



## REGULAR MEETING AGENDA

**Date:** 12/9/2019  
**Time:** 7:00 p.m.  
**City Council Chambers**  
**701 Laurel St., Menlo Park, CA 94025**

**A. Call To Order**

**B. Roll Call**

**C. Reports and Announcements**

Under “Reports and Announcements,” staff and Commission members may communicate general information of interest regarding matters within the jurisdiction of the Commission. No Commission discussion or action can occur on any of the presented items.

**D. Public Comment**

Under “Public Comment,” the public may address the Commission on any subject not listed on the agenda, and items listed under Consent Calendar. Each speaker may address the Commission once under Public Comment for a limit of three minutes. Please clearly state your name and address or political jurisdiction in which you live. The Commission cannot act on items not listed on the agenda and, therefore, the Commission cannot respond to non-agenda issues brought up under Public Comment other than to provide general information.

**E. Consent Calendar**

- E1. Approval of minutes from the November 18, 2019, Planning Commission meeting. ([Attachment](#))

**F. Public Hearing**

- F1. Use Permit and Variances/Hao Zhong/276 Hedge Road:  
Request for a use permit to add a second floor, as well as conduct interior modifications, to a single-family residence that would exceed 50 percent of the replacement value of the existing nonconforming structure in a 12-month period. The proposal would also exceed 50 percent of the existing floor area and is considered equivalent to a new structure. The subject parcel is a substandard lot with regard to minimum lot width and lot depth in the R-1-U (Single-Family Urban) zoning district. The proposal includes variance requests to construct a portion of the second-story addition within the required rear setback (on the functional left side) and for the structure to partially encroach into the daylight plane (on the right side). ([Staff Report #19-086-PC](#))
- F2. Architectural Control, Variance and Major Subdivision/Vasile Oros/706-716 Santa Cruz Avenue:  
Request for architectural control for the demolition of an existing commercial building and the construction of a new three-story mixed use building with below ground parking, retail and parking

on the first floor, office on the second floor, and office and four residential units on the third floor in the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district. The project includes a request for a major subdivision to create a vesting tentative map not to exceed four residential condominium units and one commercial area, with rights reserved for up to ten commercial condominiums, and a Below Market Rate (BMR) housing agreement for payment of an in-lieu fee in compliance with the City's below market rate housing program. The project also includes a variance request for skylights on the third floor to exceed the 38-foot height limit. Removal of one on-street parking space on Chestnut Street would be required to meet fire access requirements. As part of the proposed project, two heritage trees would be removed: one on-site tree located in the parking lot at the rear of the property and one street tree on Chestnut Street. The Planning Commission will serve as a recommending body and the City Council will be the final decision making body and take action on the proposed project at a future meeting date. ([Staff Report #19-087-PC](#))

## **G. Informational Items**

G1. Future Planning Commission Meeting Schedule – The upcoming Planning Commission meetings are listed here, for reference. No action will be taken on the meeting schedule, although individual Commissioners may notify staff of planned absences.

- Regular Meeting: December 16, 2019
- Regular Meeting: January 13, 2020
- Regular Meeting: January 27, 2020

## **H. Adjournment**

At every Regular Meeting of the Commission, in addition to the Public Comment period where the public shall have the right to address the Commission on any matters of public interest not listed on the agenda, members of the public have the right to directly address the Commission on any item listed on the agenda at a time designated by the Chair, either before or during the Commission's consideration of the item.

At every Special Meeting of the Commission, members of the public have the right to directly address the Commission on any item listed on the agenda at a time designated by the Chair, either before or during consideration of the item.

For appeal hearings, appellant and applicant shall each have 10 minutes for presentations.

If you challenge any of the items listed on this agenda in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City of Menlo Park at, or prior to, the public hearing.

Any writing that is distributed to a majority of the Commission by any person in connection with an agenda item is a public record (subject to any exemption under the Public Records Act) and is available for inspection at the City Clerk's Office, 701 Laurel St., Menlo Park, CA 94025 during regular business hours.

Persons with disabilities, who require auxiliary aids or services in attending or participating in Commission meetings, may call the City Clerk's Office at 650-330-6620.

Agendas are posted in accordance with Government Code §54954.2(a) or §54956. Members of the public can view electronic agendas and staff reports by accessing the City website at [menlopark.org/agenda](http://menlopark.org/agenda) and can receive email notification of agenda and staff report postings by subscribing to the "Notify Me" service at [menlopark.org/notifyme](http://menlopark.org/notifyme). Agendas and staff reports may also be obtained by contacting City Clerk at 650-330-6620. (Posted: 12/04/2019)



## REGULAR MEETING MINUTES - DRAFT

**Date:** 11/18/2019  
**Time:** 7:00 p.m.  
**City Council Chambers**  
**701 Laurel St., Menlo Park, CA 94025**

### A. Call To Order

Chair Andrew Barnes called the meeting to order at 7:00 p.m.

### B. Roll Call

Present: Andrew Barnes (Chair), Chris DeCardy, Michael Doran, Larry Kahle, Camille Kennedy, Henry Riggs (Vice Chair), Michele Tate

Staff: Ori Paz, Associate Planner; Matt Pruter, Associate Planner; Corinna Sandmeier, Senior Planner; Tom Smith, Senior Planner

### C. Reports and Announcements

Senior Planner Corinna Sandmeier reported that the introduction of the Transportation Impact Fee Ordinance would be heard at the City Council's November 19, 2019 meeting as well as a variance from the subdivision ordinance to reduce the front setback for 180 Elliott Drive. She said that variance did not require Planning Commission review as the project proposal adhered to the Zoning Ordinance.

Commissioner Chris DeCardy noted the Environmental Quality Commission (EQC) had prepared a document on greenhouse gas reduction targets and it looked like the EQC was going to propose it to the City Council as a 2030 target. He said the document had a series of recommendations that he thought directly intersected with the Planning Commission's work. He suggested it would be valuable for the Commission to have an interaction or study session to hear from the EQC about their plans and for them to get feedback from the Planning Commissioners regarding any questions, opportunities or concerns. He said the draft document proposed 11 specific strategies, including 100% carbon free electricity, completely electrifying all existing buildings in the City starting with City facilities, eliminating carbon emissions from construction, electrifying all municipal buildings, and preparing the City for adverse impacts of climate change through adaptation and resiliency measures in buildings. He said he would provide the document to staff for the Commission to learn more about it.

### D. Public Comment

There was none.

## E. Consent Calendar

Replying to Chair Barnes' question, Commissioner Larry Kahle asked if he should abstain from approval of E1 as he had not been on the Commission then. Chair Barnes asked for revisions and/or motions to approve individually on items E1, E2, and E3.

- E1. Approval of revised transcript from the October 7, 2019, (1350-1390 Willow Road, 925-1098 Hamilton Avenue, and 1005-1275 Hamilton Court, Proposed Willow Village Master Plan Project Environmental Impact Report Scoping Hearing), Planning Commission meeting. ([Attachment](#))

**ACTION:** Motion and second (Henry Riggs/Michele Tate) to approve Item E1 as presented; passes 6-0-1 with Commissioner Kahle abstaining.

- E2. Approval of minutes from the October 21, 2019, Planning Commission meeting. ([Attachment](#))

**ACTION:** Motion and second (DeCardy/Tate) to approve Item E2 as presented; passes 6-0-1 with Commissioner Camille Kennedy abstaining.

- E3. Approval of minutes from the November 4, 2019, Planning Commission meeting. ([Attachment](#))

**ACTION:** Motion and second (Riggs/Kennedy) to approve Item E3 as presented; passes 6-0-1 with Commissioner DeCardy abstaining.

Recognized by the Chair, Commissioner Tate clarified with Planner Sandmeier that it was not necessary for Commissioners to abstain from voting on minutes for meetings they had not attended.

## F. Public Hearing

- F1. Use Permit/Danning Jiang/203 Haight Street:  
Request for a use permit to partially demolish, construct a first-floor addition, and perform interior modifications to an existing single-story, single-family residence in the R-1-U (Single Family Urban Residential) zoning district. The existing residence is nonconforming with respect to the required left side setback and the daylight plane along both sides, and the proposed new work value would exceed 75 percent of the existing value. ([Staff Report #19-082-PC](#))

Staff Comment: Associate Planner Matt Pruter said he had no updates to the written report.

Applicant Presentation: Amaranta Hernandez, applicant, referred to the property owner, and said they were trying to keep the front façade as simple as possible and would add to the exterior of the structure at the rear. She said they would remove the wood siding on the front and keep the stucco and brick veneer for a more modern look but in keeping with the character of the neighborhood. She said the existing two-bedroom, one-bathroom home would be expanded to four bedrooms and three bathrooms.

Commissioner Kahle asked about the roof pitch for the addition at 3 ½ by 12 as that did not match the existing 4 by 12 roof pitch. Ms. Hernandez said the owner requested that the ceiling height be nine feet for the rear addition, and they did not want the back roof to be above the front roof creating an awkward roof line. Commissioner Kahle noted the removal of the wood siding from the



front and asked if there were other reasons to do that beyond simplifying the facade. Ms. Hernandez said some of the siding was in bad shape and most of the house was stucco. She said only one portion of the front façade was currently wood siding.

Chair Barnes opened the public hearing and closed it as there were no speakers.

Commission Comment: Commissioner Kahle said he supported project approval noting the one-story addition. He said he had reluctance about the dominance of stucco and use of vinyl windows, but the applicants were keeping the brick veneer even if it was somewhat hidden by landscaping. He said he thought the project was pretty straight forward and moved to approve. Commissioner Riggs seconded the motion.

**ACTION:** Motion and second (Kahle/Riggs to approve Item F1 as recommended in the staff report; passes 7-0.

1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Approve the use permit subject to the following **standard** conditions:
  - a. Development of the project shall be substantially in conformance with the plans prepared by T Square Consulting Group, Inc., consisting of 17 plan sheets, dated received October 31, 2019, and approved by the Planning Commission on November 18, 2019, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
  - b. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.
  - c. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
  - d. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.

- e. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division.
- f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits.
- g. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance.

F2. Use Permit/Magda Bach/201 Ravenswood Avenue:

Request for a use permit to operate a child daycare center within an existing church in the R-1-S (Single Family Suburban Residential) zoning district. At maximum capacity, the daycare center would have 60 children, ages two to six years old, and six staff members. The daycare center would operate between the hours of 7:00 a.m. and 6:30 p.m., Monday through Friday, and be independent of the church, which would continue operations outside the proposed daycare hours. ([Staff Report #19-083-PC](#))

Staff Report: Associate Planner Ori Paz said staff had no updates to the written report.

Application Presentation: Magda Bach, applicant, said that they had operated Alpha Kids child daycare centers for the past 14 years in San Francisco and Marin County, and were proposing to operate a third in Menlo Park as presented. She said they were not doing any structural work to the facility. Natella Stern, co-applicant, said they would be using the Sunday school building, which was designed with classrooms in place.

Commissioner Riggs said typically there would be children height toilets and sinks for a daycare facility. He said for 60 children around three toilets and two sinks in the bathroom and a sink for the classroom would be required. He noted the existing multi-stalled restrooms as those would not be at child height or visible from the play area. He asked how they solved for those state requirements.

Ms. Stern said they were allowed to use step stools for toilets and for sinks as well as a smaller size seat for the toilets. She said staff accompanied the children to the bathrooms.

Commissioner Kahle asked if there was enough room for 60 children. Ms. Bach clarified this facility would have 45 children. Ms. Stern said if there was a future increase in the number of children there was space. Ms. Bach said each toilet qualified for 15 children.

Commissioner Riggs said the staff report indicated they would use part of the church property for an outside play area and asked about the fencing. Ms. Bach said there was no construction being done and they would use portable fencing.

Chair Barnes said the staff report indicated 45 children with the potentiality of expanding to 60 children. He asked if there was a hard cap of 45 children and whether use permit revision would be needed should the facility want to add more children. Planner Paz said the 60 children cap had to

do with the trip count. He said analysis done with Transportation Division staff for preparation of the TDM plan had identified the number of trips that would keep the project below Transportation Impact Analysis (TIA) threshold. He said during peak hours the intersection at Middlefield Road and Ravenswood Avenue could be quite impacted. He said they looked at 60 children as the hard cap for the analysis. He said anything beyond that would need additional work by a transportation consultant. He said state licensing had requirements per square foot for community care licenses and those had to do with the number of students as well. He said that was handled through conditions of approval for the use permit. He said if the operators wanted to expand beyond 45 children, they would have to allocate more space within the church for that, which would change the Transportation Impact Fee (TIF). He said that additional review by the Planning Commission for expansion from 45 to 60 children was not necessary. He said if the use went beyond 60 children enrollment or to a subleasing agreement that would need to be seen by the Planning Commission as a use permit revision.

Chair Barnes asked about the portable fencing. Ms. Stern said it would be four feet in height as required and would have wheels on it for removal or something that could be folded for storage. She said they used this type of fencing for their daycare at a church in San Francisco.

Chair Barnes opened the public hearing and closed it as there were no speakers.

Commission Comment: Commissioner Riggs asked about staggering drop-offs and hours of operation.

Ms. Bach said operating hours were 7 a.m. to 6:30 p.m. She asked Planner Paz about the drop off during rush hour. Planner Paz said the TDM had windows for drop off from 7 to 8 a.m., 9 to 9:30 a.m., and an allocation of eight families that could drop off between 8 to 9 a.m. Ms. Bach said enrollment would be based on the drop offs so they could control traffic into the site.

Commissioner Kahle said he appreciated the traffic management would be monitored by an outside consultant for five years. He said in general daycare was very much needed in the area. He moved to approve as recommended in the staff report.

Chair Barnes said he was disappointed that the use was not larger than proposed as childcare was an acute need in the community. He said from experience it was extremely difficult to find sites for childcare facilities due to economic, zoning, and circulation considerations. He said he hoped over time this use would grow. He commented on the amount of traffic generated by Menlo Park families having to use childcare in Redwood City, or Mountain View or across 101 as compared to a site within the City. He seconded Commissioner Kahle's motion to approve.

**ACTION:** Motion and second (Kahle/Barnes) to approve Item F2 as recommended in the staff report; passes 7-0.

1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.

2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use would not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Approve the use permit subject to the following **standard** conditions:
  - a. Development of the project shall be substantially in conformance with the plans prepared by Xie Associates, Inc. consisting of six plan sheets, dated received November 13, 2019, and the project description letter received November 13, 2019, and approved by the Planning Commission on November 18, 2019, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
  - b. The applicant shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.
  - c. The applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
  - d. The applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
4. Approve the use permit subject to the follow **project specific** conditions
  - a. Prior to operating at the subject property, the applicant shall obtain a business license from the City of Menlo Park and demonstrate they have secured the appropriate state and county licensing.
  - b. Prior to business license issuance, the applicant shall pay the Transportation Impact Fee (TIF) in compliance with Chapter 13.26 of the Municipal Code. The current estimated TIF is \$11,753.46 based on the proposed 2,346-square foot childcare area at a fee rate of \$5.01 per square foot. Please note the City is currently updating its TIF fee schedule, tentatively scheduled to be adopted by early 2020. If payment is made after adoption of the new fee schedule, the fee shall be calculated per the new fee schedule.
  - c. The applicant shall notify the Planning Division within 30 days if the square footage used for child daycare is expanded beyond 2,346 square feet, for the calculation of additional TIF per the fee schedule current at that time. After being notified of the additional TIF, the applicant shall then have 30 days to pay.
  - d. Prior to business license issuance, the applicant shall submit specific information on the type, style, material, and height of both the permanent and temporary fencing, subject to review and approval of the Planning Division.

- e. Prior to business license issuance the applicant shall submit revised plans clarifying the extent of their property boundary, noting the dimensions of the property boundaries, identifying the locations, heights and materials of both the proposed permanent and temporary fencing, and removing unused labels and legend items, subject to review and approval of the Planning Division.
  - f. If the proposed outdoor play area is not on the subject property, the applicant shall be required to provide approval from the neighboring property owner to landscape and use the outdoor play area in the proposed location, as necessary, or revise the proposed location subject to review and approval by the Planning Division.
5. Approve the use permit revision subject to the following **ongoing, project-specific** conditions:
- a. All regular student instruction and school activities shall operate within the maximum enrollment of 60 students and six staff members. The applicant must obtain a Special Event permit for any major events outside of these regular activities. Any increase in student enrollment, staff numbers, and/or changes to the hours of operation shall require approval of a use permit revision by the Planning Commission.
  - b. The applicant shall submit a copy of the student enrollment roster to the Planning Division for the purposes of verifying the student enrollment. The roster shall be submitted annually, with the first roster submitted three months after the issuance of the business license. The Planning Division shall return the roster to the school after completion of review. The City shall not make copies of the roster or disseminate any information from the roster to the public to the extent allowed by law.
  - c. Subleasing of the site, or allowing use of the site for non-school or church related activities, shall require approval of a use permit revision by the Planning Commission.
  - d. The school shall generate no more than 16 vehicle trips during the morning peak hour (8:00 a.m. – 9:00 a.m.) and no more than 16 vehicle trips during the evening peak hour (4:45 p.m. – 5:45 p.m.). Vehicle trips include, but are not limited to: student drop-off/pick-up trips, staff trips, service and goods delivery trips, etc.
  - e. The applicant shall follow the Transportation Demand Management (TDM) program prepared by DKS Associates, dated May 30, 2019 and approved by a representative of SRI International, the property owner of the adjacent parking lot, identifying parking for the child daycare use, feasible TDM measures to reduce peak hour and daily new vehicle trips. If the ownership of the parking lot changes or SRI International rescinds their approval of its use, the applicant shall be required to secure authorization from the new owner or submit a revised TDM plan outlining a new parking and circulation plan, subject to review and approval by the Transportation and Planning Divisions. To the greatest extent possible, the applicant shall promote and encourage families to carpool to school. If necessary, the applicant shall implement a carpool or bus/shuttle program and monitor its progress. If a bus/shuttle program is to be developed, the applicant shall provide proposed bus stop locations and schedule for approval by the Transportation and Planning Divisions. The Transportation and Planning Divisions may request additional supplemental information regarding the bus/shuttle program for a comprehensive view.

- f. The applicant shall retain a transportation/traffic consultant, to be approved by the City, to monitor the peak hour trip caps by surveying all site driveways over three (3) "typical" weekdays (Tuesday, Wednesday, or Thursday) in October-November of each year. "Typical" weekdays shall exclude days immediately before or after holidays or long weekends, days of holiday periods, and days of inclement weather conditions. The trip count shall be the average of the three weekday counts and shall include vehicle, bicycle, and walk trips. If requested, the applicant shall provide evidence of student residency to support the surveyed travel mode splits.

The survey results shall be submitted to the Transportation Division in a report for review. The City will work with the consultant to finalize the scope and reserves the right to modify it yearly, if necessary. Note, the City may conduct its own additional monitoring, at the applicant's expense, if desired.

- g. If the monitoring shows that one of the peak hour trip caps is exceeded, the applicant will have 60 days to prepare a revised TDM program that incorporates additional TDM measures, and an additional 60 days to implement the revised TDM program in order to bring the site into compliance with the trip cap. A subsequent monitoring will be conducted after 60 days. If the subsequent monitoring indicates that the site still exceeds the trip cap, then the applicant shall be required to reduce student enrollment accordingly to bring the site into compliance with the trip cap. Non-compliance may also result in review of the use permit by the Planning Commission. Any proposed changes to the trip cap and/or enrollment cap will require a revision to the use permit.
- h. During normal operation of the school, school-related vehicles are not permitted to park on any public street. During school events, the applicant shall minimize any parking overflow into the surrounding neighborhood.
- i. All student drop-off/pick-up shall occur within the school site's parking lot, or designated loading and unloading zones as specified on plans approved by the City's Transportation Division.
- j. The applicant shall manage the drop-off/pick-up and parking demand so that school-related vehicles will not overflow into the surrounding neighborhood. The applicant shall communicate in writing to all parents of enrolled students the designated areas for drop-off/pick-up and parking. Consultant and City staff observations and resident complaints will be used to determine if there is neighborhood impact. If an overflow of school-related vehicles is found to occur in the neighborhood, including drop-off/pick-up or parking vehicles, then the applicant will have 30 days to implement measures to reduce the school demand and prevent overflow into the neighborhood. If overflow demand continues to occur in the neighborhood after the 30 days, the applicant will need to reduce student enrollment in order to reduce the demand. Non-compliance may also result in review of the use permit by the Planning Commission.
- k. No outdoor sound amplification shall be permitted.



- I. If after five (5) consecutive years of monitoring, no violations of the trip cap and vehicle demand overflow into the neighborhood have occurred, monitoring can be discontinued. Monitoring may be resumed at any time if the City receives complaints of the school regarding the trip cap and vehicle demand overflow. After a complaint has been received, the City will evaluate whether a potential violation has occurred, and the Community Development Director shall have the discretion to resume the monitoring. If monitoring is deemed warranted, the City will notify the applicant of the determination at least one week before initiating the monitoring program, at the applicant's expense.
- m. The Community Development Director shall review any complaints received by the City regarding operation of the school. The Community Development Director and her/his designee shall work with the applicant and the neighbors to try to resolve such complaints, when possible. The Community Development Director shall have the discretion to bring complaints to the Planning Commission for review.

F3. Use Permit/John Conway/1200 El Camino Real:

Request for a use permit to allow the sale of beer and wine for off-premises consumption at an existing convenience store, which is associated with an existing service station in the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district. As part of the review, the Planning Commission will need to determine whether the sale of alcohol at this location serves a public convenience or necessity, in accordance with the requirements of the State Department of Alcoholic Beverage Control (ABC). ([Staff Report #19-084-PC](#))

Staff Comment: Senior Planner Sandmeier said that condition of approval 5.a. was updated to clarify that a typical bottle of wine of 750 ml would be permitted for sale although the condition prohibited other types of single servings of beer and wine. She said the staff report had an update. She said in the census track there were two other off sale locations listed as Draeger's and Trader Joe's in the staff report. She said that was incorrect and the correct locations were the 7-11 and Menlo Fine Wine and Spirits.

Questions of Staff: Chair Barnes said oversaturation was referenced in the staff report as the reason the ABC had indicated that a third off-sale license would mean the census track was overconcentrated. He asked if that still held and whether it was staff's understanding that overconcentration would not be case if the development currently in the pipeline occurred. Planner Sandmeier said with the 7-11 and Menlo Fine Wine and Spirits in that track, having a third off-sale location would mean that it was overconcentrated. She said the ABC would make any other determination based on buildout when the census was redone.

Commissioner Riggs referred to a comment about supporting questionable behavior. He referred to the 7-11 as the most similar and approximate off-sale site to this one being requested and asked if they had any reports from either the police or community patrol about behavior issues in and around the 7-11. Planner Sandmeier said they did not get specific information on the 7-11 but had forwarded this application to the Police Department with a request to let staff know if they had any concerns. She said the Police Department did not express any concerns. Commissioner Riggs asked if the Police or Planning staff or others had established any relationship between the sale of packaged and enclosed alcohol with the homeless population. Planner Sandmeier said she did not think that had been looked at specifically.



Applicant Presentation: Bill Gutsell, Senior Associate Architect, CJW Architecture, Portola Valley, said the application was for a use permit to allow for the off-sale of beer and wine at Menlo Park Chevron. He noted a newly constructed hotel, a renovated motel, and residential development adding about 185 units in the area would add foot traffic in the area. He said they requested the Planning Commission find the staff report determination that the sale of alcoholic beverages from a Menlo Park location would serve as a public convenience and necessity and approve the use permit revision to allow off-sale beer and wine sales. He said use permit approval was required in conjunction with an application to the ABC for the Type 20 off-sale beer and wine license. He said beer and wine sale and display would occupy approximately 30% to 40% of the existing wall cooler area and amounted to two coolers out of the five coolers in the convenience store area. He said for staff training the property owner had access to Chevron University that provided tutorial videos on many consumer service subjects, including age-restricted alcohol sales. He said the ABC also provided additional material for licensees and their employees. He said Menlo Park Chevron had successfully utilized similar employee training in ID scanning for their existing tobacco sales. He said Menlo Chevron periodically received enforcement agency notice that underage tobacco purchase decoys had visited the location with the intent of making underage tobacco purchases. He said Menlo Park Chevron had been successful in restricting underage tobacco sales and had received no citations or violations for underage tobacco sales. He said the property owner had personally walked the surrounding area and conducted public outreach informing commercial and residential neighbors about his intent to request approval for a beer and wine off-sale license. He distributed flyers introducing himself and his intent and encountered two people, who were supportive of the request for the beer and wine off-sale license. He said today planning staff received two comments, one negative and one positive. He said that the property owner had not received any other comments.

Mr. Gutsell said regarding the census track areas he believed Planner Sandmeier clarified that Menlo Park Chevron was in a census area that included two existing licensed facilities including Menlo Fine Wine and Spirits and 7-11. He said Beltramo's that used to be in the same track was no longer operating. He said the ABC allowed a certain number of off-sale licenses within a track based on population. He said they would add one license to an area that had a minimally allowable of two licenses. He said ABC did not necessarily preclude the issuance of a license if there was a concentration. He said when the application was submitted to ABC, they conduct their own lengthy investigation and based their approval of a license on a number of factors, including, but not limited to, the business applying, its location, neighbor complaints, and police activity in the area. He said they have had no neighbor complaints or police activity for the site. He said the ABC determined based on their investigation whether to limit the hours of operation and single-serve units and decided if it was warranted to issue a license with those restrictions. He said in his discussions with the ABC he learned there was no Type 20 license moratorium in this track or in the County.

Mr. Gutsell said the staff recommendation to prohibit single-serve beer or wine was presented to them about a week and a half earlier in an email but with no indication for the reason that recommendation was being made as condition 5. He said Menlo Park Chevron took exception to that recommendation and did not want to have their single-serve container sales prohibited. He said he and the business owner had visited a number of the facilities in town and none of them had any restriction on single sales. He said he asked why this condition was being recommended and was told that the primary use of the site was a service station, which staff did not consider compatible with the single-serve sale of beer and wine. He asked the Commission to consider why single-serve alcohol sales were not compatible with a service station use. He said they thought restricting beer bottle size to 40-ounces when 48-ounce bottles were available for sale was

unreasonable. He said people buy the larger size and like a 750 ml bottle of wine take it home for consumption over a period of time or to entertain company. He said Menlo Park Chevron respectfully requested to sell single-serve units without the 40-ounce restriction shown in Attachment A and to remove recommendation 5 from the actions.

Commissioner DeCardy asked what beverage products would be displaced in the coolers for the alcohol products. John Conway, Menlo Park Chevron, said they would ask two vendors to remove products to make room for the beer and wine products. Commissioner DeCardy asked if someone who currently purchases there would have the opportunity to continue to get the same mix of products in addition to beer and wine products. Mr. Conway said the mix would be there. Commissioner DeCardy asked if he had a percentage of sales for foot traffic as opposed to auto traffic. Mr. Conway said he did not have a way to track that.

Chair Barnes asked staff to speak to the project-specific conditions of no single alcohol sales. Planner Sandmeier said staff did not think it was compatible with the automotive use. She said the primary use was as a service station and fueling station. She said the kind of secondary use was the convenience store with the proposal to sell alcoholic beverages. She said they felt it was more appropriate to not sell single servings. Chair Barnes asked if within that finding there were considerations of public good and public safety or public drunkenness and abuse of alcohol. Planner Sandmeier said it was addressing those kinds of concerns.

Chair Barnes asked the applicant why single-serve alcohol sales were important. Mr. Conway said they were seeing a real decline in automotive repair due to electric cars and a whole new fleet of cars that did not need service at the level needed before. He said in this affluent area people bought new cars regularly and dealerships provided them with service bonuses. He said they had lost a lot of market share. He said recently he spoke with competition in the area and the consensus was that automotive repair was on the decline. He said each month demand for automotive service was lessening and he was looking for another profit center for business viability. Chair Barnes asked about the economic driver behind wanting single-serve beer and wine sales. Mr. Conway said he saw that someone buying a single beer would also buy other things such as a lottery ticket and a snack.

Commissioner Kennedy said the single-serve restriction sounded subjective noting her experience buying coffee at 7-11 before a Commission meeting and seeing numerous purchases of single-serve beers by people who then got on the train.

Mr. Gutgsell said he had seen a proliferation of single-serve alcohol drink sales with an increase in the number of microbreweries. He said this was very evident at Trader Joe's, where there was a bin of partial six-packs of different beers allowing a person to create a six-pack with a variety of single-serve beers.

Chair Barnes opened the public hearing and closed it as there were no speakers.

Commission Comment: Commissioner Michael Doran said there was a trend towards single-serving wines and wine cans, and smaller bottles. He said he thought they all had this impression that single-serves would lead to drinking in the parking lot and undesirable behavior. He said he thought there actually was a trend towards moderation, which he thought was being reflected in the packaging sizes. He said people might be in relationships where one person drank, and the other did not, and single-serve sales were great for couples like that. He said he could support the

application. He said if you considered the volume of alcohol that Beltramo's sold when it was there compared to the volume likely to be sold from two refrigerator cases here there was no comparison. He said he did not have a problem with concentration or with the sale of single-serve containers.

Commissioner Tate said she agreed with Commissioner Doran and she did not see a problem with single-serve sales. She said she was fine with the rest of the application.

Commissioner Riggs said he concurred with the last couple of speakers. He said he did not see the purchase of a single-serve alcohol beverage personified as an alcoholic sitting on a bench drinking. He said it did not seem there was a clear argument against single-serve units. He moved to approve the use permit and remove the condition regarding single-serve unit sales. Commissioner Kennedy seconded the motion.

**ACTION:** Motion and second (Riggs/Kennedy) to approve Item F3 with the following modification; passes 7-0.

1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.
2. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
3. Make a finding that the proposed use would serve a public convenience because the proposed use would allow new and existing, residents, visitors and employees of the immediate vicinity a convenient location to purchase beer and wine.
4. Approve the use permit subject to the following **standard** conditions:
  - a. Development of the project shall be substantially in conformance with the plans prepared by CJ W Architecture consisting of 4 plan sheets, dated received November 12, 2019, and the project description letter dated September 10, 2019, approved by the Planning Commission on November 18, 2019, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
  - b. The applicant shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project.
  - c. The applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
- ~~5. Approve the use permit subject to the following **project specific** condition:
  - ~~a. Sales of single servings of beer and wine (including 40 ounce bottles of beer) are prohibited, but the sale of typical 750 ml bottles of wine is permitted.~~~~

## **G. Regular Business**

- G1. One Year Review/Don Fox, WineBank/1320-A Willow Road:  
Request that the Planning Commission conduct a one-year review of the use permit revision to increase the signage and advertising permitted, adjust the minimum prices of wines available for sale and consumption on-site, provide daily wine tastings, and host up to 150 wine tasting events per year at an existing wine storage facility in the LS (Life Sciences) zoning district. ([Staff Report #19-085-PC](#)) ***Continued from the PC meeting of November 4, 2019***

Staff Comment: Senior Planner Tom Smith said he had no supplement to the written report. He said no action was required and this was a one-year check-in as stipulated as part of the use permit approval. He said staff contacted the Police Department and ABC regarding the use and there had been no reports of any incidences or concerns with the operations at the site. He said staff continued to be supportive of the WineBank operations.

Questions of Staff: Commissioner DeCardy asked about condition 4.e and the history. Planner Smith said when the use permit was originally approved there had been a link between the Consumer Price Index (CPI) and the price of the wine as there was neighborhood concern that the facility be for fine wines only and then setting \$30 as the minimum price per bottle so it would continue to be a fine wine distributor.

Applicant Presentation: Don Fox, managing member of the WineBank, said the letter he sent outlined what had been happening at the WineBank over the past year. He said essentially, they tried to increase their wine tastings and sales program and had averaged two to three tastings per week. He said they have gotten some business from Facebook. He said their sales were okay but not great.

Chair Barnes opened for public comment and closed as there were no speakers.

Commission Comment: Chair Barnes thanked Mr. Fox for attending. He noted that the Planning Commission had a robust discussion the last time the WineBank was before them regarding appropriateness of the use and concern it might change neighborhood character. He said from the staff report it appeared that was not the case and it had been prudent to do this one-year review.

Commissioner Kennedy said she enjoyed reading that as hoped for employees of Facebook were a sizable portion of the WineBank's clientele. She said they walked, used the shuttle and typically were not driving. She said she remembered the robust conversation. She said it was nice that employees in that area were participating in the experiences being offered by the WineBank.

Commissioner Tate said she was pleased with the wine tasting business and that much of the business was coming from Facebook, however, she was a little challenged as there was a community there, and she as a wine drinker until a year or so ago had not been aware that the WineBank was open to the public. She said their advertising was a good thing, but she hoped they would not just target Facebook employees.

Mr. Fox said to say Facebook employees were the majority of their customers was untrue. He said most of their customers were existing wine storage customers, who were looking for fine wines that they used to get from places like Beltramo's. He said their storage facility was their primary profit center.

Commissioner Riggs asked if it was appropriate to say that this hearing served to seek a consensus of the Commission as to the positive outcome of the one-year review. He said the Commission's task was not well-defined, which was unclear for the public. Planner Smith said a one-year review was an unusual type of agenda item. He said since there was no action planned for the meeting, he did not think any sort of vote was needed. He said if there had been some issue that stood out that the Commission wanted to address that could be formalized to bring back to another meeting where a formal action might be taken.

Commissioner Riggs said he did not see any issues with the WineBank. He said it was working, was not a problem, and was an asset to the immediate industrial neighborhood, so he was glad it was there.

## **H. Informational Items**

### **H1. Future Planning Commission Meeting Schedule**

- Regular Meeting: December 9, 2019

Planner Sandmeier said the agenda for the December 9<sup>th</sup> meeting had not been finalized yet. She said it looked like it would have a couple of smaller items and one larger item, the 706 Santa Cruz Avenue project with ground floor retail, second and third floor office, and four residential units on the third floor.

Chair Barnes asked about the 2020 Commission meeting calendar. Planner Sandmeier said she thought the dates were set and would be made public soon.

- Regular Meeting: December 16, 2019

## **I. Adjournment**

Chair Barnes adjourned the meeting at 8:29 p.m.

Staff Liaison: Corinna Sandmeier, Senior Planner

Recording Secretary: Brenda Bennett



## STAFF REPORT

### Planning Commission

**Meeting Date:** 12/9/2019  
**Staff Report Number:** 19-086-PC

**Public Hearing:** Use Permit and Variance/Hao Zhong/276 Hedge Road

### Recommendation

Staff recommends that the Planning Commission approve a request for a use permit to add a second floor and conduct interior modifications to a single-family residence that would exceed 50 percent of the replacement value of the existing nonconforming structure in a 12-month period. The proposal would also exceed 50 percent of the existing floor area and is considered equivalent to a new structure. The subject parcel is located on a substandard lot with regard to lot width and depth in the R-1-U (Single-Family Urban) zoning district, at 276 Hedge Road. Staff recommends denial of variance requests to construct a portion of the second-story addition within the required rear setback (on the functional left side) and for the structure to partially encroach into the daylight plane (on the right side), with conditions of approval that would allow the project to proceed without additional Planning Commission review if revised to remove the variance elements. The recommended actions are included in Attachment A.

### Policy Issues

Each use permit and variance is considered individually. The Planning Commission should consider whether the required use permit and variance findings can be made for the proposal.

### Background

#### *Site location*

The project site is located at 276 Hedge Road, in the Flood Triangle neighborhood. The parcel is three-sided, and occupies the western corner of Hedge Road where it bends and intersects with Sheridan Drive. The site is located close to Flood Park and U.S. 101/Bayshore Freeway.

The other parcels in this area are likewise in the R-1-U zoning district, and are generally occupied by single-family residences. Most of the nearby residences are one-story in height, although a two-story house is located directly across the street and there are some others in the vicinity. Nearby styles vary, with bungalow/ranch residences the most common style.

### Analysis

#### *Project description*

The lot is three sided and oriented such that the front property line extends for the full length of the side adjacent to the right-of-way for Hedge Road. The functional left side is considered the rear property line, and the third side is considered an interior side at the right. The existing residence is considered to be a legal non-conforming structure, with a rear setback of 6.8 feet where a minimum of 20 feet is required, and



a right side setback of 2.8 feet where a minimum of five feet is required. The applicant is proposing to comprehensively renovate and expand the existing single-story, single-family residence. The expansion would include a small first floor addition and a new second floor. An existing nonconforming two-car garage features a width and depth that are each approximately six inches less than the required 20-foot by 20-foot clear space. The garage is proposed to be maintained. The Planning Commission may wish to consider whether the laundry appliances within the nonconforming garage area should be relocated from the garage as a condition of approval. The additions would conform to the setback requirements, with the exception of a small area at the back left corner on the second floor, for which the applicant is requesting a variance. This request, as well as a request to partially encroach into the daylight plane on the right side, are discussed in more detail in a following section. The existing nonconforming walls would remain, with the structural members retained.

The subject parcel's width and depth are substandard and use permit approval is required for the second-floor addition since all proposed additions would exceed 50 percent of the existing floor area and the proposal is considered a new structure. Use permit approval is also required due to the nonconforming nature of the existing structure and the fact that a separate work value threshold would be exceeded, as discussed further in the Valuation section.

The proposed residence would be a four-bedroom home with 3.5 bathrooms, with a typical layout of shared living spaces (and one bedroom) on the ground level, and the remaining bedrooms on the upper floor. Of particular note with regard to the development regulations:

- The second level would be relatively limited in size, at 31.9 percent of the maximum FAL, where the maximum allowed would be 50 percent;
- The second level would be provided entirely above the first, and the overall proposed building coverage for the lot, 25.8 percent would be limited relative to the 35 percent allowed;
- The subject site has only three property lines: one front bordering the curve of Hedge Road with a 20-foot front setback, a rear at the functional left side with a 20-foot rear setback, and an interior side separating this property from the neighboring lot to the right with a five-foot side setback.

A data table summarizing parcel and project attributes is included as Attachment C. The project plans and the applicant's project description letter are included as Attachments D and E, respectively.

### ***Design and materials***

The proposed project would remodel the current ranch style of the single-story residence to a two-story home in the modern farmhouse style. The primary exterior material would be board and batten siding, with composition shingle roofing. Windows would be wood, with painted trim, and the front elevation would feature a prominent entry beneath a gabled front porch. A front facing gable at the second story would also be featured, providing additional visual interest.

With regard to privacy, the second-floor, side-facing windows would feature a mix of sill heights, with the lowest at two feet, ten inches. On the right side, the only windows would overlook the front yard of the neighbor to the right and the heritage-size box elder (tree #1) would limit direct views of the house. On the left side, the windows would overlook the front yard of the neighboring property but the angle of the structure would prevent direct views of the home.

Overall, staff believes the design would represent an attractive and consistent aesthetic approach, and that its size and scale would be consistent with the neighboring properties, and the overall neighborhood.



### ***Valuation***

For projects involving existing nonconforming structures, the City uses standards established by the Building Division to calculate the replacement and new construction costs on which the use permit threshold is based. The City has determined that the replacement cost of the existing structure would be \$277,940, meaning that the applicant would be allowed to propose new construction and remodeling at this site totaling less than \$138,970 in any 12-month period without applying for a use permit. The City has determined that the value of the proposed work would be approximately \$351,408. Based on this estimate, the proposed project requires use permit approval by the Planning Commission, both for the addition of a second floor to a structure that is considered equivalent to a new structure on a substandard lot and for exceeding 50 percent of the replacement cost of the existing structure.

### ***Trees and landscaping***

The applicant has submitted an arborist report (Attachment F) detailing the species, size and conditions of the trees on or near the site. As part of the initial project review, the arborist report was enhanced with additional analysis and specificity. The site and its immediately adjacent areas feature four trees that are not particularly close to the construction areas and one neighboring, heritage-size box elder tree (tree #1), located immediately to the right of the side property line, that is close to the proposed work on the right side. The four non-heritage trees that will not be affected by the work include two small trees at the front of the subject site, a dogwood (tree #3) and a saucer magnolia (tree #4), as well as tree #2, a Chinese pistache street tree in front of the adjacent lot to the right, and a Crape Myrtle (tree #5) on the neighboring lot to the left. The proposed second floor addition would be within the dripline of the neighboring box elder, which would require some pruning. The arborist report includes recommendations from the project arborist relating to mitigations for the impacts to this tree and the extent of the pruning needed to accommodate the addition. Because less than 25 percent of the canopy would require pruning, a heritage tree pruning permit is not required. No heritage trees are proposed for removal.

### ***Variance***

As part of this proposal, the applicant is requesting a variance to construct a portion of the second floor within the required 20-foot rear yard setback, at the functional left side of the property. Additionally, the proposal includes a request for the second floor addition at the right side to intrude into the daylight plane. The applicant has provided three plan sheets illustrating a proposed configuration that would not require a variance. This was provided after staff relayed concerns that there appear to be reasonable and feasible alternatives to the requested variances that would preclude staff from making the variance findings.

The applicant has provided a variance request letter that is included as Attachment G. The required variance findings are evaluated below in succession. All findings are required to be addressed in order for a variance to be granted.

1. That a hardship peculiar to the property and not created by any act of the owner exists. In this context, personal, family or financial difficulties, loss of prospective profits and neighboring violations are not hardships justifying a variance. Further, a previous variance can never have set a precedent, for each case must be considered only on its individual merits;

The applicant relays a number of property aspects they believe to be unique, including:

- The shape of the parcel, notably that there is a front and rear and only one side, and therefore the lot width is severely substandard;

- The existing nonconformity in the rear setback, on the functional left side of the structure; and
- The need to meet the 20-foot rear setback with any new construction.

In staff's view, the three-sided shape that creates the substandard width and the existing nonconformity within the rear setback are unique hardships to this lot, although nonconforming structures generally are not uncommon in the City as a whole. However, as illustrated in the alternative exhibit sheets, there appears to be a reasonable and feasible reduced scope. In general, there appears to be sufficient room on the parcel for an alternate, compliant expansion scheme.

2. That such variance is necessary for the preservation and enjoyment of substantial property rights possessed by other conforming property in the same vicinity and that a variance, if granted, would not constitute a special privilege of the recipient not enjoyed by his/her neighbors;

The applicant states that the variance would be needed to prevent the proposed addition at the second story from crowding the front façade, and that the only other available area would be on the right side. They note that other, rectangular, lots typically enjoy a five-foot setback on either side that allow for a more balanced home and the fact that these lots have a smaller setback on either side would mean that granting the variance for the reduced setback would not constitute a special privilege.

From staff's perspective, the outcomes that would be gained by the variances are not clearly substantial property rights that can only be enjoyed with a variance. The requested variances would allow for a simplified roof structure and larger bedroom at the right, which may be desired features but which do not represent substantial property rights that need to be preserved. The construction of a second story could still be accomplished without the variances.

3. That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property; and

The applicant notes that the parallel orientation of the homes relative to the path of the sun, the angle of the lot and proposed placement of the second story addition would all help to alleviate impacts to the adjacent neighbors.

Staff agrees that the limited encroachments into the rear setback and the right-side daylight plane would ensure that light and air would not be impaired.

4. That the conditions upon which the requested variance is based would not be applicable, generally, to other property within the same zoning classification.

