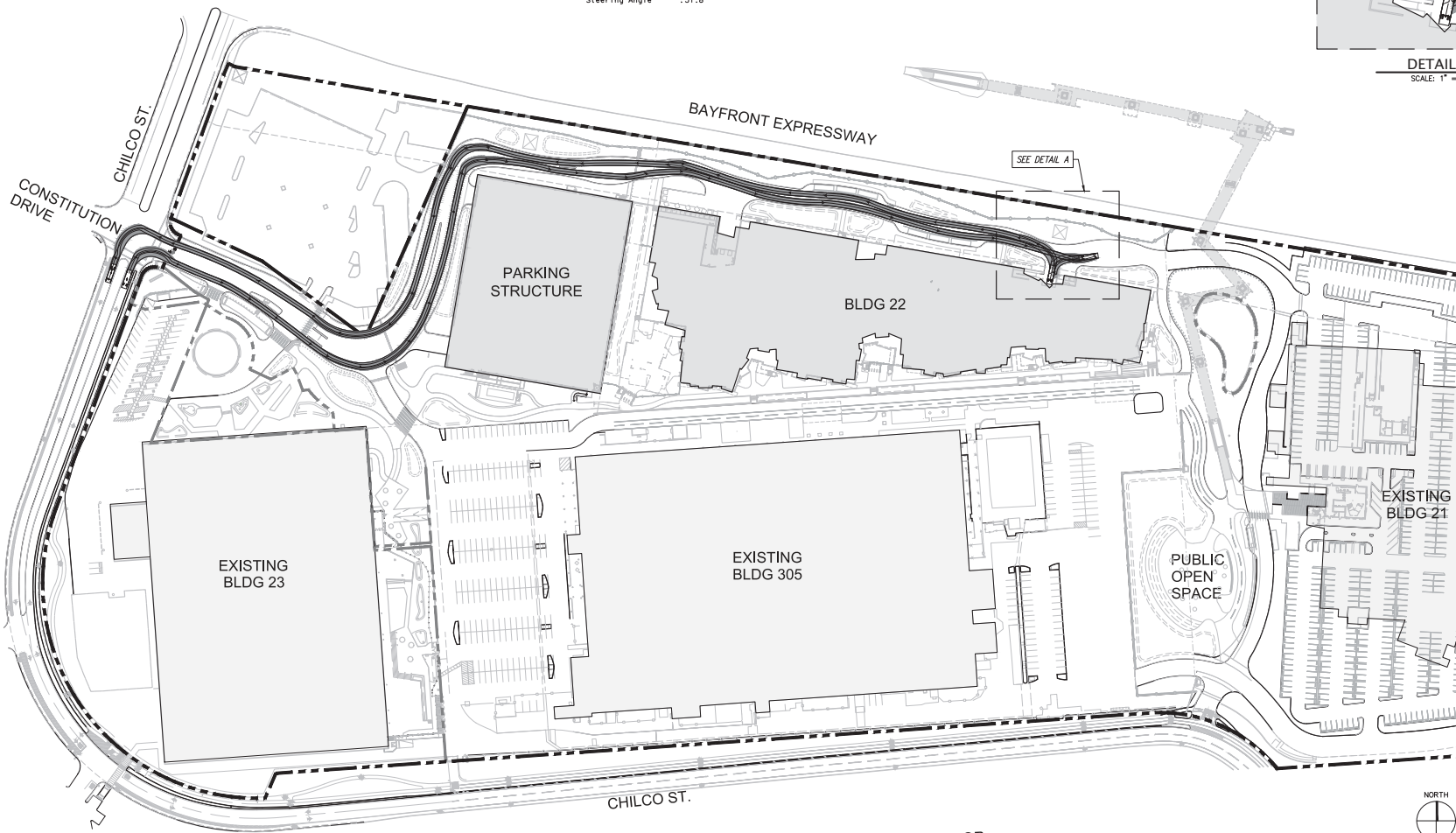
<p>CONSULTANTS</p> <p>KIER & WRIGHT CIVIL ENGINEER 2892 COLLIER CANYON ROAD LIVERMORE, CALIFORNIA 94551 USA TEL: 925-245-8788 FAX: 925-245-8798</p> <p>FACEBOOK OWNER 1 HANOVER WAY MENLO PARK, CALIFORNIA 94025</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p>PROPOSED FINAL LOADING DOCK DELIVERY PLAN</p>	<p>PROJECT NUMBER</p> <p>2015-007</p> <p>SCALE</p> <p>1" = 80'</p> <p>9-28-2017</p> <p>11x17 SHEET SIZE: N.T.S. ORIGINAL SHEET SIZE: 24" x 36"</p>	<p>SHEET NUMBER</p> <p>B-2</p> <p>© 2017 WY PARTNERS, LLP</p>
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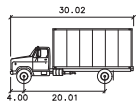
facebook box truck
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 Steering Angle : 31.8



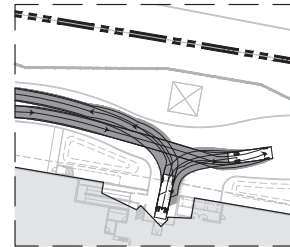
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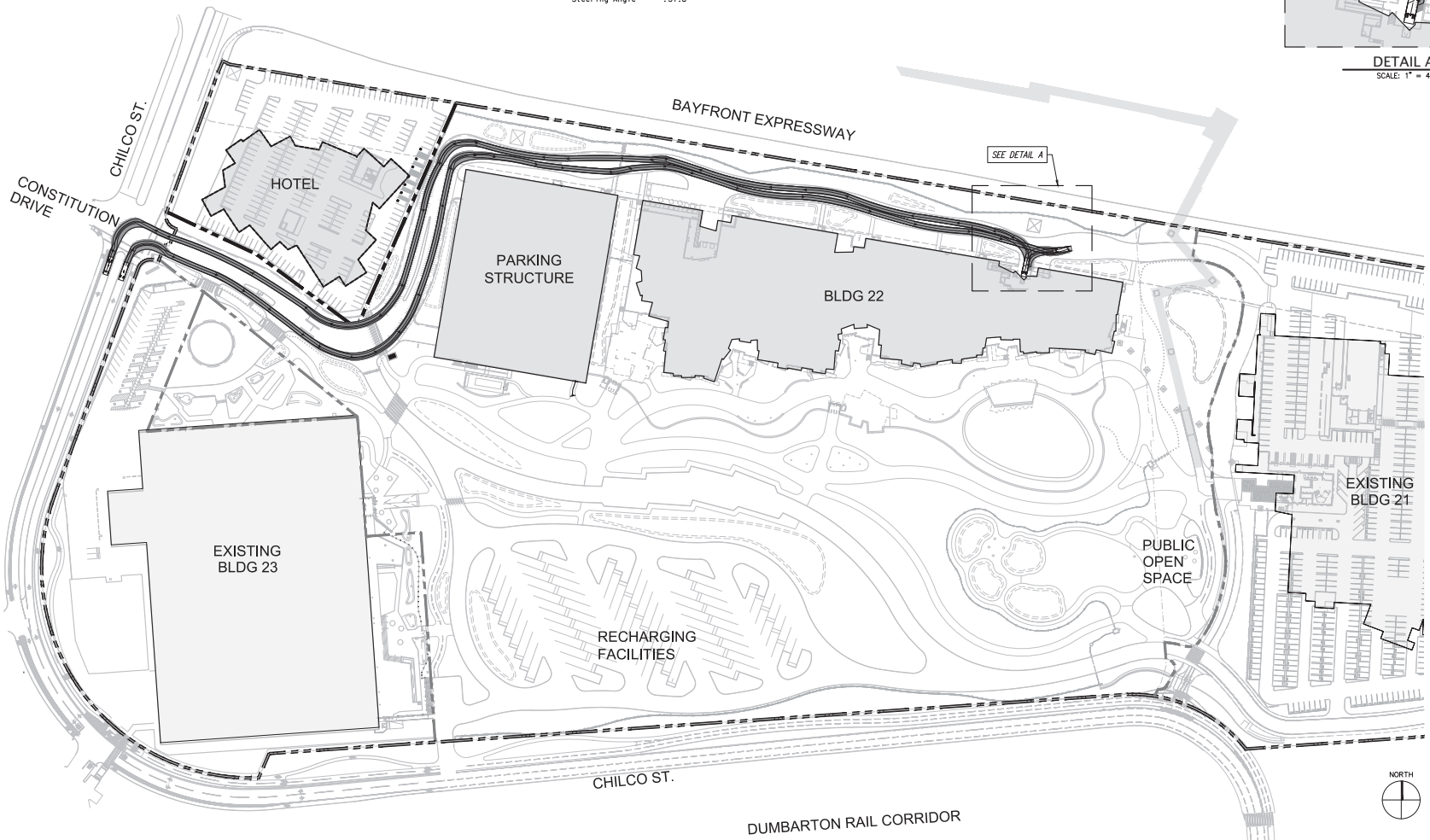
<p>CONSULTANTS</p> <p>KIER & WRIGHT CIVIL ENGINEER 2850 COLLIER CANYON ROAD LIVERMORE, CALIFORNIA 94551 USA TEL: 925-245-8788 FAX: 925-245-8798</p> <p>FACEBOOK OWNER 1 HANOVER WAY MENLO PARK, CALIFORNIA 94025</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p>PROPOSED INTERIM LOADING DOCK DELIVERY PLAN</p>	<p>PROJECT NUMBER 2015-007</p> <p>SCALE 1" = 80'</p> <p>9-28-2017</p> <p>11x17 SHEET SIZE: N.T.S. ORIGINAL SHEET SIZE: 24" x 36"</p>	<p>SHEET NUMBER</p> <p>B-3</p> <p>© 2017 KIER PARTNERS, LLP</p>
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facebook box truck
 feet
 Width : 8.01
 Track : 8.01
 Lock to Lock Time : 6.0
 Steering Angle : 31.8



DETAIL A
 SCALE: 1" = 40'



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CONSULTANTS

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

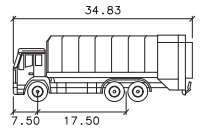
TITLE
**PROPOSED FINAL
 LOADING DOCK DELIVERY PLAN**

PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

B-4

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GARBAGE TRUCK FACEBOOK

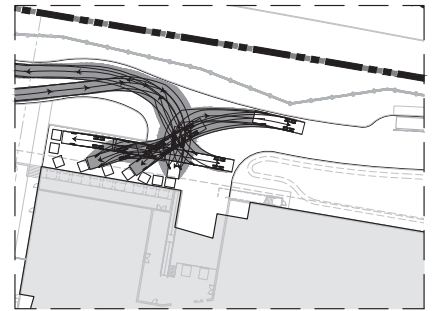
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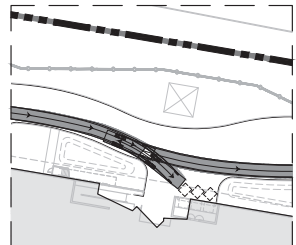
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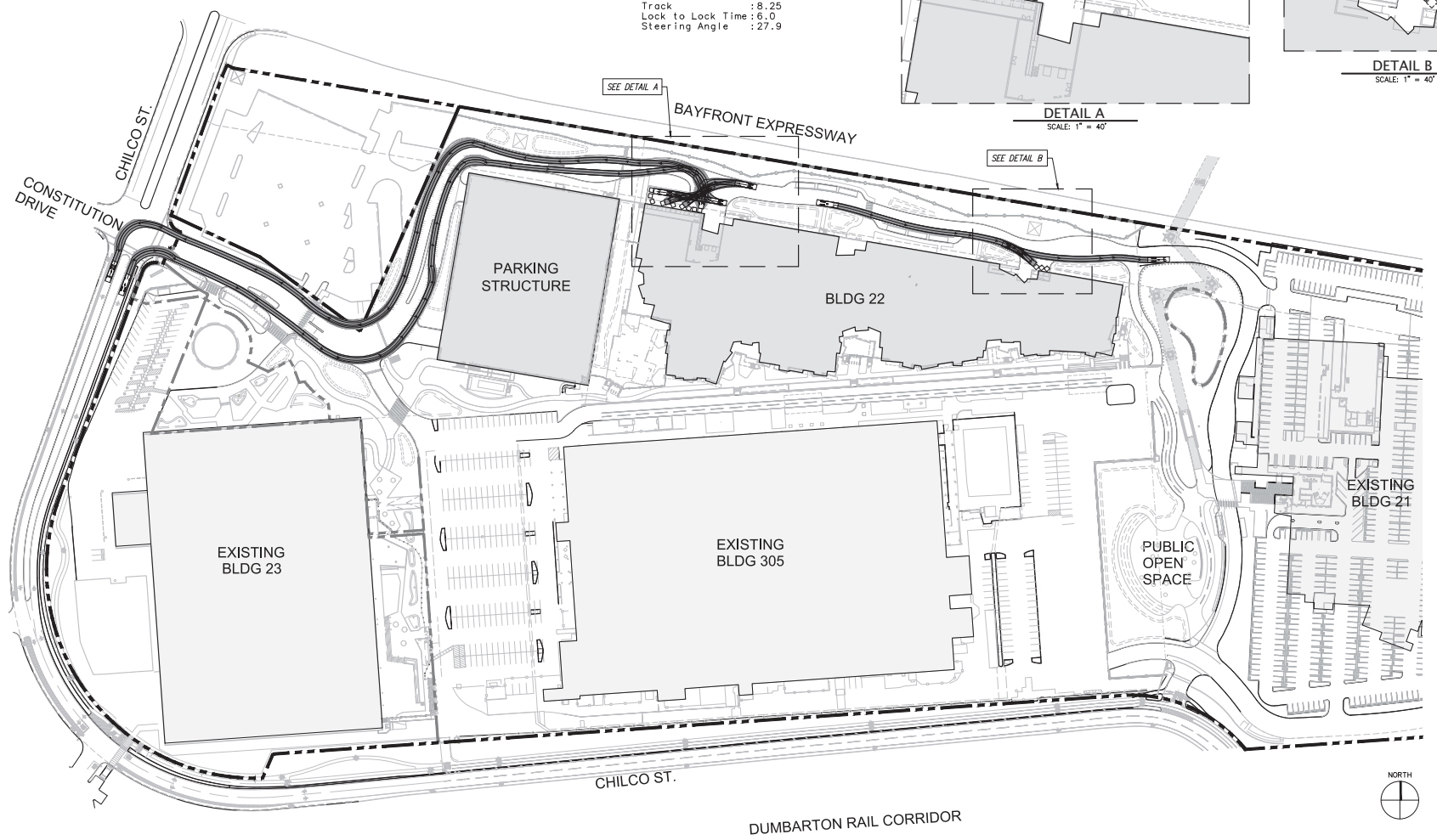
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DETAIL A
SCALE: 1" = 40'



DETAIL B
SCALE: 1" = 40'



CONSULTANTS

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MENLO PARK, CALIFORNIA 94025

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TITLE

PROPOSED INTERIM
GARBAGE TRUCK

PROJECT NUMBER
2015-007

SCALE
1" = 80'

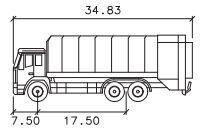
9-28-2017

11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

GT-1

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GARBAGE TRUCK FACEBOOK

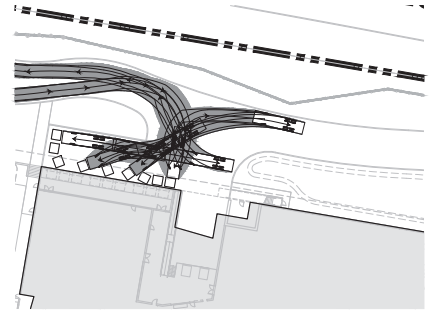
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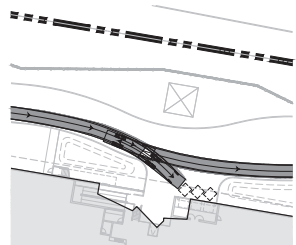
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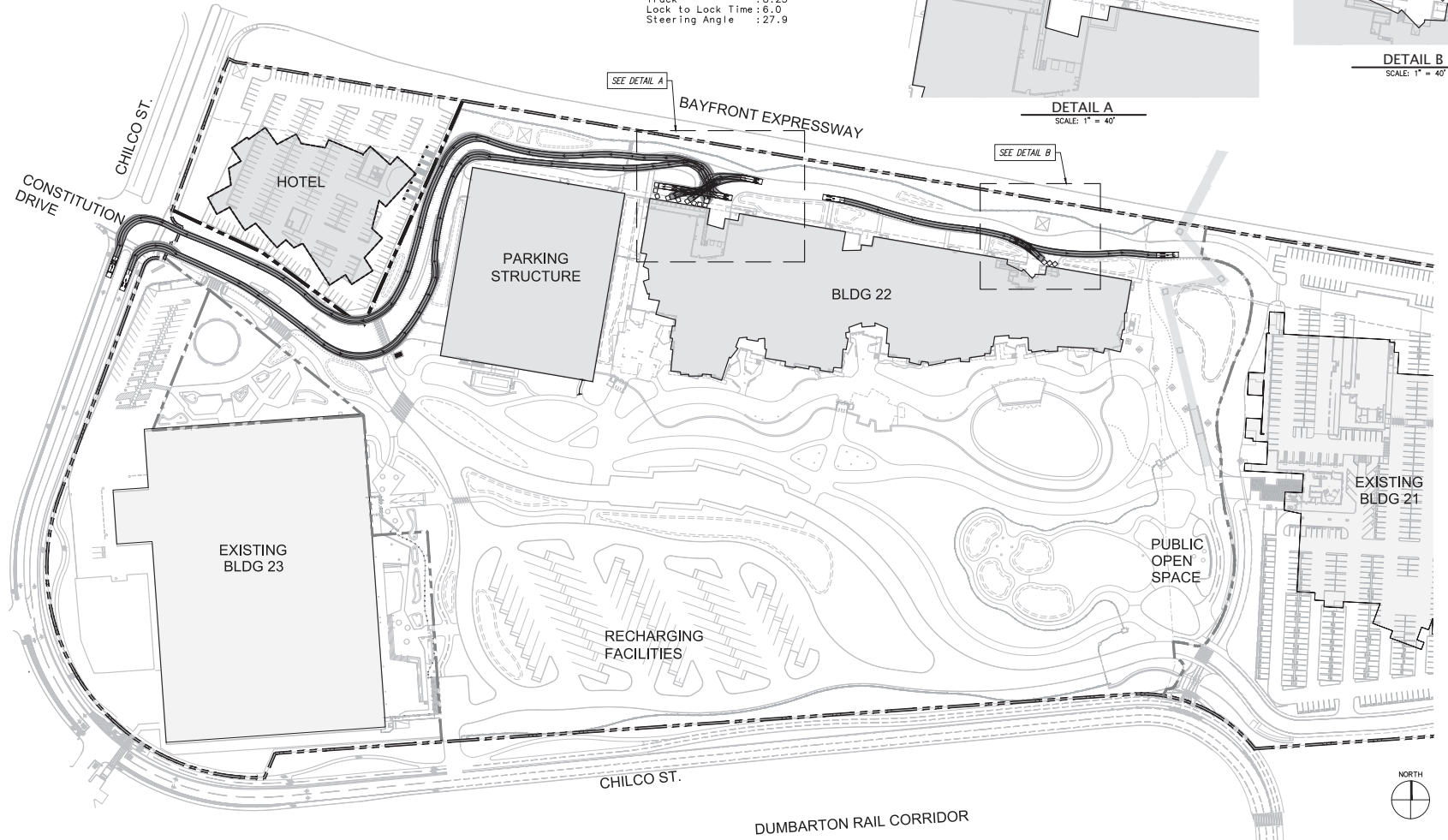
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DETAIL A
SCALE: 1" = 40'



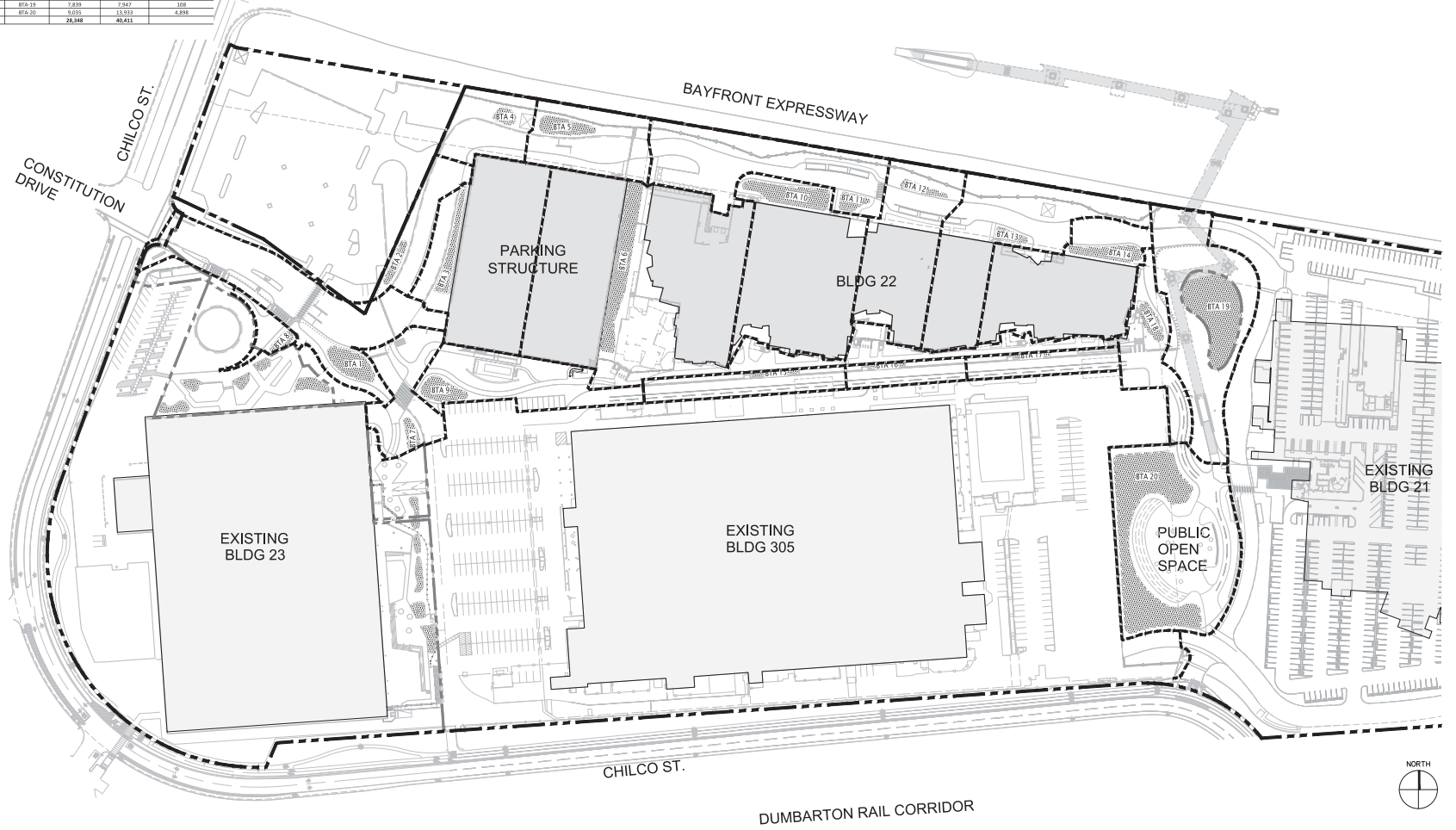
DETAIL B
SCALE: 1" = 40'



<p>CONSULTANTS</p> <p>KIER & WRIGHT CIVIL ENGINEER 2892 COLLIER CANYON ROAD LIVERMORE, CALIFORNIA 94551 USA TEL: 925-245-8788 FAX: 925-245-8798</p> <p>FACEBOOK OWNER 1 HANOVER WAY MENLO PARK, CALIFORNIA 94025</p>	<p>Facebook Campus Expansion Facebook Building 21, 22 & Hotel Site 300-309 Constitution Drive</p>	<p>TITLE</p> <p>PROPOSED FINAL GARBAGE TRUCK</p>	<p>PROJECT NUMBER</p> <p>2015-007</p> <p>SCALE</p> <p>1" = 80'</p> <p>9-28-2017</p> <p>11x17 SHEET SIZE: N.T.S. ORIGINAL SHEET SIZE: 24" x 36"</p>	<p>SHEET NUMBER</p> <p>GT-2</p> <p>© 2017 KIER PARTNERS, LLP</p>
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CATCHMENT NUMBER	CATCHMENT AREA (SF)	SITE PERVIOUS IMPERVIOUS AREA (SF)	ROOF IMPERVIOUS AREA (SF)	BIORETENTION TREATMENT AREA NUMBER	BIORETENTION TREATMENT AREA REQUIRED (SF)	BIORETENTION TREATMENT AREA PROVIDED (SF)	BIORETENTION TREATMENT AREA DIFFERENCE (SF)
CA-1	48,661	1,817	21,943	0	0	1,817	0
CA-2	11,806	3,487	7,840	0	0	3,487	387
CA-3	46,711	2,421	4,783	0	0	2,421	462
CA-4	13,918	2,177	5,720	0	0	2,177	828
CA-5	10,532	11,114	2,192	0	0	11,114	998
CA-6	115,403	22,248	22,135	0	0	22,135	664
CA-7	8,136	4,380	3,790	0	0	4,380	39
CA-8	4,244	1,761	2,018	0	0	1,761	251
CA-9	23,319	15,567	6,877	0	0	15,567	641
CA-10	47,874	3,205	445	0	0	3,205	1,113
CA-11	12,072	9,327	21,651	0	0	9,327	23
CA-12	11,737	8,472	4,905	0	0	8,472	2
CA-13	10,721	17,872	7,480	0	0	17,872	47
CA-14	38,797	2,201	0	0	0	2,201	308
CA-15	13,736	2,809	8,196	0	0	2,809	49
CA-16	7,275	1,851	1,280	0	0	1,851	19
CA-17	8,105	2,122	4,435	0	0	2,122	8
CA-18	15,754	4,970	9,612	0	0	4,970	560
CA-19*	412,479	100,850	110,540	0	0	100,850	308
CA-20**	490,516	101,000	189,412	0	0	101,000	4,898
TOTAL	1,344,503	341,000	438,973	0	0	438,973	28,348

**CA-20 includes 352,358 SF existing area from MP# 21.



LEGEND
 BIORETENTION PLANTER

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Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive


TITLE
**PROPOSED INTERIM
 STORM WATER QUALITY**

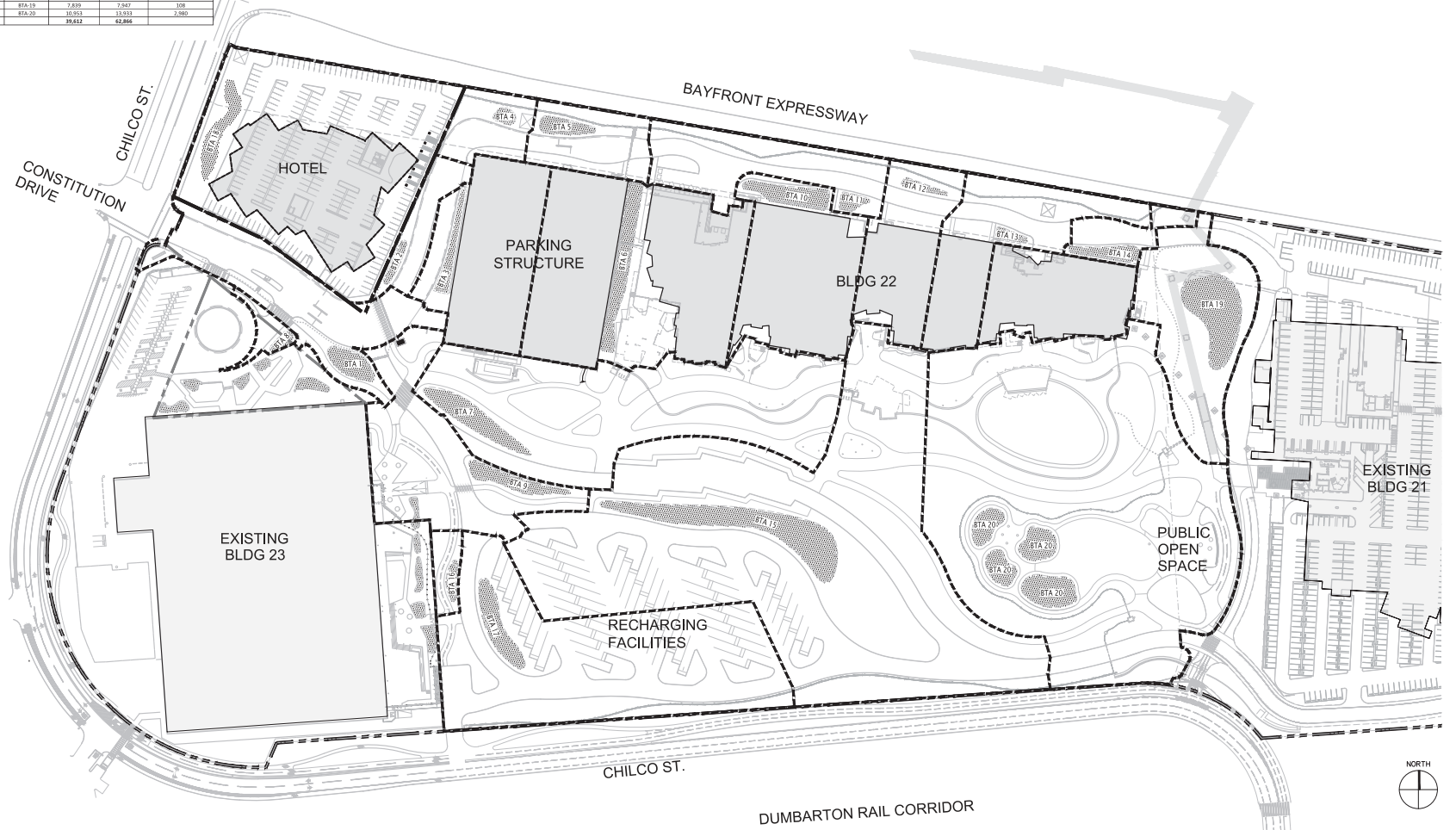
PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
SWQ-1
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CATCHMENT NUMBER	CATCHMENT AREA (SF)	SITE PERVIOUS IMPERVIOUS AREA (SF)	SITE IMPERVIOUS AREA (SF)	ROOF IMPERVIOUS AREA (SF)	BIORETENTION TREATMENT AREA NUMBER	BIORETENTION TREATMENT AREA REQUIRED (SF)	BIORETENTION TREATMENT AREA PROVIDED (SF)	BIORETENTION TREATMENT AREA DIFFERENCE (SF)
CA-1	48,645	3,317	21,943	0	BT-1	1,354	1,354	0
CA-2	11,896	3,131	8,333	0	BT-2	260	412	152
CA-3	46,711	2,211	4,783	27,850	BT-3	1,266	1,266	0
CA-4	13,918	2,177	5,720	0	BT-4	103	1,021	918
CA-5	19,557	11,133	7,397	0	BT-5	249	1,347	1,098
CA-6	39,395	6,957	39,821	73,237	BT-6	2,481	4,120	1,639
CA-7	189,315	61,352	39,080	0	BT-7	1,396	4,884	3,487
CA-8	4,184	1,364	1,638	0	BT-8	83	344	261
CA-9	50,319	24,747	24,109	0	BT-9	797	1,874	1,077
CA-10	47,818	8,706	48	41,639	BT-10	1,177	1,585	408
CA-11	15,572	3,377	23,651	0	BT-11	500	500	0
CA-12	31,737	8,472	4,925	13,728	BT-12	618	612	6
CA-13	29,172	17,827	7,406	19,395	BT-13	944	960	16
CA-14	18,797	1,091	0	15,479	BT-14	1,269	1,277	8
CA-15	736,610	107,173	119,131	0	BT-15	3,915	10,917	7,002
CA-16	5,467	3,364	1,279	0	BT-16	62	474	412
CA-17	101,465	32,081	67,545	0	BT-17	2,125	2,334	209
CA-18	111,922	15,951	50,277	0	BT-18	2,809	3,500	691
CA-19*	418,288	127,431	118,786	164,134	BT-19	7,839	2,847	5,000
CA-20**	271,792	216,460	702,085	151,489	BT-20	10,953	13,833	2,880
TOTAL	2,029,914	665,493	734,031	587,824		39,612	82,866	43,254

*CA-19 includes 312,314 SF existing area from MPK 11.
 **CA-20 includes 351,738 SF existing area from MPK 11.

LEGEND
 BIO-RETENTION PLANTER



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CONSULTANTS

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
 PROPOSED FINAL
 STORM WATER QUALITY

PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
 SWQ-2
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MPK-21 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	353,307		353,307	
Pervious Landscape	178,301	178,301		
Total Site Level	531,608	178,301	353,307	66%
Roof and Terrace Level				
Impervious Paving	319,439		319,439	
Pervious Softscape	113,800	113,800		
Total Roof and Terrace Levels	433,239	113,800	319,439	74%
MPK-21 TOTAL	964,847	292,101	672,746	70%

Note: The total area for MPK21 decreases from the existing phase to the interim phase due to the western portion of the MPK21 site being redone as part of MPK22.

MPK-22 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	624,266		624,266	
Pervious Softscape	119,904	119,904		
Total Site Level	744,170	119,904	624,266	84%
Roof and Terrace Level				
Impervious Paving	368,804		368,804	
Total Roof and Terrace Levels	368,804	0	368,804	100%
MPK-22 TOTAL	1,112,974	119,904	993,070	89%

Note: The total area for MPK22 increases from the existing phase to the interim phase due to the western portion of the MPK21 site being redone as part of MPK22.

MPK-23 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	120,024		120,024	
Pervious Softscape	53,114	53,114		
Total Site Level	173,138	53,114	120,024	69%
Roof Level				
Impervious Paving	179,810		179,810	
Total Roof Level	179,810	0	179,810	100%
MPK-23 TOTAL	352,948	53,114	299,834	85%

*PG&E SUB STATION =16,503 SQ.FT

HOTEL STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	82,673		82,673	
Pervious Softscape	31,034	31,034		
Total Site Level	113,707	31,034	82,673	73%
Roof and Terrace Level				
Impervious Paving	0		0	
Total Roof and Terrace Levels	0	0	0	0%
HOTEL TOTAL	113,707	31,034	82,673	73%

TE SITE PLAN TOTAL	2,544,476	496,153	2,048,323	81%
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Note: The total area of MPK21, 22, 23 and the hotel has increased from 2,539,928 SF to 2,544,476 SF since the last EIR submission. The area shown hereon is based on a field survey as it relates to the found and accepted monumentation which does not fit with the record parcel dimensions for the subject property (as documented on the previous EIR). A Record of Survey has been filed with the County of San Mateo pending recordation to show our findings.

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MENLO PARK, CALIFORNIA 94025

CONSULTANTS

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TITLE
IMPERVIOUS/PERVIOUS CALCULATIONS
APPROVED PROJECT

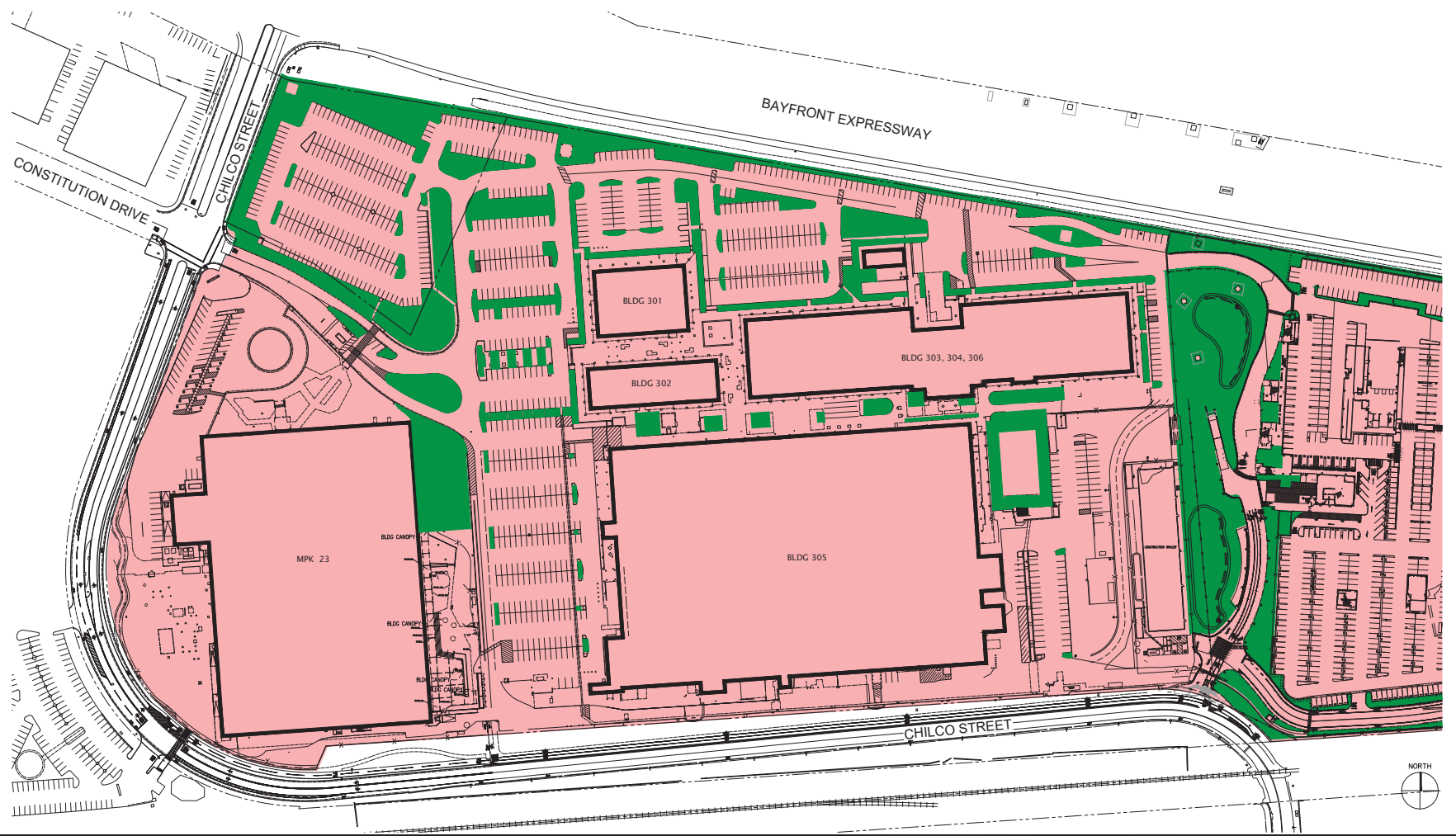
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2015-007
SCALE
N/A
9-28-2017
11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

G-1

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- LEGEND
- PROPERTY LINE
 - █ PERVIOUS SOFTSCAPE
 - █ IMPERVIOUS AREA



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CONSULTANTS

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

TITLE
IMPERVIOUS/PERVIOUS PLAN
APPROVED PROJECT

PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
G-1.1
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MPK-21 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	346,243		346,243	
Pervious Landscape	110,952	110,952		
Total Site Level	457,195	110,952	346,243	76%
Roof and Terrace Level				
Impervious Paving	319,439		319,439	
Pervious Softscape	113,800	113,800		
Total Roof and Terrace Levels	433,239	113,800	319,439	74%
MPK-21 TOTAL	890,434	224,752	665,682	75%

Note: The total area for MPK21 decreases from the existing phase to the interim phase due to the western portion of the MPK21 site being redone as part of MPK22.

MPK-22 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	480,185		480,185	
Pervious Softscape	239,345	239,345		
Total Site Level	719,530	239,345	480,185	67%
Roof and Terrace Level				
Impervious Paving	467,857		467,857	
Total Roof and Terrace Levels	467,857	0	467,857	100%
MPK-22 TOTAL	1,187,387	239,345	948,042	80%

Note: The calculations for MPK22 in the interim stage include existing building 305 and adjacent parking lot along with the new parking garage and MPK22 office building.

Note: The total area for MPK22 increases from the existing phase to the interim phase due to the western portion of the MPK21 site being redone as part of MPK22.

MPK-23 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	120,024		120,024	
Pervious Softscape	53,114	53,114		
Total Site Level	173,138	53,114	120,024	69%
Roof Level				
Impervious Paving	179,810		179,810	
Total Roof Level	179,810	0	179,810	100%
MPK-23 TOTAL	352,948	53,114	299,834	85%

*PG&E SUB STATION =16,503 SQ.FT

HOTEL STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	82,673		82,673	
Pervious Softscape	31,034	31,034		
Total Site Level	113,707	31,034	82,673	73%
Roof and Terrace Level				
Impervious Paving	0		0	
Total Roof and Terrace Levels	0	0	0	0%
HOTEL TOTAL	113,707	31,034	82,673	73%

TE SITE PLAN TOTAL	2,544,476	548,245	1,996,231	78%
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Note: The total area of MPK21, 22, 23 and the hotel has increased from 2,539,928 SF to 2,544,476 SF since the last EIR submission. The area shown hereon is based on a field survey as it relates to the found and accepted monumentation which does not fit with the record parcel dimensions for the subject property (as documented on the previous EIR). A Record of Survey has been filed with the County of San Mateo pending recordation to show our findings.

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 LIVERMORE, CALIFORNIA 94551 USA
 TEL: 925-245-8788
 FAX: 925-245-8788
FACEBOOK
 OWNER
 1 HENDER WAY
 MENLO PARK, CALIFORNIA 94025

CONSULTANTS

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

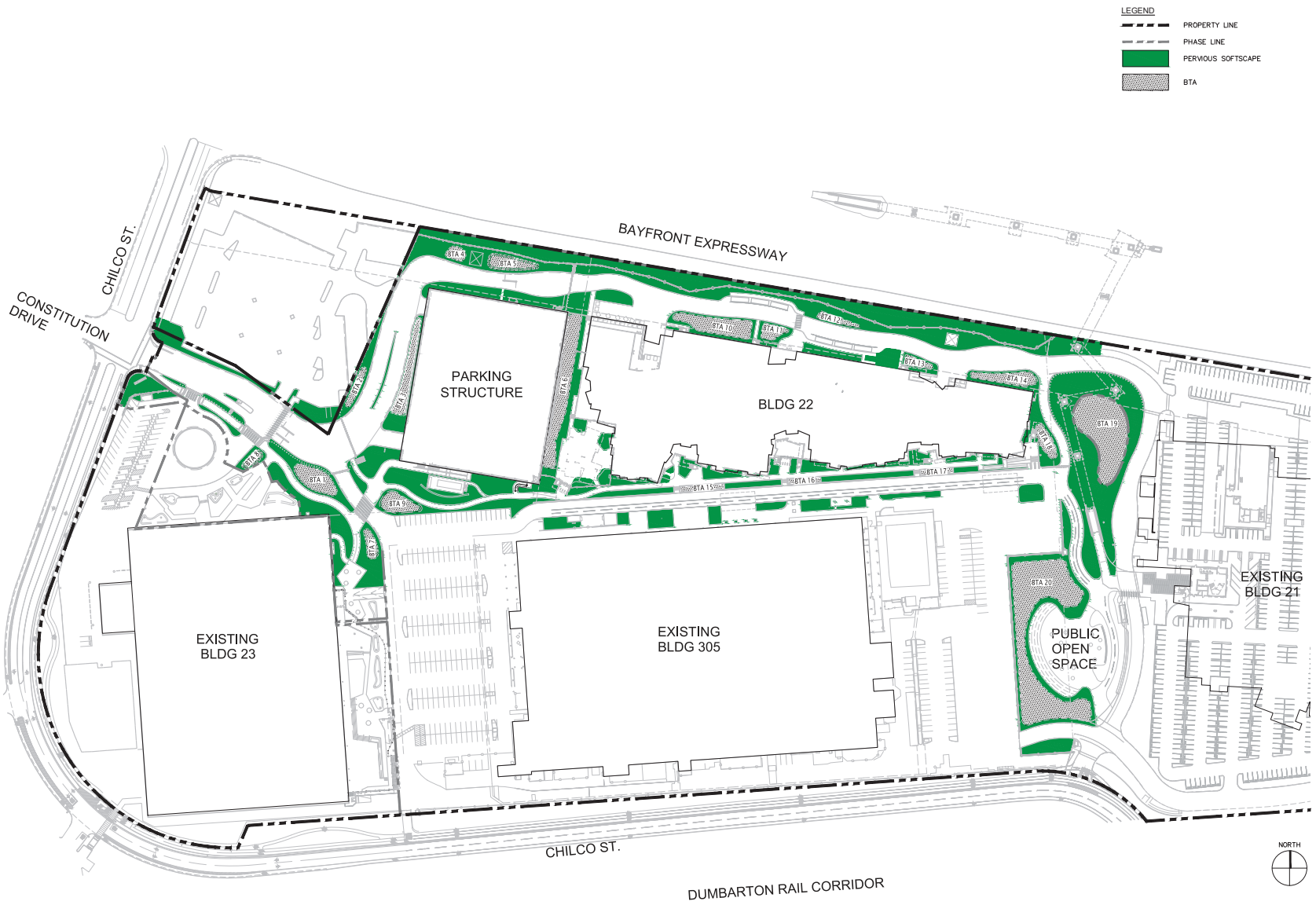
TITLE
IMPERVIOUS/PERVIOUS CALCULATIONS
INTERIM CONDITION

PROJECT NUMBER
 2015-007
 SCALE
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 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

G-2

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LEGEND

	PROPERTY LINE
	PHASE LINE
	PERVIOUS SOFTSCAPE
	BTA

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 300-309 Constitution Drive

IMPERVIOUS/PERVIOUS PLAN
 INTERIM CONDITION

PROJECT NUMBER
 2015-007
SCALE
 1" = 80'
 9-28-2017
 11x17 SHEET SIZE: N.T.S.
 ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER
 G-2.1
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MPK-21 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	346,243		346,243	
Pervious Landscape	110,952	110,952		
Total Site Level	457,195	110,952	346,243	76%
Roof and Terrace Level				
Impervious Paving	319,439		319,439	
Pervious Softscape	113,800	113,800		
Total Roof and Terrace Levels	433,239	113,800	319,439	74%
MPK-21 TOTAL	890,434	224,752	665,682	75%

MPK-23 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	120,024		120,024	
Pervious Softscape	53,114	53,114		
Total Site Level	173,138	53,114	120,024	69%
Roof Level				
Impervious Paving	179,810		179,810	
Total Roof Level	179,810	0	179,810	100%
MPK-23 TOTAL	352,948	53,114	299,834	85%

*PG&E SUB STATION =16,503 SQ.FT

MPK-22 STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	442,535		442,535	
Pervious Softscape	526,457	526,457		
Total Site Level	968,992	526,457	442,535	46%
Roof and Terrace Level				
Impervious Paving	218,395		218,395	
Total Roof and Terrace Levels	218,395	0	218,395	100%
MPK-22 TOTAL	1,187,387	526,457	660,930	56%

HOTEL STORMWATER TREATMENT ANALYSIS				
Site Level				
TYPE	TOTAL AREA (SQ.FT.)	PERVIOUS (SQ. FT.)	IMPERVIOUS SURFACE (SQ.FT.)	% IMPERVIOUS
Impervious Paving	51,777		51,777	
Pervious Softscape	17,407	17,407		
Total Site Level	69,184	17,407	51,777	75%
Roof and Terrace Level				
Impervious Paving	44,523		44,523	
Total Roof and Terrace Levels	44,523	0	44,523	0%
HOTEL TOTAL	113,707	17,407	96,300	85%

TE SITE PLAN TOTAL	2,544,476	821,730	1,722,746	68%
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Note: The total area of MPK21, 22, 23 and the hotel has increased from 2,539,928 SF to 2,544,476 SF since the last EIR submission. The area shown hereon is based on a field survey as it relates to the found and accepted monumentation which does not fit with the record parcel dimensions for the subject property (as documented on the previous EIR). A Record of Survey has been filed with the County of San Mateo pending recordation to show our findings.

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CONSULTANTS

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 300-309 Constitution Drive

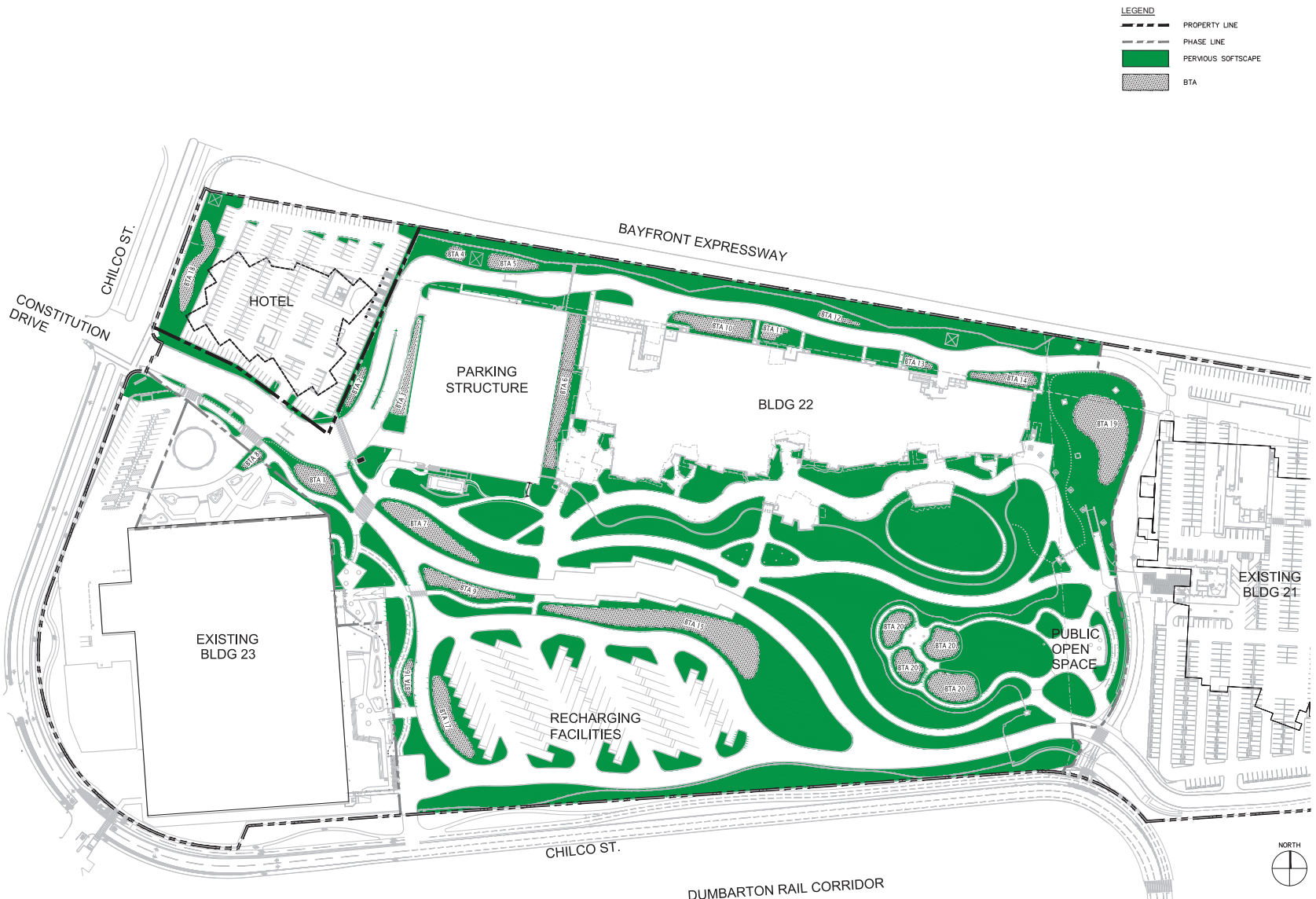
TITLE
 IMPERVIOUS/PERVIOUS CALCULATIONS
 FINAL CONDITION

PROJECT NUMBER
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 SCALE
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	PHASE LINE
	PERVIOUS SOFTSCAPE
	BTA

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 FINAL CONDITION

PROJECT NUMBER
 2015-007
 SCALE
 1" = 80'
 9-28-2017
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CONSTRUCTION



MEMO

Date: August 15, 2017
 Project: Facebook TE Campus
 Project No. 17-1042
 To: Judith Mussel
 From: Sam Nelson
 Subject: TE Campus Generator Summary

Distribution: PAE File

This memo is intended to provide an updated response to Item M1 of the Proposed Revised Project Entitlements Data Needs Matrix pertaining to stationary combustion sources.

Data Needs Matrix, Item M1

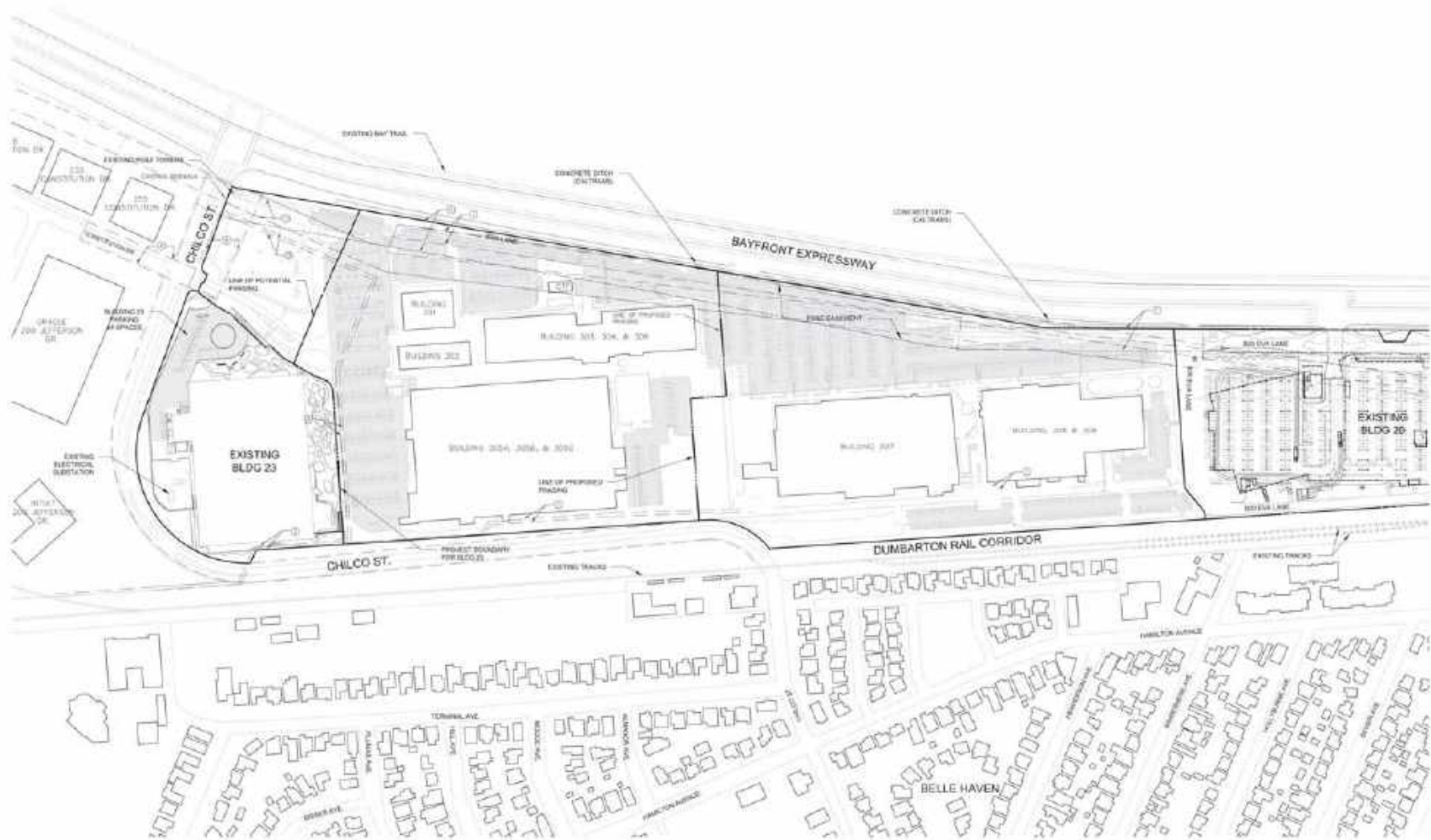
Will there be stationary combustion sources (e.g. emergency generators, diesel-powered fire pumps)? If so, please provide the types, their horsepower, their hours of operation (hours per day and days per year), what fuel they will burn (gasoline, diesel, propane, natural gas, biofuel, etc.), and their location.