The applicant highlights the typical rectangular shape of the majority of the lots within the R-1-U zoning district, and notes that other sites would not have the particular constraints that the shape of their lot applies to them.

Similar to the discussion on findings #1 and 2, staff believes there are some unique aspects of the parcel, but that it is not clear that these conditions unduly constrain development, as evidenced in the alternate exhibit.

5. That the condition upon which the requested variance is based is an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process.

The property is not within any Specific Plan area. Hence, a finding regarding an unusual factor does not apply.

Due to the above factors, staff is recommending denial of the variance requests, and has included findings to that effect in the recommended actions. Condition 6a would require that the plans be revised to remove the variance elements, subject to staff review at the building permit stage. Staff will be prepared to assist the Planning Commission in crafting the findings at the December 9, 2019 meeting, if approval of the variance request(s) is supported by a majority of the Commissioners.

### **Correspondence**

The applicant has indicated they have spoken with their neighbors and that when they explained the layout, they were able to address the privacy concerns for the windows on the stairwell. A report of the outreach conducted and a copy of the letter that they sent out is included as Attachment H, correspondence. Staff has not received any items of correspondence regarding this proposal.

### **Conclusion**

Staff believes the design would represent an attractive and consistent aesthetic approach, and that its size and scale would be consistent with the neighboring properties, and the overall neighborhood. The location of the addition and orientation of the home on the lot would help limit potential privacy concerns, which is positive. Although the property has some unique attributes, staff does not believe that these represent a hardship with regard to compliance with the setback and daylight plane requirements. Similarly, staff believes that the remaining portion of the lot where the addition could be proposed without variances can be considered substantial property rights that negate the need for the proposed variances. Staff recommends that the Planning Commission approve the use permit and deny the two variance requests.

### **Impact on City Resources**

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

### **Environmental Review**

The project is categorically exempt under Class 1 (Section 15301, "Existing Facilities") of the current California Environmental Quality Act (CEQA) Guidelines.

### **Public Notice**

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

### **Appeal Period**

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

### **Attachments**

- A. Recommended Actions
- B. Location Map
- C. Data Table
- D. Project Plans
- E. Project Description Letter
- F. Arborist Report
- G. Variance Letter
- H. Correspondence

### **Disclaimer**

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public viewing at the Community Development Department.

### **Exhibits to Be Provided at Meeting**

None

Report prepared by:  
Ori Paz, Associate Planner

Report reviewed by:  
Corinna Sandmeier, Senior Planner

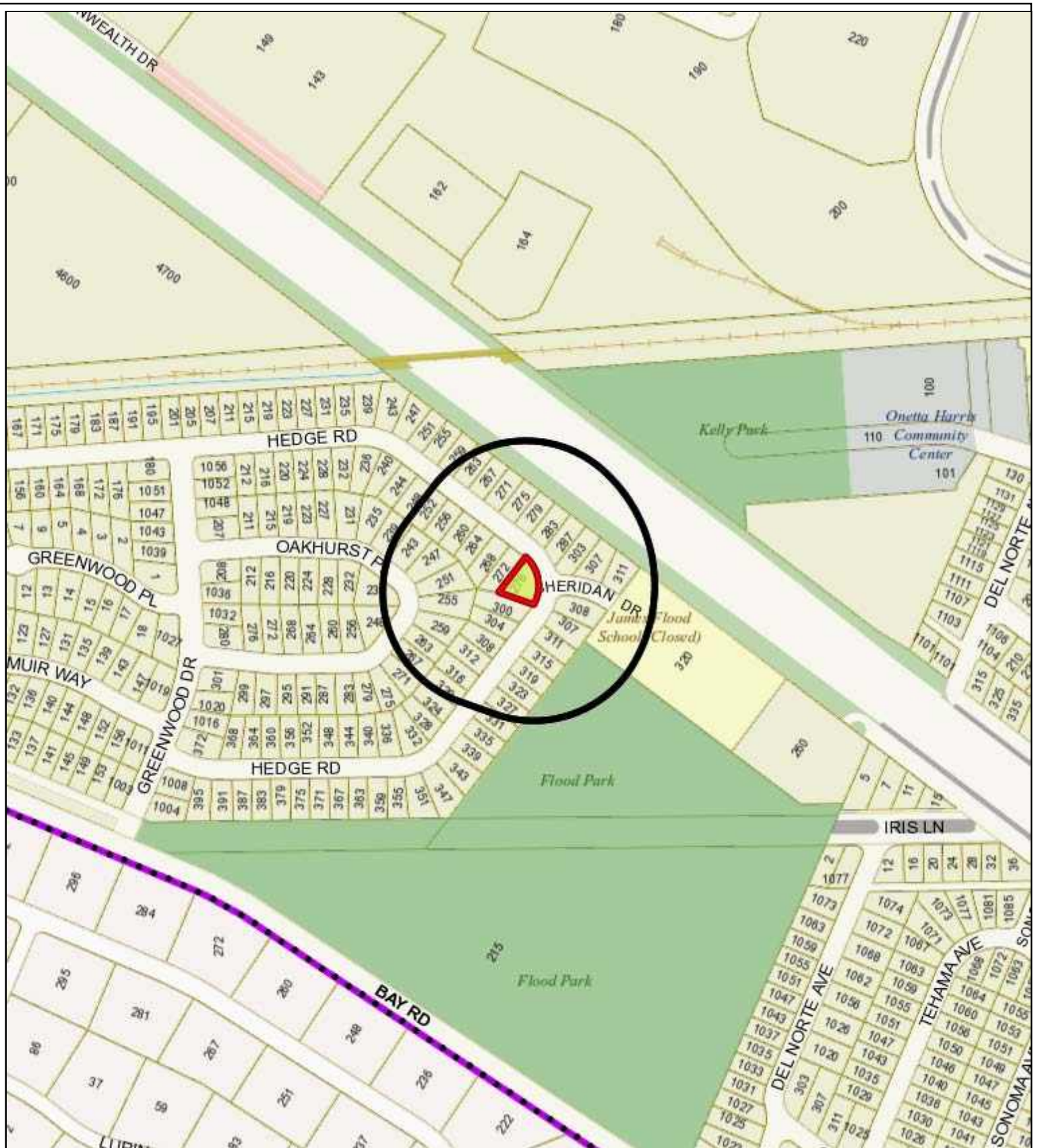
## 276 Hedge Road – Attachment A: Recommended Actions

<b>LOCATION:</b> 276 Hedge Road	<b>PROJECT NUMBER:</b> PLN2019-00086	<b>APPLICANT:</b> Hao Zhong	<b>OWNER:</b> Hao Zhong
<p><b>PROPOSAL:</b> Request for a use permit to add a second floor, as well as conduct interior modifications, to a single-family residence that would exceed 50 percent of the replacement value of the existing nonconforming structure in a 12-month period. The proposal would also exceed 50 percent of the existing floor area and is considered equivalent to a new structure. The subject parcel is a substandard lot with regard to minimum lot width and lot depth in the R-1-U (Single-Family Urban) zoning district. The proposal includes a request for variances to construct a portion of the second-story addition within the required rear setback (on the functional left side) and for the structure to partially encroach into the daylight plane (on the right side).</p>			
<b>DECISION ENTITY:</b> Planning Commission	<b>DATE:</b> December 09, 2019	<b>ACTION:</b> TBD	
<b>VOTE:</b> TBD (Barnes, DeCardy, Doran, Kennedy, Riggs, Tate)			
<p><b>ACTION:</b></p> <ol style="list-style-type: none"> <li>1. Make a finding that the project is categorically exempt under Class 1 (Section 15301, “Existing Facilities”) of the current California Environmental Quality Act (CEQA) Guidelines.</li> <li>2. Make the following findings as per Section 16.82.340 of the Zoning Ordinance pertaining to the granting of variances to construct a portion of the second-story addition within the required rear setback (on the functional left side) and for the structure to partially encroach into the daylight plane (on the right side): <ol style="list-style-type: none"> <li>a. While the three-sided shape of the parcel is a unique attribute, this does not constitute a hardship, given the space that remains on the parcel for alternate, compliant development.</li> <li>b. The requested variances would allow for a simplified roof structure and larger bedroom at the right, which may be desired features but which do not represent substantial property rights that need to be preserved. The construction of a second story could be accomplished without the variances.</li> <li>c. The rear setback encroachment and daylight plane encroachment at the right side would not be detrimental to the public health, safety, and welfare, or impair an adequate supply of light and air to the adjacent properties, given their limited size and distance to the property lines.</li> <li>d. Due to the lack of positive findings for items a) and b), the granting of these variances could be applicable, generally, to other property in the same zoning classification.</li> <li>e. The property is not within any Specific Plan area. Hence, a finding regarding an unusual factor does not apply.</li> </ol> </li> <li>3. Deny the variances.</li> <li>4. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.</li> <li>5. Approve the use permit subject to the following <b>standard</b> conditions: <ol style="list-style-type: none"> <li>a. The applicant shall be required to apply for a building permit within one year from the date of approval (by December 9, 2020) for the use permit to remain in effect.</li> <li>b. Development of the project shall be substantially in conformance with the plans prepared by Metropolis Architecture consisting of 12 plan sheets, dated received November 26, 2019 and approved by the Planning Commission on December 9, 2019, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.</li> </ol> </li> </ol>			

276 Hedge Road – Attachment A: Recommended Actions

<b>LOCATION:</b> 276 Hedge Road	<b>PROJECT NUMBER:</b> PLN2019-00086	<b>APPLICANT:</b> Hao Zhong	<b>OWNER:</b> Hao Zhong
<b>PROPOSAL:</b> Request for a use permit to add a second floor, as well as conduct interior modifications, to a single-family residence that would exceed 50 percent of the replacement value of the existing nonconforming structure in a 12-month period. The proposal would also exceed 50 percent of the existing floor area and is considered equivalent to a new structure. The subject parcel is a substandard lot with regard to minimum lot width and lot depth in the R-1-U (Single-Family Urban) zoning district. The proposal includes a request for variances to construct a portion of the second-story addition within the required rear setback (on the functional left side) and for the structure to partially encroach into the daylight plane (on the right side).			
<b>DECISION ENTITY:</b> Planning Commission	<b>DATE:</b> December 09, 2019	<b>ACTION:</b> TBD	
<b>VOTE:</b> TBD (Barnes, DeCardy, Doran, Kennedy, Riggs, Tate)			
<p><b>ACTION:</b></p> <ul style="list-style-type: none"> <li>c. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies’ regulations that are directly applicable to the project.</li> <li>d. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.</li> <li>e. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.</li> <li>f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division.</li> <li>g. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits.</li> <li>h. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the arborist report prepared by Kielty Arborist Services LLC., dated revised December 3, 2019.</li> </ul> <p>6. Approve the use permit subject to the following <b>project-specific</b> conditions:</p> <ul style="list-style-type: none"> <li>a. Simultaneous with the submittal of a complete building permit application, the applicant shall revise the plans to remove the variance elements, subject to review and approval of the Planning Division.</li> </ul>			





### City of Menlo Park

Location Map  
276 Hedge Rd.



Scale: 1:4,000

Drawn By: OP

Checked By: CDS

Date: 12/9/2019

Sheet: 1



276 Hedge Road – Attachment C: Data Table

	<b>PROPOSED PROJECT</b>	<b>EXISTING DEVELOPMENT</b>	<b>ZONING ORDINANCE</b>
Lot area	7,115.0 sf	7,115.0 sf	7,000.0 sf min.
Lot width	n/a ft.	n/a ft.	65.0 ft. min.
Lot depth	67.8 ft.	67.8 ft.	100.0 ft. min.
Setbacks			
Front	24.7 ft.	24.7 ft.	20.0 ft. min.
Rear	6.8 ft.	6.8 ft.	20.0 ft. min.
Side (left)	n/a ft.	n/a ft.	5.0 ft. min.
Side (right)	2.8 ft.	2.8 ft.	5.0 ft. min.
Building coverage	1,836.9 sf 25.8 %	1,685.3 sf 23.7 %	2,490.3 sf max. 35.0 % max.
FAL (Floor Area Limit)	2,607.0 sf	1,674.4 sf	2,828.8 sf max.
Square footage by floor	1,252.4 sf/1 <sup>st</sup> floor 903.7 sf/2 <sup>nd</sup> floor 438.0 sf/garage 12.9 sf/attic 135.6 sf/porch 10.9 sf/fireplace	1,236.4 sf/1 <sup>st</sup> floor 438.0 sf/garage 10.9 sf/fireplace	
Square footage of buildings	2,753.5 sf	1,685.3 sf	
Building height	25.8 ft.	14.8 ft.	28 ft. max.
Parking	2 covered*	2 covered*	1 covered/1 uncovered
<b>Note: Areas shown highlighted indicate a nonconforming or substandard situation.</b>			
Trees	Heritage trees: 1**	Non-Heritage trees: 4***	New Trees: 0
	Heritage trees proposed for removal: 0	Non-Heritage trees proposed for removal: 0	Total Number of Trees: 5
*Includes two substandard covered parking spaces with respect to width and depth within the garage **The heritage tree is located on the neighboring lot to the right. *** One non-heritage tree is a street tree located in the right-of-way in front of the neighboring property to the right, and one is on the neighboring lot to the left.			



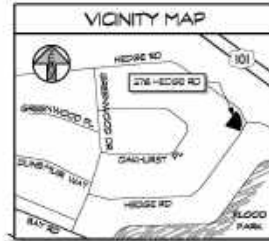
2 STREETSCAPE

UNDER NO CIRCUMSTANCE SHALL DRAINAGE RESULTING FROM THIS PROJECT DURING OR POST CONSTRUCTION DIRECTLY SHEETFLOW ACROSS AN ADJOINING PROPERTY. RUNOFF SHALL BE CONTAINED ON SITE UP TO THE 10-YEAR STORM.

STORMWATER RUNOFF FROM THE NEW IMPERVIOUS AREA SHALL BE DIRECTED TO THE LANDSCAPE.

ALL EXISTING CRACKED OR DAMAGED FEATURES ALONG THE PROPERTY FRONTAGE MUST BE REPLACED IN KIND. ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL ALSO BE REQUIRED TO BE REPLACED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.

AN ENCROACHMENT PERMIT FROM THE ENGINEERING DIVISION IS REQUIRED PRIOR TO ANY CONSTRUCTION ACTIVITIES, INCLUDING UTILITY LATERALS IN THE PUBLIC RIGHT OF WAY.

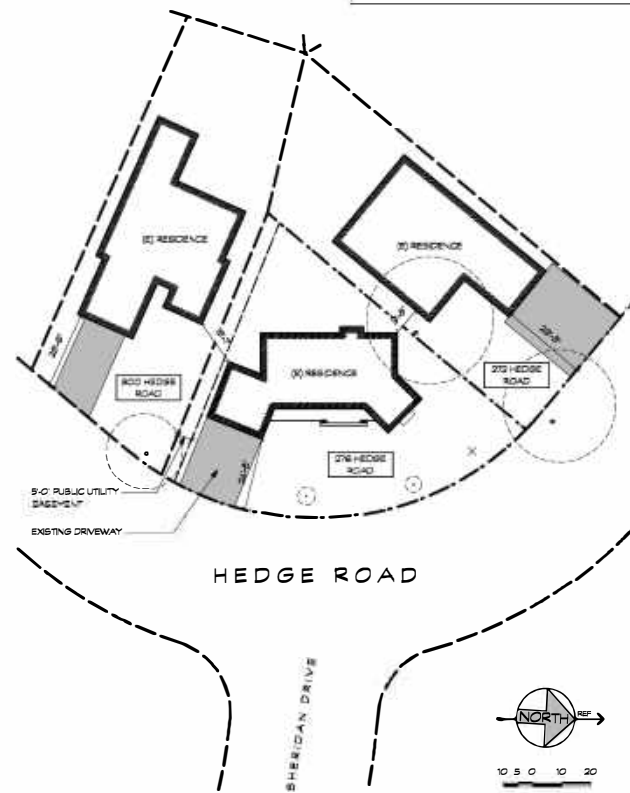


SCOPE OF WORK	
SECOND STORY ADDITION & REMODEL TO (E) ONE-STORY SINGLE FAMILY RESIDENCE	

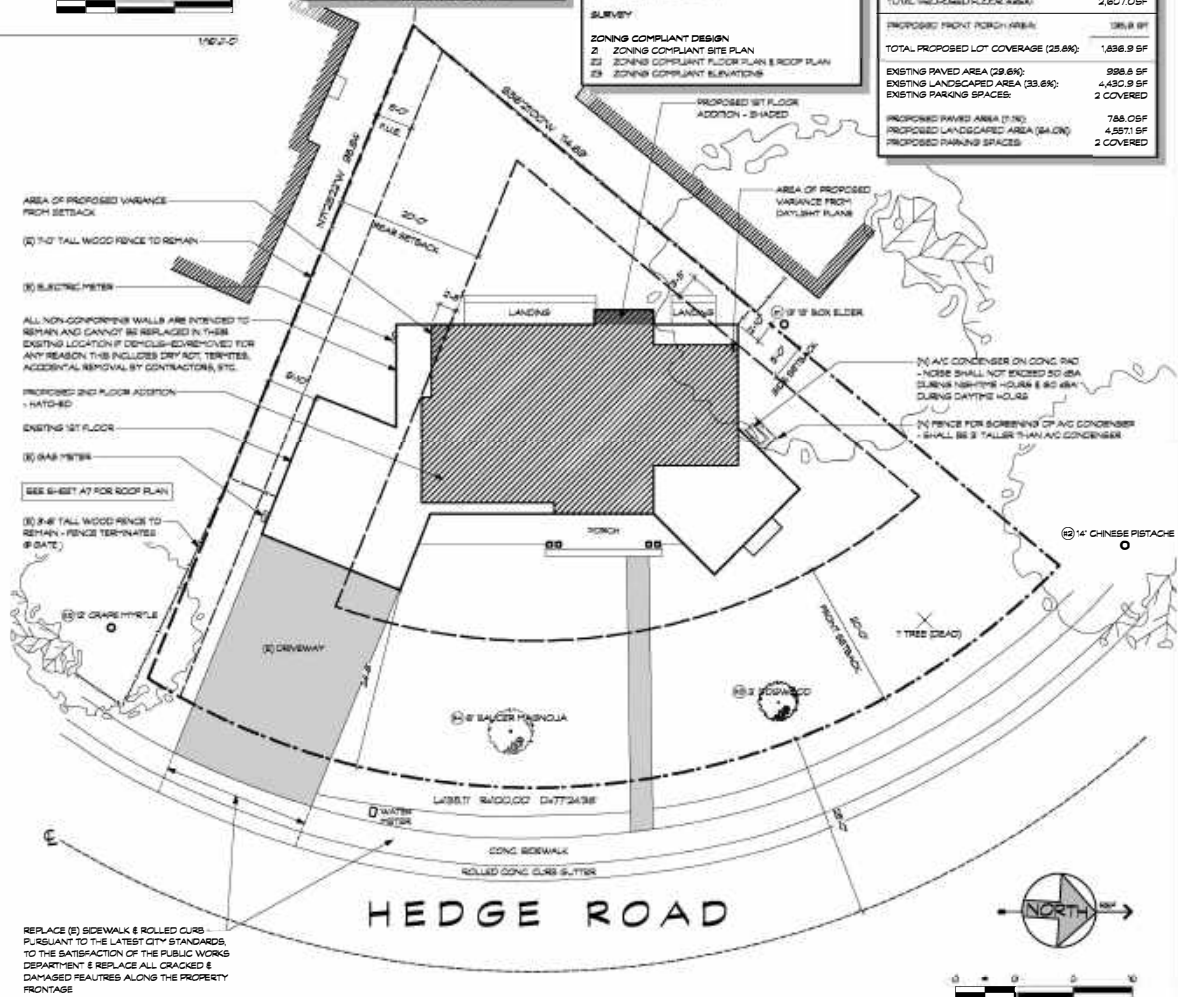
  

SHEET INDEX	
ARCHITECTURAL	
A1	SITE PLAN, STREETScape, & AREA PLAN
A2	EXISTING FLOOR PLAN
A3	EXISTING ELEVATIONS
A4	PROPOSED FIRST FLOOR PLAN
A5	PROPOSED SECOND FLOOR & ATTIC PLAN
A6	PROPOSED ELEVATIONS
A7	PROPOSED SECTIONS & ROOF PLAN
A8	CALCULATIONS
A9	ARCHITECTURAL DETAILS
SURVEY	
ZONING COMPLIANT DESIGN	
2	ZONING COMPLIANT SITE PLAN
23	ZONING COMPLIANT FLOOR PLAN & ROOF PLAN
23	ZONING COMPLIANT ELEVATIONS

SITE DATA	
APN:	055-302-140
ZONE:	R-1-U
LOT AREA:	7,119.0 SF
MAXIMUM FLOOR AREA:	2,828.8 SF
MAXIMUM LOT COVERAGE (35%):	2,490.3 SF
TOTAL EXISTING FLOOR AREA:	1,874.4 SF
EXISTING 2-STOREY AREA:	10.8 SF
TOTAL EXISTING LOT COVERAGE (24.1%):	1,688.9 SF
PROPOSED FIRST FLOOR ADDITION:	16.0 SF
PROPOSED SECOND FLOOR ADDITION:	903.7 SF
PROPOSED ATTIC AREA OVER 5'-0":	12.9 SF
TOTAL PROPOSED FLOOR AREA:	2,807.0 SF
PROPOSED FRONT PORCH AREA:	136.8 SF
TOTAL PROPOSED LOT COVERAGE (25.6%):	1,826.9 SF
EXISTING PAVED AREA (29.6%):	998.9 SF
EXISTING LANDSCAPED AREA (33.6%):	4,430.9 SF
EXISTING PARKING SPACES:	2 COVERED
PROPOSED PAVED AREA (7.1%):	788.0 SF
PROPOSED LANDSCAPED AREA (34.0%):	4,857.1 SF
PROPOSED PARKING SPACES:	2 COVERED



3 AREA PLAN



1 SITE PLAN

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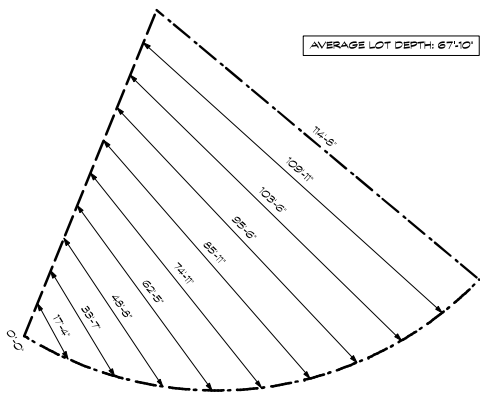


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Placentia, CA 92678  
656-318-0211

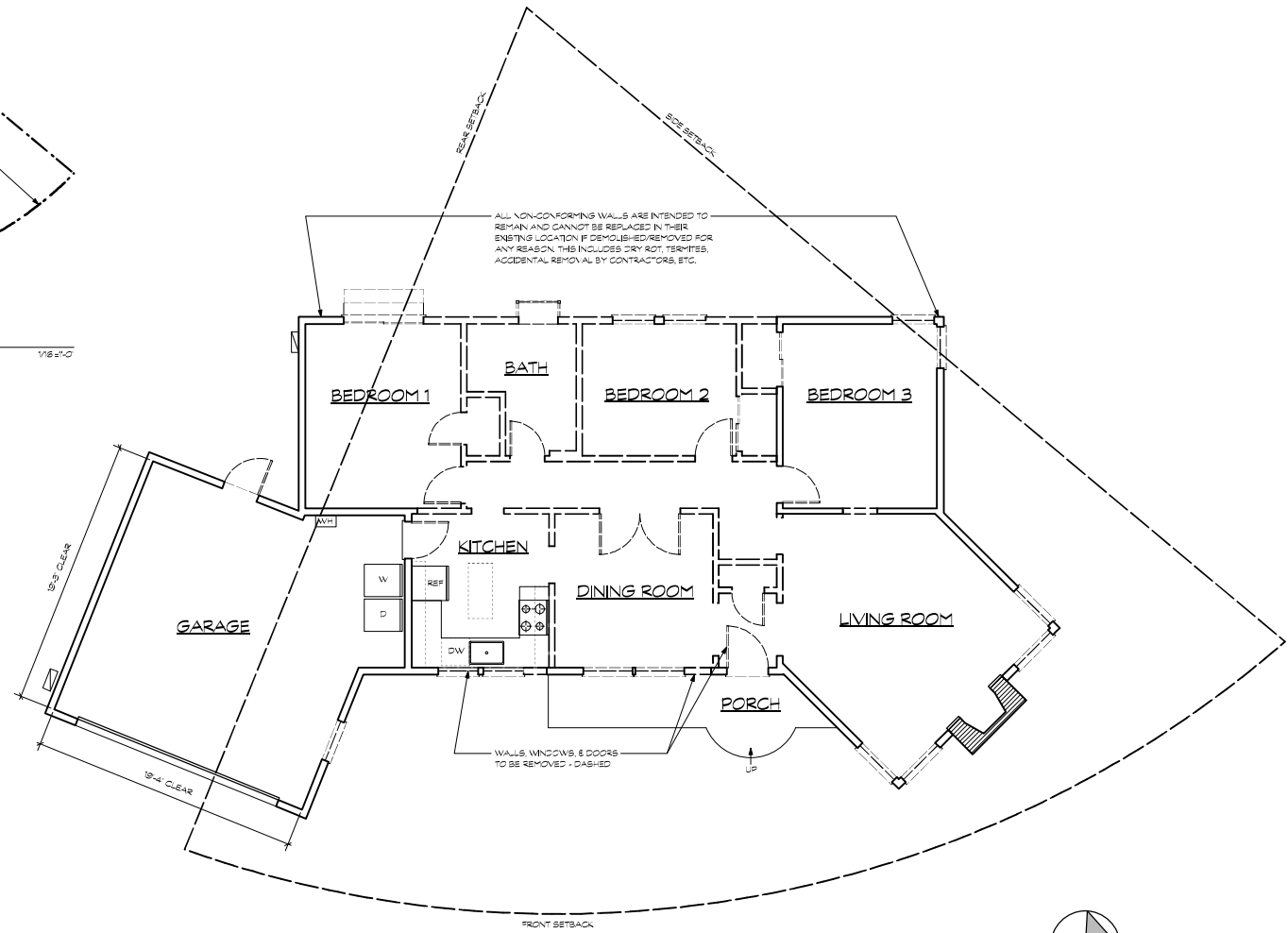
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276 HEDGE ROAD  
MENLO PARK, CALIFORNIA

Date: 27 AUG 2019  
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Job: 21048  
Sheet:

A1



2 LOT DEPTH DIAGRAM  
1/8" = 1'-0"



1 EXISTING FLOOR PLAN  
1/4" = 1'-0"

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ZHONG RESIDENCE  
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MENLO PARK, CALIFORNIA

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Scale NOTED  
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Job ZHONG  
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A2

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PLANNING REV  
26 NOV 2018

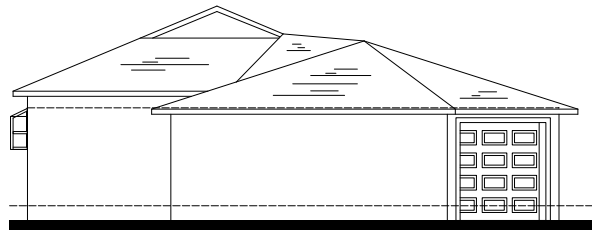


445 N. Whittier Road  
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Menlo Park, CA 94025  
(650) 318-0211

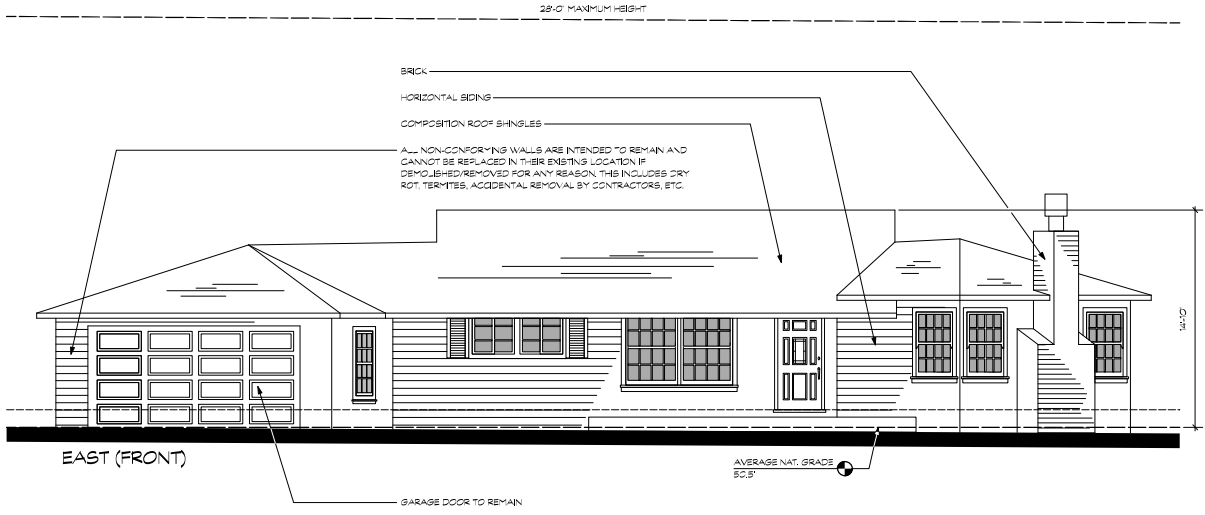
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276 HEDGE ROAD  
MENLO PARK, CALIFORNIA

Date 27 AUG 2018  
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Drawn YETKO  
Job ZHONG  
Sheet

A3



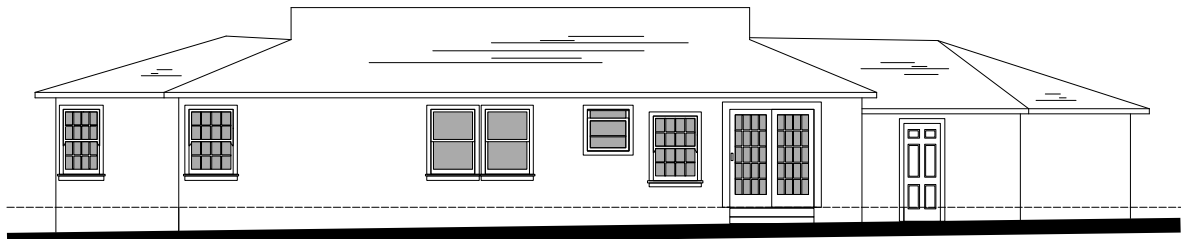
SOUTH (LEFT SIDE)



EAST (FRONT)

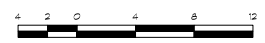


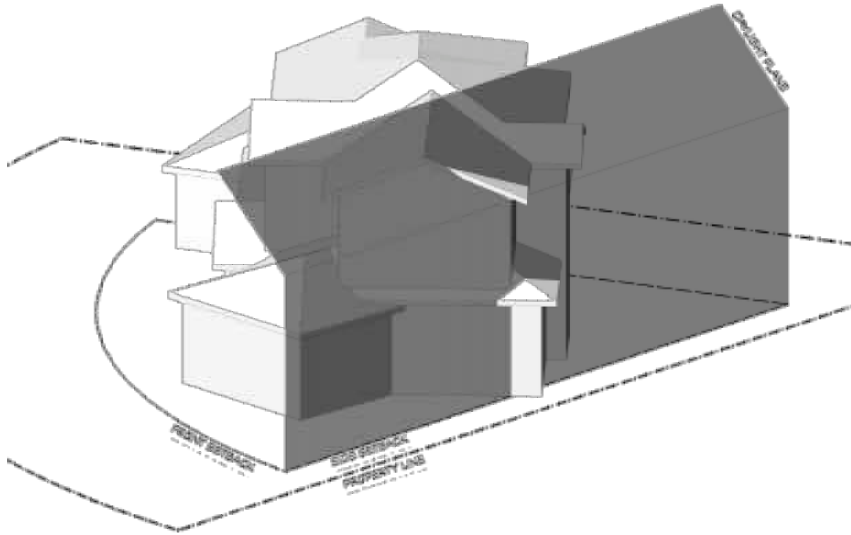
NORTH (RIGHT SIDE)



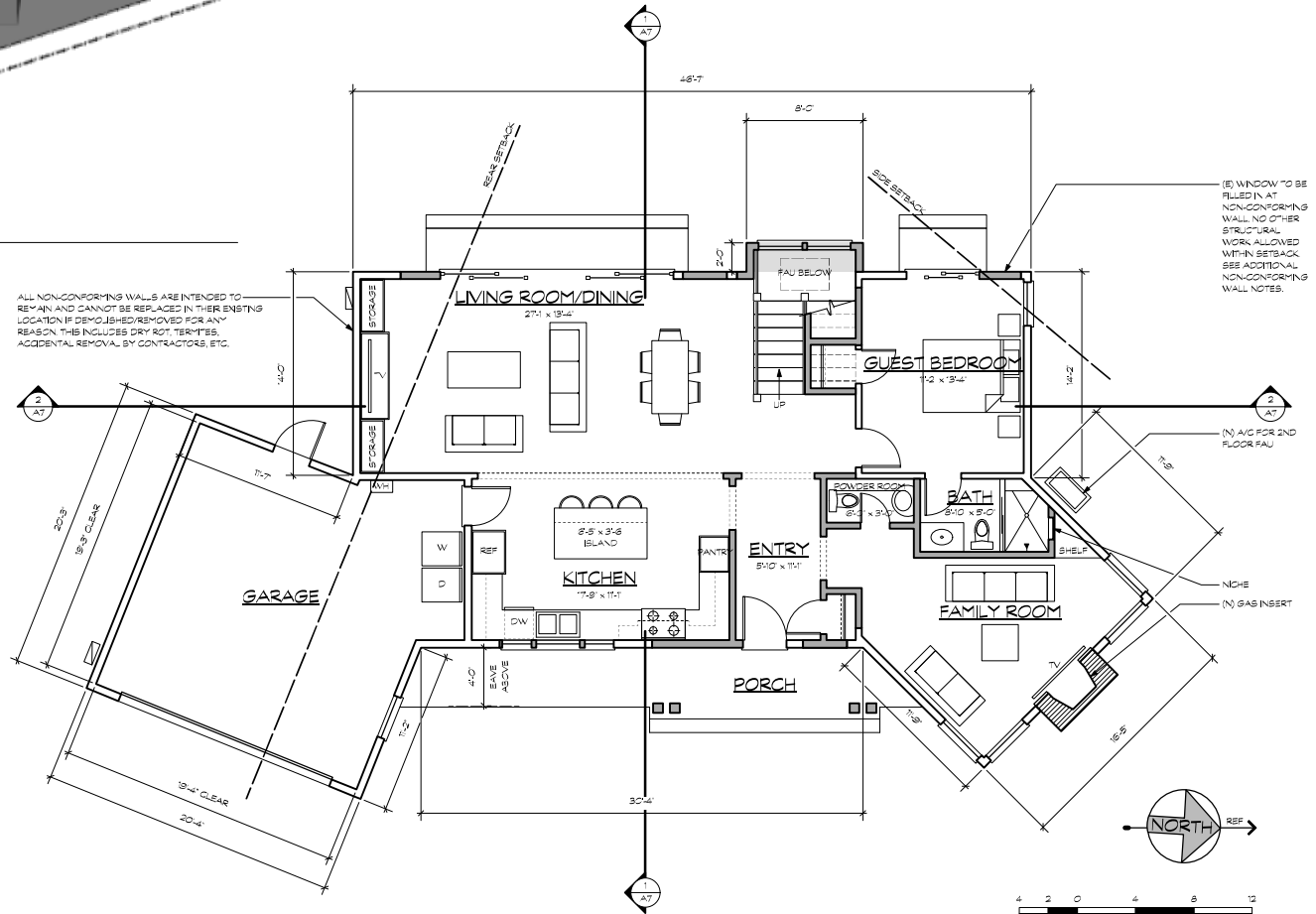
WEST (REAR)

1 EXISTING ELEVATIONS





2 DAYLIGHT PLANE MODEL



1 PROPOSED FIRST FLOOR PLAN

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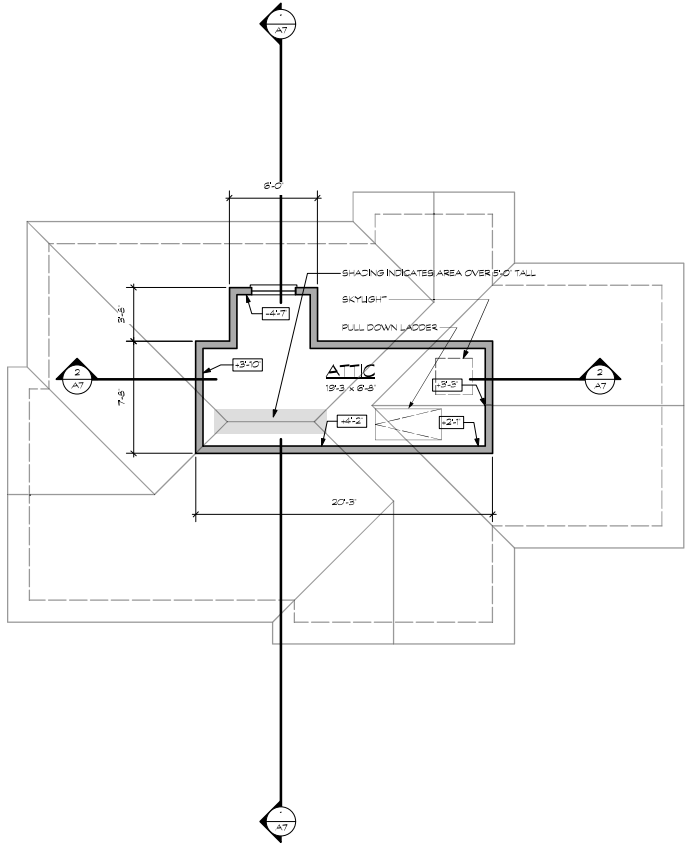


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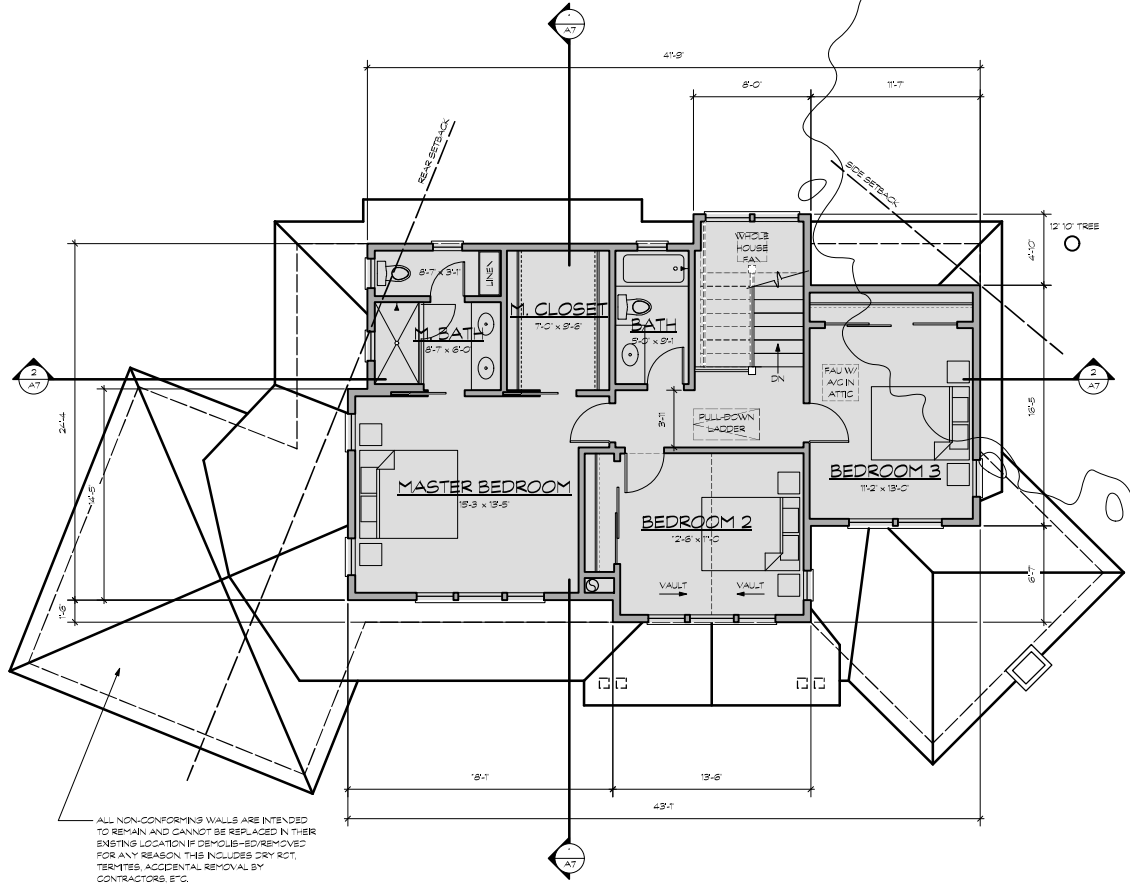
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2 PROPOSED ATTIC PLAN

1/4"=1'-0"



1 PROPOSED SECOND FLOOR PLAN

1/4"=1'-0"



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Scale NOTED  
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Job ZHONG  
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A5

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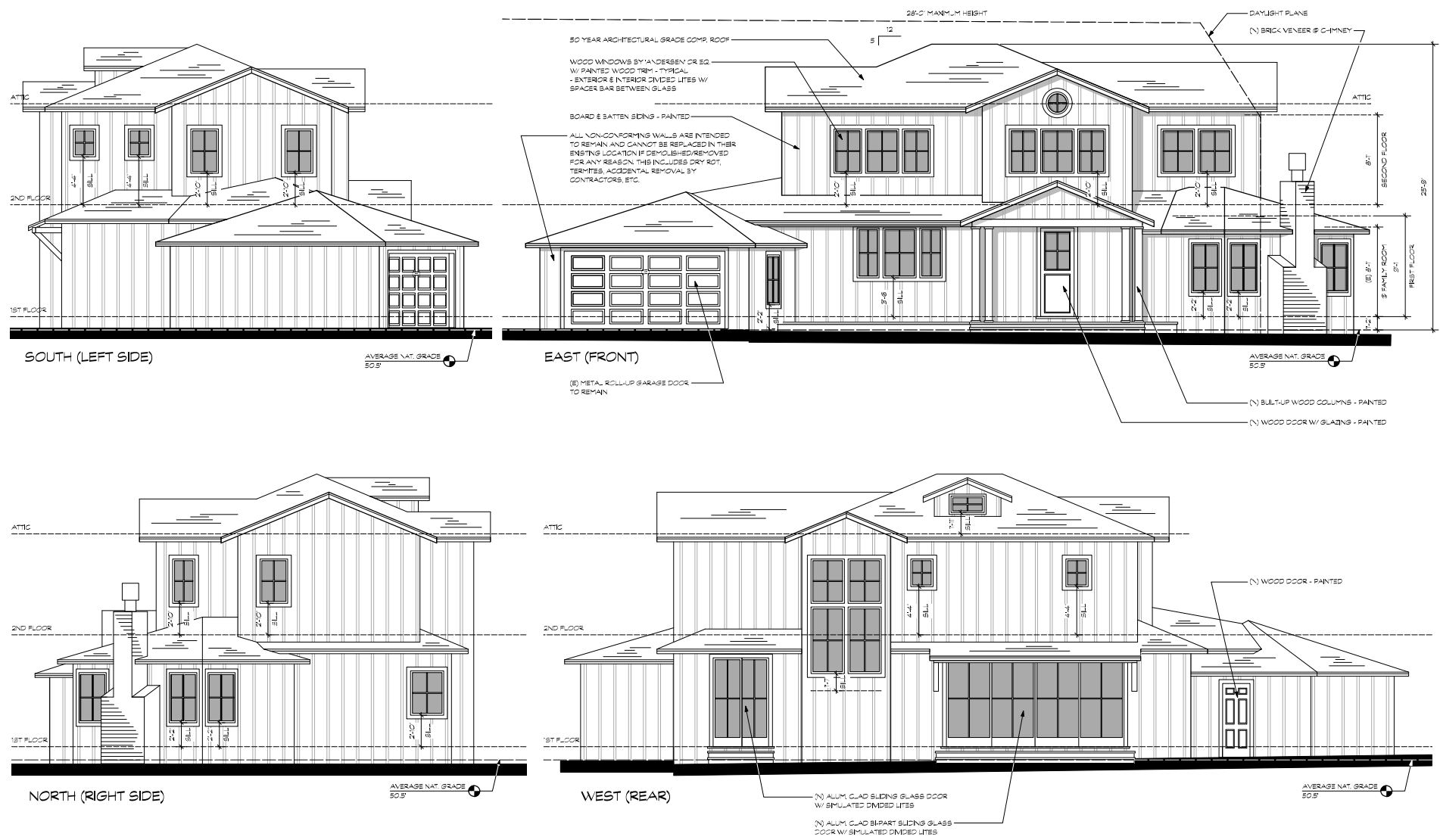


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(951) 948-0211

ZHONG RESIDENCE  
276 HEDGE ROAD  
MENLO PARK, CALIFORNIA

Date 27 AUG 2019  
Scale NOTED  
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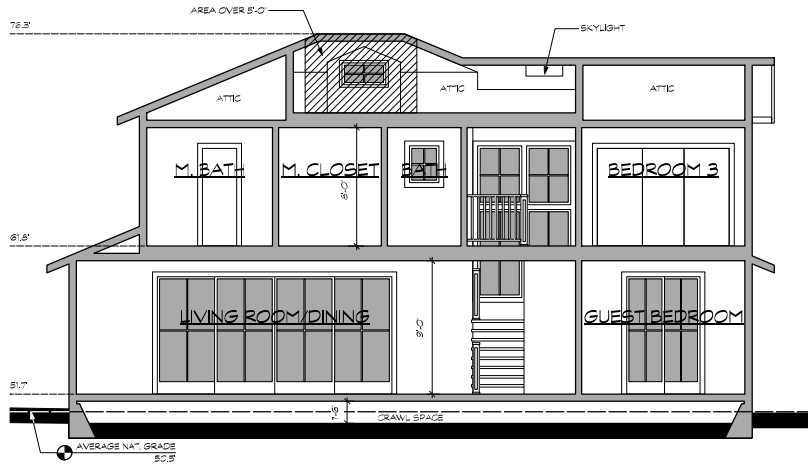
A6



1 PROPOSED ELEVATIONS

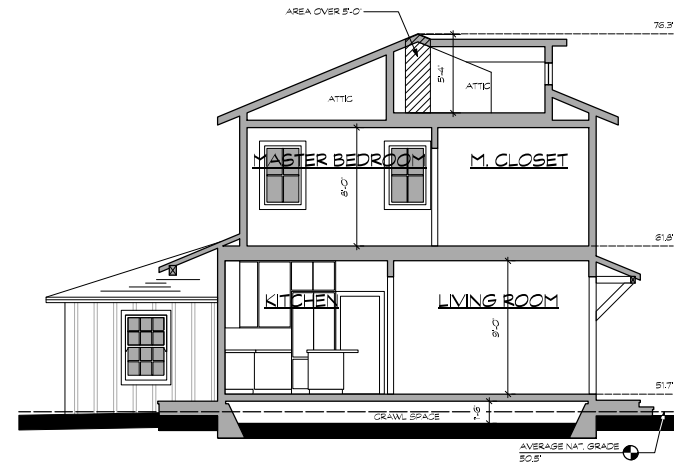






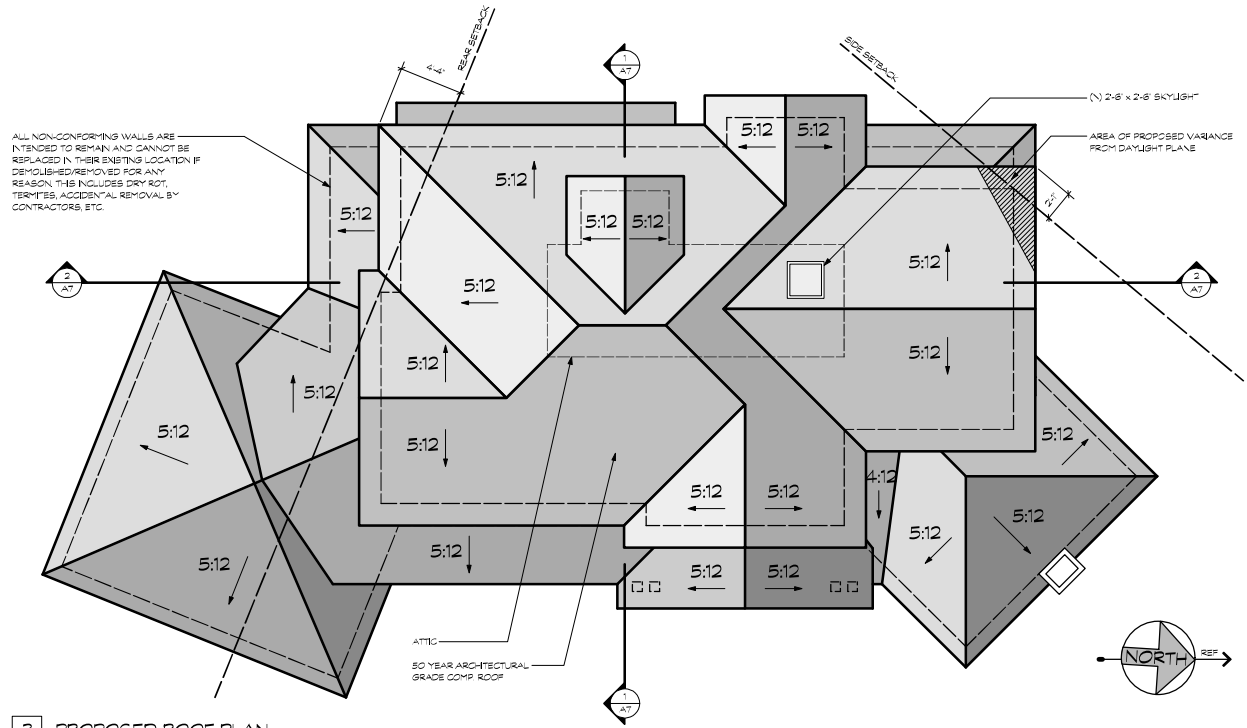
2 PROPOSED SECTION

1/4"=1'-0"



1 PROPOSED SECTION

1/4"=1'-0"



3 PROPOSED ROOF PLAN

1/4"=1'-0"

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ZHONG RESIDENCE  
276 HEDGE ROAD  
MENLO PARK, CALIFORNIA

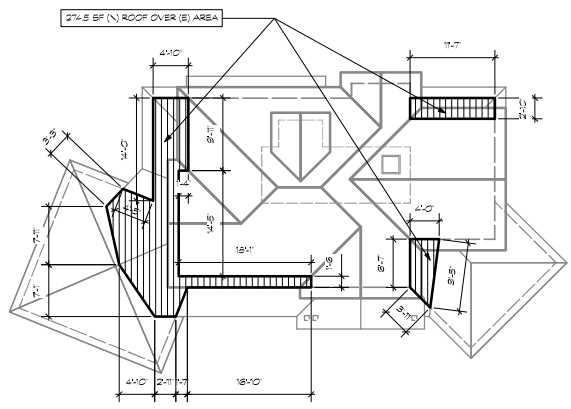
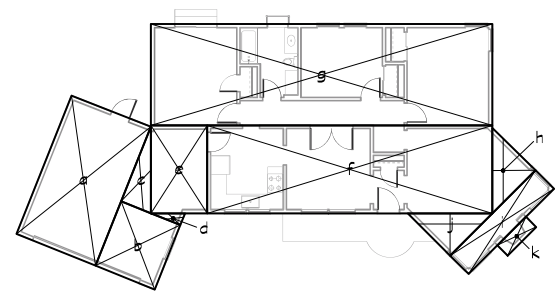
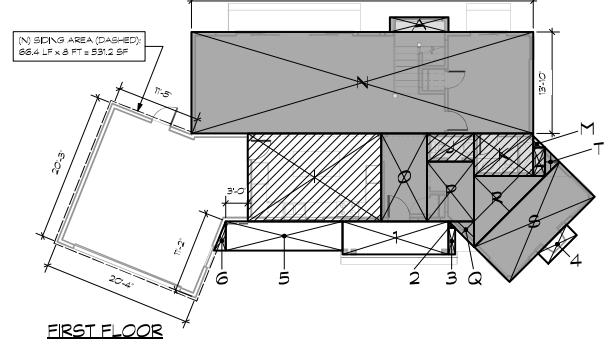
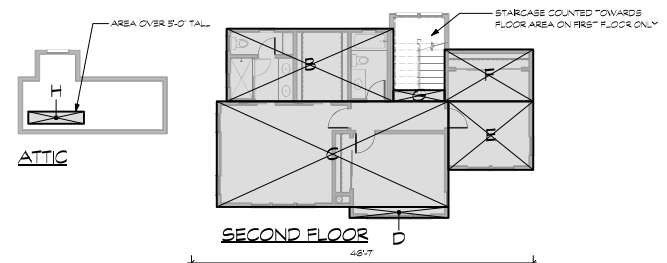
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A7



**LEGEND**

- EXISTING FLOOR AREA
- PROPOSED ADDITION
- KITCHEN REMODEL
- LIVING AREA REMODEL
- BATH-ROOM REMODEL



3 PROPOSED ROOF AREA DIAGRAM  
1/8" = 1'-0"

2 PROPOSED FLOOR AREA DIAGRAM  
1/8" = 1'-0"

1 EXISTING FLOOR AREA DIAGRAM  
1/8" = 1'-0"

EXISTING DEVELOPMENT			
	SQUARE FOOTAGE	CONSTRUCTION COST	EXISTING VALUE
EXISTING FLOOR AREA	1,236.4 SF	\$200 / SF	\$247,280.00
EXISTING GARAGE AREA	438.0 SF	\$70 / SF	\$30,660.00
TOTAL VALUE			\$277,940.00
75% OF EXISTING VALUE			\$208,455.00
PROPOSED DEVELOPMENT			
DEVELOPMENT TYPE	SQUARE FOOTAGE	CONSTRUCTION COST	DEVELOPMENT VALUE
FLOOR ADDITION	332.8 SF	\$ 200 / SF	\$66,560.00
REMODEL OF KITCHEN	218.5 SF	\$ 130 / SF	\$28,405.00
REMODEL OF BATH-ROOMS	79.2 SF	\$ 130 / SF	\$10,296.00
REMODEL OF LIVING AREAS	938.7 SF	\$ 100 / SF	\$93,870.00
NEW ROOF OVER EXISTING AREA	274.5 SF	\$50 / SF	\$13,725.00
NEW WINDOW AREA	0.0 SF	\$ 35 / SF	\$0.00
NEW SIDING AREA	531.2 SF	\$ 35 / SF	\$18,592.00
TOTAL		(126.4%)	\$387,408.00

PROPOSED COVERAGE CALCS		
	DIMENSION	SQUARE FOOTAGE
1	14'-5" x 4'-8"	64.9 SF
2	(20'-11" x 0'-11") / 2	0.4 SF
3	0'-11" x 3'-7"	3.9 SF
4	5'-8" x 2'-0"	10.9 SF
5	15'-1" x 4'-0"	63.9 SF
6	(1'-7" x 4'-0") / 2	3.2 SF
PROPOSED FRONT PORCH/CHIMNEY AREA:		148.5 SF
TOTAL PROPOSED BUILDING COVERAGE:		1,636.9 SF

PROPOSED FLOOR AREA CALCULATIONS		
	DIMENSION	SQUARE FOOTAGE
A	8'-0" x 2'-0"	16.0 SF
B	22'-2" x 9'-11"	228.4 SF
C	37'-7" x 14'-5"	454.4 SF
D	13'-8" x 1'-8"	20.2 SF
E	11'-0" x 8'-4"	107.9 SF
F	12'-0" x 1'-1"	65.3 SF
G	7'-0" x 1'-8"	10.5 SF
H	7'-7" x 1'-8"	12.9 SF
PROPOSED ADDITION AREA:		332.8 SF
	DIMENSION	SQUARE FOOTAGE
I	18'-3" x 12'-0"	218.5 SF
PROPOSED REMODEL OF KITCHEN:		218.5 SF
	DIMENSION	SQUARE FOOTAGE
J	8'-5" x 3'-10"	24.8 SF
K	8'-1" x 3'-10"	47.0 SF
L	1'-7" x 3'-11"	6.3 SF
M	(1'-7" x 1'-7") / 2	1.3 SF
PROPOSED REMODEL OF BATHROOMS:		79.2 SF
	DIMENSION	SQUARE FOOTAGE
N	48'-7" x 13'-10"	643.5 SF
O	8'-3" x 12'-0"	75.0 SF
P	8'-5" x 8'-2"	62.4 SF
Q	(3'-8" x 3'-8") / 2	6.1 SF
R	(3'-8" x 3'-8") / 2	48.7 SF
S	18'-5" x 8'-9"	11.2 SF
T	(2'-8" x 2'-8") / 2	3.8 SF
PROPOSED REMODEL OF LIVING AREAS:		938.7 SF

EXISTING FLOOR AREA CALCULATIONS		
	DIMENSION	SQUARE FOOTAGE
a	22'-3" x 11'-7"	238.0 SF (GARAGE)
b	9'-5" x 2'-8"	22.3 SF (GARAGE)
c	(11'-0" x 4'-5") / 2	24.0 SF (GARAGE)
d	(1'-8" x 4'-4") / 2	3.7 SF (GARAGE)
e	7'-5" x 12'-0"	92.0 SF (GARAGE)
f	38'-11" x 12'-0"	467.1 SF
g	48'-7" x 3'-10"	843.5 SF
h	12'-2" x 2'-11"	34.1 SF
i	(8'-3" x 8'-2") / 2	58.4 SF
j	(8'-2" x 8'-2") / 2	33.3 SF
EXISTING RESIDENCE AREA:		1,674.4 SF

EXISTING COVERAGE CALCULATIONS		
	DIMENSION	SQUARE FOOTAGE
k	5'-8" x 2'-0"	10.9 SF
TOTAL EXISTING BUILDING COVERAGE:		1,685.3 SF

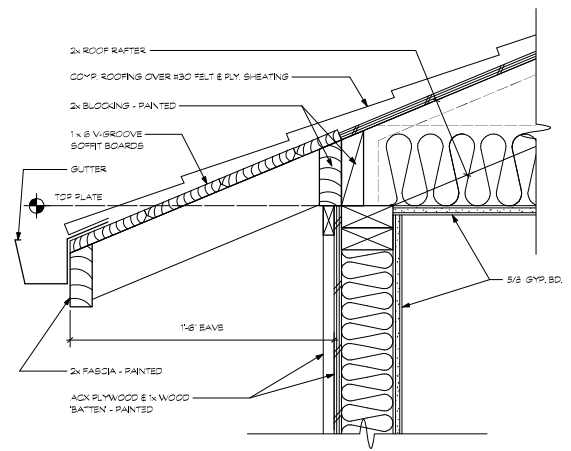


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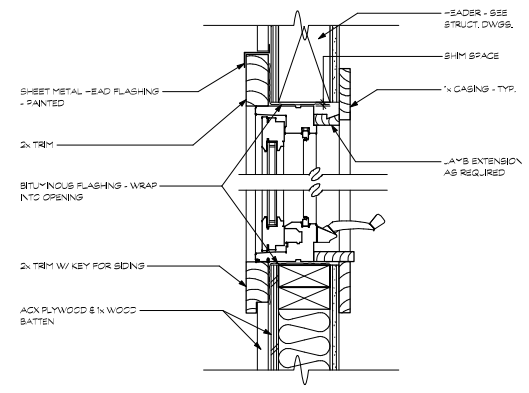
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Suite #300  
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(650) 318-0211

ZHONG RESIDENCE  
276 HEDGE ROAD  
MENLO PARK, CALIFORNIA



1 TYPICAL EAVE DETAIL

3/4" = 1'-0"



2 TYPICAL WINDOW HEAD & SILL DETAIL

3/4" = 1'-0"

Date 27 AUG 2018  
Scale NOTED  
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Job ZHONG

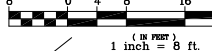




**ABBREVIATIONS**

AC	ASPHALT
BW	BACK OF WALK
CONC.	CONCRETE
TC	TOP OF CURB
FL	FLOW LINE
SDMH	STORM DRAIN MANHOLE
SSMH	SANITARY SEWER MANHOLE
F.U.E.	PUBLIC UTILITY EASEMENT

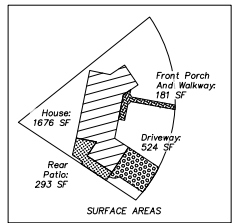
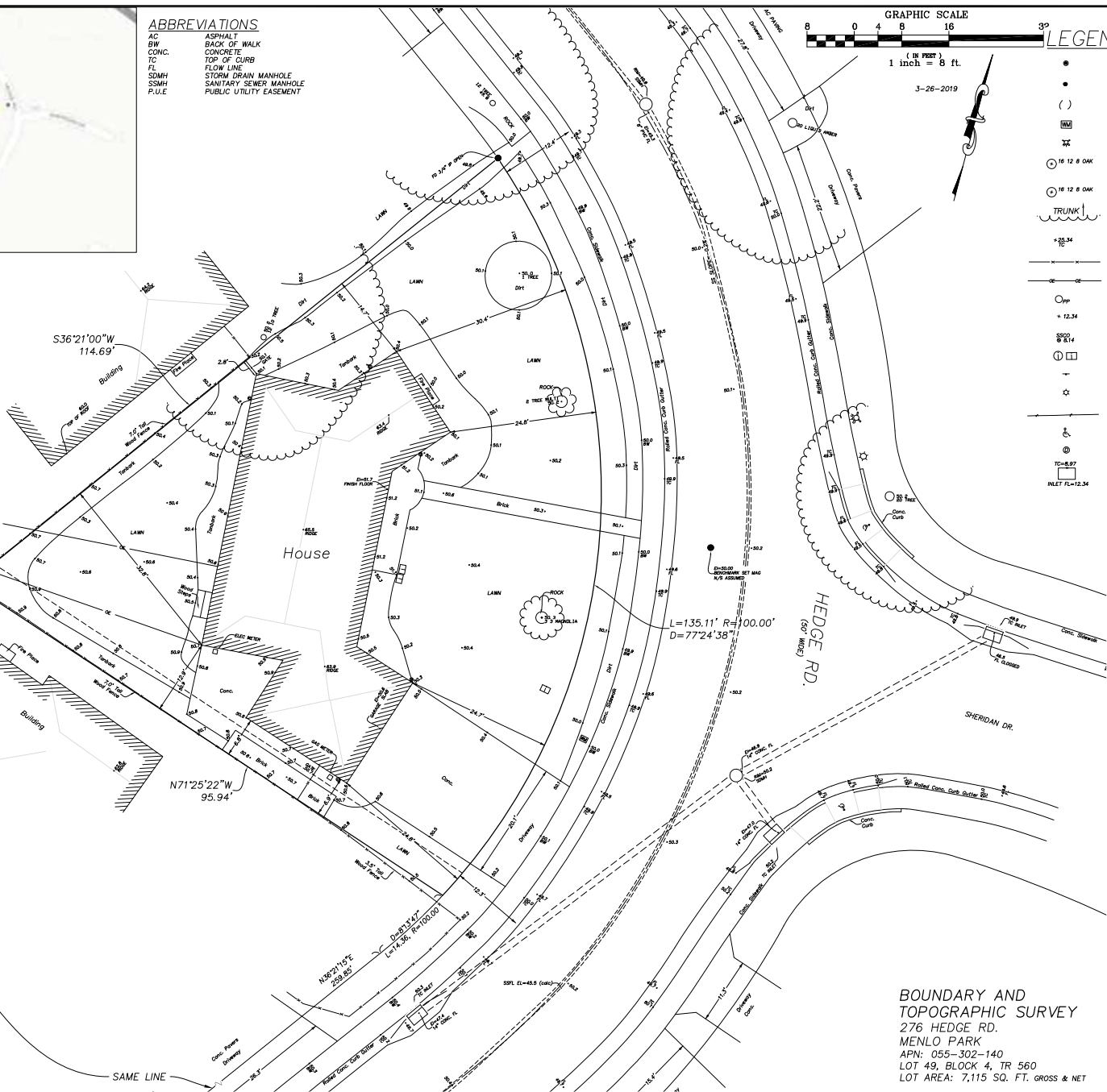
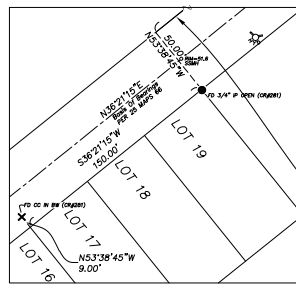
GRAPHIC SCALE



**LEGEND**

- FOUND POINT IN MONUMENT CASTING (AS NOTED)
- FOUND POINT AS NOTED
- ( ) RECORD DATA / REFERENCE
- ⊠ WATER METER OR WATER VALVE BOX
- ⊠ FIRE HYDRANT
- 18 12 8 OAK TREE - TRUNK DIAMETER IN INCHES  
TREE SPECIES IDENTIFICATION: BEST EFFORT, WE ARE NOT ARBORISTS OR DENDROLOGISTS
- 18 12 8 OAK TREE WITH MULTIPLE TRUNKS
- TRUNK TREE DRIP LINE POINTS TOWARDS TREE TRUNKS. TREE DRIP LINES ABOVE PROPERTY LOCATED AS SHOWN.
- ⊠ TOP OF CURB
- FENCE
- OVERHEAD WIRES
- POWER POLE
- ⊠ SPOT ELEVATION
- ⊠ SANITARY SEWER CLEAN OUT
- ⊠ IRRIGATION VALVE BOX
- ⊠ SIGN
- ⊠ ELECTROUER
- EDGE OF AC PAVING
- ⊠ HANDICAP RAMP OR PARKING
- ⊠ DOWN SPOUT
- ⊠ STORM DRAIN CURB INLET

**NOTES**  
 ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS. UNDERGROUND UTILITY - LOCATION IS BASED ON SURFACE EVIDENCE.  
 BUILDING LOCATION DIMENSIONS ARE MEASURED PERPENDICULAR OR RADIAL TO THE PROPERTY LINES. DIMENSIONS TO THE BUILDING ARE TAKEN AT THE EXTERIOR FINISHED SURFACE.  
 THE BUILDING EXTERIOR FINISHED SURFACE IS WOOD SIDING AND VARIES APPROXIMATELY 0.06"-0.08" IN THICKNESS. FINISH FLOOR ELEVATION TAKEN AT DOOR THRESHOLD (EXTERIOR).  
 BENCHMARK: ASSUMED DATUM, POINT AS SHOWN A CURRENT TITLE REPORT FOR THE SUBJECT PROPERTY HAS NOT BEEN EXAMINED.  
 BY L. WADE HAMMOND LAND SURVEYOR. EASEMENTS OF RECORD MAY EXIST THAT ARE NOT SHOWN ON THIS MAP. TREE SPECIES IDENTIFICATION, BEST EFFORT, WE ARE NOT ARBORISTS OR DENDROLOGISTS.  
 TREES SHOWN ARE 4" TRUNK DIAMETER OR LARGER, MEASURED 5' ABOVE GRADE.



I CERTIFY THAT THIS PARCEL'S BOUNDARY WAS ESTABLISHED BY ME OR UNDER MY SUPERVISION AND IS BASED ON A FIELD SURVEY IN CONFORMANCE WITH THE LAND SURVEYOR'S ACT. ALL MONUMENTS ARE OF THE CHARACTER AND OCCUPY THE POSITIONS INDICATED AND ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.



**L. Wade Hammond**  
 Licensed Land Surveyor  
 No. 6163  
 36660 Newark Blvd. Suite C  
 Newark, California 94560  
 Tel: (510) 579-6112 Fax: (510) 991-8054  
 wade@whlandsurveyor.com

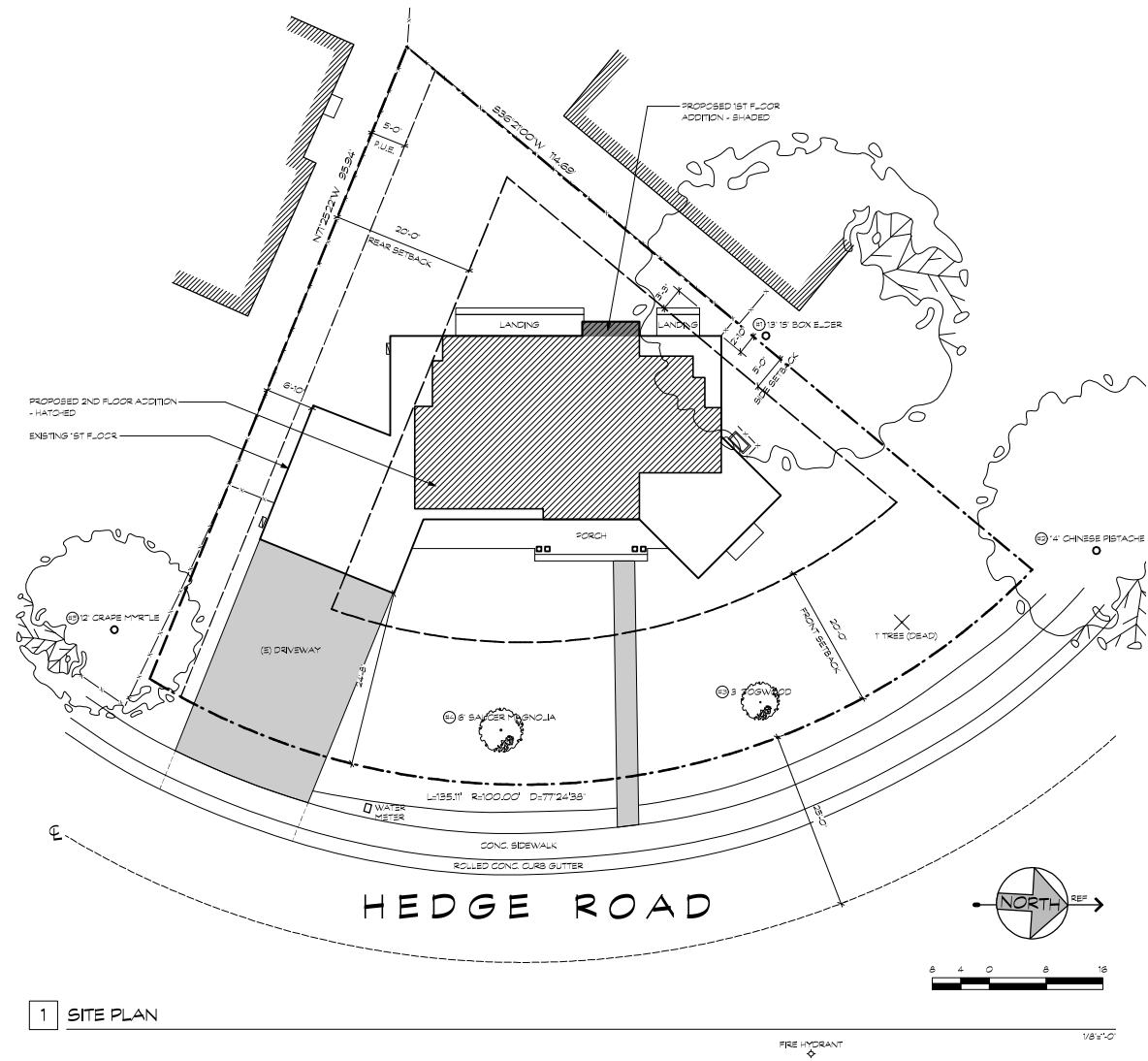
**BOUNDARY AND TOPOGRAPHIC SURVEY**  
 276 HEDGE RD.  
 MENLO PARK  
 APN: 055-302-140  
 LOT 49, BLOCK 4, TR 560  
 LOT AREA: 7,115 SQ. FT. GROSS & NET

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ZHONG RESIDENCE  
276 HEDGE ROAD  
MENLO PARK, CALIFORNIA



1 SITE PLAN

ZONING COMPLIANT DESIGN FOR REFERENCE

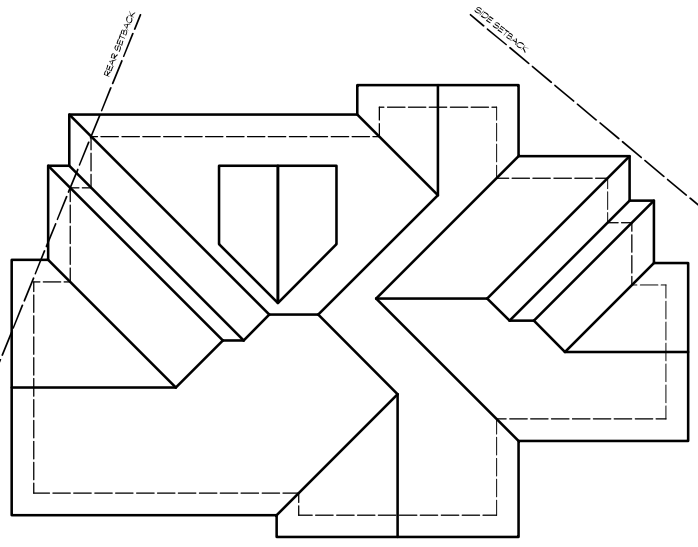
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28 NOV 2018



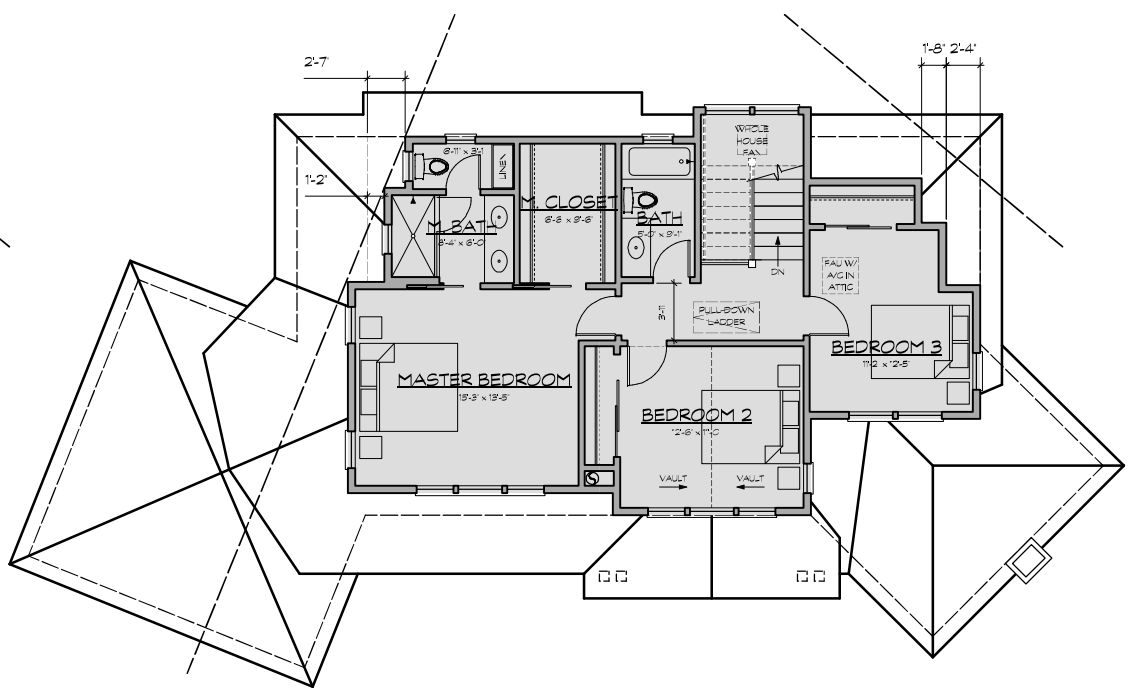
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276 HEDGE ROAD  
MENLO PARK, CALIFORNIA



2 UPPER ROOF PLAN

1/4"=1'-0"



1 SECOND FLOOR PLAN

1/4"=1'-0"



ZONING COMPLIANT DESIGN FOR REFERENCE

Date 27 AUG 2018  
Scale NOTED  
Drawn YETNG  
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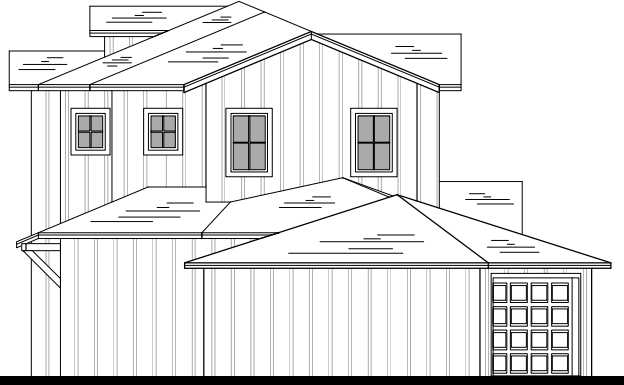
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ZHONG RESIDENCE  
276 HEDGE ROAD  
MENLO PARK, CALIFORNIA



SOUTH (LEFT SIDE)



EAST (FRONT)



NORTH (RIGHT SIDE)



WEST (REAR)

1 EXTERIOR ELEVATIONS

ZONING COMPLIANT DESIGN FOR REFERENCE



Date 27 AUG 2018  
Scale NOTED  
Drawn YETUO  
Job ZHONG  
Sheet

23





November 26, 2019

## **276 HEDGE ROAD PROJECT DESCRIPTION**

The project proposes to build a second-story addition and remodel the interior and exterior of an existing single-family residence on an irregular shaped lot. The existing house is a one-story ranch style home with an attached garage. The project will provide common areas at the first floor and private bedrooms at the second floor. The proposed addition will be constructed using conventional wood framing. The project conforms to height limits in the R1-U Zoning District and requests a variance from the rear setback requirement and daylight plane due to the irregular shape of the lot.

A portion of the existing residence is located within the rear and side yard setbacks. These nonconforming walls of the residence are proposed to remain and we acknowledge that they cannot be replaced in their existing location if demolished or removed for any reason including dry rot, termites, accidental removal by contractors, etc. This is noted on the site plan, floor plans, elevations, and roof plans.

The proposed windows on the front, left, and right side elevations on the second floor have a sill height of 2'-10". We believe there will not be a privacy concern with these windows due to the unique shape of the lot and the orientation to the neighboring properties. The proposed windows are angled away from the neighbors' side yards and look primarily towards the street fronting the property rather than directly into adjacent residences.

## **NEIGHBORHOOD OUTREACH**

Hao Zhong and Xiao Ge have contacted their neighbors regarding the project. See attached for outreach and feedback.

## **ARCHITECTURAL STYLE**

The proposed two-story house will be remodeled in the Modern Farmhouse style. The exterior materials consist of composition roof shingles, board & batten siding, and white decorative trim around wood windows. These wood windows will use simulated divided lites with interior spacer bars in a 2 over 2 grid pattern. All existing windows will be replaced to match the style of the proposed windows.

The proposed house retains the unique angled masses on either side of the existing residence and uses a new gable porch to emphasize the front entry. The simple forms of the addition and use of gable roof lines are consistent with the Modern Farmhouse style.

## Kielty Arborist Services LLC

Certified Arborist WE#0476A

P.O. Box 6187

San Mateo, CA 94403

650-515-9783

Revised December 3, 2019

Hao Zhong

Site: 276 Hedge Road, Menlo Park CA

Dear Hao Zhong

As requested on Wednesday, October 9, 2019, I visited the above site to inspect and comment on the trees. A second-floor home addition is planned for this site, and your concern as to the future health and safety of the trees has prompted this visit. Site plan A1 through A6 dated 6/28/19 was reviewed for writing this report. This report will go over the existing health of the trees, potential impacts, and mitigation measures. A tree protection plan will also be provided.

### Method:

All inspections were made from the ground; the trees were not climbed for this inspection. The trees in question were located on a map provided by you. The trees were then measured for diameter at 54 inches above ground level (DBH or diameter at breast height). The trees were given a condition rating for form and vitality. The trees condition rating is based on 50 percent vitality and 50 percent form, using the following scale.

1	-	29	Very Poor
30	-	49	Poor
50	-	69	Fair
70	-	89	Good
90	-	100	Excellent

The height of the trees was measured using a Nikon Forestry 550 Hypsometer. The spread was paced off. Comments and recommendations for future maintenance are provided.

276 Hedge 12/3/19

(2)

**Survey:**

Tree#	Species	DBH	CON	HT/SP	Comments
1* <b>P</b>	Box elder ( <i>Acer negundo</i> )	13-15	45	35/40	Fair vigor, poor form, codominant at 1 foot with poor union, topped at 20 feet in past, over existing home by 15 feet.
2* <b>P</b>	Chinese pistache ( <i>Pistacia chinensis</i> )	14.0	80	35/35	Good vigor, good form. <b>Street tree</b>
3	Dogwood ( <i>Cornus florida</i> )	3.0	65	15/8	Fair vigor, fair form, young tree.
4	Saucer magnolia ( <i>Magnolia x soulangeana</i> )	6.0	80	12/12	Good vigor, good form, young tree.
5*	Crape myrtle ( <i>Lagerstroemia sp.</i> )	12@grade	70	15/15	Good vigor, good form, <b>multi trunked tree.</b>

\*-Indicates neighbors tree **P**-Indicates protected tree by city ordinance

## NO TREE REMOVALS ARE PROPOSED ON SITE

The trees surveyed are a mix of imported species. The only “Heritage” trees surveyed are neighboring trees #1 and #2. Chinese pistache street tree #2 is not of a heritage size but is protected as it is a street tree. All heritage trees have a bold **P** (protected) next to them in the survey to indicate a protected tree. The city of Menlo Park's definition of a heritage tree is as followed:

1. Any tree having a trunk with a circumference of 47.1 inches (diameter of 15 inches) or more measured at 54 inches above natural grade.
2. Any oak tree native to California, with a circumference of 31.4 inches (diameter of 10 inches) or more measured at 54 inches above natural grade.
3. Any tree or group of trees specifically designated by the City Council for protection because of its historical significance, special character or community benefit.
4. Any tree with more than one trunk measured at the point where the trunks divide, with a circumference of 47.1 inches (diameter of 15 inches) or more, with the exception of trees that are under 12 feet in height, which are exempt from the ordinance.

All street trees regardless of size are considered protected trees.



**Summary:**

The trees on site are all small non protected trees. Neighboring trees #1 and #2 are both protected trees. Chinese pistache trees #2 is far from any proposed work and is not expected to be impacted. The Chinese pistache street tree is protected but not of a heritage size. Neighbor’s box elder tree is near the proposed second story addition. The tree was given a poor condition rating due to being topped in the past at 20 feet. The tree is also codominant at 1 foot with a poorly formed union.

**Showing topped limb (A) over home addition**



**Showing limb A that needs to be removed. Limb B is to be retained**

**Impacts/Recommendations:**

The second story addition will require pruning of the neighbor’s box elder tree #1. Only 1 limb will need to be removed (limb A in diagram). This limb is in the location of the second story addition and the only limb that needs to be removed. The limb to be removed has been topped in the past. The cut shall be made back to the parent limb. The cut will need to be done by a licensed tree care provider to make sure the cut is done correctly. Proper pruning cuts should be made outside the branch collar and should not remove or damage the branch collar. This will allow for proper closure of the wound made by the pruning cut. Any future sprout like growth shall be managed through future pruning. The pruning will result in a 15% loss of canopy and is well within industry standards. No impacts are expected from the needed pruning.

Branch B will be located 8 inches from the raised roof and can be retained. This branch is recommended to be protected by wrapping the limb with a 2-inch-thick layer of orange plastic fencing as padding for the limb. To the outside of the orange plastic fencing, wooden slats are recommended to be bound to the orange plastic fencing.



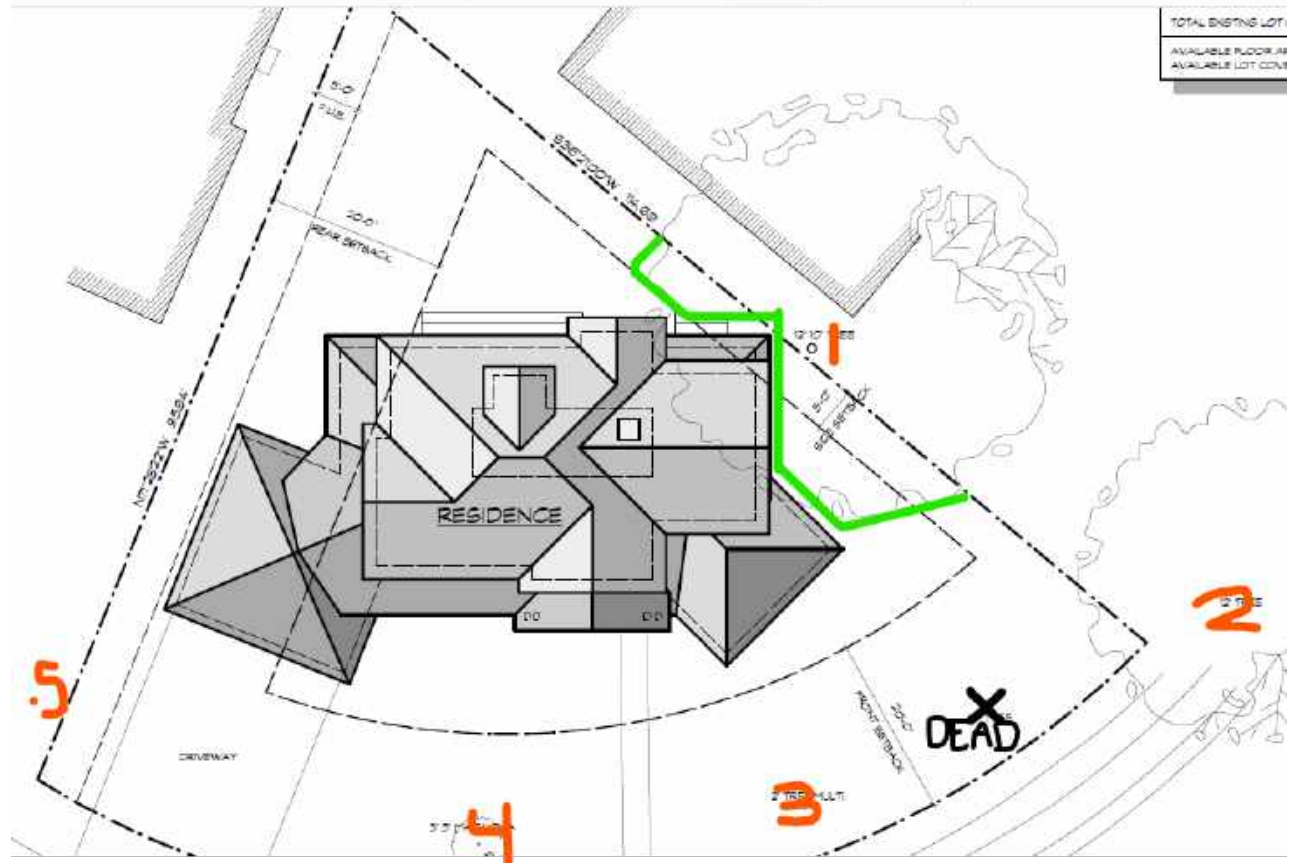
**Red line showing where the pruning cut shall take place**

The following tree protection plan will help to ensure the future survival of the trees on site.