Response

Yes, stationary combustion sources anticipated are diesel generators used for emergency and standby electrical power as follows:

Facebook TE Campus Generator Summary			
	MPK21	MPK22/Garage	Hotel
Generator Quantity	2	1	1
Rated Standby Capacity	(1) 500kW & (1) 750kW	2,000kW	500kW
Stack Height	46 in	15 in	46 in
Fuel Type	Diesel	Diesel	Diesel
Fuel Capacity	300 gallons & 660 gallons	4,000 gallons	300 gallons
Fuel Consumption Rate (100% load)	35 gal/hr & 51 gal/hr	148 gal/hr	35 gal/hr
Stack Diameter	8 in & 10 in	16 in	8 in
Exit Gas Flow Rate	3625cfm & 5358cfm	16,103cfm	3625cfm
Exit Gas Temp	901°F & 888°F	896°F	901°F
Engine Horsepower	755 HP & 1,220 HP	3,058 HP	755 HP
Engine Make/Model	Cummins DFEK & DQCB	MTU 744R554292	Cummins DFEK
Annual Hours of Operation	12 hours	12 hours	12 hours
EPA Certification	Stationary Emergency Application (Tier 2)	Stationary Emergency Application (Tier 2)	Stationary Emergency Application (Tier 2)
Emission Test Data (grams per kW-hour)	CO: 0.6 & 0.4 Nox + HC: 5.7 & 5.7 PM: 0.13 & 0.13	CO: 0.6 Nox + HC: 7.2 PM: 0.05	CO: 0.6 Nox + HC: 5.7 PM: 0.13
Exhaust Modifications	None	None	None

EXISTING CONDITIONS



ASSESSOR'S PARCEL NUMBER 055-260-250



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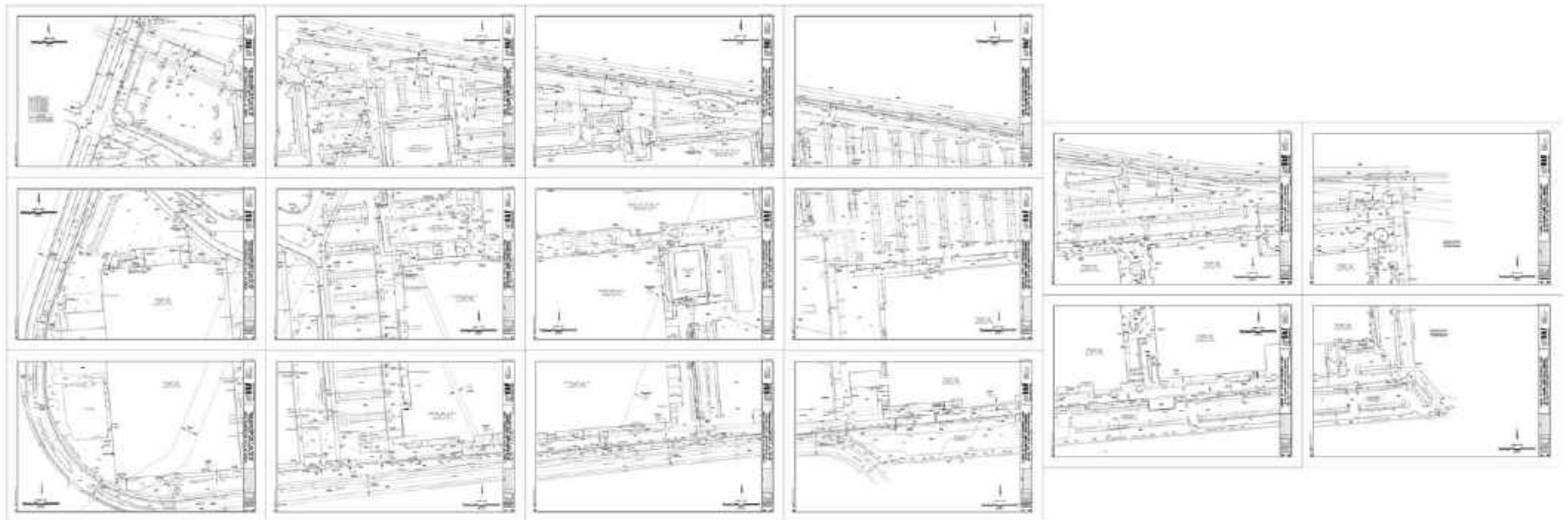
PROJECT SITE EXISTING BUILDINGS

PROJECT NUMBER
 2016-007
 SHEET

DATE: 08/11/16
 DRAWN: SUTY
 CHECKED: SUTY

SHEET NUMBER
E1-01

8/11/2016



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 (314) 241-2300

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 17000 HWY
 REDWOOD CITY, CALIFORNIA 94065

DATE PLOTTED

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ALTA SURVEY

PROJECT NUMBER
 2016-007
 SHEET

HEET NUMBER

E1-02

PLOT SCALE: 1" = 40'
 (SEE SHEET E1-01 FOR 1" = 20')

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KEY:

- OFFICE
- R&D
- MANUFACTURING
- WAREHOUSE

Note: The uses shown in the plan refer to Level 1 of each respective building.

EXISTING BUILDING NUMBER	TOTAL FLOOR AREA	OFFICE	R&D	MANUFACTURING	WAREHOUSE	TOTAL FLOOR AREA	
						EXISTING	PROPOSED
BUILDING 21	100,000	100,000	0	0	0	0	0
BUILDING 22	150,000	150,000	0	0	0	0	0
BUILDING 23	100,000	100,000	0	0	0	0	0
BUILDING 24	100,000	100,000	0	0	0	0	0
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BUILDING 99	100,000	100,000	0	0	0	0	0
BUILDING 100	100,000	100,000	0	0	0	0	0

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FACEBOOK
 OWNER
 100 W. WALTON STREET
 CHICAGO, ILLINOIS 60601

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

EXISTING LAND USE
 DIAGRAM / AREAS OF
 USE / CTF BUILDING
 LOCATION

PROJECT NUMBER
 2016-007
 SHEET
 1 OF 1 SHEET(S)
 SHEET NUMBER
 21 - 01

E2-01



- KEY:**
- OFFICE
 - R&D
 - MANUFACTURING
 - WAREHOUSE

LEVEL 1
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3141 WATSON STREET
SAN FRANCISCO, CALIFORNIA 94109
(415) 774-2300

FACEBOOK
OWNER
1600 WAY
MENLO PARK, CALIFORNIA 94025

DATE: 07/2017

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

EXISTING BLDG 23

PROJECT NUMBER
2016-007
0100

DATE: 07/2017

E2-02

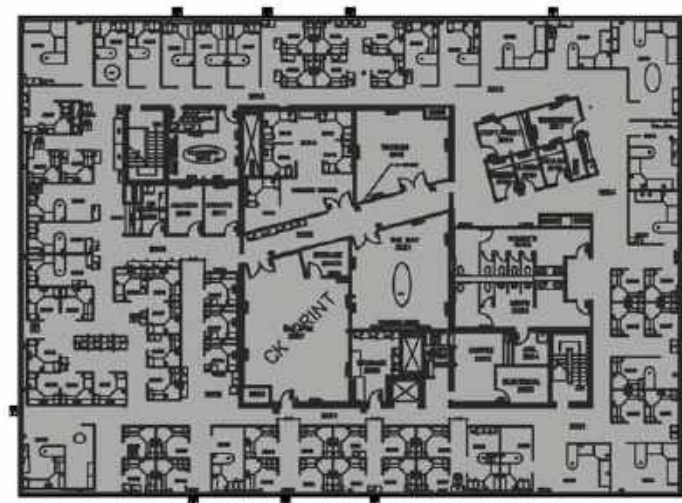
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DATE: 07/2017

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LEVEL 1



LEVEL 2

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 10000 AVENUE
 10000 AVENUE, SAN FRANCISCO, CA 94104

00000000

Facebook Campus Expansion
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EXISTING BLDG 301

2016-017
 2016-017
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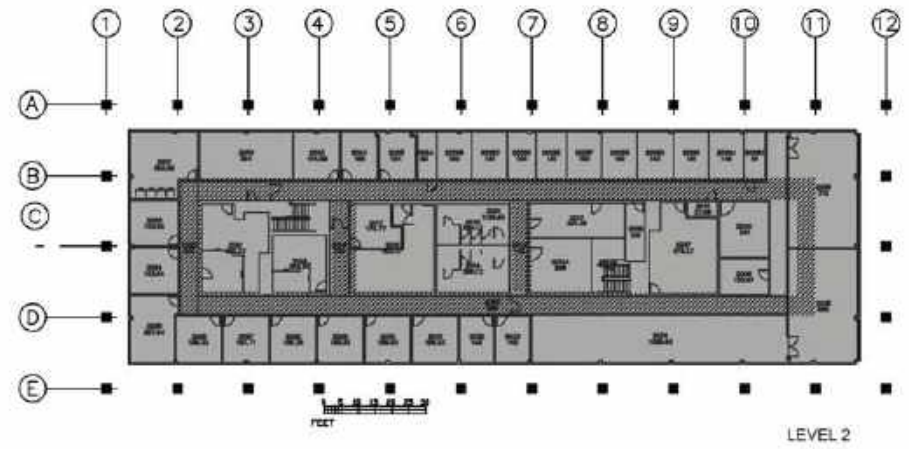
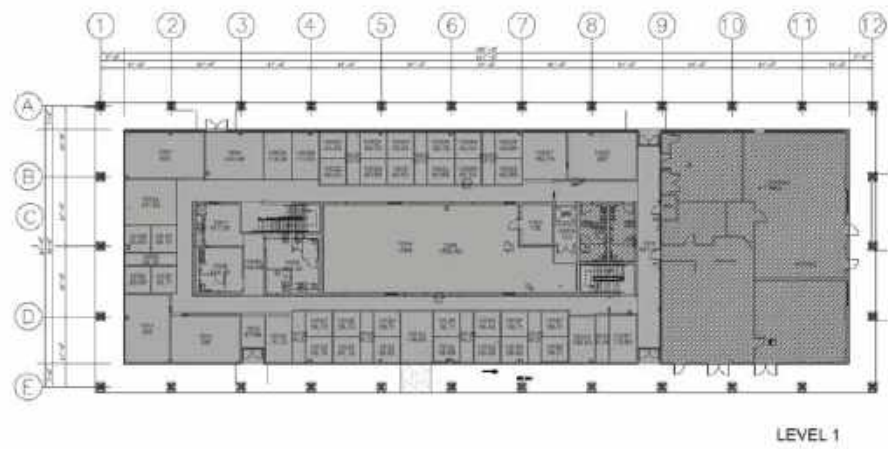
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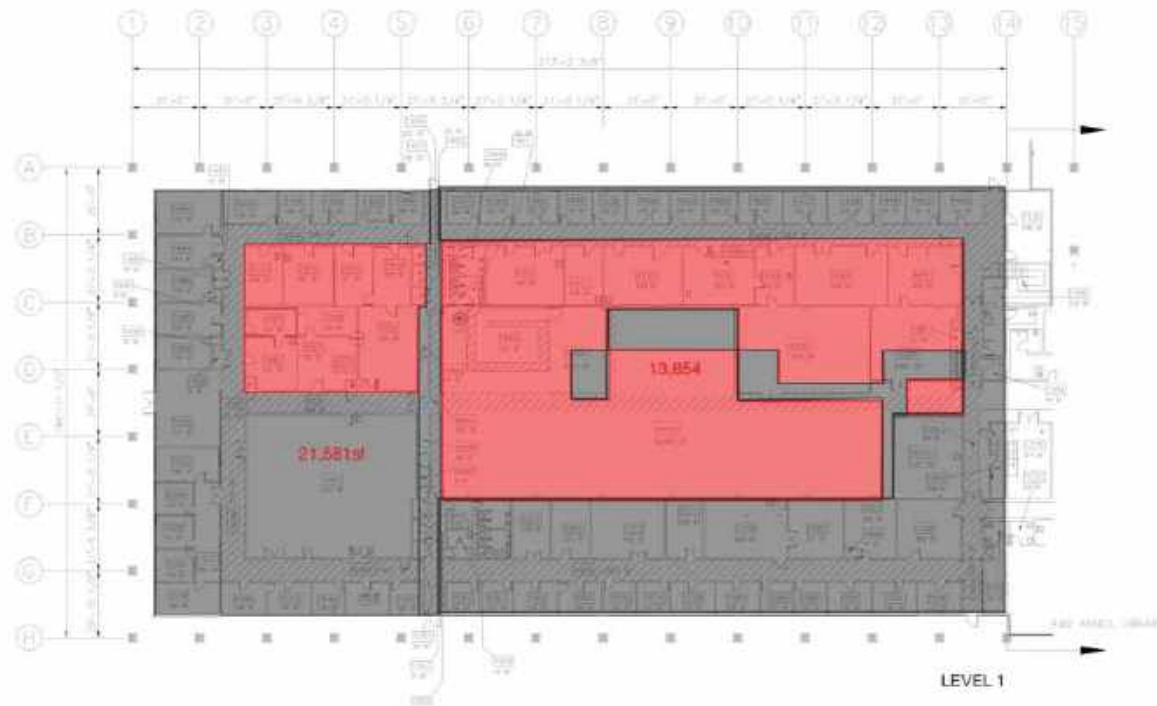
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HEET NUMBER
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PROJECT NUMBER
 2016-007

HEET NUMBER
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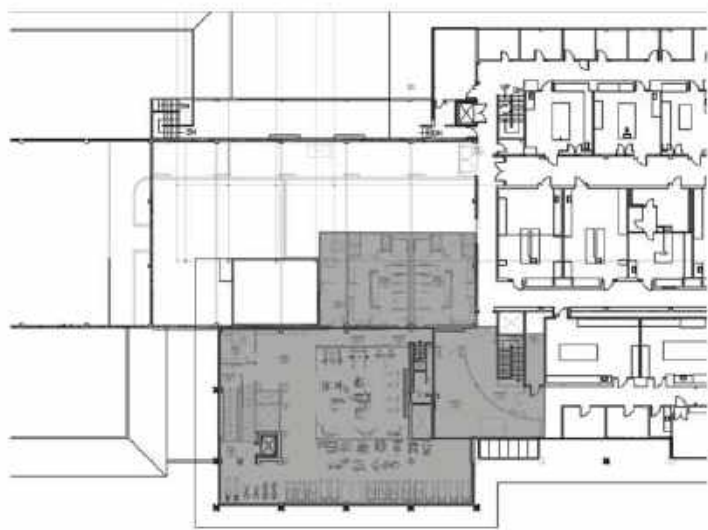
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DATE PRINTED: 6/19/16

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 - WAREHOUSE



LEVEL 1



LEVEL 2



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 REDMOND, WA 98072-4099

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EXISTING BLDG 304

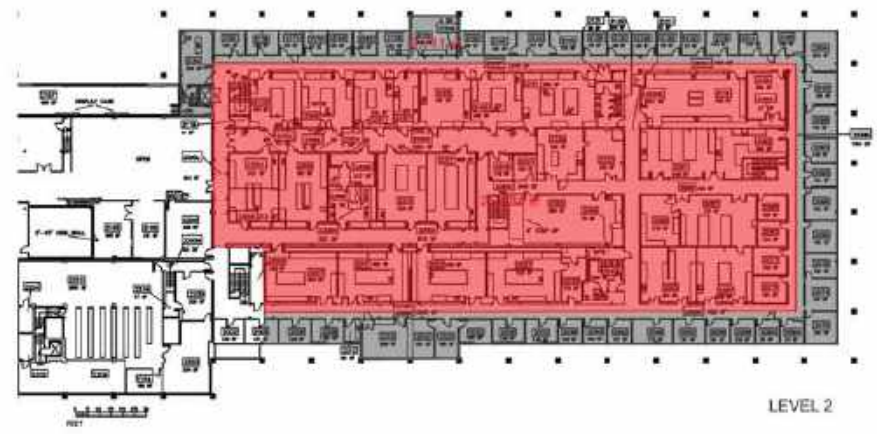
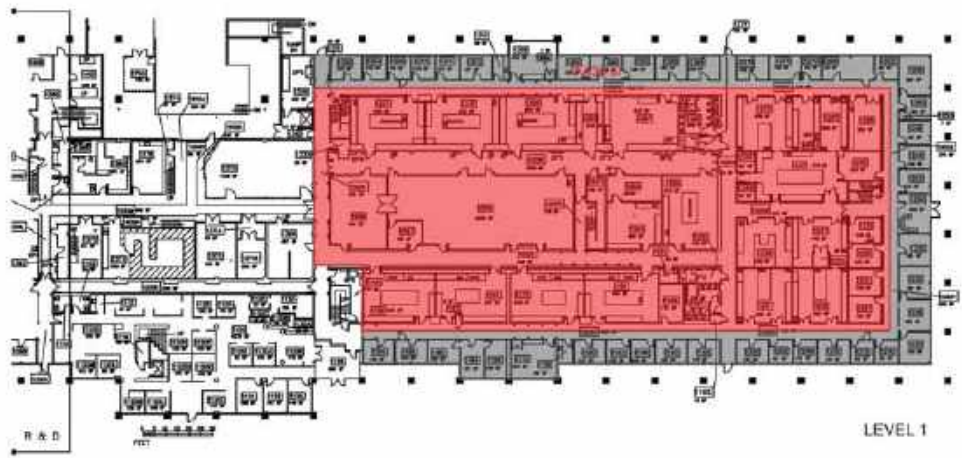
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 2016-017
 01/12

DATE PLOTTED: 01/12
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HEET NUMBER
 E2-06

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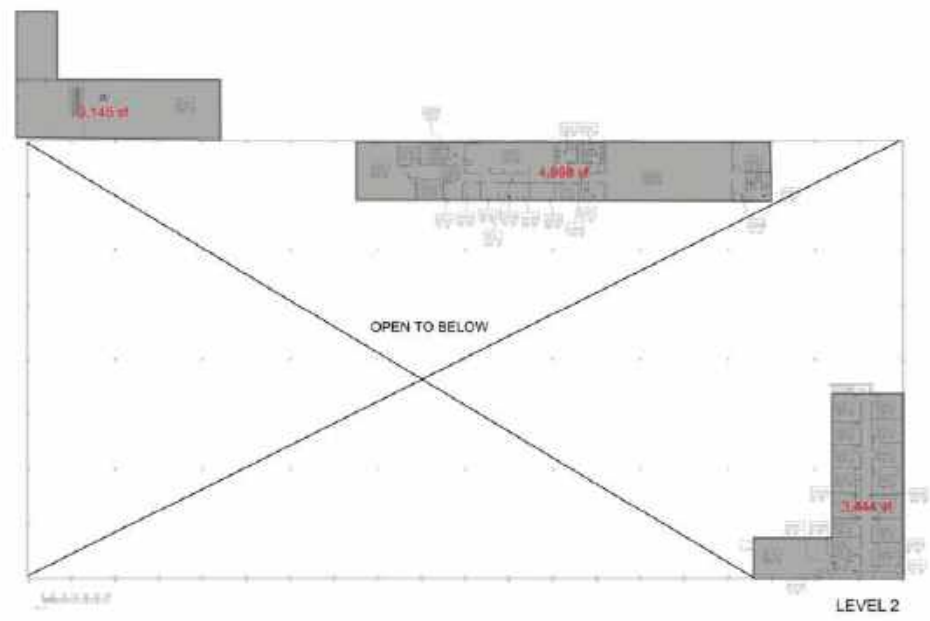
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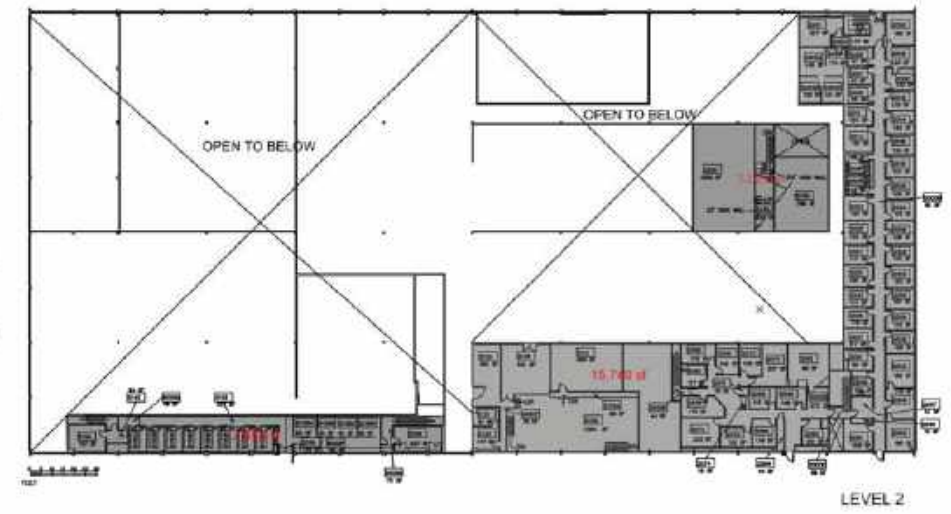
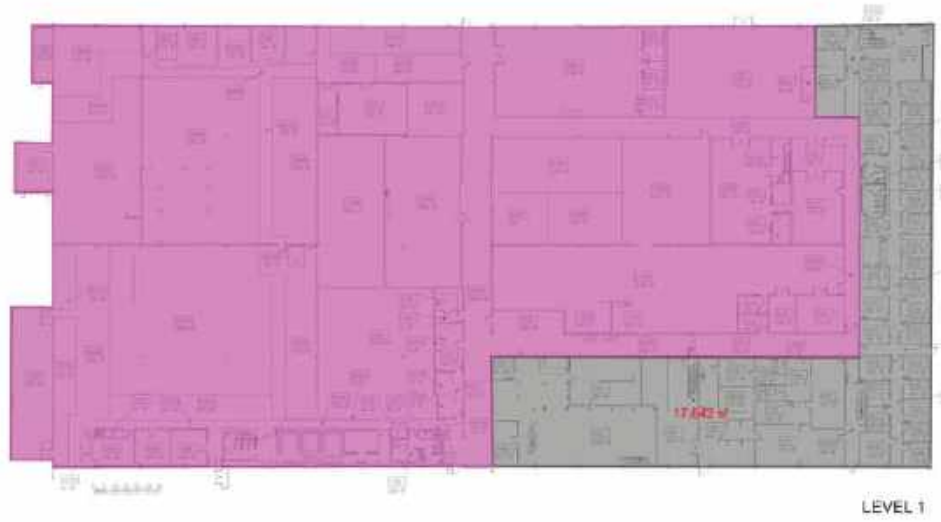
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 - WAREHOUSE



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- KEY:**
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LOS ANGELES, CALIFORNIA 90058
(213) 467-2500

FACEBOOK
OWNER
17000 WIL
MIRALOMA, CALIFORNIA 94025

Facebook Campus Expansion
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300-309 Constitution Drive

EXISTING BLDG 305 B

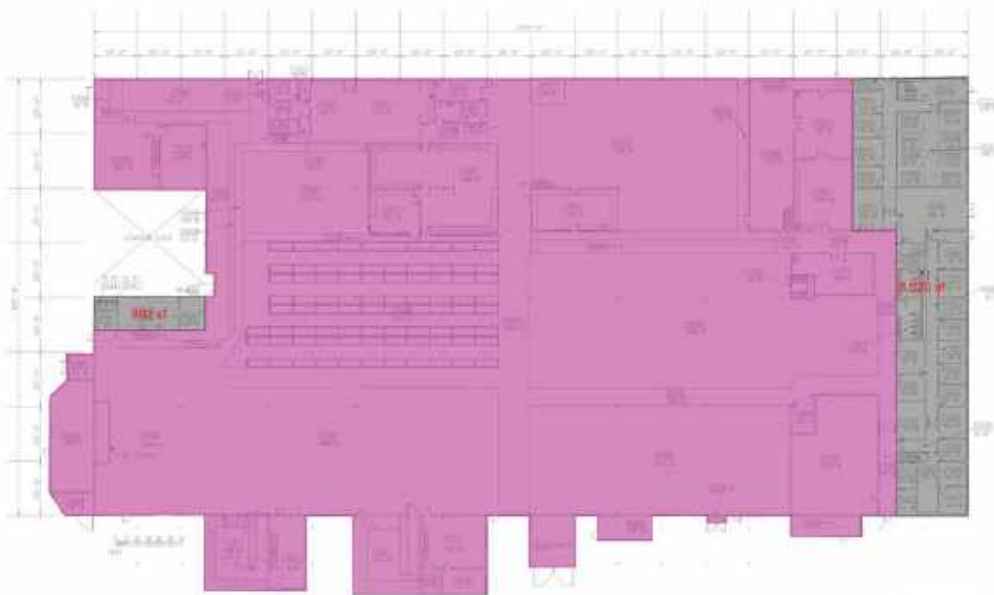
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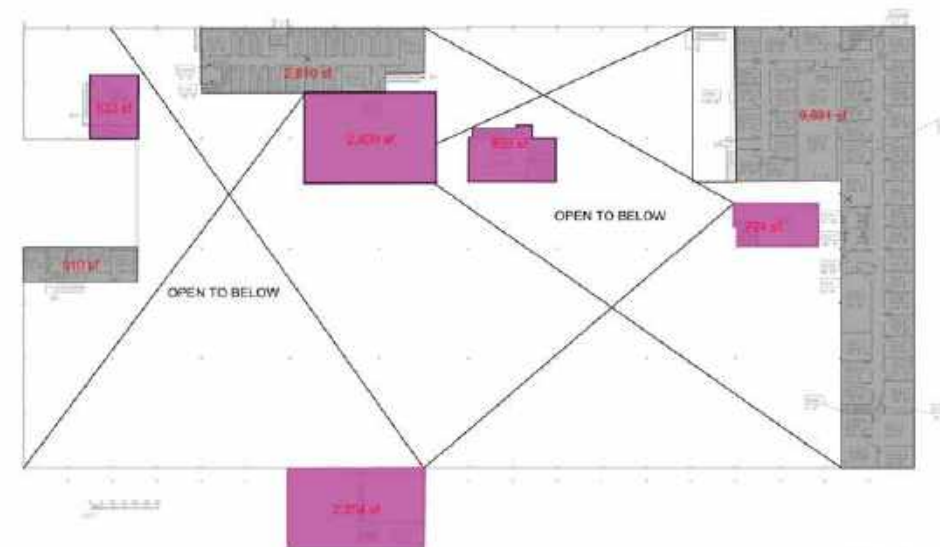
HEET NUMBER
E2-09

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- KEY:**
- OFFICE
 - R&D
 - MANUFACTURING
 - WAREHOUSE



LEVEL 1



LEVEL 2

FOR REFERENCE ONLY - HAS BEEN SUBMITTED PREVIOUSLY

GEHRY PARTNERS, LLP
 ARCHITECT
 1045 WASHINGTON STREET
 SUITE 2000, SAN FRANCISCO, CA 94108
 TEL: 415.774.8000
 WWW: GHPARTNERS.COM

DATE: 10/15/16

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

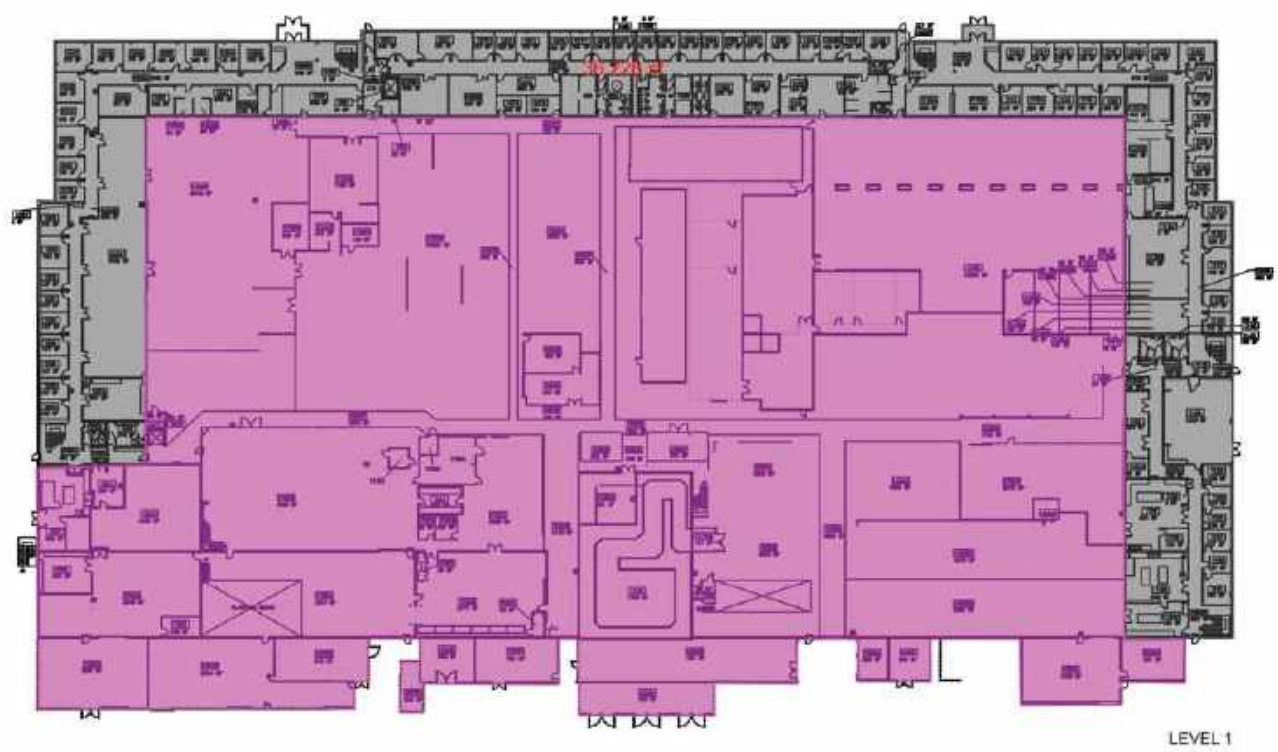
EXISTING BLDG 305 C

PROJECT NUMBER
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 DRAWN: SGT/STP/STP/STP

SHEET NUMBER
E2-10
 E2-10 (10/15/16)

KEY:

- OFFICE
- R&D
- MANUFACTURING
- WAREHOUSE



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GENRY PARTNERS, LLP.
ARCHITECT
224 WATSON STREET
LOS ANGELES, CALIFORNIA 90008
(310) 261-2500

FACEBOOK
OWNER
17000 W 11
MILPITAS, CALIFORNIA 95035

DATE: 01/11/17

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

EXISTING BLDG 307

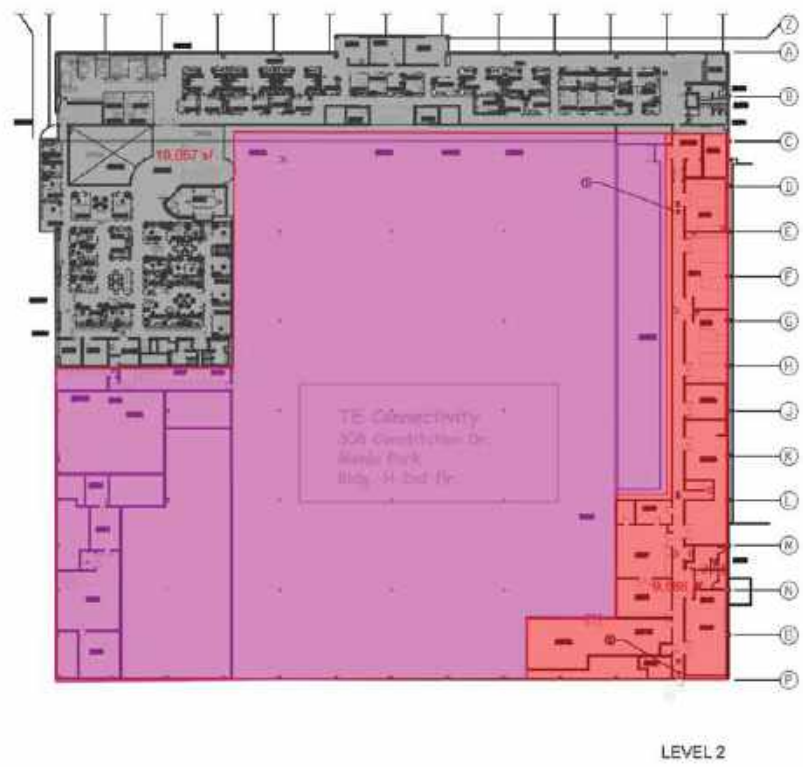
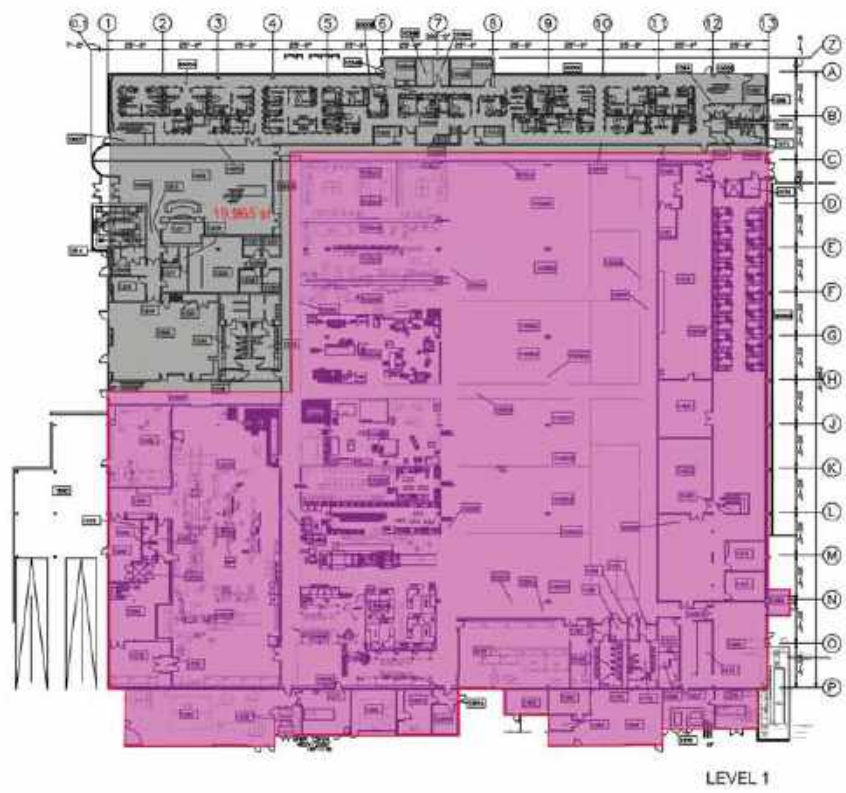
PROJECT NUMBER:
2016-007
0110

PROJECT NAME:
E2-11

SCALE: 1/8" = 1'-0"
DATE: 01/11/17 11:17 AM

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GEHRY PARTNERS, LLP
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 ONE SEASIDE DRIVE
 NEW YORK, NY 10022
 (212) 512-2000

FACEBOOK
 OWNER
 1600 ZEPHYRUS DRIVE
 MENLO PARK, CALIFORNIA 94025

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 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

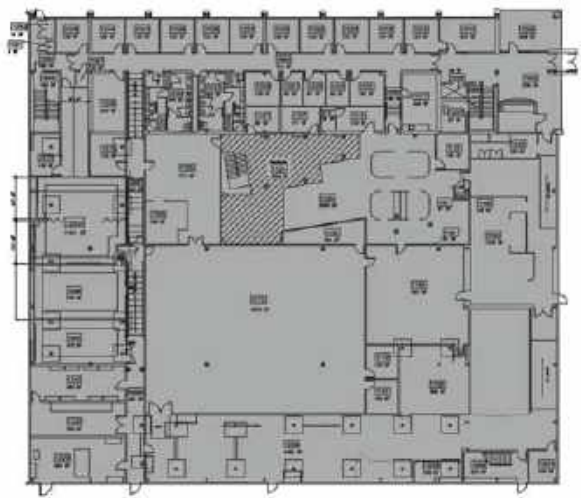
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PROJECT NUMBER
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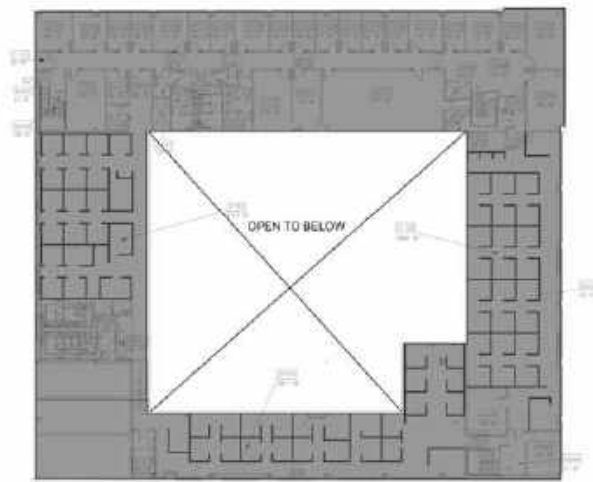
DATE
 11/13/2016

PROJECT NUMBER
 E2-12

- KEY:**
- OFFICE
 - R&D
 - MANUFACTURING
 - WAREHOUSE



LEVEL 1



LEVEL 2

FOR REFERENCE ONLY - HAS BEEN SUBMITTED PREVIOUSLY

GEHRY PARTNERS, LLP
 ARCHITECT
 1045 WASHINGTON STREET
 SUITE 2000, SAN FRANCISCO, CA 94108
 TEL: 415.774.2000
 WWW: GHPARTNERS.COM

DATE: 10/15/13

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

EXISTING BLDG 309

PROJECT NUMBER
 2016-007

HEET NUMBER
 E2-13

DATE: 10/15/13
 SCALE: AS SHOWN

DATE: 10/15/13
 SCALE: AS SHOWN

EXISTING AREAS

TOTAL SITE ACRES 58.31 ACRES
 TOTAL SITE SF 2,539,928 SF

EXISTING SITE BUILDING AREAS

Building GSF from CAD/PDF Files (2015-03-27 CHECK)

D Comments

EXISTING BUILDING ADDRESS	YEAR BUILT	# OF LEVELS	LEVEL 1 GSF	LEVEL 2 GSF	LEVEL 3 GSF	SUB TOTAL GSF	TOTAL GSF FROM CAD	
300	1965-1968		180,108	4,330			184,438	Reflects current condition, not upgrades planned for 2016 completion.
301	1967	2	17,217	17,248			34,465	
302	1967	2	15,123	15,051			30,174	
303	1966	1	35,520			35,520		
304	late 2000	1 + 2 Partial + 3 Partial	13,805	6,806	3,564	24,175		Infil between 303 and 306
306	1968	1 + 2 Partial	49,191	46,209		95,400		
303 + 304 + 306			98,516	53,015	3,564		155,095	
305A		1 + 2 Partial	84,659	10,409		95,068		
305B		1 + 2 Partial	81,092	20,228		101,320		
305C		1 + 2 Partial	77,901	15,429		93,330		
305A+305B+305C	1965, additon 1967		243,652	46,066			289,718	
307	1965-1968	1	156,414				156,414	
308	1970	2	86,941	33,088			120,029	
309	1965-1968	1 + 2 Partial	29,485	18,223			47,708	
CTF	unknown	1	2,235				2,235	
TOTAL EXISTING GSF							1,020,276	
EXISTING BUILDING COVERAGE			1,171,859					
PERCENTAGE OF BUILDING COVERAGE			46%					Building coverage / Total Site SF

FOR REFERENCE ONLY - HAS BEEN SUBMITTED PREVIOUSLY

DEHRY PARTNERS, LLP
 ARCHITECT
 1047 BRANT 19111
 105 PROSSER, SUITE 1000
 (510) 527-7331

FACEBOOK
 (201) 875-8811
 4000 S. FINE, SUITE 1000, 9400

DATE: 1/27/16

Facebook Campus Expansion
 Facebook Building 21, 22 & Hotel Site
 300-309 Constitution Drive

MATRIX: EXISTING BUILDINGS DATE OF CONSTRUCTION / EXISTING BUILDING COVERAGE OF THE PROJECT SITE

PROJECT NUMBER
 2016-017

PROJECT NAME
 E3-01

DATE: 1/27/16

PROJECT NAME

A.L.T.A. / A.C.S.M. LAND TITLE SURVEY

OF THE LANDS OF
TYCO ELECTRONICS CORPORATION

REFERENCED TITLE INSURANCE POLICY

TITLE COMPANY: FIRST AMERICAN TITLE COMPANY
1737 NORTH FIRST STREET, SUITE 500
SAN JOSE, CA 95112

TITLE REPORT NO: NCS-624373-SC

TITLE REPORT DATE: MAY 30, 2014

TITLE OFFICER: MIKE D. HICKEY

TITLE VESTED IN: TYCO ELECTRONICS CORPORATION

NATURE OF TITLE: FEE SIMPLE AS TO PARCEL ONE, EASEMENT AS TO PARCEL TWO

PROPERTY ADDRESS: 300, 301, 307 & 308 CONSTITUTION DR, MENLO PARK, CA 94025

SITE ACREAGE: 58.309 ACRES

ASSESSOR'S PARCEL NO: 055-244-010

FLOOD ZONE: FLOOD ZONE AE: SPECIAL FLOOD HAZARD AREA SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD; THE 1% ANNUAL CHANCE FLOOD (100 YEAR FLOOD) ALSO KNOWN AS THE BASE FLOOD, IS THE FLOOD THAT HAS A 1% CHANCE OF BEING EQUALLED OR EXCEEDED IN ANY GIVEN YEAR. THE SPECIAL FLOOD HAZARD AREA IS THE AREA SUBJECT TO FLOODING BY THE 1% ANNUAL CHANCE FLOOD. AREAS OF SPECIAL FLOOD HAZARD INCLUDE ZONES A, AE, AH, AD, AR, A99, V, AND VE. THE BASE FLOOD ELEVATION IS THE WATER-SURFACE ELEVATION OF THE 1% ANNUAL CHANCE FLOOD.

FLOOD RATE INSURANCE: MAP NO.06081C0306E
EFFECTIVE DATE - OCTOBER 16, 2012

ZONING: M-2 - GENERAL INDUSTRIAL DISTRICT
REQUIRED MINIMUM YARDS PER CITY OF MENLO PARK ZONING ORDINANCE: TWENTY FEET FRONT; REAR, NONE EXCEPT TWENTY FEET WHERE ADJUTING RESIDENTIAL DISTRICTS; SIDE, TEN FEET, EXCEPT THAT SIDE YARD MAY BE REDUCED TO ZERO FEET PROVIDED THE SIDE YARD IS CORRESPONDINGLY INCREASED.

LEGAL DESCRIPTION:

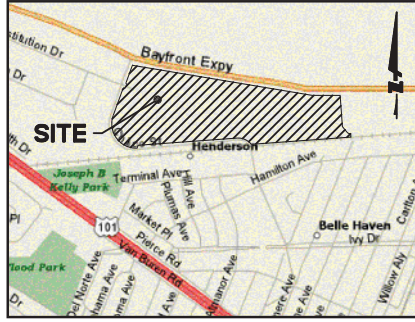
REAL PROPERTY IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

PARCEL ONE:
THAT CERTAIN REAL PROPERTY PURSUANT TO NOTICE OF MERGER AS EVIDENCED BY DOCUMENT RECORDED MARCH 31, 2011 AS INSTRUMENT NO. 2011-08089 OF OFFICIAL RECORDS, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:
BEING ALL OF PARCEL 1, PARCEL 2, AND PARCEL 3A, AS SAID PARCELS ARE DESCRIBED IN THAT CERTAIN GRANT DEED, RECORDED ON MARCH 27, 2012 AS DOCUMENT NO. 2002-059141, SAN MATEO COUNTY RECORDS,
ALSO BEING ALL OF ADJUSTED PARCEL 4, AS SAID PARCEL IS DESCRIBED IN THAT CERTAIN APPROVAL OF LOT LINE ADJUSTMENT, RECORDED ON FEBRUARY 06, 2007 AS DOCUMENT NO. 2007-018609, SAN MATEO COUNTY RECORDS, AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:
BEGINNING AT THE INTERSECTION OF THE NORTHEASTLY LINE OF SAID PARCEL 1 WITH THE SOUTHERLY RIGHT-OF-WAY LINE OF HIGHWAY 84, AS SAID RIGHT-OF-WAY LINE IS SHOWN ON CALTRANS RIGHT-OF-WAY RECORD MAP R-105.2;
THENCE ALONG THE NORTHERLY LINE OF SAID PARCEL 1, SOUTH 64° 50' 10" EAST 11.22 FEET;
THENCE CONTINUING ALONG SAID NORTHERLY LINE OF SAID PARCEL 1, AND ALONG THE NORTHERLY LINE OF SAID PARCEL 2, THE NORTHERLY LINE OF SAID PARCEL 3A, AND THE NORTHERLY LINE OF SAID ADJUSTED PARCEL 4, SOUTH 81° 12' 00" EAST 250.00 FEET;
THENCE CONTINUING ALONG SAID NORTHERLY LINE OF SAID ADJUSTED PARCEL 4, NORTH 89° 21' 50" EAST, 384.14 FEET TO THE EASTERLY LINE OF SAID ADJUSTED PARCEL 4;
THENCE LEAVING SAID NORTHERLY LINE AND ALONG SAID EASTERLY LINE OF THE FOLLOWING FOUR (4) COURSES:
1. SOUTH 04° 23' 40" EAST, 43.24 FEET;
2. SOUTH 04° 23' 39" EAST, 99.01 FEET TO THE BEGINNING OF A TANGENT CURVE HAVING A RADIUS OF 80.00 FEET;
3. ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 54° 22' 00", AN ARC LENGTH OF 26.83 FEET;
4. SOUTH 04° 51' 40" EAST, 46.16 FEET TO THE SOUTHERLY LINE OF SAID ADJUSTED PARCEL 4;
THENCE LEAVING SAID EASTERLY LINE AND ALONG SAID SOUTHERLY LINE, SOUTH 85° 08' 20" WEST, 1,375.68 FEET TO THE BEGINNING OF A NON-TANGENT CURVE, CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 231.72 FEET;
THENCE CONTINUING ALONG SAID SOUTHERLY LINE, NORTHWESTERLY ALONG CURVE TO THROUGH A CENTRAL ANGLE OF 53° 04' 38", AN ARC LENGTH OF 216.83 FEET TO THE SOUTHERLY LINE OF SAID PARCEL 2;
THENCE LEAVING SAID SOUTHERLY LINE OF SAID PARCEL 4 AND ALONG SAID SOUTHERLY LINE OF SAID PARCEL 2, SOUTH 85° 08' 20" WEST, 1,370.12 FEET TO THE EASTERLY LINE OF SAID PARCEL 1;
THENCE LEAVING SAID SOUTHERLY LINE AND ALONG SAID EASTERLY LINE, SOUTH 22° 32' 00" WEST, 42.84 FEET TO THE NORTHEASTERLY LINE OF PARCEL 49237-3, AS SAID PARCEL IS DESCRIBED IN THAT CERTAIN FINAL ORDER OF CONDEMNATION, RECORDED ON JULY 27, 1983 IN DOCUMENT NO. 83078902, SAN MATEO COUNTY RECORDS, SAID POINT BEING THE BEGINNING OF A NON-TANGENT CURVE TO THE NORTHEAST, HAVING A RADIUS OF 335.00 FEET;
THENCE ALONG SAID NORTHEASTERLY LINE, ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 112° 10' 00", AN ARC LENGTH OF 655.62 FEET TO THE SOUTHEASTERLY LINE OF SAID PARCEL 2;
THENCE LEAVING SAID NORTHEASTERLY LINE AND ALONG SAID SOUTHEASTERLY LINE THE FOLLOWING NINE (9) COURSES:
1. NORTH 19° 16' 42" EAST, 388.09 FEET TO THE BEGINNING OF A CURVE CONCAVE TO THE SOUTHWEST HAVING A RADIUS OF 20.00 FEET;
2. ALONG SAID CURVE THROUGH CENTRAL ANGLE OF 93° 36' 25", AN ARC LENGTH OF 32.67 FEET;
3. NORTH 67° 06' 53" EAST, 5.00 FEET;
4. NORTH 22° 53' 07" EAST, 30.00 FEET;
5. SOUTH 47° 09' 53" WEST, 5.83 FEET;
6. NORTH 22° 53' 07" EAST, 30.00 FEET;
7. NORTH 67° 06' 53" WEST, 5.83 FEET;
8. ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 86° 23' 35", AN ARC LENGTH OF 30.16 FEET;
9. NORTH 19° 16' 42" EAST, 238.47 FEET TO SAID WESTERLY LINE OF SAID PARCEL 1;
THENCE LEAVING SAID SOUTHEASTERLY LINE AND ALONG SAID NORTHERLY LINE, NORTH 22° 32' 00" EAST, 18.08 FEET TO THE POINT OF BEGINNING PARCEL TWO.

EXCEPTIONS:

- GENERAL AND SPECIAL TAXES AND ASSESSMENTS FOR THE FISCAL YEAR 2014-2015, A LIEN NOT YET DUE OR PAYABLE.
- THE LIEN OF SUPPLEMENTAL TAXES, IF ANY, ASSESSED PURSUANT TO CHAPTER 3.5 COMMENCING WITH SECTION 75 OF THE CALIFORNIA REVENUE AND TAXATION CODE.
- EASEMENT FOR ROAD PURPOSES OVER THAT PORTION OF THE HERIN DESCRIBED PROPERTY LYING WITHIN ANY PUBLIC OR PRIVATE ROAD.

AS DESCRIBED IN THE POLICY OF TITLE INSURANCE PREPARED BY:
FIRST AMERICAN TITLE COMPANY
ORDER NUMBER: NCS-624373-SC
DATED MAY 30, 2014



EXCEPTIONS (continued)

- AN EASEMENT FOR THE RIGHT TO ERECT AND MAINTAIN TWO INDEPENDENT LINES OF POLES WITH THE NECESSARY FIXTURES FOR TRANSMISSION OF ELECTRIC CURRENT AND INCIDENTAL PURPOSES, RECORDED JANUARY 23, 1901 IN BOOK 88 OF DEEDS, PAGE 290.
IN FAVOR OF: THE STANDARD ELECTRIC COMPANY OF CALIFORNIA, A CORPORATION
AFFECTS: AS DESCRIBED THEREIN
NOTE: THESE RIGHTS HAVE SINCE PASSED TO PACIFIC GAS AND ELECTRIC COMPANY
PLOTTED
- AN EASEMENT FOR SENIOR AND INCIDENTAL PURPOSES IN THE DOCUMENT RECORDED MAY 20, 1904 IN BOOK 106 OF DEEDS, PAGE 535.
APPROXIMATE CENTERLINE PLOTTED - WIDTH NOT DEFINED
- COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED SEPTEMBER 30, 1907 IN BOOK 143 OF DEEDS, PAGE 39, WHICH PROVIDE THAT A VIOLATION THEREOF SHALL NOT DEFEAT OR RENDER INVALID THE LIEN OF ANY FIRST MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE, BUT DELETING ANY COVENANT, CONDITION OR RESTRICTION INDICATING A PREFERENCE, LIMITATION OR DISCRIMINATION BASED ON RACE, COLOR, RELIGION, SEX, HANDICAP, FAMILIAL STATUS, NATIONAL ORIGIN, SEXUAL ORIENTATION, MARITAL STATUS, ANCESTRY, SOURCE OF INCOME OR DISABILITY, TO THE EXTENT SUCH COVENANTS, CONDITIONS OR RESTRICTIONS VIOLATE TITLE 42, SECTION 3604(C), OF THE UNITED STATES CODES OR SECTION 12905 OF THE CALIFORNIA GOVERNMENT CODE, LAWFUL RESTRICTIONS UNDER STATE AND FEDERAL LAW ON THE AGE OF OCCUPANTS IN SENIOR HOUSING OR HOUSING FOR OLDER PERSONS SHALL NOT BE CONSTRUED AS RESTRICTIONS BASED ON FAMILIAL STATUS.
IN FAVOR OF: PACIFIC GAS AND ELECTRIC COMPANY, A CALIFORNIA CORPORATION
AFFECTS: AS DESCRIBED THEREIN
- AN EASEMENT FOR LINE OF POLES AND WIRES AND INCIDENTAL PURPOSES, RECORDED MAY 19, 1910 IN BOOK 179 OF DEEDS, PAGE 298.
IN FAVOR OF: PACIFIC GAS AND ELECTRIC COMPANY, A CORPORATION
AFFECTS: AS DESCRIBED THEREIN
PLOTTED
- AN EASEMENT FOR RIGHT OF WAY TO ERECT AND MAINTAIN ELECTRIC TRANSMISSION LINES AND INCIDENTAL PURPOSES, RECORDED AUGUST 01, 1913 IN BOOK 227 OF DEEDS, PAGE 137.
IN FAVOR OF: THE UNION CONSTRUCTION CO., SIERRA AND SAN FRANCISCO, POWER CO. AND THE PACIFIC GAS AND ELECTRIC COMPANY AS DESCRIBED THEREIN
NOTE: THE INTEREST OF SIERRA AND SAN FRANCISCO POWER CO., HAS PASSED TO PACIFIC GAS AND ELECTRIC COMPANY.
THE LOCATION OF THE EASEMENT CANNOT BE DETERMINED FROM RECORD INFORMATION.
- RIGHT OF WAY OVER THE HERIN DESCRIBED PROPERTY AS CONTAINED IN ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA COUNTY OF SAN MATEO, #2137 IN ACTION ENTITLED: "PACIFIC GAS AND ELECTRIC COMPANY, A CORPORATION."
PLAINTIFF VS: NEWBRIDGE PARK REALTY COMPANY, A CORPORATION, WILLIAM A. CARNAUFF, MOSES F. X. KAVANAGH AND KATE T. KAVANAGH, AND MRS. GERALD R. HORTON, R/O. LAYMAN, CALIFORNIA PACIFIC TITLE & TRUST COMPANY, A CORPORATION, C. Y. HENDERSON, BANK OF AMERICA NATIONAL TRUST AND SAVINGS ASSOCIATION, A CORPORATION, ET AL. DEFENDANTS
DATED: JUNE 15, 1934
RECORDED: JUNE 15, 1934 INSTRUMENT NO. 42977-5, BOOK 628 OF OFFICIAL RECORDS AT PAGE 160, RECORDS OF SAN MATEO COUNTY, CALIFORNIA.
SAD INSTRUMENT CONDEMS RIGHT OF WAY FOR ELECTRIC LIGHT, HEAT AND POWER LINE ALONG A STRIP 60 FEET WIDE ACROSS NORTHERLY COURSE.
PLOTTED
- AN EASEMENT FOR RIGHT OF WAY FOR SANITARY SEWERS AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 15, 1950 AS INSTRUMENT NO. 38133-1 IN BOOK/REEL 1798, PAGE/IMAGE 293 OF OFFICIAL RECORDS.
IN FAVOR OF: THE MENLO PARK SANITARY DISTRICT AS DESCRIBED THEREIN
PLOTTED
- AN EASEMENT FOR SENIOR AND INCIDENTAL PURPOSES, RECORDED AUGUST 21, 1950 IN BOOK 1922, PAGE 256 OF OFFICIAL RECORDS.
IN FAVOR OF: MENLO PARK SANITARY DISTRICT, A POLITICAL CORPORATION AS DESCRIBED THEREIN
PLOTTED
- AN EASEMENT FOR LINE OF TOWERS WITH SUCH WIRES AND CABLES AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 05, 1952 IN BOOK 2290, PAGE 519 OF OFFICIAL RECORDS.
IN FAVOR OF: PACIFIC GAS AND ELECTRIC COMPANY, A CALIFORNIA CORPORATION AS DESCRIBED THEREIN
PLOTTED
- THIS ITEM INTENTIONALLY DELETED

EXCEPTIONS (continued)

- THE EFFECT OF A MAP PURPORTING TO SHOW THE LAND AND OTHER PROPERTY, FILED MARCH 04, 1965 IN BOOK 6, PAGE 13 OF RECORD OF SURVEYS.
- COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED MARCH 10, 1965 AS INSTRUMENT NO. 25277-1 IN BOOK 4910, PAGE 325 OF OFFICIAL RECORDS, WHICH PROVIDE THAT A VIOLATION THEREOF SHALL NOT DEFEAT OR RENDER INVALID THE LIEN OF ANY FIRST MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE, BUT DELETING ANY COVENANT, CONDITION OR RESTRICTION INDICATING A PREFERENCE, LIMITATION OR DISCRIMINATION BASED ON RACE, COLOR, RELIGION, SEX, HANDICAP, FAMILIAL STATUS, NATIONAL ORIGIN, SEXUAL ORIENTATION, MARITAL STATUS, ANCESTRY, SOURCE OF INCOME OR DISABILITY, TO THE EXTENT SUCH COVENANTS, CONDITIONS OR RESTRICTIONS VIOLATE TITLE 42, SECTION 3604(C), OF THE UNITED STATES CODES OR SECTION 12905 OF THE CALIFORNIA GOVERNMENT CODE, LAWFUL RESTRICTIONS UNDER STATE AND FEDERAL LAW ON THE AGE OF OCCUPANTS IN SENIOR HOUSING OR HOUSING FOR OLDER PERSONS SHALL NOT BE CONSTRUED AS RESTRICTIONS BASED ON FAMILIAL STATUS.
PLOTTED - ROADWAY AND WATER LINE EASEMENTS
- AN EASEMENT FOR SANITARY SEWER AND INCIDENTAL PURPOSES, RECORDED JULY 27, 1965 AS INSTRUMENT NO. 74465-1 IN BOOK 4998, PAGE 45 OF OFFICIAL RECORDS.
IN FAVOR OF: MENLO PARK SANITARY DISTRICT, A POLITICAL SUBDIVISION AS DESCRIBED THEREIN
THE LOCATION OF THE EASEMENT CANNOT BE DETERMINED FROM RECORD INFORMATION.
APPROXIMATE LOCATION PLOTTED
- AN EASEMENT FOR WATER PIPELINE AND INCIDENTAL PURPOSES, RECORDED OCTOBER 28, 1965 AS INSTRUMENT NO. 4004-2 IN BOOK/REEL 1026, PAGE/IMAGE 280 OF OFFICIAL RECORDS.
IN FAVOR OF: CITY OF MENLO PARK, A MUNICIPAL CORPORATION AS DESCRIBED THEREIN
PLOTTED
- AN EASEMENT FOR LINE OF TOWERS AND CABLES FOR TRANSMISSION OF ELECTRIC POWER AND INCIDENTAL PURPOSES, RECORDED JANUARY 24, 1968 AS INSTRUMENT NO. 13960-48 IN BOOK/REEL 9426, PAGE/IMAGE 91 OF OFFICIAL RECORDS.
IN FAVOR OF: PACIFIC GAS AND ELECTRIC COMPANY, A CALIFORNIA CORPORATION AS DESCRIBED THEREIN
THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "AGREEMENT MODIFYING AN EASEMENT" RECORDED MARCH 05, 2001 AS INSTRUMENT NO. 2001-028990 OF OFFICIAL RECORDS.
PLOTTED
- THIS ITEM INTENTIONALLY DELETED
- AN EASEMENT FOR UTILITY PURPOSES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 17, 1969 AS INSTRUMENT NO. 13178-4C IN BOOK/REEL 5099, PAGE/IMAGE 489 OF OFFICIAL RECORDS.
IN FAVOR OF: LESLIE SALT CO., A DELAWARE CORPORATION, ITS SUCCESSORS AND ASSIGNS AS DESCRIBED THEREIN
PLOTTED
- THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "RESOLUTION NO. 2774, RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO PARK ESTABLISHING PLAN LINES FOR CHILCO STREET EXTENSION BETWEEN CHILCO STREET AND THE PROPOSED EXTENSION OF WASHBURN ROAD BEHIND THE BISHANIAN INDUSTRIAL PARK AND THE LANDS OF HAYTHEM" RECORDED JUNE 02, 1975 AS INSTRUMENT NO. 54289-4 IN BOOK 6886, PAGE 570 OF OFFICIAL RECORDS.
IN FAVOR OF: PACIFIC GAS AND ELECTRIC COMPANY, A CALIFORNIA CORPORATION AS DESCRIBED THEREIN
THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "AGREEMENT MODIFYING AN EASEMENT" RECORDED MARCH 05, 2001 AS INSTRUMENT NO. 2001-028991 OF OFFICIAL RECORDS.
PLOTTED
- COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED JULY 10, 1985 AS INSTRUMENT NO. 1985-68812 OF OFFICIAL RECORDS, WHICH PROVIDE THAT A VIOLATION THEREOF SHALL NOT DEFEAT OR RENDER INVALID THE LIEN OF ANY FIRST MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE, BUT DELETING ANY COVENANT, CONDITION OR RESTRICTION INDICATING A PREFERENCE, LIMITATION OR DISCRIMINATION BASED ON RACE, COLOR, RELIGION, SEX, HANDICAP, FAMILIAL STATUS, NATIONAL ORIGIN, SEXUAL ORIENTATION, MARITAL STATUS, ANCESTRY, SOURCE OF INCOME OR DISABILITY, TO THE EXTENT SUCH COVENANTS, CONDITIONS OR RESTRICTIONS VIOLATE TITLE 42, SECTION 3604(C), OF THE UNITED STATES CODES OR SECTION 12905 OF THE CALIFORNIA GOVERNMENT CODE, LAWFUL RESTRICTIONS UNDER STATE AND FEDERAL LAW ON THE AGE OF OCCUPANTS IN SENIOR HOUSING OR HOUSING FOR OLDER PERSONS SHALL NOT BE CONSTRUED AS RESTRICTIONS BASED ON FAMILIAL STATUS.
IN FAVOR OF: PACIFIC GAS AND ELECTRIC COMPANY, A CALIFORNIA CORPORATION AS DESCRIBED THEREIN
THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "AGREEMENT MODIFYING AN EASEMENT" RECORDED MARCH 05, 2001 AS INSTRUMENT NO. 2001-028991 OF OFFICIAL RECORDS.
PLOTTED
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IN FAVOR OF: PACIFIC GAS AND ELECTRIC COMPANY, A CALIFORNIA CORPORATION AS DESCRIBED THEREIN
THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "AGREEMENT MODIFYING AN EASEMENT" RECORDED MARCH 05, 2001 AS INSTRUMENT NO. 2001-028991 OF OFFICIAL RECORDS.
PLOTTED
- THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "AGREEMENT MODIFYING AN EASEMENT" RECORDED JANUARY 14, 1994 AS INSTRUMENT NO. 94-006340 OF OFFICIAL RECORDS.
- THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "COVENANT TO RESTRICT USE OF PROPERTY, ENVIRONMENTAL RESTRICTIONS" RECORDED JANUARY 19, 2007 AS INSTRUMENT NO. 2007-009472 OF OFFICIAL RECORDS.
DOCUMENT(S) DECLARING MODIFICATIONS THEREOF RECORDED AUGUST 06, 2012 AS INSTRUMENT NO. 2012-118977 OF OFFICIAL RECORDS.
- THE TERMS, PROVISIONS AND EASEMENTS(S) CONTAINED IN THE DOCUMENT ENTITLED "EASEMENT AGREEMENT AND COVENANTS RUNNING WITH THE LAND" RECORDED MARCH 15, 2007 AS INSTRUMENT NO. 2007-039705 OF OFFICIAL RECORDS.
DOCUMENT(S) DECLARING MODIFICATIONS THEREOF RECORDED DECEMBER 18, 2007 AS INSTRUMENT NO. 2007-177074 OF OFFICIAL RECORDS.
DOCUMENT(S) DECLARING MODIFICATIONS THEREOF RECORDED DECEMBER 27, 2010 AS INSTRUMENT NO. 2010-166008 OF OFFICIAL RECORDS.
DOCUMENT(S) DECLARING MODIFICATIONS THEREOF RECORDED JULY 03, 2012 AS INSTRUMENT NO. 2012-093793 OF OFFICIAL RECORDS.
EASEMENTS AFFECTING FOR EMERGENCY ACCESS AND SANITARY SEWER, DOCUMENT(S) DECLARING MODIFICATIONS THEREOF RECORDED JANUARY 11, 2013 AS INSTRUMENT NO. 2013-006490 OF OFFICIAL RECORDS.
SANITARY SEWER EASEMENT PLOTTED - EMERGENCY ACCESS GRANTS EMERGENCY VEHICLES PASSAGE THROUGH PARKING AND ACCESS WAYS
- THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "POST-CONSTRUCTION STORMWATER TREATMENT MEASURE OPERATIONS AND MAINTENANCE FOR TYCO THERMAL CONTROL" RECORDED JULY 02, 2008 AS INSTRUMENT NO. 2008-076757 OF OFFICIAL RECORDS.
- THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "POST-CONSTRUCTION STORMWATER TREATMENT MEASURE OPERATIONS AND MAINTENANCE FOR TYCO THERMAL CONTROL" RECORDED OCTOBER 01, 2008 AS INSTRUMENT NO. 2008-110999 OF OFFICIAL RECORDS.
- THE EFFECT OF A TYPICAL ENTITLED "CITY OF MENLO PARK ENGINEERING DIVISION APPROVAL OF LOT LINE ADJUSTMENT," RECORDED JANUARY 11, 2013 AS INSTRUMENT NO. 2013-006490 OF OFFICIAL RECORDS.
- A DOCUMENT ENTITLED "GRANT DEED" RECORDED NOVEMBER 13, 2013 AS INSTRUMENT NO. 2013-156473 OF OFFICIAL RECORDS WHICH PURPORTS TO TRANSFER A PORTION OF THE PROPERTY.
THERE HAVE BEEN NO DEEDS FOUND IN THE PUBLIC RECORDS TO EFFECTUATE THE FINAL LEGAL DESCRIPTION OF THE PROPERTY IN THE ABOVE DOCUMENT.
- RIGHTS OF PARTIES IN POSSESSION.

1015 WILLOW RD
SUITE 200
PLEASANTON, CA 94686
925-386-7799 (FAX)



LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
CITY OF MENLO PARK, SAN MATEO COUNTY, CALIFORNIA

Revisions	
No.	
Date: 7/27/2014	Issue: 14-207
Drawn by: A	Checked by: A
Drawn by: A	Checked by: A
Drawn by: A	Checked by: A
Drawn by: A	Checked by: A

Drawing Number:

20147075

1 of 18

BASIS OF BEARING:

THE BEARING NORTH 15°32'58" EAST, BETWEEN TWO FOUND MONUMENTS ON WILLOW ROAD, AS SHOWN ON THAT CERTAIN PARCEL MAP FILED FOR RECORD ON DECEMBER 31, 1998 IN BOOK 71 OF PARCEL MAPS AT PAGES 15 AND 16, SAN MATEO COUNTY RECORDS, IS THE BASIS OF BEARINGS FOR THIS MAP.

UTILITY NOTE:

UTILITY INFORMATION SHOWN ON THIS PLAN IS APPROXIMATE ONLY AND IS DERIVED FROM ABOVE GROUND FIELD OBSERVATIONS.

ALTA SURVEY NOTES:

1. THE BOLD COMMENTS ARE ADDED BY THE PROFESSIONAL LAND SURVEYOR.
2. ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
3. DIMENSIONAL TIES TO IMPROVEMENTS ARE SHOWN TO THE PROPERTY LINES UNLESS NOTED.
4. "IN" OR "OUT" AS SHOWN ON THIS SURVEY INDICATE THE DISTANCE INSIDE OR OUTSIDE OF THE SUBJECT PROPERTY OR EASEMENTS.
5. THE DATE OF THE SURVEY WAS FEB. 2011 AND JUNE, 2014.
6. SITE IS ACCESSIBLE VIA CHICO STREET, BEING PUBLIC RIGHT OF WAY.
7. EXCEPTION NUMBER 5, 11, 12 AND 26 WERE FOUND TO PATH THROUGH EXISTING STRUCTURES.

SURVEYOR'S CERTIFICATE:

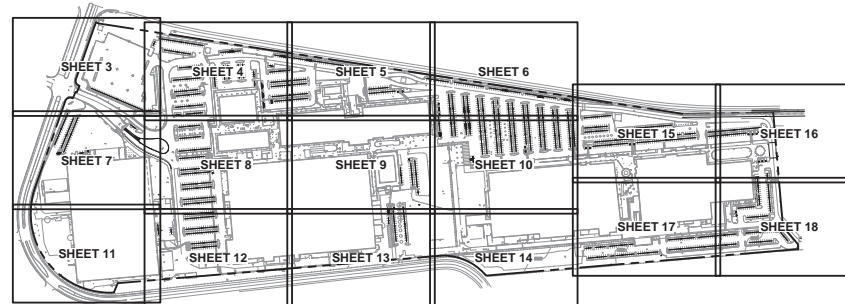
TO: HIBISCUS PROPERTIES, LLC, A DELAWARE LIMITED LIABILITY COMPANY, AND FACEBOOK INC., A DELAWARE CORPORATION, AND THEIR RESPECTIVE LESSORS, SUCCESSORS AND ASSIGNS AND FIRST AMERICAN TITLE COMPANY.
 THIS IS TO CERTIFY THAT THIS MAP OR PLAN AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH "MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS" JOINTLY ESTABLISHED AND ADOPTED BY ALTA, ACSM AND NSPS IN 2011, AND INCLUDES ITEMS 2, 3, 4, 6(b), 8, 9, 11(g), 13, 14, 16, 17, 18, 19, 21, AND 22 OF TABLE A THEREOF, PURSUANT TO THE ACCURACY STANDARDS AS ADOPTED BY ALTA, NSPS AND ACSM AND IN EFFECT ON THE DATE OF THIS CERTIFICATION. THE UNDERSIGNED FURTHER CERTIFIES THAT THE SURVEY MEASUREMENTS WERE MADE IN ACCORDANCE WITH THE "MINIMUM ANGLE, DISTANCE, AND CLOSURE REQUIREMENTS FOR SURVEY MEASUREMENTS WHICH CONTROL LAND BOUNDARIES FOR ALTA/ACSM LAND TITLE SURVEYS."

DAVIS THRESH, P.L.S. 5758
 LICENSE EXPIRES: 09/30/2014



DATED _____

SHEET LAYOUT



LEGEND

- | | |
|--|---|
| AC = ASPHALT CONCRETE | —○— = SIGN |
| A = AREA INLET (STORM DRAIN) | ◀ = TRAFFIC SIGNAL |
| AL = AREA LIGHT | ◻ = TRAFFIC SIGNAL W/ ELECTROLER |
| ATM = AUTOMATIC TELLER MACHINE | ◻ = ELECTROLER |
| BFP = BACKFLOW PREVENTER | ◻ = ELECTROLER ON 2' x 2' CONCRETE BASE |
| BUILD = BUILDING | ◻ = ELECTROLER ON 1' x 1' CONCRETE BASE |
| BW = BACK OF WALK | ⊕ = FIRE HYDRANT |
| CALTRANS = CALIFORNIA DEPARTMENT OF TRANSPORTATION | ⊕ = MONITOR WELL |
| CC = CONCRETE | ◻ = 2' x 2' CONCRETE COLUMN |
| CCP = CONCRETE PAD | ○ = TRASH CAN |
| CLF = CHAIN LINK FENCE | ⊕ = 6" STEEL PIPE |
| CMIP = CORRUGATED METAL PIPE | ○ = MANHOLE |
| COMM BOX = COMMUNICATION BOX | ▣ = CATCH BASIN AT CURB |
| DI = DIAMETER | —◆— = GUY WIRE |
| DIA = DRAIN LINE | ⊕ = POST INDICATOR VALVE |
| DWEY = DRIVEWAY | |
| EB = ELECTRICAL BOX | |
| ELEC = ELECTRIC OR ELECTRICAL | |
| EP = EDGE OF PAVEMENT | |
| ETW = EDGE OF TRAVEL WAY | |
| FF = FINISH FLOOR | |
| FL = FLOWLINE | |
| IRR = IRRIGATION | |
| IRR BOX = IRRIGATION BOX | |
| IR = IRRIGATION | |
| GM = GAS METER | |
| GR = GROUND | |
| HCP = HANDICAP PARKING | |
| HCR = HANDICAP RAMP | |
| JF = JOINT FOLE | |
| LG = LIP OF GUTTER | |
| LC = COMMUNICATION CABLE LINE COMPANY | |
| MH = MANHOLE | |
| MW = MONITOR WELL | |
| PIV = POST INDICATOR VALVE (FOR FIRE LINE) | |
| PG&E = PACIFIC GAS AND ELECTRIC COMPANY | |
| PM = PAINT MARKS ON THE GROUND | |
| PVC = POLYVINYL CHLORIDE | |
| RCP = REINFORCED CONCRETE PIPE | |
| RIL = RAIN WATER LEADER | |
| SDMH = STORM DRAIN MANHOLE | |
| SLB = SIGNAL LIGHT BOX | |
| SSCO = SANITARY SEWER CLEANOUT | |
| SSMH = SANITARY SEWER MANHOLE | |
| SW = SIDEWALK | |
| T = TREE | |
| TB = TELEPHONE BOX | |
| TC = TOP FACE OF CURB | |
| TMH = TELEPHONE MANHOLE | |
| TSM = TRAFFIC SIGNAL BOX | |
| UB = UTILITY BOX | |
| UC = UTILITY CABINET | |
| UNK = UNKNOWN | |
| VLT = VALVE | |
| W/ = WITH | |
| WB = WATER BOX | |
| WV = WATER VALVE | |

SITE PARKING COUNTS

COMPACT PARKING	=	8
MOTORCYCLE PARKING	=	5
SECURITY PARKING	=	5
SERVICE VEHICLES PARKING	=	7
HANDICAP PARKING	=	27
REGULAR PARKING	=	1,477
LEASE AREA PARKING	=	161 (REGULAR PARKING SPACES)
TOTAL	=	1,685 PARKING STALLS

925 WILLOW RD
 SUITE 202
 PLEASANTON, CA 94588
 925-386-7789 (PHONE)
 925-386-7789 (FAX)



LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
 CITY OF MENLO PARK, SAN MATEO COUNTY, CALIFORNIA

Rev.	Date	By	Check

Date: 7/27/2014
 Scale: 1"=20'
 Design: N/A
 Drawn: BKF
 Job No: 201407075.00

Drawing Number:
20147075
 2 OF 18

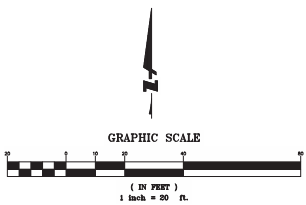
5675 MELLOW RD
 SUITE 200
 PLEASANTON, CA 94588
 925-386-7799 (PHONE)
 925-386-7799 (FAX)



LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
 CITY OF MENLO PARK, SAN MATEO COUNTY, CALIFORNIA

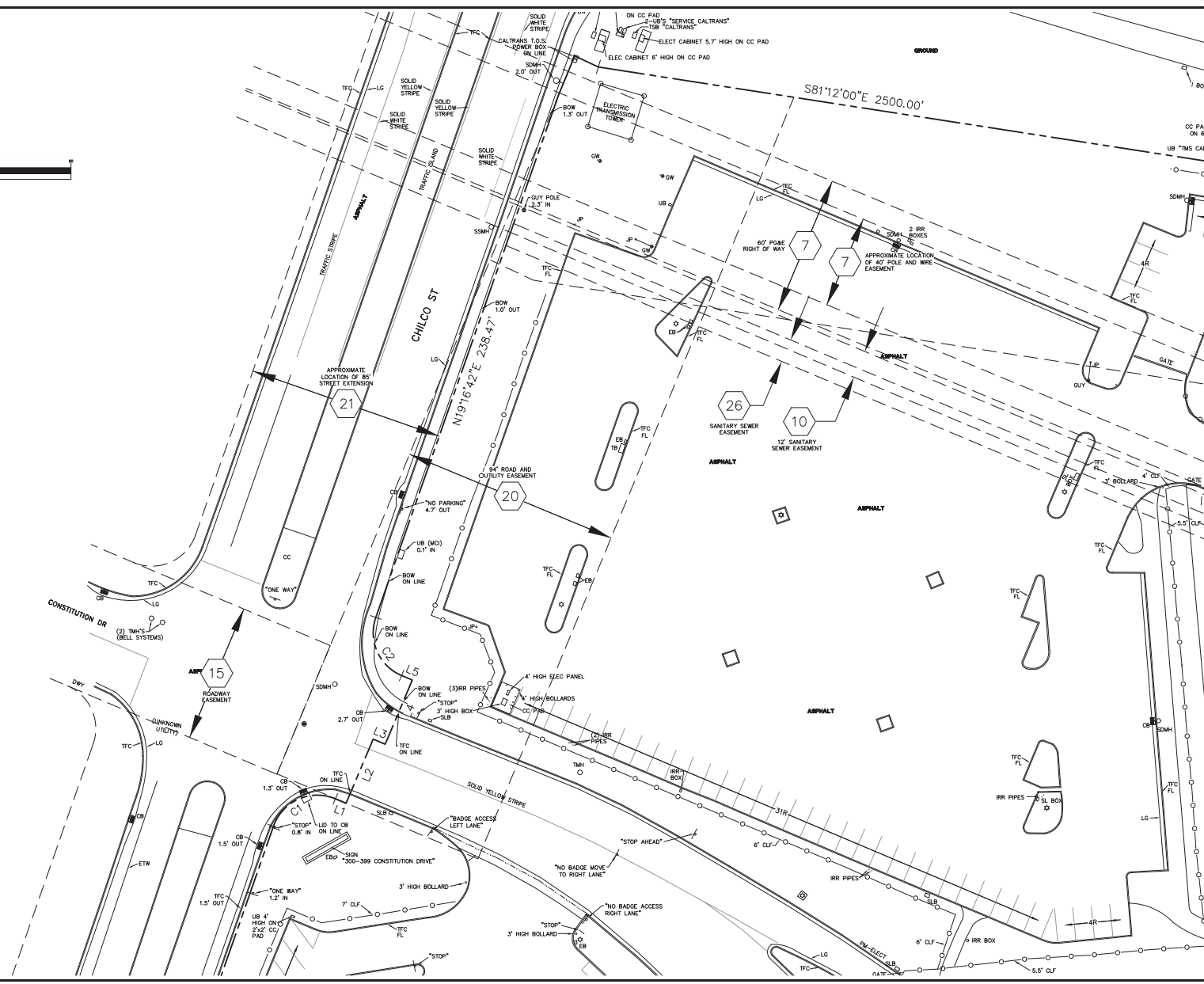
Revision	No.	Date	By	Check

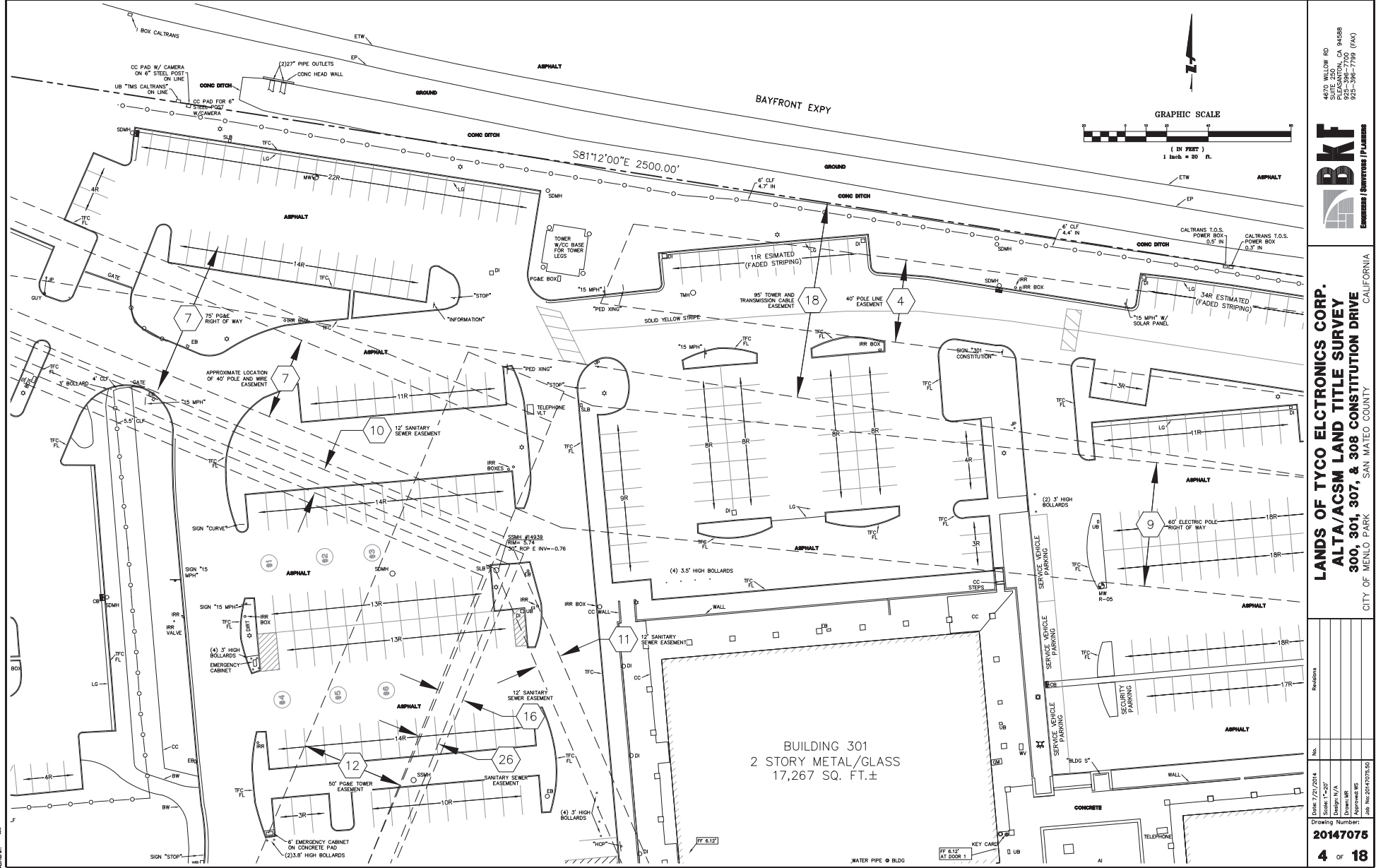
Date: 7/27/2014
 Sheet: 14 of 20
 Drawing No.: 201407075
 Job No.: 201407075-00
201407075
3 OF 18



LINE TABLE		
LINE NO.	LENGTH	DIRECTION
L1	5.00	S67°06'53"E
L2	30.00	N22°53'07"E
L3	5.63	S67°06'53"E
L4	30.00	N22°53'07"E
L5	5.00	N67°06'53"W

CURVE TABLE			
CURVE NO.	LENGTH	RADIUS	DELTA
C1	32.67	20.00	093°36'25"
C2	30.16	20.00	086°23'35"





625 BAYVIEW RD
 SUITE 202
 PLEASANTON, CA 94588
 925-386-7799 (FAX)

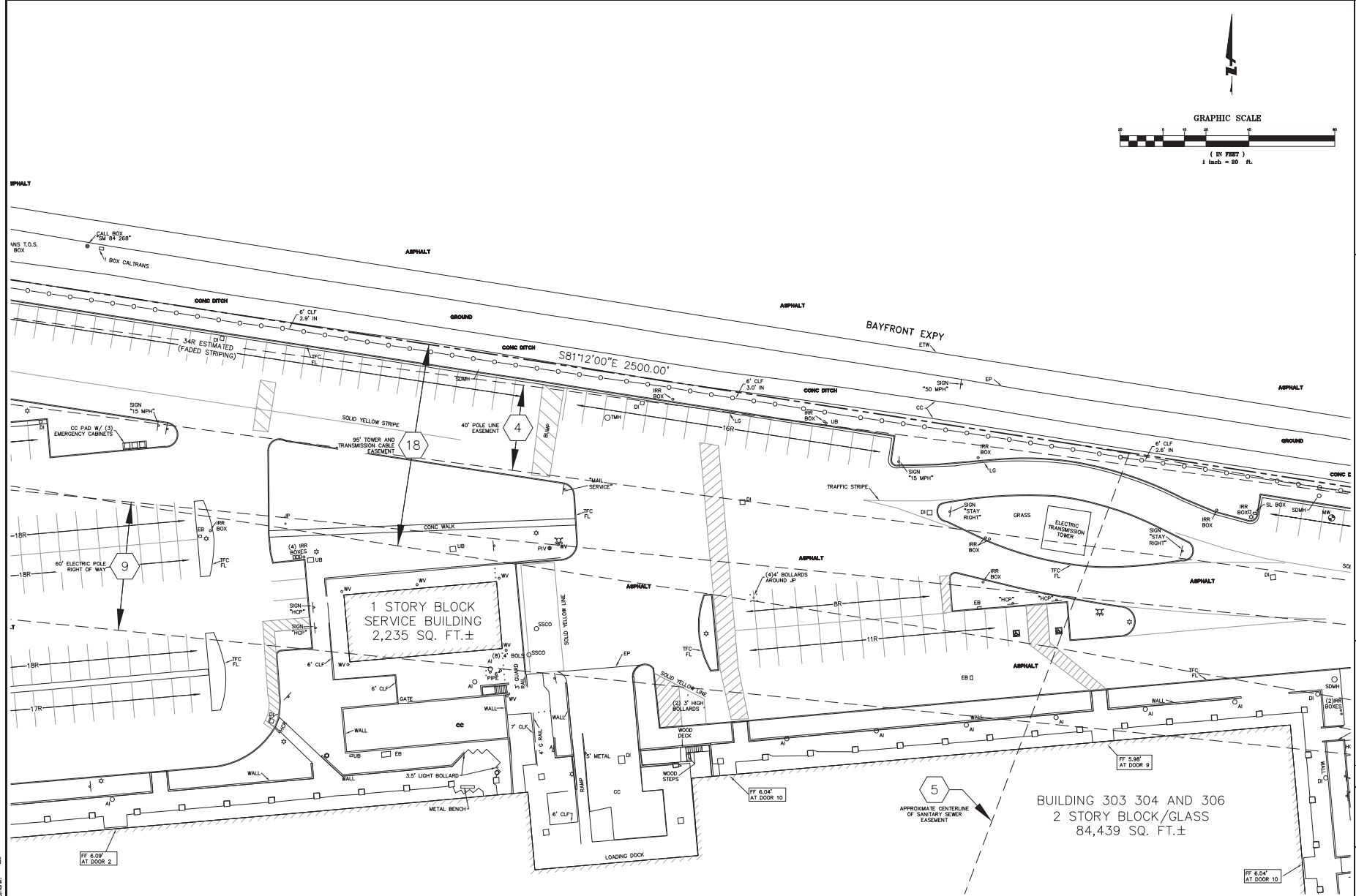
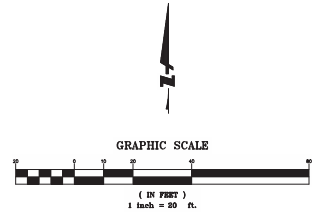


LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
 SAN MATEO COUNTY
 CITY OF MENLO PARK
 CALIFORNIA

Revisions	No.	Date	By	Checked

Drawing Number:
20147075
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625 WILLOW RD
SUITE 200
PLEASANTON, CA 94588
925-386-7799 (FAX)



LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
CITY OF MENLO PARK SAN MATEO COUNTY CALIFORNIA

Revisions	No.	Description
	1	DATE: 7/27/2014 SCALE: 1/4"=20' DRAWN BY: [Name] CHECKED BY: [Name] JOB NO: 201407075.00

20147075
5 OF 18

650 WILLOW RD
SUITE 200
PLEASANTON, CA 94588
925-386-7799 (FAX)

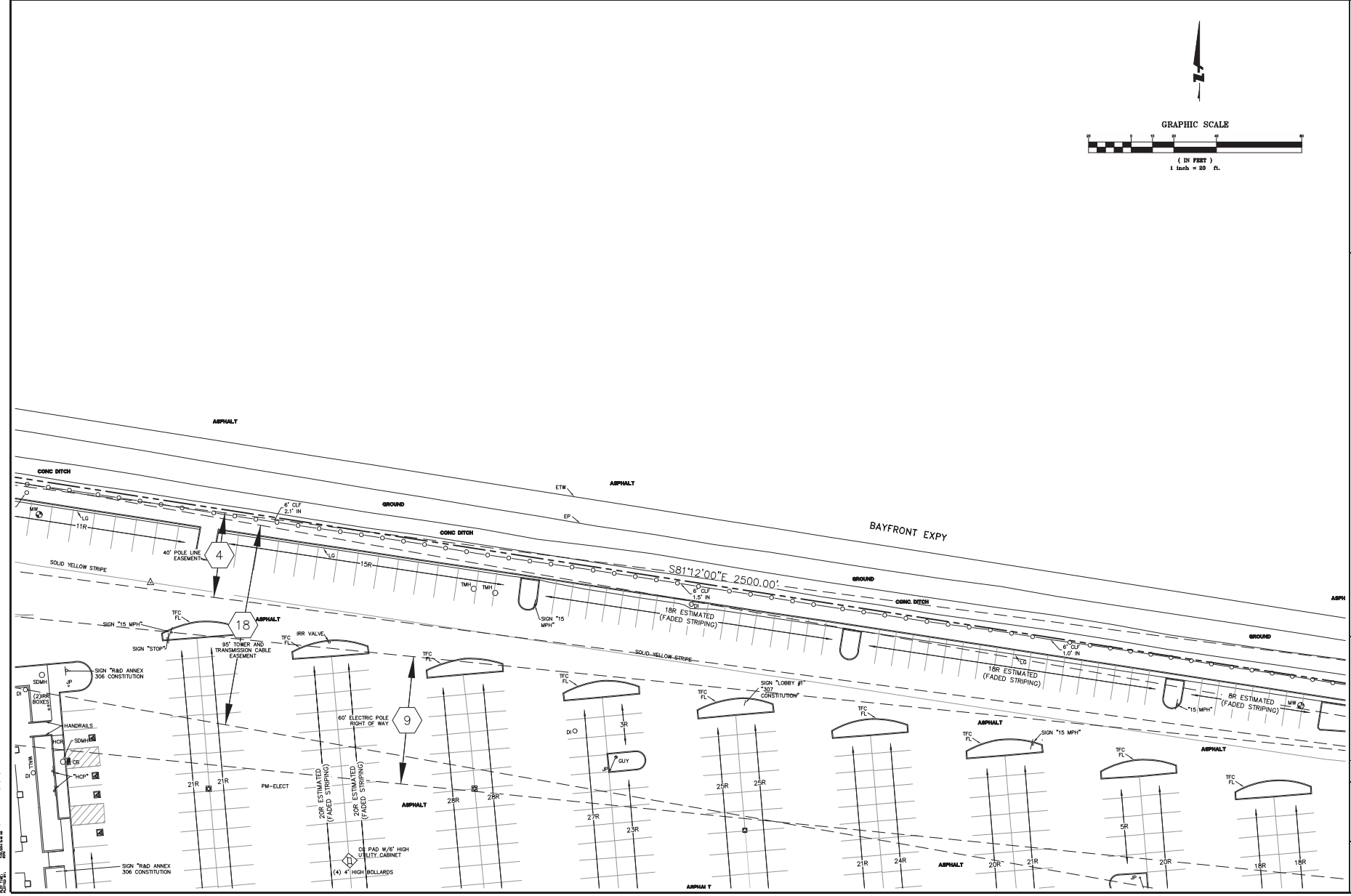
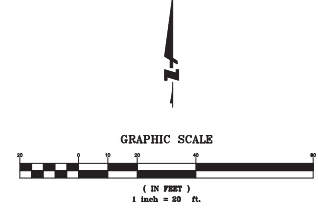


**LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE**
CITY OF MENLO PARK SAN MATEO COUNTY CALIFORNIA

Revisions	No.	Date

Date: 7/27/2014
Scale: 1"=20'
Design: N/A
Drawn: J. W. S.
Job No: 201407075.00

20147075
6 OF 18



300 BAYVIEW RD
SUITE 200
PLEASANTON, CA 94588
925-386-7799 (FAX)

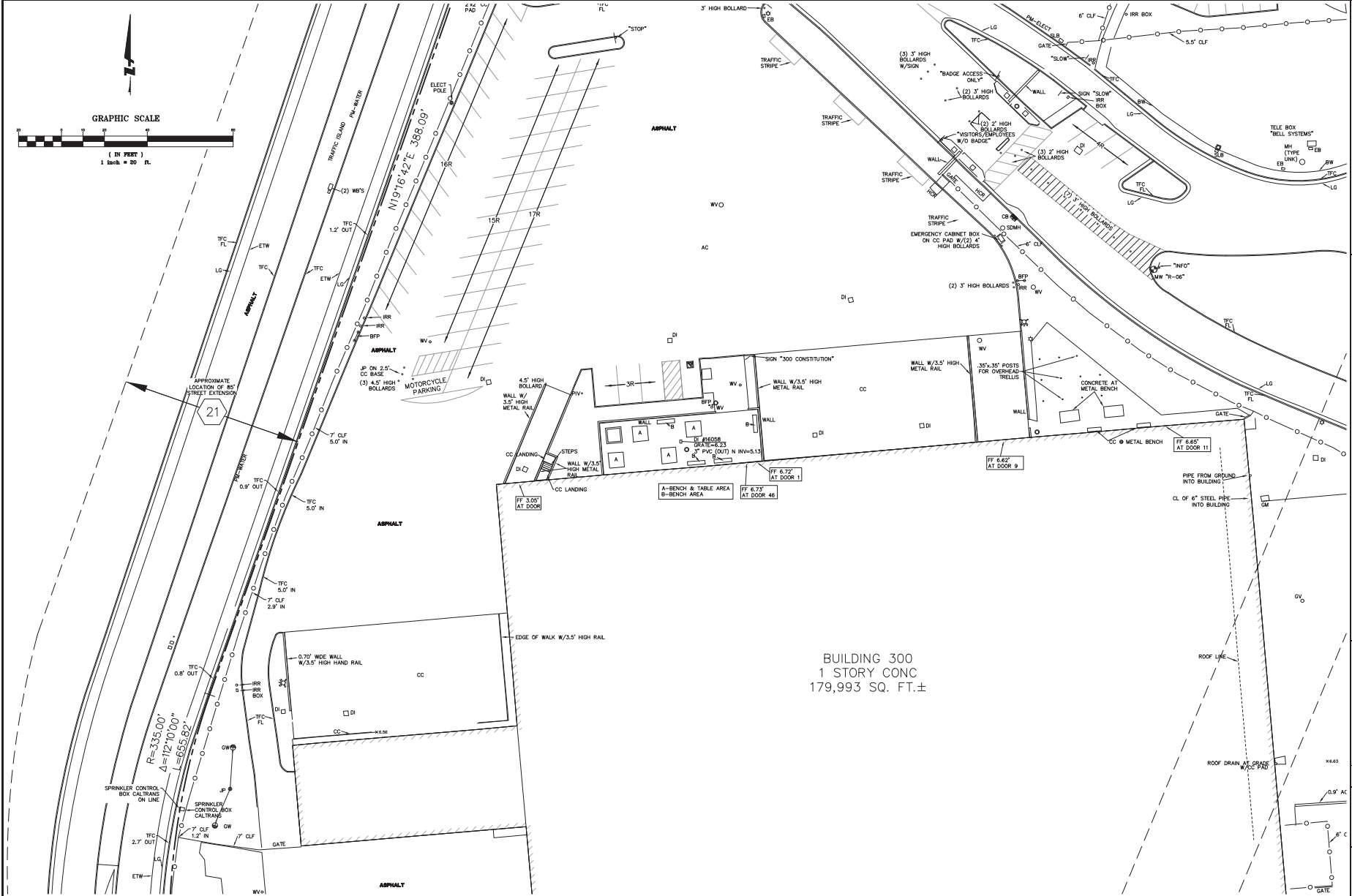


LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
CITY OF MENLO PARK SAN MATEO COUNTY CALIFORNIA

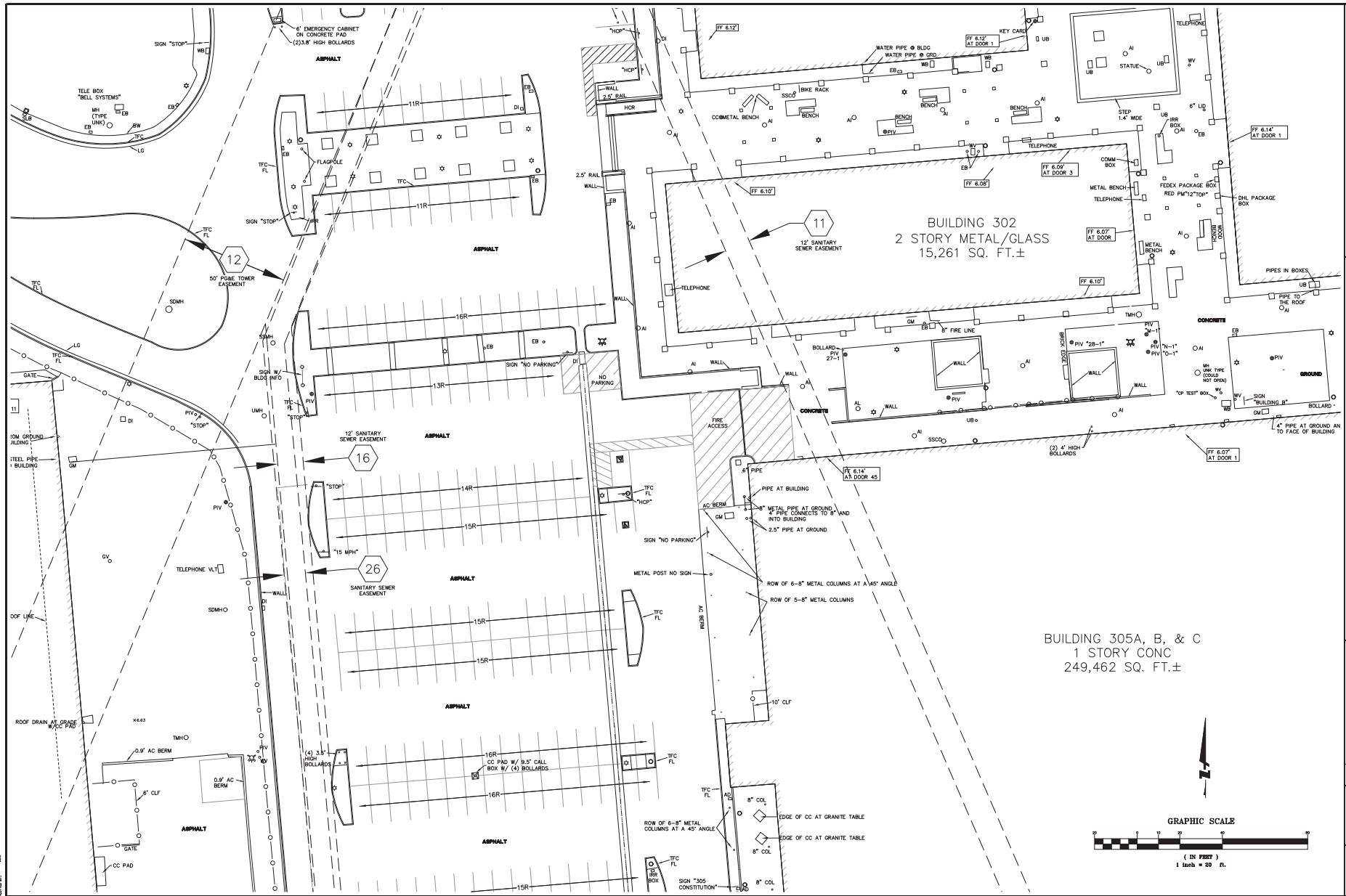
Revision	No.	Date

Date: 7/27/2014
 Sheet: 14 of 20
 Design: N/A
 Drawing: ALTA
 Job No: 2014070750

Drawing Number:
201407075
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BUILDING 300
1 STORY CONC
179,993 SQ. FT.±



300 WILLOW RD
 SUITE 200
 PLEASANTON, CA 94588
 925-386-7799 (FAX)



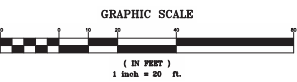
LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
 SAN MATEO COUNTY
 CITY OF MENLO PARK
 CALIFORNIA

Revision	No.	Description

Date: 7/27/2014
 Sheet: 14 of 27
 Design: N/A
 Drawn: N/A
 Job No: 201407075.00
20147075
8 OF 18

BUILDING 303 304 AND 306
2 STORY BLOCK/GLASS
84,439 SQ. FT.±

BUILDING 305A, B, & C
1 STORY CONC
249,462 SQ. FT.±



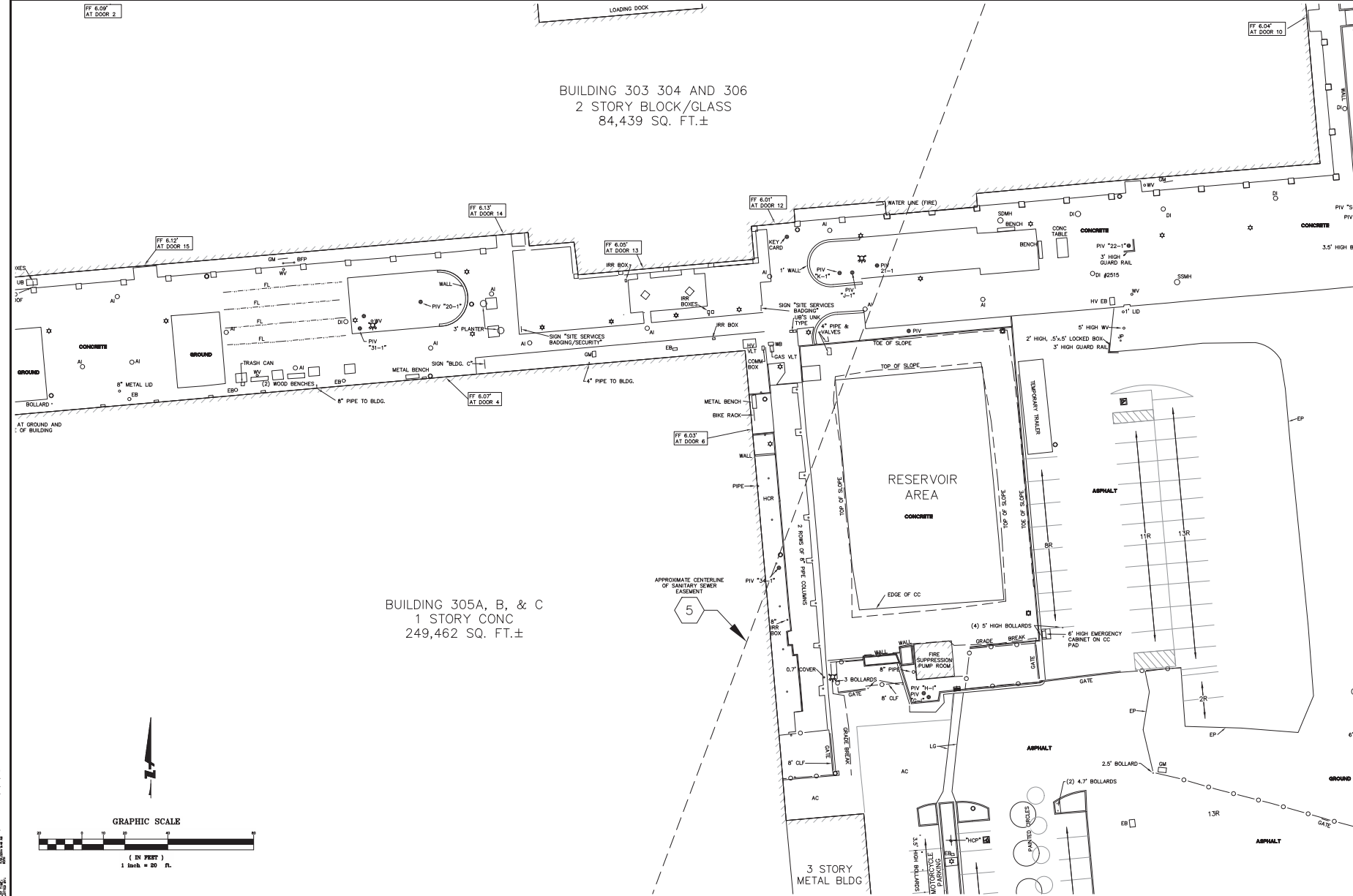
855 WILLOW RD
SUITE 200
PLEASANTON, CA 94588
925-386-7799 (FAX)

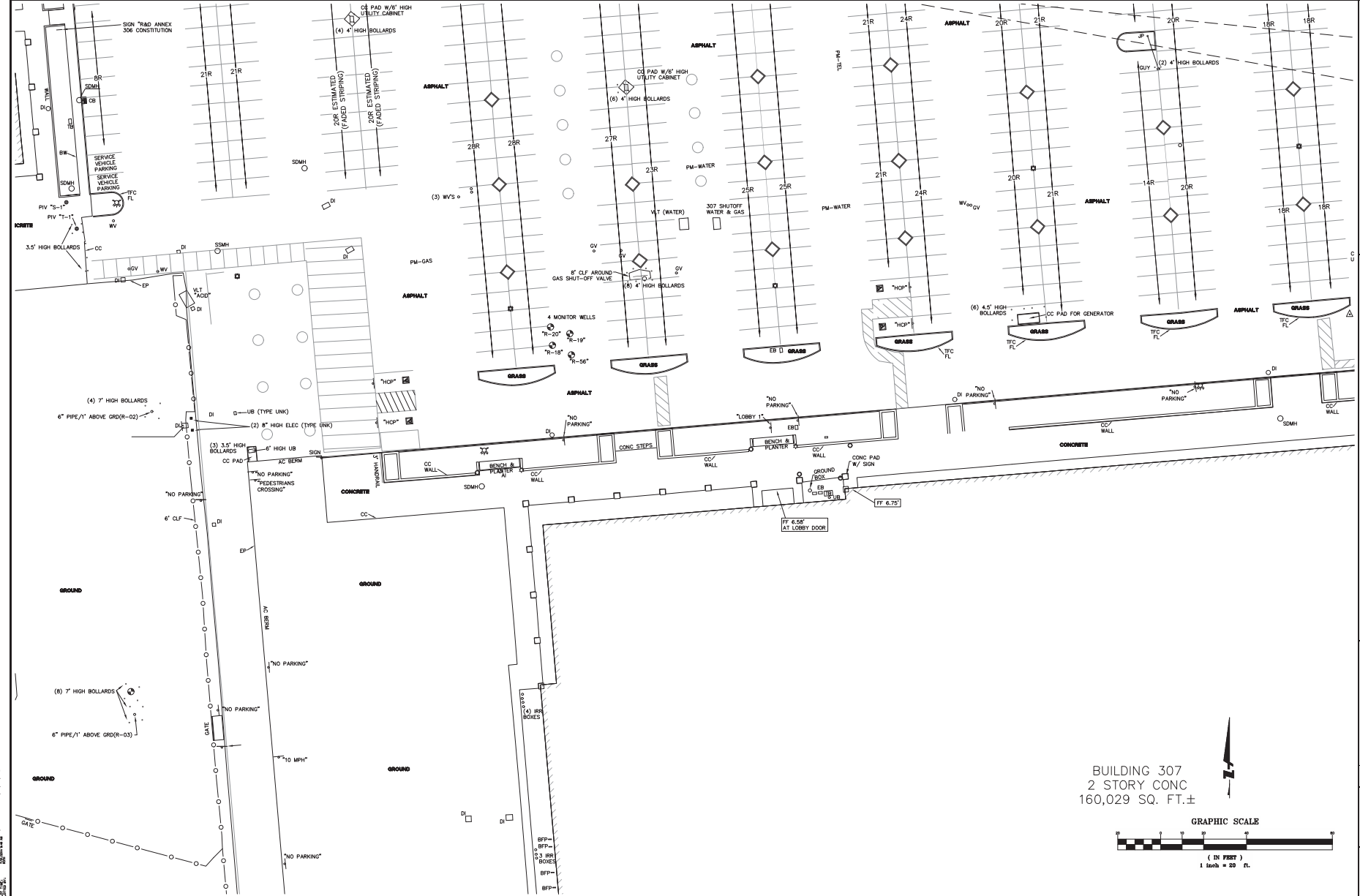


LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
CITY OF MENLO PARK, SAN MATEO COUNTY, CALIFORNIA

Revision	No.	Date

Date: 7/27/2014
Sheet: 14-207
Design: N/A
Drawn: J. S.
Job No: 201407075.00
Drawing Number: 20147075
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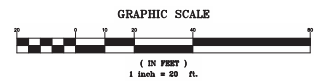
525 W. LOW RD
 SUITE 200
 PLEASANTON, CA 94588
 925-386-7799 (FAX)



**LANDS OF TYCO ELECTRONICS CORP.
 ALTA/ACSM LAND TITLE SURVEY
 300, 301, 307, & 308 CONSTITUTION DRIVE**

CITY OF MENLO PARK SAN MATEO COUNTY CALIFORNIA

BUILDING 307
 2 STORY CONC
 160,029 SQ. FT.±



Revision	No.	Date

Drawing Number:
20147075
 10 OF 18

5675 BULLOW RD
 SUITE 200
 PLEASANTON, CA 94588
 925-386-7799 (PHONE)
 925-386-7799 (FAX)

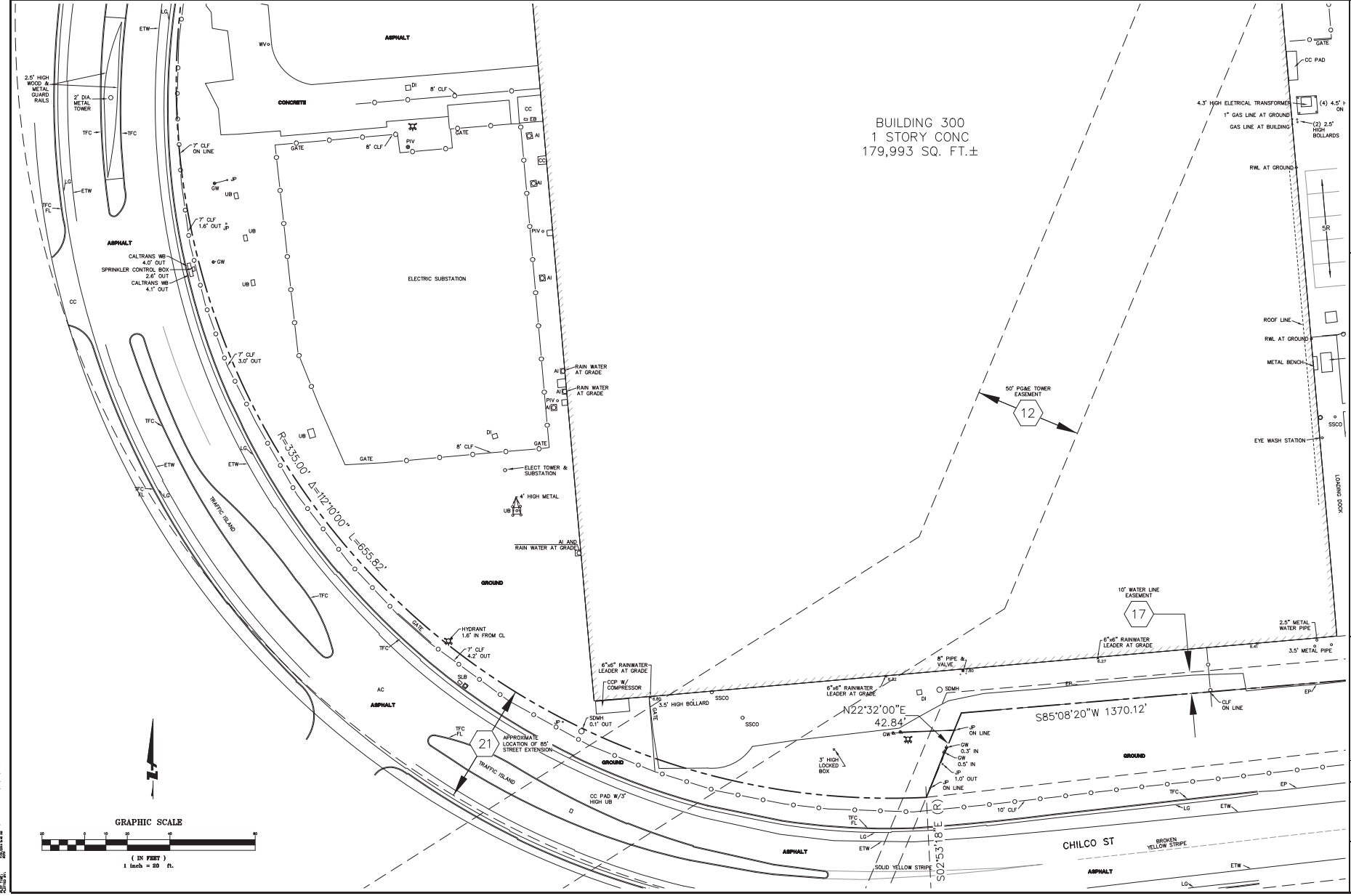


LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
 CITY OF MENLO PARK SAN MATEO COUNTY CALIFORNIA

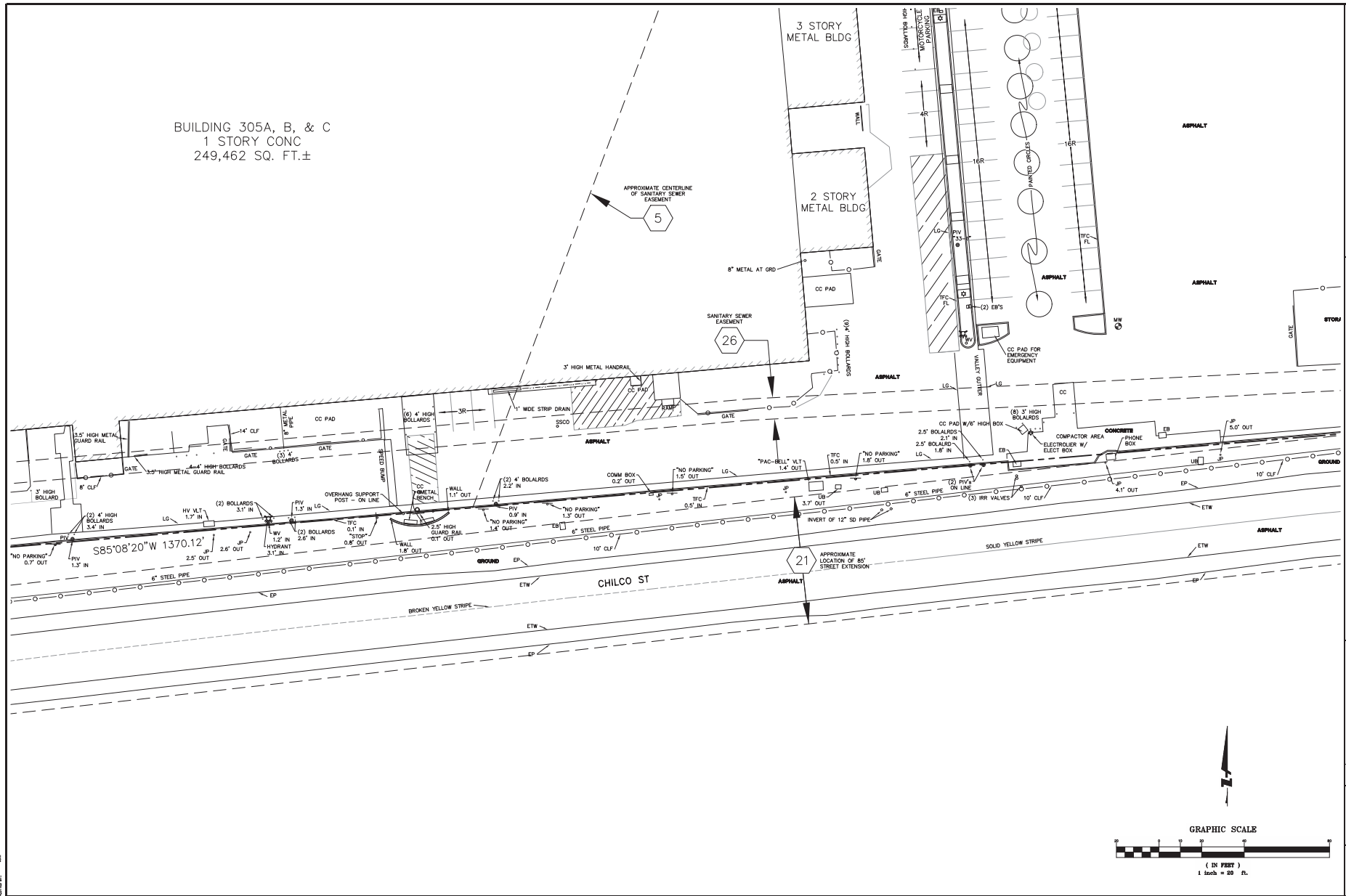
Revisions	No.	Date	By

Date: 7/27/2014
 Sheet: 14 of 27
 Design: N/A
 Drawn: MJS
 Job No: 201407075.00
20147075
11 OF 18