**Tree Protection Plan:**

*Tree Protection Zones*

Tree protection zones should be installed and maintained throughout the entire length of the project. Fencing for tree protection zones should be 6' tall, metal chain link material supported by metal 1.5" diameter poles, pounded into the ground to a depth of no less than 2'. The distance between metal support poles shall not be more than 10'. The location for the protective fencing for the protected trees on site should be placed at the tree driplines where possible. Where it is not possible to place tree protection zones at the dripline because of approved proposed work or existing hardscapes, the tree protection fencing shall be placed at the edge of the proposed work or hardscapes, but not closer than 2 feet from the trunk of any tree. No equipment or materials shall be stored or cleaned inside the protection zones. Areas where tree protection fencing needs to be reduced for access, should be mulched with 6" of coarse wood chips with ½ inch plywood on top (landscape barrier). The plywood boards should be attached together in order to minimize movement. The spreading of chips will help to reduce compaction and improve soil structure. All tree protection measures must be installed prior to any demolition or construction activity at the site. The only tree on site to be protected is box elder tree #1. Below is a diagram showing the recommended tree protection zone for tree #1. Limb B (as discussed in previous paragraph) is to be protected by wrapping the limb with a 2-inch-thick layer of orange plastic fencing as padding for the limb. To the outside of the orange plastic fencing, wooden slats are recommended to be bound to the orange plastic fencing.



**Showing tree locations and recommended tree protection fencing location**

**Avoid the following conditions:**

**DO NOT:**

- A. Allow run off of spillage of damaging materials into the area below any tree canopy.
- B. Store materials, stockpile soil, or park or drive vehicles within the TPZ.
- C. Cut, break, skin, or bruise roots, branches, or trunks without first obtaining authorization from the City Arborist.
- D. Allow fires under and adjacent to trees.
- E. Discharge exhaust into foliage.
- F. Secure cable, chain, or rope to trees or shrubs.
- G. Trench, dig, or otherwise excavate within the dripline or TPZ of the tree(s) without first obtaining authorization from the City Arborist.
- H. Apply soil sterilant under pavement near existing trees.

*Landscape Buffer*

Where tree protection does not cover the entire root zone of the trees at the dripline, or when a smaller tree protection zone is needed for access, a landscape buffer consisting of wood chips spread to a depth of six inches with plywood or steel plates placed on top will be placed where foot traffic is expected to be heavy. The landscape buffer will help to reduce compaction to the unprotected root zone.



*Root Cutting and Grading*

Avoid injury to tree roots. When a ditching machine, which is being used outside of the dripline of trees, encounters roots smaller than 2", the wall of the trench adjacent to the trees shall be hand trimmed, making clear, clean cuts through the roots. All damaged, torn and cut roots shall be given a clean cut to remove ragged edges, which promote decay. Trenches shall be filled within 24 hours, but where this is not possible, the side of the trench adjacent to the trees shall be kept shaded with four layers of dampened, untreated burlap, wetted as frequently as necessary to keep the burlap wet. Roots 2" or larger, when encountered, shall be reported immediately to the Project Arborist, who will decide whether the Contractor may cut the root as mentioned above or shall excavate by hand or with compressed air under the root. Root is to be protected with dampened burlap. All roots to be cut shall be monitored and documented. Large roots (over 2" diameter) or large masses of roots to be cut must be inspected by the Project Arborist. The Project Arborist, at this time, may recommend irrigation or fertilization of the root zone. Existing grades underneath the protected tree driplines are to remain as is. If grade changes greater than 4 inches are to take place, special mitigation measures will be needed to reduce impacts to the trees.

***NO ROOT CUTTING EXPECTED ON THIS SITE****Trenching and Excavation (for any reason)*

Route pipes outside of the area that is 10 times the diameter of a protected tree to avoid conflict with roots. If this is not possible, trenching for irrigation, drainage, electrical or any other reason shall be done by hand in combination with an air spade when inside the dripline of a protected tree. Hand digging and the careful placement of pipes below or besides protected roots will significantly reduce root loss, thus reducing trauma to the tree. All trenches shall be backfilled with native materials and compacted to near its original level, as soon as possible. Trenches to be left open for a period of time, will require the covering of all exposed roots with burlap and be kept moist. The trenches will also need to be covered with plywood to help protect the exposed roots. When utilities need to be placed within a distance of 3 times the diameter or less of a protected tree on site, the Contractor shall bore beneath the dripline of the tree. The boring shall take place not less than 3' below the surface of the soil in order to avoid encountering "feeder" roots.

***NOT EXPECTED ON THIS SITE****Pruning*

Any needed or recommended pruning shall be supervised by the Project Arborist, and must be done by a licensed tree care provider. All pruning must stay underneath 25% of the total foliage of the canopy.

*Irrigation*

Normal irrigation shall be maintained on this site at all times. The imported trees will require normal irrigation. On a construction site, I recommend irrigation during winter months, 1 time per month. Seasonal rainfall may reduce the need for additional irrigation. During the warm season, April – November, my recommendation is to use heavy irrigation, 2 times per month. This type of irrigation should be started prior to any excavation. The irrigation will improve the vigor and water content of the trees. The on-site arborist may make adjustments to the irrigation recommendations as needed. The foliage of the trees may need cleaning if dust levels are extreme. Removing dust from the foliage will help to reduce mite and insect infestation.



*Construction related damage to trees*

Any damage due to construction activities shall be reported to the Project Arborist or City Arborist within six hours so that remedial action can be taken.

*Inspections*

The city of Menlo Park will require the Project Arborist to inspect the site before the building permit can be picked up to make sure the tree protection fencing has been well installed. Monthly inspections are also required. The pruning of the box elder tree must be inspected.

The information included in this report is believed to be true and based on sound arboricultural principles and practices.

Sincerely, Kevin R. Kielty Certified Arborist WE#0476A



**Kielty Arborist Services**

P.O. Box 6187  
San Mateo, CA 94403  
650-515-9783

**ARBORIST DISCLOSURE STATEMENT**

Arborists are tree specialists who use their education, knowledge, training and experience to examine trees, recommend measures to enhance the beauty and health of trees, and attempt to reduce the risk of living near trees. Clients may choose to accept or disregard the recommendations of the arborist, or seek additional advice.

Arborists cannot detect every condition that could possibly lead to the structural failure of a tree. Trees are living organisms that fail in ways we do not fully understand. Conditions are often hidden within trees and below ground. Arborists cannot guarantee that a tree will be healthy or safe under all circumstances, or for a specified period of time. Likewise, remedial treatments, like a medicine, cannot be guaranteed.

Treatment, pruning, and removal of trees may involve considerations beyond the scope of the arborist's services such as property boundaries, property ownership, site lines, disputes between neighbors, landlord-tenant matters, etc. Arborists cannot take such issues into account unless complete and accurate information is given to the arborist. The person hiring the arborist accepts full responsibility for authorizing the recommended treatment or remedial measures.

*Trees can be managed, but they cannot be controlled. To live near a tree is to accept some degree of risk. The only way to eliminate all risks is to eliminate all trees.*

Arborist: Kevin R. Kielty  
Kevin R. Kielty

Date: December 3, 2019



October 24, 2019

City of Menlo Park  
Community Development Department  
701 Laurel Street, Menlo Park, CA 94025

Project: Variance application for 276 Hedge Road

Dear Planning Commission,

We are requesting a variance for a second floor addition and remodel to an existing single-story residence on a reverse pie-shaped lot at 276 Hedge Road. We believe that the five findings can be made for the variance from the rear setback and daylight plane requirements in the Zoning Ordinance.

1. The hardship at 276 Hedge Road is created by the shape of the lot, the placement of the existing building, and the setback requirements. The narrow rear portion of the lot and the placement of the existing residence limit the buildable area for a second floor addition. The subject lot has three property lines as opposed to a standard lot, which has four. With the current orientation of the house, this locates the side of the existing residence within a 20-foot rear yard setback. The property does not have a left side setback due to the irregular shape of the lot, further limiting the buildable area on the lot. As demonstrated in the alternate exhibits, to make the design compliant with the strict setbacks of the lot we would lose the clean roof forms and simple massing that embody the Modern Farmhouse style.
2. The variance is necessary to use the rear portion of the lot more efficiently and to not overwhelm the front façade by pushing the second floor forward on the property. The majority of properties in the neighborhood have rectangular lots with typical 5-foot side yard setbacks. Due to the 20'-0" rear setback being at the side of the house rather than parallel to the back, the owner is forced to locate any proposed development to the right of their property. Most other neighboring lots have two 5'-0" side yard setbacks, which allows the existing houses to be within the buildable area. Therefore, granting this variance will not allow special privilege.
3. Granting this variance will not be detrimental to the public health, safety, or welfare, and will not impair an adequate supply of light and air to adjacent properties. The neighboring properties are angled away from the subject lot and the addition is set further east than the neighboring houses, which limits any shading caused by the addition to avoid impact to the neighboring houses. The majority of the daylight coming from the south will still reach both adjacent properties because the houses are not located parallel

to each other and only have a corner of the proposed addition facing them. This also causes minimal impact on air supply to the adjacent lots.

4. The majority of the lots within in the R-1-U zone are rectangular, therefore the requested variance for this irregular shaped lot would not be applicable for other properties within this zone.
5. The Specific Plan regulations do not apply to the subject lot.

Sincerely,

Lawrence Kahle  
Metropolis Architecture  
445 N. Whisman Rd Suite #300  
Mountain View, CA 94043  
(650) 318-0211

**Hao Zhong and Family**

276 Hedge Rd.  
Menlo Park, CA 94025  
(765) 714-8757  
zh.anse@gmail.com

**Our Neighbor**

300 Hedge Rd.  
Menlo Park, CA 94025

Dear Neighbor,

I am writing to introduce me and my family, and let you know that we are planning to build a second-floor addition to our home at 276 Hedge Road.

I work for Facebook, and my wife and I have two children. We are excited to join the neighborhood as soon as the home addition project is finished. Since we bought our home earlier this year, we have rented it out to cover the costs before construction.

In the design for the second-floor addition, we've tried to protect neighbors' privacy by minimizing the windows facing neighbours on the second floor. Please review the drawings of the site plan and elevations in the attachment.

We are currently applying for building permits in the city government. If the permits are granted, we hope to commence building work around June, 2020. The construction will last for about 10 months. We will do our best to minimize the amount of noise disruption to you throughout the construction.

We apologize for any potential inconvenience. Please don't hesitate to reach out to me at (765)-714-8757 (phone) or [zh.anse@gmail.com](mailto:zh.anse@gmail.com) (email) if you have any concerns about our plans.

Sincerely,

**Hao Zhong**

28th October, 2019



## Outreach timeline

date	event
Oct. 29, 2019	Delivered mail to 300 Hedge Rd and 272 Hedge Rd (see attached for the mail sent to 300 Hedge Rd.; 272 Hedge Rd's mail is identical)
Nov. 24, 2019	Hao Zhong and Xiao Ge meeting with neighbors at 272 Hedge Rd

### **Feedback by 272 Hedge Rd based on the meeting on Nov. 24, 2019.**

Our neighbor at 272 Hedge Rd invited us to discuss the window design on the second floor for privacy concerns, as well as construction noise. After we explained about the house orientation and privacy considerations of window design, our neighbor realized they misunderstood the house orientation, and learned that the windows of the two bathrooms facing backyard on the second floor would not be concerning. In addition, they had been concerned about the windows over the staircase, because one of their windows faces directly at the back side of our house. They did not know the inner part of the house so they did not know what's inside the windows. But they are happy with window as-is after we told them that the windows are for the staircase. Our neighbor at 272 Hedge Rd also don't have any objection to the pruning of the tree as long as the city is okay with it.

Regarding construction noise, which might be unavoidable in certain stages of construction, we promised to notify our neighbors of any major noise ahead of time, so that they would be prepared and well informed throughout the project. To conclude, our neighbor at 272 Hedge Rd is very supportive of our project.



## STAFF REPORT

### Planning Commission

**Meeting Date:** 12/9/2019

**Staff Report Number:** 19-087-PC

**Public Hearing:** Architectural Control, Variance, Major Subdivision, Heritage Tree Removal Permit, and Below Market Rate Housing Agreement/Form 4 Architecture/706-716 Santa Cruz Avenue

### Recommendation

Staff recommends that the Planning Commission review and provide a recommendation that the City Council make the necessary findings and approve the proposed project at 706-716 Santa Cruz Avenue, located in the El Camino Real Downtown/Specific Plan (SP-ECR/D) zoning district, as outlined in Attachment A. The Planning Commission should provide recommendations to the City Council on the following resolutions for the entitlements for the proposed project:

1. Architectural control for the demolition of an existing commercial building and the construction of a new three-story, mixed-use building with below-grade parking, retail space and parking on the first level, office uses on the second level, and office uses and four residential units on the third level (Draft Resolution and Recommended Conditions of Approval in Attachment B);
2. Major Subdivision to create a Vesting Tentative Map not to exceed four residential condominium units and one commercial area, with rights reserved to allow up to ten commercial condominiums (Draft Resolution and Recommended Conditions of Approval in Attachment B);
3. Variance request to allow skylights on the third floor exceed the 38-foot maximum height limit;
4. Removal of one on-street parking space on Chestnut Street;
5. Heritage Tree Removal Permits for two heritage trees, one on-site tree located in the parking lot at the rear of the property and one street tree on Chestnut Street (Draft Resolution in Attachment C); and,
6. Below Market Rate (BMR) Housing Agreement for payment of an in-lieu fee in compliance with the City's below market rate housing program (Draft Resolution in Attachment D and BMR Term Sheet in Attachment E).

### Policy Issues

The proposed project requires the Planning Commission and City Council to consider the merits of the project, including project consistency with the El Camino Real/Downtown Specific Plan. The Commission and Council will need to consider architectural control, subdivision map, and variance findings. Further, resolutions regarding heritage tree removal permits and the BMR Housing Agreement for the project will need to be considered. The Planning Commission is a recommending body on the policy issues. The policy issues summarized here are discussed in greater detail throughout the staff report.

## **Background**

### ***Site location***

The subject site is 23,454 square feet and is located at 706-716 Santa Cruz Avenue, and is part the El Camino Real/Downtown Specific Plan (SP-ECR/D) zoning district and is within the Downtown (D) sub-district. The site is currently developed with a single-story commercial building and is occupied by several tenants, including Juban Yakiniku House (restaurant) and a computer repair service store. A private surface parking lot is located on the rear half of the site and is currently accessed by driveways on Chestnut Street and Chestnut Lane. A location map is included as Attachment F.

The subject site is a corner lot with frontages on Santa Cruz Avenue, Chestnut Street, and Chestnut Lane, where Santa Cruz Avenue serves as the front and Chestnut Lane serves as the rear. The surrounding properties are likewise part of the SP-ECR/D district, and generally consist of commercial buildings. Using Santa Cruz Avenue in a north-south orientation, the parcel to the west of the project site and across Chestnut Lane is occupied by Axion Learning Center and several small businesses. The parcels across Chestnut Street to the south and Santa Cruz Avenue to the east contain multiple small businesses and restaurants. The parcel to the north of the site is occupied by Ace Hardware. To the northwest of the property is City parking plaza #1.

### ***Previous Planning Commission review***

On December 11, 2017 the Planning Commission reviewed an architectural control request for a mixed-use building containing 13,018 square feet of retail space on the first level, 19,128 square feet of non-medical office space on the second level, and four residential units totaling 14,762 square feet on the third level, and continued the project to a future meeting date with the following direction:

- Planning staff to confer with Public Works (Transportation/Engineering) and the Menlo Park Fire Protection District as to what could be committed to on Chestnut Lane to resolve a few Commissioners' and a neighboring property owner's concerns regarding pedestrian safety and vehicular impacts such as not requiring a sidewalk on the project side, expanding the sidewalk on the side opposite the project and installing safety barriers, and making traffic one-way.
- Based on information provided on Chestnut Lane's design feasibility, revise the site layout and circulation accordingly and also look at revising the design by using different materials, changing building rhythms, or some combination of efforts to create a building with more character and an intimate, pedestrian scale.

The December 11, 2017 Planning Commission staff report ([Hyperlink with all attachments](#)) and minutes are included as Attachments G and H, respectively. After the Planning Commission meeting, the applicant hired a new architect to prepare a comprehensive redesign of the project that also responded to the Planning Commission's direction. The primary changes included architectural and design style changes to the building facades and relocation of the proposed Chestnut Lane sidewalk within the property.

On September 17, 2018, the revised project was reviewed by the Planning Commission as a study session item. The Planning Commission provided the following feedback on the proposal:

- Concerns about the maintenance of the plantings on the third floor terraces;
- General support of the revised garage entry placement;
- Direction to switch the location of the trash and the electrical closet to provide better access to the trash;
- General support of the revised design and materials;
- Support for some kind of barrier between the proposed sidewalk and Chestnut Lane for pedestrian



- safety;
- Suggestions that the storefronts should be smaller scale to avoid losing the “Main Street”; and
- Interest in how signage would work on the building and how it could add interest to the storefronts.

The September 17, 2018 Planning Commission staff report (Hyperlink with all attachments) and minutes are included as Attachments I and J, respectively. The applicant has also provided a letter (Attachment K) indicating the changes made to the plans as a result of the Planning Commission meeting. The primary changes to the plans include refinements to the plans and elevations to comply with the development requirements, confirmation that the third floor planters could be maintained, and modifications to the location of the trash and electrical closet. No changes were made to the building massing or scale of the storefronts; however, lighting, planter pots, and future signage for each individual tenant would help create pedestrian scale storefronts.

## Analysis

### ***Project description***

The applicant is proposing to demolish the existing commercial building and construct a new mixed-use development consisting of two levels of below grade parking, retail space on the first level, non-medical office space on the second level, and non-medical office space and four residential units on the third level. The retail space would consist of 12,049 square feet, the office space would consist of 23,454 square feet and the residential units would consist of a total of 11,405 square feet. The residential units would include four, three-bedroom condominium units. A data table summarizing the parcel and project attributes is included as Attachment L. The project plans and the applicant’s project description letter are included as Attachments M and N, respectively. A table outlining attributes of the previous project proposals and current project proposal is included below.

Metric	Previous Proposal	Revised Proposal	Current Proposal
Lot Size	23,454 sf	23,454 sf	23,454 sf
GFA (Maximum)	46,908 sf	46,908 sf	46,908 sf
GFA (Proposed)	46,908 sf	46,903 sf	46908 sf
Non-Medical Office GFA (Maximum)	23,454 sf	23,454 sf	23,454 sf
Non-Medical Office GFA (Proposed)	19,128 sf	23,454 sf	23,454 sf
Retail GFA	13,018 sf	12,035 sf	12,049 sf
Residential Units (Maximum)	13	13	13
Residential Units (Proposed)	4 (14,762 sf)	4 (11,414 sf)	4 (11,405 sf)
Parking (Required)	55	68	68
Parking (Provided)	55	65	69

The proposal would meet the Specific Plan’s Base level standards, which were established to achieve

inherent public benefits, such as the redevelopment of underutilized properties, the creation of more vitality and activity, and the promotion of healthy living and sustainability. The maximum permitted base floor area ratio (FAR) for the D sub-district is 2.0 for all uses, inclusive of office, and the maximum FAR for non-medical office uses is half of the overall FAR. As a result, the subject parcel is limited to 46,908 square feet of total gross floor area and 23,454 square feet of office. The proposed project would be developed at these limits, with a total of 46,908 square feet (2.0 FAR) of gross floor area and a total of 23,454 square feet (1 FAR) of office space, including proportionally calculated common areas such as the lobby and stairs. The FAR has been calculated per the definition of Gross Floor Area, which includes all levels of a structure, with exemptions for covered parking and certain non-usable/non-occupiable areas.

The development would have a residential density of 7.4 dwelling units per acre, in compliance with the limit of 25 dwelling units per acre. The development would be 37.75 feet where 38 feet is the maximum allowed height and would adhere to the façade height limit of 30 feet. The applicant is requesting a variance for the third floor skylights to exceed the 38-foot maximum height, which is discussed in a following section. A four-foot tall parapet wall is proposed for the rooftop mechanical equipment screening and is not included in the maximum height of the building, as is permitted by the Specific Plan. The development complies with the building profile. The elevator, stairwells, awnings and parapet walls encroach into the building profile, which is permitted. The development would have a zero setback along the Santa Cruz Avenue, Chestnut Street and Chestnut Lane frontages and the side property line to the north, which is the required minimum and maximum setback for the zoning district. As specified by the Specific Plan, the development would be required to achieve LEED Silver certification (condition 8b).

### ***Design and materials***

The Specific Plan includes a detailed set of design standards and guidelines. Compliance with the standards and guidelines are evaluated in the Standards and Guidelines Project Compliance Worksheet (Attachment O). The following discussion highlights and expands on topics addressed in the Standards and Guidelines Project Compliance Worksheet.

### **Design Program and Concept**

Parking would be located on two below grade parking levels, except for four surface parking spaces. Parking and services would be accessed along Chestnut Lane at the far north edge of the lot to be as far away from businesses across Chestnut Lane as possible. Parking and services would account for about one-third of the first-floor footprint and would be minimally visible, even from Chestnut Lane. At the garage there would be a roll down metal gate set 22 feet 11 inches back from the edge of Chestnut Lane. The proposal would retain a sidewalk on Chestnut Lane with a covered colonnade walkway of five feet one inches of walking depth within an approximately seven-foot public access easement.

The public entry to the office spaces would be primarily at Lobby 1, which would be mid-façade at the major modulation along the Chestnut Street frontage. This entry has a small plaza space mostly covered by a second-floor balcony and entry canopy. Retail spaces 2 and 3 would also have secondary access/exit doors to this small entry plaza. Office, retail and residential users would share the entrance and stairs located at Lobby 1. Private entry to residential uses would also be provided off a separate lobby (Lobby 2) at the building corner at Chestnut Street and Chestnut Lane with doors facing Chestnut Street.

Three separate retail suites are noted on the first-floor plan, with three entry points at the modulations between storefronts, but retail spaces 1 and 2 could be combined into one larger suite facing Santa Cruz Avenue and all retail spaces could be divided into additional tenant spaces.

The retail entrances are well marked with recessed double doors and canopies, but storefronts themselves do not function as retail access points. Storefronts would have floor to ceiling glass recessed back from the two-story stone-clad primary façade plane. Above the storefronts would be large glazed window bays at the office level. The large bay windows are treated as special design features articulated with dark brown metal frames and curved canopies.

The building corner at Santa Cruz Avenue and Chestnut Street also would have a wide zone of glass at the first two levels to create a glaze building corner appearance. The proposal includes a substantial amount of clear glazing on the first level that could afford views into retail/food service spaces. The plans do not currently show operable windows or glazed doors facing the street other than at the three entry points at the building modulations but operable storefronts could be added at the building permit stage as long as the proposed storefronts are consistent with the proposed colors and materials.

While the first floor would be exclusively retail, except for lobbies, services and four parking spaces, the second floor would be a single large, 19,099-square foot office space with a large, linear terrace along the north/interior side lot line. There would be some small balconies at the modulations along Santa Cruz Avenue and Chestnut Street as well as a balcony next to the elevator and stair facing Chestnut Street. A feature of the second level offices would be the bay windows in three groupings along each street frontage.

The third floor would have 3,440 square feet of office space and 10,130 gross square feet of residential space in four units. This level's office space and its four residential units would be set back from the primary two-story façade along all frontages behind deep terraces, which address the building profile and façade height requirements. The depth of the terraces would be sufficient so that the building would appear two-stories tall along the sidewalk. The upper level's residential units would still be visible from across the street nearby to some extent particularly at the taller forms at the major modulations along the Santa Cruz Avenue and Chestnut Street frontages and at the Chestnut Street and Chestnut Lane corner. The alignment of the upper walls facing the streets, however, would also be varied with offsets and projections so that the mass of the upper floor would not be perceived as a singular box. This shape and form offsets on the third floor, plus the deep eaves at the second-floor roof projecting out from the primary façade plane as well as the third-floor planters at terrace edges would soften the building mass and reduce its overall scale.

Open space in the form of terraces would be a significant aspect of the design concept. The four residential units on the third level would each have large private terraces, mostly uncovered. There would also be a large terrace on third level for the office space facing the Chestnut Street and Santa Cruz Avenue intersection. On the second floor there would be a large terrace facing the interior lot line on the north side of the property and smaller balconies at major and minor modulations. The terrace would serve as an open space, but also allow light to access the north side of the office space, and increase light and glazing at the north side of residential units 2 and 3.

The flat upper roof (except for shallow slope for drainage) would not be accessible except for service through a roof hatch. The roof edge would mostly be extended eaves above the third floor with some parapets such as at modulations to give additional height as seen from the street. The mechanical equipment would be centered on the roof and possible PV panels would sit towards the Chestnut Street side of the roof relative to the mechanical equipment. The roof would also include some skylights on the third-floor office space that would not be visible from the street.

### Architectural Character

The building would have a contemporary design character with extensive zones of glazing accented by dark brown metal window frames. The glazed zones would provide transparency and would be articulated and highlighted by the metal window frames and panels, and metalwork at canopies and eaves. The glazed zones along with the deep eaves would complement and deemphasize the stone-clad primary façade planes, which otherwise would appear as strongly repetitive modules. Warm to neutral colors for stone cladding at the primary façade, deep wood clad soffit, and landscape at planters set just back from the second-floor's eaves would soften the overall appearance of the building. Projecting features, primarily the bay windows with curved canopies, would make strong focal points at each module and help articulate the form, while providing scale at the primary façade modules. Additionally, entrance canopies and sunshades would provide functional coverage for rain and shade from sun while also providing street level façade elements that would facilitate pedestrian scale.

The glazed building corner and treatment of the recessed but taller wall surfaces with gray color at the major modulations would help differentiate parts of the building and respond to specific locational aspects of the design. Also, the deep horizontal eave lines with linearly treated wood soffit material would provide a signature visual line against the sky, while providing a crowning feature that would help mitigate the visual mass of the building.

Overall, the forms have been well composed with focal points, material articulation, detailing, and a pleasant street scale on both the Santa Cruz Avenue and Chestnut Street frontages. The deep second floor eaves with the landscaped terrace edges would also make a sophisticated statement at the lower façade's roofline.

Along Chestnut Lane, the design responds to adjacency conditions by shifting the garage entry to the northwest corner of the lot. The garage opening would be set back from the rest of the Chestnut Lane façade, and the colonnade style walkway would allow pedestrians access through to parking in the adjacent city lots. The façade appearance would be similar to the street facades, even though it would face a secondary road (see rendering on A2.5).

### Materials and Detailing

In regard to building materials, finishes, and colors, the design would feature light beige stone tiles at the primary façade walls at the first and second levels. The stone would have a gold tint and display prominent veins on the finish. The joint pattern on the stone would simulate blocks about the size of concrete masonry units in a running bond pattern as seen on the renderings. The primary façade's light beige stone tiles would be accented by medium beige stone tiles at the wall base and at narrow horizontal bands on the upper wall.

The building modulations' vertical walls and the elevator tower would be rendered in warm gray stone cladding. These stone tiles would be somewhat cooler and more neutral than the beige stone but would have a similar darkness and surface appearance. The elevations/renderings show these stone tiles being square in pattern and the suggested joints being as narrow as possible to minimally express the joints between tile units. This would create a textural contrast with the running bond pattern of the beige tile sections of the facades. This difference in tile pattern would treat the gray clad forms as more monolithic elements, which would help the modulations stand out from the primary facades.

The main contrast to the stone cladding would be the extensive clear glass framed and modulated with dark brown metal mullions, doors, panels, sunshades, guardrails, and fascia trim. All the metalwork would be the same color, except the minimally visible gray planters and stainless steel, rail tops at the third-floor terraces.

The contrast of lighter warm stone color with glazing and dark brown metal creates a clear differentiation between the solids and voids in the façade, but the overall color palette at walls would be calm. Warming up the building's appearance would be the horizontal wood soffits that appear to be narrow wood strips stained to maintain their natural wood color or to have a slightly reddish cherry color. Since the overhangs are deep, the wood soffits would create a strong visual element as seen from the street.

Emphasis in the design is placed on patterns of metal and glass and the projecting glass bays with curved canopies as well as the contrast of the heavy stone at chunky columns with the lighter glazed zones and the deep, sweeping horizontal eaves and wood soffits. The detailing appears to be clean with narrow window mullions and large windows. The fenestration would have a varied pattern of horizontally proportioned panes and large square panes. Deep recesses from the wall face to the glazing and deep overhangs at rooflines and canopies provide ample opportunities for shadows on wall faces and windows.

Detailing at the edges of overhangs where the metal and wood elements come together are expressed as very clean lines with modern joinery in the renderings. Likewise, the stone tile parapets do not show any metal roof coping lapping down the wall face.

Exterior building lighting would generally be limited to wall sconces placed at the stone piers of the primary façade. These proposed fixtures, shown on Sheet E2.2 (first floor plan fixture locations) and Sheet E4.1 (fixture image), would have a narrow vertical cylinder shape with a natural copper finish on the fixture (with patina) and an opal glass light cover. The proposed fixture has a sophisticated modern look that matches the building well. The covered walkway along Chestnut Lane would be illuminated with flush mounted LED lights, also shown on Sheet E4.1.

## ***Parking and circulation***

### **Vehicular**

Vehicular access for the site would be provided by the garage entrance at the rear of the property on Chestnut Lane. Four parking spaces would be provided on the first level behind the first level retail use. An additional 65 parking spaces would be provided in a two level below-grade garage. These three levels of parking would provide a total of 69 off-street parking spaces, which is consistent with the Specific Plan requirements of 68 parking spaces. This property is part of the P (parking) district. When a P parcel is redeveloped, parking for the first 1.0 FAR is satisfied by replacing the parking provided on the parcel, in this case 18 spaces. The parking for the remaining FAR is provided based on the Specific Plan parking requirements. General office uses require 3.8 spaces per every 1,000 square feet and residential units require one space for every residential unit. For this development, the remaining office FAR is 12,049 square feet, which requires 46 parking spaces, and the four residential units require a total of four parking spaces.

The parking would be shared between the retail, office, and residential tenants and the parking spaces would not be designated for a specific use. The Specific Plan allows mixed-use developments to share parking. The parking garage entrance would feature a gate, which would remain open during retail store hours but would be closed in the evenings with card access for office and residential tenants.

Lobby 1 located in the center of the building would provide direct access from the garage to the retail, office, and residential uses. In addition, Lobby 2 located at the southwest corner of the building would provide access to residential uses. Covered parking and associated circulation (elevators/stairs) are exempt from the FAR calculations, as noted earlier. During the staff review process, the garage plans and parking requirements were reviewed by staff to confirm the accuracy of the conclusions of the plans and the Transportation Demand Management (TDM) program.



### Bicycle

The project would provide required bicycle parking in both short-term and long-term configurations. Short-term bicycle parking would be provided via racks in the public right-of-way. Long-term bicycle parking would be located in the underground garage level, with access provided both by the garage ramp as well as the elevators and stairs. Similar to vehicular parking, covered bicycle parking is exempt from FAR calculations.

### Pedestrian

The existing sidewalks on Chestnut Street and Santa Cruz Avenue would remain and would be repaired/replaced as needed to match the existing sidewalk. The existing sidewalks include a four-foot furnishing zone and a six-foot clear walking zone. New bike racks would be provided in the furnishing zone to meet the short-term bike parking requirements. The design of the bike racks would be consistent with the City standards.

The retail space, as well as the main lobbies, would feature direct access from the Santa Cruz Avenue, Chestnut Street, and Chestnut Lane sidewalks. Access to the office space and residential units on the second and third levels would also be provided from lobbies on Chestnut Street and Chestnut Lane. As part of the project, a public access easement would provide a four-foot wide sidewalk on the property along the rear of the property on Chestnut Lane. The sidewalk would increase pedestrian access and connectivity by providing a path of travel from Chestnut Street to the public parking plaza and the tenants fronting the parking plaza.

### Fire Access

Fire access for the project would be located on Chestnut Lane. To accommodate fire access in this location signage and red curbing would be installed preventing parking and/or stopping on Chestnut Lane. In order to meet the turning radius for the fire truck, one on-street parking space on Chestnut Street, closest to the intersection of Chestnut Street and Chestnut Lane, would be removed.

### ***Tentative Map***

The applicant is requesting approval of a vesting tentative map for a major subdivision to create four residential condominium units and one commercial area, with rights reserved to allow up to ten commercial condominiums on the existing shared lot. The potential condominium subdivision would allow the individual residential units and commercial condominiums to be bought or sold independently. The tentative map would give the property owner flexibility to divide the retail and office space into no more than 10 units without requiring an additional tentative map.

State law outlines factors that the Planning Commission may consider in reviewing the request for subdivisions. Specifically, there are five factors for the Planning Commission to consider.

The first consideration is whether the proposed subdivision is in conformance with the City's General Plan. The General Plan land use designation for the subject property is El Camino Real/Downtown Specific Plan, which is consistent with the SP-ECR/D zoning district. The proposed subdivision would not conflict with General Plan goals and policies, and would comply with the Zoning Ordinance and Subdivision Ordinance.

The second factor to consider is whether the site of the subdivision is physically suitable for the proposed type or density of the development. The proposed subdivision would meet all applicable regulations of the Subdivision Ordinance as well as all development regulations pertaining to the D district within the Specific Plan. The existing lot contains one commercial building in a developed area and the proposed subdivision would result in a three-story mixed-use building including retail and office use and four residential units.

The third and fourth factors are concerned with whether the design of the subdivision or proposed improvements is likely to cause substantial environmental damage or serious public health problems. The proposed subdivision is located within a fully developed neighborhood and all necessary utilities are readily available. In addition, the development of the properties would need to adhere to specific conditions of the Engineering Division, all applicable building codes, and requirements of other agencies such as the Sanitary District, Menlo Park Fire Protection District, and other utility companies. Adherence to the recommended conditions of approval, the mitigation monitoring and reporting program, and all applicable codes would eliminate substantial or serious environmental or public health impacts.

The final factor to consider is whether the proposed subdivision would conflict with any public access easements. No public access easements currently exist on the site, so there is no conflict.

Staff has reviewed the vesting tentative map and has found the map to be in compliance with State and City regulations subject to the conditions outlined in Attachment B. All standard and project specific conditions of approval would need to be complied with prior to recordation of the final map. The applicant would need to apply for the final map within two years of the approval date of the vesting tentative map. In order to deny the proposed subdivision, the City Council would need to make specific findings that would identify conditions or requirements of the State law or the City's ordinance that have not been satisfied.

### **Variance**

As part of this proposal, the applicant is requesting a variance to allow six skylights on the third floor to exceed the 38-foot maximum height limit by approximately three feet, two inches. The applicant has provided a variance request letter that has been included as Attachment P. The required variance findings are evaluated below in succession:

- 1. That a hardship peculiar to the property and not created by any act of the owner exists. In this context, personal, family or financial difficulties, loss of prospective profits and neighboring violations are not hardships justifying a variance. Further, a previous variance can never have set a precedent, for each case must be considered only on its individual merits;*

The applicant states that multiple hardships are presented by the parking requirement to replace the 18 existing spaces onsite and the requirement to incorporate a sidewalk as part of the project. Staff believes that the parking and the sidewalk requirement are not hardships that would affect the total height of the proposed skylights since feasible alternative options exist which would still meet the desired goal of providing 1.0 FAR of office space. The office space could be located over the required sidewalk by an overhang thus allowing more office area on the second level and not limiting the allowable footprint for the office space. The below grade parking also does not directly affect the proposed plate heights since it would be fully below grade. Additionally, the proposed third floor office space could be accommodated without the increased height provided from the skylights. Furthermore, the proposed skylights appear to be primarily motivated by the desire to have taller plate heights for the third floor office space, which is not considered in the variance findings.

- 2. That such variance is necessary for the preservation and enjoyment of substantial property rights possessed by other conforming property in the same vicinity and that a variance, if granted, would not constitute a special privilege of the recipient not enjoyed by his/her neighbors;*

The applicant states that the requested variance is necessary to achieve the allowed 1.0 FAR for office



space, mitigate the lower plate heights for the third floor office space and make the project financially feasible. Staff believes that there are reasonable alternatives for the enjoyment of property rights relative to other properties in the vicinity. The second floor office space currently has a plate height of 12.5 feet. If the goal is to increase the 10.25-foot plate height for the third floor, office space the second floor could be slightly reduced and the third floor increased to achieve plate heights of 11.4 feet for both floors. Additionally, skylights meeting the height requirement could be proposed that would provide light and the appearance of taller ceilings. The proposed variance is not necessary to achieve the allowed 1.0 FAR of office space and make the project financially feasible. Additionally, permitting the skylights to encroach further into the maximum height limit and effectively increasing the ceiling height above the maximum height requirement could constitute a special privilege.

- 3. That the granting of the variance will not be materially detrimental to the public health, safety, or welfare, or will not impair an adequate supply of light and air to adjacent property; and*

Although the skylights would affect the maximum height limit, staff believes that the limited size of the encroachment would not be particularly detrimental to the public health, safety, and welfare, or impair an adequate supply of light and air to the adjacent properties.

- 4. That the conditions upon which the requested variance is based would not be applicable, generally, to other property within the same zoning classification.*

The applicant cites the requirement to replace the existing onsite parking and to provide a sidewalk onsite as examples of the uniqueness of this situation. Staff believes that the particular site and development requirements, while presenting some constraints to development, are not particularly unique in this area. Many Specific Plan properties are required to provide sidewalk improvements and public access easements over portions of the property when redeveloped. Several other parcels in the specific plan are also required to provide replacement parking if they are redeveloped. Additionally, the two level below grade parking garage does not directly affect the plate heights of the proposed office spaces since it is located below grade. Staff believes that the justifications for this particular variance request would be broadly applicable to other properties in the Specific Plan area.

- 5. That the condition upon which the requested variance is based is an unusual factor that was not anticipated or discussed in detail during any applicable Specific Plan process.*

While maximum heights and allowed projections were discussed in detail during the Specific Plan process, skylights and their relationship to the maximum height limits was not discussed in detail.

Approval of a variance requires that all five findings be made; since staff believes several are not addressed, denial of the variance request is recommended. Findings to this effect are included in the recommended actions. Condition 8c allows the project to be revised and continue with administrative approval with the modification of the skylights to conform to all requirements of the Specific Plan. For the Planning Commission's reference, staff provided the applicant with feedback during the initial review process that the required variance findings did not appear to be applicable, and encouraged revisions to not exceed the maximum permitted height. However, the applicant elected to pursue this request, as is their option. The Commission does have the discretion to approve the variance if all of the findings to that effect can be specified.

### ***Undergrounding of Overhead Utilities***

Currently, overhead lines run along the Chestnut Lane and Chestnut Street frontages. The goal is to fully underground the lines along the property frontages; however, if this is infeasible as determined by the Director of Public Works, plans to partially underground the lines could be explored. The undergrounding plans are outlined on Sheet JT-2. The plans include the full undergrounding of the lines on Chestnut Lane. To accommodate the undergrounding a new pole would be installed in the parking plaza requiring the removal of one, non-heritage street tree. The plan also includes partial undergrounding along Chestnut Street. The lines running adjacent to the property would be undergrounded but the lines running across Chestnut Street would remain. One pole would be removed on Chestnut Street and one pole would be added further down Chestnut Street to accommodate the undergrounding. The final undergrounding plans would be finalized prior to building permit issuance, subject to review and approval by PG&E, City of Menlo Park, and the Menlo Park Fire Protection District.

### ***Open Space, Trees, and Landscaping***

#### Open Space

The project would exceed the minimum private open space requirement for the residential units. The minimum private open space requirement is 80 square feet for every residential unit. Each residential unit would have a private terrace, the smallest of which would be 178 square feet. The D zoning district does not require common open space for the entire development.

#### Trees

The applicant has submitted an arborist report (Attachment Q) detailing the species, size, and conditions of the significant trees on or near the site. The report determines the present condition, discusses the impacts of the proposed improvements, and provides recommendations for tree preservation. All recommendations identified in the arborist report would be ensured through condition 7q.

The applicant is proposing to remove two heritage trees, one street tree and one tree on the subject site, and three non-heritage trees. The heritage street tree is a 16-inch diameter Victorian box tree (Tree #10) located on Chestnut Street. According to the arborist report, this tree is in poor health with poor structure and decayed branches. The on-site heritage tree is a 29-inch diameter Californian bay tree (Tree #12) located at the southwest corner of the property that conflicts with the proposed construction. Based on the arborist report this tree is in fair condition, but has poor structure. Three non-heritage trees (Trees #11, #13 and #15) are also proposed for removal, two of which are street trees. Tree #11 is a 12-inch Victorian box tree located on Chestnut Street in poor health and Tree #15 is a 2-inch Chinese pistache in poor health. The City Arborist has conditionally approved of the heritage tree removals as tree #10 is in declining health and tree #12 is in conflict with the proposed development. A public notice of the City Arborist's conditional approval will be sent to residents within a 300-foot radius of the trees prior to the City Council meeting. The notice would identify the removals with a 10-day appeal period. If the tree removals are appealed, an EQC meeting would be scheduled for the trees prior to the City Council meeting. If no appeal is received, the heritage tree removal approvals would be conditional subject to the City Council's final action on the project and issuance of the building permits for the project.

The arborist report outlines tree protection measures to mitigate or avoid impacts to the existing trees. Tree protection fencing is required around the tree protection zone. Any digging and/or trenching in the tree protection zone shall be manually performed. The arborist report indicated that there should be no cutting of roots greater than one inch in diameter without prior assessment by the project arborist. The arborist report also references the potential street tree and heritage street tree removals and/or protection measures for the parking plaza trees referenced in the previous section on undergrounding.

The City's heritage tree replacement guideline for commercial/mixed-use projects is to replace heritage trees at a 2:1 ratio, although this can be adjusted at the City Arborist's discretion. Non-heritage street trees must be replaced at a 1:1 ratio. The heritage tree replacements must be of a species that can grow to heritage-size and street tree replacements must be consistent with the City designated street tree species. The applicant is proposing to provide four tree replacements as street trees to compensate for the loss of the two heritage trees and two non-heritage street trees. This would represent a 1:1 replacement ratio for the heritage trees and a 1:1 ratio for the street trees. The three tree replacements along the Chestnut Street frontage would be 48-inch-box Saratoga Laurel trees, which would be a larger size than the typical minimum 15-gallon planting. The remaining tree replacement would be located in the public parking plaza and the exact location of the tree replacement would be identified at the building permit stage. The City Arborist recommends approval of this replanting ratio based on the restricted planting area on the site due to the zero-foot setback requirements and the larger size of the replacement trees.

### Landscaping

The recesses on Chestnut Street and Santa Cruz Avenue would feature planter pots. Raised planters are proposed along Chestnut Lane between the public right-of-way and the proposed sidewalk. Additionally, three new Saratoga Laurel street trees would be planted along the Chestnut Street frontage, and brick paving would be used at entries and for the sidewalk at the corner of Chestnut Street and Santa Cruz Avenue. On the second level, the office terrace would feature planter pots and built-in planting areas. On the third level, rooftop landscape along the terrace edges outboard of the glass guardrails (lavender, kangaroo paw, and grasses) is a main design theme, and tall clumps of bamboo in planters on roof terraces would soften the building's presentation and provide color periodically. The plantings would include native grasses and low water plants. The plantings are subject to change and refinement at the building permit stage. Overall, considering the urban location with no setbacks allowed, there would be considerable use of landscape on the project site.

### ***Trash and recycling***

The development would have a shared trash and recycling area on the first level, adjacent to the surface parking. The plans have been reviewed and tentatively approved by the City's refuse collector, Recology.

### ***Below Market Rate Housing Agreement***

The applicant is required to comply with Chapter 16.96 of City's Municipal Code ("BMR Ordinance"), and with the BMR Housing Program Guidelines adopted by the City Council to implement the BMR Ordinance ("BMR Guidelines") as the project would exceed 10,000 square feet of commercial gross floor area. Because the project does not include five or more residential units, there is no BMR requirement that derives from the residential uses themselves. However, the increase in commercial square footage results in a requirement for 1.1 of a BMR unit (either on- or off-site) or the payment of in-lieu fees. The applicant's BMR proposal includes payment of an in-lieu fee of approximately \$410,262.48. The actual in-lieu fee is subject to change and would be based on the fees at the time of payment.

The revised project's BMR proposal was reviewed by the Housing Commission at their meeting on May 1, 2019. The Housing Commission unanimously recommended approval for the payment of an in-lieu fee based on the relatively small number of proposed units, construction costs of the units, and maintenance costs that would limit opportunities to incorporate a BMR unit as part of the proposed project. A draft resolution approving the BMR agreement is included as Attachment D and a draft BMR term sheet is included as Attachment E.

### **Correspondence**

Staff received have not received any additional letters regarding the proposed project. However, the neighboring rear property owner at 1142-1150 Chestnut Lane has been in communication with the applicant about the project since the Planning Commission study session.

### **Conclusion**

Staff believes the proposed contemporary architectural style of the building would provide well-proportioned massing and facades that are ordered but not too minimal. Forms and façade composition would be supported by varied use of materials, finishes, and color. The proposal would adhere to the extensive standards and guidelines established by the Specific Plan, as verified in detail in the Standards and Guidelines Compliance Worksheet.

The proposal would meet the Specific Plan's Base level standards, which were established to achieve inherent public benefits, such as the redevelopment of underutilized properties, the creation of more vitality and activity, and the promotion of healthy living and sustainability. Vehicular and bicycle parking requirements would be met, and the development would also provide a positive pedestrian experience. The removal of the two heritage trees is justified due to the trees' declining health and/or conflict with the proposed development. Three new street trees would be located along Chestnut Street and the existing trees would be protected during construction. New landscaping would be planted throughout the site and the private open space would exceed the minimum standards. Staff believes that there are feasible alternatives to the variance request for the third floor skylights, which can be addressed with the building permit. Staff recommends that the Planning Commission recommend approval of the proposed architectural control, BMR agreement, and major subdivision and denial of the proposed variance to the City Council.

### **Impact on City Resources**

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project. In addition, the proposed development would be subject to payment of Transportation Impact Fee (TIF), Specific Plan Transportation Infrastructure Proportionate Cost-Sharing Fee, and the El Camino Real/Downtown Specific Plan Preparation Fee. These required fees were established to account for projects' proportionate obligations.

### **Environmental Review**

The Specific Plan process included detailed review of projected environmental impacts through a program Environmental Impact Report (EIR), as required by the California Environmental Quality Act (CEQA). In compliance with CEQA requirements, the Draft EIR was released in April 2011, with a public comment period that closed in June 2011. The Final EIR, incorporating responses to Draft EIR comments, as well as text changes to parts of the Draft EIR itself, was released in April 2012, and certified along with the final Plan approvals in June 2012.

The Specific Plan EIR identifies no impacts or less-than-significant impacts in the following categories: Aesthetic Resources; Geology and Soils; Hydrology and Water Quality; Land Use Planning and Policies; Population and Housing; and Public Services and Utilities. The EIR identifies potentially significant environmental effects that, with mitigation, would be less than significant in the following categories: Biological Resources; Cultural Resources; Hazards and Hazardous Materials. The EIR identifies potentially significant environmental effects that will remain significant and unavoidable in the following categories: Air

Quality; Greenhouse Gases and Climate Change; Noise; and Transportation, Circulation and Parking. The Final EIR actions included adoption of a Statement of Overriding Considerations, which is a specific finding that the project includes substantial benefits that outweighs its significant, adverse environmental impact.

As specified in the Specific Plan EIR and the CEQA Guidelines, program EIRs provide the initial framework for review of discrete projects. In particular, projects of the scale of 706-716 Santa Cruz Avenue are required to be analyzed with regard to whether they would have impacts not examined in the Program EIR. This conformance checklist, which analyzes the project in relation to each environmental category in appropriate detail, is included as Attachment Q. As detailed in the conformance checklist, the proposed project would not result in greater impacts than were identified for the Program EIR. Relevant mitigation measures have been applied and would be adopted as part of the Mitigation Monitoring and Reporting Program (MMRP), which is included as Attachment R. Full compliance with the MMRP would be ensured through condition 8a. No new impacts have been identified and no new mitigation measures are required for the proposed project. Mitigations include construction-related best practices regarding air quality and noise, payment of transportation-impact-related fees (condition 8j), and implementation of a Transportation Demand Management (TDM) program (Attachment S). The MMRP also includes three completed mitigation measures related to cultural resources and hazardous materials. An environmental site assessment phase I, historic resource evaluation, and cultural resources evaluation were performed by qualified professionals and determined that the proposed project would have no additional impacts. These studies are available for review upon request.

Specific Plan Maximum Allowable Development

Per Section G.3, the Specific Plan establishes the maximum allowable net new development as follows:

- Residential uses: 680 units; and
- Non-residential uses, including retail, office and hotel: 474,000 square feet.

These totals are intended to reflect likely development throughout the Specific Plan area. As noted in the Plan, development in excess of these thresholds will require amending the Specific Plan and conducting additional environmental review.

If the project is approved and implemented, the Specific Plan Maximum Allowable Development would be revised to account for the net changes as follows:

<b>Table 2: Specific Plan Totals</b>		
	<b>Dwelling Units</b>	<b>Commercial Square Footage</b>
Existing	0	15,175
Proposed	4	35,503
Net Change	4	20,328
% of Maximum Allowable Development	0.6	4.3
Available Units & Commercial SF in SP if Project is Approved	183	63,474
Available Units & Commercial SF in SP if all Pending Projects in SP are Approved	162	19,312

## Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

## Attachments

- A. Recommended Action
- B. Draft Resolution Approving the Findings and Conditions for the Vesting Tentative Map and Architectural Control and denying Variance
- C. Draft Resolution Approving the Heritage Tree Removal Permits
- D. Draft Resolution Approving the BMR Agreement
- E. BMR Term Sheet
- F. Location Map
- G. Hyperlink: Planning Commission staff report, December 11, 2017 – <https://www.menlopark.org/DocumentCenter/View/16188/F3---706-Santa-Cruze-Ave?bidId=>
- H. Planning Commission Excerpt Minutes, December 11, 2017
- I. Hyperlink: Planning Commission staff report, September 17, 2018 – <https://www.menlopark.org/DocumentCenter/View/18660/H1---706-Santa-Cruz-Ave---Staff-Report?bidId=>
- J. Planning Commission Excerpt Minutes, September 17, 2018
- K. Response to study session comments
- L. Data Table
- M. Project Plans
- N. Project Description Letter
- O. Specific Plan Standards and Guidelines Compliance Worksheet
- P. Variance Letter
- Q. Arborist Report
- R. EIR Conformance Checklist
- S. Mitigation Monitoring and Reporting Program (MMRP)
- T. Transportation Demand Management (TDM) program

## Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public viewing at the Community Development Department.

## Exhibits to Be Provided at Meeting

Color and materials board



Report prepared by:  
Kaitie Meador, Senior Planner

Report reviewed by:  
Corinna Sandmeier, Senior Planner



**Attachment A  
Recommended Actions  
706-716 Santa Cruz Avenue**

**Architectural Control, Variance, and Vesting Tentative Map**

1. Adopt a Resolution of the City Council of the City of Menlo Park Approving Findings and Conditions for the Architectural Control and Vesting Tentative Map and denying a Variance for a project at 706-716 Santa Cruz Avenue (Attachment B)

**Heritage Tree Removal Permits**

2. Adopt a Resolution of the City Council of the City of Menlo Park Approving Heritage Tree Removal Permits for a project located at 706-716 Santa Cruz Avenue (Attachment C)

**Below Market Rate (BMR) Housing Agreement**

3. Adopt a Resolution Approving a Below Market Rate Housing Agreement with 706-716 Santa Cruz Ave LLC for a project located at 706-716 Santa Cruz Avenue (Attachment D)

*DRAFT – December 9, 2019*

RESOLUTION NO. \_\_\_\_\_

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO PARK APPROVING FINDINGS AND CONDITIONS FOR ARCHITECTURAL CONTROL AND A VESTING TENTATIVE SUBDIVISION MAP AND DENYING A VARIANCE FOR THE PROJECT LOCATED AT 706-716 SANTA CRUZ AVENUE**

**WHEREAS**, the City of Menlo Park (“City”) has received an application from 706-716 Santa Cruz Ave., LLC (“Applicant”), for an architectural control permit for a three story mixed use development with below grade parking and a vesting tentative subdivision map to create four residential condominium units and one commercial area, with rights reserved to allow up to 10 commercial condominiums, and a variance to allow third floor skylights to exceed the maximum height located at 706-716 Santa Cruz Avenue (“Project Site”);

**WHEREAS**, the findings and conditions for Architectural Control, Variance and Vesting Tentative Subdivision Map would ensure that all City requirements are applied consistently and correctly as part of the project’s implementation;

**WHEREAS**, the proposed project meets the El Camino Real/Downtown Specific Plan parking requirements and includes the removal of one on street parking space on Chestnut Street for fire access;

**WHEREAS**, Applicant has elected to satisfy the BMR requirement for the proposed project by payment of an in-lieu fee in accordance with the City’s Below Market Rate Housing Program;

**WHEREAS**, all required public notices and public hearings were duly given and held according to law; and

**WHEREAS**, after notice having been lawfully given, a public hearing was scheduled and held before the Planning Commission of the City of Menlo Park on December 9, 2019 whereat all persons interested therein might appear and be heard; and

**WHEREAS**, the Planning Commission of the City of Menlo Park having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter voted affirmatively to recommend to the City Council of the City of Menlo Park to approve the findings and conditions for Architectural Control and Vesting Tentative Subdivision Map and deny the Variance; and

**WHEREAS**, after notice having been lawfully given, a public hearing was scheduled and held before the City Council of the City of Menlo Park on January 28, 2020 whereat all persons interested therein might appear and be heard; and

**WHEREAS**, the City Council reviewed the project on January 28, 2020, and found the project is within the scope of the project covered by the El Camino Real/Downtown Specific Plan Program EIR, which was certified on June 5, 2012; and

**WHEREAS**, the City Council of the City of Menlo Park having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter voted affirmatively to approve the findings and conditions for Architectural Control and Vesting Tentative Subdivision Map and deny the Variance.

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Menlo Park hereby approves the findings and conditions for Architectural Control and Vesting Tentative Subdivision Map and denies the Variance attached hereto as Exhibit A and incorporated herein by this reference.

I, Judi Herren, City Clerk of Menlo Park, do hereby certify that the above and foregoing Council Resolution was duly and regularly passed and adopted at a meeting by said Council on the TBD day of TBD, 2020, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this \_\_\_\_\_ day of \_\_\_\_\_, 2020.

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Judi Herren  
City Clerk

<b>LOCATION:</b> 706-716 Santa Cruz Avenue	<b>PROJECT NUMBER:</b> PLN2016-00111	<b>APPLICANT:</b> Form 4 Architecture	<b>OWNER:</b> 706-716 Santa Cruz Ave, LLC
<p><b>REQUEST:</b> Request for architectural control for the demolition of an existing commercial building and the construction of a new three-story mixed use building with below ground parking, retail and parking on the first floor, office on the second floor, and office and four residential units on the third floor in the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district. The project includes a request for a major subdivision to create a vesting tentative map not to exceed four residential condominium units and one commercial area, with rights reserved for up to ten commercial condominium units, and a Below Market Rate (BMR) housing agreement for payment of an in-lieu fee in compliance with the City's below market rate housing program. The project also includes a variance request for skylights on the third floor to exceed the 38-foot height limit. Removal of one on-street parking space on Chestnut Street would be required to meet fire access requirements. As part of the proposed project, two heritage trees would be removed: one on-site tree located in the parking lot at the rear of the property and one street tree on Chestnut Street.</p>			
<b>DECISION ENTITY:</b> City Council		<b>DATE:</b> TBD	<b>ACTION:</b> TBD
<b>VOTE:</b> TBD (Carlton, Combs, Mueller, Nash, Taylor)			
<p><b>ACTION:</b></p> <ol style="list-style-type: none"> <li>1. Make findings with regard to the California Environmental Quality Act (CEQA) that the proposal is within the scope of the project covered by the El Camino Real/Downtown Specific Plan Program EIR, which was certified on June 5, 2012. Specifically, make findings that: <ol style="list-style-type: none"> <li>a. A checklist has been prepared detailing that no new effects could occur and no new mitigation measures would be required (Attachment R).</li> <li>b. Relevant mitigation measures have been incorporated into the project through the Mitigation Monitoring and Reporting Program (Attachment S), which is approved as part of this finding.</li> <li>c. Upon completion of project improvements, the Specific Plan Maximum Allowable Development will be adjusted by 4 residential units and 20,328 square feet of non-residential uses, accounting for the project's net share of the Plan's overall projected development and associated impacts.</li> </ol> </li> <li>2. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval: <ol style="list-style-type: none"> <li>a. The general appearance of the structure is in keeping with the character of the neighborhood. The building will feature a contemporary design style and will meet relevant massing and modulation standards of the El Camino Real/Downtown Specific Plan, which will provide variety and visual interest, as well as avoid long or monotonous facades. The proposed exterior materials and finishes would be high quality in nature and would reinforce the neighborhood compatibility.</li> <li>b. The development will not be detrimental to the harmonious and orderly growth of the City. With the exception of the variance request, the project would meet the relevant development standards of the El Camino Real/Downtown Specific Plan.</li> <li>c. The development will not impair the desirability of investment or occupation in the neighborhood. The construction and ongoing occupation of the site would proceed in accordance with all applicable City requirements and procedures, as verified in these conditions of approval and would not impair the desirability of investment or occupation in the neighborhood.</li> </ol> </li> </ol>			

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<p><b>ACTION:</b></p> <ul style="list-style-type: none"> <li>d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking. The proposed project meets the parking requirements.</li> <li>e. The development is consistent with the El Camino Real/Downtown Specific Plan, as verified in detail in the Standards and Guidelines Compliance Worksheet (Attachment O).</li> </ul> <p>3. Recommend that the City Council make the findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.</p> <p>4. Approve the Below Market Rate Housing In-Lieu Fee Agreement (Attachment E) in accordance with the City's Below Market Rate Housing Program.</p> <p>5. Make the following findings as per Section 16.82.340 of the Zoning Ordinance pertaining to the granting of a variance:</p> <ul style="list-style-type: none"> <li>a. While the location of the site and the development requirement provide some unique constraints, the required parking and the sidewalk are not hardships that would affect the total height of the proposed skylights since feasible alternative options exist which would still meet the desired goal of providing 1.0 FAR of office space. The office space could be located over the required sidewalk by an overhang thus allowing more office area on the second level and not limiting the allowable footprint for the office space. The below grade parking also does not directly affect the proposed plate heights since it would be fully below grade. Additionally, the proposed third floor office space could be accommodated without the increased height provided from the skylights.</li> <li>b. There are reasonable alternatives for the enjoyment of property rights relative to other properties in the vicinity. The plate height for the second floor office space could be slightly reduced to increase the foot plate height for the third floor office space. Additionally, skylights meeting the height requirement could be proposed that would provide light and the appearance of taller ceilings. The proposed variance is not necessary to achieve the allowed 1.0 FAR of office space and make the project financial feasible. Additionally, permitting the skylights to encroach further into the maximum height limit and effectively</li> </ul>			

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<p><b>ACTION:</b></p> <p>increasing the ceiling height above the maximum height requirement could constitute a special privilege.</p> <p>c. Although the skylights would affect the maximum height limit, staff believes that the limited size of the encroachment would not be particularly detrimental to the public health, safety, and welfare, or impair an adequate supply of light and air to the adjacent properties.</p> <p>d. While the particular site and development requirements present some constraints to development, they are not particularly unique in this area. Many Specific Plan properties are required to provide sidewalk improvements and public access easements over portions of the property when redeveloped. Several other parcels in the specific plan are also required to provide replacement parking if they are redeveloped. Additionally, the two level below grade parking garage does not directly affect the plate heights of the proposed office spaces since it is located below grade. The granting of this variance could be applicable, generally, to other property in the Specific Plan area.</p> <p>e. While maximum heights and allowed projections were discussed in detail during the Specific Plan process, skylights and their relationship to the maximum height limits was not discussed in detail.</p> <p>6. Deny the variance.</p> <p>7. Approve the architectural control subject to the following <b>standard</b> conditions:</p> <p>a. Development of the project shall be substantially in conformance with the plans prepared by Form 4 Architecture, consisting of 87 plan sheets, dated received on December 3, 2019, approved by the Planning Commission on December 9, 2019, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.</p> <p>b. Prior to building permit issuance, the applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.</p>			

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<p><b>ACTION:</b></p> <ul style="list-style-type: none"> <li>c. Prior to building permit issuance, the applicant shall comply with all Sanitary District, Menlo Park Fire Protection District, California Water Company and utility companies' regulations that are directly applicable to the project.</li> <li>d. Prior to commencing any work within the right-of-way or public easements, the applicant shall obtain an encroachment permit from the appropriate reviewing jurisdiction.</li> <li>e. Simultaneous with the submittal of a complete building permit application or Final Map, Stormwater Pollution Prevention Program Best Management Practices (BMPs) for construction shall be implemented to protect water quality, in accordance with the approved Stormwater Pollution Prevention Plan (SWPPP). BMP plan sheets are available electronically for inserting into Project plans. The plan is subject to the review and approval of the Engineering Division.</li> <li>f. Simultaneous with the submittal of a complete building permit application or Final Map approval, the applicant shall submit a plan for: 1) construction safety fences around the periphery of the construction area, 2) dust control, 3) air pollution control, 4) erosion and sedimentation control, 5) tree protection fencing, and 6) construction vehicle parking. The plans shall be subject to review and approval by the Building, Engineering, and Planning Divisions prior to issuance of a building permit. The fences and erosion and sedimentation control measures shall be installed according to the approved plan prior to commencing construction.</li> <li>g. Simultaneous with the submittal of a complete building permit application or Final Map, the Applicant shall submit a draft "Stormwater Treatment Measures Operations and Maintenance (O&amp;M) Agreement" with the City subject to review and approval by the Engineering Division. The property owner will be responsible for the operation and maintenance of stormwater treatment measures for the project. The agreement shall be recorded and documentation shall be provided to the City prior to final occupancy.</li> <li>h. Simultaneous with the submittal of a complete building permit application or Final Map, the applicant shall submit a Grading and Drainage Plan for review and approval by the Engineering Division. Post-construction runoff into the storm drain shall not exceed pre-construction runoff levels. A Hydrology Report will be required to the satisfaction of the Engineering Division. Slopes for the first 10 feet perpendicular to the structure must be</li> </ul>			



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<p><b>ACTION:</b></p> <p>5% minimum for pervious surfaces and 2% minimum for impervious surfaces, including roadways and parking areas, as required by CBC §1804.3. Discharges from the garage ramp and underground parking areas are not allowed into the storm drain system. Discharge must be treated with an oil/water separator and must connect to the sanitary sewer system. This will require a permit from West Bay Sanitary District.</p> <ul style="list-style-type: none"> <li>i. Simultaneous with the submittal of a complete building permit application or Final Map, the Applicant shall submit engineered Off-Site Improvement Plans (including specifications &amp; engineers cost estimates), for approval by the Engineering Division, showing the infrastructure necessary to serve the Project. The Improvement Plans shall include, but are not limited to, all engineering calculations necessary to substantiate the design, proposed roadways, drainage improvements, utilities, traffic control devices, retaining walls, sanitary sewers, and storm drains, pump/lift stations, street lightings, common area landscaping and other project improvements. All public improvements shall be designed and constructed to the satisfaction of the Engineering Division. The Off-Site Improvements Plan shall be approved prior to issuance of a building permit or Final Map.</li> <li>j. Prior to building permit issuance or Final Map approval, the applicant shall submit plans to remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for the review and approval of the Engineering Division.</li> <li>k. During the design phase of the construction drawings, all potential utility conflicts shall be potholed with actual depths recorded on the improvement plans submitted for City review and approval.</li> <li>l. Simultaneous with the submittal of a complete building permit application or Final Map, and as part of the off-site improvements plan, the applicant shall submit plans for street light design per City standards, at locations approved by the City. All street lights along the project frontages shall be painted Mesa Brown and upgraded with LED fixtures compliant with PG&amp;E standards, and are subject to the review and approval of the Engineering Division.</li> <li>m. Simultaneous with the submittal of a complete building permit application or Final Map, the applicant shall provide documentation indicating the amount of irrigated landscaping. If the</li> </ul>			

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<p><b>ACTION:</b></p> <p>project proposes more than 500 square feet of irrigated landscaping, it is subject to the City's Water Efficient Landscaping Ordinance (Municipal Code Chapter 12.44).</p> <ul style="list-style-type: none"> <li>n. Simultaneous with the submittal of a complete building permit application or Final Map, the applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.</li> <li>o. If construction is not complete by the start of the wet season (October 1 through April 30), the applicant shall implement a winterization program to minimize the potential for erosion and sedimentation. As appropriate to the site and status of construction, winterization requirements shall include inspecting/maintaining/cleaning all soil erosion and sedimentation controls prior to, during, and immediately after each storm event; stabilizing disturbed soils through temporary or permanent seeding, mulching, matting, tarping or other physical means; rocking unpaved vehicle access to limit dispersion of mulch onto public right-of-way; and covering/tarping stored construction materials, fuels, and other chemicals. Plans to include proposed measures to prevent erosion and polluted runoff from all site conditions shall be submitted for review and approval of the Engineering Division prior to beginning construction.</li> <li>p. The Applicant shall retain a civil engineer to prepare "as-built" or "record" drawings of public improvements, and the drawings shall be submitted in AutoCAD and Adobe PDF formats to the Engineering Division prior to Final Occupancy.</li> <li>q. Street trees and heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the recommendations of the arborist report prepared by Arbor Resources, dated October 26, 2018. Applicant shall submit a tree preservation plan, detailing the location of and methods for all tree protection measures as part of a complete building permit application and is subject to review and approval by the City prior to building permit issuance.</li> <li>r. Prior to building permit issuance, the applicant shall pay all Public Works fees. Refer to City of Menlo Park Master Fee Schedule.</li> </ul>			

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<p><b>ACTION:</b></p> <ul style="list-style-type: none"> <li>s. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a lighting plan, providing the location, architectural details and specifications for all exterior lighting subject to review and approval by the Planning Division.</li> <li>t. Simultaneous with the submittal of a complete building permit application, a design-level geotechnical investigation report shall be submitted to the Building Division for review and confirmation that the proposed development fully complies with the California Building Code. The report shall determine the project site's surface geotechnical conditions and address potential seismic hazards. The report shall identify building techniques appropriate to minimize seismic damage.</li> <li>u. A complete building permit application will be required for any remediation work that requires a building permit. No remediation work that requires approval of a building permit shall be initiated until the applicant has received building permit approvals for that work. All building permit applications are subject to the review and approval of the Building Division.</li> <li>v. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans for construction related parking management, construction staging, material storage and Traffic Control Handling Plan (TCHP) to be reviewed and approved by the City. The applicant shall secure adequate parking for any and all construction trades, until the parking podium is available on the project site. Construction parking in the public parking plazas will be subject to City review and approval. The plan shall include construction phasing and anticipated method of traffic handling for each phase.</li> <li>w. All public right-of-way improvements, including frontage improvements and the dedication of easements and public right-of-way, shall be completed to the satisfaction of the Engineering Division prior to building permit final inspection.</li> <li>x. Within two years from the date of approval of the tentative map, the Applicant shall submit a Final Map for City approval.</li> <li>y. Applicant shall adhere to the Subdivision Map Act and Chapter 15 of the City's Municipal Code.</li> </ul>			

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<b>VOTE:</b> TBD (Carlton, Combs, Mueller, Nash, Taylor)			
<p><b>ACTION:</b></p> <ul style="list-style-type: none"> <li>z. Prior to building permit issuance or Final Map approval, Applicant shall submit an updated Storm Water Management plan with review of 3rd party engineer's certification.</li> <li>aa. Prior to building permit issuance or Final Map approval, Applicant shall submit draft Covenants, Conditions and Restrictions (CC&amp;Rs) to the City for review and approval. The CC&amp;Rs shall provide for the maintenance of all infrastructure and utilities within the Project site or constructed to serve the Project. This shall include, but not be limited to, the private open spaces, shared parking spaces, common walkways, common landscaping, and the stormwater drainage and sewer collection systems.</li> <li>bb. Prior to building permit issuance or Final Map approval, the Applicant shall enter into a Subdivision Improvement Agreement and provide a performance bond for the completion of the off-site improvements as shown on the approved project improvement plans. The Applicant shall obtain an encroachment permit, from the appropriate reviewing jurisdiction, prior to commencing any work within the right-of-way or public easements.</li> <li>cc. Prior to building permit issuance or Final Map approval, Applicant shall submit plans to underground the overhead utilities on Chestnut Street and Chestnut Lane consistent with the undergrounding scope indicated in the approved plan set. The scope of the undergrounding will be to the satisfaction of the Fire District, the City of Menlo Park and PG&amp;E. All lateral connections to overhead electric, fiber optic, and communication lines shall be placed in a joint trench.</li> <li>dd. All agreements shall run with the land and shall be recorded with the San Mateo County Recorder's Office prior to building permit final inspection.</li> <li>ee. Street trees shall be from the City-approved street tree species or to the satisfaction of City Arborist. Irrigation within public right of way shall comply with City Standard Details LS-1 through LS-19.</li> <li>ff. Prior to issuance of the building permit, the Owner/Applicant shall submit designs to demonstrate the proposed shoring tie-back/soil nails system does not adversely affect any existing or future utilities and/or any other City infrastructure, to the satisfaction of the Engineering Division. I-beams and appurtenances associated with the shoring plan, other than tie-back cables/soil nails, cannot be placed in the ROW.</li> </ul>			

<b>LOCATION:</b> 706-716 Santa Cruz Avenue	<b>PROJECT NUMBER:</b> PLN2016-00111	<b>APPLICANT:</b> Form 4 Architecture	<b>OWNER:</b> 706-716 Santa Cruz Ave, LLC
<b>REQUEST:</b> Request for architectural control for the demolition of an existing commercial building and the construction of a new three-story mixed use building with below ground parking, retail and parking on the first floor, office on the second floor, and office and four residential units on the third floor in the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district. The project includes a request for a major subdivision to create a vesting tentative map not to exceed four residential condominium units and one commercial area, with rights reserved for up to ten commercial condominium units, and a Below Market Rate (BMR) housing agreement for payment of an in-lieu fee in compliance with the City's below market rate housing program. The project also includes a variance request for skylights on the third floor to exceed the 38-foot height limit. Removal of one on-street parking space on Chestnut Street would be required to meet fire access requirements. As part of the proposed project, two heritage trees would be removed: one on-site tree located in the parking lot at the rear of the property and one street tree on Chestnut Street.			
<b>DECISION ENTITY:</b> City Council		<b>DATE:</b> TBD	<b>ACTION:</b> TBD
<b>VOTE:</b> TBD (Carlton, Combs, Mueller, Nash, Taylor)			
<p><b>ACTION:</b></p> <ul style="list-style-type: none"> <li>gg. Prior to issuance of the building permit, the Owner/Applicant shall enter into a Tie-Back Agreement with the City and pay the associated fees for the tie-backs encroaching and remaining into the right of way associated with the project in a form approved by the City Attorney, which agreement shall be recorded and shall be binding on future owners of the property. Notarized agreements will be required between the project and the adjacent property owners if the project plans to tie-back encroaching into private properties.</li> <li>hh. Prior to issuance of the building permit, the Applicant shall install reference elevation/benchmarks to monitor ground movement in the vicinity of the shoring system at the current centerlines of Santa Cruz Avenue, Chestnut Street and Chestnut Lane before, during and after excavations. The benchmarks shall be surveyed by a licensed surveyor and tied to an existing city monument or benchmark. The benchmarks shall be monitored for horizontal and vertical displacement of Oak Grove Avenue improvements. Tie-back systems shall comply with the City's Tie-Back Guidelines.</li> <li>ii. A landscape audit report shall be submitted to the Engineering Division prior to final inspection.</li> <li>jj. The streets adjoining the project shall receive an asphalt concrete overlay at the completion of improvements. Existing striping, markings, and legends shall be replaced in kind, or as modified by the City Engineer. <ul style="list-style-type: none"> <li>a) Santa Cruz Avenue: Replacement of curb/gutter, sidewalk and 3" grind/overlay from the curb to the median island.</li> <li>b) Chestnut Street: Replacement of curb/gutter, sidewalk and 3" grind/overlay from curb to curb.</li> <li>c) Chestnut Lane: Construction of a valley gutter and 3" grind/overlay from curb to curb.</li> </ul> </li> </ul> <p>8. Approve the architectural control subject to the following <b>project-specific</b> conditions:</p> <ul style="list-style-type: none"> <li>a. The applicant shall address all Mitigation Monitoring and Reporting Program (MMRP) requirements as specified in the MMRP (Attachment S). Failure to meet these requirements may result in delays to the building permit issuance, stop work orders during construction, and/or fines.</li> </ul>			

<b>LOCATION:</b> 706-716 Santa Cruz Avenue	<b>PROJECT NUMBER:</b> PLN2016-00111	<b>APPLICANT:</b> Form 4 Architecture	<b>OWNER:</b> 706-716 Santa Cruz Ave, LLC
<p><b>REQUEST:</b> Request for architectural control for the demolition of an existing commercial building and the construction of a new three-story mixed use building with below ground parking, retail and parking on the first floor, office on the second floor, and office and four residential units on the third floor in the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district. The project includes a request for a major subdivision to create a vesting tentative map not to exceed four residential condominium units and one commercial area, with rights reserved for up to ten commercial condominium units, and a Below Market Rate (BMR) housing agreement for payment of an in-lieu fee in compliance with the City's below market rate housing program. The project also includes a variance request for skylights on the third floor to exceed the 38-foot height limit. Removal of one on-street parking space on Chestnut Street would be required to meet fire access requirements. As part of the proposed project, two heritage trees would be removed: one on-site tree located in the parking lot at the rear of the property and one street tree on Chestnut Street.</p>			
<b>DECISION ENTITY:</b> City Council		<b>DATE:</b> TBD	<b>ACTION:</b> TBD
<b>VOTE:</b> TBD (Carlton, Combs, Mueller, Nash, Taylor)			
<p><b>ACTION:</b></p> <ul style="list-style-type: none"> <li>b. Simultaneous with the submittal of a complete building permit application, the applicant shall submit an updated LEED Checklist, subject to review and approval of the Planning Division. The Checklist shall be prepared by a LEED Accredited Professional (LEED AP). The LEED AP should submit a cover letter stating their qualifications, and confirm that they have prepared the Checklist and that the information presented is accurate. Confirmation that the project conceptually achieves LEED Silver certification shall be required before issuance of the building permit. Prior to final inspection of the building permit or as early as the project can be certified by the United States Green Building Council, the project shall submit verification that the development has achieved final LEED Silver certification.</li> <li>c. Simultaneous with the submittal of a complete building permit application, the applicant shall submit revised plans with the removal of the skylights that do not conform to current requirements, subject to Planning Division review and approval.</li> <li>d. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a full shoring plan subject to review and approval of the Planning and Building Divisions.</li> <li>e. Chestnut Lane along the property frontage shall not be used as a loading zone. Simultaneous with the submittal of a complete building permit application or Final Map, the applicant shall provide plans that include a red curb and no stopping signs on Chestnut Lane as part of the off-site improvement plan, subject to the review of the Engineering, Transportation and Planning Divisions.</li> <li>f. Any nonstandard improvements within public right-of-way shall be maintained in perpetuity by the owner. Owner shall execute an Agreement to maintain non-standard sidewalks and planting strips if any. Agreement shall be recorded prior to final occupancy.</li> <li>g. Prior to issuance of building permits, the applicant shall identify the location of the parking plaza replacement tree. The location, species and size of the proposed tree shall be subject to City Arborist review and approval.</li> <li>h. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a signed copy of the BMR agreement, subject to review and approval of the Planning</li> </ul>			

<b>LOCATION:</b> 706-716 Santa Cruz Avenue	<b>PROJECT NUMBER:</b> PLN2016-00111	<b>APPLICANT:</b> Form 4 Architecture	<b>OWNER:</b> 706-716 Santa Cruz Ave, LLC
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<b>DECISION ENTITY:</b> City Council		<b>DATE:</b> TBD	<b>ACTION:</b> TBD
<b>VOTE:</b> TBD (Carlton, Combs, Mueller, Nash, Taylor)			
<p><b>ACTION:</b></p> <p>and Housing Divisions. The payment of the BMR in-lieu fee must be made prior to issuance of the building permits.</p> <ul style="list-style-type: none"> <li>i. Prior to issuance of building permit, the applicant shall submit the El Camino Real/Downtown Specific Plan Preparation Fee, which is established at \$1.13/square foot for all net new development. For the subject proposal, the fee is estimated at \$22,970.64 (\$1.13 x 20,328 net new square feet).</li> <li>j. Prior to issuance of building permit, the applicant shall submit all relevant transportation impact fees (TIF), subject to review and approval of the Transportation Division. Such fees include: <ul style="list-style-type: none"> <li>i. The TIF is estimated to be \$122,274, which includes a credit for the existing retail square footage. The fee was calculated as follows: (\$5.01/s.f. x 24,454 s.f. office) + (\$5.01/s.f. x 12,049 s.f. retail) + (\$2,083.03/unit x 4 multi-family units). Please note this fee is updated annually on July 1st based on the Engineering News Record Bay Area Construction Cost Index. Fees are due before a building permit is issued.</li> <li>ii. The City has adopted a Supplemental Transportation Impact Fee for the infrastructure required as part of the Downtown Specific Plan. The fee is calculated at \$398.95 per PM peak hour vehicle trip, with a credit for the existing trips. The proposed project is estimated to generate 75 PM peak hour trips, so the supplemental TIF is estimated to be \$10,372. Payment is due before a building permit is issued and the supplemental TIF will be updated annually on July 1st along with the TIF.</li> </ul> </li> <li>k. Prior to issuance of building permits or the Final Map recordation, the applicant shall pay the Recreation In-Lieu Fee (Municipal Code 15.16.020) in effect at the time of payment (currently \$78,400 per residential unit, total \$313,600).</li> <li>l. Prior to issuance of each building permit the Applicant shall pay the applicable Building Construction Street Impact Fee in effect at the time of payment to the satisfaction of the Public Works Director. The current fee is calculated by multiplying the valuation of the construction by 0.0058.</li> </ul>			



<b>LOCATION:</b> 706-716 Santa Cruz Avenue	<b>PROJECT NUMBER:</b> PLN2016-00111	<b>APPLICANT:</b> Form 4 Architecture	<b>OWNER:</b> 706-716 Santa Cruz Ave, LLC
<p><b>REQUEST:</b> Request for architectural control for the demolition of an existing commercial building and the construction of a new three-story mixed use building with below ground parking, retail and parking on the first floor, office on the second floor, and office and four residential units on the third floor in the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district. The project includes a request for a major subdivision to create a vesting tentative map not to exceed four residential condominium units and one commercial area, with rights reserved for up to ten commercial condominium units, and a Below Market Rate (BMR) housing agreement for payment of an in-lieu fee in compliance with the City's below market rate housing program. The project also includes a variance request for skylights on the third floor to exceed the 38-foot height limit. Removal of one on-street parking space on Chestnut Street would be required to meet fire access requirements. As part of the proposed project, two heritage trees would be removed: one on-site tree located in the parking lot at the rear of the property and one street tree on Chestnut Street.</p>			
<b>DECISION ENTITY:</b> City Council		<b>DATE:</b> TBD	<b>ACTION:</b> TBD
<b>VOTE:</b> TBD (Carlton, Combs, Mueller, Nash, Taylor)			
<p><b>ACTION:</b></p> <p>m. The parking garage gate shall remain open during retail store hours to allow retail tenants and customers to park in the garage.</p>			

**DRAFT – December 9, 2019**  
**RESOLUTION NO. \_\_\_\_\_**

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO  
PARK APPROVING HERITAGE TREE REMOVAL PERMITS FOR A  
PROJECT LOCATED AT 706-716 SANTA CRUZ AVENUE**

**WHEREAS**, the City of Menlo Park (“City”) received applications from 706-716 Santa Cruz Ave., LLC, (“Applicant”) for the removal of two heritage trees at the property located at 706-716 Santa Cruz Avenue (“Project Site”) as more particularly described and shown in Exhibit A; and

**WHEREAS**, the requested tree removals are necessary in order to construct a three story mixed use development with below grade parking on the Project Site; and

**WHEREAS**, the removal of Heritage Trees within the City is subject to the requirements of Municipal Code Chapter 13.24, Heritage Trees; and

**WHEREAS**, the City’s Contract Arborist reviewed the requested tree removals; and

**WHEREAS**, the City’s Contract Arborist determined that the requested removals are justified in recognition of factors #1 (tree condition/health) and factors #2 (proposed construction); and

**WHEREAS**, the City Arborist reviewed and approved the work of the City’s Contract Arborist; and

**WHEREAS**, all required public notices and public hearings were duly given and held according to law; and

**WHEREAS**, the site plan proposes a one-to-one replacement ratio; and

**WHEREAS**, the tree removal approvals would be conditional on City Council action on the project and issuance of building permits for the project; and

**WHEREAS**, after notice having been lawfully given, a public hearing was scheduled and held before the Planning Commission of the City of Menlo Park on December 9, 2019, where at all persons interested therein might appear and be heard; and

**WHEREAS**, the Planning Commission of the City of Menlo Park having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter voted affirmatively to recommend to the City Council of the City of Menlo Park to approve the Heritage Tree Removal Permits for the two heritage trees and at a replacement ratio of one new tree for each existing tree; and

**WHEREAS**, after notice having been lawfully given, a public hearing was scheduled and held before the City Council of the City of Menlo Park on January 28, 2020 whereat all persons interested therein might appear and be heard; and

**WHEREAS**, the City Council reviewed the project on January 28, 2020, and found the project is within the scope of the project covered by the El Camino Real/Downtown Specific Plan Program EIR, which was certified on June 5, 2012; and

**WHEREAS**, the City Council of the City of Menlo Park having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter voted affirmatively to approve the Heritage Tree Removal Permits at a replacement ratio of one-to-one.

**NOW, THEREFORE, BE IT RESOLVED** that the City Council of the City of Menlo Park hereby approves the Heritage Tree Removal Permit for two heritage trees as identified in Project Plan Sheet L4.0, attached by this reference herein as Exhibit A.

I, Judi Herren, City Clerk of Menlo Park, do hereby certify that the above and foregoing Council Resolution was duly and regularly passed and adopted at a meeting by said Council on the \_\_\_\_\_ day of \_\_\_\_\_, 2020, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this \_\_\_\_ day of \_\_\_\_\_, 2020.