BUILDING 300
 1 STORY CONC
 179,993 SQ. FT.±



BUILDING 305A, B, & C
1 STORY CONC
249,462 SQ. FT.±



500 WILLOW RD
SUITE 200
PLEASANTON, CA 94588
925-386-7799 (FAX)

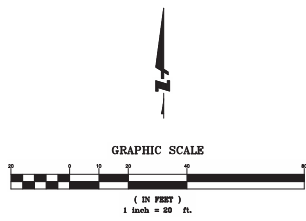


LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
CITY OF MENLO PARK SAN MATEO COUNTY CALIFORNIA

Revisions	No.	Date

Date: 7/27/2014
Sheet: 14 of 27
Design: N/A
Drawn: JLS
Check: JLS
Job No: 201407075.00

Drawing Number:
20147075
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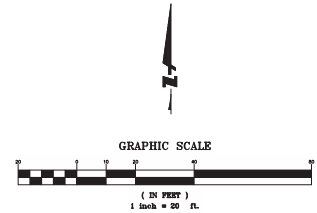
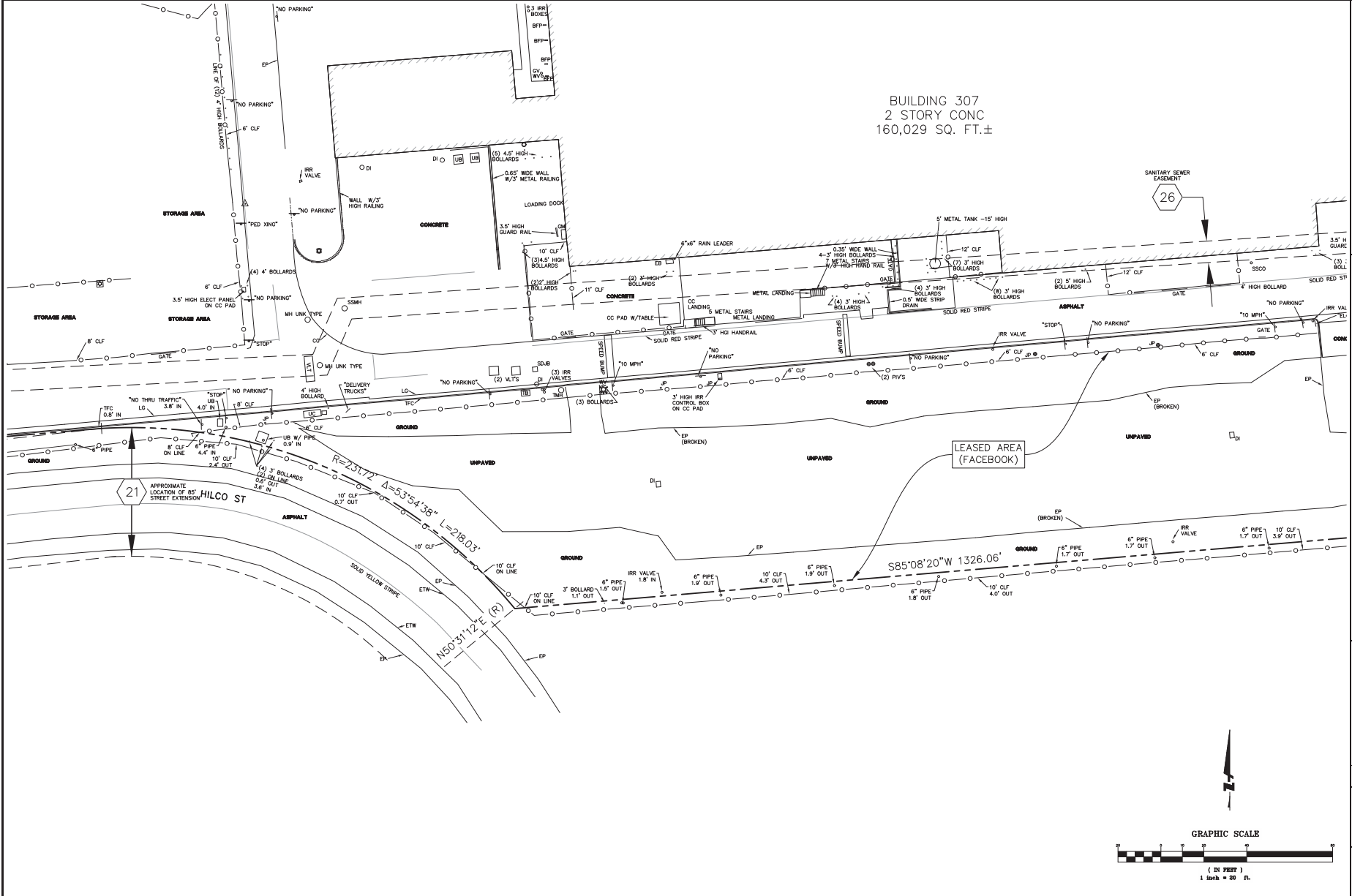
525 WILLOW RD
SUITE 202
PLEASANTON, CA 94588
925-386-7799 (FAX)



CALIFORNIA
SAN MATEO COUNTY
CITY OF MENLO PARK

**LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE**

BUILDING 307
2 STORY CONC
160,029 SQ. FT.±



Revisions
No.
Date: 7/27/2014
Scale: 1"=20'
Design: N/A
Drawn: BKF
Job No: 201407075.00

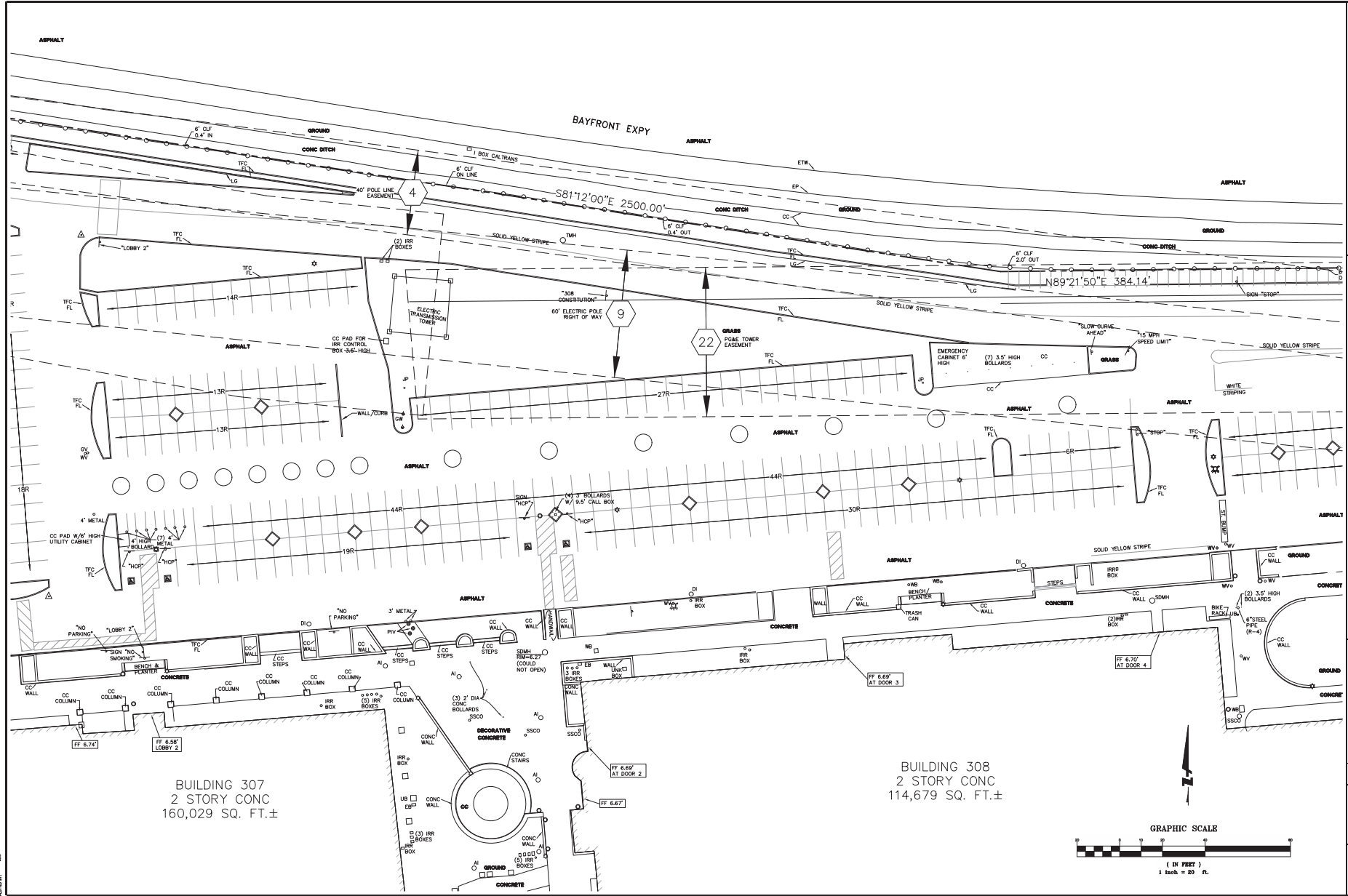
Drawing Number:
20147075
14 OF 18

6501 BAYVIEW RD
SUITE 200
PLEASANTON, CA 94588
925-386-7799 (FAX)



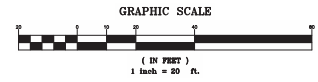
CALIFORNIA
CITY OF MENLO PARK
SAN MATEO COUNTY

**LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE**



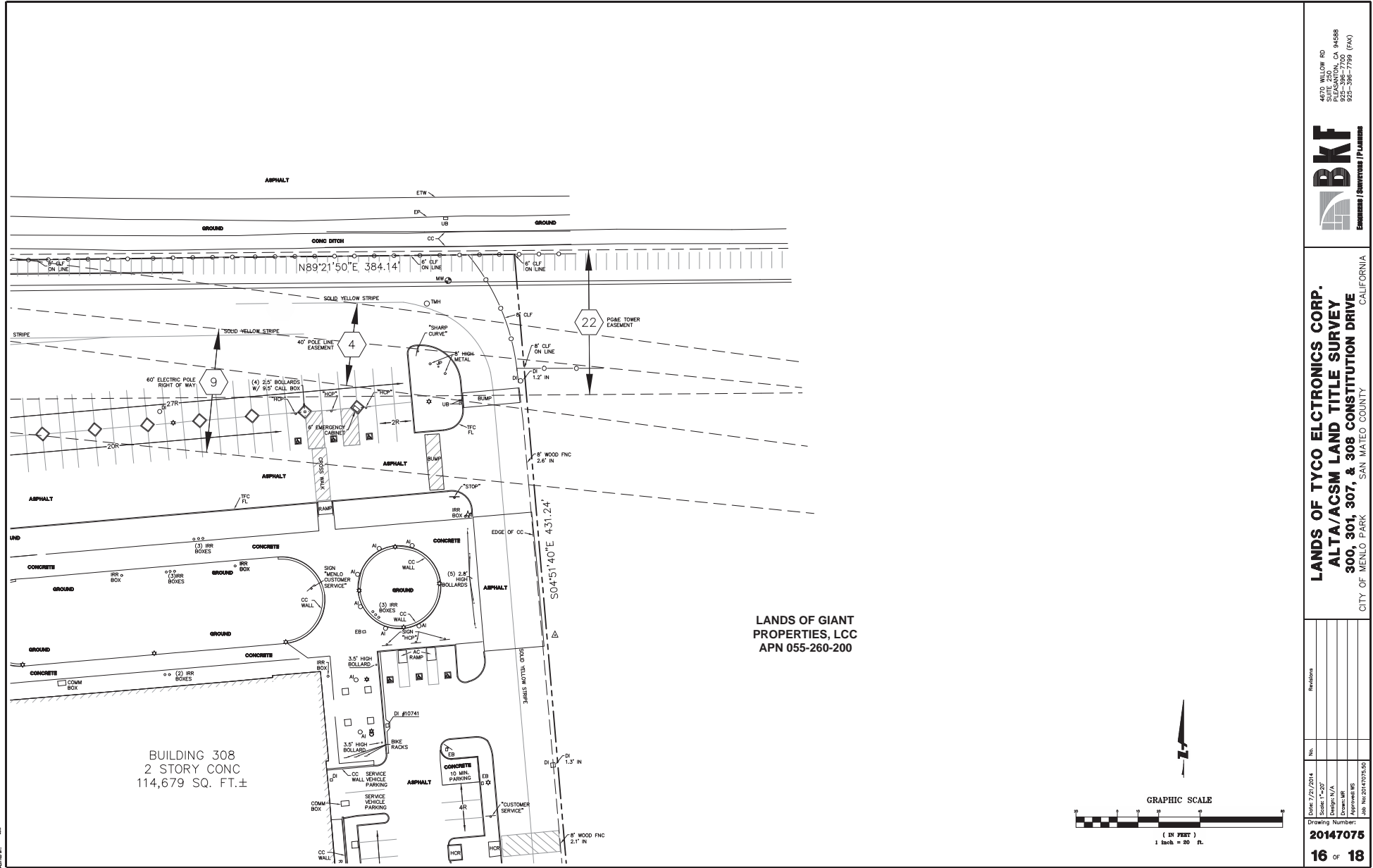
BUILDING 307
2 STORY CONC
160,029 SQ. FT.±

BUILDING 308
2 STORY CONC
114,679 SQ. FT.±



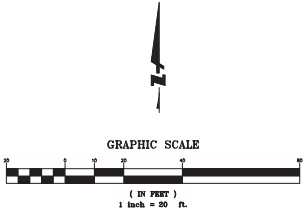
Revisions	No.	Date

Date: 7/27/2014
Scale: 1/4"=20'
Design: N/A
Drawn: BKF
Job No: 201407075.00
20147075
15 OF 18



LANDS OF GIANT
 PROPERTIES, LCC
 APN 055-260-200

BUILDING 308
 2 STORY CONC
 114,679 SQ. FT.±



6750 WILLOW RD
 SUITE 200
 PLEASANTON, CA 94588
 925-386-7799 (FAX)



LANDS OF TYCO ELECTRONICS CORP.
 ALTA/ACSM LAND TITLE SURVEY
 300, 301, 307, & 308 CONSTITUTION DRIVE
 CITY OF MENLO PARK SAN MATEO COUNTY CALIFORNIA

Revisions	No.	Date	By	Check

Date: 7/27/2014
 Scale: 1"=20'
 Design: N/A
 Drawn: BKF
 Job No: 201407075.00

Drawing Number:
201407075

16 OF 18

650 WILLOW RD
SUITE 200
PLEASANTON, CA 94588
925-386-7799 (FAX)



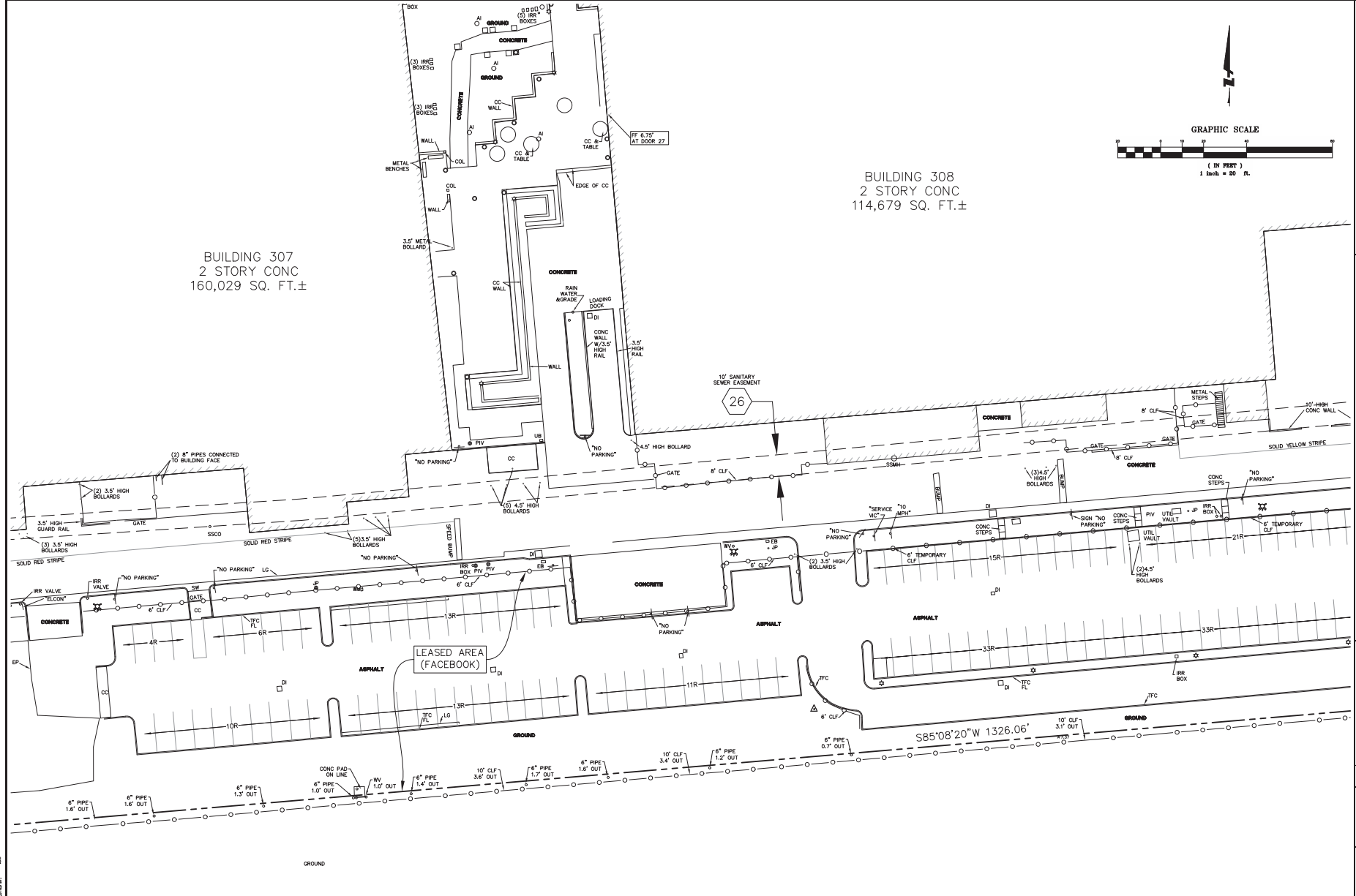
CALIFORNIA
CITY OF MENLO PARK
SAN MATEO COUNTY

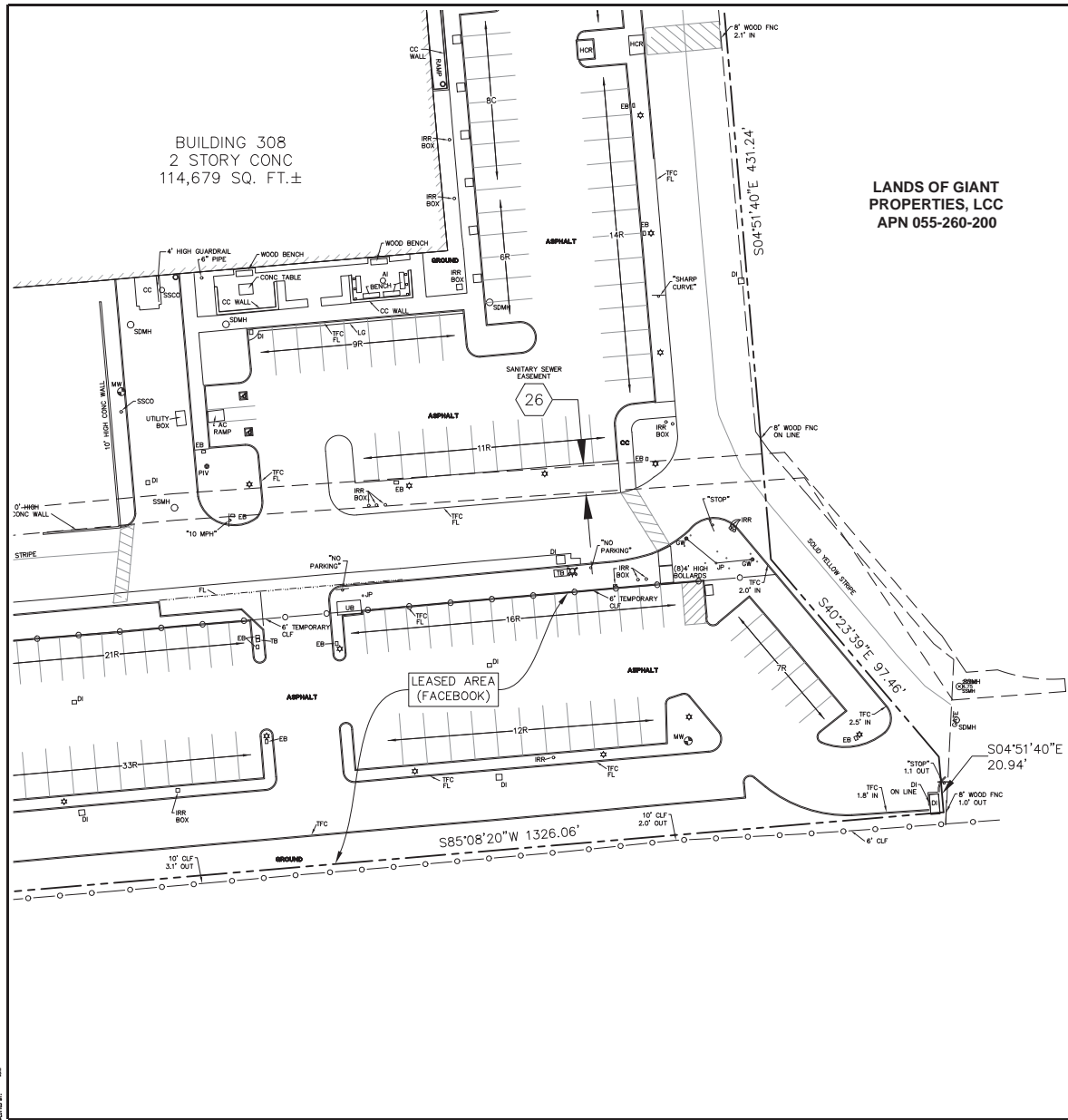
**LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE**

Revision	No.	Description

Date: 7/27/2014
Sheet: 1 of 2
Drawn: J.A.
Checked: J.S.
Job No: 201407075

20147075
17 OF 18





6751 WILLOW RD
 SUITE 200
 PLEASANTON, CA 94588
 925-386-7799 (FAX)

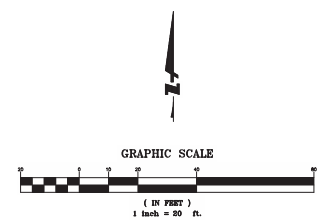


LANDS OF TYCO ELECTRONICS CORP.
ALTA/ACSM LAND TITLE SURVEY
300, 301, 307, & 308 CONSTITUTION DRIVE
 CITY OF MENLO PARK SAN MATEO COUNTY CALIFORNIA

Revisions	No.	Date

Date: 7/27/2014
 Scale: 1"=20'
 Design: N/A
 Drawn: BKF
 Job No: 201407075.00

Drawing Number:
20147075
 18 OF 18



Facebook Campus Expansion Project
Building 22 Design Review and Request for Conditional
Development Permit Amendment
Project Description (Revised October 2017)

I. Introduction

Facebook is requesting design review for Building 22 and corresponding revisions to the site and phasing plan for the Facebook Campus Expansion Project. These changes generally include (i) shifting the parking program from surface parking beneath Building 22 into a stand-alone parking structure, (ii) reducing the footprint of Building 22, (iii) creating additional landscape reserve space, and (iv) installing new recharging facilities for Facebook's fleet of shuttle buses and trams. No material changes are proposed to the hotel, which has not yet been designed and construction of which is anticipated to commence after TE vacates the site.

The revised program reflects a refined architectural design for Building 22 and a site plan that was designed to stay within the scope of the Environmental Impact Report (EIR) and CDP in order to avoid any significant new impacts or any substantial increase in the severity of previously identified impacts. Facebook is not requesting any new uses or square footage.

As of April 27, 2017, this project description has been revised to include the following:

- Facebook will commit to demolishing Building 305 within twelve months of the date that TE vacates Building 305 (subject to receipt of all applicable permits and approvals, including any permits and approvals from state or federal agencies). In addition, Facebook agrees not to provide TE with any additional renewal or extension rights above and beyond what is in TE's existing lease (i.e., September 2022 will remain the "outside expiration date" for TE's lease).
- Facebook anticipates that there will be no net increase in the number of workers and visitors on-site during the interim

period when Building 22 and Building 305 could both be occupied (which is anticipated to be no more than three years). TE currently has approximately 110 workers within Building 305 working in 8-hour shifts, 24 hours a day (for a total of approximately 330 employees). While this existing condition could remain through September 2022 if TE exercises its renewal right, at the latest, the number of TE workers on-site is anticipated to be less than the number of workers and guests anticipated to be present at the hotel.

As of October 2017, this project description has been further revised to reflect design changes that were made in response to the comments expressed by the Planning Commission during the May 22, 2017 study session. These design changes include, but are not limited to:

- Lowering the foundation of the parking structure by approximately 6', which was the maximum reduction found to be feasible, and setting back the extent of the roof-top solar canopy feature for the parking structure which will reduce the appearance of an additional story such that the garage "reads" as approximately 72' high rather than 84' high as previously presented;
- Eliminating the "green wall" façade for the parking structure on each of the elevations to break up the mass of the garage and let natural light penetrate through the garage to add transparency;
- Providing enhanced landscaping in front of the parking structure (including large trees and understory vegetation) and corresponding façade planters which will provide screening and create a more balanced site; and
- Enhancing opportunities for passive recreation and create a more inviting and welcoming space by refining the public open space area to the southeast of Building 22.

II. Background

In November 2016, the City Council approved the Facebook Campus Expansion project. An EIR was prepared and certified

to analyze the environmental impacts from the project pursuant to the California Environmental Quality Act (CEQA). The EIR analyzed the overall effects of the project and identified mitigation measures to mitigate the project's significant impacts where feasible.

The project approvals included a Development Agreement, the CDP, a zoning ordinance text amendment (to accommodate the proposed hotel), a lot line adjustment, heritage tree removal permits, and a below-market rate housing agreement, among other approvals (collectively, the Approvals). The City Council also adopted CEQA Findings and a Mitigation Monitoring and Reporting Program for the project, as well as a Statement of Overriding Considerations concluding that the project's substantial benefits outweigh its environmental effects.

The project as described in the EIR contains two office buildings (Buildings 21 and 22) and a 200 room hotel, as well as a pedestrian and bicycle bridge across the Bayfront Expressway and a new, approximately two-acre publicly accessible plaza and open space. As described in the EIR's project description and on the City's website for the project, the project would be phased in over time as the existing tenants (Pentair and TE Connectivity) vacate the site.

In November 2016, the Planning Commission and City Council approved Building 21 through the City's design review process, concurrently with their approval of the project. Facebook commenced construction of Building 21 in December 2016, and anticipates completing Building 21 in mid-2018.

A. The Approved Project

The approved project as described in the EIR and the Approvals involves two office buildings, comprising approximately 962,000 gsf of office and amenity uses, and a 200 room hotel to be constructed on a portion of a 58 acre site located at 300 to 309 Constitution Drive within the City of Menlo Park. The project is subject to a site-wide trip cap to limit the number of peak hour and daily trips to and from the site.

Pursuant to the CDP and the Approvals, development on the site

is limited to a .45 FAR for office uses and a maximum of .55 FAR for all uses (including the hotel), a building height limit of 75', and a minimum requirement of 3,533 parking spaces for the two new office buildings, hotel, and Building 23.

1. Building 21

Building 21 will be located in the eastern portion of the Project site and contain approximately 512,900 gsf of office and event uses. Building 21 will be a multi-story building on a podium structure above an at-grade parking lot. Facebook commenced construction of Building 21 in December 2016 after City approval in November 2016.

No changes are proposed to Building 21, with the exception of the location of the proposed open-air bridge connecting Building 21 and Building 22.

2. Building 22

Building 22 will be located in the northwestern portion of the Project site and contain approximately 449,500 gsf of office and event uses. At the time the EIR was prepared and as described in the Approvals, Building 22 had not yet been designed and it was contemplated that design review for Building 22 would occur in the future.

As described in the EIR, Building 22 was anticipated to be similar to Building 21. It was to include a ground level that had multiple lobbies and parking spaces for approximately 1,294 vehicles, a first floor accommodating office and amenity space, and potentially a mezzanine level. Useable open space would be provided on the roof, which would include landscaped areas, walking paths, and HVAC equipment, similar to Building 21. An outdoor terraced area was also anticipated to be located adjacent to the food court/dining area on the south side of Building 22. The food court would be separated from the main level by the outdoor terraced area, which would allow for outdoor dining. It was also anticipated that Building 22 would be connected to Building 21 through an open-air bridge.

In late 2016, Facebook commenced the design process for Building 22. After reviewing more than fifty different design

schemes prepared by Gehry Partners, Facebook selected a preferred design. The revised design for Building 22 is further described below, including changes from the conceptual design that was evaluated in the EIR.

3. Hotel

The project includes a 200-room hotel that could be developed as part of a future phase near the corner of Chilco Street and SR 84. Although the hotel had yet to be designed, the EIR studied a maximum hotel envelope that provided approximately 73,200 gsf of hotel and support space, approximately 1,800 gsf of office space, approximately 13,700 gsf of amenities, and 86,100 gsf of circulation, wall, structure, and stair space. Included in the amenities would be food and beverage areas for the public, multi-function space, a fitness room, a pool, and deck areas. The hotel was also assumed to be approximately 75 feet in height.

No changes are proposed to the hotel, which still has not yet been designed and would be subject to a future design review process. Facebook has also been actively meeting with potential hotel developers/partners and expects to select a preferred developer/partner within the next 12 months.

4. Publicly Accessible Pedestrian and Bicycle Bridge; Publicly Accessible Plaza

As part of the Approvals, Facebook committed to constructing a new publicly accessible pedestrian and bicycle bridge across the Bayfront Expressway and a two-acre publicly accessible park for passive recreational uses and community events.

No changes are proposed to the publicly accessible pedestrian and bicycle bridge or the public park in connection with the current application. However, the overall scope and geographic limits of the public park have been refined in consultation with City staff, and the updated plans have significantly expanded the usable footprint of the public park by adding an additional pedestrian path and passive recreation space into the area occupied by the original footprint of Building 22.

B. Project Phasing and Schedule

1. MPK 21 (Phase 1)

Phase 1 involves construction of Building 21 as well as the commencement of permitting for the pedestrian and bicycle bridge. Consistent with the schedule identified in the EIR, grading and utility work for Building 21 began in late fall 2016, foundation permits were issued in December 2016, and construction is well underway. The permitting process for the pedestrian and bicycle bridge has also commenced and Caltrans is currently in the process of reviewing plans. No changes are proposed with respect to construction of Phase 1 of the project.

2. MPK 22 and Hotel (Phases 2 and 3)

As described in the EIR and contemplated in the Approvals, construction of the project will be phased to allow existing tenants to continue operating. The construction schedule analyzed in the EIR contemplated that construction of Building 22 would start in early 2018 with demolition of Buildings 301-306 and the CTF, and that construction of the hotel would commence in early 2019. This schedule was proposed by Facebook in order to ensure the most conservative environmental analysis possible under a scenario in which the existing tenants on the site vacate prior to the expiration of their leases. Pentair has vacated the site and TE has vacated Buildings 302, [303/304/306](#), and the CTF, all of which are located within the western portion of the site.

Currently, the only building occupied by TE is Building 305. TE's lease rights for Building 305 expire in September 2019. TE has one option to extend the term of its lease for an additional 3 years (i.e., until September 2022). TE also has the right to terminate its lease early. If TE extends its lease for Building 305, there is also an incentive for TE to terminate its lease early (in September 2020); if TE elects not to exercise this early termination option and vacate Building 305 in September 2020, it will forego this incentive payment. Currently, TE is not anticipated to vacate Building 305 until September 2020 – assuming TE elects to take advantage of its incentive option - or September 2022 at the latest. Facebook has had discussions with

TE regarding a potential early termination of the lease of Building 305, but as of this time it does not appear that TE will surrender that building any earlier than September 2020 as it remains necessary for TE's operations.

Because it now appears that TE may remain in Building 305 until September 2020 (or 2022 at the very latest), Facebook has made modifications to the site plan for Phase 2 and refined the design for Building 22 to allow construction to occur while Building 305 remains occupied. Accordingly, the anticipated construction phasing schedule is as follows:

Phase 2

Demolition of Buildings 301, 302, [303/304/306](#) and the CTF, would occur during Phase 2. It is anticipated that demolition would begin in late-2017.

Construction of the parking structure would start following approval of the requested project revisions with grading and utility work followed immediately by the foundations and structure. The parking structure is anticipated to be complete in early 2019.

Construction of Building 22 would start in mid-2018 with grading and utility work. Foundations would start in mid-2018, construction of the core and shell would start in late 2018, and tenant improvements would start in mid-2019. Construction of Building 22 is expected to be complete by late 2019.

Phase 3

Phase 3 would begin upon demolition of Building 305 which is anticipated to occur in late 2020 or late 2022 depending on when TE vacates Building 305.

If TE vacates Building 305 in September 2020, then construction of the hotel could start in late 2020 with demolition of the surface parking. Grading and utility work would start in early 2021, foundations would start in mid-2021, and construction of the core and

shell would start in late 2021. Construction of the hotel would take approximately 18 months, with full buildout by mid-2022. Completion of the open space improvements and recharging facilities is anticipated to take approximately 12 months would be completed by late 2021.

If TE vacates Building 305 in September 2022, then construction of the hotel could start in late 2022 with demolition of the surface parking. Grading and utility work would start in early 2023, foundations would start in mid-2023, and construction of the core and shell would start in late 2023. Construction of the hotel would take approximately 18 months, with full buildout by mid-2024. Completion of the open space improvements and recharging facilities would be completed by late 2023.

3. Other Timing Considerations under the Project Approvals.

Neither the CDP nor the DA requires a specific phasing schedule (e.g., that demolition of Building 305 precede construction of Building 22). Section 4.4 (Timing) of the DA expressly states that “. . . no moratorium or other limitation affecting the development and occupancy of the Project or the rate, timing or sequencing thereof shall apply to the Project.” The only requirement with respect to TE’s lease occurs under the DA, which provides that Facebook’s transient occupancy tax guarantee payment obligation is triggered two years after the “TE Vacation Date.” That term is defined as the date lease agreement between Facebook and Tyco Electronics Corporation has been terminated and TE has vacated all buildings leased by TE on the Property, which is September 2022 (assuming TE exercises its extension option) unless TE agrees to terminate the lease and vacate the buildings early.

However, the City has expressed some concern about further delays that might arise if TE were able to extend the term of its

lease beyond what is now permitted under the lease and a desire for more certainty surrounding the ultimate demolition of Building 305. In response to that concern, Facebook will commit to demolishing Building 305 within twelve months of the date that TE vacates Building 305 (subject to receipt of all applicable permits and approvals, including any permits and approvals from the Department of Toxic Substances Control and/or the U.S. Environmental Protection Agency). In addition, Facebook agrees not to renew or extend TE's lease beyond the outside expiration date of September 2022 (assuming that TE exercises its existing renewal option and foregoes its incentive to vacate in 2020).

III. Building 22 Design and Changes to the Project

In late 2016, Facebook requested that Gehry Partners undertake a reevaluation of the conceptual drawings for Building 22. Gehry Partners and Facebook determined that the footprint of the overall building could be reduced in size without altering the overall envelope or impact of the project, such that Building 22 would remain well within the previously approved footprints and square footage envelopes while accommodating TE's continued use of Building 305 through the end of its lease.

The revised project conforms to the permitted FAR limits, setback requirements, building coverage and minimum open space requirements, maximum height limit of 75', and total number of parking spaces permitted. In addition, the revised project does not require any change in the permitted uses, density or intensity of uses, provisions for the reservation or dedication of land, restrictions or requirements relating to subsequent discretionary actions, any monetary obligations of Facebook, or any conditions or covenants limiting or restricting the use of the site.

The only change with respect to the overall site calculations is that total building coverage (at full buildout) will be reduced from approximately 1,311,977 square feet (52% of the site) to 1,000,935 square feet (approx. 39% of the site). During the interim phase where Building 305 remains on-site, total building

coverage will be approximately 1,206,415 square feet (approximately 48% of the site).

With respect to FAR limits, construction of Building 22 would not exceed the site's .45 FAR requirement for office uses. While Building 305 remains, total FAR on-site would remain below the site's .55 FAR limit. As contemplated in the Project Approvals, including the Development Agreement, construction of the hotel could commence after TE fully vacates the site, which would ensure that at no point in time would the site's FAR exceed 0.55.

1. Building 22 Design Narrative

The design for Building 22 contemplates an approximately 75' tall, four-story office building with an adjacent parking structure to the west. Access to the building will be provided through lobbies that are located at the east, center, and west ends of the building. A bus and tram stop/terminal will be located north of the building. Pedestrian bridges will connect the west lobbies to the parking structure. The design approach aims to provide a highly functional office building for Facebook while respecting the characteristics of the adjacent neighborhood. The office program includes a variety of conference meeting rooms, offices, food service venues, and extensive support spaces, consistent with the program analyzed for Building 22 in the EIR. It is the design intent to bring as much natural light as possible into the office work spaces, including through the use of an interior atrium space that would extend throughout the building, and provide easy access to the outdoors. The office space is planned to consist of open floor plans totaling approximately 449,500 sf., also consistent with the CDP and EIR. The building is oriented to face the Belle Haven neighborhood south of the site, and aims to enhance the local environment with California native vegetation.

In addition, Facebook is requesting clarification in the CDP to permit architectural skylights that would extend beyond the current 75' foot height limit. The skylights would not extend

higher than the proposed rooftop mechanical equipment, and would not create additional visual obstruction. Facebook therefore requests that this design feature be included as part of the City's design review process.

2. Parking Structure

Parking would be consolidated in a new 7-story, 72' 4" tall parking structure (measured from average finished grade to the guard rail), with below grade parking in a basement level, rooftop safety railings, elevator hoistways, and a solar canopy that would extend above the height limit as permitted under the CDP. New redwood trees would screen the eastern, western, and southern elevations, and Fremont poplar trees would screen the northern elevation. Planters would also interspersed throughout the exterior edge of each elevation. The parking structure is currently anticipated to accommodate approximately 1,736 spaces, which conforms to the CDP's parking requirements.

3. Intensity of Use

No change in the intensity of the uses on-site is proposed. While the existing conditions at Building 305 could remain for some limited duration while Building 22 is occupied, the total number of workers associated with Building 305 is well below the projected number of workers, visitors and guests for the hotel, and the site would remain subject to the peak hour and daily trip caps established in the CDP that apply to the entire site (including the existing buildings).

4. Site Access and Circulation

No changes are proposed with respect to site access. Circulation on the western portion of the site would be refined to reflect the consolidation of parking for the office buildings in a new parking structure. Analysis from Fehr & Peers has confirmed that the proposed circulation would not create queuing issues or modify the conclusions reached in the traffic analysis conducted for the EIR. Truck access would continue to be provided through a controlled driveway on Chilco Street at Constitution Drive, and would serve TE's interim use of Building 305 so long as it occupies the premises.

5. Landscaping, Site Design and Open Space

Compared to the proposed site plan evaluated in the EIR, the refined site plan would increase the amount of landscaping and other pervious materials on-site, and result in additional natural areas including stormwater treatment areas.

In total, there would be 2.2 acres of publically accessible space, which is larger than the 2 acre requirement in the CDP. In addition, the improvements are designed to enhance opportunities for passive recreation and gatherings, and create an inviting and welcoming space. For example, the public space consists of a park, plaza, and open space, with various meandering pathways and seating areas. The west edge of the lawn contains a picnic area with a series of stormwater gardens, pathways, and tree groves. The public space also provides flexibility and reserve space for potential future uses. No reduction in the amount of replacement trees is sought.

6. Final Site Plan post-Building 305 Demolition

Facebook proposes to replace the existing asphalt parking lots with additional landscaping, a shuttle and tram drop-off area, as well as recharging facilities for Facebook's shuttle buses and trams in an area previously identified as a surface parking lot. The unenclosed facility would be screened and accommodate approximately 50 shuttle buses and up to 23 trams in the area located east of Building 23.

October 11/16 2017

Re: Facebook Campus Expansion Project (MPK 22) – Revisions in Response to Planning Commission Comments.

Dear Members of the Menlo Park Planning Commission:

In advance of the October 16, 2017, hearing on the proposed revisions to the Facebook Campus Expansion Project, we are pleased to provide you with an update on various design changes to the program since we last presented the project in May.

Since that study session, we have worked with Gehry Partners and our consultant team to thoroughly evaluate all of your comments, questions, and opinions expressed during the study session. We are very excited to present the final design for the project and believe that our latest design changes address your issues of concern, while leaving the overall design approach intact.

These design changes include, but are not limited to:

- Lowering the foundation of the parking structure by approximately 6', which was the maximum reduction found to be feasible, and setting back the extent of the roof-top solar canopy feature for the parking structure which will reduce the appearance of an additional story such that the garage "reads" as approximately 72' high rather than 84' high as previously presented;
- Eliminating the "green wall" façade for the parking structure on each of the elevations to break up the mass of the garage and let natural light penetrate through the garage to add transparency;
- Providing additional screening and create a more balanced site plan with enhanced landscaping in front of the parking structure (including large trees and understory vegetation) and corresponding façade planters; and
- Enhancing opportunities for passive recreation and create a more inviting and welcoming space by refining the public open space area to the southeast of Building 22.

Below, we further describe these changes as well as other refinements incorporated into the design. We believe the process of receiving input and incorporating changes into our design approach has improved on the original design, and look forward to presenting the refined design to you at the hearing on October 16, 2017.

I. Design Revisions Incorporated into the Final Project

A. Overall Site Plan and Building 22 Design

We appreciate the positive feedback we heard regarding the architectural design of Building 22 and our efforts to provide a diversity of architecture. The design allows for modular use of space, while reflecting a different type of architecture and floor plan as compared to Buildings 20 and 21. Our goal was to ensure that the new Gehry Partners designed building was every bit as innovative and refined as the original concept. Based on the Planning Commission's support for the design, no material modifications are proposed to the design for Building 22, although the architecture and landscape design have been refined to add detail.

B. Proposed Parking Structure

We heard a number of concerns about the parking structure's height, massing, and the feasibility of the initially proposed "green wall" feature. In response, we worked closely with our design and engineering teams to evaluate potential alternatives that would make the parking structure feel more subservient to the office buildings and appear more attractive and visually interesting.

These efforts were subject to a number of site constraints, including the low water table, 75 foot height limit, Building 305's lease line, PG&E's easement, emergency vehicle access on the west side, and the 40 foot setback requirement from Building 22. In addition, the parking structure must satisfy the parking requirements for Buildings 22 and 23 and accommodate no less than 1,736 spaces. For those reasons, we determined that it would not be feasible to substantially reduce the structure's massing or enhance its articulation without sacrificing functionality or eliminating a substantial portion of the required parking.

Nonetheless, our efforts have resulted in several changes that we believe address some of the core concerns. In particular, we modified the initial design by (i) reducing the overall height of the structure, (ii) reducing the footprint of the rooftop PV solar canopy, and (iii) eliminating the "green wall" on each of the facades, as well as the mesh screen on the upper-most level. The new design includes a simpler façade that will be screened by large trees and strategically located planters on each of the parking levels generally following the contours of the tree line. The simpler design reduces the size of the structure, draws less attention, and will be easier to maintain.

Height: The CDP imposes a height limit of 75 feet, measured from average finished grade.¹ As initially proposed, the parking structure was 78 feet high measured from average finished grade to the guard rail at the highest parking level, which required an exception to the height limit in the CDP.² To address sea level rise, the first level was designed at grade (elevation of 9 feet 6 inches) to allow floodwater to freely flow into and out of the garage, similar to the approach used for Buildings 20 and 21. That design eliminated the need for underground parking, which would create enormous challenges including, but not limited to, substantial dewatering and dry-proofing to prevent floodwater up to the 13 foot elevation level.

¹ Solar panels and elevator towers are exempt from the height limit per the CDP.

² The staff report prepared in May in advance of the Planning Commission study session identified the height of the parking structure as 83 feet which reflected the height of the façade screening, which was originally proposed as extending past the guard rails on the top story and up to (but not including) the PV solar canopy.

Within these constraints, we were able to further “sink” the parking structure and make other changes to reduce the overall bulk and height. The structure’s height is now 72 feet 4 inches from the average finished grade to the guard rail – approximately 5 feet 2 inches lower than initially proposed and 2 feet 2 inches below the height limit. The reduced height was accomplished by removing the eighth level and adding below grade parking in a basement level. The basement level extends 3 feet 7 inches below the average finished grade, but importantly remains above the water table. This also eliminated the added head clearance necessary to accommodate surface draining on the ground level. The revised design is thus compliant with the CDP and, as further described below, allows the building to “read” as a story shorter.

Solar PV Array: The rooftop solar canopy was originally designed to extend 6’ beyond the facade of the structure. The original intent was to maximize electrical output, protect cars from the elements, and provide shade. However, we heard concerns that the canopy combined with the original screening registered as an additional story that increased the structure’s height and massing.

To minimize visual impacts and the appearance of an additional story, we reduced the footprint of the solar canopy by bringing it in from the edges of the building by approximately 12 feet. The structure now has seven clearly defined levels. It is also more transparent at the top, and less “heavy” feeling than the original design which we believe is an improvement on the original proposal.

Façade: As originally proposed, the exterior of the parking structure was covered with metal mesh screens and planters to create a “green wall” effect on each elevation. Vines covered the majority of the structure to screen the garage from view. The Planning Commission and City staff expressed concerns about the structure’s monolithic appearance, sustainability of the vines and maintenance requirements, as well as the ability to ensure that 20% of the structure is permeable for proper ventilation.

With those concerns in mind, we took a different approach to decorating and screening the structure. Vines will no longer screen the entire structure. Instead, numerous redwood trees have been added to the landscape plan in front of the eastern, western, and southern elevations. In addition, Fremont poplar trees have been added in front of the northern elevation. The poplars were selected for the north elevation due to their spreading canopy, which is not a preferable perch for raptors that might prey on the plover or marsh species that inhabit the salt basins. Planters are interspersed throughout the exterior edge of each elevation and generally follow the contours of the adjacent tree line. The planters will contain a mix of shrubs and creeping vines to provide seasonal variety and added texture to the elevations. The strategically placed landscaping minimizes the structure’s monolithic appearance and obscures the structure from view more than the original design. The structure will rise above the tree line, but articulation is enhanced as the parking levels that are visible vary depending on the size of the trees. The simplified design also emphasizes the structure’s functionality rather than drawing attention to its presence, consistent with Gehry Partners’ design intent.

C. Landscaping and Open Space

The Modified Project’s landscape improvements are designed to activate the open space, enhance public access and connectivity between the Belle Haven community and the Bay Trail, improve biking and pedestrian circulation, and create new campus social spaces. As originally designed, the landscape surrounding the parking structure and Building 22 will be similar to Buildings 20 and 21. Low

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landforms (up to 4-feet above existing grades) will be constructed and planted with understory plants. Stormwater treatment areas will be integrated within the landscape and planted with a wide variety of native species.

The Planning Commission had few comments concerning the general landscaping and open space program. However, there was some concern regarding the expanded publicly accessible open space located in the southeast part of the Building 22 site (east of the public plaza).

The first phase of the publicly accessible open space will be completed concurrent with Building 22 and include a plaza, planting areas and access to the multi-use bridge over Bayfront Expressway, totaling approximately 1.6 acres. Following removal of Building 305, the park and plaza will be expanded to include an additional area southeast of Building 22, resulting in a 2.2 acre publicly accessible space that is larger than the 2 acre requirement in the CDP.

The Commission specifically noted that the meandering pathway around the open space appeared to lack a destination and areas for recreation. To encourage use of the open space, we redesigned the pathways and significantly enlarged the lawn area adjacent to the plaza. We have also added long park benches and seating areas throughout the park. The west edge of the lawn is now defined by a picnic area with a series of stormwater gardens, pathways, and tree groves. These revisions combine to enhance opportunities for passive recreation and gatherings, and create a more inviting and welcoming space.

D. Mechanical Equipment Enclosure

Although the Planning Commission did not directly comment on the proposed screening material for mechanical equipment, we understand that City staff has expressed concerns about the type of material that would be used for the mechanical equipment enclosures. After careful consideration of several materials, including glass curtainwall and metal panels, we worked with staff and propose a tight metal mesh cladding which would be double-layered to create an additional layer of screening. This material furthers our design approach by softening the appearance of those structures and is a signature material on many of Gehry Partner's iconic buildings. The other options we considered made the enclosures appear more prominent, rather than less, which distracted from the landscaping, open space, and building elements that should be the focus of the viewer's attention. We continue to evaluate mesh options, and will continue work with City staff to ensure the final product is consistent with the overall design intent.

E. Conclusion

As detailed above, we believe that all of the design changes to the Modified Project are consistent with the guidance you provided during the study session. We have been working on this project with Gehry Partners since late 2016, with over 50 different design plans being evaluated before the Modified Project was selected. The result is a feasible design plan which enhances the architectural design and minimizes potential impacts to the Belle Haven neighborhood. The design plan also ensures that the buildings and associated improvements are compatible with the rest of Facebook's existing campus, while ensuring an enhanced architectural legacy for the City of Menlo Park.

Again, we look forward to presenting the refined project to you at the October 16, 2017, hearing and to breaking ground next year.

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With appreciation,



Fergus O'Shea
Director Campus Development

Cc: Alex McIntyre, City Manager
Chip Taylor, Assistant City Manager
Arlinda Heineck, Community Development Director
Kyle Perata, Senior Planner

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facebook

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ISA Tree Risk Assessment Qualified

E-mail: molly@sbcacree.com

Date: March 28, 2016

To: Rayna DeNoird, CMG

Subject: Tree Survey

Location: 301-309 Constitution Drive

Assignment: Arborist was asked to tag and survey all trees located on site, and City trees along Chilco Ave.

City of Menlo Park Ordinance

Definitions of Heritage Tree:

1. Any tree having a trunk with a circumference of 47.1 inches (diameter of 15 inches) or more measured at 54 inches above natural grade.
2. Any oak tree native to California, with a circumference of 31.4 inches (diameter of 10 inches) or more measured at 54 inches above natural grade.
3. Any tree or group of trees specifically designated by the City Council for protection because of its historical significance, special character or community benefit.
4. Any tree with more than one trunk measured at the point where the trunks divide, with a circumference of 47.1 inches (diameter of 15 inches) or more, with the exception of trees that are under twelve (12) feet in height, which are exempt from the ordinance.¹

Summary

- Scope of Survey – The tree survey recorded information on seven-hundred seventy (770) trees located on the grounds of 301-309 Constitution Drive and along the west end of Chilco St. Metal number tags were attached to all trees. Data was taken on Tree Size, Health and Structural Condition, Suitability for Retention, and Pertinent Notes.
- Two-hundred seventy-four (274) trees surveyed qualify as “Heritage Trees”.
- Thirty-four (34) different species were noted in the survey. The species most represented on site include London Plane (*Platanus x hispanica*) with one-hundred twenty-nine (129) specimens

¹ <http://www.menlopark.org/205/Heritage-Trees>

surveyed; Olive (*Olea europea*) representing sixty-seven (67) specimens; Monterey Pine (*Pinus radiata*) with sixty-eight (68); and Silver Dollar Gum (*Eucalyptus polyanthemos*) with fifty-four (54) specimens.

- Twenty-five (25) trees surveyed were dead; most are London Plane located along the southern property line. One (1) qualifies in size as “Heritage”.
- Trees given a “Poor” suitability for retention rating was based on severe health decline and resulting pathogen infestations, and/or poor past pruning often associated with poor tree placement. Soil conditions are considered limiting and the root cause of poor performance.

Summary of Tree Species

Table on following page provides information on the tree species surveyed and the number qualifying as Heritage Trees, with suitability for retention and pertinent notes. The survey data is provided in *Appendix 1*.

	Species	Common Name	Amount	Overall Condition	Amount of Heritage Trees	Suitability for Retention	Notes
1	<i>Acacia melanoxylon</i>	Black Wood Acacia	4	F	0	F	
2	<i>Acer palmatum</i>	Japanese Maple	3	F-P	0	P	Poorly pruned
3	<i>Alnus rhombifolia</i>	White Alder	8	F-P	1	F-P	On decline spiral
4	<i>Cedrus deodara</i>	Deodar Cedar	3	F	1	F	Located along southern perimeter
5	<i>Celtis sinensis</i>	Chinese Hackberry	3	P	0	P	Failure to thrive
6	<i>Eucalyptus conferruminata</i>	Bushy Yate	27	F-P	17	F-P	Poorly pruned; large heading cuts on almost all trees, Appropriate species for site
7	<i>Eucalyptus globulus 'Compacta'</i>	Dwarf Blue Gum	32	F	32	P	Most have been headed for high voltage lines
8	<i>Eucalyptus polyanthemos</i>	Silver Dollar Gum	54	F-P	8	P	Stressed, Lerp Psyllid
9	<i>Eucalyptus sideroxylon</i>	Red Iron Bark	14	F-P	1	P	No value
10	<i>Fraxinus udhei</i>	Shamel Ash	15	F	4	F	A few nice trees
11	<i>Gleditsia triacanthos inermis</i>	Honey Locust	2	P	0	P	Tip dieback, Located in courtyard



	Species	Common Name	Amount	Overall Condition	Amount of Heritage Trees	Suitability for Retention	Notes
31	<i>Schinus terebinthifolius</i>	Brazilian Pepper	16	P	9	P	Soil vol limitations, Dieback, Perimeter trees doing well
32	<i>Tristaniopsis laurina</i>	Water Gum	5	F	2	F	Poorly pruned
33	<i>Washingtonia robusta</i>	Mexican Fan Palm	1	P	0	P	No feet of clear trunk
Totals:			770		274		

End Report

Appendices

1. Tree Survey Data

Submitted By:



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ISA Tree Risk Assessment Qualified
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Date: October 21, 2016
To: Rayna deNiord, CMG
Subject: Facebook Bayfront Expressway, Tree Removal and Retention Review
Location: TE Site North Boundary (along Bayfront Expressway)
Assignment: Arborist was asked to review all trees located along the Bayfront Expressway frontage of the Facebook property to determine which trees may be suitable for retention in the new landscape.

Introduction

The original tree survey was conducted in early 2015. The most recent inspection was conducted to determine which trees along the Bayfront Expressway frontage could be retained, particularly in the areas where they can screen the powerline towers.

Inspection notes can be found on the accompanying Excel spread sheet. The last column provides comments from the recent inspection. Last column in spread sheet has recent comments. Significant changes in either structural condition of health have been yellow highlighted on the survey data. Trees most suitable for retention are highlighted in green.

Changes Noted

Tree Health - The most significant change noted since the prior survey data was recorded appears to be decline in tree health and death of a number of trees. The death and decline can be attributed to the cumulative effect of the prior draught coupled with water conservation. Trees that are accustomed to having supplemental moisture do not adapt well the sudden changes.

The Lerp Psyllid in the eucalyptus trees appears to have increased, also a likely result of the absence of supplemental irrigation.

Tree Structural Condition - Additional changes noted is the severity of lean in some trees, most notably Aleppo Pines but also in some of the Monterey Pines that have not died from lack of moisture. One Aleppo pine has fallen due to root failure and a number of other Aleppo Pines appear close. Trees that lean toward the parking lot are of most concern.

Tree Suitability

One-hundred six (106) trees were reviewed along the north perimeter of the Facebook property that is bordering the Bayfront Expressway. The suitability ratings are noted below.

- **GOOD SUITABILITY:** Ten (10) trees were assigned a Good Suitability rating. These trees considered to have good structural qualities and are in good health.
- **FAIR SUITABILITY:** Sixteen (16) trees were assigned a FAIR SUITABILITY rating. This rating is for trees that are in marginal condition but could be retained with pruning and/or health mitigation.
- **POOR SUITABILITY:** Thirty-six (36) trees were assigned a poor rating. This indicates that the tree is in poor condition and is not recommended for retention. In extreme cases one could be retained.
- **NOT SUITABLE:** Thirty-five (35) trees were noted as NOT SUITABLE meaning that under no circumstances can the tree can be retained due to health and/or structure.
- **DEAD:** Nine (9) trees were noted as DEAD.

Discussion of Species and Problems Noted

Acacia melanoxylon 3 - Two of the three trees are dead and the 3rd is dying and structurally problematic.

Eucalyptus conferruminata 1 – Nice tree but leaning over parked cars.

Eucalyptus polyanthemos -19 – Most have Lerp Psyllid. Over pruned and in poor health.

Myoporum laetum – 12 – Thrips, most ore dead or dying.

Pinus halepensis – 36 – One has fallen and many have a significant lean. This species is noted for root failure with one tree, # 502 having fallen.

Pinus radiata – 23 – Five Monterey Pines are now dead. Many are dying which is quite common due to the draught years. Some are savable but the future for this tree does not look good.

Pittosporum undulatum – 1 – The one tree is almost dead.

Platanus x hispanica – 8- Most are stunted and subject to anthracnose and powdery mildew. Likely not planted properly. Some could be saved but it would be better to plant new healthy trees in a suitable root environment.

Prunus cerasifera – 1 – This tree appears healthy but not suitable in the new landscape.

Quercus agrifolia – 2- Both oaks are in poor health.