\_\_\_\_\_  
Judi Herren  
City Clerk

**EXISTING TREE INVENTORY - 11/20/2018**

Tag No.	Botanical Name	Common Name	Trunk Diameter (in.)	Species	Remarks
1	Tree outside	Labelled Luster	38		
2	Lagerströmia indica	Cash Myrtle	7		
3	Lagerströmia indica	Cash Myrtle	5		
4	Persea caroliniana	Waterbury Elm	11		
5	Persea caroliniana	Waterbury Elm	10		
6	The COCAIN	Labelled Luster	12		
7	Physalis peruviana	Flowering Pear	2		
8	Physalis peruviana	Flowering Pear	3		
9	Physalis peruviana	Flowering Pear	5		
10	Physalis peruviana	Flowering Pear	36		
11	Persea caroliniana	Waterbury Elm	14		
12	Ulmus crinitus	California Elm	23		
13	Persea caroliniana	Waterbury Elm	12		
14	Persea caroliniana	Waterbury Elm	16		
15	Platanus occidentalis	Cottonwood	2		

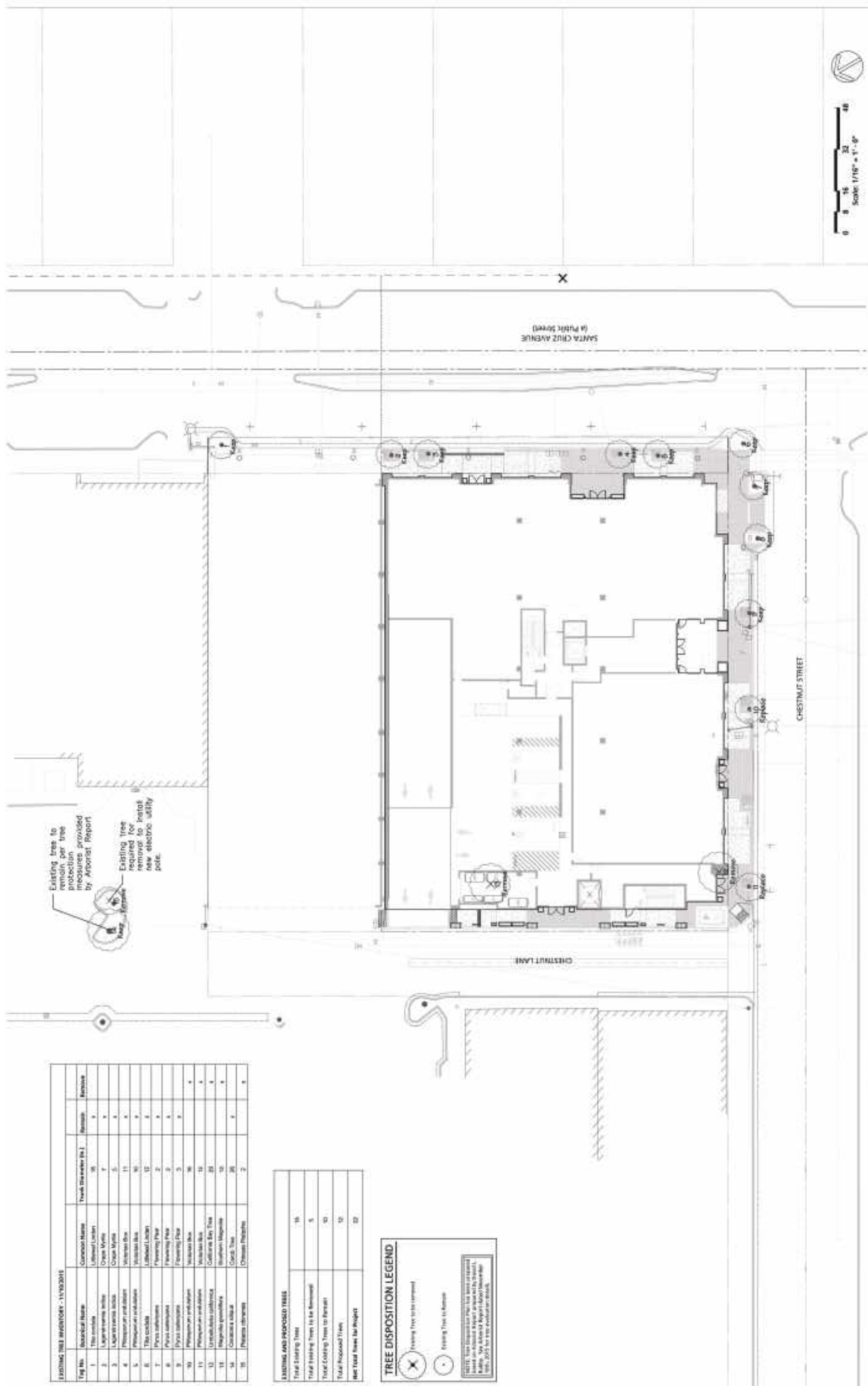
**EXISTING AND PROPOSED TREES**

Total Existing Trees	15
Total Existing Trees to be Retained	5
Total Existing Trees to be Retain	10
Total Proposed Trees	12
<b>Net Total Trees for Project</b>	<b>22</b>

**TREE DISPOSITION LEGEND**

- Existing Tree to be Retained
- Existing Tree to be Removed

NOTE: Tree Disposition Plan has been prepared based on an on-site report prepared by the client. It is recommended that the client verify the tree status with a qualified arborist prior to construction.



**DRAFT – December 9, 2019**  
**RESOLUTION NO. \_\_\_\_\_**

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO  
PARK APPROVING THE BELOW MARKET RATE HOUSING  
AGREEMENT BETWEEN THE CITY OF MENLO PARK AND 706-716  
SANTA CRUZ AVE, LLC, FOR A PROJECT LOCATED AT 706-716  
SANTA CRUZ AVENUE**

**WHEREAS**, the City of Menlo Park (“City”) has received an application from 706-716 Santa Cruz Ave, LLC (“Applicant”), for an architectural control permit for a three story mixed use development with below grade parking and a vesting tentative subdivision map to create four residential condominium units and one commercial area, with rights reserved to allow up to 10 commercial condominiums, and a variance to allow third floor skylights to exceed the maximum height, located at 706-716 Santa Cruz Avenue (“Project Site”); and

**WHEREAS**, all required public notices and public hearings were duly given and held according to law; and

**WHEREAS**, the City Council reviewed the project on January 28, 2020, and found the project is within the scope of the project covered by the El Camino Real/Downtown Specific Plan Program EIR, which was certified on June 5, 2012; and

**WHEREAS**, after notice having been lawfully given, a public meeting was scheduled and held before the Housing Commission of the City of Menlo Park on May 1, 2019 to review the initial draft BMR Agreement Term Sheet, for the payment of an in-lieu fee, whereat all persons interested therein might appear and be heard; and

**WHEREAS**, the Housing Commission of the City of Menlo Park having fully reviewed, and considered and evaluated all the testimony and evidence submitted in this matter voted affirmatively to recommend the Planning Commission of the City of Menlo Park to approve the BMR Agreement; and

**WHEREAS**, after notice having been lawfully given, a public hearing was scheduled and held before the Planning Commission of the City of Menlo Park on December 9, 2019 whereat all persons interested therein might appear and be heard; and

**WHEREAS**, the Planning Commission of the City of Menlo Park having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter voted affirmatively to recommend to the City Council of the City of Menlo Park to approve the BMR Agreement; and

**WHEREAS**, after notice having been lawfully given, a public hearing was scheduled and held before the City Council of the City of Menlo Park on January 28, 2020 whereat all persons interested therein might appear and be heard.

**WHEREAS**, on January 28, 2020 the City Council of the City of Menlo Park has read and considered that certain BMR Agreement between the City and the Applicant that satisfies the requirement that Developer comply with Chapter 16.96 of the City's Municipal Code and with the Below Market Rate Housing Program Guidelines.

NOW, THEREFORE, the City Council of the City does RESOLVE as follows:

1. Public interest and convenience require the City to enter into the Agreement described above and incorporated herein as Exhibit A.

2. The City of Menlo Park hereby approves the Agreement and the City Manager is hereby authorized on behalf of the City to execute the Agreement.

I, Judi Herren, City Clerk of Menlo Park, do hereby certify that the above and foregoing Council Resolution was duly and regularly passed and adopted at a meeting by said Council on the \_\_\_\_\_ day of \_\_\_\_\_, 2020, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this \_\_\_\_ day of \_\_\_\_\_, 2020.

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Judi Herren  
City Clerk

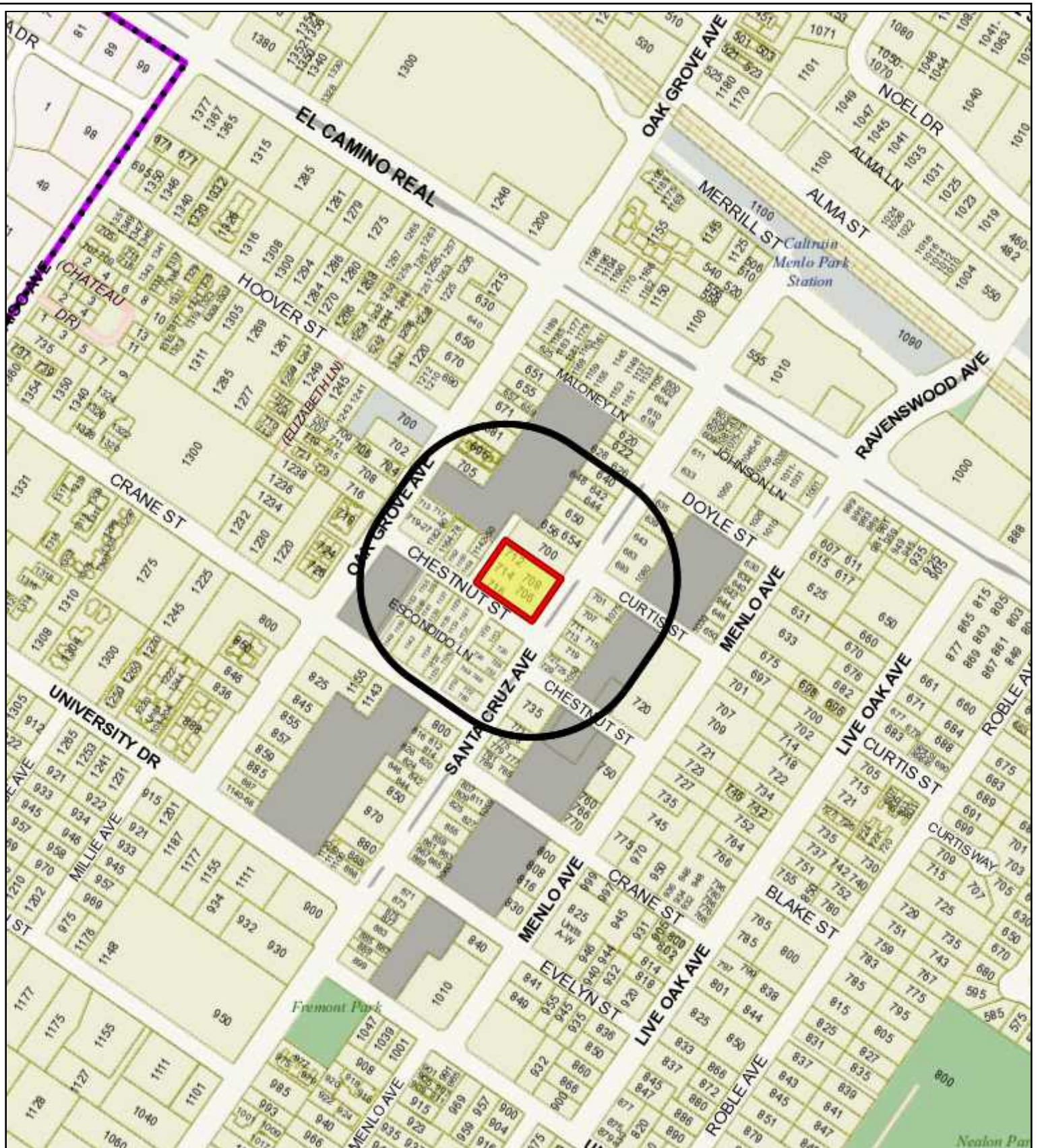
**706-716 Santa Cruz Avenue****Draft Below Market Housing (BMR) In Lieu Fee Agreement Term Sheet**

1. Applicant owns property known as Assessor's Parcel Number: 071-102-250 ("Property"), more commonly known as 706-716 Santa Cruz Avenue, Menlo Park.
2. Applicant is requesting architectural control approval to demolish an existing commercial building, and construct a new three-story mixed use building with two levels of underground parking and associated site improvements. The commercial portion of the subject building is greater than 10,000 square feet in gross floor and therefore, Applicant is required to comply with Chapter 16.96 of City's Municipal Code ("BMR Ordinance") and with the Below Market Rate Housing Program Guidelines ("Guidelines") adopted by the City Council to implement the BMR Ordinance;
3. Property is located within the El Camino Real/Downtown Specific Plan General Plan land use designation and the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district in the City of Menlo Park;
  - a. Property's General Plan land use and zoning designations permit residential uses;
  - b. Applicant is proposing four market rate residential units as part of the proposed project;
  - c. Financial feasibility limits opportunities to develop on-site BMR residential units as part of the proposed project;
4. Applicant does not own any other sites within the City that are zoned for residential land uses;
5. Applicant has elected to satisfy the BMR requirement for the proposed project through payment of an in lieu fee;
  - a. The BMR in lieu fee is estimated at \$410,262.48;
  - b. The equivalent unit requirement is 1.1 unit, which would be rounded to one unit, however;
  - c. Therefore, the Applicant is proposing to pay the in lieu fee, which would be adjusted based on the existing and proposed square footages at the time of building permit issuance. The applicable fee per square foot is adjusted annually on July 1. The table in Item 6 below shows the proposed in lieu fee and its calculation.
6. The table below provides the estimated in lieu fee:



Table 1: BMR Requirements and Applicant Proposal

Use	Use Group	Fee per square foot	Square feet	Component Fees
<b>Existing Building - Office</b>	A	17.79	(0)	(0)
<b>Existing Building – Non Office</b>	B	9.66	(12,758)	(123,242.28)
<b>Proposed Building - Office</b>	A	17.79	23,454	417,246.66
<b>Proposed Building – Non Office</b>	B	9.66	12,035	116,258.10
<b>BMR In-Lieu Fee Option</b>				410,262.48



City of Menlo Park  
 Location Map  
 706 - 716 Santa Cruz Ave







## REGULAR MEETING MINUTES - EXCERPTS

**Date:** 12/11/2017  
**Time:** 7:00 p.m.  
**City Council Chambers**  
**701 Laurel St., Menlo Park, CA 94025**

### A. Call To Order

Chair Drew Combs called the meeting to order at 7:00 p.m.

### B. Roll Call

Present: Andrew Barnes, Drew Combs (Chair), Susan Goodhue, Larry Kahle (Vice Chair), John Onken, Henry Riggs, Katherine Strehl

Staff: Deanna Chow, Principal Planner, Michele Morris, Assistant Planner, Kaitie Meador, Associate Planner; Arnold Mammarella, Consulting Architect

### F. Public Hearing

- F4. Architectural Control and Major Subdivision/Vasile Oros/706-716 Santa Cruz Avenue: Request for architectural control for the demolition of an existing commercial building and the construction of a new three-story mixed use building with a below ground parking lot, retail and parking on the first floor, office on the second floor, and four residential units on the third floor in the SP-ECR/D (El Camino Real Downtown/Specific Plan) zoning district. Major subdivision to create six condominiums, including four residential units, one commercial/retail unit, and one office unit, with rights reserved to allow up to ten commercial condominium units. Below Market Rate (BMR) housing agreement for compliance with the City's below market rate housing program. Removal of one on-street parking space on Chestnut Street to meet fire access requirements. As part of the proposed project, two heritage trees will be removed; one on-site tree located in the parking lot at the rear of the property and one street tree on Chestnut Street. ([Staff Report #17-072-PC](#))

Staff Comment: Associate Planner Meador said the project was a subdivision that would allow up to 10 commercial condominiums. She said the original public notice indicated there would be four residential units and two commercial units only. She said under the subdivision they were allowed up to 10 commercial units and that had been updated for the agenda and the staff report. She said conditions of approval, 6.n. and 7.a.a currently referenced improvements to Oak Grove Avenue and that should be corrected to Santa Cruz Avenue, Chestnut Street and Chestnut Lane. She said staff had received two emails since publication of the staff report and those were printed for the Commission's review. She said the City's consulting architect was also available to answer questions.

Questions of Staff: Commissioner Strehl asked staff to restate the information related to the subdivision. Associate Planner Meador said the project would create four residential units and two

commercial units. She said the two commercial units could be divided into up to 10 commercial condominiums. Commissioner Strehl confirmed with staff that the latter could occur without coming back to the Planning Commission for review.

Commissioner Barnes clarified with staff that there could potentially be 14 condominium units, four residential and 10 commercial.

Commissioner Strehl asked why the City would allow up to 10 future subdivisions for the project, and whether the purpose was to sell those units. Associate Planner Meador suggested asking the applicant about their intention.

Chair Combs asked why the project was not brought to the Commission as a study session as this proposal was the most complex project for the downtown Santa Cruz Avenue area that he had seen during his Commission tenure. Principal Planner Chow said that study sessions were discussed as appropriate when submitted projects raised questions about use or architecture, and/or based on public feedback after a project application notice was released. She said this was not a bonus level project. She said although the first project under the Downtown Specific Plan on Santa Cruz Avenue it met the guidelines and staff thought it appropriate to bring to the Commission as an action item.

Commissioner Strehl said she thought it would have been better for the project to come to the Commission as a study session first so guidance might be provided prior to being an action item.

Applicant Presentation: Ken Hayes, Hayes Group Architects, said he would make a project presentation on behalf of his client, Mr. Oros. He said present tonight were Mr. Oros, their landscape architect, utility consultant, and legal counsel.

Mr. Hayes said the project site was about 0.50-acres located on the corner of Chestnut Avenue and Santa Cruz Avenue. He said a private, paved surface parking lot was located at the rear of the site between Chestnut Lane and the existing building. He said the surrounding buildings were all one- or two-story commercial buildings with personal service, office and retail uses. He said the parcel was located in the downtown sub-district of the Downtown Specific Plan and allowed for 2.0 Floor Area Ratio (FAR), a façade wall height of 30-feet, building height of 38-feet, with a zero setback requirement, and in a zone permitting a variety of uses. He said they were proposing residential, retail and office use with retail on the first floor, office on the second floor, and residential on the third floor with outdoor common space and a private yard with balconies for both office and residential uses. He said the project would be fully parked in concealed, at grade, and underground parking areas, would respond to the neighborhood context and promote the pedestrian-oriented goals of the Downtown Specific Plan.

Mr. Hayes said the parcel was in a zero setback zone so the property lines basically defined the building envelope. He said they wanted to create lots of opportunities for windows, visual access inside and outside to promote pedestrian activity and friendliness on the sidewalk. He said the sidewalks would be completely improved using brick to match existing and wrapped frontages on Chestnut Avenue and Santa Cruz Avenue. He said they would provide a sidewalk on the Chestnut Lane side of the property for community connectivity that would extend to the properties further to the east. He said the building would have two entry points along Santa Cruz Avenue into the first floor retail space and another entry point on Chestnut Avenue for first floor retail. He said there was

a kind of plaza area at the intersection of Chestnut Lane and Chestnut Street to create a pause in the streetscape. He said that would also be the main entrance for the upper floor users and would include elevator and stair access. He said access to the at-grade and underground parking garage was located on Chestnut Lane. He noted the second and third stories were set back from the first floor.

John Hanna, project attorney, said regarding the 10 condominium units question, that whereas they used to do condominium plans as part of the tentative map that had changed a few years ago so that a condominium map was done as a one-lot map. He said with this project they were doing a three lot map. He said one of the lots was being divided into the residential condominiums. He said the owner had no intention at the moment of doing anything other than leasing the first two floors. He said land developers in these cases want to have flexibility five or 10 years on to sell either the retail on the ground floor or the office on the second floor. He said if no one purchaser was available for those properties the owner would have the ability to sell individual condominiums and divide into multiple units at that time. He said the subdivision map act said that once a map was approved for a condominium project that the further division of a lot on that map into condominium units did not constitute a subdivision requiring another map, provided that the number of units divided into that condominium plan did not exceed the maximum number approved by the City in the final map.

Mr. Hanna said regarding undergrounding utilities that they had gone back and forth with staff a number of times on the issue. He said a guideline for the Downtown Specific Plan stated that utilities should be underground for new projects. He said conditions 6.e and 7.q recommended by staff had to do with the architectural plan and the map. He said it was also posited there that staff would work with a developer with the undergrounding of the utilities as the plans were finalized. He said they understood that to mean that utilities necessary to serve this project would be undergrounded on Chestnut Lane where there were utility wires. He said on Chestnut Street the only undergrounding that would occur would be from the corner of Chestnut Lane partway down the block towards Santa Cruz Avenue, which would be the undergrounding for the utilities for this project, including the transformers. He said they would not take down the poles and underground all of the utilities that served other properties down Chestnut Street.

Commissioner Onken asked about the number seven vault adequacy for the utilities. Tim Fowle, RGA Design, said his firm was an applicant design consulting group. He said the number seven vault would have a single service transformer to feed the new development at 706 Santa Cruz Avenue. He said they intended to install two subsurface vaults to house PG&E junction boxes. He said they received recent criteria design from PG&E and would develop accordingly once the scope of the undergrounding was completed.

Commissioner Onken asked if there was any differentiation in the map act between residential and commercial condominium uses. Mr. Hanna said the residential owners would be required to have a Homeowners Association (HOA) composed of just them. He said the owner(s) of the commercial parcels would not be a member of that HOA but would be responsible for the overall exterior maintenance of the project.

Commissioner Barnes said the applicant's representatives seemed to indicate there was a consensus in terms of staff's understanding and the applicant's understanding as to what utilities needed to be undergrounded. He said the correspondence in the staff report indicated a difference

of opinion about it. Associate Planner Meador said Mr. Hanna's reference and understanding was they would underground Chestnut Lane as required for fire access and then would only underground Chestnut Street if there was additional support from the City. She said staff's preference was for them to underground utilities on both Chestnut Lane and Chestnut Street depending on the feasibility. She said it might transpire that only Chestnut Lane was undergrounded but that would require additional staff review for that conclusion to be reached. Commissioner Barnes asked about the reasons behind staff's preference for what should be done. Associate Planner Meador said staff would prefer the undergrounding of Chestnut Street as it was more visible and would be an aesthetic improvement which was part of the Specific Plan guidelines.

Commissioner Barnes said that the term "feasibility" appeared with some frequency in the staff report and asked if that referred to what was feasible for this project to bear. Principal Planner Chow said they would continue to work with the applicant and feasibility would look at the financial aspect, site constraints, and the needed coordination with a number of outside agencies such as the Fire District and PG&E. She said the Fire District might designate Chestnut Street as the fire access in which case undergrounding utilities there would be required to meet the Fire District's requirements. Commissioner Barnes asked if there was basic agreement that if the utility did not service this particular site that it should not be the responsibility of the applicant. Principal Planner Chow said staff's preference was to have the pole on Chestnut Street removed and utilities undergrounded along the frontage of this property to create a clean line. She said the discussion they wanted to continue with this applicant was what could be done now for undergrounding utilities as part of this project and what could be done with more partners in the future. Commissioner Barnes asked about the sequence of approval for the project noting that such a discussion had monetary impacts. Principal Planner Chow said tonight the Planning Commission was asked to take action on the architectural control and the Below Market Rate Housing Agreement (BMR) and make a recommendation on the final map subdivision to the City Council. She said in the time before the building permit, they would resolve utility undergrounding and offsite improvements.

Commissioner Strehl said she did not understand the commercial condominium division and what entity would be responsible for the garage. She asked if each office condominium would be responsible toward some portion of the maintenance facilities. Mr. Hanna said the CC&R's that would be recorded for the project would address that issue. He said a percentage of the overall cost of insurance, maintenance and whatever overall was needed would be assigned to each one of the units and parcels in the project. He said hypothetically if the office parcel was assigned 33% of the cost that if then subdivided into three equal units each would have an 11% shared cost responsibility. Mr. Hanna said whatever the long-term undergrounding project was for the downtown area they were not dodging their responsibility towards but wanted to deter it until such a plan became effective with the undergrounding of an entire block, street or area and not have undergrounding done piecemeal.

Commissioner Riggs said he understood the applicant's perspective on undergrounding and the City's preference to have utilities on Chestnut Street undergrounded. He asked what the City's justification was in asking this developer, not asking for bonus level, to do more than what the Specific Plan stated in E.3.07, under Utilities Guidelines. He read: *All utilities in conjunction with new residential and commercial development should be placed underground.* He said the applicant seemed to be saying that the utilities used in conjunction with the site development would be

undergrounded. Principal Planner Chow said she thought the City would look at utilities located across the frontage, and if some of those served others along the frontage they would look to see if that could be accommodated with the project. She said this was a conversation that needed to be continued as not all the answers were available tonight from all the different parties involved. She said they were trying to develop a solution that recognized the spirit of the Specific Plan, was feasible for the applicant, and allowed the project to move forward while being resolved.

Chair Combs opened the public hearing.

Public Comment:

- Evelyn McMillan said she was a 40 year Menlo Park resident. She said her family owned the building on Chestnut Lane directly across from the project and that would be the most impacted by it. She said their two-story building was mostly glass and the opening from the project's garage would have vehicles driving directly toward their building. She said they were required to provide a five-foot easement when the building was developed in the 1950s and that ran the length of their property. She said one of their first floor retail tenants had been with them 45 years and another for 20 years. She said the upstairs office space had abundant natural light and had attracted architects, designers and artists over the years. She thought the construction impacts would force her tenants to leave either because of loss of business or noise. She said these spaces were small with reasonable rents that were hard to come by in Menlo Park. She said Mr. Hayes seemed to indicate the opening of the garage was midway down Chestnut Lane but on the drawings the garage was offset toward their building off Chestnut Lane and more at a two-thirds configuration rather than the exact center. She said she appreciated the property owner's right to develop but her family and her tenants for decades would lose revenue during the construction process. She said it was the long-term impact on Chestnut Lane that really concerned them.
- Wouter Suverkropp, Mountain View, said he was a member of the family who owned the building on Chestnut Lane directly opposite the proposed project. He said they acknowledged the rights of the applicant to develop their parcel but they would like to achieve a good outcome. He said they were very concerned about the long-term impacts of this project on their building and their tenants who have served residents for decades as well as for future development on Chestnut Lane. He said Chestnut Lane was currently 25 feet wide and with the proposed project sidewalk would be reduced to 20 feet wide. He said their concerns centered mostly on the entrance to the parking garage which at 30 feet in width would line up exactly with their building's storefront. He said their ground floor tenants and customers would look directly into the parking garage opening and all of the traffic going in and out, the increased noise, vehicle emissions, headlights, taillights, and increased safety hazards would be a constant source of distraction. He said their building had been hit twice already by cars exiting the existing surface parking of this property, and in the one incident, fortunately the building was unoccupied when a car drove through their glass storefront. He said trucks loading and unloading for the project's retail business would block Chestnut Lane including the fire access lane. He said the lane was two-way and cars would be driving closer to the curb making pedestrian access on Chestnut Lane less desirable. He said the plan had confusing and inconsistent information about trash pickup in that the conformance check list stated that trash containers would be accessed by Chestnut Lane but condition 16 seemed to prohibit that. He said the applicant was requesting a subdivision into six condominiums with the right in the



future to expand into 14 condominiums. He said the effects of those future subdivisions were unstudied and might have unintended consequences. He said their attorney Camas Steinmetz would talk about a solution that would eliminate their concerns and the danger to their building while enhancing the village like pedestrian shopping area and retail experience on Chestnut Lane in compliance with the El Camino Real/Downtown Specific Plan.

- Ann McMillan said she was the daughter of the family who owned the building across Chestnut Lane from the project site and that she was ceding her three minutes of speaking time to Camas Steinmetz.
- Camas Steinmetz said she was an attorney representing the property owners of 1142 to 1150 Chestnut Lane directly across from the project site. She said a letter she submitted about this project was included in the staff report on pages 142 to 144. She said her clients had spoken tonight about how this proposed project would significantly impact their building and tenants. She said the proposed project also would threaten the overall consistency of the El Camino Real/Downtown Specific Plan. She said one of the five guiding principles in the Specific Plan was to sustain Menlo Park's village character as described on page C4: *The Specific Plan recognizes and builds upon the unique qualities of downtown Menlo Park, in particular its small town character of lower scale buildings and diverse and local neighborhood serving businesses. The Specific Plan accommodates future development in ways that complement the area's existing character.* She said there were two changes to the proposed project that would both further this vision and alleviate impacts on her clients' building. She said first they suggested that the garage entrance be relocated to Chestnut Street. She said this was raised in her letter as their primary request but was not addressed in the staff report. She said in a conversation her clients had with the applicant last January she understood that the developer expressed interest in locating the project's first floor retail components directly across from her clients' building storefront. She said moving the parking garage entrance would permit that configuration and complement the existing character of the area, building upon the character of lower scale buildings and diverse and local neighborhood serving businesses. She said the existing surface parking for the proposed project was accessed from Chestnut Street so their suggested change would preserve an existing condition and align the garage entrance directly across from where Ryan Lane intersected Chestnut Street. She said they requested that the sidewalk along her clients' building be widened to provide a buffer from the project and its impacts. She said this would further all five guiding principles of the Specific Plan by improving the overall pedestrian experience. She said it would also help resolve the inequity of the fact that the City was treating similarly situated properties differently. She said her clients' property and their neighboring' properties were burdened with the five-foot sidewalk easement yet the City was not requiring a similar sidewalk easement dedication of the project applicant. She said the resulting 15-foot width after subtracting the widening sidewalks and the sidewalk on the project site side would meet City standards if Chestnut Lane were restricted to one-way traffic. She said that was not addressed in the staff report. She said they realized this was the first redevelopment in the downtown and evaluated under the Specific Plan and would serve as a test case for upholding the Specific Plan vision. She said they urged the Commission to continue the hearing and direct staff to carefully study the two project changes they were requesting to insure that the project both mitigated its impacts on existing development and furthered Specific Plan goals and policies to the maximum extent possible.



- Richard Poe, Menlo Park resident, said he was a local real estate broker. He said he supported the project and understood there were many things for the Commissioners to consider but he thought it was time for the City to catch up with communities like Palo Alto and others who have had this type of development occur. He said in disclosure he was representing an owner who would be coming before the Commission for a project at 840 Menlo Avenue that was within the downtown corridor. He thanked Commissioners Barnes and Riggs for raising the question of whether the Specific Plan, the code and the staff's analysis of it has only to do with the undergrounding of the utilities that served a project or whether they were expecting developers to pay for undergrounding of utilities for other people in the neighborhood. He said he did not think staff's comments on that were complete and deserved the Commission's more careful attention. He said with his project that it was clear staff was requesting that the developer do offsite undergrounding. He said the position of most developers he dealt with was they liked undergrounding as it looked much better and increased property values but it was simply impossible and unrealistic to do on a project by project basis, and there had to be a comprehensive approach. He said he would encourage a common solution for owners to agree to long-term for the undergrounding. He asked about a red zone on Chestnut Street that was not a loading zone. He said there were four parking spaces there now. He asked if all four spaces disappeared, how they would prevent trucks loading and unloading from parking on Chestnut Street.
- Kevin Cunningham said he was a resident of Orinda and a property owner in Menlo Park. He said he echoed the comments of the last speaker. He said the project proposal was a well-thought out project long overdue in the downtown Menlo Park. He said he thought a neighborhood could be revitalized and enhanced through development and still maintain the integrity of what made that area special. He said he understood this project fulfilled all of the obligations and was within the envelope of what was allowable under the Specific Plan. He said he thought the goal of the Specific Plan to enhance Menlo Park's character was met by this proposal. He said while no project was perfect that the benefits of this project to Menlo Park and its visitors was a large positive, and he enthusiastically supported it.
- Daniel Minkoff, Oakland resident, said he was a property owner in Menlo Park, and was currently working on a redevelopment of 650 and 660 Live Oak Avenue, which was a mixed-use project approved about one year prior by the Planning Commission. He said he commended the architectural and development team on this project. He said mixed-use projects were challenging and to have all three uses in one project work well and look good was something that all mixed-use projects should strive for. He said projects often had a front that looked good and a back that tended to get cheaper materials and less attention. He said it appeared that all of the three main elevations for this project had received attention noting the use of deep inset windows in a way that dealt with the particular environmental conditions of the different directions involved. He said he supported project approval.
- Fran Dehn, Chamber of Commerce, said since the Specific Plan approval the City had seen projects along El Camino Real that were reviewed and approved to date. She said 706 to 716 Santa Cruz Avenue was the first project to come forward in the traditional retail area. She gave the applicant kudos for moving ahead with a development project along Santa Cruz Avenue under the Specific Plan. She said regarding utility undergrounding in the staff report on page 7, paragraph 2, that there seemed to be an incorrect reference to C2-1 and referred to a different drawing, noting she did not have all the drawings. She said the ultimate desire to streamline the

line of vision and underground all utilities was a good goal but perhaps conceptually flawed if it was to be achieved project by project as suggested in the initial paragraph of the staff report. She said the applicant has agreed to all of the solutions desired along Chestnut Lane but in terms of Chestnut Street the alternative option was more realistic, which she thought also met the requirements of the Menlo Park Fire Protection District without furthering burdening the applicant with a piecemeal solution to a downtown. She said there were other ways to accomplish the long-term goal such as the step process referred to by Planning staff and Mr. Hanna this evening in ongoing conversations with the City. She suggested establishing an effective process to accomplish the undergrounding goal of the Specific Plan. She said the utility pole in this situation actually serviced another property and she asked whether that was the applicant's responsibility to take care of something that serviced a completely different set of properties. She said she looked forward to the project moving ahead.

- Vasile Oros, project applicant, said Ms. McMillan and he were property owners, and he did not want to do anything that would take away value from her property. He said although she had a five-foot easement required on her property, he had 18 surface parking spaces he was required to preserve. He said he would like to have only a five-foot easement, build a two-story building and not have to provide any parking spaces. He said the parking spaces were a grandfathered requirement and his understanding was whatever was grandfathered had to continue.

Chair Combs closed the public hearing.

Commission Comment: Commissioner Onken said that page 4 of the staff report described parking and how the count was made. He said currently the site had 22 spaces. He asked what was required for the new project and how was that accomplished. Associate Planner Meador said the project was part of the P parking zone which meant the first 1.0 FAR had to be covered by the existing parking. She said for the existing parking that credit was given if the property owner had to make ADA compliant parking, and that was how they came to 18 existing spaces for this site. She said parking for the remaining office FAR used the standard Specific Plan ratio and one parking space was required for each residential unit. She said the parking was private to the site.

Commissioner Barnes asked about likely tenants for the 13,000 square foot retail space. Mr. Oros said he thought viable tenants would be restaurants noting that retail was challenging. Replying to Commissioner Barnes about potential tenants for the office space, Mr. Oros said he had no prospective tenants at this time. Commissioner Barnes asked if the four residential units on the third floor would be for sale upon completion. Mr. Oros said he hoped to lease the units. Commissioner Barnes asked why four units and not more. Mr. Oros said his thinking was that with fewer residential units there was less impact on the office and retail spaces as the main users would be commercial. Commissioner Barnes asked about bicycle parking for the project. Mr. Hayes said there would be long and short term spaces. He said short term spaces were near the residential entrance in the plaza-type area down by the corner of Chestnut Lane and Chestnut Street and there would be 12 short term bicycle parking racks, each holding two bicycles. He said there was an existing three bicycle parking rack that would be replaced as well. He said there were nine long-term bicycle parking spaces in the parking garage and they expected residents to take their bicycles by elevator to their units. Commissioner Barnes asked about the construction time length. Mr. Hayes said they had submitted a logistics plan to staff and the construction period was estimated at 14 months. He said a construction fence would be constructed along Chestnut Lane so that traffic was not stopped.

Commissioner Barnes said although he thought the garage location was appropriate for the site he was sensitive to the impact of cars traveling in and out of the garage on the neighbor's building across Chestnut Lane. He asked if they had thought about mitigations not just for construction impacts but for the cars traveling in and out of the parking garage. Mr. Hayes said they spoke with Ms. McMillan about a year ago and looked at some alternatives that did not work out. He said from that exercise they came up with planters to create some space on the building side so it did not feel it was right on top of Chestnut Lane. He said that having the garage entrance a bit wider than the minimum of 24-feet would create more room where the cars would be coming in and out. He said the existing building generated 47 peak hour trips with the current retail use. He said the proposed project would increase peak hour trips by 31 new net trips. He said striping both sides of Chestnut Lane would help mitigate traffic so people did not park on Chestnut Lane and deliveries would be prohibited there. He said deliveries did take place occasionally in that location now. Commissioner Barnes asked where deliveries would take place. Mr. Hayes showed a slide that deliveries were made early in the morning. He said staff did not want unloading to happen at Chestnut Lane but deliveries were made in that area now and no complaints were made. He said the property owner indicated that the trucks come early in the morning and left before the downtown woke up. He said mitigation for the neighbor included no deliveries on Chestnut Lane. He said trash would be moved to a particular site with other trash facilities on garbage pickup day. Commissioner Barnes asked if there were food and beverage uses on the new site if the existing area shown in the blue level could be used by delivery trucks. Mr. Hayes said having a loading zone on these small parcels downtown impeded pedestrian friendliness and walkability. He said having a place the community could support for deliveries in certain hours of the day made a lot of sense. He said the particular noted location was one that had been working and it would be great if that could continue there.

Commissioner Barnes said it seemed reasonable to use a public lot to serve businesses there. He asked staff if thought had been given to where deliveries would be made and whether the space indicated with a blue oval was representative of a future loading zone area to service the new development. Associate Planner Meador said when staff reviewed potential loading zones for the project one option was double parking on Chestnut Street, which would be allowed. She said they did not explore the parking plaza as a loading zone. She said based on the use loading would be limited for the site or have a short, minimal loading time. Commissioner Barnes asked if they would allow loading in the public plaza and what the decision process was for that. Principal Planner Chow said they would need to talk further with the Transportation Division about that; she noted that Chestnut Street was the public right of way. She said if loading and unloading happened now in the parking plaza before and after hours that was an informal arrangement. She said parking plazas were intended to be used for parking and the parking plaza was not striped for loading.

Commissioner Strehl asked where retail customers would park. Mr. Hayes said there were nine covered spaces at grade that included an EV charging space and an accessible parking space. He said those spots were available for people doing business in the project building. Commissioner Strehl asked about the vehicle access plan for the garage whether the entrance was one way and the exit the other way. Mr. Hayes said the diagram showed that if there were two cars with one entering and one exiting the garage simultaneously that the one exiting the garage would not be able to turn left easily based on engineering radii.

Commissioner Strehl confirmed with staff that the sidewalk widening on Chestnut Lane would narrow the Lane. She asked why the sidewalk was not part of the project property as opposed to using public right of way. She said narrowing that Lane concerned her. She said it was a convenient way in and out of the public parking plaza. She understood why they would not want their garage entrance on Chestnut Street but it seemed to be a significant imposition to the property owners directly across the Lane. She asked if the proposed sidewalk to the parking plaza would have a zigzag alignment as buildings did not line up on the Lane. Associate Planner Meador said that currently the sidewalk would not be a straight line but would be connected and in the future when other properties redeveloped those would make the sidewalk a continuous line. Commissioner Strehl asked about garbage pickup. Mr. Hayes said it would not be on Chestnut Lane. He said they would need to pull dumpsters out and have them picked up on Chestnut Street or to an alternative location and then return them to the garage area.

Commissioner Onken said that the applicant had tried to address Chestnut Lane and the neighbors had very justifiable concerns. He said he thought the new sidewalk on the applicant's side of Chestnut Lane was pointless. He suggested if a do-over was possible that widening the sidewalk on the other side of Chestnut Lane to 10-12 feet with safety features would create a unique walkway in Menlo Park. He said additionally installing minimal safety features on the applicant's side and making the Lane one way so that people exiting the garage would turn right and exit through the public parking plaza seemed preferable. He asked how to make changes on the west side of Chestnut Lane such as widening the sidewalk there. Principal Planner Chow said that they would want to involve Engineering and Transportation staff if this was something the Commission wanted pursued. She said increasing the pedestrian accessibility and connectivity was why they had added the sidewalk so there would be a sidewalk on each side of the lane. She said to change the direction of the Chestnut Lane public right of way would involve discussion with Transportation Division staff. She said if the Commission wanted that explored staff could return with feedback on that. Commissioner Onken said those measures would help to resolve the neighbors' concerns and he would like to see those in place before he approved the use permit. He said he was familiar with the building design proposed and he thought it would be an asset to the community. He said the façade could be varied more and that materials might be mixed more. He said he liked the terraces up Chestnut Street. He said knowing of these buildings in other cities there was a variety of retail that could be offered within a strong frame.

Commissioner Strehl said she did not understand why a sidewalk would be wanted on the project side as it would cross the garage driveway and would not be a very safe place for crossing. She said widening the sidewalk on the other side and tuning that up would make more sense.

Chair Combs said the space behind Ace Hardware was used by Menlo Presbyterian, and a person parking on Chestnut Street to reach that space would have an accessible advantage with the proposed new sidewalk on Chestnut Lane. He said he wanted to point out some value for locating the sidewalk there but he was not saying it outweighed the value of the solution being offered by Commissioners Onken and Strehl.

Commissioner Goodhue said the proposed rear elevation was not ugly but it was not as nice as the Santa Cruz Avenue and Chestnut Avenue elevations. She said weekly she drove on Chestnut Lane for an appointment along there. She said leaving the public parking plaza she then takes a left on Chestnut Avenue to get to Santa Cruz Avenue, noting that left turn was challenging. She said the whole area needed rethinking. She said if they wanted to make something unique on

Chestnut Lane there was a tremendous opportunity to make the sidewalk on the opposite side more attractive rather than making a current situation worse with a rolled curb sidewalk for fire truck access. She said walking down Chestnut Lane that she tended to walk in the middle to be visible as the sidewalk was narrow. She said she was not sure the garage entrance was in the right place, and if Chestnut Lane was not made one-way, the garage entrance would cause problems at that intersection.

Mr. Hayes said having the sidewalk on the other side expanded was a good idea but noted the subject project imposed some constraints on that idea. He said the project building had a stairway coming down the back of it and a public way was needed to get people out of that area and away from the building as needed. He said if there was no sidewalk on the project side of the street that a crosswalk would be needed to get across to the widened sidewalk in front of the neighbor's building. He said this area was intended as the project's fire access lane and a 25-foot width was required for that which was why the Fire District wanted the curb rolled on the project side of the Lane. He said if there was no sidewalk on the project side that perhaps they could extend the base of the building two feet allowing for more space for ivy plantings. He said the sidewalk on the opposite side could not be widened to 10 feet as the project would still need to keep a 25-foot wide zone. He said if Chestnut Lane was one-way it could be narrower except they would have the same issue with the fire trucks needing 25-foot width.

Chair Combs said overall he liked the project and the mixed-use elements. He said he had a procedural concern in that the Commission had been presented with a substantial project for action that had impacts without having had an opportunity to get public concerns prior so those might be addressed before bringing the project forward for action. He said he was not a fan of the architectural style. He said although it was a base level development under the Specific Plan that within the Plan benefits were provided to developers. He said he was not sure this project's design elements were what the City or community expected in return. He said he expected something with a bit more character. He said the project spoke to existing architecture such as the Wells Fargo building but he did not think that architecture should be the measure for this project.

Commissioner Barnes said he would like to hear from the consulting architect noting that the Specific Plan was very prescriptive in what was expected for projects in the area. Arnold Mammarella, consulting architect, said there was discussion with the applicant, the architect, staff and him about the development of the building. He said one element was how to make this development a pedestrian-oriented, friendly, intimately scaled streetscape. He said the size of the building with three floors was not debated but the question was how to have it fit within the character of the downtown and how the downtown would develop. He said staff was pushing for more variety in the design and more things to make it pedestrian-oriented in terms of the architecture so that it was not quite as ordered and rigid. He said the project architect was very capable in developing this type of architecture and was able to articulate those forms without a lot of divisions in the fenestrations and the frames to meet the Specific Plan requirements. He said the question was whether the Commission supported this architectural plan or wanted to see something that was more intimate in scale and more diverse in its presentation. He said the Specific Plan provided some images that seemed to speak to such scale and diversity but it was not deliberate in saying that.

Commissioner Barnes said in his view the project conformed to the Downtown Specific Plan guidelines very ably. He said he liked the architecture and that it took the village character theme,



which was articulated in the Plan. He said he would not have a basis for modulating the design based on preference as it conformed to everything set forth in the Plan. He said he recognized the difficulty of having three different uses in one building and he thought the proposal went far to accommodate all three uses. He said he liked the idea of expanding the sidewalk on the opposite side of the project on Chestnut Lane as well as providing some type of buffer for where the project's lot line ended and for traffic to turn in and out of the garage. He said the sidewalk width was important but the streetscape aspect from his perspective was less important. He said staff stated a preference for a sidewalk along the back of the project property that continued down Chestnut Lane to the public parking lot. He said continuing the sidewalk would remove all the parking behind the other buildings through the public parking lot. Principal Planner Chow said the sidewalk continuation would be to 700 Santa Cruz Avenue. She referred to sheet B1 of the staff report that showed the jog in the parcel lines. Commissioner Barnes asked the reason for the sidewalk. Principal Planner Chow said it was for safety and to have pedestrian access on both sides of Chestnut Lane. She said the Plan did not describe in detail how there would be connections on Chestnut Lane but page D19 described a connection behind the properties. Commissioner Barnes confirmed with staff that was a connection specifically at the corner of this property from Chestnut Lane to Chestnut Street. Commissioner Barnes asked if the sidewalk was eliminated and two-feet of space taken for the base of the building whether vehicles would be able to make a legitimate turn into the parking garage. Mr. Hayes said that if the radius to turn was tightened up vehicles would need to swing wider into Chestnut Lane to make the turn.

Commissioner Riggs said he did not know how the last car in the nine parking spaces would get out as there was no pocket to back into. He asked how the brick façade was expressed and whether the grout would have contrast to create a pattern or if the façade was muted. He said other questions he had included why five tree replacements had been reduced to three tree replacements and how construction would proceed on a zero lot line on Chestnut Lane. He asked what would happen to Chestnut Lane when the new building was constructed with zero lot line on all four sides and a sidewalk taken out of the 25-foot right of way. He said with the garage entry and Chestnut Lane connecting to the public parking lot that he did not think Chestnut Lane could afford to give up five feet of drive space, and that there would be impacts from vehicles trying to get in and out. He said a great deal of experience and skill had gone into the project plans and great problem solving. He said the height of the building was correct but looked oversized because of the brick frames and modulation. He said the building length was 200 feet when storefronts in Menlo Park were 25-feet wide or 50 to 60 foot wide which was why modulations were required. Mr. Hayes said the modulations were at 28 feet. Commissioner Riggs said if the streetscape had been illustrated with the adjacent buildings in their detail the pedestrian scale would be more visible. He said the formality and rhythm to the architecture could be read as rigid. He said the design would be fantastic on Page Mill Road and Sand Hill Road but it needed to be more intimate in this area. He said they might need another approach in façade and materials and that he could not support the project design.

Commissioner Kahle asked the applicant to pull up the slide of the rendering of the corner of Chestnut and Santa Cruz Avenues. He said he appreciated the design and the careful thought that went into it. He said he would have appreciated a study session to have an earlier look at this and have the opportunity to provide feedback as it was a much needed improvement to the downtown and was the first major project in the downtown Specific Plan area. He said he was not convinced the design was as best fitted to the site as it could be. He said at the corner of Chestnut and Santa Cruz Avenues would be a two-story brick clad cube and third story mass for residential unit 3. He

said he was more drawn to the glass fins and the building's nice and unique character on the opposite corner noting the rendering of the corner of Chestnut Lane. He said more glass was apparent on the third floor with a thin roof line that drew him in more. He said he was not sure the Roman brick was the best direction for the project. He said the colors were a bit muted and he would like something as an accent or highlight that would draw a viewer in more. He said some of the more successful Hayes Group projects in downtown Palo Alto had great character at the street level façade and read more commercial than this proposal did. He said he appreciated the discussion on the wider sidewalk on Chestnut Lane and thought that would be a great solution. He said someone had mentioned earlier about potentially having the garage entrance elsewhere, and if they considered having it from the Chestnut Street side. Mr. Hayes said he never considered interrupting the sidewalk on Chestnut Street for that as under the Specific Plan the City was trying to create connectivity and continuity of retail storefronts that were pedestrian-friendly and walkable. He said if the garage driveway was put there it would interrupt the sidewalk and just 25-feet past the entrance the sidewalk would be interrupted again by Chestnut Lane. He said the place for the garage access was on Chestnut Lane. Commissioner Kahle said he liked the design but it needed a bit more finesse to fit the site better.

Commissioner Onken said the whole issue about Chestnut Lane which he saw as an important mitigation for this project was somewhat out of the Commission's hands. He said they could direct staff to look at changing Chestnut Lane and find out a month later that nothing would be done to it to change it or that the Fire District was stipulating the sidewalks were absolutely necessary, He moved to continue the project directing staff to specifically get input and response from Public Works in terms of the commitment to improvements along Chestnut Lane before the Commission recommend the project to Council. Principal Planner Chow said they also needed the Fire District's requirements. Chair Combs said the motion as stated had no action items for the applicant and only for staff to resolve issues around Chestnut Lane. He suggested that was unnecessarily burdening the applicant for matters outside his control.

Principal Planner Chow clarified that the Commission had the authority to approve the architectural control and Below Market Rate (BMR) agreement and the only item continued on to the City Council with Planning Commission recommendation was the tentative map.

Commissioner Goodhue said she loved the multitude of uses in the project which was exactly what was needed downtown. She said she was struggling to want the building as presented, and thought the project should be continued. Mr. Hayes asked if it was the style. Commissioner Goodhue said she appreciated the thought given to the project but intimacy was the thing that was missing. Mr. Hayes noted that the Specific Plan dictated the floor heights of 15-feet and 12-foot ceilings and scale. Commissioner Riggs said the 30-foot height was fine but the sense of scale had to do with materials, arrangements and rhythms. Commissioner Goodhue said volumes. She said she agreed on the procedural level that this was a perfect candidate for a study session when the project was in the earlier stages.

Commissioner Strehl said she thought the project needed to be continued and more time spent on the design to improve it as well as get resolution of the issues on Chestnut Lane.

Commissioner Onken said he appreciated the aesthetic concerns but he thought those concerns were well resolved in the project.

Chair Combs asked staff about Commissioner Onken's motion noting it was to continue the project to investigate what could be done on Chestnut Lane. He said as is if seconded and approved, there was no direction for the applicant about the project itself. Principal Planner Chow said Commissioners had expressed concerns with the architecture, the massing, the materials, and the rhythms so potentially they would move in two steps. She said first staff could come back with more clarity about Chestnut Lane to the Commission, and at that time the Commission might provide additional direction to the applicant. She said if they wanted the applicant to change the design that potentially that could come back as a study session after information about Chestnut Lane was received. She said Commissioner Onken's motion as stated would not require the applicant to do anything at this time.

Commissioner Onken said the Commission could approve the BMR agreement and make the subdivision recommendation to City Council this evening. He said if there were aesthetic concerns that he did not think anything could be done for a project of this size until Chestnut Lane was resolved.

Commissioner Barnes said he thought they could vote on various parts of the actions required. He said regarding Chestnut Lane that there needed to be information upon which to make decisions. He said if it proved the aesthetics was the stickler then they could have a discussion on that.

Commissioner Riggs said he did not see this project process different from what the Commission has been asked to handle in the past. He said he would like to second the motion for continuation and request that it also include direction to the applicant to look at the design scale and how the facades worked particularly with the pedestrian scale in terms of the character and the difference between facades that were rigid or were intimate. Commissioner Onken said to clarify that the applicant would look at aesthetics and character but staff and Public Works would be charged to look at Chestnut Lane as the answer to that would not come from the applicant.

Principal Planner Chow asked if the Commission's intent was to see a revised design when they brought this back with information clarifying what could be done on Chestnut Lane. She said staff once a determination was made on what was feasible with Chestnut Lane would then have the applicant work on a revised design based on that information. Chair Combs said the design revision should address all the concerns raised about the design and not just specifically in regards to what could be done on Chestnut Lane. He asked Commissioner Onken if he saw this coming back as an item for approval or in some other form. Commissioner Onken said he would like the project to return to the Commission as it came for this meeting with all actions to be taken.

Chair Combs said the motion was to continue this item to a future meeting to have staff address issues regarding Chestnut Lane and have the applicant and architect look at the issues raised about the façade to make the architecture a more intimate pedestrian scale and create a building with more character as suggested by Commissioner Riggs.

**ACTION:** Motion and second (Onken/Riggs) to continue the project to a future meeting date with the following direction; passes 7-0.

- Planning staff to confer with Public Works (Transportation/Engineering) and the Menlo Park Fire Protection District as to what could be committed to on Chestnut Lane to resolve concerns with pedestrian safety and vehicular impacts such as not requiring a sidewalk on the project



side, expanding the sidewalk on the side opposite the project and installing safety barriers, and making traffic one-way.

- Applicant based on information provided on Chestnut Lane design feasibility to revise design accordingly and also look at revising design by using different materials, changing building rhythms, or some combination of efforts to create a building with more character and an intimate, pedestrian scale.

## **H. Adjournment**

Chair Combs adjourned the meeting at 11:05 p.m.

Staff Liaison: Principal Planner Deanna Chow

Recording Secretary: Brenda Bennett

Approved by the Planning Commission on January 22, 2018



## REGULAR MEETING MINUTES - EXCERPTS

**Date:** 9/17/2018  
**Time:** 7:00 p.m.  
**City Council Chambers**  
**701 Laurel St., Menlo Park, CA 94025**

**A. Call To Order**

Vice Chair Andrew Barnes called the meeting to order at 7:02 p.m.

**B. Roll Call**

Present: Andrew Barnes (Vice Chair), Drew Combs, Camille Kennedy, John Onken, Henry Riggs (arrived at 7:04 p.m.)

Absent: Susan Goodhue (Chair)

Staff: Arnold Mammarella, Contract Architect; Kaitie Meador, Associate Planner; Ori Paz, Assistant Planner; Kyle Perata, Acting Principal Planner; Matt Pruter, Associate Planner

**H. Study Session**

- H1. Architectural Control and Major Subdivision/Vasile Oros/706-716 Santa Cruz Avenue: Study Session on a request for architectural control for the demolition of an existing commercial building and the construction of a new three-story mixed use building with a below ground parking lot, retail and parking on the first floor, office on the second floor, and office and four residential units on the third floor in the SP-ECR/D (El Camino Real Downtown/Specific Plan) zoning district. The project includes a request for a major subdivision to create six parcels including four residential units, one commercial/retail unit, and one office unit, and a Below Market Rate (BMR) housing agreement for compliance with the City's below market rate housing program. Removal of one on-street parking space on Chestnut Street would be required to meet fire access requirements. As part of the proposed project, two heritage trees would be removed: one on-site tree located in the parking lot at the rear of the property and one street tree on Chestnut Street. ([Staff Report #18-082-PC](#))

Staff Comment: Associate Planner Meador said correspondence was received after publication of the staff report. She said those had been provided to the Commission at the dais. She said one was an email communication between the applicant and a neighbor that focused on trash collection for the site and a few other things. She said a letter was received supporting the project. She said two other emails had concerns with the project as far as its consistency with other projects in the downtown area and its size. She said the City's consulting contract architect, Arnold Mammarella, was present.

Applicant Presentation: Bob Giannini, Form4 Architecture, project architect, said one of the major goals of the Specific Plan was that buildings having minimal or zero lot lines have a special relationship with the street. He said they worked to create a building that was extroverted using clear glass and bay windows. He said the retail on the bottom was meant to be a glassy kind of warm modern expression and the office space on the second floor with its bay windows pushed out into the street with a glimpse of the roof and residential units. He said they re-worked the geometry of the building to address concerns about Chestnut Lane by pushing the driveway to the underground garage to the far edge of the site and beyond the shops that were across Chestnut Lane. He said the garage driveway was minimum width and went straight into the garage, so cars could get in and out quicker. He said the bottom area contained a staging area for trash and deliveries and allowed access to the back of all the retail units. He noted they had brought a materials board. He said the building base would be a buff-colored limestone with the base of the columns slightly darker. He said the upper material was a gray in either stone or tile that wrapped down through the major modulation.

Mr. Giannini said this application tried to resolve several of the major issues from the previous proposal related to Chestnut Lane. He said the street width was the same as before, but they had pulled the building back and created a five-foot sidewalk behind the columns with wall plantings to create an interesting entrance into Chestnut Lane. He said related to trash and deliveries they made the garage somewhat deeper to another half-level that freed up several ground parking spaces to use for staging for trash and deliveries.

Commissioner Riggs asked about the availability of space to allow for the ceiling height on the third floor for the office space there. Mr. Giannini said the ceiling height on the third floor would be relatively low, but they had made a good portion of the roof of that space skylight. He said they were within the height limit of 38-feet.

Commissioner Onken said it seemed a complicated mixed-use strategy and asked the level of confidence that access for different uses was accommodated. Mr. Giannini said he was sure it would all work. He said the office lobby had an elevator and a stair and the elevator was specifically for the office. He said the residential lobby had an elevator and stair and that elevator was specifically for the residential used and would pass through the office floor. He said the stairs were connected by a corridor on the third floor so both uses have two exits. He said in an emergency each of the two uses would share each of the two lobbies.

Commissioner Onken asked about the plantings and maintaining those. Mr. Giannini said the edge of the roof deck above was the planter. Commissioner Onken said the edge of the deck was the glass guardrail. Mr. Giannini said the plantings would be a low-maintenance plant material and they would study how to maintain it. He said the idea of putting planters on an edge like that was something they had done often and sometimes with very large trees. He said the value of having plantings up in the air like that was huge and the logistics of doing that could be worked out. He said that when they returned next they could discuss that in more detail. Commissioner Onken said he would like to see how they would support that and the question of maintenance of it.

Commissioner Strehl asked about the restroom facility for the third-floor office space. Mr. Giannini said he would need to check on that and get back to the Commission.

Vice Chair Barnes opened the public comment period.

Public Comment:

- Wouter Suverkropp said he was representing Lynn McMillan, one of the owners of the two-story building on Chestnut Lane opposite the project site. He thanked the applicant and his team for work on the project. He said their remaining concerns were that garbage collection would occur on Chestnut Lane, which meant trash bins on the lane or illegally in the municipal parking lot or potential idling of the garbage truck if the bins were to be brought up individually. He said they proposed requiring red curb on both sides of Chestnut Lane as that would resolve the issue, insure safety (the ability of full size emergency vehicles to access the lane), and minimize disruption for all addresses on that lane. He said secondly to require that future owners of the building maintain that agreement by having a development agreement mandating staging of trash collection vehicles and loading and unloading vehicles inside the property as just proposed by the applicant. He said the garbage collection door was located towards Chestnut Lane and relatively close to the residential lobby. He said they proposed moving the door to the entry exit ramp. He said that would simplify the garbage truck parking in the municipal parking lot and dealing with the trash bin. He said the project proposed a sidewalk along Chestnut Lane with planters under an overhang. He said they were concerned that the space behind the planters effectively created an enclosed space at the back of the building. He said they were requesting that the planters be removed, and the wall be activated with some attractive wall treatment to make the space more open.

Vice Chair Barnes closed the public comment period.

Commission Comment: Commissioner Onken said there was some confusion around the materials. Associate Planner Meador said the applicant had brought a new materials board. Commissioner Onken said the darker one was a porcelain tile and questioned if the lighter one was stone or tile.

Mr. Giannini said they were still studying materials. He said as shown on the materials board the limestone was the buff color, the base was a porcelain and the gray was porcelain. He said they were thinking they could get a better gray color if they went with porcelain. Commissioner Onken said they had had mixed results with people using porcelain tiles where it seemed to be one thing and turned out to be another.

Commissioner Onken noted the other project application and now this one for the site. He said he thought the proposed location for the garage access was much improved and in the right place. He said the points about the trash enclosure and electrical closet (or whatever it was) flipping seemed perfectly doable. He said it was a good suggestion and would help the project if trash was dragged out in front of where the cars were. He said he was not sure if the planters on Chestnut Lane added anything other than separation. He said the first-floor transparency was good. He said the project as proposed was still a modern substantial development that was the same project from one corner to the others, but it was somewhat less monumental than the previously proposed project. He said it also had a higher level of finesse and detail, which was good for the community. He said generally it was on the right track.

Commissioner Kennedy said she saw the previously proposed project when the Housing Commission reviewed it. She said this proposal was much improved. She said whether the

planters along the building on Chestnut Lane, were attractive or not, she thought they or something else there would force pedestrians/people to exit off that sidewalk into Chestnut Lane in one place. She said she thought that was good to have such a forced exit and if not there to create it somewhere else. She said having the area entirely permeable to the roadway did not seem ideal.

Commissioner Combs said the project was improved since the previously presented project and noted he appreciated the location of the garage entry. He said the project would tend to set the tone for projects coming after as it was the first downtown project under the Specific Plan. He said he was not sure what tone it would set and what other projects would draw from it for architectural conversation. He said in many respects the building was a nicely designed, fine looking building but it could be placed in the middle of a parking lot. He said there was nothing that registered it was located on a pedestrian-heavy, small downtown strip. He said he would have liked at least the pretense of smaller storefronts. He said the proposal certainly met the Specific Plan requirements.

Commissioner Riggs said he agreed with Commissioner Combs that the project had added burden as the first to be developed on Santa Cruz Avenue under the Specific Plan. He said in architectural terms, Menlo Park was finding its way. He said modern architecture was born of an excess of the formal and over decorated architecture of the Baroque period in Europe. He said it was clear what rules it was breaking and what its message was. He said with this project unfortunately it had no context to work with, so it was a standalone building. He said he would like the building to have more unity and he had issue with the tallest and boldest element of a fairly lateral building being a recessed tower. He said he did not know if that could be addressed. He said he found it problematic that the third-floor materials occasionally dropped down into the second floor as that caused the building to not be read as a building. He said successes of the design were the fun of transparency and play of the glass recessed at the lower level and put forward on the second level, the pedestrian scale in large part due to the drop from the bay windows, the notably different color of the column bases, visible joints in the stone bringing solidity, the use of color other than beige, the garage solution, and the wonderful element of the sidewalk and Chestnut Lane. He said the base color appeared brown on the materials board. He said in a larger scale and more sunlight, he thought it would be more colorful and used nicely at the second-floor line. He asked regarding the planting on the roof whether the applicant could show them another project where something similar had been done and worked. He said he might not get staff or Commission support for his encouragement to the architect to challenge the requirements of the Specific Plan that conflicted with having a good building.

Vice Chair Barnes said that as a non-architect the plans presented were the easiest for him to understand since being on the Planning Commission. He noted the retail facing Santa Cruz Avenue and asked what retail was anticipated there. Mr. Vasile Oros said he was the project proponent and expected restaurant and coffee shop use. He said other retail was purchased so much online that he was not sure what other retail would work there.

Vice Chair Barnes asked staff if this project would be able to get bulb outs for seating along Santa Cruz Avenue. Acting Principal Planner Perata said he did not know directly but he recalled it was a process years ago working with the businesses in an application process with some cost sharing. He said he did not know if there was the opportunity to expand that to additional restaurants. He said they could investigate and report back.

Vice Chair Barnes said the project was under parked by three spaces and asked how hard it would be to get those spaces. Mr. Giannini said there was an inconsistency on the data sheet to be corrected to show 65 spaces, which was the minimum required number of spaces and what they were providing. He said they were showing a fire pump room in the garage, which they might not need. He said if they did not they could pick up a space or two.

Vice Chair Barnes said page seven of the staff report indicated seven revisions needed. Mr. Giannini said they would resolve those noting the magnitude of the numbers was small as to whether a column counted for floor area ratio (FAR) or not. He said they might have to count the space above Chestnut Lane as FAR. He said also there was a minor revision to make the modulation perfect under the Specific Plan requirements.

Vice Chair Barnes said the staff report asked the Commission to weigh in on three areas. The first was whether further refinement of the design and/or materials was needed. He said he thought it was well done and he had no issues with the design or materials. He said regarding the question of further modification to the design's access and circulation on Chestnut Lane that what was being proposed now worked well for the site.

Vice Chair Barnes asked about the radius needed for turning into the garage and whether upon exiting one could turn left or right. Mr. Giannini noted that the radius was the same as for any turn into a driveway. He noted the garage entrance and exit were flat for some distance and then ramped down or up. He said vehicles could make a left or right onto Chestnut Lane. He said the visibility was good as the building was set back some creating a staging area before a vehicle was out onto Chestnut Lane. Vice Chair Barnes asked if removing the planters would improve visibility for vehicles entering or exiting the garage. Mr. Giannini said it would not as the columns would block view before the planters would. He said he would look at removing the columns and cantilevering the second floor, noting that would resolve one of the FAR issues and improve visibility if that change did not hurt the elevation.

Vice Chair Barnes said staff asked the Commission to consider whether to require photo-voltaic (PV) installation on the roof. He asked if that or using 100% renewable energy had been contemplated for this project. Mr. Giannini said per City guidelines they had to reserve space on the roof for PVs. He said that was shown on the plans but at this point there was not a plan to do PVs. He said their energy models did not show inability to meet LEED requirements without the use of PVs, so they had not contemplated it. Vice Chair Barnes said there was the option of using 100% renewable energy source.

Vice Chair Barnes said the project refinements suited Santa Cruz Avenue noting the prior proposal was bulky. He said related to the project setting the bar for future projects in the downtown under the Specific Plan he thought the project was fine in its aesthetic and architecture.

Commissioner Onken said Commissioner Riggs had brought up scale and the question of whether the building should be more unified or more of a single entity. He referred to comments about whether this building was appropriate for Santa Cruz Avenue. He said he thought that they needed to go back to the founding principles of this land development, which was whether they were happy that smaller lots were being combined into larger projects. He said if they were then the question was whether they wanted the larger building or a building that gave the feel of smaller buildings. He said the latter was what he thought the Specific Plan attempted to do. He said he would like to

see further development as to how the actual retail signage would work noting concerns of losing “Main Street” and to solve for that by putting more focus on the retailers less than the building.

Commissioner Riggs said regarding unity of a building and being out of scale, one trick that was used was to have a central area that might be half of a façade express itself as the building with the remaining portions appearing to be wings to the building. He said that was pushing too far as the project already had one architectural direction. He said the rhythm of the columns worked and he liked the Chestnut Street façade the best.

Replying to Vice Chair Barnes, Associate Planner Meador said the amount of the project’s office square footage triggered a requirement of Below Market Rate (BMR) housing. She said previously the project required .9 of a BMR unit. She said with this proposal it was 1.2 BMR units. She said they needed to review the BMR requirement by taking the project back to the Housing

**J. Adjournment**

Vice Chair Barnes adjourned the meeting at 9:34 p.m.

Staff Liaison: Kyle Perata, Acting Principal Planner

Recording Secretary: Brenda Bennett

Approved by the Planning Commission on October 8, 2018



**706 Santa Cruz Ave  
Response to Study Session Comments**

December 10, 2019

To: The City of Menlo Park Planning Division  
Ms. Kaitie Meador

From: 706 Santa Cruz Ave., LLC. and  
From4 Architecture, Robert Giannini, Architect

Subject: Response to Study Session Comments

Dear Kaitie:

*Following are the comments we heard at the Study Session, and our responses:*

**Planning Commission Study Session Feedback:**

- Confirm that the proposed uses can all be accommodated within the proposed building. For example will there be restrooms for the 3<sup>rd</sup> floor office use.  
[Men and Women Toilet Rooms were added to the third floor.](#)
- Provide additional information on how the landscaping on the building will be maintained.  
[There was concern from some Commissioners that the planters shown on the 3rd floor decks might pose a maintenance challenge. We reviewed this with several landscape architects including the landscape architect for this particular project, and there was no concern from any of them. Maintaining the planters would not be unlike maintaining plants in a window box.](#)
- Relocate the trash room to the driveway side.  
[Trash room door was moved to the north \(driveway\) side of the trash room and the gas meters which were located at this side of the room were relocated. The gas meters were then relocated again in subsequent submittals.](#)
- Remove the raised planters along Chestnut Lane and enhance the façade wall behind the columns (neighbor comment). Many commissioners agreed that the planters could be removed but some also suggested keeping some other kind of barrier to prevent pedestrians from walking through the columns (bollards or at grade planter).  
[The plans show that we removed \(2\) of the planters along Chestnut Lane in response to this. The two planters which were to the north and south of the pair of doors into the garage \(by the accessible parking\) remained, one in front of the doors and one at the south side were removed. Based on the discussions it seems this will be the best compromise to mixed comments from the Commissioners.](#)
- Desire for more intimate storefronts to maintain the existing Downtown character.  
[The storefronts, and indeed the entire project, was designed to fit into the context. Due to the strong horizontal capping the second floor the building will appear to be 2 rather than 3 story. The third story is set back and buffered with planting. The storefronts have been designed with great detail and are executed in warm materials creating intimate and high end entries into retail spaces as well as the building lobbies.](#)
- Provide more details on the retail tenant signage and branding (sign guidelines attached for your reference).  
[For this project sign design must adhere to City guidelines and will also be dependent on the nature of the future users. As there is no requirement to finalize sign design with the building shell we have elected to defer sign design and approval to the future when tenants are known.](#)



- Outdoor seating options for the retail tenants.  
We were informed that no seating will be allowed on our sidewalk. Therefore all images of seating has been removed from the plans and renderings. No outdoor seating will be provided.
- Consider the design/material treatment of the modulations and ensure that they do not detract from the unity of the overall design.  
We have carefully followed the design guidelines relative to the modulations and what contrast of materials is required. The base of the building (2 stories) is buff colored limestone. The third floor is slate gray tile. Where the module occurs the gray material drops down creating the desired contrast. Due to the formulaic nature of the guidelines the design is quite unified.

Thanks very much for your attention and review of the various design aspects of this project!

*Form4 Architecture*



*Robert Giannini  
Architect, President*

706-716 Santa Cruz Avenue – Attachment L: Data Table

	<b>PROPOSED PROJECT</b>	<b>EXISTING DEVELOPMENT</b>	<b>ZONING ORDINANCE</b>
Lot area	23,454 sf	23,454 sf	n/a sf min.
Setbacks			
Santa Cruz Ave	0 ft.	0 ft.	0 ft. min./max.
Chestnut St	0 ft.	76.3 ft.	0 ft. min./max.
Chestnut Ln	0 ft.	0 ft.	0 ft. min./max.
Side (interior)	0 ft.	0 ft.	0 ft. min.
Density	4 du 7.4 du/acre	n/a du n/a du/acre	13.5 du max. 25 du/acre max.
FAR (Floor Area Ratio)	46,908 sf 200 %	12,758 sf 54 %	46,908 sf max. 200 % max.
Square footage by use			
Residential	11,405 sf	n/a sf	
Retail	12,049 sf	12,758 sf	
Office	23,454 sf	n/a sf	
Building height	37.75** ft.	31.8 ft.	38.0 ft. max.
Parking	69 spaces	18 spaces	68 spaces; first 1.0 FAR covered by replacement of existing parking spaces; 1 space per du min. (residential); 3.8 spaces per 1,000 sf min. (non-medical office).
Trees	Heritage trees 4*	Non-Heritage trees 11*	New Trees 4*
	Heritage trees proposed for removal 2*	Non-Heritage trees proposed for removal 3*	Total Number of Trees 14*
*Includes street trees			
**Variance requested for the third floor skylights to exceed the height limit.			

# 706 SANTA CRUZ

ARCHITECTURAL REVIEW

706 Santa Cruz Ave., LLC.



FORM4 ARCHITECTURE • THE GUZZARDO PARTNERSHIP • KIER & WRIGHT • ADAPTURE • INTERFACE ENGINEERING

## 706 SANTA CRUZ AVE. MENLO PARK

Architectural Review  
706 Santa Cruz Ave., LLC.

CS 0.0  
November 25, 2019



## PROJECT DESCRIPTION:

A new three story mixed use building with one and a half levels of below grade parking, ground floor residential and office lobbies, parking & retail; second floor office and balconies; and third floor residential units, office and balconies.

### INDEX

CS 0.0 Cover Sheet  
CS 0.1 Drawing Index & Project Data  
CS 0.2 Code Compliance

### MASTER PLAN

MP 0.1 Vicinity Map  
MP 0.2 Street Views Context  
MP 0.3 Existing Photos  
MP 0.4 Existing Building Elevation Photos  
MP 1.0 Floor Plans, Area Analysis & LEED  
MP 1.1 Floor Plans & Area Analysis  
MP 1.2 Detailed Area Analysis  
MP 1.3 Detailed Area Analysis  
MP 1.4 Existing Building Area  
MP 2.1 Shadow Study  
MP 3.0 Site Line Study  
MP 3.1 Site Line Study

### ARCHITECTURAL

A 0.1 Area Plan  
A 0.2 Existing Site Plan  
A 0.3 Proposed Site Plan  
A 1.1 Ground Floor Plan  
A 1.2 Second Floor Plan  
A 1.3 Third Floor Plan  
A 1.4 Roof Plan  
A 1.5 Parking Level P1  
A 1.6 Parking Level P2  
A 2.1 Santa Cruz Rendering  
A 2.2 Santa Cruz Elevation & Modulation Analysis  
A 2.3 Chestnut Street Rendering  
A 2.4 Chestnut Street Elevation & Modulation Analysis  
A 2.5 Chestnut Lane Rendering  
A 2.6 Chestnut Lane Elevation & Modulation Analysis  
A 2.7 North Property Line Elevation  
A 3.1 Materials  
A 3.2 Transparency Analysis  
A 3.3 Projection Analysis  
A 3.4 Material & Color Board  
A 4.1 Building Section & Height Analysis  
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A 4.4 Wall Sections  
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### ELECTRICAL

E 0.1 SYMBOL LIST AND GENERAL NOTES  
E 2.0 PARKING LEVEL 2 PLAN - ELECTRICAL  
E 2.1 PARKING LEVEL 1 PLAN - ELECTRICAL  
E 2.2 FIRST FLOOR PLAN - ELECTRICAL  
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### LANDSCAPE

L 1.0 Landscape Notes & Legend  
L 2.1 Landscape Plan  
L 2.2 Landscape Plan  
L 2.3 Landscape Plan  
L 3.0 Landscape Details  
L 4.0 Tree Disposition Plan

### CIVIL

C 1.0 Topographic Survey & Boundary Survey  
C 2.0 Preliminary Grading, Drainage, Utility Plan Ground Floor  
C 2.1 Preliminary Offsite Improvement Plans  
C 2.2 Preliminary Grading, Drainage, Utility Plan Second Floor  
C 3.0 Preliminary Stormwater Management Plan  
C 3.1 Preliminary Stormwater Management Plan  
C 4.0 Erosion Control  
C 5.0 Menlo Park Fire Turning  
C 5.1 Passenger Turning Radius

### VESTING TENTATIVE MAP

1 Vesting Tentative Map  
2 Proposed Conditions Lower Level  
3 Proposed Conditions Ground Level  
4 Conceptual Grading, Drainage, Utility

### UTILITY UNDERGROUNDING

GAS RELOCATION  
JT-1 Joint Trench Title Sheet  
JT-2 Joint Trench Intent  
NB RULE 16 ELECTRIC & GAS SERVICE  
JT-1 Joint Trench Title Sheet  
JT-2 Joint Trench Intent  
REMOVAL OF OVERHEAD LINES (WRO)  
JT-1 Joint Trench Title Sheet  
JT-2 Joint Trench Intent

### SHORING PLANS

SH-0.00 Cover Sheet  
SH-1.00 Temporary Shoring Plan - Notes  
SH-2.00 Temporary Shoring Plan - Plan  
SH-3.00 Temporary Shoring Plan - Elevation  
SH-3.01 Temporary Shoring Plan - Elevation  
SH-3.02 Temporary Shoring Plan - Elevation  
SH-3.03 Temporary Shoring Plan - Elevation  
SH-3.04 Temporary Shoring Plan - Elevation  
SH-4.00 Temporary Shoring Plan - Section

### CONSTRUCTION PHASING

CP-1 Crane Location & Site Access  
CP-2 Phase 1 - Demo  
CP-3 Phase 2 - Shoring & Mass Excavation  
CP-4 Phase 3 - Footings / Slab / Podium  
CP-5 Phase 4 - Erect Steel / Metal Stud Roof  
CP-6 Phase 5 - Exterior Skin / Interior Build-out  
CP-7 Parking Management  
CP-8 Temporary Fencing & Scaffolding

## PROJECT DATA:

CODE & ZONING	
APN	71102250
ZONING	SP-ECR/D
CONSTRUCTION TYPE	Type II-b
OCCUPANCY	
Parking Levels	
Retail Level 1	
Office Level 2 & 3	
Residential Level 3 (4 units)	
SITE AREA	23,454 sf 0.538 acres

BUILDING AREA NOTES & EXEMPTIONS	
Maximum FAR = 2 FAR	2.0 46,908 sf allowed
Allowable Office Area = 1 FAR	1.0 23,454 sf allowed
Maximum Residential Density	25 units per acre
Exclusion for covered parking:	N.P.M.C. section 16.04.325 (C3)
Exclusion for Trash & Recycling	N.P.M.C. section 16.04.325 (C6)
Exclusion for Area with No AC or Windows	N.P.M.C. section 16.04.325 (C1)
Exclusion for Area with Noise Generating Equipment	N.P.M.C. section 16.04.325 (C2)
Exclusion for Vent Shafts	N.P.M.C. section 16.04.325 (C5)

BUILDING AREA (please see sheet CS 1.1 for graphic depiction of data & detailed breakdown)						
	Office	Retail	Residential	FAR Subtotal	Excluded (in FAR)	Total Floor
Parking Level 1	-	-	-	-	22,579	22,579
Parking Level 2	-	-	-	-	7,934	7,934
Ground Floor	914	12,049	1,908	13,972	7,064	21,036
2nd Floor	19,099	-	766	19,866	805	20,671
3rd Floor	1,440	-	10,130	11,570	2,275	13,845
	23,454	12,049	11,405	46,908	28,957	66,865
Percentage of each use:	50.00%	25.69%	24.31%	100.00%		
Allowable Exclusion for Area with No AC or Windows	1,407 sf allowable (3% of gross area)				750 sf proposed	
Allowable Exclusion for Area with Noise Generating Equipment	469 sf allowable (2% of gross area)				357 sf proposed	

PARKING REQUIRED:	
Exemption from Parking Requirement = 1 FAR	23,454 sf
Ground Floor Retail	12,049 sf
Exemption Credit that may be used on 2nd floor:	11,405 This is the exemption allowed less the retail area of the ground floor.
Total Office Area	23,454 sf
Less exemption credit	11,405 sf
Office Area to be parked at 3.8 spaces per 1000 sf:	12,049 sf
Office Parking Required	45.79 spaces
Replace Lost Surface Parking	18 spaces
Residential Requirement (1 per unit)	4 spaces
Total Parking Required	67.79 spaces rounded up to 68 spaces required

PARKING PROVIDED:	
Surface Parking	
Standard	1
ADA	3
	4
Garage Parking	65
Total Parking Provided	69 (68 stalls required)

ELECTRICAL VEHICLE CHARGING	
Compl'd for 15% of stalls provided	10.35 rounded up to 11 spaces required
Total Standard EV Stalls provided	10
Total Accessible EV Stalls provided	1
Total EV Stalls Provided	11
(5 charging units to 10 stalls at level #1) (one accessible stall at level #1)	

BICYCLE PARKING REQUIRED:	
Short Term at 1/200 Ofc, 1/500 retail, 1/100 units residen?	6
Long Term at 1/100 Ofc, 1/200 retail, 1/4000 residential	9 (for office & retail a minimum of 2 are required)
Total Required:	15

BICYCLE PARKING PROVIDED:	
Short Term	12 (provided along Chestnut St. & Santa Cruz Ave)
Long Term	10 (provided at Garage Level 1A)
Total Required:	22



CODE COMPLIANCE

706 Santa Cruz Ave  
Garage and Shell Mixed-Use Building



Code Analysis

November 5, 2019

<b>APN #</b>	<b>071-102-250</b>
<b>Project Address:</b>	706-716 Santa Cruz Ave Menlo Park, CA 94025
<b>Legal Jurisdiction:</b>	City of Menlo Park, CA
<b>County:</b>	San Mateo County
<b>Building Codes:</b>	2016 California Building Code 2016 California Plumbing Code, 2016 California Mechanical Code, 2016 California Electrical Code, 2016 California Fire Code, 2016 California Green Building Code, 2008 California Energy Code City of Menlo Park Building Codes & Ordinances
<b>Construction Type:</b>	Type II-B Shell Office Building
<b>Sprinkler System:</b>	100% Sprinklered (CBC 903.3 / NFPA 13)
<b>Fire Alarm:</b>	Fire Alarm provided (CBC 907)
<b>Building Occupancy:</b>	A-3 occupancy (CBC 303.4 – assembly) B occupancy (CBC 304 – shell office / business) M occupancy (CBC 309 – mercantile) R-2 occupancy (CBC 310.4 – residential) S-2 occupancy (CBC 311.3 – storage / garage)
<b>Net Building Area:</b>	Basement Level P2: 7,934 sf [S-2] Basement Level P1: 22,579 sf [S-2] <b>Total: 30,513 sf</b> Ground Level L1: 20,138 sf [M: 11,817 / S-2: 6,984 sf / B (lobby): 1,400 sf] Level L2: 20,693 sf [B] Level L3: 11,844 sf [B: 2,317 sf / R-2: 9,527 sf] <b>Total: 54,029 sf</b> L3 Roof Deck 1,485 sf [A-3]

Cons\_CalcAnalysis-GARAGE-OFFICE\_TYPE\_BB.doc  
1

**Building Area:**

Allowable Building Area per story (CBC 506):

**II-B Construction (separated uses with height increase):**

**II-B for "A-3" Occupancy (CBC 506):**  
SM = 9,500 sf (with height increase)

**II-B for "B" Occupancy (CBC 506):**  
SM = 69,000 sf

**II-B for "M" Occupancy (CBC 506):**  
SM = 37,500 sf

**II-B for "R-2" Occupancy (CBC 506):**  
SM = 16,000 sf (with height increase)

**II-B for "S-2" Occupancy (CBC 506):**  
S1 = 104,000 sf & SM = 76,000 sf

**Building Height – II-B Construction:**

Allowable Building Height (CBC 504):

**II-B for "A-3" Occupancy (CBC 506):**

Ht = 75 ft / Stories = 3 (height increase from 2 to 3 stories)

**II-B for "B" Occupancy (CBC 506):**  
Ht = 75 ft / Stories = 4

**II-B for "M" Occupancy (CBC 506):**  
Ht = 75 ft / Stories = 3

**II-B for "R-2" Occupancy (CBC 506):**  
Ht = 75 ft / Stories = 5 (without area increase)

**II-B for "S-2" Occupancy (CBC 506):**  
Ht = 75 ft / Stories = 4

**Separated Occupancies Allowable Area and Height**

Note: No area increase due to frontage considered.

Garage Basement: 29,494 < 104,000, complies (CBC 506.1.3)

First Floor: 6,984/78,000 + 11,817/37,500 = 0.09 + 0.32 = 0.41 < 1.0

Second Floor: 20,693/69,000 = 0.30 < 1.0

Third Floor: 3,035/69,000 + 9,874/16,000 + 1,485/9,500 = 0.03 + 0.60 + 0.16 = 0.79 < 1.0

Sum of all floor ratios = 0.41 + 0.30 + 0.79 = 1.5 < 3.0

The sum of ratios for each story is less than 1.0 (CBC 506.4.2) and the sum of ratios for all floors is less than 3.0 (CBC 506.2.4), therefore, building area is below allowable and complies with requirements. The proposed building is three stories tall with the highest point of the building at 53'-0" above the grade plane. This complies with the code limits listed above.

Cons\_CalcAnalysis-GARAGE-OFFICE\_TYPE\_BB.doc  
2

**Wall and Shaft Fire Rating Requirements**

Separation by Use (table 508.4):

- S-2 and M 1 hr
- S-2 and B 1 hr
- R-2 and B 1 hr
- R-2 and R-2 1 hr (CBC 708)

Per CBC 420, 708 and 711 wall and floor assembly separating dwelling units shall have a fire resistive rating of 1-hr.

**Garage & Podium Construction Type/ II-B**

Per Table 601 any new construction to be:

- Bearing walls- Exlr & Inlr 0 hr
- Structural frame 0 hr
- Partitions (permanent) 0 hr
- Floors & Roof / Podium 0 hr
- Shafts >= 4 Stories 2 hr (CBC 713.4)
- Shafts < 4 Stories 1 hr (CBC 713.4)

Wall framing at Stair 1 & 2 and elevators 1 & 2 shall have 2-hour fire resistive rating.

**Exterior Wall Rating and Openings**

Exterior Wall Rating per Table 602:

- Exterior Wall 0 < 5' 2 hr (M only – table 602)
- Exterior Wall 0 < 10' 1 hr (S to 10' at M)
- Exterior Wall 10' to 30' 0 hr

**Exterior Wall Openings**

Table 705.8

- Separation 0-3' Not Permitted
- Separation 15-20' 75% (UP, S)
- Separation > 20' No Limit

**Occupant Load and Egress Requirements**

Occupant load per use (table 1004.1.2)

- A-2: Assembly = 15 sf/occ net
- S-2: Parking Garage = 200 sf/occ gross
- M: Mercantile = 60 sf/occ gross
- B: Office = 100 sf/occ gross
- R-2: Residential = 200 sf/occ gross

Maximum occupant load assumed at second floor "B" occupancy with 10% maximum allowed accessory assembly occupancy:

18,674 \* 0.1 = 1,868 & 18,674 – 1,868 = 16,806 SF

16,806/100 + 1,868/15 = 169 + 125 = 294 occupants total / 147 per exit

18,674/294 = 63.5 SF / OCC

Stairway minimum width = 147\*0.3 = 44.1' & Minimum width of components = 147\*0.2 = 30"

**Residential Emergency Egress**

CBC Section 1030: 44" max. AFF; 5.7 sf min open area; 24" min high, 20" min. wide.

**Residential Ventilation Requirements**

CBC Section 1203.5: 4% of floor area

Cons\_CalcAnalysis-GARAGE-OFFICE\_TYPE\_BB.doc  
3







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# 706 SANTA CRUZ AVE. MENLO PARK

Architectural Review  
706 Santa Cruz Ave., LLC.

VICINITY MAP

MP 0.1  
November 25, 2019







4. Santa Cruz Ave Proposed Streetscape



3. Santa Cruz Ave Existing Streetscape



2. Chestnut St. Proposed Streetscape



1. Chestnut St. Existing Streetscape

FORM4 ARCHITECTURE • THE GUZZARDO PARTNERSHIP • KIER & WRIGHT • ADAPTURE • INTERFACE ENGINEERING

## 706 SANTA CRUZ AVE. MENLO PARK

Architectural Review  
706 Santa Cruz Ave., LLC.

## STREET VIEWS CONTEXT

MP 0.2  
November 25, 2019







1: View looking Southwest down Santa Cruz Ave



2: View looking North at corner of Santa Cruz Ave & Chestnut St.



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## 706 SANTA CRUZ AVE. MENLO PARK

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706 Santa Cruz Ave., LLC.

EXISTING SITE PHOTOS

MP 0.3  
November 25, 2019







Existing Building East Elevation



Existing Building South Elevation



Existing Building West Elevation

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## 706 SANTA CRUZ AVE. MENLO PARK

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706 Santa Cruz Ave., LLC.