END



2016 APPROVED EIR

BUILDING 301-309 SUMMARY -- PER FALL 2016 APPROVED EIR

REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
149		GOOD health	2:1	298
66		FAIR-POOR health	1:1	66
59		GOOD health	1:1	59
274 TOTAL				423 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE

496		See tree survey data	NA	NA
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BUILDING 307-309 SUMMARY (MPK 21 SITE) -- ALREADY REMOVED PER FALL 2016

REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
22		GOOD health	2:1	44
18		FAIR-POOR health	1:1	18
8		GOOD health	1:1	8
48 TOTAL				70 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE

307		See tree survey data	NA	NA
-----	--	----------------------	----	----

2017 PROPOSED EIR

BUILDING 301-306 SUMMARY -- EXCLUDING MPK 21 REMOVED TREES

REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
127		GOOD health	2:1	254
48		FAIR-POOR health	1:1	48
51		GOOD health	1:1	51
226 TOTAL				353 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE

189		See tree survey data	NA	NA
-----	--	----------------------	----	----

MPK 22 INTERIM PHASE

REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
86		GOOD health	2:1	172
29		FAIR-POOR health	1:1	29
33		GOOD health	1:1	33
148 TOTAL				234 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE

101		See tree survey data	NA	NA
-----	--	----------------------	----	----

MPK 22 FINAL PHASE

REMOVAL TOTALS	SYMBOL	DESCRIPTION	REPLACEMENT VALUE	REPLACEMENT TOTALS
Tree Removal & Required Replacement Values: HERITAGE				
41		GOOD health	2:1	82
19		FAIR-POOR health	1:1	19
18		GOOD health	1:1	18
78 TOTAL				119 TOTAL

Tree Removal & Required Replacement Values: NON-HERITAGE

88		See tree survey data	NA	NA
----	--	----------------------	----	----

NOTE: All proposed tree removals are required as a result of construction impacts.

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CONSULTANTS

Facebook Campus Expansion
Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TITLE
HERITAGE TREE DISPOSITION
SCHEDULE

Interim Condition

PROJECT NUMBER
2015-007
SCALE
NTS

July 26, 2017
11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

L1-100

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TREE LEGEND	
SYMBOL	DESCRIPTION
	GOOD health
	FAIR-POOR health
	GOOD health
	Non-heritage tree
	Tree with GOOD suitability for retention
	Tree with FAIR suitability for retention

MPK 22: FAIR suitability for retention										
Tag #	Species	DBH	Height	Structure	Health	Suitability for Retention	Notes	Total Trees	Total Heritage	
383	<i>Pinus halepensis</i>	24	25	G	F	F-P	Old tag #272, Lean, PP, CD	1	1	
384	<i>Pinus halepensis</i>	8	20	G	P	F-G	Seedling?, EB, SP	1	0	
385	<i>Pinus halepensis</i>	29	45	G	F	F-G	Old tag #540, CD, Stub cuts, Large pruning wounds	1	1	
441	<i>Pinus halepensis</i>	20.5	25	F	F	F	Cable in tree, CD	1	1	
459	<i>Pinus halepensis</i>	15	30	G	F-P	F	Significant lean, Large pruning wounds, Crowded	1	1	
462	<i>Pinus halepensis</i>	26.5	25	G	F-P	G	CD, Lean	1	1	
470	<i>Platanus x hispanica</i>	8.5	35	F-G	F-G	G	Anthraxnose, CD, High voltage power lines	1	0	
479	<i>Pinus radiata</i>	12.5	40	F-G	G	F	Lean	1	0	
481	<i>Pinus radiata</i>	14	40	F	G	F		1	0	
500	<i>Pinus radiata</i>	12.5	40	F	F-G	F		1	0	
507	<i>Pinus radiata</i>	17	40	F	G	F		1	1	
508	<i>Eucalyptus conferruminata</i>	9.5	25	G	F	F-P	Lean over parking lot, Vehicle damage	1	0	
511	<i>Pinus radiata</i>	14	45	F	F	F		1	0	
Total MPK 22								13	6	

MPK 22: GOOD suitability for retention										
Tag #	Species	DBH	Height	Structure	Health	Suitability for Retention	Notes	Total Trees	Total Heritage	
387	<i>Pinus halepensis</i>	20	25	F-P	F-P now F	F	Off color, H, Lean, CD	1	1	
440	<i>Pinus halepensis</i>	29	40	F-G	F	G	CD, DW	1	1	
457	<i>Pinus halepensis</i>	29 @ 2'	45	G	G	G	CD	1	1	
458	<i>Pinus halepensis</i>	16.5	30	F-G	F	F	Crowded, DW	1	1	
464	<i>Pinus halepensis</i>	28.5 @ base	45	G	F-G	G	Large pruning wound, Nice tree	1	1	
Total MPK 22								5	5	

NOTES

1) Exhibit shows overlay of existing heritage and non-heritage trees with the proposed plans for MPK 21 & MPK 22.

2) SBCA Arborist revisited the site on 19-October 2016 to review all existing trees located along the Bayfront Expressway frontage of the TE site. The task was to determine which ones may be suitable for retention in the new landscape to provide screening to the site and existing PG&E towers. Note: the original survey was conducted in early 2015.

3) Inspection notes for all one-hundred six trees (106) trees surveyed can be found in accompanying **TREE SURVEY DATA, 19-Oct 2016**. The last column provides comments from the recent inspection. Significant changes in either structural condition or health have been yellow highlighted on the survey data.

4) Trees most suitable for retention are highlighted in green and given a ranking of 'GOOD'.

5) Trees that could be suitable for retention are highlighted in blue and given a ranking of 'FAIR'.

6) All existing heritage and non-heritage trees of 'GOOD' or 'FAIR' suitability for retention that fall within the **MPK 21** project site are in conflict with the proposed site plan and will not be possible to retain.

7) All existing heritage and non-heritage trees of 'GOOD' or 'FAIR' suitability for retention that fall within the **MPK 22** project site could potentially be kept for screening as they fall within proposed landscape areas. This will be determined at the time of further project development.

8) For trees of 'GOOD' or 'FAIR' suitability for retention & plan location, see schedule & plan on this sheet.

9) For all other trees of 'POOR', 'NOT SUITABLE', or 'DEAD' suitability for retention, see accompanying **TREE SURVEY DATA, 19-Oct 2016**.

10) SBCA report includes further information on analysis of trees suitable for retention, see accompanying **ARBORIST REPORT, 19-Oct 2016**.

11) See Tree Disposition Plans, Data, and Arborist Report included with the Heritage Tree Removal permit and EIR documentation for full TE site documentation

SUITABILITY KEY

CHANGES IN STRUCTURE AND HEALTH (Yellow): First letter indicates prior condition (March 2015) with the second letter after "now" indicating recent assessment where there is a significant change in either health or structural condition.

GOOD SUITABILITY (Green): Indicates trees in good condition that could be retained if not in conflict.

FAIR SUITABILITY (Blue): Indicates trees in fair condition that could possibly be retained if not in conflict.

HOTEL SITE TREES IDENTIFIED FOR FURTHER EVALUATION

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Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TITLE
HERITAGE TREE POTENTIAL
PRESERVATION SCHEDULE

Interim Condition

PROJECT NUMBER
2015-007

SCALE
1" = 75'-0"

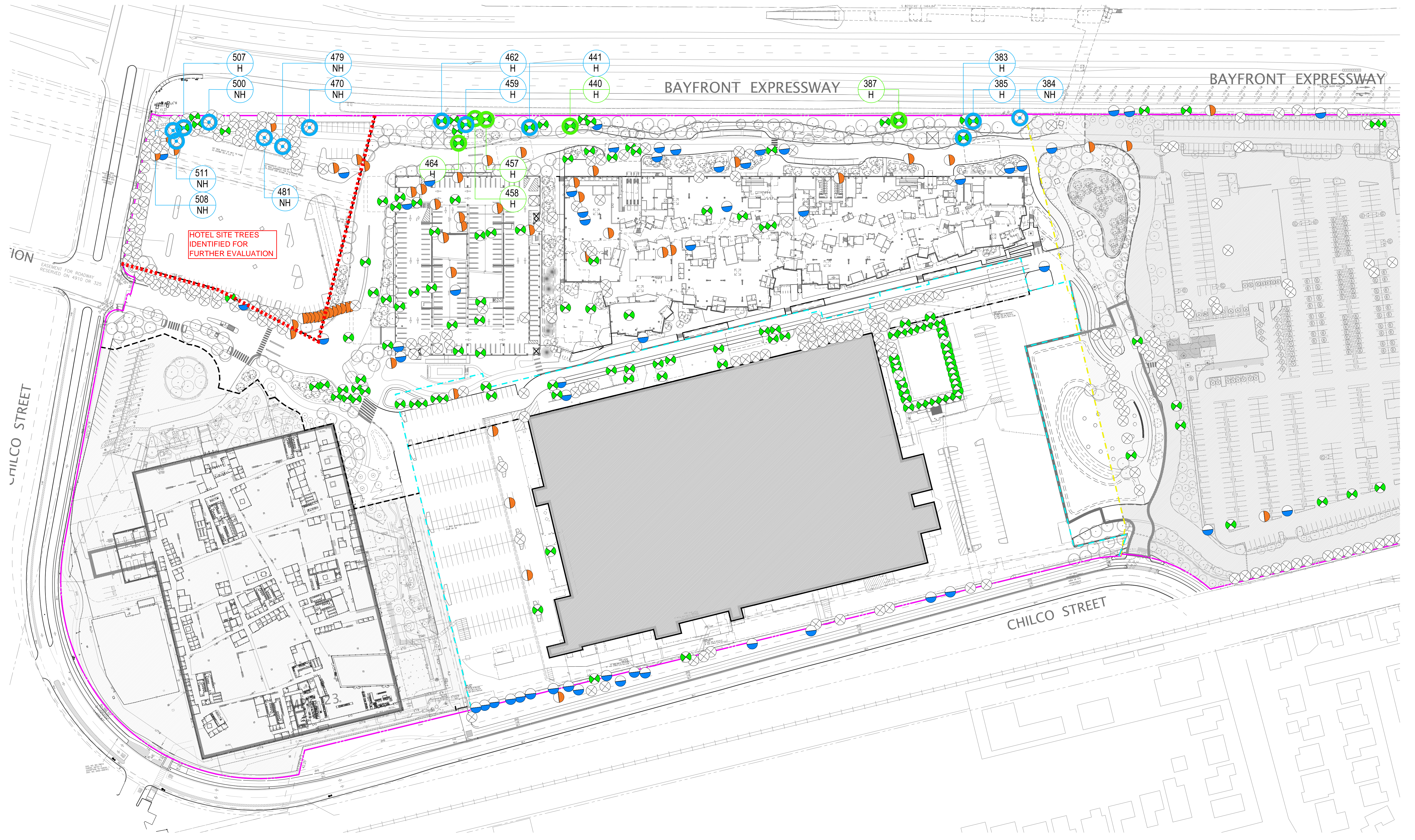
July 26, 2017

11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

L1-120

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HOTEL SITE TREES
IDENTIFIED FOR
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PRESERVATION PLAN

Interm Condition

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2015-007
SCALE
1" = 75'-0"

July 26, 2017
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SHEET NUMBER

L1-121

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Molly Batchelder, Consulting Arborist
WC ISA Certified Arborist #9613A
ISA Tree Risk Assessment Qualified
E-mail: molly@sbcatree.com

Date: October 21, 2016
To: Rayna deNiord, CMG
Subject: Facebook Bayfront Expressway, Tree Removal and Retention Review
Location: TE Site North Boundary (along Bayfront Expressway)
Assignment: Arborist was asked to review all trees located along the Bayfront Expressway frontage of the Facebook property to determine which trees may be suitable for retention in the new landscape.

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Bayfront Expressway Frontage Tree Suitability
Facebook MPK 21, 22 & 23
10-21-16
2 of 3

Tree Suitability

One-hundred six (106) trees were reviewed along the north perimeter of the Facebook property that is bordering the Bayfront Expressway. The suitability ratings are noted below.

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- **FAIR SUITABILITY:** Sixteen (16) trees were assigned a FAIR SUITABILITY rating. This rating is for trees that are in marginal condition but could be retained with pruning and/or health mitigation.
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- **DEAD:** Nine (9) trees were noted as DEAD.

Discussion of Species and Problems Noted

Acacia melanoxylon - 3 - Two of the three trees are dead and the 3rd is dying and structurally problematic.

Eucalyptus conferruminata - 1 - Nice tree but leaning over parked cars.

Eucalyptus polyanthemus - 19 - Most have Lerp Psyllid. Over pruned and in poor health.

Myoporum laetum - 12 - Thrips, most are dead or dying.

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Prunus cerasifera - 1 - This tree appears healthy but not suitable in the new landscape.

Quercus agrifolia - 2 - Both oaks are in poor health.

END

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MEMORANDUM

Date: July 26, 2017 To: Kyle Perata
Re: MPK 22 Heritage Tree Preservation Analysis From: Chris Gullard
Project: Facebook Campus Expansion
Cc: Gabe Patee, Steve Tsuruoka, Ryan Patterson, Marinus Lamprecht, Danielle Douthett, Judith Mussel, Rayna Deniord

1 of 2

Heritage Tree Preservation for Site Screening Suitability Analysis – MPK 22 Project Update

As part of the Original Campus Expansion and EIR Facebook's design team and project arborist worked with the City to evaluate the suitability of retention of existing trees along the north edge of the project site. One-hundred-six (106) existing trees – both heritage and non-heritage – along the Bayfront edge of the TE site were analyzed to determine whether any could be retained for screening of the site & existing PG&E towers. The health of each tree was re-evaluated by SBCA Arborists in October 2016 – this information is included in the accompanying documents:

- 161021-TE-Site-Heritage-Tree-Preservation-Analysis_ARBORIST-REPORT.pdf
- 161021-TE-Site-Heritage-Tree-Preservation-Analysis_TREE-DATA.pdf

SBCA identified twenty-six (26) existing trees as candidates for retention based on current health & structure:

- **GOOD SUITABILITY:** There were a total of ten (10) trees identified as GOOD suitability, all heritage
- **FAIR SUITABILITY:** There were a total of sixteen (16) trees identified as FAIR suitability, eight (8) heritage, eight (8) non-heritage.

The existing tree locations were overlaid on the proposed plans for MPK 21 & MPK 22 to determine where conflicts with the improvements occur.

444 Bryant St San Francisco Ca 94107 415 495 3070 cmgslate.com



2 of 2

Eight (8) of the trees identified as possible candidates for retention were within the MPK 21 project area and they were removed consistent with the EIR and Tree Removal Permit for MPK 21.

Of the remaining eighteen (18) trees (heritage & non-heritage) that are within the MPK 22 and Hotel project areas, (11) are within the MPK 22 project area and (7) are within the Hotel project area.

MPK 22: The (11) trees within the MPK 22 area will be impacted by construction (Utilities and Grading) and will need to be removed.

Hotel Site: At this point we cannot determine whether the remaining (7) trees on the Hotel site can be preserved because the building and site design remain conceptual. For purposes of the EIR Amendment and Heritage Tree review it should be assumed that these trees will be removed. Facebook and the design team will continue make every effort to retain suitable trees where feasible and appropriate based on further evaluation. The (7) trees identified as suitable for retention on the Hotel Site are Tree #s 470, 479, 481, 500, 507, 508 and 511. The species, size and condition of these trees (as of October 2016) are included in the Heritage Tree – Potential Preservation Analysis Schedule on sheet L1-120 and the locations are clearly indicated on sheet L1-121 of the July 17, 2017 EIR document Submittal.

Tree Removal Permits and Landscape Plans confirming tree replacement quantities and sizes will be submitted with each phase of construction as part of the building permit review process.

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Facebook Building 21, 22 & Hotel Site
300-309 Constitution Drive

TITLE
HERITAGE TREE POTENTIAL
PRESERVATION
ARBORIST REPORT & MEMO
Interim Condition

PROJECT NUMBER
2015-007
SCALE
1" = 75'-0"

July 26, 2017

11x17 SHEET SIZE: N.T.S.
ORIGINAL SHEET SIZE: 24" x 36"

SHEET NUMBER

L1-122

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MPK22 FINAL PHASE - Total Existing Trees:	81
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Heritage Trees	58
-----------------------	-----------

To Remove:	Total	Replacement Value	Replacement Totals
Fair-Good health	37	2:1	74
Fair-Poor health	17	1:1	17
Poor-Dead health	4	1:1	4
Total	58		95
To Remain:			
Good Health	0		
Total	0		

Non Heritage Trees	23
---------------------------	-----------

To Remove:	23
To Remain:	

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
639	<i>Pittosporum tobira</i>	5.5 @ base	15	F	F-P	N	P		Lean, CD
640	<i>Pittosporum tobira</i>	5.5 @ base	15	F	F	N	P		CD
641	<i>Quercus agrifolia</i>	4	25	G	G	N	G		Relocate?
642	<i>Pittosporum tobira</i>	4	15	P	G	N	P		Internal decay, Hollow
643	<i>Tristaniopsis laurina</i>	7.5	25	G	F-P	N	F		Old tag #250
644	<i>Leptospermum laevigatum</i>	13.5 @ base	15	F	F	N	F		Off color, Multi
645	<i>Leptospermum laevigatum</i>	40 @ base	12	F	F	Y	F	1	Multi
646	<i>Leptospermum laevigatum</i>	20 @ base	15	F	F	Y	F	1	Multi
647	<i>Leptospermum laevigatum</i>	19 @ base	12	F	F	Y	F	1	Multi, Rhamnus understory

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
648	<i>Leptospermum laevigatum</i>	9 @ base	12	P	P	N	P		Vandalism w chain saw
649	<i>Leptospermum laevigatum</i>	20 @ base	12	F	F	Y	F	1	Multi
650	<i>Leptospermum laevigatum</i>	37 @ base	12	F	F	Y	F	1	Multi
651	<i>Leptospermum laevigatum</i>	35 @ base	12	F	F	Y	F	1	Multi
652	<i>Leptospermum laevigatum</i>	19 @ base	12	F	F	Y	F	1	Multi
653	<i>Leptospermum laevigatum</i>	15 @ base	12	F	F	Y	F	1	Multi
654	<i>Leptospermum laevigatum</i>	13 @ base	12	F	F	N	F		Multi
655	<i>Leptospermum laevigatum</i>	18.5 @ base	12	F	F	Y	F	1	Multi
656	<i>Leptospermum laevigatum</i>	18 @ base	12	F	F	Y	F	1	Multi
657	<i>Leptospermum laevigatum</i>	15 @ base	12	F	F	Y	F	1	Multi
658	<i>Leptospermum laevigatum</i>	15 @ base	12	F	F	Y	F	1	Multi
659	<i>Leptospermum laevigatum</i>	21 @ base	12	F	F	Y	F	1	Multi
660	<i>Leptospermum laevigatum</i>	17.5 @ base	12	F	F	Y	F	1	Multi
661	<i>Leptospermum laevigatum</i>	35 @ base	12	F	F	Y	F	1	Multi
662	<i>Leptospermum laevigatum</i>	23 @ base	12	F	F	Y	F	1	Multi
663	<i>Leptospermum laevigatum</i>	21.5 @ base	12	F	F	Y	F	1	Multi
664	<i>Leptospermum laevigatum</i>	22 @ base	12	F	F	Y	F	1	Multi
665	<i>Leptospermum laevigatum</i>	30 @ base	12	F	F	Y	F	1	Multi

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
666	<i>Leptospermum laevigatum</i>	15 @ base	12	F	F	Y	F	1	Multi
667	<i>Leptospermum laevigatum</i>	17 @ base	12	F	F	Y	F	1	Multi
668	<i>Leptospermum laevigatum</i>	16 @ base	12	F	F	Y	F	1	Multi
669	<i>Leptospermum laevigatum</i>	17 @ base	12	F	F	Y	F	1	Multi
670	<i>Leptospermum laevigatum</i>	6 @ base	12	F	F	N	F		Multi
671	<i>Leptospermum laevigatum</i>	20 @ base	12	F	F	Y	F	1	Multi
672	<i>Leptospermum laevigatum</i>	22 @ base	12	F	F	Y	F	1	Multi
673	<i>Leptospermum laevigatum</i>	26 @ base	12	F	F	Y	F	1	Multi
674	<i>Leptospermum laevigatum</i>	14 @ base	12	F	F	Y	F	1	Multi
675	<i>Leptospermum laevigatum</i>	21.5 @ base	12	F	F	Y	F	1	Multi
676	<i>Leptospermum laevigatum</i>	17.5 @ base	12	F	F	Y	F	1	Multi
677	<i>Leptospermum laevigatum</i>	27 @ base	12	F	F	Y	F	1	Multi
678	<i>Leptospermum laevigatum</i>	23.5 @ base	12	F	F	Y	F	1	Multi
679	<i>Leptospermum laevigatum</i>	25 @ base	12	F	F	Y	F	1	Multi
680	<i>Leptospermum laevigatum</i>	28 @ base	12	F	F	Y	F	1	Multi
690	<i>Washingtonia robusta</i>	0' of CT	-	G	G	N	P		Seedling
691	<i>Tristaniopsis laurina</i>	5	15	F	P	N	P		CD
692	<i>Eucalyptus globulus 'Compacta'</i>	34 @ base	25	P	G	Y	P	1	Multi, H
693	<i>Eucalyptus globulus 'Compacta'</i>	30.5 @ base	25	P	F-G	Y	P	1	Tortoise shell beetle

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
694	<i>Prunus cerasifera</i>	13 @ base	20	F	G	N	P		Seeding, Sprouts
695	<i>Malus spp.</i>	8.5 @ base	10	F	G	N	F		CD
696	<i>Melaleuca citrina</i>	7	20	F	G	N	F		Multi
697	<i>Schinus terebinthifolius</i>	10.5	20	G	G	N	G		Lean, Nice tree
698	<i>Eucalyptus globulus 'Compacta'</i>	34	25	P	G	Y	P	1	Multi, PP, H for high voltage power lines
699	<i>Eucalyptus globulus 'Compacta'</i>	25.5	25	P	G	Y	P	1	Multi, PP, H for high voltage power lines
700	<i>Schinus terebinthifolius</i>	9	20	F	G	N	F-G		Sprouts, Crossing branches, Nice little grove
701	<i>Schinus terebinthifolius</i>	6.5	20	F	G	N	G		EB, Nice little grove
702	<i>Schinus terebinthifolius</i>	13.5	20	F-P	G	N	F-G		CD, Nice little grove
703	<i>Schinus terebinthifolius</i>	23 @ base	20	P	G	Y	F-G	1	CDEB, Nice little grove
704	<i>Eucalyptus globulus 'Compacta'</i>	46 @ base	25	F	G	Y	P	1	Multi, H for high voltage power lines
705	<i>Eucalyptus globulus 'Compacta'</i>	28 @ base	20	P	F	Y	P	1	Multi, H for high voltage power lines
706	<i>Fraxinus udhei</i>	19.5 @ base	25	P	G	Y	P	1	Multi, Seedling, Growing in fence
707	<i>Eucalyptus globulus 'Compacta'</i>	40 @ base	25	P	G	Y	P	1	Multi, H for high voltage power lines
708	<i>Cedrus deodara</i>	7	25	F-P	F	N	F		One sided
709	<i>Acacia melanoxylon</i>	11	25	P	G	N	P		CDEB
710	<i>Cedrus deodara</i>	16 @ base	25	F-P	G	Y	F-P	1	Significant lean, CD
711	<i>Eucalyptus globulus 'Compacta'</i>	34	25	P	G	Y	P	1	CD, H for high voltage power lines
712	<i>Eucalyptus globulus 'Compacta'</i>	31 @ base	35	P	F-G	Y	P	1	CD, H for high voltage power lines
713	<i>Eucalyptus globulus 'Compacta'</i>	30 @ base	25	P	F-G	Y	P	1	Multi, H for high voltage power lines
714	<i>Myoporum laetum</i>	21 @ base	20	P	P-D	Y	P	1	Thrips

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
715	<i>Eucalyptus globulus 'Compacta'</i>	23 @ base	25	P	F-G	Y	P	1	Multi, H for high voltage power lines
716	<i>Eucalyptus globulus 'Compacta'</i>	25 @ base	20	P	F	Y	P	1	CD, H for high voltage power lines
717	<i>Eucalyptus globulus 'Compacta'</i>	23.5 @ base	25	P	G	Y	P	1	Multi, H for high voltage power lines
718	<i>Eucalyptus globulus 'Compacta'</i>	28 @ base	25	P	G	Y	P	1	Inside closed fence, CD, H for high voltage power lines
719	<i>Eucalyptus globulus 'Compacta'</i>	21 @ base	25	P	G	Y	P	1	Inside closed fence, H for high voltage power lines
720	<i>Eucalyptus globulus 'Compacta'</i>	28 @ base	25	P	G	Y	P	1	Multi, H for high voltage power lines
721	<i>Cedrus deodara</i>	8	25	G	P	N	F-P		Lean
724	<i>Olea europaea</i>	13.5 @ 2'	20	F	F	N	F	1	PP, Multi
725	<i>Olea europaea</i>	17 @ base	15	P	P	Y	P	1	H, Multi
726	<i>Olea europaea</i>	21 @ base	20	P	F	Y	F	1	Large pruning wounds, Multi
727	<i>Olea europaea</i>	11 @ 2'	20	F	F	N	F		H, Multi
728	<i>Olea europaea</i>	13.5	15	P	P	N	P	1	H, Multi
731	<i>Olea europaea</i>	14	20	P	F-P	N	F-P		Internal decay, Multi
732	<i>Olea europaea</i>	19 @ base	15	P	P	Y	P	1	Internal decay, Multi, Dieback, PP

HOTEL SITE - Total Existing Trees:	85
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Heritage Trees	20
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To Remove:	Total	Replacement Value	Replacement Totals
Fair-Good health	4	2:1	8
Fair-Poor health	2	1:1	2
Poor-Dead health	14	1:1	14
Total	20		24
To Remain:			
Good Health	0		
Total	0		

Non Heritage Trees	65
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To Remove:	65	
To Remain:		

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
466	<i>Pinus halepensis</i>	16	20	P	P	Y	P	1	H for high voltage power lines
467	<i>Pinus halepensis</i>	20	35	P	F-P	Y	P	1	Lean, H for high voltage power lines
468	<i>Pinus halepensis</i>	20	30	P	F	Y	P	1	Lean, Dieback, H for high voltage power lines
469	<i>Pinus halepensis</i>	9	25	F-P	F	N	P		Significant lean, Dieback, H for high voltage power lines
470	<i>Platanus x hispanica</i>	8.5	35	F-G	F-G	N	G		Anthraxnose, CD, High voltage power lines
471	<i>Pinus radiata</i>	10	30	P	F-P	N	P		
472	<i>Pinus radiata</i>	11	30	F	F-P	N	P		
473	<i>Pinus radiata</i>	10	25	P	F	N	P		Lean
474	<i>Pinus radiata</i>	7	30	F	F	N	F		Lean, DW
475	<i>Pinus radiata</i>	12	40	F	F	N	F		DW

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
476	<i>Pinus radiata</i>	6	25	F	F	N	F-P		
477	<i>Prunus cerasifera</i>	6	15	F-G	F-G	N	F		CD
478	<i>Platanus x hispanica</i>	5.5	20	F	F-P	N	F-P		Large pruning wounds
479	<i>Pinus radiata</i>	12.5	40	G	F-G	N	F		Lean
480	<i>Pinus radiata</i>	12.5	40	G	F-G	N	F		Lean
481	<i>Pinus radiata</i>	14	40	G	F	N	F		
482	<i>Platanus x hispanica</i>	5.5	25	P	P	N	P		Under pine canopy
483	<i>Platanus x hispanica</i>	6.5	25	F-P	P	N	P		Lean
484	<i>Pinus radiata</i>	14	40	F	F	N	F		Multi top
485	<i>Myoporum laetum</i>	17 @ base	15	P	P-D	Y	P	14	6 main stems, Thrips, Almost dead
486	<i>Pinus radiata</i>	10	40	F	F	N	F		DW
487	<i>Myoporum laetum</i>	13	20	P	P	N	P		Thrips, CD
488	<i>Myoporum laetum</i>	14	20	P	P	N	P		CD, Thrips
489	<i>Myoporum laetum</i>	5.5	20	P	P	N	P		Thrips
490	<i>Myoporum laetum</i>	12	25	P	P	N	P		Thrips
491	<i>Myoporum laetum</i>	5.5	25	P	P	N	P		Thrips
492	<i>Myoporum laetum</i>	4	10	P	P	N	P		Thrips, H
493	<i>Pinus halepensis</i>	13	30	F-P	G	N	F-P		Significant lean, CD top
494	<i>Pinus radiata</i>	11	40	F-G	F	N	F		
495	<i>Pinus halepensis</i>	15	30	F	G	Y	F	1	Significant lean, CD top
496	<i>Platanus x hispanica</i>	7	25	F	P	N	P		Large pruning wounds
497	<i>Pinus radiata</i>	12	40	F-G	F	N	F		
498	<i>Pinus radiata</i>	11	40	F	F-P	N	F-P		
499	<i>Pinus halepensis</i>	10	20	P	F	N	P		Significant lean
500	<i>Pinus radiata</i>	12.5	40	F-G	F	N	F		
501	<i>Platanus x hispanica</i>	6	20	G	P	N	P		
502	<i>Pinus halepensis</i>	17	40	F-G	G	Y	G	1	Lean
503	<i>Platanus x hispanica</i>	6.5	20	P	P	N	P		
504	<i>Pinus radiata</i>	17.5	40	F	F-G	Y	F	1	Lean, DW
505	<i>Pinus radiata</i>	11	25	P	F	N	P		In canopy, Crowded, CDEB

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
506	<i>Pinus radiata</i>	14	40	F	F-G	N	F		Lean
507	<i>Pinus radiata</i>	17	40	G	F	Y	F	1	
508	<i>Eucalyptus conferruminata</i>	9.5	25	F	G	N	F-P		Lean over parking lot, Vehicle damage
509	<i>Platanus x hispanica</i>	6	25	P	P	N	P		
510	<i>Myoporum laetum</i>	25.5 @ 1.5'	25	P	P-D	Y	P	1	Almost dead
511	<i>Pinus radiata</i>	14	45	F	F	N	F		
512	<i>Pinus radiata</i>	26	50	F	F-P	Y	P	1	Top dead
513	<i>Myoporum laetum</i>	11.5 @ 2'	20	P	P	N	P		Old tag #573, CD, Thrips
514	<i>Pinus radiata</i>	17	25	F	F	Y	P	1	Old tag #574, Lean, H for high voltage power lines
515	<i>Myoporum laetum</i>	12	25	P	P	N	P		Thrips, Lean, High voltage power lines
516	<i>Pinus radiata</i>	15	25	F-P	P	Y	P	1	Large pruning wounds, CD, High voltage power lines
544	<i>Myoporum laetum</i>	22 @ base	25	P	P	Y	P	1	Thrips, Multi
545	<i>Myoporum laetum</i>	44 @ base	25	P	P	Y	P	1	3 main stems, Thrips
546	<i>Myoporum laetum</i>	30 @ base	25	P	P	Y	P	1	4 main stems, Thrips
547	<i>Myoporum laetum</i>	21 @ base	25	P	P	Y	P	1	CD, Thrips
548	<i>Myoporum laetum</i>	17 @ base	25	P	P	Y	P	1	4 main stems, Thrips
549	<i>Myoporum laetum</i>	21.5 @ base	25	P	P	Y	P	1	5 main stems, Thrips
550	<i>Myoporum laetum</i>	26.5 @ base	25	P	P	Y	P	1	5 main stems, Thrips
741	<i>Myoporum laetum</i>	43 @ base	30	P	P	Y	P	1	Thrips, 3 main stems
742	<i>Platanus x hispanica</i>	8	35	P	P	N	P		
743	<i>Platanus x hispanica</i>	7.5	35	P	P	N	P		Old tag #68, Anthracnose

Tag #	Species	DBH	Height	Structure	Health	Heritage Tree	Suitability for Retention	Heritage Tree Count	Notes
746	<i>Platanus x hispanica</i>	7	20	F	P	N	P		Old tag #65, Lean, Anthracnose
748	<i>Platanus x hispanica</i>	3.5	10	P	P	N	P		Old tag #63, Anthracnose
750	<i>Platanus x hispanica</i>	12.5	40	F-G	F-P	N	P		Old tag #61, Anthracnose
765	<i>Platanus x hispanica</i>	8	30	G	F	N	F-G		Old tag #46, Anthracnose
768	<i>Prunus cerasifera</i>	10	15	P	F-P	N	P		Old tag #20, Internal decay, Multi
769	<i>Platanus x hispanica</i>	9.5	20	F	G	N	F		Old tag #11, Surface roots, H
770	<i>Platanus x hispanica</i>	8	10	P	G	N	P		Old tag #19, Surface roots, H
771	<i>Platanus x hispanica</i>	8.5	20	F	F	N	F		Old tag #10, Surface roots, H
772	<i>Platanus x hispanica</i>	9.5	10	P	G	N	P		Old tag #18, Surface roots, H
773	<i>Platanus x hispanica</i>	8.5	20	F	F	N	F		Old tag #9, Surface roots
774	<i>Platanus x hispanica</i>	9.5	10	P	G	N	P		Old tag #17, Surface roots
775	<i>Platanus x hispanica</i>	10.5	20	F	F	N	F		Old tag #8, Surface roots
776	<i>Platanus x hispanica</i>	9	10	P	G	N	P		Old tag #16, H, Surface roots
777	<i>Platanus x hispanica</i>	10.5	20	F	F	N	F		Old tag #7, Surface roots
778	<i>Platanus x hispanica</i>	9	10	P	G	N	P		Old tag #15, H, Surface roots
779	<i>Platanus x hispanica</i>	6	20	F	F	N	F		Surface roots
780	<i>Platanus x hispanica</i>	8	15	P	G	N	P		Surface roots
781	<i>Platanus x hispanica</i>	9	25	G	F	N	F-G		Surface roots
782	<i>Platanus x hispanica</i>	11.5	25	G	F	N	F-G		Old tag #4
783	<i>Platanus x hispanica</i>	8.5	25	G	F	N	F-G		Old tag #3
784	<i>Platanus x hispanica</i>	7.5	25	G	F	N	F-G		Old tag #2
785	<i>Platanus x hispanica</i>	8.5	15	P	G	N	P		Old tag #13, Internal decay, Headed
786	<i>Platanus x hispanica</i>	11	25	G	F	N	F-G		Old tag #5
787	<i>Platanus x hispanica</i>	10	30	F	P	N	F		Old tag #14, Anthracnose

**DRAFT SECOND AMENDED AND RESTATED
CONDITIONAL DEVELOPMENT
PERMIT**

Facebook Campus Expansion Project
(Buildings 20-23 and Hotel)

1. GENERAL INFORMATION:

- 1.1 Applicant: Hibiscus Properties, LLC (and its successors and assigns)
- 1.2 Nature of Project: For purposes of this Second Amended and Restated Conditional Development Permit, the Facebook Campus Expansion Project (Project) includes Buildings 20-23, Building 305 (interim phase only), a Parking Garage, an Electrical Vehicle Charging Facility, and the Hotel as follows:
- a. Buildings 21-22 and Hotel. Zoning Ordinance Text Amendment, Rezoning, Second Amended and Restated Conditional Development Permit, Development Agreement for 301-309 Constitution Drive as amended by the Amendment to the Development Agreement (collectively, the “Amended Development Agreement”), Below Market Rate (BMR) Housing Agreement, Lot Line Adjustment, Heritage Tree Removal Permits, and Environmental Impact Report (EIR), including the Addendum prepared to analyze certain project modifications, for the demolition of the on-site buildings, with the exception of Building 23 and Building 305 (interim phase only), and the subsequent redevelopment of the site with two office buildings totaling no more than 962,400 square feet of office uses and an up-to 200 room hotel of approximately 174,800 square feet, 3,533 new parking spaces located at grade and within a multi-story parking garage, an electrical vehicle charging facility, publicly accessible open space, and a pedestrian and bicycle bridge.
 - b. Building 305. The existing building addressed as 305 Constitution Drive would remain in the interim phase only until the earlier of (i) the expiration of the current lease between the property owner (Hibiscus Properties, LLC) and TE Connectivity (TE) (September 4, 2022) and vacation of Building 305 by TE, or (ii) the termination of the current lease at an earlier date and vacation of Building 305 by TE. If the lease has expired and/or been terminated and TE refuses to vacate, the Applicant agrees to use commercially reasonable efforts to cause TE to vacate. Continued industrial operations, such as manufacturing and

distribution activities associated with TE, are permitted in the interim phase only and upon termination of the current lease and vacation of Building 305 by TE no further industrial operations are permitted on site. Building 305 shall be demolished within one year of the termination of the existing lease and vacation of Building 305 by TE, but in no event later than September 4, 2023. If Applicant diligently pursues demolition of Building 305 but is delayed for reasons beyond its control (e.g., force majeure), Applicant's deadline for demolishing Building 305 shall be equitably extended, subject to the approval of the City Manager which shall not be unreasonably withheld.

- c. Building 20. Building 20 was approved pursuant to a Conditional Development Permit and Development Agreement for 312-313 Constitution Drive in 2013, and includes one building totaling no more than 433,656 square feet over approximately 1,499 parking spaces. This Second Amended and Restated Conditional Development Permit incorporates and carries forward, as applicable, conditions that were included in the original Conditional Development Permit for Building 20 and replaces that Conditional Development Permit in its entirety.
- d. Building 23. Building 23 was approved pursuant to a Use Permit in 2014, and comprises a single-story office building totaling approximately 180,108 square feet of gross floor area. The 518 parking spaces for Building 23 are included in the proposed 3,533 new parking spaces associated with Buildings 21-22 and the Hotel. This Second Amended and Restated Conditional Development Permit incorporates and carries forward, as applicable, conditions that were included in the Use Permit for Building 23 and replaces that Use Permit in its entirety.

For purposes of determining the Floor Area Ratio (FAR), building coverage and building setbacks for the Project, the two existing parcels (APNs 055-260-250 and 055-260-290) comprising the Project Site shall be considered to be one parcel, bounded by Bayfront Expressway to the north, Willow Road to the east, the Dumbarton Rail Corridor to the south, and Chilco Street to the south and west. The Access Parcel (Facebook Way) shall continue to be an unbuildable parcel and therefore, not included in the lot area for purposes of determining the development regulations.

1.3 Project Location (Project Site):

- a. Building 21, Building 22, Parking Garage, Electric Vehicle Charging Facility, and Hotel: 301-309 Constitution Drive
- b. Building 305: 305 Constitution Drive (interim phase only)

- c. Building 20: 1 Facebook Way, Building 20 (formerly 312 and 313 Constitution Drive)
- d. Building 23: 1 Facebook Way, Building 23 (formerly 300 Constitution Drive)

The address for Buildings 21-22 shall be determined by the Applicant, subject to the reasonable approval of the Building Official, who shall have final authority to determine the addressing at Buildings 21-22. The address for the Hotel will be determined by the Applicant and the City's Building Official prior to issuance of the Core and Shell phase of the building permit for the Hotel.

1.4 Project Phasing: Buildings 21, 22, and the Hotel shall be constructed in the following phases:

- a. Phase 1: Building 21, and the initiation of permitting process for the Bicycle and Pedestrian Bridge.
- b. Phase 2: The "Interim Phase" which includes the demolition of Buildings 301, 302, 303, 304, 306, and the chemical transfer facility (CTF), construction of Building 22 and the associated Parking Garage Structure, construction of the Bicycle and Pedestrian Bridge, and the partial construction of the Publicly Accessible Open Space.
- c. Phase 3: The "Final Phase" which includes the demolition of Building 305, construction of the Hotel, completion of the full extent of the Publicly Accessible Open Space, and the construction of the Electrical Vehicle Charging Facility and associated landscape improvements. The construction of the Hotel may begin after completion of the demolition of Building 305.

If desired, the Hotel may be constructed prior to Building 22, provided that the buildings proposed to be demolished in Phase 2 are removed. In addition, at no time may the buildings constructed on-site exceed the allowable Floor Area Ratio identified herein.

1.5 Assessor's Parcel Numbers (Post Lot Line Adjustment): 055-260-250 (Hotel); 055-260-290 (Buildings 20, 21, 22, 23, and in the interim phase Building 305), and 055-260-997 (Access Parcel, Facebook Way)

1.6 Property Owner(s): Hibiscus Properties, LLC

1.7 Area of Project Site: Two parcels, excluding the Access Parcel that is unbuildable, totaling approximately 80.44 acres (approximately 3,504,041 square feet). This includes Parcel 055-260-250 (Hotel) with approximately 2.6 acres and Parcel 055-260-290 (Building 20, 21, 22, 23, Parking Garage, Electric Vehicle Charging Facility, Building 305 (interim phase only) with approximately 77.71 acres. The Project Site parcels, post lot line adjustment, are as shown on Exhibit A attached hereto and being more particularly described in Exhibit B attached hereto.

- 1.8 Zoning: O (Office); previously M-2(X) (General Industrial, Conditional Development), subject to the Amended Development Agreement.
- 1.9 Conditions Precedent: Applicant's obligations as set forth herein are expressly conditioned on the resolution of all legal challenges, if any, to the EIR Addendum and/or the Project. If no litigation or referendum is commenced challenging the EIR Addendum and/or the Project, Applicant's rights and obligations will vest on the passing of all applicable statutes of limitation, consistent with the Amended Development Agreement. If litigation or a referendum is commenced and Applicant determines to terminate the Amendment to the Development Agreement during the pendency of any such proceeding, the Amended and Restated Conditional Development Permit for 301-309 Constitution Drive shall survive and control the permitted uses on those sites.

2. DEVELOPMENT STANDARDS:

- 2.1 Floor Area Ratio (FAR) shall not exceed 55 percent of the Project Site.
 - 2.1.1 The maximum FAR for office uses at the site shall not exceed 45 percent.
 - 2.1.2 The maximum FAR including the Hotel shall not exceed 55 percent.
- 2.2 Building coverage shall not exceed 55 percent of the Project Site.
- 2.3 Building setbacks shall be substantially in accordance with the approved plans, and in no case shall the minimum setback be less than 40 feet for Building 20 from each property line and not less than 100 feet for Building 21 from Bayfront Expressway and 60 feet from the south property line. Building 22, the Parking Garage, and Electrical Vehicle Charging Facility, and the Hotel shall be a minimum of 20 feet from each external property line and outside the PG&E easement along Bayfront Expressway. The actual setbacks shall be determined through the individual architectural review by the Planning Commission for each building or phase (Identified in Section 6.1.4). The internal property line between the two parcels (055-260-250 and 055-260-290) is not considered a property line for setback purposes or any other Zoning Ordinance standard.
- 2.4 The minimum setback for ancillary structures, such as bus canopies and shelters, security stations, and other structures, accessory in nature, shall be 20 feet from each exterior property line, with the exception of emergency generators which may be constructed at the southern edge of the Property as shown on the Project Plans. The locations of the structures shall be substantially in compliance with the locations identified in the approved Project Plans (defined below).

- 2.5 Building height, inclusive of temporary structures, shall not exceed 75 feet for Buildings 21, the Parking Garage (as measured to the top of the safety railing), and the Hotel. The roof deck of Building 22 shall not exceed 75 feet in height. However, the skylight element of Building 22 may be higher, but shall not exceed 87 feet in height. Skylight maintenance platforms may be located on top of the skylight and extend up to ten feet in height above the skylight level, for a total potential height of 97 feet. The number of skylight maintenance platforms shall be limited to two platforms. Building 20 shall not exceed 73 feet in height. All heights shall be measured from the average level of the highest and lowest point of the finished grade of that portion of the lot covered by the structure. Height excludes elevator equipment rooms, elevator hoistways, electrical equipment, solar panels, ventilating and air conditioning equipment, and associated screening.
- 2.6 The on-site circulation and parking spaces shall be maintained in a manner that is substantially consistent with the Project Plans, with a minimum of 3,533 parking spaces for Buildings 21 (1,476 spaces), 22 (1,294 spaces), 23 (518 spaces), and the Hotel (245 spaces). Parking shall be installed in a manner that is substantially in compliance with the project phasing plans and approved Project Plans. A minimum of 1,446 parking spaces are required for Building 20, with a maximum of 1,499 parking spaces, for a total minimum aggregate parking count at the Project Site of 4,979 spaces at full build out of the Project. Parking for Building 22 and a portion of parking for Building 23 would be provided in a multi-story parking garage.
- 2.7 All roof-top equipment shall comply with the noise requirements in Chapter 16.08 (Roof Mounted Equipment) of the Zoning Ordinance, with the exception of smoke exhaust fans for the fire and smoke suppression system as outlined in Section 2.7.1.
- 2.7.1 Smoke exhaust fans for Building 22's smoke ventilation system may exceed the noise limitations outlined in Chapter 16.08 (Roof Mounted Equipment) during routine testing provided that routine testing is conducted between 8:00 a.m. and 6:00 p.m. Monday through Friday and complies with the noise requirements of Chapter 8.06 of the municipal code. All other roof mounted equipment shall comply with the Zoning Ordinance requirements (Chapter 16.08) for noise. In the event of an emergency, noise generated by the smoke ventilation system would be exempt from the Zoning Ordinance and municipal code requirements.
- 2.8 Roof-mounted equipment is required to be screened to the tallest horizontal member of the equipment. However, the screening for Building 22 is not required to be completely opaque. The perforations in the screen shall be no more than a-half inch. Roof screening may be composed of chain link or similar material, provided it is painted in a color consistent with the design of the building. The proposed roof mounted equipment screening shall be comprised of two panels installed in an off-set

configuration to provide a more opaque mechanical screen (effectively a quarter-of-an-inch gap) between perforations in the material. The design of the roof mounted equipment screening shall be subject to review and approval of the Planning Division.

- 2.9 All ground mounted equipment shall be fully screened and integrated into the site design. The ground mounted equipment shall comply with the noise requirements in Chapter 8.06 (Noise) of the Municipal Code.

3. USES:

- 3.1 The development is comprised of up to four office buildings (referred to as Buildings 20, 21, 22, and 23) totaling no more than 1,576,164 square feet of gross floor area (GFA), with potential usable roof gardens and amenities that are accessible to the occupants of the buildings, as well as a public open space that would be used for passive recreational uses and community events. Building 20, 21, and the Hotel would be generally constructed on top of surface parking lots and Building 22 would have an at grade first level. The development includes a multi-story parking garage to accommodate the required parking for Building 22 and a portion of the required parking for Building 23. Buildings 20 and 21 would be connected with usable floor area. Buildings 21 and 22 would be connected through an open bridge. Enclosed bridges would be included in the calculations of GFA and FAR for the Project. In addition to the office buildings, a Hotel of up to 200 rooms with a restaurant and bar that is open to the public is also permitted. In the interim phase only, Building 305 would remain on-site, concurrent with Building 22, but would be demolished prior to construction of the Hotel. The Hotel may be constructed prior to Building 22 or any time after demolition of Building 305. Permitted uses on the Project Site shall include the following:

- 3.1.1 Administrative and professional offices, excluding medical/dental offices serving the general population;
- 3.1.2 Medical and dental uses to serve on-site employees and contractors are permissible;
- 3.1.3 General industrial uses including but not limited to warehousing, manufacturing, printing and assembling;
- 3.1.4 Amenities and related uses intended to serve employees, contractors, and visitors, such as neighborhood-serving convenience retail, banks, community facility space, fitness facilities and restaurants, including those that serve alcoholic beverages;
- 3.1.5 Outdoor seating and tables (including those intended to be used for the consumption of food and beverages), temporary structures, and events associated with those uses listed above on the Project Site including on the roof, subject to approved building permits and Fire District permits, as applicable;

- 3.1.6 Activities involving the use of hazardous materials, such as emergency power generators, incidental to those uses listed above and subject to an approved Hazardous Materials Business Plan, Building Permit, San Mateo County Health Permit, and Menlo Park Fire Protection District permit;
- 3.1.7 Cellular telecommunications facilities if fully screened or integrated into the design of the building;
- 3.1.8 Hotels that do not include conference or banquet facilities, but include a restaurant and hotel bar that are open to the public;
- 3.1.9 Recreational uses consistent with a public park;
- 3.1.10 Community events, including but not limited to farmers markets, movie nights, concerts, community block parties, and food trucks, provided the activities comply with Chapter 8.06 (Noise) of the municipal code, unless a special events permit is approved by the City, as outlined in Chapter 8.06.060 (Temporary permits, special event permits and use permits) of the municipal code; and
- 3.1.11 Electrical Vehicle Charging Facility for electric buses, trams, and shuttles, including the outside and overnight storage of electric vehicles.
- 3.2 Administratively permitted uses listed in the O (Office) zoning district may be permitted through an administrative permit, unless otherwise allowed in Section 3.1.
- 3.3 Conditionally permitted uses listed in the O (Office) zoning district may be permitted through a use permit, unless otherwise allowed in Section 3.1.
4. SIGNS:
 - 4.1 The maximum permissible sign area for the Project Site is 600 square feet, which may be distributed throughout the Project Site. Each building may utilize a different percentage of the maximum allowed square footages. Vehicular directional signage and signage not visible from the public right-of-way or adjacent properties shall not count against the maximum sign area and is only subject to building permit review, as applicable. The square footage, location and materials for all signage that counts towards the maximum permissible sign area shall be subject to review and approval by the Planning Division through the Sign Permit process, with an application and applicable filing fees. The proposed signs shall be reviewed by the Planning Division for conformance with the City's Sign Design Guidelines and Chapter 16.92 (Signs- Outdoor Advertising) of the Zoning Ordinance.
 - 4.2 The maximum allowed sign area identified in Section 4.1 may be exceeded through a use permit.
5. RECORDATION:

- 5.1 The Facebook Campus Expansion Project Second Amended and Restated Conditional Development Permit and the Amendment to the Development Agreement shall be concurrently recorded in the Official Records of the County of San Mateo, State of California.
- 5.2 The Second Amended and Restated Conditional Development Permit shall be in full force and effect on the Effective Date of the Amended Development Agreement, subject to the provisions of Section 1.9. The Second Amended and Restated Conditional Development Permit supersedes the Amended and Restated Conditional Development Permit.

6. MODIFICATIONS:

- 6.1 Modifications to the approved Project may be considered according to the following five tier review process. The five tiers are in order of progressively more substantial review for changes to the Project as opposed to initial project approval:
 - 6.1.1 Substantially Consistent Modifications are made at the staff level. Substantially Consistent Modifications are changes to or modifications of the Project that are in substantial compliance with and/or substantially consistent with the Project Plans and the Project Approvals (as defined in the Facebook Campus Expansion Development Agreement and the West Campus Development Agreement). Substantially Consistent Modifications are generally not visible to the public and do not affect permitted uses, density or intensity of use, restrictions and requirements relating to subsequent discretionary actions, monetary obligations, material modifications to the conditions of approval identified in Sections 7, 9, 10, 11, 12, 13, 14, and 15, conditions or covenants limiting or restricting the use of the Property or similar material elements based on the determination that the proposed modification(s) is consistent with other building and design elements of the Second Amended and Restated Conditional Development Permit, and will not have an adverse impact on the character and aesthetics of the Property. The determination as to whether a requested change is a Substantially Consistent modification will be made by the Community Development Director (in his/her reasonable discretion).
 - 6.1.2 Minor Modifications are made at the staff level, but the Planning Commission is provided information regarding these modifications. The determination as to whether a requested change is a Minor Modification is determined by the Community Development Director (in his/her reasonable discretion). A Minor Modification is similar in nature to a Substantially Consistent Modification, except that Minor Modifications generally are visible to the public and result in minor exterior changes to the Project aesthetics. Any member of the Planning Commission may request within seven days of receipt of the

informational notice that the item(s) be reviewed by the Planning Commission.

- 6.1.3 Major Modifications are reviewed by the Planning Commission as a Regular Business item, and publicly noticed. Major Modifications are changes or modifications to the Project that are not in substantial compliance with and/or substantially consistent with the Project Plans and Project Approvals. Major modifications include, but are not limited to, significant changes to the exterior appearance of the buildings or appearance of the Property, and changes to the Project Plans, which are determined by the Community Development Director (in his/her reasonable discretion) to not be in substantial compliance with and/or substantially consistent with the Project Plans and Project Approvals. The Planning Commission's decision shall be based on the determination that the proposed modification is compatible with other building and design elements or onsite/offsite improvements of the Second Amended and Restated Conditional Development Permit and would not have an adverse impact on safety and/or the character and aesthetics of the site. Planning Commission decisions on Major Modifications may be appealed to the City Council. City Council shall have final authority to approve Major Modifications. Major Modifications that also require Conditional Development Permit Amendments (see Section 6.1.5 below) shall be considered in accordance with Section 6.1.5.
- 6.1.4 Design Review of Project Plans for the Hotel, and the Electric Vehicle Charging Station is required for the hotel building and the charging facility. The Planning Commission shall review the design plans through a formal architectural control application. The Applicant is required to submit an architectural control application and pay all applicable fees for the Planning Division's review of the proposed project plans, subject to review and approval by the Planning Commission. The Planning Commission's action will be based on conformance with this CDP and the required findings for architectural control, as enumerated in Chapter 16.68.020 (Architectural Control) of the Zoning Ordinance.
- 6.1.5 Conditional Development Permit Amendments are reviewed by the Planning Commission and the City Council. Conditional Development Permit Amendments are required where the Applicant seeks revisions to the Project which involve (a) the relaxation of the development standards identified in Section 2, (b) material changes to the uses identified in Section 3, or (c) material modifications to the conditions of approval identified in Sections 7, 9, 10, 11, 12, 13, 14, and 15. Such revisions may also require modifications to the Amended Development Agreement or the Development Agreement for 312-313 Constitution Drive in the case of changes affecting Building 20. If the Applicant wishes to make a change that requires an amendment to this Second Amended and Restated Conditional Development Permit, it shall

apply, in writing with all applicable plans and fees, to the Planning Division for review and recommendation to the Planning Commission. The Planning Commission shall then forward its recommendation to the City Council for amendment(s) to this Second Amended and Restated Conditional Development Permit.

For purposes of clarification, Substantially Consistent Modifications, Minor Modifications, Design Review for the Hotel, and Major Modifications will not constitute Conditional Development Permit Amendments or require modifications to the Amended Development Agreement or the Development Agreement for 312-313 Constitution Drive.

7. TRIP CAP:

7.1 Consistent with the Project proposal and to minimize environmental and community impacts resulting from utilization of the Project Site, Applicant shall enforce a trip cap.

7.1.1 Trip Cap: The trip cap sets the maximum number of morning and evening peak period vehicle trips and daily vehicle trips (Trip Cap). The parameters and requirements of the Trip Cap are specified in the Facebook Campus Expansion Project Trip Cap Monitoring and Enforcement Policy, which is included as Exhibit C and incorporated herein. The Trip Cap applies to Buildings 20, 21, 22, 23, existing buildings prior to demolition, and the Hotel. A separate Trip Cap is enforced on the East Campus through the site specific Amended and Restated Conditional Development Permit for 1601 Willow Road.

7.1.2 Implementation: The Trip Cap counting equipment shall be installed and in good working order prior to occupancy of Building 21, unless otherwise approved, to the satisfaction of the Public Works Director.

8. CONSTRUCTION PERMITS SEQUENCING:

8.1 The Community Development Director or his/her designee shall have authority to determine the sequencing of building permits and sub-phases for each building/phase of construction. In general, the construction for each building will consist of the following phases: Demolition, Grading and Utilities, Foundation Only, Core and Shell, Tenant Improvements, and Landscaping. Certain conditions contained within this Second Amended and Restated Conditional Development Permit may be triggered by a specific phase of construction for each individual building, which will be noted using the terminology above.

8.2 Access Improvements (Public Right-of-Way Encroachment Permit): The new site access from Bayfront Expressway shall be constructed prior to temporary occupancy for Building 21. The Applicant shall follow the procedures below for submittal, review, and construction of the site access

- 8.2.1 Submit improvement plans to the City for approval for those portions of the Project that require offsite improvements in the Caltrans right-of-way (Public ROW Improvements and Site Access). This includes all work in the Caltrans right-of-way, including, but not limited to, utility improvements, curb cuts, driveway, traffic signal, and other frontage improvements, as applicable;
- 8.2.2 Receive City approvals for such improvement plans;
- 8.2.3 Submit the improvement plans to Caltrans and request encroachment permit approvals;
- 8.2.4 Submit documentation of approval from Caltrans to the City prior to issuance of Core and Shell phase building permit for Building 21;
- 8.2.5 Complete the Public ROW Improvements, inclusive of installation of new traffic signal on Bayfront Expressway prior to temporary occupancy of Building 21; and
- 8.2.6 If Applicant diligently pursues approval from Caltrans but is delayed in obtaining approval due to no fault or lack of diligence on the part of the Applicant, Applicant's obligation to submit documentation of approval prior to issuance of Core and Shell phase building permit for Building 21 shall be extended automatically, subject to the approval of the City Manager which shall not be unreasonably withheld. No such extension shall relieve the Applicant's obligation to complete the Public ROW Improvements prior to temporary occupancy of Building 21.

9. PROJECT SPECIFIC CONDITIONS - GENERAL:

- 9.1 Project Plans: Development of the Project shall be substantially in conformance with the plans submitted by Gehry Partners, LLC dated received October 10, 2017, recommended for approval to the City Council by the Planning Commission on October 16, 2017 (Project Plans), and approved by the City Council on November __, 2017, except as modified by the conditions contained herein and in accordance with Section 6 (Modifications) of this document. The Hotel and Electric Vehicle Charging Station shall be substantially in conformance with the Planning Commission's design review of the detailed plans for each building as set forth in Section 6. Buildings 20, 21, and 23 shall be in substantial conformance with the plans approved for those buildings in prior approvals.
- 9.2 Below Market Rate Housing Agreement: The Applicant has recorded a Below Market Rate (BMR) Housing Agreement relative to Buildings 21, 22, and the Hotel in the Official Records of the County of San Mateo, State of California. Previous BMR Housing Agreements related to Buildings 20 and 23 remain unchanged. The BMR Housing Agreement requires that the Applicant satisfy its obligations under the BMR Ordinance and Guidelines by one of the following methods:
 - a. Paying the in lieu fee;

- b. Delivering off-site residential units; or
- c. Paying a portion of the in lieu fee and delivering off-site residential units.

Based upon the current fee (FY 2016-217) per square foot, the estimated BMR fee for the project would be \$6,534,438.95. The required number of residential units for the project would be 20 units. If the Applicant proceeds with a combined in lieu fee payment and provision of off-site units, each unit shall equate to a credit of \$326,721.95 toward the in-lieu fee. The timing of the provision of units or payment of the fee is based on the construction phase for each building and is further documented in the BMR Agreement for the Project.

9.2.1 Bonding for Building 305 Demolition: To ensure that Building 305 will be demolished and therefore, continue to allow the Applicant to receive credit for the existing square footage of Building 305 in the calculation for the BMR fees and equivalent number of units in the Building 22 construction phase, the Applicant shall post a bond for the completion of the demolition of Building 305.

1. A cost estimate for the demolition of Building 305 shall be provided to the City on the Effective Date of the Second Amended and Restated Development Permit as defined in section 5.2 for review and approval of the Assistant Community Development Director.
2. A bond for 200% of the approved cost estimate shall be posted prior to issuance of the foundation only permit phase for Building 22.

9.3 Construction Fencing: The Applicant shall submit a plan for construction safety fences around the periphery of the construction area concurrent with the building permit for each phase of construction. The fences shall be installed according to the plan prior to commencing construction for each individual phase of each building. The plan shall be reviewed and approved by the Building and Planning Divisions prior to issuance of a demolition permit for each building.

9.4 Truck Route Plan: The Applicant shall submit a truck route plan concurrent with the building permit application for each stage of construction based on the City's municipal code requirements, for review and approval by the Transportation Division. The Applicant shall also submit a permit application and pay applicable fees relating to the truck route plan, to the satisfaction of the Public Works Director.

9.5 Salvaging and Recycling of Construction and Demolition Debris: For each building, the Applicant shall comply with the requirements of Chapter 12.48 (Salvaging and Recycling of Construction and Demolition Debris) of the City of Menlo Park Municipal Code, which compliance shall be subject to review and approval by the Public Works Department.

- 9.6 Utility Improvements: Concurrent with submittal of the application for the Grading and Utilities phase for each building phase, the Applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering, and Building Divisions prior to building permit issuance. Landscaping shall properly screen all utility equipment that is installed outside of a building and cannot be placed underground; subject, however, to the requirements of the City, the Menlo Park Fire Protection District, the West Bay Sanitary District, PG&E, and any other applicable agencies regarding utility clearances and screening. The plan for new utility installations/upgrades shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes and other equipment boxes. The screening shall be compatible and unobtrusive and subject to the review and approval of the Planning Division which approval will be required prior to the City's approval of the final building permit inspection for the Core and Shell phase for each building (or equivalent permit inspection for the electric vehicle charging facility).
- 9.7 Grading and Drainage Plan, Inclusive of Erosion and Sedimentation Control Plan: Concurrent with submittal of the Grading and Utility plans for each phase, the Applicant shall submit a Grading and Drainage Plan, including an Erosion and Sedimentation Control Plan, for review and approval by the Engineering Division prior to building permit issuance. The Grading and Drainage Plan shall be prepared based on the City's Grading and Drainage Plan Guidelines and Checklist, the City approved Hydrology Report and Stormwater Management Plan for the Project, and the Project Applicant Checklist for the National Pollution Discharge Elimination System (NPDES) Permit Requirements.
- 9.8 Landscape Plan: The Applicant shall submit a detailed on-site landscape plan for each building phase and for the Bicycle and Pedestrian bridge, including the size, species, and location, and an irrigation plan for review and approval by the Planning, Engineering, and Transportation Divisions, prior to building permit issuance for the Tenant Improvement phase for each building and for the building permit for the Bicycle and Pedestrian bridge. The landscape plans shall illustrate the retention of the maximum number of trees feasible, to the satisfaction of the Planning Division and City Arborist. The landscape plans shall include all onsite landscaping (including heritage tree replacements), adequate sight distance visibility, screening for outside utilities with labels for the utility boxes sizes and heights, and documentation confirming compliance with the Water Efficient Landscaping Ordinance (Municipal Code Chapter 12.44). The landscape plans shall include an appropriate mix of native and adapted species to complement the nearby Don Edwards San Francisco Bay National Wildlife Refuge and shall be reviewed and approved by the Community Development Director and Public Works Director prior to building permit issuance for the Tenant Improvement phase for each building and for the building permit for the Bicycle and Pedestrian bridge. The landscape plan for Building 22 shall include the design and planting for the both the interim and the final designs of the Publicly Accessible Open Space, which is subject to review and approval of the Community Development Director. Heritage tree replacements (outlined in

9.10) shall be identified on the landscape plan for each individual building phase.

- 9.9 Heritage Tree Protection and Removal: The Applicant shall submit a heritage tree preservation suitability analysis and, if applicable, preservation guidelines concurrent with, or prior to, the submittal of the Grading and Utility phase for each building phase. This plan should assess the possibility of retaining heritage trees in good health within each phase of the Project based on the review of the Project Arborist. If any heritage trees are preserved during construction, the Project Arborist shall submit a letter confirming adequate installation of tree protection measures, per the recommendations in the suitability analysis and preservation guidelines. The Applicant shall retain an arborist throughout the term of the Project, and the project arborist shall submit periodic inspection reports to the Building Division documenting compliance with the preservation guidelines, as applicable. The heritage tree suitability and preservation plan shall be subject to review and approval by the Planning Division and City Arborist prior to Grading and Utilities permit issuance for each building phase. The Applicant shall, in good faith, attempt to design the project to retain heritage trees in good condition. The heritage trees approved for removal shall be removed as necessary during each building phase, subject to review and approval of the Planning Division and City Arborist.
- 9.10 Heritage Tree Replacements: The applicant is permitted to remove up to 274 heritage trees on-site, 149 of which are in good condition, as determined by the Project Arborist in the *Tree Survey Report* prepared by SBCA Tree Consulting dated March 28, 2016 and shown on Sheet L0.100 in the project plans. Heritage trees in good condition are required to be replaced at a 2:1 ratio and heritage trees in fair-to-poor condition or dead are permitted to be replaced at a 1:1 ratio. A minimum of 423 heritage tree replacements are required for the project. Heritage tree replacements shall be a minimum of 24-inch box size and are required to be planted at grade. The number of heritage tree replacements proposed for each building or phase shall correspond to the number of heritage trees removed in each phase. The number of heritage tree replacements and the species and size shall be identified in the landscape plans for each building, subject to review and approval of the Planning Division and City Arborist.
- 9.11 Landscape Maintenance: Site landscaping, inclusive of landscaping on the living roofs, shall be maintained to the satisfaction of the Community Development Director so long as the building constructed as part of the Project is located on the Project Site. Significant revisions to site landscaping (inclusive of roof landscaping) shall require review by the Building Official, Public Works Director, City Arborist, and Community Development Director to confirm the proposed changes comply with accessibility and exiting requirements, stormwater requirements and are substantially consistent with the Conditional Development Permit approval consistent with the procedure outline in Section 6, Modifications.
- 9.12 Water Supply Assessment (WSA) Compliance: After one year from occupancy of Building 21 and annually thereafter, the Applicant shall submit

documentation to the City to confirm that water usage does not exceed the estimated water consumption for the Project documented in the WSA dated February 3, 2016. The estimated total water consumption is 88 million gallons per year, a net increase of 30 million gallons. The Public Works Director shall review the documentation along with City records for water usage at the site to confirm that water usage does not exceed the estimated water usage in the WSA. Compliance with the maximum limits shall be phased on a per building basis to account for the phasing of the development of the Project. If compliance is not achieved, the Applicant shall submit a plan outlining corrective measures to the City, subject to review and satisfaction of the Public Works Director. If the corrective measures fail to bring the Project into compliance within 90 days, the Applicant shall pay a fine in accordance with law as determined by the Public Works Director on a daily basis until the water usage is brought into compliance, or City's Public Works Director may prohibit the use of water for irrigation or enforce compliance as an infraction pursuant to Chapter 1.12 of the City's Municipal Code until compliance with the water budget is achieved. Building 20 and Building 23 shall have separate water meters (or sub-meters) and plumbing systems and are not subject to the WSA water usage limits for Buildings 21-22 and the Hotel. Building 305 is part of the Project (interim phase only) and is subject to the water usage limits set forth by the WSA. Actual water usage associated with Building 22 shall include Building 305 toward the phased in water usage limits set forth in the WSA.

- 9.13 Stationary Noise Source Compliance Data: Concurrent with the Core and Shell building permit phase submittal for each individual building (or the equivalent permit submittal for the electric vehicle charging facility), the applicant shall provide a plan that details that all on-site stationary noise sources comply with the standards listed in Section 8.06.030 of the City's Noise Ordinance. This plan shall be subject to review and approval by the Planning and Building Divisions prior to each building permit issuance.
- 9.14 Compliance with City Requirements: The Applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the Project to the satisfaction of the Community Development Director.
- 9.15 Building Construction Street Impact Fee: Prior to issuance of each building permit, the Applicant shall pay the applicable Building Construction Street Impact Fee in effect at the time of payment, to the satisfaction of the Public Works Director. The amount of such fees may be reduced in the reasonable discretion of the Public Works Director depending on the extent to which construction vehicle traffic is occurring on City streets, and shall be subject to a credit for work performed in connection with Phases 5 and 6 of the Chilco Streetscape Improvements pursuant to the Amended Development Agreement.
- 9.16 School Impact Fee: Prior to issuance of each building permit, the Applicant shall pay the applicable School Impact Fee for the Project in effect at the time of payment, to the satisfaction of the Building Official.

- 9.17 West Bay Sanitary District Requirements: The Applicant shall comply with all regulations of the West Bay Sanitary District that are directly applicable to the Project to the satisfaction of the Building Official.
- 9.18 Menlo Park Fire Protection District Requirements: The Applicant shall comply with all Menlo Park Fire Protection District regulations governing site improvements, Fire Code compliance, and access verification that are directly applicable to the Project to the satisfaction of the Building Official.
- 9.19 Power and Communications Requirements: The Applicant shall comply with all regulations of PG&E and other applicable communication providers (i.e., AT&T and Comcast) that are directly applicable to the Project to the satisfaction of the Building Official.
- 9.20 Stormwater Operations and Maintenance Agreement: Prior to tentative occupancy for each building, the Applicant shall enter into, or amend the existing Operations and Maintenance Agreement with the City, as applicable. The Operations and Maintenance Agreement shall establish a self-perpetuating drainage system maintenance program (to be managed by the Applicant) that includes annual inspections of any infiltration features and stormwater detention devices (if any), and drainage inlets, flow through planters, and other Best Management Practices (BMP). Any accumulation of sediment or other debris shall be promptly removed. Funding for long-term maintenance of all BMPs must be specified in the Operations and Maintenance Agreement. There may be separate Operations and Maintenance Agreements for each individual parcel within the Campus Expansion Project site, or one combined agreement as may be determined by the City and Applicant. The Operation and Maintenance Agreements shall be subject to review and approval of the City Attorney and the Public Works Director and shall be recorded prior to final inspection of the Tenant Improvement phase. An annual report documenting the inspection and any remedial action conducted shall be submitted to the Public Works Department for review. This condition shall be in effect for the life of the Project.
- 9.21 [Intentionally Deleted.]
- 9.22 Caltrans Approval: Prior to issuance of the building permit for the Building 21 Core and Shell phase the Applicant shall provide verification of Caltrans approval of the signalized intersection location as set forth in Section 8.2.
- 9.23 Improvements in the Caltrans Right-of Way: Prior to temporary occupancy of Building 21, the Applicant shall complete all Public ROW intersection improvements (inclusive of installation of the new traffic signal on Bayfront Expressway) and provide verification that Caltrans and the City has accepted the improvements, to the satisfaction of the Public Works Director.
- 9.24 Access and Improvements: Access points and all improvements on Bayfront Expressway are subject to the review and approval of Caltrans. Prior to submitting improvement plans to Caltrans, the Applicant shall submit plans

to the Public Works Director for his/her review and approval prior to submittal to Caltrans.

- 9.25 Accessibility: All pedestrian pathways shall comply with applicable Federal and State accessibility requirements, to the satisfaction of the Public Works Director and Building Official.
- 9.26 On-site Pedestrian Deterrents: The on-site pedestrian deterrent materials and color shall be consistent with the materials and colors used for Building 20. If the pedestrian deterrent materials and color deviate from the approved materials and colors for Building 20, the revised proposal shall meet the satisfaction of the Public Works Director, Building Official and Menlo Park Fire Protection District.
- 9.27 Generator Screening: Consistent with Project Plans, the Applicant shall screen all generators prior to building permit final inspection for Tenant Improvements for each building, to the satisfaction of the Community Development Director.
- 9.28 Refuse and Recyclables: All garbage bins and carts shall be located within a trash enclosure that meets the requirements of the solid waste disposal provider (Recology), and the City Public Works Department and Planning Division for the lifetime of the Project. If additional trash enclosures are required to address the on-site trash bin and cart storage requirements of the Applicant, a complete building permit submittal shall be submitted inclusive of detailed plans, already approved by Recology, for review and approval of the Planning Division and the Public Works Department prior to each building permit issuance.
- 9.29 Special Event Tents: The Applicant shall obtain required building and Fire District permits for erection of special event tents requiring such permits, to the satisfaction of the Building Official.
- 9.30 Special Events Tents, Roof: Use of a special event tent on the roof level is limited to single day events a maximum of eight times per calendar year for the Project Site, with the events occurring between the hours of 9 a.m. and 11 p.m. This condition applies only to Buildings 20-22. The set-up and break down of the tent shall not occur more than three days in advance of an event and shall be completed within three days of completion of said event. The tent shall be a maximum size of approximately 80 feet by 180 feet with a maximum vertical peak of 28 feet above the main roof level, for a maximum height of 73 feet above average natural grade for Building 20 and a maximum of 28 feet above the roof level for Buildings 21 and 22, to the satisfaction of the Building Official. Menlo Park Fire Protection District approval shall be required each time the tent is erected.
- 9.31 Alcoholic and Beverage Control: The Applicant shall ensure that all on-site suppliers of alcoholic beverages for the cafes/restaurants and special events contractors apply for and receive approval of the appropriate Alcoholic and Beverage Control (ABC) license prior to any on-site alcohol sales and/or service, to the satisfaction of the Community Development Director.

- 9.32 Leadership in Energy and Environmental Design (LEED): The Applicant will design the office buildings and hotel to perform to LEED 2009 Building Design and Construction (BD+C) Gold equivalency. The Applicant may satisfy this obligation by delivering a report from its LEED consultant. That report shall be submitted prior to or concurrent with issuance of TCO's for each respective building and is subject to approval by the Community Development Director (not to be unreasonably withheld or conditioned).
- 9.33 Lighting: Concurrent with building permit submittal for each individual building as appropriate, the Applicant shall submit a lighting plan, including photometric contours, manufacturer's specifications on the fixtures, and mounting heights to ensure safe access and to illustrate the light and glare do not spillover to neighboring properties, to the satisfaction of the Community Development Director and Public Works Director.
- 9.34 Transportation Demand Management Program: The Applicant shall implement a commercially reasonable Transportation Demand Management (TDM) Program to the satisfaction of the Public Works Director and Transportation Manager in order to satisfy the Trip Cap as outlined in Section 7.
- 9.35 Parking Intrusion: The Applicant shall actively work to prevent the parking of employee and visitor vehicles (whose occupant(s)' final destination is the Project Site) or private shuttles in adjacent neighborhoods, including, but not limited to, the Belle Haven neighborhood, on other public streets in the City, and on public streets in the City of East Palo Alto to the satisfaction of the Public Works Director. The City reserves the right to require monitoring of neighborhood parking intrusions consistent with the specifications of the West Campus Trip Cap Monitoring and Enforcement Policy, attached hereto as Exhibit A and incorporated herein.
- 9.36 Primary Entrance Designation: The Applicant shall designate the two stoplight controlled entrances on Bayfront Expressway, the right-in only entrance on Bayfront Expressway, and the entry at the intersection of Constitution Drive and Chilco Street as the primary entrance points to the Project Site. Trucks serving Buildings 21 and 22 shall access the site via Constitution Drive, except for trucks serving multiple buildings within the Property which access the site via Willow Road. The use of the Willow Road entrance shall continue to be primarily used by Facebook shuttles, delivery and service vehicles, and emergency responders, with minimal access for single occupancy vehicles, to the satisfaction of the Public Works Director and Transportation Manager.
- 9.37 Transportation Impact Fee: Prior to building permit issuance for the Foundation Only Permit (Section 8.1), the applicant shall pay the Transportation Impact Fee per the direction of the Transportation Division in compliance with Chapter 13.26 of the Municipal Code. The current estimated total transportation impact fee is \$1,628,094.91, although the final fee shall be the fee in effect at the time of payment and shall be prorated per building. The Transportation Impact Fee escalates annually on July 1.

- 9.38 Relocation of the existing 30 inch on-site storm drain (Building 23): The Applicant shall relocate the existing on-site 30-inch storm drain line to Chilco Street, subject to the review and approval of the Building, Planning, and Engineering Divisions and prior to issuance of the Foundation Only Building Permit for Building 22 and the associated Parking Garage. Upon completion of the relocation, the Applicant may request abandonment of the City's easement recorded as part of the conditions of approval for Building 23 (300 Constitution Drive and identified in the recorded "Agreement Regarding Storm Drain Line.") If the applicant elects to utilize the remaining existing on-site storm drain line as a private storm drain line, then concurrent with the building permit submittal for Building 22, the applicant shall submit a plan for the relocation of the on-site storm drain line outside of the footprint of the deck structure associated with Building 23 as described above. The storm drain line shall be relocated, and the portion of the line to be abandoned shall be capped and filled, prior to temporary occupancy of Building 22.
- 9.39 Construction Equipment Emissions: Concurrent with the submittal of each individual building permit and/or sub-phase, the Applicant shall submit documentation of compliance with Tier 4 emissions standards for all off-road diesel engines used during construction, subject to review and approval of the Building and Planning Divisions. If Tier 4 emissions compliant pile rigs are not available, the Applicant may utilize modified Tier 2 and Tier 3 pile rigs, provided that documentation of consistency with Tier 4 emissions standards for the modified Tier 2 and Tier 3 pile rigs is submitted to the City. The Community Development Director shall review and determine that the modified pile rigs are consistent with Tier 4 emissions standards. City may periodically monitor compliance with this condition.
- 9.40 Construction Parking: Construction parking shall be accommodated per the Construction and Parking Phasing plans in the approved Project Plans.
- 9.41 Construction Hours: Typical construction hours shall be 7:00 a.m. to 10:00 p.m. Monday through Friday and from 8:00 a.m. to 5:00 p.m. on Saturdays. Construction activities taking place outside of the noise ordinance exemption work hours of 8:00 a.m. to 6:00 p.m. Monday through Friday are required to comply with the noise limitations set forth in Chapter 8.06 (Noise) of the municipal code. The Applicant shall not conduct any noise generating exterior building work or site work on Sundays or national holidays. Prior to the issuance of a building permit for each individual phase, the Applicant shall submit a construction work plan and acoustical analysis to the City documenting the expected work hours and compliance with the Noise Ordinance (Chapter 8.06) subject to review and approval of the Building and Planning Divisions.
- 9.42 Lot Line Adjustment: The Applicant has recorded the lot line adjustment with the County of San Mateo to relocate the property line along the eastern boundary of the TE Campus Parcel to the northwestern corner of the Project Site, creating a specific parcel for the Hotel.

- 9.43 Hazardous Materials: Hazardous materials, incidental to office and hotel uses, are permitted to be stored and used at the site. The applicant is required to submit all required documents for typical use permit applications involving hazardous materials (other than those that are incidental to office and hotel uses), including but not limited to the hazardous materials information form, generator supplemental information form, and chemical inventories to the Planning Division for review and routing to the outside reviewing agencies. The use and storage of hazardous materials (other than those that are incidental to office and hotel uses) shall not be permitted until the Planning Division receives approval from the Menlo Park Building Division, Menlo Park Fire Protection District, West Bay Sanitary District, and the San Mateo County Environmental Health Division. If approved by all outside agencies, the Community Development Director or his/her designee shall issue a letter authorizing the use and storage of hazardous materials. Hazardous materials associated with the current operations of TE within Building 305 are permitted as an existing use on-site. If operations substantially change prior to TE vacating the site, Applicant shall request that TE notify the Menlo Park Planning Division and provide documentation of approval of the modified hazardous materials inventory statement (HMIS) and hazardous materials business plan (HMBP) from the Menlo Park Fire Protection District and San Mateo County Environmental Health Division.
- 9.44 Caltrans Maintenance Agreements: If Caltrans requires any of the improvements to be constructed within the Caltrans right-of-way associated with the Project, including but not limited to the Bicycle and Pedestrian Bridge Improvements, to be maintained by the City, the Applicant shall enter into a maintenance agreement with the City to maintain those improvements, at the sole cost and responsibility of the Applicant. Such maintenance agreement shall be recorded and shall be binding on property owners and run with the land.
- 9.45 Lease Agreement with TE Connectivity (Building 305): Applicant has a lease agreement with TE Connectivity (TE) for Building 305. The expiration date for this lease agreement is September 4, 2022, although it may be earlier terminated by TE. Applicant shall not for any reason extend the lease with TE for Building 305 beyond September 4, 2022.
- 9.46 Occupancy of Building 305: Upon expiration or earlier termination of the current lease agreement with TE, Applicant shall not enter into a new agreement with TE or any other entity to occupy Building 305, including any temporary uses of Building 305 between the date TE vacates Building 305 and the demolition of the building as required in Section 9.47. The intent of this Section is to prevent delays in the demolition of Building 305. This Section only applies to Building 305 (and not other portions of the property including other buildings located on the property)) and will automatically terminate once Building 305 has been demolished.
- 9.47 Demolition of Building 305: Within one year from the date that the current lease with TE terminates or expires and TE vacates Building 305, but no later than September 4, 2023, Applicant shall demolish Building 305 in its entirety. Applicant shall obtain all necessary permits for the demolition. If Applicant

diligently pursues demolition of Building 305 but is delayed for reasons beyond its control (e.g., force majeure), Applicant's deadline for demolishing Building 305 shall be equitably extended, subject to the approval of the City Manager which shall not be unreasonably withheld. Applicant agrees to use commercially reasonable efforts to cause TE to timely secure any environmental close-outs that TE must perform in order for Applicant to pursue demolition of Building 305.

9.48 The Applicant shall comply with Mitigation Measure BIO-1 of the ConnectMenlo General Plan Update Mitigation Monitoring and Reporting Program to ensure that any potential impacts of the Project on the Refuge (north of the site) are mitigated. BIO-1 shall apply to all phases of the Project as applicable and compliance shall be documented by the Applicant concurrent with building permit submittal for each phase and reviewed and accepted by the Planning Division.

9.49 Based on soils samples at the site, it is not expected that the California Department of Toxic Substance Control (DTSC) would require a vapor barrier for Building 22. However, the Applicant has voluntarily agreed to install a vapor barrier under Building 22. This barrier shall be installed to the satisfaction of the Building and Planning Divisions.

9.50 Prior to issuance of building permits for the parking garage and Building 22, the Applicant shall provide documentation of abandonment or relocation of all easements within the footprints of each structure, subject to review and approval of the Planning and Engineering Divisions.

10. PROJECT SPECIFIC CONDITIONS - UNDERCROSSING IMPROVEMENTS

10.1 Public Access Easements: The Applicant shall continue to maintain the public access easement(s) under Bayfront Expressway near Willow Road for utilization of the undercrossing by members of the public.

10.2 Stormwater Operations and Maintenance Agreement: The Applicant shall continue to comply with the West Campus Undercrossing Improvements Operations and Maintenance Agreement. The Operations and Maintenance Agreement establishes a self-perpetuating drainage system maintenance program (to be managed by the property owner or property manager) that includes annual inspections of any infiltration features and stormwater detention devices (if any), and drainage inlets, flow through planters, and other BMPs. Any accumulation of sediment or other debris shall be promptly removed. Funding for long-term maintenance of all BMPs is specified in the Operations and Maintenance Agreement. An annual report documenting the inspection and any remedial action conducted shall continue to be submitted to the Public Works Director for review.

11. PROJECT SPECIFIC CONDITIONS - BICYCLE AND PEDESTRIAN BRIDGE

11.1 Sequencing Bicycle and Pedestrian Bridge over Bayfront Expressway: The following outlines the basic sequencing of required permits and construction related to the Bicycle and Pedestrian Bridge over Bayfront

Expressway and the design and construction of the publicly accessible open space, as identified in the Project Plans.

- i. Bonding: The Applicant shall post a bond to complete the Conceptual Bicycle and Pedestrian Bridge Improvements depicted in the plan set associated with Caltrans Encroachment permit number EA 2K590 to be drawn on if the Project is not developed as anticipated.
 1. A cost estimate for the construction of the Bicycle and Pedestrian Bridge Improvements shall be provided to the City on the Effective Date of the Second Amended and Restated Development Permit as defined in section 5.2 for review and approval of the Public Works Director.
 2. A bond for 200% of the approved cost estimate shall be posted within 30 days of the Effective Date of the Second Amended and Restated Conditional Use Permit as defined in section 5.2, unless Applicant has already posted a bond pursuant to the original Amended and Restated Conditional Development Permit.

11.2 City Approval: The Applicant shall apply for City approval of the Bicycle and Pedestrian Bridge and Public Open Space as follows:

- 11.2.1 Submit a substantially complete set of Bicycle and Pedestrian Bridge Improvement plans to the City concurrent with the building permit submittal for the Core and Shell phase for Building 21. For purposes of this Section 11.2.1, "substantially complete" shall mean 65% structural plans and details.
- 11.2.2 Outside Agency Approval: Submit applications to applicable outside agencies within 30 days of City approval of the Bicycle and Pedestrian Bridge Improvement plans, and diligently pursue approvals from those outside agencies. Applicable agencies with permitting authority for the Bicycle and Pedestrian Bridge Improvements include:
 - Caltrans;
 - Pacific Gas and Electric (PG&E);
 - U.S. Fish and Wildlife Service; and
 - Other Agencies with Jurisdiction.
- 11.2.3 Submit documentation of approval from the agencies with permitting authority for the Bicycle and Pedestrian Bridge to the City for review and confirmation by the City Building Official and Public Works Director prior to issuance of a building permit for the Bicycle and Pedestrian Bridge.

- 11.3 Caltrans Approval: Prior to building permit issuance for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall submit all necessary improvement plans and documents required by Caltrans for work associated with the Project and under their jurisdiction. The design plans shall be subject to review and approval of the Public Works Director prior to submittal to Caltrans. The Applicant shall diligently pursue permitting approval to the satisfaction of the Public Works Director. Upon Caltrans approval and the approval of any other agencies, including the USFWS and/or the Refuge, with jurisdiction over the Bicycle and Pedestrian Bridge Improvements, the Applicant shall exercise good faith efforts to diligently construct the bridge and associated improvements prior to granting occupancy for Building 22. Construction of the bridge and associated improvements shall be subject to all avoidance and minimization measures imposed by USFWS and/or the Refuge during the permitting process to ensure that any impacts on the Refuge or wildlife are minimized.
- 11.4 Pacific Gas and Electric (PG&E): Prior to building permit issuance for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall submit all necessary improvement plans and documents required by Pacific Gas and Electric (PG&E) for work associated with the Project and under PG&E's jurisdiction, including, but not limited to bridge location and separation between the high voltage power lines. The plans shall be submitted to the Public Works Director for review and approval prior to submittal to PG&E. The Applicant shall diligently pursue permitting approval to the satisfaction of the Public Works Director.
- 11.5 U.S. Fish and Wildlife Service (USFWS): Prior to building permit issuance for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall submit all necessary improvement plans and documents required by U.S. Fish and Wildlife Service (USFWS) for work associated with the Project and under the USFWS jurisdiction, including, but not limited to touchdown of the bridge adjacent to the Don Edward's Wildlife Refuge (Refuge) and the overlook on the northern side of the Bay Trail, encroachment permits for construction staging, realignment of the Bay Trail to the north into the Refuge's right of way to provide east-west connectivity around the bridge's northern ramp, and potentially a low Bay Trail fence. The plans shall be submitted to the Public Works Director for review and approval prior to submittal to USFWS. The Applicant shall diligently pursue permitting approval to the satisfaction of the Public Works Director.
- 11.6 Bay Trail Project Coordination: Prior to building permit issuance for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall work cooperatively with the Bay Trail Project on the design of the Bicycle and Pedestrian Bridge Improvements to ensure that the bridge touchdown is compliant with the Bay Trail requirements to the maximum extent practicable, all to the satisfaction of the Public Works Director.
- 11.7 Public Access: Concurrent with complete plan set submittal for construction of the Bicycle and Pedestrian Bridge Improvements, the Applicant shall

submit a plat and legal description for a public access easement(s) for utilization of the Bicycle and Pedestrian Bridge to the satisfaction of the Public Works Director. The form of public access easement shall permit Applicant to establish reasonable rules and regulations governing its use and to temporarily suspend access to the Bicycle and Pedestrian Bridge in case of emergencies. The acceptance of the deed or dedication requires Menlo Park City Council approval prior to occupancy of Building 22.

- 11.8 Utility Improvements: Concurrent with building permit submittal for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Community Development Director and Public Works Director prior to building permit issuance. Landscaping shall properly screen all utility equipment that is installed outside of a structure or building and cannot be placed underground; subject, however, to the requirements of the Menlo Park Fire Protection District, the West Bay Sanitary District, PG&E and any other applicable agencies regarding utility clearances and screening. The plan for new utility installations/upgrades shall show exact locations of all meters. The screening shall be compatible and unobtrusive and subject to the review and approval of the Community Development Director prior to building permit issuance.
- 11.9 Grading and Drainage: Concurrent with building permit submittal for the Bicycle and Pedestrian Bridge Improvements, the applicant shall submit a Grading and Drainage Plan, including an Erosion and Sedimentation Control Plan, for review and approval by the Engineering Division prior to building permit issuance. The Grading and Drainage Plan shall be prepared based on the City's Grading and Drainage Plan Guidelines and Checklist, the City approved Hydrology Report for the Project, and the Project Applicant Checklist for the National Pollution Discharge Elimination System (NPDES) Permit Requirements to the satisfaction of the Public Works Director.
- 11.10 Lighting: Concurrent with building permit submittal for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall submit a lighting plan, including photometric contours, manufacturer's specifications on the fixtures, and mounting heights to ensure safe access and to illustrate the light and glare do not spillover beyond the extent of the pathway on the Bicycle and Pedestrian Bridge and the access to the bridge, subject to the satisfaction of the Community Development Director and Public Works Director.
- 11.11 Comply with Applicable Requirements: The Applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the Project, to the satisfaction of the Community Development Director.
- 11.12 Building Construction Street Impact Fee: Prior to issuance of the building permit for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall pay the applicable building construction street impact fee in effect at the time of payment to the satisfaction of the Public Works Director. The

amount of such fees may be reduced in the reasonable discretion of the Public Works Director depending on the extent to which construction vehicle traffic is occurring on City streets, and shall be subject to a credit for work performed in connection with Phases 5 and 6 of the Chilco Streetscape Improvements pursuant to the Amended Development Agreement for 301-309 Constitution Drive.

- 11.13 Utility and Communication Provider Requirements: As applicable, the Applicant must comply with all regulations of Pacific Gas and Electric, West Bay Sanitary District and other applicable communication providers (i.e., AT&T and Comcast) that are directly applicable to the Project, to the satisfaction of the Community Development Director.
- 11.14 Operations and Maintenance Agreement: Prior to approval of building permit final for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall enter into an Operations and Maintenance Agreement with the City. Alternatively, an existing Operation and Maintenance Agreement may be amended to include the Bicycle and Pedestrian Bridge. The Operations and Maintenance Agreement shall establish a self-perpetuating drainage system maintenance program (to be managed by the Property Owner or property manager) that includes annual inspections of any infiltration features and stormwater detention devices (if any), and drainage inlets, flow through planters, and other BMPs. Any accumulation of sediment or other debris shall be promptly removed. Funding for longterm maintenance of all BMPs must be specified in the Operations and Maintenance Agreement. The Operation and Maintenance Agreement shall be subject to review and approval of the City Attorney and the Public Works Director and shall be recorded prior to building permit final inspection. An annual report documenting the inspection and any remedial action conducted shall be submitted to the Public Works Director for review.
- 11.15 Maintenance Agreement: Prior to approval of building permit final inspection for the Bicycle and Pedestrian Bridge Improvements, the Applicant shall enter into a Maintenance Agreement with the City to maintain the Bicycle and Pedestrian Bridge, including but not limited to typical cleaning and repairs, at the Applicant's sole cost. This long-term Maintenance Agreement shall provide that at the end of the useful life of the improvements, Applicant shall have the right to demolish the improvements and shall have no obligation to replace or reconstruct the improvements.
- 11.16 Construction and Demolition Debris: As applicable, the Applicant shall comply with the requirements of Chapter 12.48 (Salvaging and Recycling of Construction and Demolition Debris) of the City of Menlo Park Municipal Code, subject to review and approval by the Community Development Director or his/her designee.
- 11.17 Erosion and Sedimentation Control: Concurrent with building permit submittal for the Bicycle and Pedestrian Bridge Improvements, the

Applicant shall submit a plan for construction of safety fences around the periphery of the construction area and a demolition Erosion and Sedimentation Control Plan. The fences and erosion and sedimentation control measures shall be installed according to the plan prior to commencing construction. The plans shall be reviewed and approved by the Community Development Director or his/her designee prior to issuance of a demolition permit.

- 11.18 Construction: Construct the Bicycle and Pedestrian Bridge prior to the occupancy of Building 22.
- 11.19 Building Permit Final: All building permits associated with the Bicycle and Pedestrian Bridge Improvements shall receive final inspection approval from the City prior to granting occupancy for Building 22 to the satisfaction of the Public Works Director and Community Development Director or his/her designee.
- 11.20 Extensions: If Applicant diligently pursues approval from the outside agencies but is delayed in obtaining approval and subsequently construction is delayed due to no fault or lack of diligence on the part of the Applicant, Applicant's obligations to construct the Pedestrian and Bicycle Bridge Improvements shall be extended automatically, subject to the approval of the City Manager which shall not be unreasonably withheld. In addition, the City Manager may extend the timeline for delivery of the Bicycle and Pedestrian Bridge due to construction delays due to circumstances outside the Applicant's reasonable control. In reviewing the request to extend the timeline for delivery, the City Manager shall consider evidence from the Applicant of a good faith effort to complete construction of the bridge prior to occupancy of Building 22.

12. PROJECT SPECIFIC CONDITIONS - PUBLIC OPEN SPACE

- 12.1 Sequencing Public Open Space: The following outlines the basic sequencing of required permits and review for the Publicly Accessible Open Space as identified in the Project Plans.
 - 12.1.1 The Publicly Accessible Open Space shall be constructed in two phases. The first phase (interim) shall be constructed prior to occupancy of Building 22 and the second phase (final) shall be constructed within 18 months of the date the current lease with TE for Building 305 terminates or expires and TE vacates Building 305, but in no event later than March 4, 2024.
- 12.2 City Approval: The Applicant shall submit the landscape and site improvement plans for the interim phase of the proposed Publicly Accessible Open Space concurrent with the submittal of the building permit for the Grading and Drainage phase for Building 22. The landscape and site improvements plans for the final phase shall be submitted

concurrent with the demolition permit for Building 305. The applicant shall submit the plans as follows:

- 12.2.1 Submit complete set of interim phase of the Public Open Space Improvement plans to the City concurrent with the Grading and Drainage building permit phase for Building 22, and submit a complete plan set for the final phase concurrent with the submittal of a demolition permit for Building 305.
 - 12.2.2 The project plans for each phase shall identify the location of all structures, hardscaping, and landscaping, and shall be accompanied by a project description letter documenting conformance with the approved site plan and this Amended and Restated Conditional Development Permit.
 - 12.2.3 The design of the Publicly Accessible Open Space shall be reviewed and approved by the Community Development Director and Public Works Director.
 - 12.2.4 The City shall expeditiously process and review the permit plans for the Publicly Accessible Open Space.
 - 12.2.5 The Applicant shall construct and complete the interim Publicly Accessible Open Space concurrent with Building 22, specifically with the Landscaping building permit phase associated with Building 22. However, the permit for the Publicly Accessible Open Space can be issued separately from the building permits for Building 22.
 - 12.2.6 Final Inspection and/or temporary occupancy for Building 22 shall not be granted by the City until the interim Publicly Accessible Open Space is completed to the satisfaction of the Public Works Director and Community Development Director.
 - 12.2.7 The final phase of the Publicly Accessible Open Space shall be completed within 18 months of the date the current lease with TE for Building 305 terminates or expires and TE vacates Building 305, but in no event later than March 4, 2024. If Applicant diligently pursues completion of Phase 2 but is delayed due to no fault or lack of diligence on the part of the Applicant, Applicant's obligation shall be extended automatically, subject to the approval of the City Manager which shall not be unreasonably withheld.
- 12.3 Public Access Irrevocable License: Concurrent with complete plan set submittal for the interim and final phase permits for the Publicly Accessible Open Space, respectively, the Applicant shall submit a plat and legal description and proposed form of irrevocable license for public utilization of the Publicly Accessible Open Space to the satisfaction of the Public Works Director and City Attorney. The form of irrevocable license shall ensure that Applicant has reasonable control over the public open space.

- 12.3.1 **Interim Phase:** The irrevocable license agreement requires City Manager approval and shall be recorded with the County of San Mateo prior to final inspection for the Tenant Improvements building permit phase for Building 22.
- 12.3.2 **Final Phase:** Upon demolition of Building 305 and submittal of the plans for the full extent of the Publicly Accessible Open Space, the Applicant shall submit an amended and restated irrevocable license for the public utilization of the full extent of the Publicly Accessible Open Space in substantially the same form as the irrevocable license recorded for the interim phase and to the satisfaction of the City Attorney. The amended and restated irrevocable license agreement requires City Manager approval and shall be recorded with the County of San Mateo. The irrevocable license shall be approved and recorded prior to completion of the final phase of the Publicly Accessible Open Space.
- 12.4 **Utility Improvements:** Concurrent with the permit submittal for the improvement plans for each phase of the Publicly Accessible Open Space, the Applicant shall submit a plan for any new utility installations or upgrades within the public open space for review and approval of the Community Development Director and Public Works Director prior to permit issuance. Landscaping shall properly screen any utility equipment that is installed outside of a structure/building and cannot be placed underground; subject, however, to the requirements of the Menlo Park Fire Protection District, the West Bay Sanitary District, PG&E and any other applicable agencies regarding utility clearances and screening. The plan for new utility installations/upgrades shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes and other equipment boxes. The screening shall be compatible and unobtrusive and subject to the review and approval of the Community Development Director prior to landscaping and site improvement permit issuance.
- 12.5 **Grading and Drainage:** Concurrent with permit submittal for each phase of the Publicly Accessible Open Space, the applicant shall submit a Grading and Drainage Plan, including an Erosion and Sedimentation Control Plan, for review and approval by the Engineering Division prior to building permit issuance. The Grading and Drainage Plan shall be prepared based on the City's Grading and Drainage Plan Guidelines and Checklist, the City approved Hydrology Report for the Project, and the Project Applicant Checklist for the National Pollution Discharge Elimination System (NPDES) Permit Requirements to the satisfaction of the Public Works Director.
- 12.6 **Landscape Plan:** Concurrent with permit submittal for each phase of the Publicly Accessible Open Space, the Applicant shall submit a detailed on-site landscape plan, including the size, species, and location, and an irrigation plan for review and approval by the Community Development Director and Public Works Director. The landscape plan shall include all

onsite landscaping, adequate sight distance visibility, screening for outside utilities with labels for the utility boxes sizes and heights, fencing inclusive of fence height and materials, and documentation confirming compliance with the Water Efficient Landscaping Ordinance (Municipal Code Chapter 12.44), if applicable. If heritage tree replacements are proposed, the plans shall document the species, size, and number for compliance with the Amended and Restated Conditional Development Permit, subject to review and approval of the City Arborist and Planning Division. The landscape plan shall include an appropriate mix of native and adapted species to complement the nearby Don Edwards San Francisco Bay National Wildlife Refuge and shall be reviewed and approved by the Community Development Director and Public Works Director prior to building permit issuance.

- 12.7 Lighting: Concurrent with the permit submittal for each phase of the Publicly Accessible Open Space, the Applicant shall submit a lighting plan, including photometric contours, manufacturer's specifications on the fixtures, and mounting heights to ensure safe access and to illustrate the light and glare do not spillover to neighboring properties, to the satisfaction of the Community Development Director and Public Works Director.
- 12.8 Comply with Applicable Requirements: The Applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project, to the satisfaction of the Community Development Director.
- 12.9 Building Construction Street Impact Fee: Prior to issuance of the building permit for each phase of the Publicly Accessible Open Space, the Applicant shall pay the applicable building construction street impact fee in effect at the time of payment to the satisfaction of the Public Works Director. The amount of such fees may be reduced in the reasonable discretion of the Public Works Director depending on the extent to which construction vehicle traffic is occurring on City streets, and shall be subject to a credit for work performed in connection with Phases 5 and 6 of the Chilco Streetscape Improvements pursuant to the Amended Development Agreement.
- 12.10 Utility and Communication Provider Requirements: The Applicant must comply with all regulations of Pacific Gas and Electric, West Bay Sanitary District and other applicable communication providers (i.e., AT&T and Comcast) that are directly applicable to the Project, to the satisfaction of the Community Development Director.
- 12.11 Stormwater Operations and Maintenance Agreement: Prior to permit final for each phase of the Publicly Accessible Open Space, the Applicant shall enter into an Operations and Maintenance Agreement with the City or amend the previous agreement for the Bicycle and Pedestrian Bridge. The Operations and Maintenance Agreement shall establish a self-perpetuating drainage system maintenance program (to be managed by the property owner or property manager) that includes annual inspections of any infiltration features and stormwater detention devices (if any), and drainage

inlets, flow through planters, and other BMPs. Any accumulation of sediment or other debris shall be promptly removed. Funding for long-term maintenance of all BMPs must be specified in the Operations and Maintenance Agreement. The Operation and Maintenance Agreement or amended agreement, shall be subject to review and approval of the City Attorney and the Public Works Director and shall be recorded prior to building permit final inspection. An annual report documenting the inspection and any remedial action conducted shall be submitted to the Public Works Director for review.

- 12.12 Construction and Demolition Debris: If applicable, the Applicant shall comply with the requirements of Chapter 12.48 (Salvaging and Recycling of Construction and Demolition Debris) of the City of Menlo Park Municipal Code, subject to review and approval by the Building Official.
- 12.13 Erosion and Sedimentation Control: Concurrent with building permit submittal for each phase of the Publicly Accessible Open Space, the Applicant shall submit a plan for construction of safety fences around the periphery of the construction area and a Demolition Erosion and Sedimentation Control Plan. The fences and erosion and sedimentation control measures shall be installed according to the plan prior to commencing construction. The plans shall be reviewed and approved by the Building Official prior to issuance of a demolition permit.
- 12.14 Building Permit Final: All building or landscaping permits associated with the interim phase of the Publicly Accessible Open Space PAOS shall receive final inspection approval prior to issuance of temporary occupancy or building permit final for the Tenant Improvements building permit phase for Building 22. All building or landscaping permits associated with the final phase of the Publicly Accessible Open Space shall receive final inspection approval no later than 18 months after the date the current lease with TE terminates or expires and TE vacates Building 305, but in no event later than March 4, 2024.
- 12.15 Extensions and Sequencing Modifications: If Applicant diligently pursues approval of the Publicly Accessible Open Space but is delayed in obtaining approval due to no fault or lack of diligence on the part of the Applicant, the City Manager has the authority to extend the timeline for completion of each phase of the Publicly Accessible Open Space for a reasonable period of time. In addition, the City Manager may extend the timeline for delivery of the Publicly Accessible Open Space due to construction delays due to circumstances outside the Applicant's reasonable control. In reviewing the request to extend the timeline for delivery, the City Manager shall consider evidence from the Applicant of a good faith effort to construct the interim Publicly Accessible Open Space prior to issuance of the Foundation Only building permit phase for Building 22, and to construct the final phase before 18 months from the date the current lease with TE terminates or expires and vacates Building 305.

13. PROJECT SPECIFIC CONDITIONS - ON-SITE RECYCLED WATER

- 13.1 Timing: Prior to issuance of the Foundation Only building permit for Building 21, the Applicant shall submit documentation to the City's Building Official of compliance with State and Local requirements for the on-site water recycling facility. State and local approvals shall be submitted prior to issuance of the Tenant Improvement building permit.
- 13.2 City Submittal Requirements: Concurrent with the submittal of the Foundation Only building permit phase for Building 21, the following items shall be submitted to the Community Development Director or his/her designee for review and determination of the use of alternate means and methods:
- 13.2.1 Description of On-site System: The submittal package for the on-site water recycling system shall include a description of the proposed system, including but not limited to the following documentation: Treatment technology and level of treatment, compliance with State regulations (not approvals), system capacity, maintenance and operational plans, flood protection measures, and recycled water uses.
- 13.2.2 Project Plan Set: Concurrent with the submittal of the Building 21 Foundation Only building permit phase, the Applicant shall submit the schematic design plan set for the proposed on-site wastewater treatment system to the City. The plan set should document the location of the membrane bio-reactor (MBR) or the constructed drain and fill wetlands, and document how the proposed on-site wastewater system would be designed and operated. The plans shall show the locations for recycled water usage, including irrigation emitter types, and hours of operation for the irrigation system.
- 13.2.3 California State Water Board Permit: Concurrent with the submittal of the building permit application, the Applicant shall submit a copy of the Water Reuse Permit application to the California Regional Water Quality Control Board, San Francisco Bay Region (Water Board) - Division of Drinking Water and Watershed Management Division.
- 13.3 Building Permit Review and Determination: The Community Development Director or his/her designee will be responsible for evaluating the permit plans and documents submitted by the Applicant. The Community Development Director has authority to grant the use of alternate means and methods for the on-site recycled wastewater system. Prior to issuing a building permit for the on-site recycled water system, a copy of the approved Water Reuse Permit for the Project must be submitted to the City for the Community Development Director's review.

- 13.4 Construction: The Applicant shall construct the onsite water treatment system and obtain all required approvals and inspections from the City and other agencies of jurisdiction prior to building permit final for the Tenant Improvements phase of Building 21. The on-site treatment system shall be constructed in accordance with the approved building permit plans, subject to review and approval of the Community Development Director or his/her designee.
- 13.5 Maintenance And Operations Agreement: Prior to final inspection of the Tenant Improvements phase of the building permit for Building 21, the Applicant shall enter into an agreement for the operation and maintenance of the system or similar instrument to require the Applicant to consistently maintain and operate the system in compliance with the building permit from the City and the permits from the Department of Water Resources and/or County of San Mateo Environmental Health. The form of agreement shall be subject to review and approval of the City Attorney, Community Development Director, and the Public Works Director and shall be recorded prior to final inspection of the Tenant Improvements phase or temporary occupancy for Building 21. An annual report documenting the inspection record and compliance with City, County, and State requirements shall be submitted to the City for the Community Development Director's and Public Works Director's review for compliance. Applicant shall have the right to decommission the recycled water system in the future provided that the Applicant demonstrates that recycled water will be used for landscaping and other non-potable uses via an alternative means (e.g., connecting the office buildings to a recycled water system operated by the City and/or West Bay Sanitary District).
- 13.6 Extensions and Sequencing Modifications: If Applicant diligently pursues approval of the on-site recycled water system but is delayed in obtaining approval due to no fault or lack of diligence on the part of the Applicant, the City Manager has the authority to extend the timeline for submitting documentation of State and local approvals until after issuance of the Tenant Improvement building permit and to extend the timeline for completion of the on-site recycled water system for a reasonable period of time. In addition, the City Manager may extend the timeline for installation of the on-site recycled water system due to construction delays due to circumstances outside the Applicant's reasonable control. In reviewing the request to extend the timeline for delivery, the City Manager shall consider evidence from the Applicant of a good faith effort to obtain approvals to construct the on-site recycled water system and shall ensure that if temporary occupancy is granted to Building 21, adequate safeguards shall be in place to ensure that only potable water is used in the building's plumbing fixtures. If Applicant is unable to obtain all permits necessary to construct and operate an on-site recycled water system, Applicant shall comply with requirements set forth in Section 12 of the Amended Development Agreement.

14. PROJECT SPECIFIC CONDITIONS - ACCESS PARCEL (FACEBOOK WAY)

- 14.1 Recordation: The Lot Line Adjustment that established an Access Parcel (055-260-997) and a Main Parcel (055-260-290) for Building 20, including the private road name for the Project Site, shall be maintained as part of the Lot Line Adjustment between Parcel Numbers 055-260-250 and 055260-290.
 - 14.2 Common Ownership: The Access Parcel (055-260-997) and a Main Parcel (055-260-290) shall remain in common ownership in perpetuity, to the satisfaction of the Public Works Director.
 - 14.3 Road Naming: The name of the private road (Facebook Way) shall be maintained per the previously recorded lot line adjustment. Future changes to the road name shall require the applicant to submit a plat map and legal description specifying the new road name for the review of the Public Works Director, and said document shall be recorded, or the applicant shall comply with such other procedures as the Public Works Director determines in his/her reasonable discretion. The provided documentation shall be subject to review and approval of the Building Official, Public Works Director, and Menlo Park Fire Protection District.
 - 14.4 Access Parcel Use: The Access Parcel shall be solely for road purposes and provision of vehicular, bicycle and pedestrian access, and shall be an unbuildable parcel. No permanent or temporary structures are permitted to encroach into the access parcel, in perpetuity, to the satisfaction of the Public Works Director and Building Official.
15. PROJECT SPECIFIC CONDITIONS - MITIGATION MEASURES CARRIED FORWARD FROM BUILDING 20 APPROVAL

Mitigation measures that are associated with both the Project, the East Campus and the West Campus only need to be satisfied once.

- 15.1 Implement Bird-Safe Design Standards into West Campus Building and Lighting Design: All new buildings and lighting features constructed or installed at the West Campus shall be implemented to at least a level of "Select Bird-Safe Building" standards as defined in the City of San Francisco Planning Department's "Standards for Bird-Safe Buildings," adopted July 14, 2011. These design features shall include minimization of bird hazards as defined in the standards. With respect to lighting, the West Campus shall:
 - Be designed to minimize light pollution including light trespass, over-illumination, glare, light clutter, and skyglow while using bird-friendly lighting colors when possible.
 - Avoid uplighting, light spillage, event search lights, and use green and blue lights when possible.

- Turn off unneeded interior and exterior lighting from dusk to dawn during migrations: February 15 through May 31 and August 15 through November 30.
 - Include window coverings on rooms where interior lighting is used at night that adequately block light transmission and motion sensors or controls to extinguish lights in unoccupied spaces (West Campus MM-BR-4.2).
- 15.2 Record Additional Restrictions: The Applicant shall ensure that the updated OMMP (West Campus Mitigation Measure HM-2.1) includes provisions for disclosing information in DTSC-approved remediation reports along with any other requirements pertaining to post-construction, long-term operation and maintenance of subsurface utilities or maintenance or repair of foundations. Any such documentation shall be recorded in the Office of the County Recorder and a copy shall be provided to the City (West Campus MM-HM-5.1).
- 15.3 Limit Generator Testing to Daytime Hours on the West Campus: The Applicant shall limit generator testing to between the hours of 8:00 a.m. and 6:00 p.m (West Campus MM-NO-1.2).
16. PROJECT SPECIFIC MITIGATION MEASURES - BUILDINGS 21, 22, AND HOTEL
- 16.1 The Applicant shall comply with all mitigation measures identified in the EIR and the associated Mitigation Monitoring and Reporting Program for the Project attached hereto as Exhibit D. and any modifications thereto as identified in the Addendum
17. GENERAL CONDITIONS:
- 17.1 Indemnity By Applicant: Applicant shall indemnify, defend and hold harmless the City, and its elective and appointive boards, commissions, officers, agents, contractors, and employees (collectively, City Indemnified Parties) from any and all claims, causes of action, damages, costs or expenses (including reasonable attorneys' fees) arising out of or in connection with, or caused on account of, the development and occupancy of the Project, any Approval with respect thereto, or claims for injury or death to persons, or damage to property, as a result of the operations of Applicant or its employees, agents, contractors, representatives or tenants with respect to the Project (collectively, Applicant Claims); provided, however, that the Applicant shall have no liability under this Section for Applicant Claims that (a) arise from the gross negligence or willful misconduct of any City Indemnified Party, or (b) arise from, or are alleged to arise from, the repair or maintenance by the City of any improvements that have been offered for dedication by the Applicant and accepted by the City.
- 17.2 Covenants Run with the Land: All of the conditions contained in this Second Amended and Restated Conditional Development Permit shall run

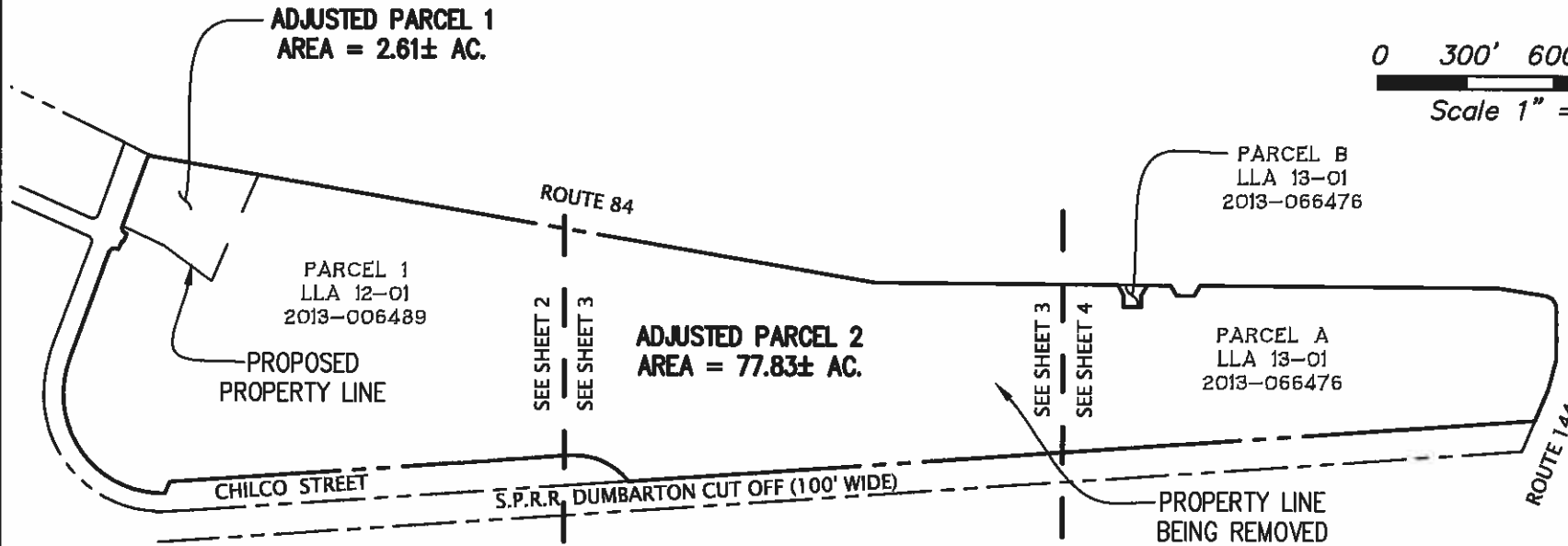
with the land comprising the Property and shall be binding upon, and shall inure to the benefit of the Applicant and its heirs, successors, assigns, devisees, administrators, representatives and lessees, except as otherwise expressly provided in this Second Amended Conditional Development Permit.

- 17.3 Consistency: In the event of a conflict between the Amended Development Agreement, the Development Agreement for the West Campus, and this Second Amended and Restated Conditional Development Permit, the more restrictive provision in any of such documents shall control.
- 17.4 Severability: If any condition of this Second Amended Conditional Development Permit, or any part hereof, is held by a court of competent jurisdiction in a final judicial action to be void, voidable or enforceable, such condition, or part hereof, shall be deemed severable from the remaining conditions of this Second Amended Conditional Development Permit and shall in no way affect the validity of the remaining conditions hereof.
- 17.5 Exhibits: The exhibits referred to herein are deemed incorporated into this Second Amended Conditional Development Permit in their entirety.

- Exhibit A: Plat to Accompany Legal Description
Exhibit B: Legal Description
Exhibit C: West Campus Trip Cap Monitoring and Enforcement Policy
Exhibit D: MMRP for Buildings 21-22 and Hotel
Exhibit E: Updated Mitigation Measures from EIR Addendum



0 300' 600' 1200'
Scale 1" = 600 ft



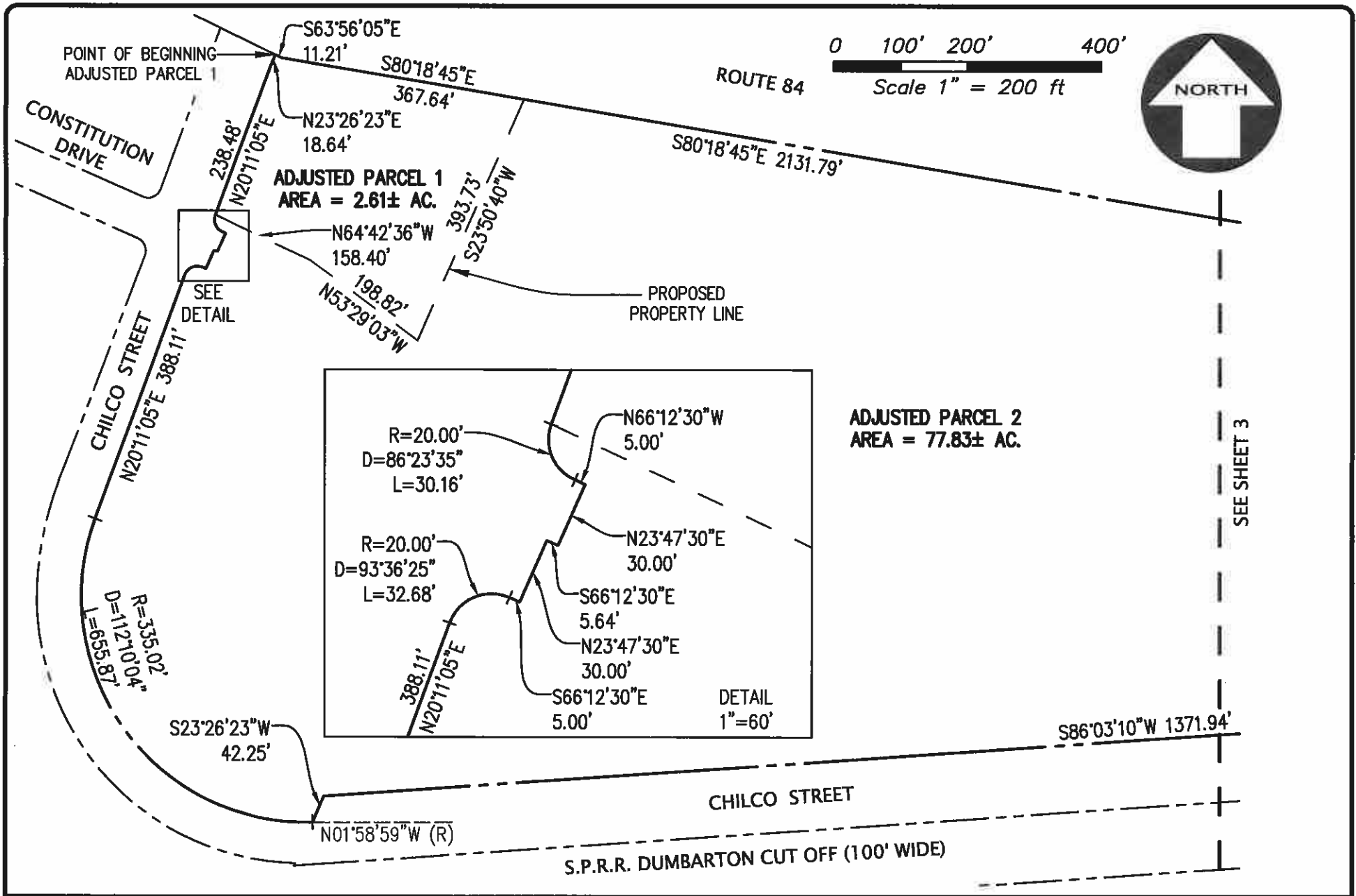
KIER & WRIGHT
CIVIL ENGINEERS & SURVEYORS, INC.
 2850 Collier Canyon Road Phone (925) 245-8788
 Livermore, California 94551 Fax (925) 245-8796

EXHIBIT "A"
PLAT TO ACCOMPANY LEGAL DESCRIPTION

MENLO PARK,

CALIFORNIA

DATE	NOVEMBER, 2016
SCALE	1" = 600'
BY	RJH
JOB NO.	A15571-3
SHEET	1 OF 4



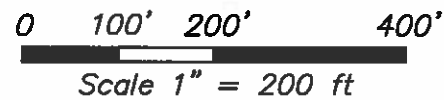
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EXHIBIT "A"
PLAT TO ACCOMPANY LEGAL DESCRIPTION

MENLO PARK,

CALIFORNIA

DATE	NOVEMBER, 2016
SCALE	1" = 200'
BY	RJH
JOB NO.	A15571-3
SHEET	2 OF 4



ROUTE 84

S80°18'45"E 2131.79'

N89°38'32"W 823.77'

SEE SHEET 2

ADJUSTED PARCEL 2
AREA = 77.83± AC.

R=231.73'
D=53°54'34"
L=218.04'

S86°03'10"W 1371.94'

S51°25'59"W (R)

S86°03'10"W 3091.29'

S.P.R.R. DUMBARTON CUT OFF (100' WIDE)

SEE SHEET 4



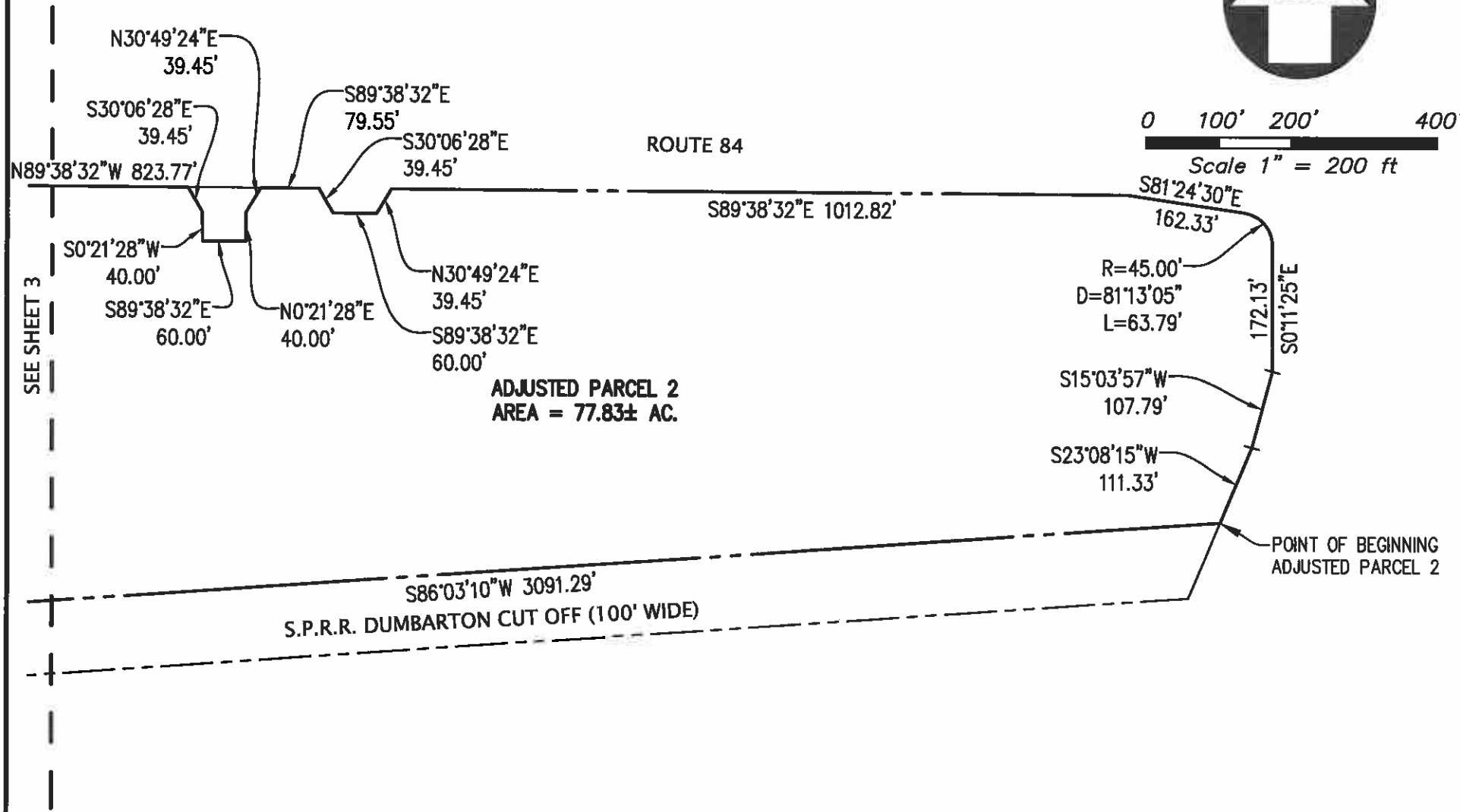
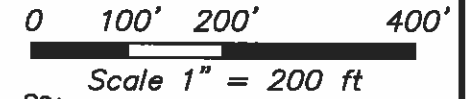
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EXHIBIT "A"
PLAT TO ACCOMPANY LEGAL DESCRIPTION

MENLO PARK,

CALIFORNIA

DATE	NOVEMBER, 2016
SCALE	1" = 200'
BY	RJH
JOB NO.	A15571-3
SHEET	3 OF 4



ADJUSTED PARCEL 2
AREA = 77.83± AC.

POINT OF BEGINNING
 ADJUSTED PARCEL 2



KIER & WRIGHT
 CIVIL ENGINEERS & SURVEYORS, INC.
 2850 Collier Canyon Road Phone (925) 245-8788
 Livermore, California 94551 Fax (925) 245-8796

EXHIBIT "A"
PLAT TO ACCOMPANY LEGAL DESCRIPTION

MENLO PARK,

CALIFORNIA

DATE	NOVEMBER, 2016
SCALE	1" = 200'
BY	RJH
JOB NO.	A15571-3
SHEET	4 OF 4

EXHIBIT 'B'
LEGAL DESCRIPTION

ADJUSTED PARCEL 1

REAL PROPERTY SITUATE IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEING A PORTION OF PARCEL 1, AS SAID PARCEL IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 12-01, RECORDED JANUARY 11, 2013, AS INSTRUMENT NO. 2013-006489, OFFICIAL RECORDS OF SAN MATEO COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE INTERSECTION OF THE SOUTHERLY RIGHT OF WAY OF ROUTE 84, AS SAID ROUTE IS SHOWN ON THAT CERTAIN CALTRANS RIGHT OF WAY MAP FOR ROUTE 84 IN THE COUNTY OF SAN MATEO ON SHEETS R-105.2 THROUGH R-105.4, WITH THE EAST RIGHT OF WAY LINE OF CHILCO STREET, AS SAID RIGHT OF WAY IS DESCRIBED AS PARCEL 46737-3 IN THAT CERTAIN DOCUMENT RECORDED JULY 27, 1983 AS DOCUMENT NUMBER 83-078012, OFFICIAL RECORDS OF SAN MATEO COUNTY, SAID INTERSECTION ALSO BEING THE NORTHWEST CORNER OF SAID PARCEL 1;

THENCE ALONG SAID SOUTHERLY RIGHT OF WAY, SOUTH 63° 56' 05" EAST, 11.21 FEET;

THENCE CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY, SOUTH 80° 18' 45" EAST, 367.64 FEET;

THENCE SOUTH 23° 50' 40" WEST, 393.73 FEET;

THENCE NORTH 53° 29' 03" WEST, 198.82 FEET;

THENCE NORTH 64° 42' 36" WEST, 158.40 FEET TO A POINT ON SAID EASTERLY RIGHT OF WAY;

THENCE ALONG SAID EASTERLY RIGHT OF WAY, NORTH 20° 11' 05" EAST, 238.48 FEET;

THENCE CONTINUING ALONG SAID EASTERLY RIGHT OF WAY, NORTH 23° 26' 23" EAST, 18.64 FEET TO THE **POINT OF BEGINNING**.

CONTAINING 2.61 ACRES OF LAND, MORE OR LESS.

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.



RICHARD J. HICKENBOTTOM, LS 8654
LICENSE EXPIRES: 12/31/17

11/9/16

DATE

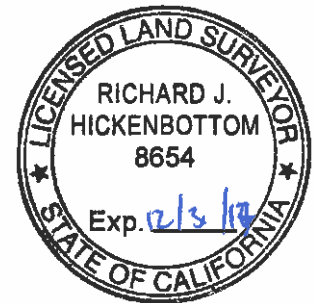


EXHIBIT 'B'
LEGAL DESCRIPTION

ADJUSTED PARCEL 2

REAL PROPERTY SITUATE IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEING A PORTION OF PARCEL 1, AS SAID PARCEL 1 IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 12-01, RECORDED JANUARY 11, 2013 AS INSTRUMENT NO. 2013-006489, AND ALL OF PARCEL A, AS SAID PARCEL A IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 13-01, RECORDED MAY 2, 2013 AS INSTRUMENT NO. 2013-066476, OFFICIAL RECORDS OF SAN MATEO COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE INTERSECTION OF THE NORTH LINE OF THE 100 FOOT WIDE SOUTHERN PACIFIC RAILROAD DUMBARTON CUT OFF RIGHT OF WAY WITH THE WESTERLY RIGHT OF WAY OF ROUTE 144, AS SAID ROUTE 144 IS SHOWN ON THAT CERTAIN CALTRANS RIGHT OF WAY MAP FOR ROUTE 84 IN THE COUNTY OF SAN MATEO ON SHEETS R-105.2 THROUGH R-105.4;

THENCE ALONG SAID NORTH LINE, SOUTH 86° 03' 10" WEST, 3091.29 FEET TO THE INTERSECTION OF SAID NORTH LINE WITH THE NORTHERLY RIGHT OF WAY LINE FOR CHILCO STREET, AS DESCRIBED AS PARCEL 45831-1 IN THAT CERTAIN DOCUMENT RECORDED JUNE 29, 1982 AS DOCUMENT NUMBER 82-054425, OFFICIAL RECORDS OF SAN MATEO COUNTY;

THENCE ALONG THE SAID NORTHERLY RIGHT OF WAY OF CHILCO STREET THE FOLLOWING THREE (3) COURSES:

- 1) ALONG THE ARC OF A NON-TANGENT CURVE TO THE LEFT, THE CENTER OF WHICH BEARS SOUTH 51° 25' 59" WEST, HAVING A RADIUS OF 231.73 FEET, THROUGH A CENTRAL ANGLE OF 53° 54' 34" FOR AN ARC LENGTH OF 218.04 FEET,
- 2) SOUTH 86° 03' 10" WEST, 1371.94 FEET, AND
- 3) SOUTH 23° 26' 23" WEST, 42.25 FEET TO A POINT ON THE EASTERLY RIGHT OF WAY OF CHILCO STREET, AS DESCRIBED AS PARCEL 46737-3 IN THAT CERTAIN DOCUMENT RECORDED JULY 29, 1983 AS DOCUMENT NUMBER 83-078012, OFFICIAL RECORDS OF SAN MATEO COUNTY;

THENCE ALONG SAID EASTERLY RIGHT OF WAY THE FOLLOWING NINE (9) COURSES:

- 1) ALONG THE ARC OF A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 01° 58' 59" WEST, HAVING A RADIUS OF 335.02 FEET, THROUGH A CENTRAL ANGLE OF 112° 10' 04" FOR AN ARC LENGTH OF 655.87 FEET,
- 2) NORTH 20° 11' 05" EAST, 388.11 FEET,
- 3) ALONG THE ARC OF A CURVE THE RIGHT, HAVING A RADIUS OF 20.00 FEET, THROUGH A CENTRAL ANGLE OF 93° 36' 25" FOR AN ARC LENGTH OF 32.68 FEET,
- 4) SOUTH 66° 12' 30" EAST, 5.00 FEET,
- 5) NORTH 23° 47' 30" EAST, 30.00 FEET,
- 6) SOUTH 66° 12' 30" EAST, 5.64 FEET,
- 7) NORTH 23° 47' 30" EAST, 30.00 FEET,
- 8) NORTH 66° 12' 30" WEST, 5.00 FEET, AND
- 9) ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 20.00 FEET, THROUGH A CENTRAL ANGLE OF 86° 23' 35" FOR AN ARC LENGTH OF 30.16 FEET;

THENCE SOUTH 64° 42' 36" EAST, 158.40 FEET;

THENCE SOUTH 53° 29' 03" EAST, 198.82 FEET;

THENCE NORTH 23° 50' 40" EAST, 393.73 FEET TO A POINT ON THE SOUTHERLY RIGHT OF WAY OF ROUTE 84, AS SAID ROUTE IS SHOWN ON SAID CALTRANS RIGHT OF WAY MAP;

THENCE ALONG SAID SOUTHERLY RIGHT OF WAY OF ROUTE 84 THE FOLLOWING TWO (2) COURSES:

1) SOUTH 80° 18' 45" EAST, 2,131.79 FEET, AND

2) SOUTH 89° 38' 32" EAST, 823.77 FEET TO THE NORTHWEST CORNER OF PARCEL B, AS SAID PARCEL IS DESCRIBED IN SAID LOT LINE ADJUSTMENT NO. 13-01;

THENCE ALONG THE BOUNDARY OF SAID PARCEL B THE FOLLOWING FIVE (5) COURSES:

1) SOUTH 30° 06' 28" EAST, 39.45 FEET,

2) SOUTH 0° 21' 28" WEST, 40.00 FEET,

3) SOUTH 89° 38' 32" EAST, 60.00 FEET,

4) NORTH 0° 21' 28" EAST, 40.00 FEET, AND

5) NORTH 30° 49' 24" EAST, 39.45 FEET TO A POINT ON SAID SOUTHERLY RIGHT OF WAY OF ROUTE 84;

THENCE ALONG SAID SOUTHERLY RIGHT OF WAY OF ROUTE 84 THE FOLLOWING NINE (9) COURSES:

1) SOUTH 89° 38' 32" EAST, 79.55 FEET,

2) SOUTH 30° 06' 28" EAST, 39.45 FEET,

3) SOUTH 89° 38' 32" EAST, 60.00 FEET,

4) NORTH 30° 49' 24" EAST, 39.45 FEET,

5) SOUTH 89° 38' 32" EAST, 1012.82 FEET,

6) SOUTH 81° 24' 30" EAST, 162.33 FEET,

7) ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 45.00 FEET, THROUGH A CENTRAL ANGLE OF 81° 13' 05" FOR AN ARC LENGTH OF 63.79 FEET,


8) SOUTH 0° 11' 25" EAST, 172.13 FEET, AND

9) SOUTH 15° 03' 57" WEST, 107.79 FEET TO SAID WESTERLY RIGHT OF WAY OF ROUTE 114;

THENCE ALONG SAID WESTERLY RIGHT WAY, SOUTH 23° 08' 15" WEST, 111.33 FEET TO THE **POINT OF BEGINNING.**

CONTAINING 77.83 ACRES OF LAND, MORE OR LESS.

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.


RICHARD J. HICKENBOTTOM, LS 8654
LICENSE EXPIRES: 12/31/17

11/9/16
DATE



FACEBOOK WEST CAMPUS EXPANSION TRIP CAP MONITORING AND ENFORCEMENT POLICY¹

This policy applies to Building 20, Building 23, and the Facebook Campus Expansion Project (the Facebook Campus Expansion Project is referred to as the “Project”) for which entitlements are currently being sought. This policy also applies to any existing buildings (e.g. Building 305) that will remain during any interim phase of the Project. For purposes of this policy, the term “West Campus” is intended to include both the existing buildings on-site as well as Buildings 21, 22 and the hotel that are proposed as part of the Project. A separate trip cap monitoring and enforcement policy exists for Buildings 10-19 (also known as the East Campus or Classic Campus).

DEFINITIONS

Trip – A single vehicle (car, truck, van, shuttle, etc.) arriving at a location in Menlo Park, whose occupant(s)’ final destination is the West Campus, or a single vehicle departing from a location in Menlo Park, whose occupant(s)’ origin is the West Campus. Therefore, for example, a roundtrip by a single vehicle arriving at a location in Menlo Park and departing from a location in Menlo Park whose occupant(s)’ destination and origin is the West Campus equals two trips. A vehicle transiting from the East Campus to the West Campus or from the West Campus to the East Campus is a trip. A single shuttle coming from outside Menlo Park or from the Menlo Park CalTrain station that makes stops at multiple campuses shall only count as one trip against the Daily Trip Cap; however, campus entries and exits associated with such intra-campus shuttle trips occurring during the peak periods shall count fully against the Peak Hour Trip Caps. Shuttles and trams utilizing the undercrossing between the East and West Campus shall not count against either the Daily Trip Cap or the Peak Hour Trip Caps. Trips also do not include bicycles (or other self-powered modes of travel).

Peak Hour Trip Cap – The maximum number of trips allowed in each hour of the AM Peak Period or the PM Peak Period.

Peak Period – Roadway morning and evening commuter peak travel times:

- AM Peak Period - 7:00 AM to 9:00 AM
- PM Peak Period - 4:00 PM to 6:00 PM

Daily Trip Cap – The maximum number of trips per day.

Trip Cap – Generally refers to the AM Peak Hour Trip Caps, the PM Peak Hour Trip Caps and the Daily Trip Cap.

¹ This Trip Cap Monitoring and Enforcement Policy was prepared by the City of Menlo Park in consultation with Facebook.

TRIP CAP

Facebook must comply with the Trip Cap and may not exceed the Trip Cap without an application for and approval of a change to the Amended and Restated Conditional Development Permit (CDP) for the West Campus. If the Trip Cap is exceeded without the appropriate approval, Facebook is in violation of the CDP. The Trip Cap also includes Mitigation Measure TRA-1.2 as identified in the EIR for the Project and associated Mitigation, Monitoring and Reporting Program.

The Trip Cap proposed as part of Facebook's West Campus is as follows:

- AM Peak Period Trip Caps:
 - 2,250 trips are permitted between 7:00 a.m. and 8:00 a.m.
 - 2,250 trips are permitted between 8:00 a.m. and 9:00 a.m.
- PM Peak Period Trip Caps:
 - 2,255 trips are permitted between 4:00 p.m. and 5:00 p.m.
 - 2,255 trips are permitted between 5:00 p.m. and 6:00 p.m.
- Daily Trip Cap: 26,440 trips

MONITORING

To monitor compliance with the Trip Cap, traffic counts shall be taken at the West Campus. The monitoring shall be done through automated means (e.g., imbedded loop detectors in the pavement in each travel lane or video detection) approved by the City.² All vehicular entrances to the West Campus shall be included in the monitoring. Facebook shall be solely responsible for paying all costs related to monitoring, including, but not limited to, development, installation, maintenance and repair of all monitoring equipment.

The City reserves the option to require Facebook to monitor neighborhood parking intrusion in the Belle Haven neighborhood, parking on other public streets in the City, or parking at any off-site parking lot(s) in Menlo Park (other than the East Campus or any other property or properties leased or owned by and occupied by Facebook) if it is observed or suspected that vehicles whose occupant(s)' final destination is the West Campus are parking at any of these locations. If the City requires monitoring of these off-site locations and, after investigation, it is confirmed that vehicle occupant(s) whose final destination is the West Campus are parking vehicles at these off-site locations (other than the East Campus or any other property or properties leased or owned and occupied by Facebook), the trips to these locations will be counted toward the Trip Cap.

Monitoring program details are as follows:

² City approvals related to monitoring equipment will be through the Director of Public Works or his/her designee.

- **Monitoring Days/Times** – Each hour within the AM Peak Period, each hour within the PM Peak Period and total daily trips will be monitored on all non-holiday weekdays. Holidays are those days identified as State holidays in California Government Code Section 6700.
- **Exclusions – Two types of exclusions from the Trip Cap shall be permissible as discussed below:**
 - Special Events: To account for special events and their effect on trips, Facebook may have up to 12 special event exclusions per year or 12 days on which one or more of the AM Peak Hour Trip Caps, PM Peak Hour Trip Caps or Daily Trip Cap are exceeded, but are not considered violations of the Trip Cap. These special events do not represent typical operating conditions at the West Campus. A special event will be defined as an activity that is not typical of the normal operations of the West Campus and will likely involve more than Facebook employees. If the Trip Cap has been violated as a result of a special event, Facebook shall provide documentation to the City that a special event took place. Upon City review and approval, in the City’s sole and reasonable discretion, an exclusion for a special event shall apply.
 - Non-event exclusions: For non-special events, Facebook will be allowed three days on which one or more of the AM Peak Hour Trip Caps, PM Peak Hour Trip Caps or Daily Trip Cap are exceeded within a 180 day period without incurring penalties. These non-event exclusion days are intended to allow Facebook time to correct the Trip Cap violation. If Facebook exceeds the Trip Cap on more than three days within a 180 day period, then the non-event exclusion is exhausted and penalties will be imposed for violations of the Trip Cap until compliance is reached for a consecutive 180 day period. Additional violations, if any, within the 180 day compliance period, will re-set the 180 day compliance period. If after a consecutive 180 day period, Facebook remains in full compliance with the Trip Cap, then the three day exclusion will become available again.
- **Count Equipment** – Automated count equipment will be designed and constructed at Facebook’s sole expense to collect data on the number of trips at each of the West Campus driveways and send the data back to the City offices. The type of count equipment (initial and any future changes) shall be approved by the City, in consultation with Facebook and considering the latest technologies for detection, counting and reporting. The City shall not unreasonably withhold approval of initial count equipment or any future equipment which achieves the result envisioned in this document. The City shall also approve the count equipment that will be used to monitor off-site locations, if the City exercises the option to require such monitoring. The City shall not unreasonably withhold approval of such additional count equipment.
- **Initial Calibration Process** – Once any new count equipment has been established, a calibration process will be undertaken to determine the reliability and accuracy of the count equipment. Depending on the type of equipment, the count accuracy can be affected by a number of environmental factors which will need to be confirmed. This

calibration process would be conducted prior to final building permit sign-off for occupancy of the Building 21.

- **Determination of Reliability (Sensitivity) Factor** – Based on the calibration analysis, the City and Facebook will agree to a reliability factor for the count stations which will be used to evaluate the count results consistent with what the City and Facebook have historically agreed upon. The reliability factor would represent the margin of error inherent in the vehicle counting equipment, and would address the exclusion of trips whose final destination is not the West Campus (i.e. wrong turns, uninvited guests, etc). The reliability factor would also account for single shuttles coming from outside Menlo Park or from the Menlo Park CalTrain station and making stops at multiple campuses. Periodically, the reliability factor, based on reporting from Facebook, may be modified to address the anticipated or actual number of shuttles coming from outside Menlo Park or from the Menlo Park CalTrain station making stops as part of one trip at multiple campuses outside of the peak period. At a minimum, Facebook shall provide an annual report to the Transportation Manager for each upcoming year that provides data on the proposed number of shuttle trips so that the City may analyze whether the reliability factor is accurately accounting for single shuttles coming from outside Menlo Park or from the Menlo Park CalTrain station and making stops at multiple campuses.
- **Periodic Count Equipment Testing/Recalibration** – The vehicle detection system will be periodically tested to ensure the accuracy of the monitoring counts. During the first two years of operation, testing will be conducted at six month intervals. If these tests show that the system is operating reliably, then testing can be reduced to once a year. If the equipment is thought to be out of calibration, Facebook will work with the City to test and calibrate the equipment if necessary. The City will have final approval, which approval shall be granted or withheld in a reasonable manner, on all testing and calibration.
- **Installation and Repairs** – New count equipment shall be installed and in good working order prior to final building permit sign-off for occupancy of Building 21. The City shall have final approval, which approval shall be granted or withheld in a reasonable manner, of the contractor completing the installation and the maintenance contractor completing any repairs. Non-emergency repairs and maintenance of the monitoring equipment shall occur only on evenings and weekends, unless otherwise approved by the City. The Transportation Division shall be notified at least 48 hours in advance of any non-emergency repairs or maintenance work. The City Transportation Division shall be notified within 24 hours of any emergency repairs. City inspection and approval of any repairs or maintenance is required. Failure to keep monitoring equipment operational in good working order will be considered a violation of the Trip Cap after two working days, unless the repairs/maintenance require additional time as approved by the City and Facebook is diligently pursuing such repairs/maintenance. The Trip Cap penalty will not be enforced during the repair/maintenance of the monitoring equipment. If the City, in its sole and reasonable discretion, determines that Facebook is not diligently pursuing the repairs/maintenance, the City may elect to perform the repairs/maintenance and charge the cost of the repair/maintenance, staff time, and 15 percent penalty fee to Facebook.

- **Access to Count Equipment/Reporting** – The City shall have the ability to access the count equipment at any time after reasonable prior notice to Facebook. Facebook will not have access to the count equipment, unless approved by the City or in case of the need for emergency repairs. The City shall not unreasonably withhold approval of access for repair/maintenance contractors. Facebook shall have “read-only” access to the reporting data, but shall have the ability to record such data and run history reports in order to track trends. Reporting data shall be provided to Facebook and the City in real time. Real time data will provide Facebook the opportunity to take immediate action, if necessary, to avoid violating the Trip Cap.

ENFORCEMENT

Facebook shall be responsible not only for monitoring, but also for achieving compliance with the Trip Cap, which includes, by definition, all trip cap measurements on a daily basis (the AM Peak Hour Trip Caps, the PM Peak Hour Trip Caps and the Daily Trip Cap). The City shall enforce compliance with the Trip Cap.

If, on a given day, the results of the monitoring indicate that the number of trips is at or below the Trip Cap, considering the reliability factor, then Facebook is considered in compliance. If, however, the monitoring, considering the reliability factor, reveals that any of the AM Peak Hour Trip Caps or the PM Peak Hour Trip Caps or the Daily Trip Cap has been exceeded (after accounting for any permitted exclusions), Facebook is in violation of its CDP and the City may take steps to enforce the Trip Cap.

The specifics for enforcement are as follows:

- **Threshold** – If there are any AM Peak Hour Trip Cap, PM Peak Hour Trip Cap or Daily Trip Cap violations that do not qualify for an exclusion as discussed above, then penalties will be imposed.
- **Penalties** – Monetary penalties will be imposed for violations of the Trip Cap in excess of the threshold. Penalties are calculated on a per trip basis and progressively increasing penalties will be imposed for subsequent violation(s) of the Trip Cap based on a tiered system described in the table below. Penalties will be applied for each violation including the AM Peak Hour, PM Peak Hour and the Daily Period. If any of the AM Peak Hour Trip Caps, and/or PM Peak Hour Trip Caps and Daily Trip Cap are exceeded on the same day, the penalty paid shall be the greater of the sum of the penalties for the AM Peak Hour and PM Peak Hour *or* the Daily penalty. The penalty payment schedule is shown in the table below. The base penalties shall be adjusted annually as set forth below (the intent is for the same penalty rate to apply to both the East and West Campuses):

Penalty Tier ¹	Applicability	Penalty Amount
Tier 1	Tier 1 is the default tier and applies for the month unless one of the other tiers is applicable.	\$55.57 per trip per day
Tier 2	Tier 2 applies for the month if either (a) penalties were imposed in both of the 2 months immediately preceding that month or (b) penalties were imposed in any 4 of the 6 months immediately preceding that month. Tier 2 will not apply if Tier 3 applies.	\$111.13 per trip per day
Tier 3	Tier 3 applies for the month if penalties were imposed in each of the 6 months immediately preceding that month.	\$222.27 per trip per day

- 1 Only one tier is applicable for any given violation. In addition, the penalty amounts are shown in 2016 dollars based on the original 2012 penalty amounts that applied to the original project approvals for Building 20 adjusted by CPI.

An example table showing the penalty amounts:

Penalty Cost Per Day			
Vehicles over Trip cap	Tier 1	Tier 2	Tier 3
100	\$5,557	\$11,113	\$22,227
500	\$27,784	\$55,567	\$111,134
1000	\$55,567	\$111,134	\$222,269
2000	\$111,134	\$222,269	\$444,537

Example calculations :

Daily penalty greater:

7:00-8:00 AM Peak Hour exceeds the AM Peak Hour Trip Cap by 100 trips

5:00-6:00 PM Peak Hour exceeds the PM Peak Hour Trip Cap by 50 trips

Daily trips exceed the Daily Trip Cap by 400 trips

The payment would be:

AM Peak Hour penalty = 100 trips x \$55.57/trip = \$5,557

PM Peak Hour penalty = 50 trips x \$55.57/trip = \$2,778.50

Total Peak Period penalty = \$8,335.50

Daily penalty = 400 trips x \$55.57/trip = \$22,228

Penalty Paid = \$22,228

AM Peak Hour and PM Peak Hour penalty greater:

7:00-8:00 AM Peak Hour exceeds the AM Peak Hour Trip Cap by 100 trips

5:00-6:00 PM Peak Hour exceeds the PM Peak Hour Trip Cap by 50 trips

Daily trips exceed the Daily Trip Cap by 100 trips

The payment would be:

AM Peak Hour penalty = 100 trips x \$55.57/trip = \$5,557

PM Peak Hour penalty = 50 trips x \$55.57/trip = \$2,778.50

Total Peak Period penalty = \$8,335.50

Daily penalty = 100 trips x \$55.57/trip = \$5,557

Penalty Paid = \$8,335.50

The base penalties are stated in 2016 dollars (based on the original 2012 penalty amounts that applied to the approval of Building 20, as adjusted by CPI) and shall be adjusted annually per the Consumer Price Index for All Urban Consumers All Items in the San Francisco-Oakland-San Jose Metropolitan Area [1982-84=100] (the intent is for the same penalty rate to apply to both the East and West Campuses). Penalties are due and payable to the City within 30 days of the issuance of an invoice, which the City shall issue on a monthly basis. The City shall use the penalties collected for programs or projects designed to reduce trips or traffic congestion within Menlo Park and the City shall share 25 percent of the penalties collected with the City of East Palo Alto for use on transportation systems and solutions that help reduce traffic in the City of East Palo Alto around the East and West Campuses. In addition to monetary penalties, failure to comply with the Trip Cap is considered a violation of the CDP and could result in revocation of the CDP.

Violations of the Trip Cap for the East Campus are independent of violations of the West Campus Trip Cap. This means, for instance, that if there are violations of the Trip Cap at the East Campus for the six months immediately preceding a particular month, but there are no violations of the Trip Cap at the West Campus during that same period, Tier 3 would be applicable to the East Campus and Tier 1 would be applicable to the West Campus.

- **Interim Measure** – If Facebook determines that it needs to secure parking in another location as an interim measure to maintain compliance with the Trip Cap, Facebook may, through the City’s entitlement process, obtain approval for the use of another private property in Menlo Park (not the East or West Campus) that includes both a building and associated parking. Trips to such an off-site location will not count toward the Trip Cap only if there will be no more trips to that off-site location than is allowed under the then current use of that property.
- **Compliance** – If after non-compliance, Facebook comes back into compliance with the Trip Cap and maintains compliance for 180 consecutive days, the scale of penalties will revert to the base level and the relevant threshold would once again apply before there is non-conformance and the onset of penalties.

**Facebook Campus Expansion Project
Mitigation Monitoring and Reporting Program**

Exhibit D

The Mitigation Monitoring and Reporting Program (MMRP) of the certified Environmental Impact Report (EIR) for the Approved Project will be included as Exhibit D of the recorded Second Amended and Restated Conditional Development Permit (CDP). The EIR Addendum included two updated mitigation measures to ensure consistency with the certified EIR, which have been included in Exhibit E to the Amended and Restated CDP. There were no changes to the MMRP and therefore it has not been attached to this Draft Second Amended and Restated CDP. The approved MMRP is available for reference at the following link:

<http://menlopark.org/DocumentCenter/View/15757>

Facebook Campus Expansion Project
Proposed Revised Project (Building 22 CDP Amendment)
Updated Mitigation Measures

The environmental review for the Proposed Revised Project analyzed the proposed modifications to the Approved Project for consistency with the certified EIR for the Approved Project. The analysis determined that the Proposed Revised Project would not result in new impacts or an increase in the severity of previously identified impacts. Therefore, an Addendum was prepared and published by the City of Menlo Park on September 28, 2017. The Addendum included updated mitigation measures to ensure consistency with the certified EIR. The applicable revised mitigation measures are presented below, with additions show with an underline and deletions with a ~~strikeout~~.

Section 3 (Air Quality)

AQ-2.2: Offset NO_x Emissions Generated during Project Construction and Operation that are above the BAAQMD NO_x Average Daily Emission Threshold. The Project Sponsor shall, prior to occupancy of the first building within the Project, enter into an agreement with the City to develop an alternative or complementary offsite mitigation program to offset construction and operational NO_x emissions to the level established by the BAAQMD thresholds for the years in which the Proposed Revised Project's emissions exceed the BAAQMD threshold. The offsite mitigation program shall require the Project Sponsor to provide a one-time payment to the City to establish a program to fund emissions reduction projects through grants and similar mechanisms within the City of Menlo Park. The amount of such payment shall be based on the then-current BAAQMD Carl Moyer Program cost-effectiveness limit multiplied by emissions that exceed BAAQMD's average daily threshold for each year that emissions exceed the threshold plus a five percent administrative fee to fund procurement of offsite emissions reductions for the Proposed Revised Project's projected construction and operational emissions.

Potential projects shall be limited to those that will reduce emissions each year in which the project's emissions exceed the BAAQMD threshold through the end of ~~2025, which is when the Project's operational~~ the last year that emissions are projected to be below ~~exceed~~ the average daily thresholds, including, but not limited to, the following:

- Alternative-fuel, low-emissions school buses, transit buses, and other vehicles.
- Diesel engine retrofits and repowers.
- Bike-sharing programs.
- Electric vehicle charging stations and plug-ins.

All offsite reductions must be quantifiable, verifiable, and enforceable. The Project Sponsor shall engage a qualified air quality expert to coordinate with the City to identify a list of potential projects that would be eligible for funding. Emissions reduction projects shall be funded so that the Project's emissions are reduced each year until ~~the end of 2025~~ no threshold exceedances occur. The air quality expert retained by the Project Sponsor shall provide a report within one year of occupancy of the first building within the Project site, identifying the projects that were funded and associated NO_x emissions expected to be realized for each year out until ~~the end of 2025~~ no

threshold exceedances occur. Annual reporting of the implementation of emissions reduction projects shall be required until the Proposed Revised Project's emissions are less than the BAAQMD threshold without the offsets.

If a sufficient number of emissions reduction projects are not identified to meet the required performance standards in the city of Menlo Park, the Project Sponsor shall consult with a qualified air quality expert to ensure conformity is met through some other means of achieving the performance standard of net zero operational emissions in excess of BAAQMD's average daily thresholds ~~through 2025~~, including, but not limited to, payment of a one-time mitigation offset fee to BAAQMD's Strategic Incentives Division plus a 5 percent administrative fee to fund one or more emissions reduction projects within the San Francisco Bay Area Air Basin. Reporting for any emissions reduction projects outside the city shall be completed on the same schedule as indicated above for emissions reduction projects in the city.

If annual reports indicate that emissions reductions do not adequately reduce project emissions to a level below the BAAQMD threshold for any year, then a penalty of 200 percent shall be imposed that will require the Project Sponsor to obtain an additional year of offsets based on the amount of emissions by which the Proposed Revised Project's emissions exceed the BAAQMD threshold for the following year (e.g., if the 2019 emissions exceed the threshold by 5 tons, then 10 tons of emissions must be provided by 2020).

Section 3.8 (Biological Resources)

BIO-3.1: Conduct Pre-construction Surveys for Nesting Migratory Birds. The Project Sponsor shall implement the following measures to reduce impacts on nesting migratory birds:

- To facilitate compliance with state and federal law (California Fish and Game Code and the MBTA) and prevent impacts on nesting birds, the Project Sponsor shall avoid construction during the nesting season (February 1 through August 31) or conduct pre-construction surveys, as described below.
- If it is not feasible to avoid the nesting season, the Project Sponsor shall hire a qualified wildlife biologist with demonstrated experience to conduct a survey for nesting birds, including raptors, no earlier than 3 days prior to the commencement of ground-disturbing activities and vegetation removal (including clearing, grubbing, and staging). The area surveyed shall include all construction areas as well as areas within 250 feet outside the boundaries of the areas to be cleared or as otherwise determined by the biologist. A nest survey shall be required prior to implementation of Phase 1, ~~and~~ Phase 2, and Phase 3 of the Project and when construction work stops at a portion of the site where suitable nesting habitat remains for more than 15 days. Additionally, at least one nest survey shall be conducted at the beginning of each year of Project implementation between February and May. As discussed in Chapter 2, *Project Description*, Project implementation will occur between 2016 and ~~2022~~ 2024. The need for additional surveys shall be determined by the qualified wildlife biologist and based on the results of the initial survey.
- If the biologist finds active nests during the survey, he or she shall establish species-specific no-disturbance buffer zones for each nest with use of high-visibility fencing, flagging, or pin flags. No construction activities shall be allowed within the buffer zones. The size of the buffer shall be based on the species sensitivity to disturbance and planned

work activities in the vicinity; typical buffer sizes are 250 feet for raptors and 50 feet for other birds. The buffer shall remain in effect until the nest is no longer active.

- If structure demolition activities cannot occur outside of the nesting season, the Project Sponsor or its contractor shall remove inactive nests from the structure to be demolished and install nest exclusion measures (i.e., fine mesh netting, panels, or metal projectors) outside of the nesting season. All exclusionary devices shall be monitored and maintained throughout the breeding season to ensure that they are successful in preventing the birds from accessing cavities or nest sites. No more than 3 days prior to building demolition activities, a qualified biologist shall conduct a pre-construction survey of all potential nesting habitat on the structure to be demolished and the surrounding areas for the presence of active nests. If active nests are found on the building or in the affected area, then demolition activities shall not proceed until the biologist verifies that all nests on the building are inactive.
- After all surveys and/or nest deterrence activities are completed, the biologist shall complete a memorandum detailing the survey effort and results and submit the memorandum to the City within 7 days of survey completion.

This document is recorded for the benefit of the City of Menlo Park and is entitled to be recorded free of charge in accordance with Sections 6103 and 27383 of the Government Code.

RECORDING REQUESTED BY
AND WHEN RECORDED MAIL TO:

City of Menlo Park
Attn: City Clerk
701 Laurel Street
Menlo Park, CA 94025

DRAFT AMENDMENT TO DEVELOPMENT AGREEMENT
(301-309 CONSTITUTION DRIVE, MENLO PARK, CA
[APNs 055-260-250 and 055-260-290])

BY AND BETWEEN

CITY OF MENLO PARK,
A CALIFORNIA MUNICIPAL CORPORATION

AND

HIBISCUS PROPERTIES, LLC,
A DELAWARE LIMITED LIABILITY COMPANY

SEPARATE PAGE, PURSUANT TO GOVT. CODE 27361.6

THIS AMENDMENT TO DEVELOPMENT AGREEMENT (“Amendment”) is made and entered into as of this ___ day of _____, 2017, by and between the City of Menlo Park, a municipal corporation of the State of California (“City”) and Hibiscus Properties LLC, a Delaware limited liability company (“Facebook”). This Amendment modifies the Development Agreement dated December 14, 2016 and recorded in the Official Records of the County of San Mateo as Document Number 2016-133794 (“Development Agreement”).

RECITALS

The City and Facebook are entering this Amendment based on the following facts, understandings and intentions:

A. Facebook owns those certain parcels of real property collectively and commonly known as 301 thru 309 Constitution Drive in the City of Menlo Park, California (“Property”), as shown on Exhibit A attached hereto and being more particularly described in Exhibit B attached hereto.

B. The City examined the environmental effects of the Project (as defined in the Development Agreement), in an Environmental Impact Report (“EIR”) prepared pursuant to the California Environmental Quality Act (“CEQA”). The Project included the demolition of existing buildings on the Property (Buildings 301-309 and the Chemical Transfer Facility) and the subsequent construction of two new office buildings (Buildings 21 and 22) and a Hotel. On November 1, 2016, the City Council reviewed and certified the EIR.

C. On November 1, 2016, the City Council approved the Project and the Development Agreement by introducing Ordinance No. 1021 (“Enacting Ordinance”). The City Council conducted a second reading on the Enacting Ordinance on November 15, 2016 and adopted the Enacting Ordinance, making it effective on December 16, 2016. The Development Agreement was recorded on December 16, 2016, in the Official Records of the County of San Mateo.

D. On February 7, 2017, Facebook applied to modify the Project. Project modifications include, but are not limited to, changing the design of Building 22 to encompass a four-story building with a reduced building footprint, shifting the surface parking beneath Building 22 into a stand-alone eight-story parking garage (seven stories above grade and one story located below finished grade), increasing the height of Building 22 to accommodate architectural skylights, modifying the open space and adding an electrical vehicle charging facility for future electric buses and trams (“Revised Project”). The Revised Project also includes an interim phase during which existing Building 305 would not be demolished, but could remain on the Property and occupied by TE Connectivity (“TE”) while Building 22 is constructed and operational.

E. When the Development Agreement was negotiated, the demolition of Building 305 was necessary for the construction of Building 22. As described in the certified EIR, the City and Facebook originally anticipated that the construction of Building 22 would start in early 2018 and that TE would have vacated Building 305 prior to that date. Because the Revised Project would accommodate a different phasing schedule than originally anticipated that would allow the construction of Building 22 to commence prior to demolition of Building 305, the Parties desire to enter this Amendment to ensure that both the City and Facebook retain the rights and privileges identified in the Development Agreement despite the modifications presented by the Revised Project. This Amendment also addresses an additional funding contribution by Facebook to the City’s General Fund which is intended to fund services that benefit the safety of the local community.

F. In compliance with CEQA, pursuant to 14 California Code of Regulations Section 15164, the City prepared an addendum to the previously certified EIR finding that the Revised Project did not involve any new significant environmental impacts or any substantial increase in the severity of any previously identified significant impact.

G. On October 16, 2017, the Planning Commission held a duly noticed public hearing to review the Revised Project, including the addendum to the certified EIR and the Second Amended and Restated Conditional Development Permit, which addressed the proposed modifications requested by Facebook, and recommended that the City Council approve the Revised Project and this Amendment to preserve the rights and privileges as originally negotiated in the Development Agreement.

H. On November ____, 2017, the City Council held a duly noticed public hearing at which it reviewed the Revised Project, including in the addendum to the certified EIR and the Second Amended and Restated Conditional Development Permit, and voted to approve the Revised Project and enter into this Amendment to preserve the rights and privileges as originally negotiated in the Development Agreement.

NOW, THEREFORE, in consideration of the mutual covenants and promises of the City and Facebook herein contained, the City and Facebook agree as follows:

1. The Revised Project includes revised site plans, updated mitigation measures identified in the addendum to the certified EIR and a Second Amended and Restated Conditional Development Permit, which supersedes the Amended and Restated Conditional Development Permit. All references in the Development Agreement to defined terms that are affected by these modifications are updated to include the Revised Project, as approved by the City Council.

2. Section 6.3.1 of the Development Agreement is hereby amended to change the definition of the Guarantee Commencement Date to preserve the Development Agreement's anticipated timing for the commencement of the TOT Guarantee Payments identified in Section 6.3 and the date upon which the Utility User's Tax Cap would no longer apply to the Property as described in Section 6.5 of the Development Agreement by deleting the existing language in Section 6.3.1 and replacing it with the following (new language underscored):

“Facebook’s obligation to make TOT Guarantee Payments, if any, shall commence upon July 1 of the second full City fiscal year following the earlier of (i) the TE Vacation Date, or (ii) the date the City issues the first building permit for Building 22 (“Guarantee Commencement Date”). The TOT Guarantee Payments, if any, shall be calculated with respect to each City fiscal year (July 1 through June 30) during the Guarantee Payment Period (“Revenue Calculation Period”), the first such year commencing as of the Guarantee Commencement Date. Facebook’s obligation to make TOT Guarantee Payments shall apply to the period (“Guarantee Payment Period”) commencing on the Guarantee Commencement Date and continuing until thirty-nine (39) years after the Guarantee Commencement Date.

3. Section 9.1.5 of the Development Agreement provides that Facebook shall construct, operate, and maintain a new two-acre publicly accessible open space upon issuance of building permits for Building 22. With the Revised Project, construction of the new two-acre publicly accessible open space would be phased over time, since a portion of the full two-acre area is currently occupied by Building 305 and full buildout of the public open space could not occur until Building 305 is

demolished. The specific timing and sequencing of the public open space is addressed in the Second Amended and Restated Conditional Development Permit. Therefore, Section 9.1.5 of the Development Agreement is modified to allow completion of the public open space upon demolition of Building 305 in accordance with the Second Amended and Restated Conditional Development Permit.

4. In addition to the local community benefits to be provided by Facebook pursuant to Section 9 of the Development Agreement, Facebook shall contribute a total of Nine Million Dollars (\$9,000,000) to the City's general fund to be spent on services that benefit the safety of local community but otherwise in the sole discretion of the City (the "City Services Contribution"). Payment of the City Services Contribution shall occur in five equal installments over a five-year period (i.e., \$1,800,000 per year for five years). The annual payments shall be payable on July 1 of the City's fiscal year, and the first payment will be payable on July 1, 2018. The amount of the payment each fiscal year shall be adjusted based upon the Consumer Price Index for All Urban Consumers for San Francisco-Oakland-San Jose as measured from February to February (<https://data.bls.gov/cgi-bin/surveymost>). The City agrees to provide an annual update to Facebook regarding the City's use of the City Services Contribution as part of the annual review process required by Section 12.1 of the Development Agreement. The City Services Contribution shall not be payable if Facebook elects to terminate this Amendment pursuant to Section 6 of this Amendment.

5. As required by the Development Agreement, this Amendment shall be recorded by the City Clerk not later than 10 days after the City Council approval of the Amendment.

6. If litigation or a referendum is commenced seeking to set aside the Revised Project, the Second Amended and Restated Conditional Development Permit or this Amendment, Facebook may elect to terminate this Amendment and the Second Amended Restated Conditional Development Permit and proceed with the original Project. In the event of a termination pursuant to Section 6 of this Amendment, the Amended and Restated Conditional Development Permit and Development Agreement shall survive and control the rights and obligations of the parties and the permitted uses on the Property.

7. Except to the extent expressly modified by this Amendment, the terms of the Development Agreement shall remain effective without impairment or modification.

8. This Amendment may be executed in any number of counterparts, each of which so executed shall be deemed an original, but all of which when taken together shall constitute but one Amendment.

IN WITNESS WHEREOF, the Parties have executed this Agreement as of the day and year first above written.

**** signatures on next page****

“City”

CITY OF MENLO PARK, a municipal corporation of the State of California

By: _____
Mayor

Attest:

City Clerk

Approved as to Form:

By: _____
City Attorney

“Facebook”

HIBISCUS PROPERTIES, LLC, a Delaware limited liability company:

By: _____

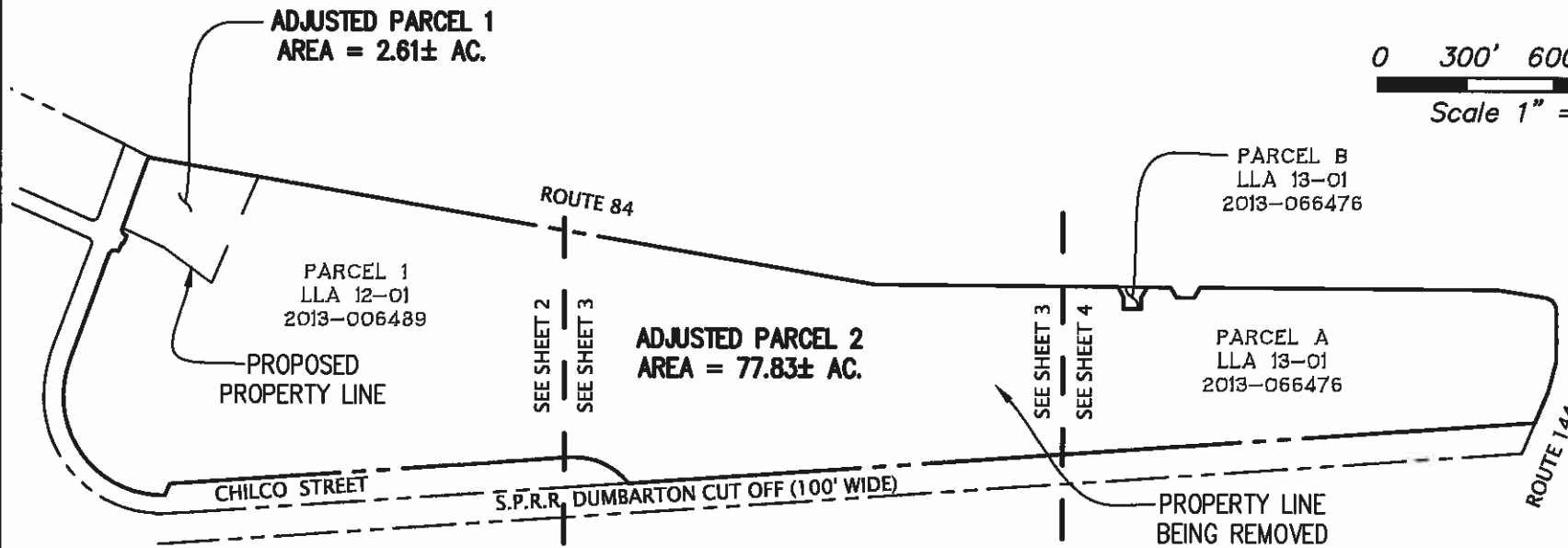
Name: _____

Title: _____

EXHIBIT A



0 300' 600' 1200'
Scale 1" = 600 ft



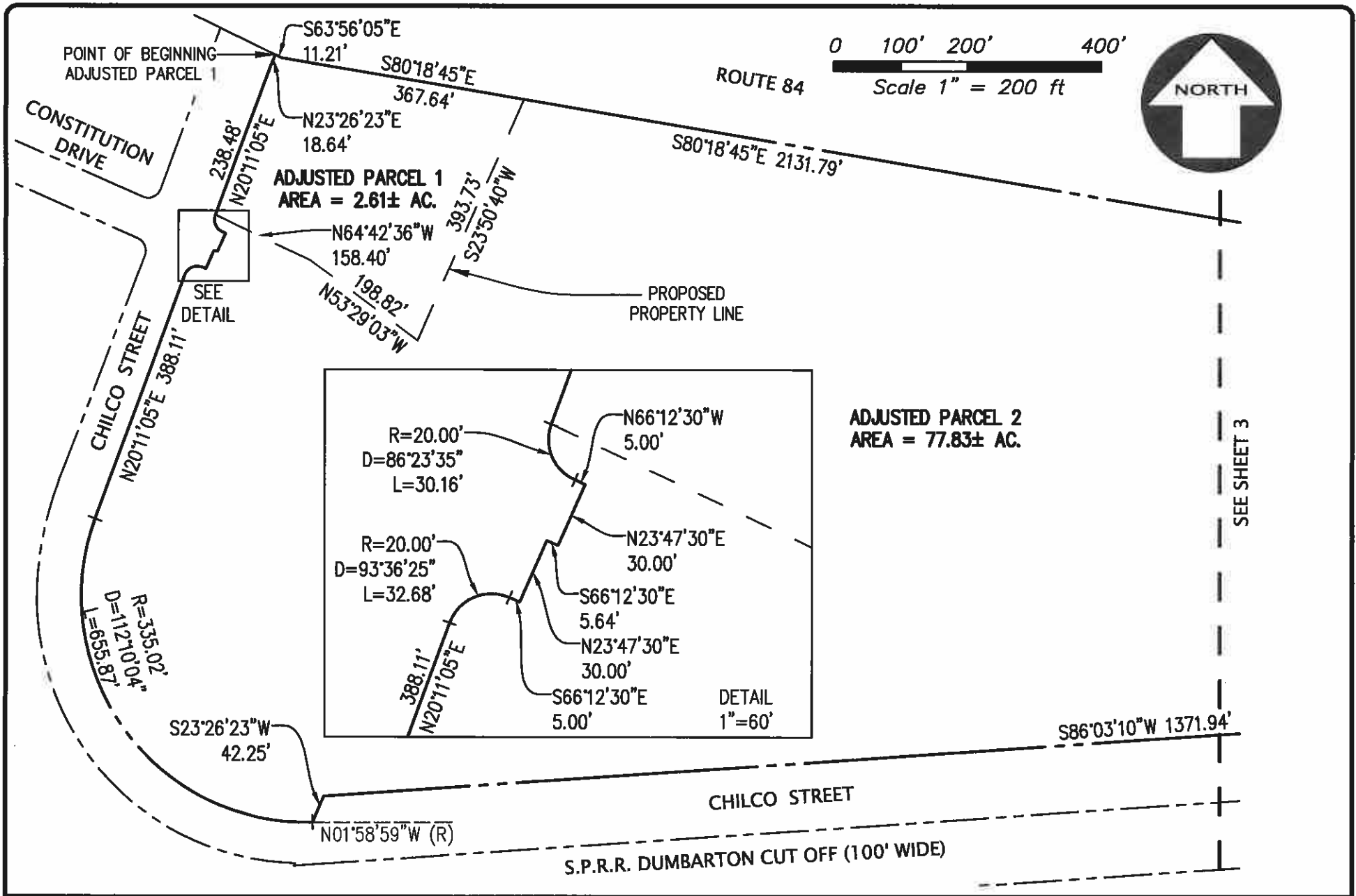
KIER & WRIGHT
CIVIL ENGINEERS & SURVEYORS, INC.
 2850 Collier Canyon Road Phone (925) 245-8788
 Livermore, California 94551 Fax (925) 245-8796

EXHIBIT "A"
PLAT TO ACCOMPANY LEGAL DESCRIPTION

MENLO PARK,

CALIFORNIA

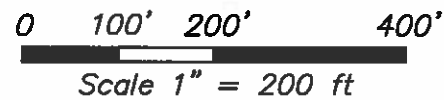
DATE	NOVEMBER, 2016
SCALE	1" = 600'
BY	RJH
JOB NO.	A15571-3
SHEET	1 OF 4



KIER & WRIGHT
CIVIL ENGINEERS & SURVEYORS, INC.
 2850 Collier Canyon Road Phone (925) 245-8788
 Livermore, California 94551 Fax (925) 245-8796

EXHIBIT "A"
PLAT TO ACCOMPANY LEGAL DESCRIPTION
MENLO PARK, CALIFORNIA

DATE	NOVEMBER, 2016
SCALE	1" = 200'
BY	RJH
JOB NO.	A15571-3
SHEET	2 OF 4



ROUTE 84

S80°18'45"E 2131.79'

N89°38'32"W 823.77'

SEE SHEET 2

ADJUSTED PARCEL 2
AREA = 77.83± AC.

R=231.73'
D=53°54'34"
L=218.04'

S86°03'10"W 1371.94'

S51°25'59"W (R)

S86°03'10"W 3091.29'

S.P.R.R. DUMBARTON CUT OFF (100' WIDE)

SEE SHEET 4



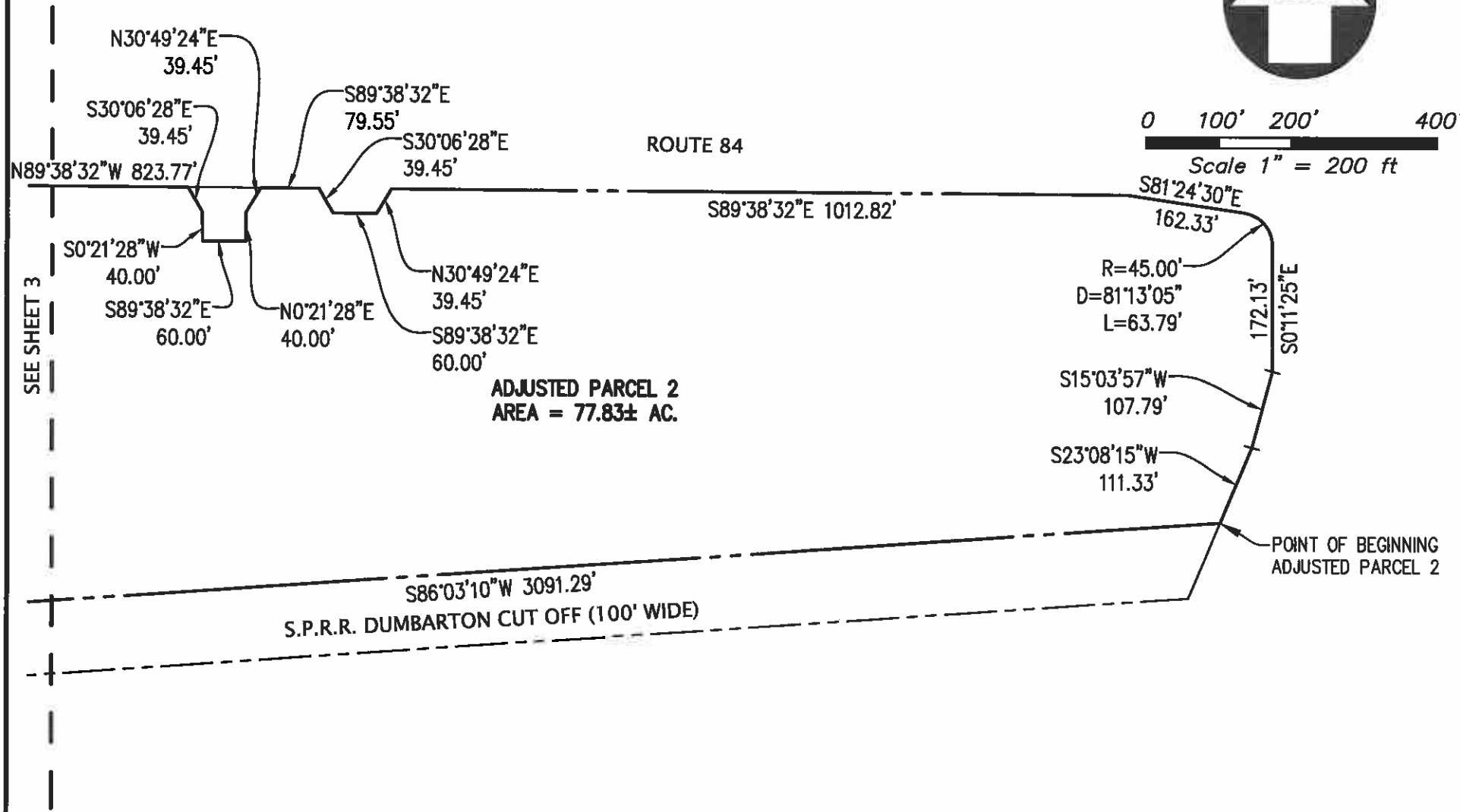
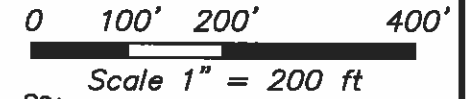
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Livermore, California 94551 Fax (925) 245-8796

EXHIBIT "A"
PLAT TO ACCOMPANY LEGAL DESCRIPTION

MENLO PARK,

CALIFORNIA

DATE	NOVEMBER, 2016
SCALE	1" = 200'
BY	RJH
JOB NO.	A15571-3
SHEET	3 OF 4



KIER & WRIGHT
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EXHIBIT "A"
PLAT TO ACCOMPANY LEGAL DESCRIPTION

MENLO PARK,

CALIFORNIA

DATE	NOVEMBER, 2016
SCALE	1" = 200'
BY	RJH
JOB NO.	A15571-3
SHEET	4 OF 4

EXHIBIT B

EXHIBIT 'B'
LEGAL DESCRIPTION

ADJUSTED PARCEL 1

REAL PROPERTY SITUATE IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEING A PORTION OF PARCEL 1, AS SAID PARCEL IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 12-01, RECORDED JANUARY 11, 2013, AS INSTRUMENT NO. 2013-006489, OFFICIAL RECORDS OF SAN MATEO COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE INTERSECTION OF THE SOUTHERLY RIGHT OF WAY OF ROUTE 84, AS SAID ROUTE IS SHOWN ON THAT CERTAIN CALTRANS RIGHT OF WAY MAP FOR ROUTE 84 IN THE COUNTY OF SAN MATEO ON SHEETS R-105.2 THROUGH R-105.4, WITH THE EAST RIGHT OF WAY LINE OF CHILCO STREET, AS SAID RIGHT OF WAY IS DESCRIBED AS PARCEL 46737-3 IN THAT CERTAIN DOCUMENT RECORDED JULY 27, 1983 AS DOCUMENT NUMBER 83-078012, OFFICIAL RECORDS OF SAN MATEO COUNTY, SAID INTERSECTION ALSO BEING THE NORTHWEST CORNER OF SAID PARCEL 1;

THENCE ALONG SAID SOUTHERLY RIGHT OF WAY, SOUTH 63° 56' 05" EAST, 11.21 FEET;

THENCE CONTINUING ALONG SAID SOUTHERLY RIGHT OF WAY, SOUTH 80° 18' 45" EAST, 367.64 FEET;

THENCE SOUTH 23° 50' 40" WEST, 393.73 FEET;

THENCE NORTH 53° 29' 03" WEST, 198.82 FEET;

THENCE NORTH 64° 42' 36" WEST, 158.40 FEET TO A POINT ON SAID EASTERLY RIGHT OF WAY;

THENCE ALONG SAID EASTERLY RIGHT OF WAY, NORTH 20° 11' 05" EAST, 238.48 FEET;

THENCE CONTINUING ALONG SAID EASTERLY RIGHT OF WAY, NORTH 23° 26' 23" EAST, 18.64 FEET TO THE **POINT OF BEGINNING**.

CONTAINING 2.61 ACRES OF LAND, MORE OR LESS.

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.



RICHARD J. HICKENBOTTOM, LS 8654
LICENSE EXPIRES: 12/31/17

11/9/16

DATE

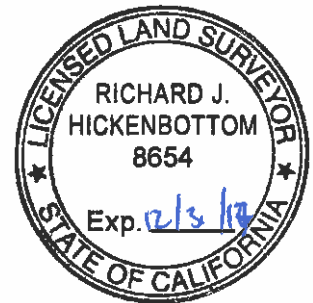


EXHIBIT 'B'
LEGAL DESCRIPTION

ADJUSTED PARCEL 2

REAL PROPERTY SITUATE IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEING A PORTION OF PARCEL 1, AS SAID PARCEL 1 IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 12-01, RECORDED JANUARY 11, 2013 AS INSTRUMENT NO. 2013-006489, AND ALL OF PARCEL A, AS SAID PARCEL A IS DESCRIBED IN THAT CERTAIN LOT LINE ADJUSTMENT NO. 13-01, RECORDED MAY 2, 2013 AS INSTRUMENT NO. 2013-066476, OFFICIAL RECORDS OF SAN MATEO COUNTY, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE INTERSECTION OF THE NORTH LINE OF THE 100 FOOT WIDE SOUTHERN PACIFIC RAILROAD DUMBARTON CUT OFF RIGHT OF WAY WITH THE WESTERLY RIGHT OF WAY OF ROUTE 144, AS SAID ROUTE 144 IS SHOWN ON THAT CERTAIN CALTRANS RIGHT OF WAY MAP FOR ROUTE 84 IN THE COUNTY OF SAN MATEO ON SHEETS R-105.2 THROUGH R-105.4;

THENCE ALONG SAID NORTH LINE, SOUTH 86° 03' 10" WEST, 3091.29 FEET TO THE INTERSECTION OF SAID NORTH LINE WITH THE NORTHERLY RIGHT OF WAY LINE FOR CHILCO STREET, AS DESCRIBED AS PARCEL 45831-1 IN THAT CERTAIN DOCUMENT RECORDED JUNE 29, 1982 AS DOCUMENT NUMBER 82-054425, OFFICIAL RECORDS OF SAN MATEO COUNTY;

THENCE ALONG THE SAID NORTHERLY RIGHT OF WAY OF CHILCO STREET THE FOLLOWING THREE (3) COURSES:

- 1) ALONG THE ARC OF A NON-TANGENT CURVE TO THE LEFT, THE CENTER OF WHICH BEARS SOUTH 51° 25' 59" WEST, HAVING A RADIUS OF 231.73 FEET, THROUGH A CENTRAL ANGLE OF 53° 54' 34" FOR AN ARC LENGTH OF 218.04 FEET,
- 2) SOUTH 86° 03' 10" WEST, 1371.94 FEET, AND
- 3) SOUTH 23° 26' 23" WEST, 42.25 FEET TO A POINT ON THE EASTERLY RIGHT OF WAY OF CHILCO STREET, AS DESCRIBED AS PARCEL 46737-3 IN THAT CERTAIN DOCUMENT RECORDED JULY 29, 1983 AS DOCUMENT NUMBER 83-078012, OFFICIAL RECORDS OF SAN MATEO COUNTY;

THENCE ALONG SAID EASTERLY RIGHT OF WAY THE FOLLOWING NINE (9) COURSES:

- 1) ALONG THE ARC OF A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 01° 58' 59" WEST, HAVING A RADIUS OF 335.02 FEET, THROUGH A CENTRAL ANGLE OF 112° 10' 04" FOR AN ARC LENGTH OF 655.87 FEET,
- 2) NORTH 20° 11' 05" EAST, 388.11 FEET,
- 3) ALONG THE ARC OF A CURVE THE RIGHT, HAVING A RADIUS OF 20.00 FEET, THROUGH A CENTRAL ANGLE OF 93° 36' 25" FOR AN ARC LENGTH OF 32.68 FEET,
- 4) SOUTH 66° 12' 30" EAST, 5.00 FEET,
- 5) NORTH 23° 47' 30" EAST, 30.00 FEET,
- 6) SOUTH 66° 12' 30" EAST, 5.64 FEET,
- 7) NORTH 23° 47' 30" EAST, 30.00 FEET,
- 8) NORTH 66° 12' 30" WEST, 5.00 FEET, AND
- 9) ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 20.00 FEET, THROUGH A CENTRAL ANGLE OF 86° 23' 35" FOR AN ARC LENGTH OF 30.16 FEET;

THENCE SOUTH 64° 42' 36" EAST, 158.40 FEET;

THENCE SOUTH 53° 29' 03" EAST, 198.82 FEET;

THENCE NORTH 23° 50' 40" EAST, 393.73 FEET TO A POINT ON THE SOUTHERLY RIGHT OF WAY OF ROUTE 84, AS SAID ROUTE IS SHOWN ON SAID CALTRANS RIGHT OF WAY MAP;

THENCE ALONG SAID SOUTHERLY RIGHT OF WAY OF ROUTE 84 THE FOLLOWING TWO (2) COURSES:

1) SOUTH 80° 18' 45" EAST, 2,131.79 FEET, AND

2) SOUTH 89° 38' 32" EAST, 823.77 FEET TO THE NORTHWEST CORNER OF PARCEL B, AS SAID PARCEL IS DESCRIBED IN SAID LOT LINE ADJUSTMENT NO. 13-01;

THENCE ALONG THE BOUNDARY OF SAID PARCEL B THE FOLLOWING FIVE (5) COURSES:

1) SOUTH 30° 06' 28" EAST, 39.45 FEET,

2) SOUTH 0° 21' 28" WEST, 40.00 FEET,

3) SOUTH 89° 38' 32" EAST, 60.00 FEET,

4) NORTH 0° 21' 28" EAST, 40.00 FEET, AND

5) NORTH 30° 49' 24" EAST, 39.45 FEET TO A POINT ON SAID SOUTHERLY RIGHT OF WAY OF ROUTE 84;

THENCE ALONG SAID SOUTHERLY RIGHT OF WAY OF ROUTE 84 THE FOLLOWING NINE (9) COURSES:

1) SOUTH 89° 38' 32" EAST, 79.55 FEET,

2) SOUTH 30° 06' 28" EAST, 39.45 FEET,

3) SOUTH 89° 38' 32" EAST, 60.00 FEET,

4) NORTH 30° 49' 24" EAST, 39.45 FEET,

5) SOUTH 89° 38' 32" EAST, 1012.82 FEET,

6) SOUTH 81° 24' 30" EAST, 162.33 FEET,

7) ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 45.00 FEET, THROUGH A CENTRAL ANGLE OF 81° 13' 05" FOR AN ARC LENGTH OF 63.79 FEET,


8) SOUTH 0° 11' 25" EAST, 172.13 FEET, AND

9) SOUTH 15° 03' 57" WEST, 107.79 FEET TO SAID WESTERLY RIGHT OF WAY OF ROUTE 114;

THENCE ALONG SAID WESTERLY RIGHT WAY, SOUTH 23° 08' 15" WEST, 111.33 FEET TO THE **POINT OF BEGINNING.**

CONTAINING 77.83 ACRES OF LAND, MORE OR LESS.

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.


RICHARD J. HICKENBOTTOM, LS 8654
LICENSE EXPIRES: 12/31/17

11/9/16
DATE



Memorandum

To: Erin Efner and Jessica Viramontes, ICF

From: Chelsea Guerrero and Stephanie Hagar, BAE Urban Economics

Date: August 9, 2017

Re: Addendum to Fiscal Impact Analysis for the Proposed Revised Facebook Campus Expansion Project

Introduction and Purpose

The City Council of the City of Menlo Park (City) approved the Facebook Campus Expansion Project (Approved Project) in November 2016. The Approved Project involved the demolition of existing buildings located at the former TE Connectivity (TE) campus located at 301-309 Constitution Drive (Project site), and the construction of two new office buildings and a hotel. BAE analyzed the fiscal impacts of the Adopted Project in the Fiscal Impact Analysis (FIA) Report for the proposed Facebook expansion, which was completed in September 2016.

In February 2017, the Project Sponsor submitted an application to modify the Approved Project (Proposed Revised Project). Under the Proposed Revised Project, the site plan would be modified to allow the building located at 305 Constitution Drive (Building 305) to remain occupied on site while Building 22 is constructed. Although Facebook is not obligated to build the hotel, it is assumed based on the application for the Proposed Revised Project that construction of the hotel would begin within one year of the date when Building 305 is vacated by TE. Under the current lease, TE will vacate no later than September 2022. The Project Sponsor has indicated that TE could choose to vacate Building 305 in September 2020, instead of September 2022 when the current lease ends.

This memorandum serves as an addendum to the 2016 FIA Report for the Approved Project, with revised findings based on the Proposed Revised Project. As noted above, there are two vacation dates that could trigger the demolition of Building 305 and the construction of the hotel – September 2022 or potentially September 2020. The analysis presented in this memorandum evaluates both of these potential phasing dates for the Proposed Revised Project.

San Francisco

2600 10th St., Suite 300
Berkeley, CA 94710
510.547.9380

Sacramento

803 2nd St., Suite A
Davis, CA 95616
530.750.2195

Los Angeles

448 South Hill St., Suite 701
Los Angeles, CA 90013
213.471.2666

Washington DC

1400 I St. NW, Suite 350
Washington, DC 20005
202.588.8945

New York City

49 West 27th St., Suite 10W
New York, NY 10001
212.683.4486

Proposed Revised Project Phasing

The Proposed Revised Project would consist of three phases. Phase 1 would include the construction of Building 21, which is currently under construction. Phase 2 would involve the demolition of Buildings 301-304, 306, and the Chemical Transfer Facility (CTF) and the construction of Building 22. Phase 3 would begin with the demolition of Building 305 and would be followed by the construction of the hotel. Construction on the hotel would take approximately 18 months.

The analysis presented in this memorandum incorporates two phasing assumptions related to the demolition of Building 305, reflecting two potential completion dates for the hotel. The two phasing scenarios are detailed in Table 1, on the next page. Under Scenario 1, TE would vacate Building 305 at the expiration of their current lease in September 2022, with full buildout of the hotel occurring by mid-2024. Under Scenario 2, TE would terminate their lease early and vacate Building 305 in September 2020. Construction of the hotel could then start in late 2020 and would be completed by mid-2022.

At project completion, the Proposed Revised Project would result in the same gross square footages and generate the same number of employees as the Approved Project. Consistent with the FIA prepared for the Approved Project, the baseline existing employment at the Project site is estimated at 600 employees. The analysis presented in this memorandum also accounts for the portion of this employment that would remain in Building 305 during the interim period while Building 22 is constructed. According to information provided by the Project Sponsor in the Proposed Revised Project application materials, employment at Building 305 is estimated at approximately 330 employees.

Table 1: Proposed Revised Project Phasing, 2015-2035

Scenario 1	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025
Gross New Square Footage	0	0	0	512,900	962,400	962,400	962,400	962,400	962,400	1,137,200	1,137,200
Office	0	0	0	512,900	962,400	962,400	962,400	962,400	962,400	962,400	962,400
Hotel	0	0	0	0	0	0	0	0	0	174,800	174,800
Net New Hotel Rooms	0	0	0	0	0	0	0	0	0	200	200
Demolition of Existing	0	0	221,969	221,969	221,969	221,969	221,969	511,687	511,687	511,687	511,687
301 Constitution Dr.	0	0	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465
302 Constitution Dr.	0	0	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174
303, 304, and 306 Constitution Dr.	0	0	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095
305 Constitution Dr.	0	0	0	0	0	0	0	289,718	289,718	289,718	289,718
Chemical Transfer Facility	0	0	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235
Net New Service Population (a)	0	0	(90)	1,043	2,043	2,043	2,043	1,933	1,933	1,983	1,983
Gross New Employees (b)	0	0	0	3,400	6,400	6,400	6,400	6,400	6,400	6,550	6,550
Less: Existing Employment on Site (c)	<u>0</u>	<u>0</u>	<u>(270)</u>	<u>(270)</u>	<u>(270)</u>	<u>(270)</u>	<u>(270)</u>	<u>(600)</u>	<u>(600)</u>	<u>(600)</u>	<u>(600)</u>
Net New Employees	0	0	(270)	3,130	6,130	6,130	6,130	5,800	5,800	5,950	5,950
Scenario 2											
Gross New Square Footage	0	0	0	512,900	962,400	962,400	962,400	1,137,200	1,137,200	1,137,200	1,137,200
Office	0	0	0	512,900	962,400	962,400	962,400	962,400	962,400	962,400	962,400
Hotel	0	0	0	0	0	0	0	174,800	174,800	174,800	174,800
Net New Hotel Rooms	0	0	0	0	0	0	0	200	200	200	200
Demolition of Existing	0	0	221,969	221,969	221,969	511,687	511,687	511,687	511,687	511,687	511,687
301 Constitution Dr.	0	0	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465
302 Constitution Dr.	0	0	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174
303, 304, and 306 Constitution Dr.	0	0	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095
305 Constitution Dr.	0	0	0	0	0	289,718	289,718	289,718	289,718	289,718	289,718
Chemical Transfer Facility	0	0	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235
Net New Service Population (a)	0	0	(90)	1,043	2,043	1,933	1,933	1,983	1,983	1,983	1,983
Gross New Employees (b)	0	0	0	3,400	6,400	6,400	6,400	6,550	6,550	6,550	6,550
Less: Existing Employment on Site (c)	<u>0</u>	<u>0</u>	<u>(270)</u>	<u>(270)</u>	<u>(270)</u>	<u>(600)</u>	<u>(600)</u>	<u>(600)</u>	<u>(600)</u>	<u>(600)</u>	<u>(600)</u>
Net New Employees	0	0	(270)	3,130	6,130	5,800	5,800	5,950	5,950	5,950	5,950

Notes:

(a) Service population equals the resident population plus a portion of the employment population to reflect the reduced demand from commercial uses.

To estimate service population, employees are multiplied by:

1/3

(b) According to the Draft EIR for the Project, the worker generation associated with the Project is: 6,550

(c) Consistent with the FIA prepared for the Approved Project, total existing employment at the Project site is estimated at 600 employees.

This total includes 330 employees in Building 305 and 270 employees in Buildings 301-304, 306, and the CTF.

Sources: City of Menlo Park, 2015; ICF, 2016; BAE, 2017.

Table 1: Proposed Revised Project Phasing, 2015-2035 (continued)

Scenario 1	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035
Gross New Square Footage	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200
Office	962,400	962,400	962,400	962,400	962,400	962,400	962,400	962,400	962,400	962,400
Hotel	174,800	174,800	174,800	174,800	174,800	174,800	174,800	174,800	174,800	174,800
Net New Hotel Rooms	200	200	200	200	200	200	200	200	200	200
Demolition of Existing	511,687	511,687	511,687	511,687	511,687	511,687	511,687	511,687	511,687	511,687
301 Constitution Dr.	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465
302 Constitution Dr.	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174
303, 304, and 306 Constitution Dr.	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095
305 Constitution Dr.	289,718	289,718	289,718	289,718	289,718	289,718	289,718	289,718	289,718	289,718
Chemical Transfer Facility	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235
Net New Service Population (a)	1,983	1,983	1,983	1,983	1,983	1,983	1,983	1,983	1,983	1,983
Gross New Employees (b)	6,550	6,550	6,550	6,550	6,550	6,550	6,550	6,550	6,550	6,550
Less: Existing Employment on Site (c)	(600)	(600)	(600)	(600)	(600)	(600)	(600)	(600)	(600)	(600)
Net New Employees	5,950	5,950	5,950	5,950	5,950	5,950	5,950	5,950	5,950	5,950
Scenario 2										
Gross New Square Footage	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200	1,137,200
Office	962,400	962,400	962,400	962,400	962,400	962,400	962,400	962,400	962,400	962,400
Hotel	174,800	174,800	174,800	174,800	174,800	174,800	174,800	174,800	174,800	174,800
Net New Hotel Rooms	200	200	200	200	200	200	200	200	200	200
Demolition of Existing	511,687	511,687	511,687	511,687	511,687	511,687	511,687	511,687	511,687	511,687
301 Constitution Dr.	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465	34,465
302 Constitution Dr.	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174	30,174
303, 304, and 306 Constitution Dr.	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095	155,095
305 Constitution Dr.	289,718	289,718	289,718	289,718	289,718	289,718	289,718	289,718	289,718	289,718
Chemical Transfer Facility	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235	2,235
Net New Service Population (a)	1,983	1,983	1,983	1,983	1,983	1,983	1,983	1,983	1,983	1,983
Gross New Employees (b)	6,550	6,550	6,550	6,550	6,550	6,550	6,550	6,550	6,550	6,550
Less: Existing Employment on Site (c)	(600)	(600)	(600)	(600)	(600)	(600)	(600)	(600)	(600)	(600)
Net New Employees	5,950	5,950	5,950	5,950	5,950	5,950	5,950	5,950	5,950	5,950

Notes:

(a) Service population equals the resident population plus a portion of the employment population to reflect the reduced demand from commercial uses.

To estimate service population, employees are multiplied 1/3

(b) According to the Draft EIR for the Project, the worker generation associated with the Project is 6,550

(c) Consistent with the FIA prepared for the Approved Project, total existing employment at the Project site is estimated at 600 employees.

This total includes 330 employees in Building 305 and 270 employees in Buildings 301-304, 306, and the CTF.

Sources: City of Menlo Park, 2015; ICF, 2016; BAE, 2017.

Fiscal Impact Methodology for Addendum

The analysis presented in this memorandum draws heavily from the assumptions in the FIA Report that BAE completed in September 2016. Except as noted, the methodology used for this addendum is the same as the methodology used in the 2016 FIA Report.

The analysis presented in this memorandum incorporates the revised phasing assumptions related to the demolition of Building 305 and the subsequent construction of the hotel. BAE adjusted the phasing for this addendum to show completion of the hotel in 2024 (Scenario 1) and 2022 (Scenario 2), five years and three years later respectively than the projected hotel completion year (2019) shown in the 2016 FIA Report. As a result, this addendum anticipates that revenues to the City in the form of transient occupancy tax (TOT) and other revenues from the hotel will not begin in 2019 as shown in the 2016 FIA Report, but will begin in 2024 for Scenario 1 and 2022 for Scenario 2.

The revised phasing schedule also required that this analysis estimate the impacts of the demolition of Building 305 separately from the impacts of the demolition of the other buildings on site. The 2016 FIA assumed that Buildings 301-309 would all be demolished within the same year, which reflected the anticipated phasing at the time the 2016 FIA was completed. Consequently, the 2016 FIA addressed all revenue and expenditure impacts resulting from the demolition of buildings that existed on site prior to initiation of the Project on a site-wide basis, rather than for individual buildings. However, the Proposed Revised Project phasing anticipates demolition of Building 305 at a later date than the other buildings that were on site prior to the start of the Project, creating impacts from demolition of separate buildings in two separate years.

To analyze the impacts of the demolition across multiple years, BAE prorated the total site-wide revenue and expenditure impacts from the 2016 FIA Report. Property tax and property tax in-lieu of vehicle license fees were prorated based on the square footage of Building 305 as a share of the total square footage on site prior to initiation of the Project. For sales tax revenues, business license fee revenues, UUT revenues, and City expenditures, estimates from the 2016 FIA were prorated based on the number of employees in Building 305 as a share of the total number of employees on the Project site prior to initiation of the Project.

Consistent with the original FIA, this analysis does not consider the fiscal impacts to the City that would result from any Development Agreement between the Project Sponsor and the City of Menlo Park. For reference, a Development Agreement was adopted by the City Council for the Approved Project. The Development Agreement will be modified with the Proposed Revised Project for consistency with the requested CDP amendment. However, consistent with the previous report, the amendment of the Development Agreement is not considered in this analysis.

City General Fund Fiscal Impact Analysis

This section presents the findings from the FIA, revised to reflect the phasing of the Proposed Revised Project. Since the Proposed Revised Project would include the same uses and result in the same gross square footage and number of employees as the Approved Project at build out, the annual revenue and expenditure impacts from of the Proposed Revised Project at project completion would be the same as the impacts at buildout that were identified in the 2016 FIA Report. This section provides a summary of the fiscal impacts at full build out, which are the same as shown in the 2016 FIA Report. In addition, this section shows year-by-year estimates of the net fiscal impacts of the Proposed Revised Project through 2035, the cumulative impact of the net fiscal impacts through 2035, and the net present value of the net fiscal impacts through 2035, all of which would be affected by the changes to phasing for the Proposed Revised Project.

Summary of Net Fiscal Impact to the General Fund at Build Out

Table 2 provides a summary of the annual recurring net fiscal impact of the Proposed Revised Project at full build out and occupancy in constant 2015 dollars. At project completion, the annual fiscal impacts of both phasing scenarios would be the same as the annual fiscal impacts of the Approved Project, generating approximately \$2.3 million annually in new General Fund revenues and increasing the City's General Fund expenditures by \$1.1 million annually, resulting in an annual net positive fiscal impact of approximately \$1.2 million annually.

Table 2: Summary of Net Fiscal Impact to the City of Menlo Park General Fund at Build Out, Proposed Revised Project

	<u>Scenario 1</u>	<u>Scenario 2</u>
Total Revenues	\$2,291,600	\$2,291,600
Sales Tax (a)	(\$31,600)	(\$31,600)
Property Tax	\$456,900	\$456,900
ILVLF	\$131,000	\$131,000
TOT (b)	\$1,220,400	\$1,220,400
Utility Users Tax	\$30,600	\$30,600
Other Revenues	\$484,300	\$484,300
Total Expenditures	\$1,106,800	\$1,106,800
Human Resources	\$62,100	\$62,100
Community Development	\$89,800	\$89,800
Community Services	\$160,200	\$160,200
Library	\$51,900	\$51,900
Police	\$742,800	\$742,800
Net Fiscal Impact	<u>\$1,184,800</u>	<u>\$1,184,800</u>

Notes:

Figures presented are in constant 2015 dollars.

(a) Sales tax figure represents the conservative estimate shown in Table 4 of the prior FIA report.

(b) Transit Occupancy Tax figure represents average estimate shown in Table 13 of the prior FIA report.

Source: BAE, 2017.

As with the 2016 FIA Report for the Approved Project, the analysis shown in this addendum estimates that the Proposed Revised Project would provide an annual fiscal surplus due largely to the TOT revenues from the hotel component of the project. The hotel would generate approximately \$1.2 million per year in TOT revenue, which is approximately equal to the total fiscal surplus generated by either the Approved Project or the Proposed Revised Project. As identified in this report, prior to construction of the hotel (or without the construction of the hotel), the Project would essentially have a neutral impact on the City's General Fund. Since the Proposed Revised Project anticipates a later hotel construction date than the Approved Project, the Proposed Revised Project will take longer to generate a net positive fiscal impact to the City (until 2024 for Scenario 1 or 2022 for Scenario 2, as detailed below).

Total 20 Year Impact

Table 3 summarizes the projected annual net fiscal impacts to the Menlo Park General Fund through 2035. The analysis in Table 3 relies on a set of inflation assumptions to forecast the long-term impact of price increases on revenues, expenditures, and the resulting net fiscal impact to the City. While this type of analysis can be useful, it should be noted that this type of long-term analysis is highly sensitive to changes in the projected rate of inflation.

As shown in Table 3, under both scenarios, the Proposed Revised Project would initially have a small net negative fiscal impact on the City's General Fund. The net fiscal impact would turn positive after the completion of the hotel in 2024 (Scenario 1) or 2022 (Scenario 2). After the completion of the hotel, the City's General Fund surplus would increase each year through the end of the projection period. In 2035, both scenarios would each have a net positive fiscal impact totaling \$2.2 million.

Although this analysis did not include an evaluation of a development program that would not include the hotel, these findings suggest that if the hotel is not constructed there would be a small net negative fiscal impact to the City. However, the magnitude of the negative fiscal impact would be minimal relative to the City's overall annual General Fund expenditures, which totaled \$48.2 million in the 2015/2016 fiscal year and are projected to increase over time. Because the magnitude of the fiscal impact is relatively small and the fiscal analysis uses conservative assumptions to avoid overestimating potential revenues or underestimating potential costs to the City, the negative fiscal impact to the City from a hypothetical scenario that would not include a hotel would essentially represent a neutral net fiscal impact to the City.

The projected annual net fiscal impact of the Approved Project from the 2016 FIA Report, which is shown in Table 4, was also estimated at \$2.2 million at the end of the forecast period in 2035. However, the 2016 FIA Report estimated that the Approved Project would begin to have a positive net fiscal impact in 2019 due to completion of the hotel in 2019 under the

anticipated phasing for the Approved Project, which is five (Scenario 1) or three (Scenario 2) years before the Proposed Revised Project would begin to have a net positive fiscal impact.

Table 3: Projected Net Fiscal Impact to the City of Menlo Park General Fund, Proposed Revised Project, 2015-2035

Scenario 1	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025
Total Revenues	\$0	\$0	(\$87,200)	\$586,100	\$1,216,900	\$1,254,100	\$1,292,400	\$1,208,000	\$1,245,300	\$3,134,700	\$3,246,900
Sales Tax (a)	\$0	\$0	(\$41,500)	(\$10,200)	\$18,600	\$19,000	\$19,400	(\$36,300)	(\$37,000)	(\$37,800)	(\$38,500)
Property Tax	\$0	\$0	(\$400)	\$225,400	\$431,700	\$440,300	\$449,100	\$457,500	\$466,700	\$546,000	\$557,000
ILVLF	\$0	\$0	(\$100)	\$64,600	\$123,800	\$126,300	\$128,800	\$131,200	\$133,800	\$156,600	\$159,700
TOT (b)	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,737,000	\$1,806,500
Utility Users Tax	\$0	\$0	(\$7,900)	\$19,200	\$45,200	\$47,000	\$48,800	\$39,000	\$40,500	\$43,600	\$45,300
Other Revenues	\$0	\$0	(\$37,300)	\$287,100	\$597,600	\$621,500	\$646,300	\$616,600	\$641,300	\$689,300	\$716,900
Total Expenditures	\$0	\$0	\$54,300	(\$654,900)	(\$1,333,900)	(\$1,387,200)	(\$1,442,700)	(\$1,419,800)	(\$1,476,600)	(\$1,575,300)	(\$1,638,200)
Human Resources	\$0	\$0	\$3,000	(\$36,800)	(\$74,900)	(\$77,900)	(\$81,000)	(\$79,700)	(\$82,900)	(\$88,400)	(\$91,900)
Community Development	\$0	\$0	\$4,400	(\$53,100)	(\$108,200)	(\$112,500)	(\$117,000)	(\$115,100)	(\$119,700)	(\$127,800)	(\$132,900)
Community Services	\$0	\$0	\$7,900	(\$94,800)	(\$193,000)	(\$200,700)	(\$208,800)	(\$205,500)	(\$213,800)	(\$228,000)	(\$237,100)
Library	\$0	\$0	\$2,600	(\$30,700)	(\$62,500)	(\$65,000)	(\$67,600)	(\$66,600)	(\$69,200)	(\$73,900)	(\$76,800)
Police	\$0	\$0	\$36,400	(\$439,500)	(\$895,300)	(\$931,100)	(\$968,300)	(\$952,900)	(\$991,000)	(\$1,057,200)	(\$1,099,500)
Net Fiscal Impact	\$0	\$0	(\$32,900)	(\$68,800)	(\$117,000)	(\$133,100)	(\$150,300)	(\$211,800)	(\$231,300)	\$1,559,400	\$1,608,700
Scenario 2											
Total Revenues	\$0	\$0	(\$87,200)	\$586,100	\$1,216,900	\$1,137,100	\$1,172,000	\$2,922,600	\$3,026,700	\$3,134,700	\$3,246,900
Sales Tax (a)	\$0	\$0	(\$41,500)	(\$10,200)	\$18,600	(\$34,900)	(\$35,600)	(\$36,300)	(\$37,000)	(\$37,800)	(\$38,500)
Property Tax	\$0	\$0	(\$400)	\$225,400	\$431,700	\$439,800	\$448,600	\$524,800	\$535,300	\$546,000	\$557,000
ILVLF	\$0	\$0	(\$100)	\$64,600	\$123,800	\$126,100	\$128,600	\$150,500	\$153,500	\$156,600	\$159,700
TOT (b)	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,606,000	\$1,670,200	\$1,737,000	\$1,806,500
Utility Users Tax	\$0	\$0	(\$7,900)	\$19,200	\$45,200	\$36,000	\$37,500	\$40,300	\$41,900	\$43,600	\$45,300
Other Revenues	\$0	\$0	(\$37,300)	\$287,100	\$597,600	\$570,100	\$592,900	\$637,300	\$662,800	\$689,300	\$716,900
Total Expenditures	\$0	\$0	\$54,300	(\$654,900)	(\$1,333,900)	(\$1,312,800)	(\$1,365,200)	(\$1,456,500)	(\$1,514,700)	(\$1,575,300)	(\$1,638,200)
Human Resources	\$0	\$0	\$3,000	(\$36,800)	(\$74,900)	(\$73,700)	(\$76,700)	(\$81,700)	(\$85,000)	(\$88,400)	(\$91,900)
Community Development	\$0	\$0	\$4,400	(\$53,100)	(\$108,200)	(\$106,500)	(\$110,700)	(\$118,200)	(\$122,900)	(\$127,800)	(\$132,900)
Community Services	\$0	\$0	\$7,900	(\$94,800)	(\$193,000)	(\$190,000)	(\$197,600)	(\$210,800)	(\$219,200)	(\$228,000)	(\$237,100)
Library	\$0	\$0	\$2,600	(\$30,700)	(\$62,500)	(\$61,600)	(\$64,000)	(\$68,300)	(\$71,000)	(\$73,900)	(\$76,800)
Police	\$0	\$0	\$36,400	(\$439,500)	(\$895,300)	(\$881,000)	(\$916,200)	(\$977,500)	(\$1,016,600)	(\$1,057,200)	(\$1,099,500)
Net Fiscal Impact	\$0	\$0	(\$32,900)	(\$68,800)	(\$117,000)	(\$175,700)	(\$193,200)	\$1,466,100	\$1,512,000	\$1,559,400	\$1,608,700

Notes:

Figures presented are adjusted for inflation.

Revenue Escalation factor: 4.0%

Sales Tax Escalation factor: 2.0%

Property Tax Escalation factor: 2.0%

Expenditure Escalation factor: 4.0%

(a) Sales tax figure represents conservative estimate shown in Table 4 of the FIA report for the Approved Project.

(b) Transient Occupancy Tax figure represents average estimate shown in Table 13 of the FIA report for the Approved Project.

Source: BAE, 2017.

Table 3: Projected Net Fiscal Impact to the City of Menlo Park General Fund, Proposed Revised Project, 2015-2035 (continued)

Scenario 1	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035
Total Revenues	\$3,363,100	\$3,483,800	\$3,609,100	\$3,739,100	\$3,873,900	\$4,013,800	\$4,159,200	\$4,310,000	\$4,466,400	\$4,628,800
Sales Tax (a)	(\$39,300)	(\$40,100)	(\$40,900)	(\$41,700)	(\$42,500)	(\$43,400)	(\$44,200)	(\$45,100)	(\$46,000)	(\$47,000)
Property Tax	\$568,100	\$579,500	\$591,000	\$602,900	\$614,900	\$627,200	\$639,800	\$652,600	\$665,600	\$678,900
ILVLF	\$162,900	\$166,100	\$169,500	\$172,900	\$176,300	\$179,800	\$183,400	\$187,100	\$190,800	\$194,700
TOT (b)	\$1,878,700	\$1,953,900	\$2,032,100	\$2,113,300	\$2,197,900	\$2,285,800	\$2,377,200	\$2,472,300	\$2,571,200	\$2,674,000
Utility Users Tax	\$47,100	\$49,000	\$51,000	\$53,000	\$55,100	\$57,300	\$59,600	\$62,000	\$64,500	\$67,000
Other Revenues	\$745,600	\$775,400	\$806,400	\$838,700	\$872,200	\$907,100	\$943,400	\$981,100	\$1,020,300	\$1,061,200
Total Expenditures	(\$1,703,800)	(\$1,772,000)	(\$1,842,800)	(\$1,916,600)	(\$1,993,200)	(\$2,073,100)	(\$2,156,000)	(\$2,242,100)	(\$2,331,800)	(\$2,425,200)
Human Resources	(\$95,600)	(\$99,400)	(\$103,400)	(\$107,500)	(\$111,800)	(\$116,300)	(\$121,000)	(\$125,800)	(\$130,800)	(\$136,100)
Community Development	(\$138,200)	(\$143,800)	(\$149,500)	(\$155,500)	(\$161,700)	(\$168,200)	(\$174,900)	(\$181,900)	(\$189,200)	(\$196,800)
Community Services	(\$246,600)	(\$256,500)	(\$266,700)	(\$277,400)	(\$288,500)	(\$300,100)	(\$312,100)	(\$324,500)	(\$337,500)	(\$351,000)
Library	(\$79,900)	(\$83,100)	(\$86,400)	(\$89,900)	(\$93,500)	(\$97,200)	(\$101,100)	(\$105,100)	(\$109,300)	(\$113,700)
Police	(\$1,143,500)	(\$1,189,200)	(\$1,236,800)	(\$1,286,300)	(\$1,337,700)	(\$1,391,300)	(\$1,446,900)	(\$1,504,800)	(\$1,565,000)	(\$1,627,600)
Net Fiscal Impact	\$1,659,300	\$1,711,800	\$1,766,300	\$1,822,500	\$1,880,700	\$1,940,700	\$2,003,200	\$2,067,900	\$2,134,600	\$2,203,600
Scenario 2										
Total Revenues	\$3,363,100	\$3,483,800	\$3,609,100	\$3,739,100	\$3,873,900	\$4,013,800	\$4,159,200	\$4,310,000	\$4,466,400	\$4,628,800
Sales Tax (a)	(\$39,300)	(\$40,100)	(\$40,900)	(\$41,700)	(\$42,500)	(\$43,400)	(\$44,200)	(\$45,100)	(\$46,000)	(\$47,000)
Property Tax	\$568,100	\$579,500	\$591,000	\$602,900	\$614,900	\$627,200	\$639,800	\$652,600	\$665,600	\$678,900
ILVLF	\$162,900	\$166,100	\$169,500	\$172,900	\$176,300	\$179,800	\$183,400	\$187,100	\$190,800	\$194,700
TOT (b)	\$1,878,700	\$1,953,900	\$2,032,100	\$2,113,300	\$2,197,900	\$2,285,800	\$2,377,200	\$2,472,300	\$2,571,200	\$2,674,000
Utility Users Tax	\$47,100	\$49,000	\$51,000	\$53,000	\$55,100	\$57,300	\$59,600	\$62,000	\$64,500	\$67,000
Other Revenues	\$745,600	\$775,400	\$806,400	\$838,700	\$872,200	\$907,100	\$943,400	\$981,100	\$1,020,300	\$1,061,200
Total Expenditures	(\$1,703,800)	(\$1,772,000)	(\$1,842,800)	(\$1,916,600)	(\$1,993,200)	(\$2,073,100)	(\$2,156,000)	(\$2,242,100)	(\$2,331,800)	(\$2,425,200)
Human Resources	(\$95,600)	(\$99,400)	(\$103,400)	(\$107,500)	(\$111,800)	(\$116,300)	(\$121,000)	(\$125,800)	(\$130,800)	(\$136,100)
Community Development	(\$138,200)	(\$143,800)	(\$149,500)	(\$155,500)	(\$161,700)	(\$168,200)	(\$174,900)	(\$181,900)	(\$189,200)	(\$196,800)
Community Services	(\$246,600)	(\$256,500)	(\$266,700)	(\$277,400)	(\$288,500)	(\$300,100)	(\$312,100)	(\$324,500)	(\$337,500)	(\$351,000)
Library	(\$79,900)	(\$83,100)	(\$86,400)	(\$89,900)	(\$93,500)	(\$97,200)	(\$101,100)	(\$105,100)	(\$109,300)	(\$113,700)
Police	(\$1,143,500)	(\$1,189,200)	(\$1,236,800)	(\$1,286,300)	(\$1,337,700)	(\$1,391,300)	(\$1,446,900)	(\$1,504,800)	(\$1,565,000)	(\$1,627,600)
Net Fiscal Impact	\$1,659,300	\$1,711,800	\$1,766,300	\$1,822,500	\$1,880,700	\$1,940,700	\$2,003,200	\$2,067,900	\$2,134,600	\$2,203,600

Notes:

Figures presented are adjusted for inflation.

Revenue Escalation factor: 4.0%
 Sales Tax Escalation factor: 2.0%
 Property Tax Escalation factor: 2.0%
 Expenditure Escalation factor: 4.0%

(a) Sales tax figure represents conservative estimate shown in Table 4 of the FIA report for the Approved Project.

(b) Transient Occupancy Tax figure represents average estimate shown in Table 13 of the FIA report for the Approved Project.

Source: BAE, 2017.

Table 4: Projected Net Fiscal Impact to the City of Menlo Park General Fund, Approved Project, 2015-2035

Project	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025
Total Revenues	\$0	\$0	(\$194,000)	\$476,100	\$2,632,300	\$2,725,400	\$2,822,100	\$2,922,600	\$3,026,700	\$3,134,700	\$3,246,900
Sales Tax (a)	\$0	\$0	(\$92,300)	(\$62,000)	(\$34,200)	(\$34,900)	(\$35,600)	(\$36,300)	(\$37,000)	(\$37,800)	(\$38,500)
Property Tax	\$0	\$0	(\$800)	\$224,900	\$494,600	\$504,500	\$514,500	\$524,800	\$535,300	\$546,000	\$557,000
ILVLF	\$0	\$0	(\$200)	\$64,500	\$141,800	\$144,600	\$147,500	\$150,500	\$153,500	\$156,600	\$159,700
TOT (b)	\$0	\$0	\$0	\$0	\$1,427,700	\$1,484,800	\$1,544,200	\$1,606,000	\$1,670,200	\$1,737,000	\$1,806,500
Utility Users Tax	\$0	\$0	(\$17,600)	\$9,100	\$35,800	\$37,200	\$38,700	\$40,300	\$41,900	\$43,600	\$45,300
Other Revenues	\$0	\$0	(\$83,100)	\$239,600	\$566,600	\$589,200	\$612,800	\$637,300	\$662,800	\$689,300	\$716,900
Total Expenditures	\$0	\$0	\$120,700	(\$585,600)	(\$1,294,800)	(\$1,346,600)	(\$1,400,500)	(\$1,456,500)	(\$1,514,700)	(\$1,575,300)	(\$1,638,200)
Human Resources	\$0	\$0	\$6,800	(\$32,800)	(\$72,600)	(\$75,600)	(\$78,600)	(\$81,700)	(\$85,000)	(\$88,400)	(\$91,900)
Community Development	\$0	\$0	\$9,800	(\$47,500)	(\$105,100)	(\$109,300)	(\$113,600)	(\$118,200)	(\$122,900)	(\$127,800)	(\$132,900)
Community Services	\$0	\$0	\$17,500	(\$84,800)	(\$187,400)	(\$194,900)	(\$202,700)	(\$210,800)	(\$219,200)	(\$228,000)	(\$237,100)
Library	\$0	\$0	\$5,600	(\$27,400)	(\$60,700)	(\$63,100)	(\$65,700)	(\$68,300)	(\$71,000)	(\$73,900)	(\$76,800)
Police	\$0	\$0	\$81,000	(\$393,100)	(\$869,000)	(\$903,700)	(\$939,900)	(\$977,500)	(\$1,016,600)	(\$1,057,200)	(\$1,099,500)
Net Fiscal Impact	\$0	\$0	(\$73,300)	(\$109,500)	\$1,337,500	\$1,378,800	\$1,421,600	\$1,466,100	\$1,512,000	\$1,559,400	\$1,608,700
Project Alternative											
Total Revenues	\$0	\$0	(\$194,000)	\$273,400	\$2,239,800	\$2,321,000	\$2,405,300	\$2,492,900	\$2,583,600	\$2,678,100	\$2,775,900
Sales Tax (a)	\$0	\$0	(\$92,300)	(\$71,600)	(\$52,700)	(\$53,800)	(\$54,800)	(\$55,900)	(\$57,100)	(\$58,200)	(\$59,400)
Property Tax	\$0	\$0	(\$800)	\$157,200	\$365,000	\$372,300	\$379,700	\$387,300	\$395,100	\$403,000	\$411,000
ILVLF	\$0	\$0	(\$200)	\$45,100	\$104,700	\$106,800	\$108,900	\$111,100	\$113,300	\$115,600	\$117,900
TOT (b)	\$0	\$0	\$0	\$0	\$1,427,700	\$1,484,800	\$1,544,200	\$1,606,000	\$1,670,200	\$1,737,000	\$1,806,500
Utility Users Tax	\$0	\$0	(\$17,600)	\$900	\$19,800	\$20,600	\$21,400	\$22,200	\$23,100	\$24,100	\$25,000
Other Revenues	\$0	\$0	(\$83,100)	\$141,800	\$375,300	\$390,300	\$405,900	\$422,200	\$439,000	\$456,600	\$474,900
Total Expenditures	\$0	\$0	\$120,700	(\$372,400)	(\$877,000)	(\$912,000)	(\$948,500)	(\$986,400)	(\$1,025,800)	(\$1,066,900)	(\$1,109,600)
Human Resources	\$0	\$0	\$6,800	(\$20,900)	(\$49,300)	(\$51,200)	(\$53,300)	(\$55,400)	(\$57,600)	(\$59,900)	(\$62,300)
Community Development	\$0	\$0	\$9,800	(\$30,300)	(\$71,100)	(\$74,000)	(\$76,900)	(\$80,000)	(\$83,200)	(\$86,500)	(\$90,000)
Community Services	\$0	\$0	\$17,500	(\$53,900)	(\$126,900)	(\$132,000)	(\$137,300)	(\$142,800)	(\$148,500)	(\$154,400)	(\$160,600)
Library	\$0	\$0	\$5,600	(\$17,400)	(\$41,100)	(\$42,700)	(\$44,400)	(\$46,200)	(\$48,000)	(\$50,000)	(\$52,000)
Police	\$0	\$0	\$81,000	(\$249,900)	(\$588,600)	(\$612,100)	(\$636,600)	(\$662,000)	(\$688,500)	(\$716,100)	(\$744,700)
Net Fiscal Impact	\$0	\$0	(\$73,300)	(\$99,000)	\$1,362,800	\$1,409,000	\$1,456,800	\$1,506,500	\$1,557,800	\$1,611,200	\$1,666,300

Notes:

Figures presented are adjusted for inflation.

Revenue Escalation factor: 4.0%

Sales Tax Escalation factor: 2.0%

Property Tax Inflation Rate: 2.0%

Expenditure Inflation Rate: 4.0%

(a) Sales tax figure represents conservative estimate shown in Table 4.

(b) Transient Occupancy Tax figure represents average estimate shown in Table 13.

Source: BAE, 2015.

Table 4: Projected Net Fiscal Impact to the City of Menlo Park General Fund, Approved Project, 2015-2035 (continued)

Project	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035
Total Revenues	\$3,363,100	\$3,483,800	\$3,609,100	\$3,739,100	\$3,873,900	\$4,013,800	\$4,159,200	\$4,310,000	\$4,466,400	\$4,628,800
Sales Tax (a)	(\$39,300)	(\$40,100)	(\$40,900)	(\$41,700)	(\$42,500)	(\$43,400)	(\$44,200)	(\$45,100)	(\$46,000)	(\$47,000)
Property Tax	\$568,100	\$579,500	\$591,000	\$602,900	\$614,900	\$627,200	\$639,800	\$652,600	\$665,600	\$678,900
ILVLF	\$162,900	\$166,100	\$169,500	\$172,900	\$176,300	\$179,800	\$183,400	\$187,100	\$190,800	\$194,700
TOT (b)	\$1,878,700	\$1,953,900	\$2,032,100	\$2,113,300	\$2,197,900	\$2,285,800	\$2,377,200	\$2,472,300	\$2,571,200	\$2,674,000
Utility Users Tax	\$47,100	\$49,000	\$51,000	\$53,000	\$55,100	\$57,300	\$59,600	\$62,000	\$64,500	\$67,000
Other Revenues	\$745,600	\$775,400	\$806,400	\$838,700	\$872,200	\$907,100	\$943,400	\$981,100	\$1,020,300	\$1,061,200
Total Expenditures	(\$1,703,800)	(\$1,772,000)	(\$1,842,800)	(\$1,916,600)	(\$1,993,200)	(\$2,073,100)	(\$2,156,000)	(\$2,242,100)	(\$2,331,800)	(\$2,425,200)
Human Resources	(\$95,600)	(\$99,400)	(\$103,400)	(\$107,500)	(\$111,800)	(\$116,300)	(\$121,000)	(\$125,800)	(\$130,800)	(\$136,100)
Community Development	(\$138,200)	(\$143,800)	(\$149,500)	(\$155,500)	(\$161,700)	(\$168,200)	(\$174,900)	(\$181,900)	(\$189,200)	(\$196,800)
Community Services	(\$246,600)	(\$256,500)	(\$266,700)	(\$277,400)	(\$288,500)	(\$300,100)	(\$312,100)	(\$324,500)	(\$337,500)	(\$351,000)
Library	(\$79,900)	(\$83,100)	(\$86,400)	(\$89,900)	(\$93,500)	(\$97,200)	(\$101,100)	(\$105,100)	(\$109,300)	(\$113,700)
Police	(\$1,143,500)	(\$1,189,200)	(\$1,236,800)	(\$1,286,300)	(\$1,337,700)	(\$1,391,300)	(\$1,446,900)	(\$1,504,800)	(\$1,565,000)	(\$1,627,600)
Net Fiscal Impact	\$1,659,300	\$1,711,800	\$1,766,300	\$1,822,500	\$1,880,700	\$1,940,700	\$2,003,200	\$2,067,900	\$2,134,600	\$2,203,600
Project Alternative										
Total Revenues	\$2,877,500	\$2,983,100	\$3,092,700	\$3,206,300	\$3,324,400	\$3,447,100	\$3,574,400	\$3,706,500	\$3,843,900	\$3,986,300
Sales Tax (a)	(\$60,600)	(\$61,800)	(\$63,000)	(\$64,300)	(\$65,500)	(\$66,900)	(\$68,200)	(\$69,600)	(\$70,900)	(\$72,400)
Property Tax	\$419,300	\$427,700	\$436,200	\$444,900	\$453,800	\$462,900	\$472,200	\$481,600	\$491,200	\$501,100
ILVLF	\$120,200	\$122,600	\$125,100	\$127,600	\$130,100	\$132,700	\$135,400	\$138,100	\$140,900	\$143,700
TOT (b)	\$1,878,700	\$1,953,900	\$2,032,100	\$2,113,300	\$2,197,900	\$2,285,800	\$2,377,200	\$2,472,300	\$2,571,200	\$2,674,000
Utility Users Tax	\$26,000	\$27,100	\$28,100	\$29,300	\$30,400	\$31,700	\$32,900	\$34,200	\$35,600	\$37,000
Other Revenues	\$493,900	\$513,600	\$534,200	\$555,500	\$577,700	\$600,900	\$624,900	\$649,900	\$675,900	\$702,900
Total Expenditures	(\$1,153,900)	(\$1,200,100)	(\$1,248,100)	(\$1,298,100)	(\$1,350,000)	(\$1,404,000)	(\$1,460,100)	(\$1,518,600)	(\$1,579,400)	(\$1,642,400)
Human Resources	(\$64,800)	(\$67,400)	(\$70,100)	(\$72,900)	(\$75,800)	(\$78,900)	(\$82,000)	(\$85,300)	(\$88,700)	(\$92,200)
Community Development	(\$93,600)	(\$97,300)	(\$101,200)	(\$105,300)	(\$109,500)	(\$113,900)	(\$118,400)	(\$123,200)	(\$128,100)	(\$133,200)
Community Services	(\$167,000)	(\$173,700)	(\$180,700)	(\$187,900)	(\$195,400)	(\$203,200)	(\$211,300)	(\$219,800)	(\$228,600)	(\$237,700)
Library	(\$54,000)	(\$56,200)	(\$58,400)	(\$60,800)	(\$63,200)	(\$65,700)	(\$68,400)	(\$71,100)	(\$74,000)	(\$76,900)
Police	(\$774,500)	(\$805,500)	(\$837,700)	(\$871,200)	(\$906,100)	(\$942,300)	(\$980,000)	(\$1,019,200)	(\$1,060,000)	(\$1,102,400)
Net Fiscal Impact	\$1,723,600	\$1,783,000	\$1,844,600	\$1,908,200	\$1,974,400	\$2,043,100	\$2,114,300	\$2,187,900	\$2,264,500	\$2,343,900

Notes:

Figures presented are adjusted for inflation.

Revenue Escalation factor: 4.0%

Sales Tax Escalation factor: 2.0%

Property Tax Inflation Rate: 2.0%

Expenditure Inflation Rate: 4.0%

(a) Sales tax figure represents conservative estimate shown in Table 4.

(b) Transient Occupancy Tax figure represents average estimate shown in Table 13.

Source: BAE, 2015.

Net Present Value Calculation of Net Fiscal Impact

The first column of Table 5 provides the cumulative fiscal impact of the Proposed Revised Project over a 20-year timeframe in current (inflated) dollars. As shown, the cumulative net fiscal impact of the Proposed Revised Project during this period would be approximately \$21.4 million under Scenario 1 and approximately \$24.7 million under Scenario 2. Column 2 shows the NPV of the net fiscal impacts of the Proposed Revised Project over the 20-year timeframe. Based on a discount rate of 4 percent, the NPV under Scenario 1 would be approximately \$11.4 million. Under Scenario 2, the NPV would be approximately \$13.8 million.

Table 5: Total Net Fiscal Impact and Net Present Value of Fiscal Impacts to General Fund, Proposed Revised Project and Approved Project, 2015-2035

	Total Net Impact, 2015 Dollars	Net Present Value of Total Net Impact, 2015 Dollars (a)
Scenario 1		
Revenues	\$52,744,400	\$30,031,943
Expenditures	(\$31,330,900)	(\$18,645,975)
Net Fiscal Impact	\$21,413,500	\$11,385,968
Scenario 2		
Revenues	\$56,003,000	\$32,352,412
Expenditures	(\$31,253,800)	(\$18,581,867)
Net Fiscal Impact	\$24,749,200	\$13,770,544
Approved Project		
Revenues	\$60,440,000	\$35,835,991
Expenditures	(\$31,148,100)	(\$18,485,000)
Net Fiscal Impact	\$29,291,900	\$17,350,991

Note:

(a) Discount rate used for municipal financial analysis:
4.0%

Source: BAE, 2017.

Table 5 also shows the total net fiscal impact and NPV for the Approved Project. As shown, the Approved Project would have a higher cumulative fiscal impact (approximately \$29.3 million) and NPV (approximately \$17.4 million). The NPV for the Approved Project is approximately \$6.0 million higher than the NPV for Scenario 1 and approximately \$3.6 million higher than the NPV for Scenario 2. The higher cumulative fiscal impact and NPV from the Approved Project are due to the earlier anticipated hotel construction date for the Approved Project, which results in TOT and other hotel revenues over a longer period prior to 2035.

Summary

The Approved Project and the Proposed Revised Project would both generate a net fiscal surplus of approximately \$1.2 million annually. This fiscal surplus would be largely dependent on the successful development and operation of the hotel. The hotel would generate approximately \$1.2 million per year in TOT revenue, which is approximately equal to the total

fiscal surplus generated by both the Approved Project or the Proposed Revised Project. Since the Proposed Revised Project anticipates a later hotel construction date than the Approved Project, the Proposed Revised Project could take up to five years longer to generate a net positive fiscal impact to the City.