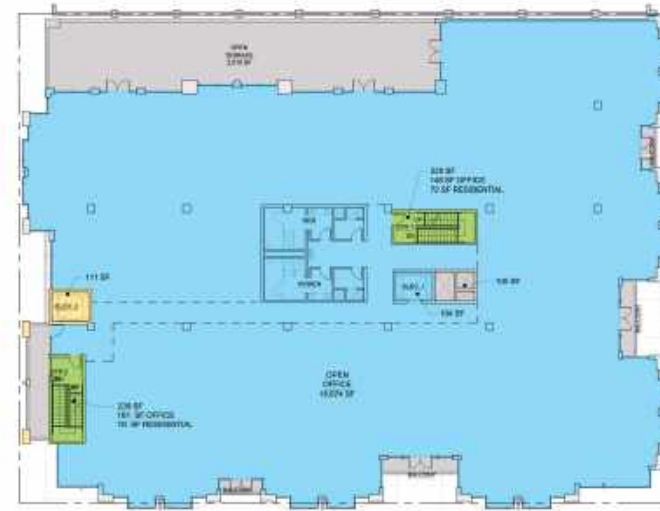
## EXISTING BUILDING ELEVATION PHOTOS

MP 0.4  
November 25, 2019





THIRD FLOOR PLAN



SECOND FLOOR PLAN



LEED v4 for BD+C: Core and Shell  
Project Checklist

Project Name: 706 Santa Cruz  
Date: 7/27/18  
Zipcode: 94025 **DRAFT**

Y	N	NA	Code	Description	Points
Y			0	Integrative Process	1
Y			0	23 Location and Transportation	20
Y			0	LEED for Sustainable Development Location	20
Y			0	Sensitive Land Protection	2
Y			0	High Priority Site	3
Y			0	Sustaining Density and Shared Uses	6
Y			0	Access to Quality Transit	0
Y			0	Moove Facilities	1
Y			0	Reduced Parking Footprint	1
Y			0	Green Vehicles	1
Y			0	21 Sustainable Sites	11
Y			0	Construction Activity Pollution Prevention	Required
Y			0	Site Assessment	1
Y			0	Site Development - Protect or Restore Habitat	1
Y			0	Open Space	1
Y			0	Revegetation	3
Y			0	New Plant Protection	2
Y			0	Light Pollution Reduction	1
Y			0	Sensit Design and Construction Guidelines	1
Y			0	9 Water Efficiency	11
Y			0	Outdoor Water Use Reduction	Required
Y			0	Indoor Water Use Reduction	Required
Y			0	Ballpck Level Water Metering	Required
Y			0	Outdoor Water Use Deductions	2
Y			0	Indoor Water Use Reduction	0
Y			0	Coating Tower Water Use	2
Y			0	Water Metering	1
Y			0	18 15.0 Energy and Atmosphere	33
Y			0	Functional Commissioning and Verification	Required
Y			0	Minimum Energy Performance	Required
Y			0	Building Level Energy Metering	Required
Y			0	Functional Reliability Management	Required
Y			0	Operational Commissioning	6
Y			0	Optimize Energy Performance	18
Y			0	Advanced Energy Metering	1
Y			0	Demand Response	3
Y			0	Renewable Energy Production	1
Y			0	Enhanced Reliability Management	1
Y			0	Green Power and Carbon Offsets	2
Y			0	4 Materials and Resources	14
Y			0	Storage and Collection of Recyclables	Required
Y			0	Construction and Demolition Waste Management Planning	Required
Y			0	Building Life-Cycle Impact Reduction	0
Y			0	Building Product Disclosure and Optimization - Environmental Product Declarations	3
Y			0	Building Product Disclosure and Optimization - Sourcing of Raw Materials	3
Y			0	Building Product Disclosure and Optimization - Material Ingredients	3
Y			0	Construction and Demolition Waste Management	2
Y			0	4 Indoor Environmental Quality	10
Y			0	Minimum Indoor Air Quality Performance	Required
Y			0	Environmental Tobacco Smoke Control	Required
Y			0	Enhanced Indoor Air Quality Strategies	2
Y			0	Low-Glinting Materials	3
Y			0	Construction Indoor Air Quality Management Plan	1
Y			0	Daylight	3
Y			0	Quality Views	1
Y			0	9 Innovation	6
Y			0	Innovation	1
Y			0	LEED Accredited Professionals	1
Y			0	3 Regional Priority	4
Y			0	Regional Priority: Optimize Energy Performance (10 points)	1
Y			0	Regional Priority: Building life-cycle impact reduction (3 points)	1
Y			0	Regional Priority: BPOC sourcing of raw materials	1
Y			0	Regional Priority: Indoor Water Use Reduction (1 point)	1
Y			0	40 TOTALS	100

Certified: 40 to 49 points, Silver: 50 to 59 points, Gold: 60 to 79 points, Platinum: 80 to 90

Note: The points on this scorecard are preliminary and are subject to change as the project develops.



FIRST FLOOR PLAN

AREA LEGEND

- RETAIL
- OFFICE
- RESIDENTIAL
- SHARED
- EXCLUDED
- 1% 8-5% EXCLUDED

TRUE NORTH PROJECT NORTH

0 16' 32'

SCALE: 1/8" = 1'-0"

706 SANTA CRUZ AVE. MENLO PARK

Architectural Review  
706 Santa Cruz Ave., LLC.

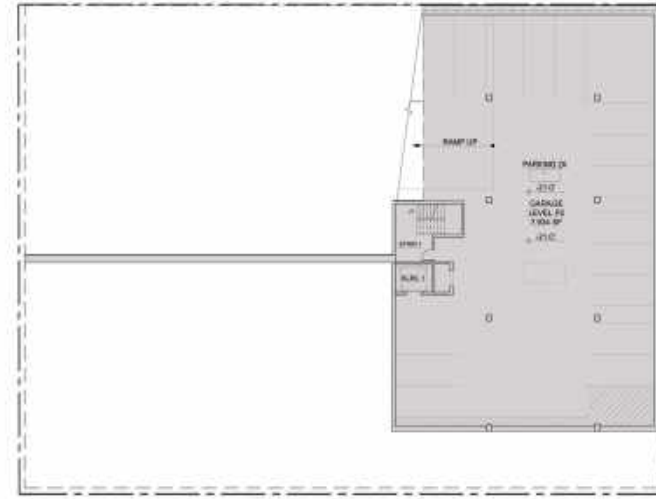
FLOOR PLANS, AREA ANALYSIS & LEED  
(see following sheets for enlarged plans)

MP 1.0  
November 25, 2019

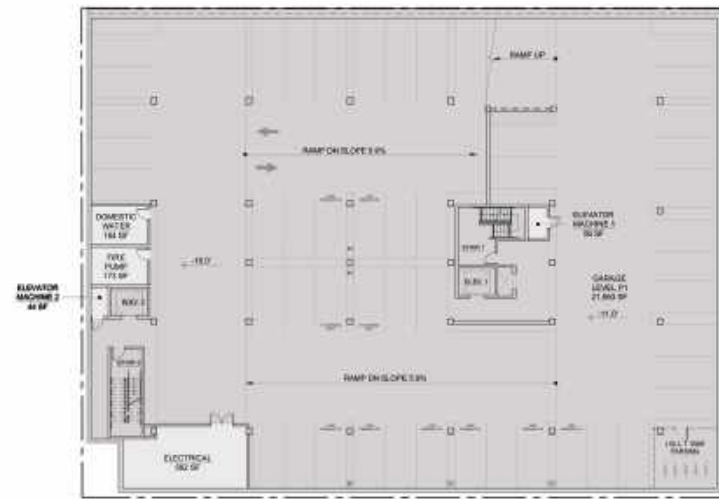


DETAILED AREA SUMMARY							
Garage P1 & P2		AREA	Area Classification				
		OFFICE	RETAIL	RESIDENTIAL	EXCLD	EXCLD 3%	EXCLD 1%
Garage P1	21,560				21,560		
P1 Domestic Water	184						184
P1 Fire Pump	173						173
P1 Electrical Room	562					562	
P1 Elev. Mach. 1	56					56	
P1 Elev. Mach. 2	44					44	
Garage P2	7,934				7,934		
	30,513				29,494	662	357
<b>1st FLOOR</b>							
Covered Parking	6,499				6,499		
Trash Enclosure	426				426		
Mechanical Shaft	55				55		
MDF Room	63					63	
Exterior Columns	36	24		12			
Retail 1/2	7,531		7,531				
Retail 3	4,286		4,286				
Lobby 1	634	317	165	152			
Stair 1	265	179		86			
Stair 1 Lobby	259	130	67	63			
Elev 1	104	104					
Lobby 2	507			507			
Inaccessible Spaces	25					25	
Stair 2	239	161		78			
Elev 2	111			111			
subtotal:	21,036	914	12,049	1,008	6,976	88	0
<b>2nd FLOOR</b>							
Office	18,674	18,674			2,019		
North Terrace	2,019				2,019		
Exterior Columns	18	12		6			
Inaccessible Spaces	151				151		
Elev 1	104	104					
Stair 1	220	148		72			
Elev 2	111			111			
Stair 2	239	161		78			
Mechanical Shaft	105				105		
subtotal:	21,641	10,099	0	266	2,275	0	0
<b>3rd FLOOR</b>							
Office	2,317	2,317					
Balcony 5	1,485			1,485			
Apt 1	2,500			2,500			
Balcony 1	1,187			1,187			
Apt 2	2,274			2,274			
Balcony 2	441			441			
Apt 3	2,354			2,354			
Balcony 3	178			178			
Apt 4	2,399			2,399			
Balcony 4	1,191			1,191			
Hallway	1,044	704		340			
Mechanical Shaft	105				105		
Stair 1	222	150		72			
Elev 1	105	105					
Stair 2	244	164		80			
Elev 2	111			111			
subtotal:	18,157	3,440	0	10,130	4,587	0	0
total:	23,454	12,049	11,405	43,332	750	357	
FAR Ratio:	0.500	0.257	0.243				
Weighted Ratio:	0.673		0.327				
total FAR area:	46,908						

F.A.R. SUMMARY	
Site Area:	23,454 sf
Max. FRI:	1.0
Allow Building Area:	46,908 sf
Proposed Building Area:	46,908 sf
Max. Office Area:	23,454 sf (max. office area = 1.0 FAR)
Proposed Office Area:	23,454 sf
Allow. Area Excluded at 3%:	1,407 sf
Proposed Excluded at 3%:	750 sf
Allow. Area Excluded at 1%:	469 sf
Proposed Excluded at 1%:	357 sf

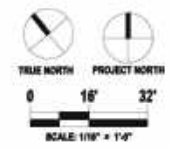


**GARAGE LEVEL P2**



**GARAGE LEVEL P1**

- AREA LEGEND
- RETAIL
  - OFFICE
  - RESIDENTIAL
  - SHARED
  - EXCLUDED
  - 1% & 3% EXCLUDED



# 706 SANTA CRUZ AVE. MENLO PARK

Architectural Review  
706 Santa Cruz Ave., LLC.

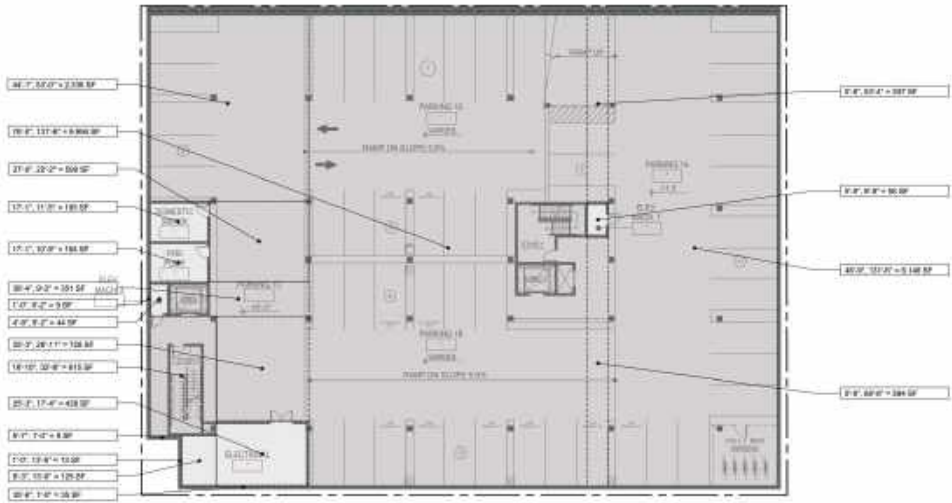
## FLOOR PLANS & AREA ANALYSIS

(see following sheets for enlarged plans)

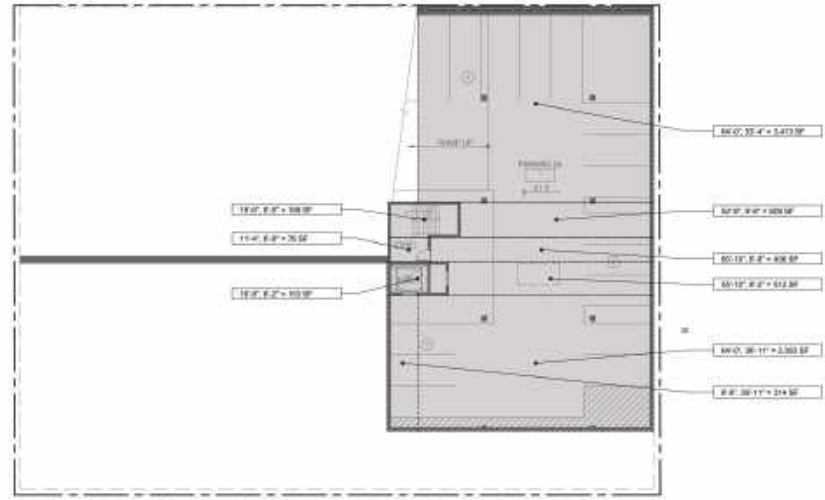
MP 1.1  
November 25, 2019



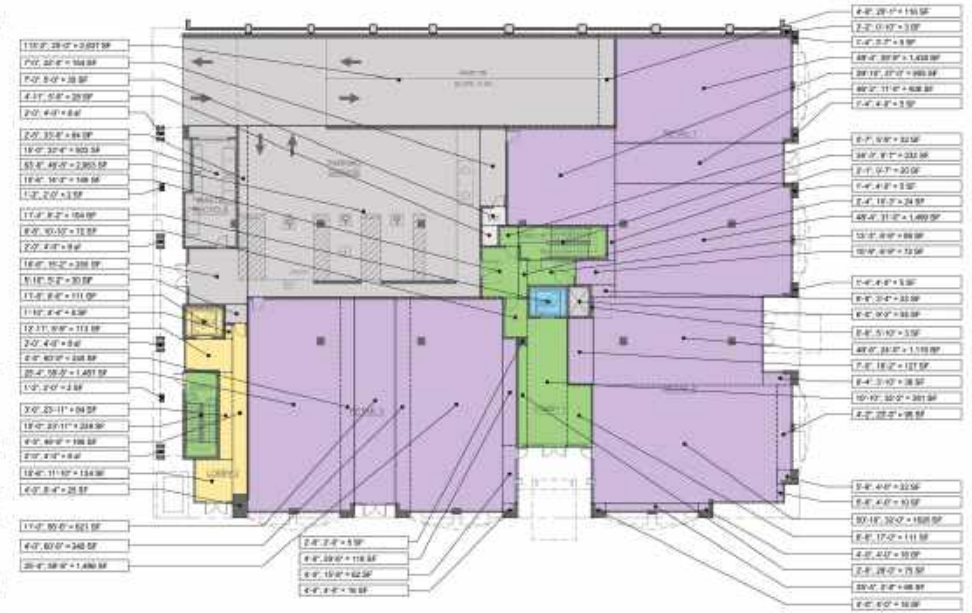




**GARAGE LEVEL P1**

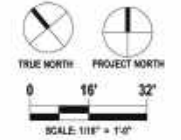


**GARAGE LEVEL P2**



- ASBL LEGEND**
- RETAIL
  - OFFICE
  - RESIDENTIAL
  - SHARED
  - EXCLUDED
  - 1% & 3% EXCLUDED

**GROUND FLOOR PLAN**

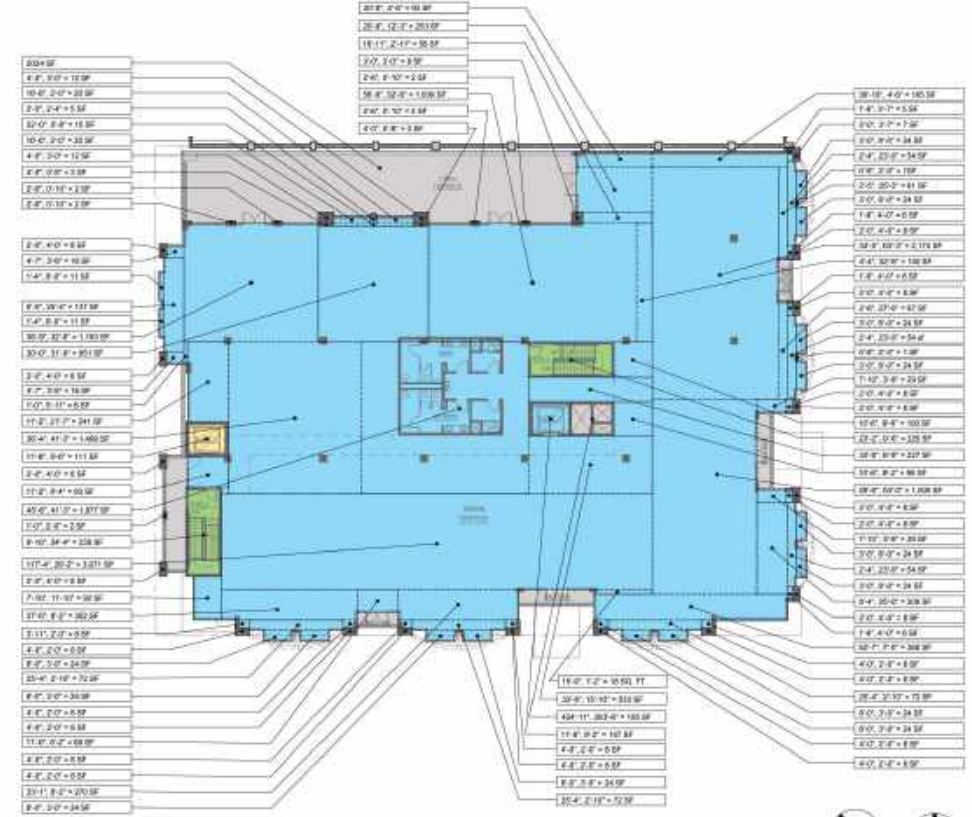


**AREA LEGEND**

- RETAIL
- OFFICE
- RESIDENTIAL
- PARKING
- EXCLUDED
- 1% & 3% EXCLUDED



**THIRD FLOOR PLAN**



**SECOND FLOOR PLAN**

TRUE NORTH PROJECT NORTH

0 16' 32'

SCALE 1/8" = 1'-0"

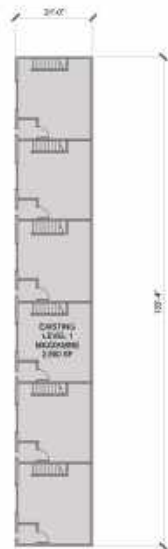
**706 SANTA CRUZ AVE. MENLO PARK**

Architectural Review  
706 Santa Cruz Ave., LLC.

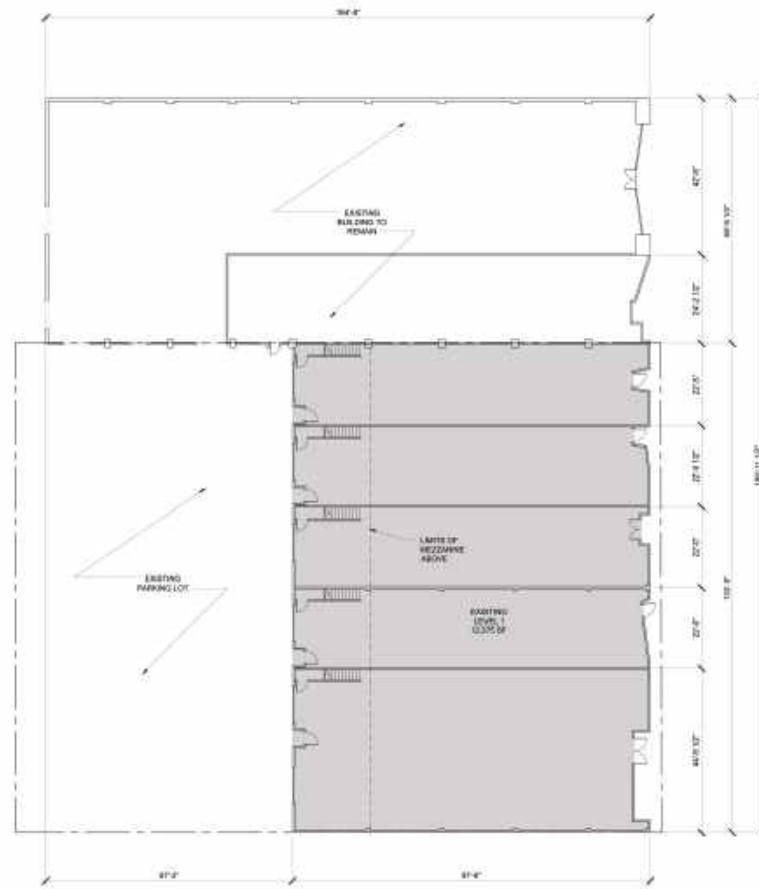
**DETAILED AREA ANALYSIS**  
(see following sheets for enlarged plans)

MP 1.3  
November 25, 2019

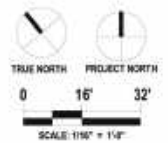




**EXISTING FIRST FLOOR MEZZANINE PLAN - 2,800 SF**



**EXISTING FIRST FLOOR PLAN - 12,375 SF**



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## 706 SANTA CRUZ AVE. MENLO PARK

Architectural Review  
706 Santa Cruz Ave., LLC.

EXISTING BUILDING AREA

MP 1.4  
November 25, 2019







Winter Solstice 9am



Winter Solstice 12pm



Winter Solstice 3pm



Summer Solstice 9am



Summer Solstice 12pm



Summer Solstice 3pm

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## 706 SANTA CRUZ AVE. MENLO PARK

Architectural Review  
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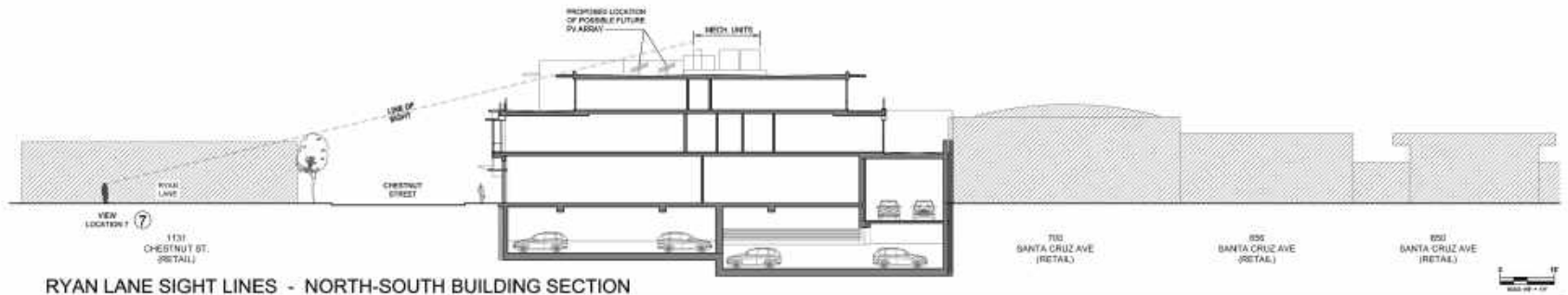
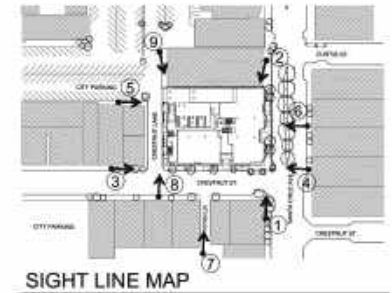
### SHADOW STUDY

MP 2.1  
November 25, 2019



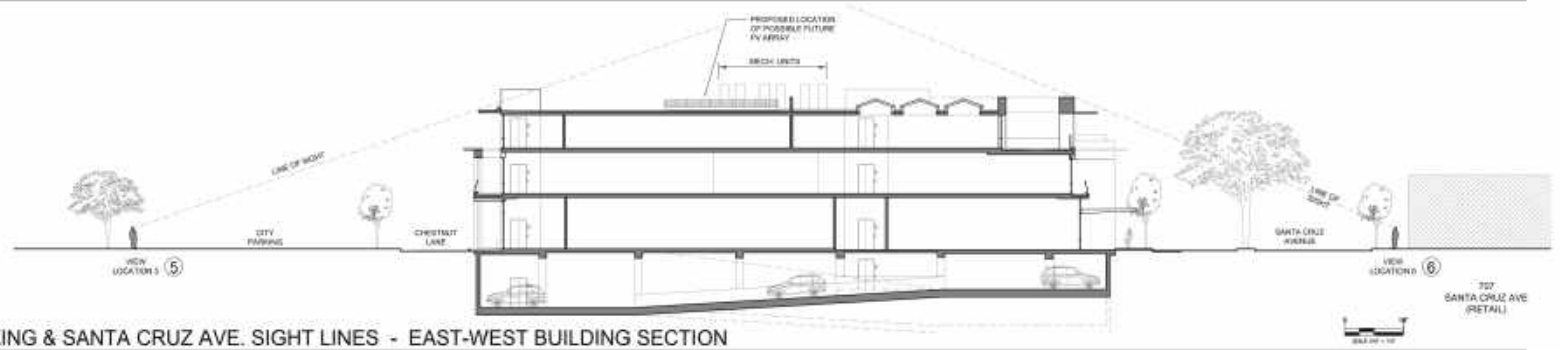
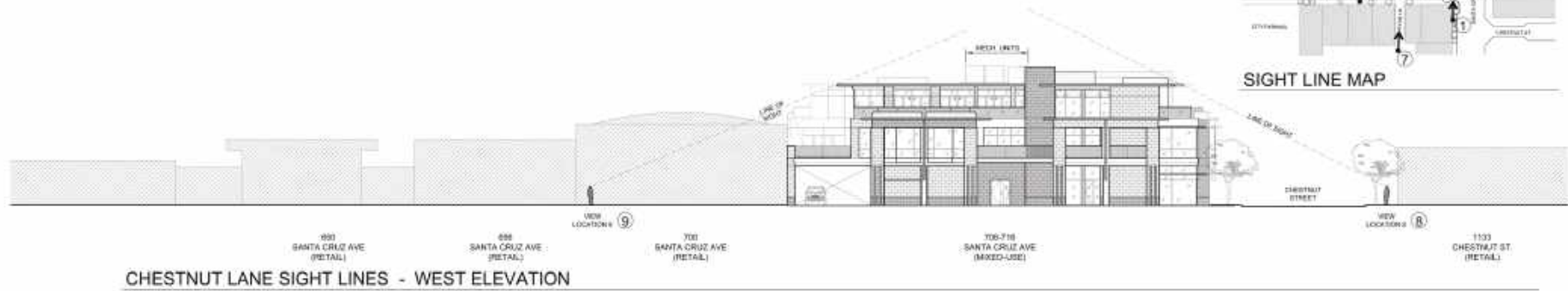


SANTA CRUZ AVENUE SIGHT LINES - EAST ELEVATION



RYAN LANE SIGHT LINES - NORTH-SOUTH BUILDING SECTION





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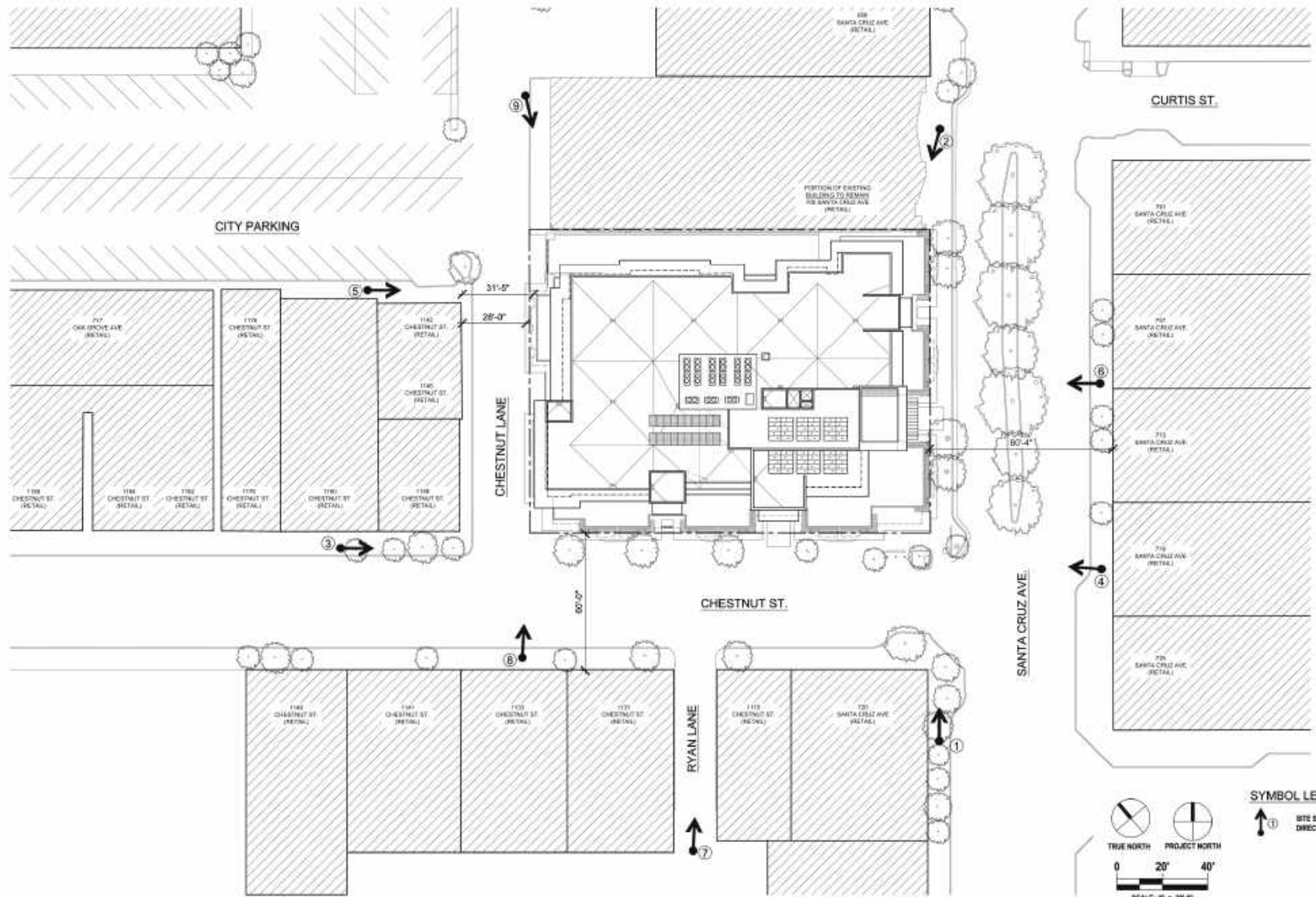
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## SITE LINE STUDY

MP 3.1  
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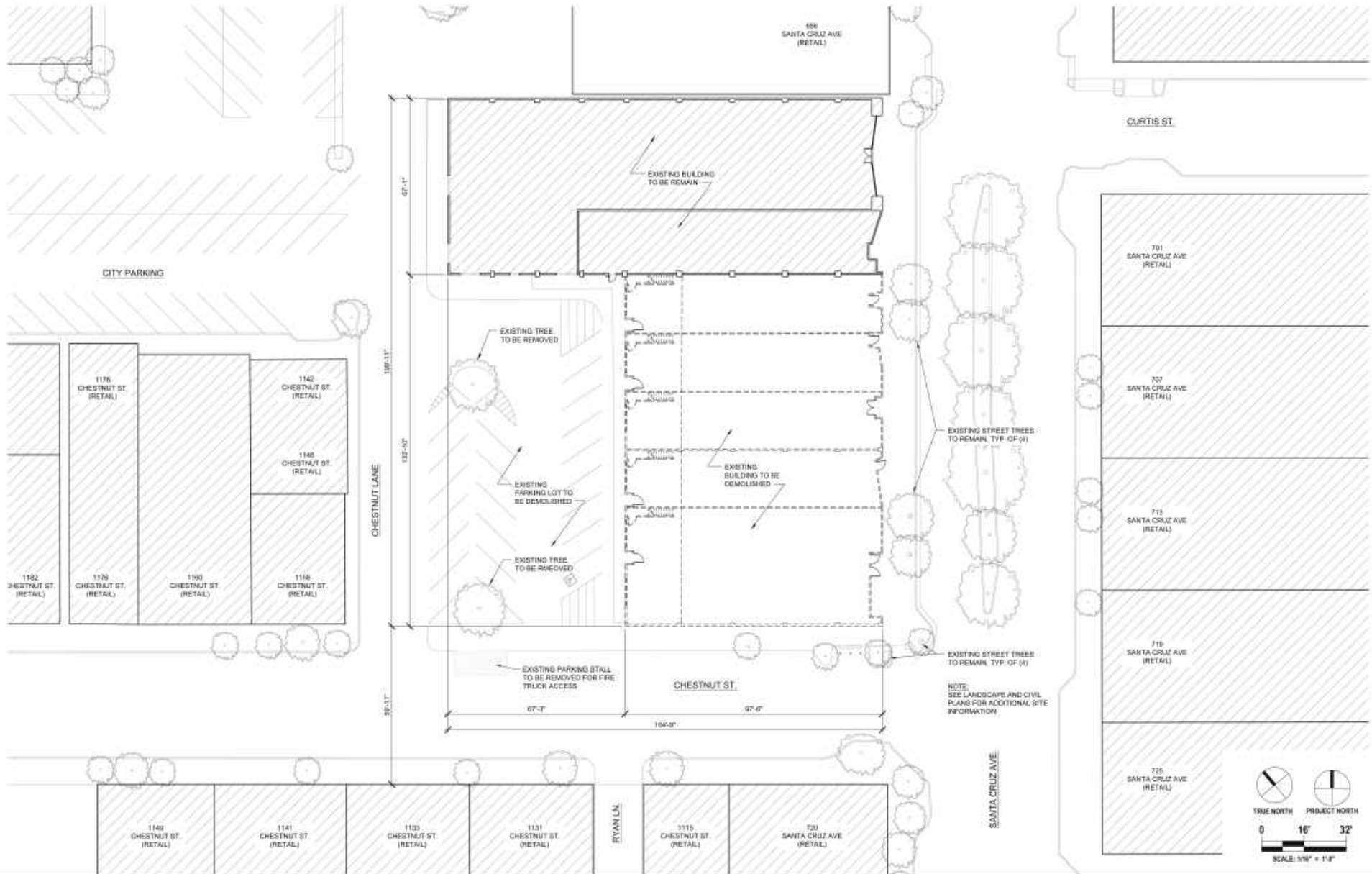
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## AREA PLAN

A 0.1  
November 25, 2019





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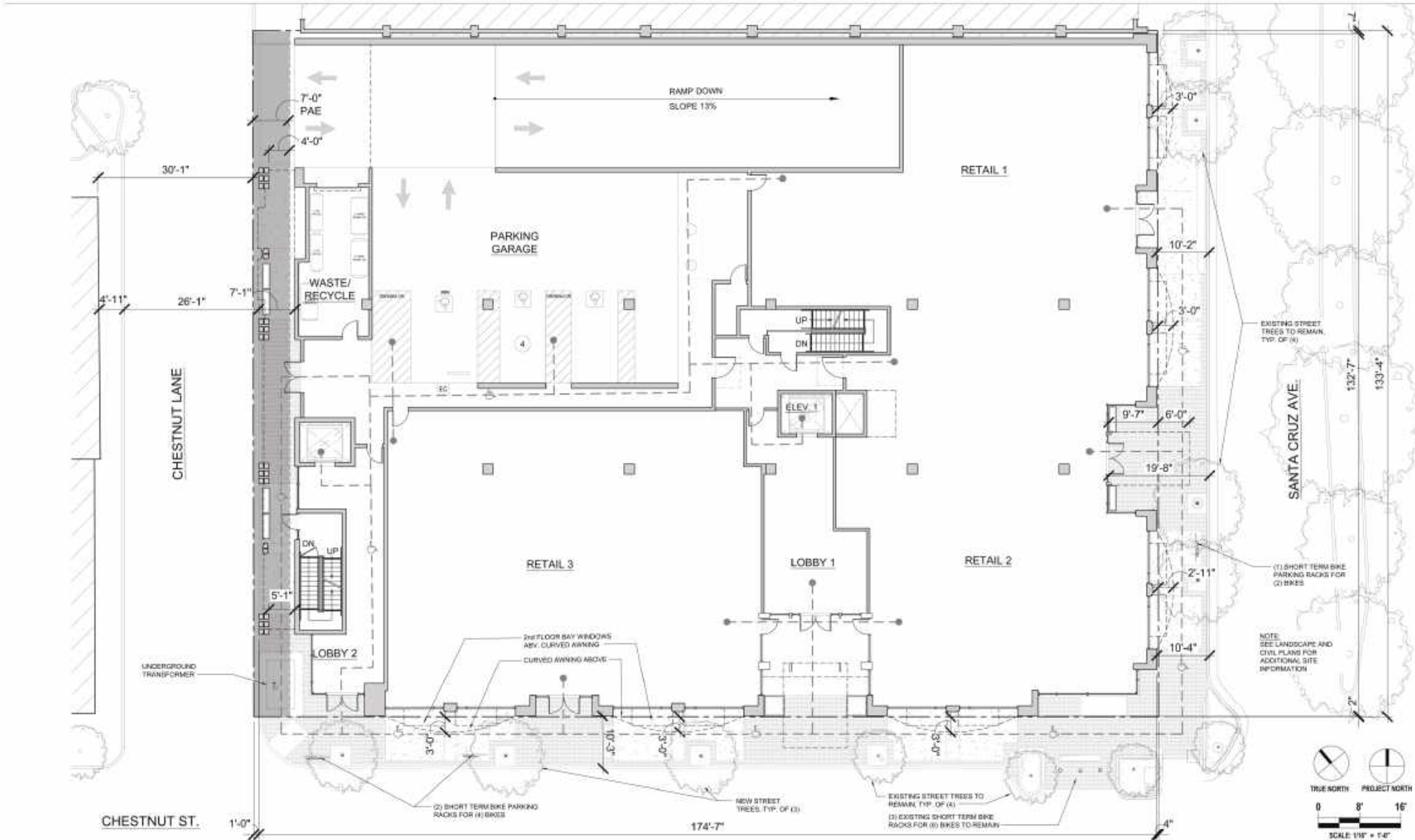
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## EXISTING SITE PLAN

A 0.2  
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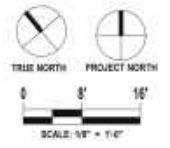
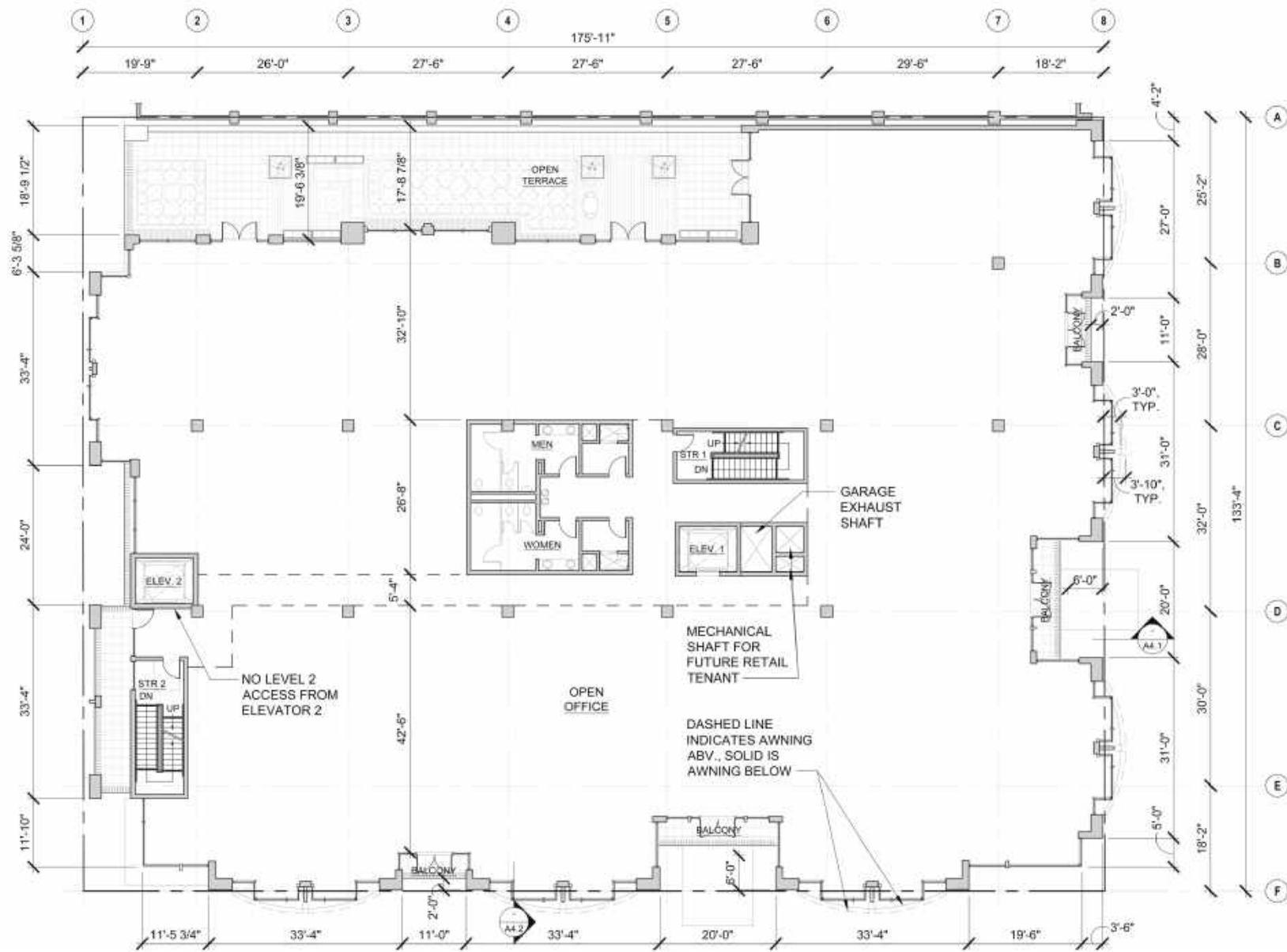
**PROPOSED SITE PLAN**

A 0.3  
November 25, 2019









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## SECOND FLOOR PLAN

A 1.2  
November 25, 2019

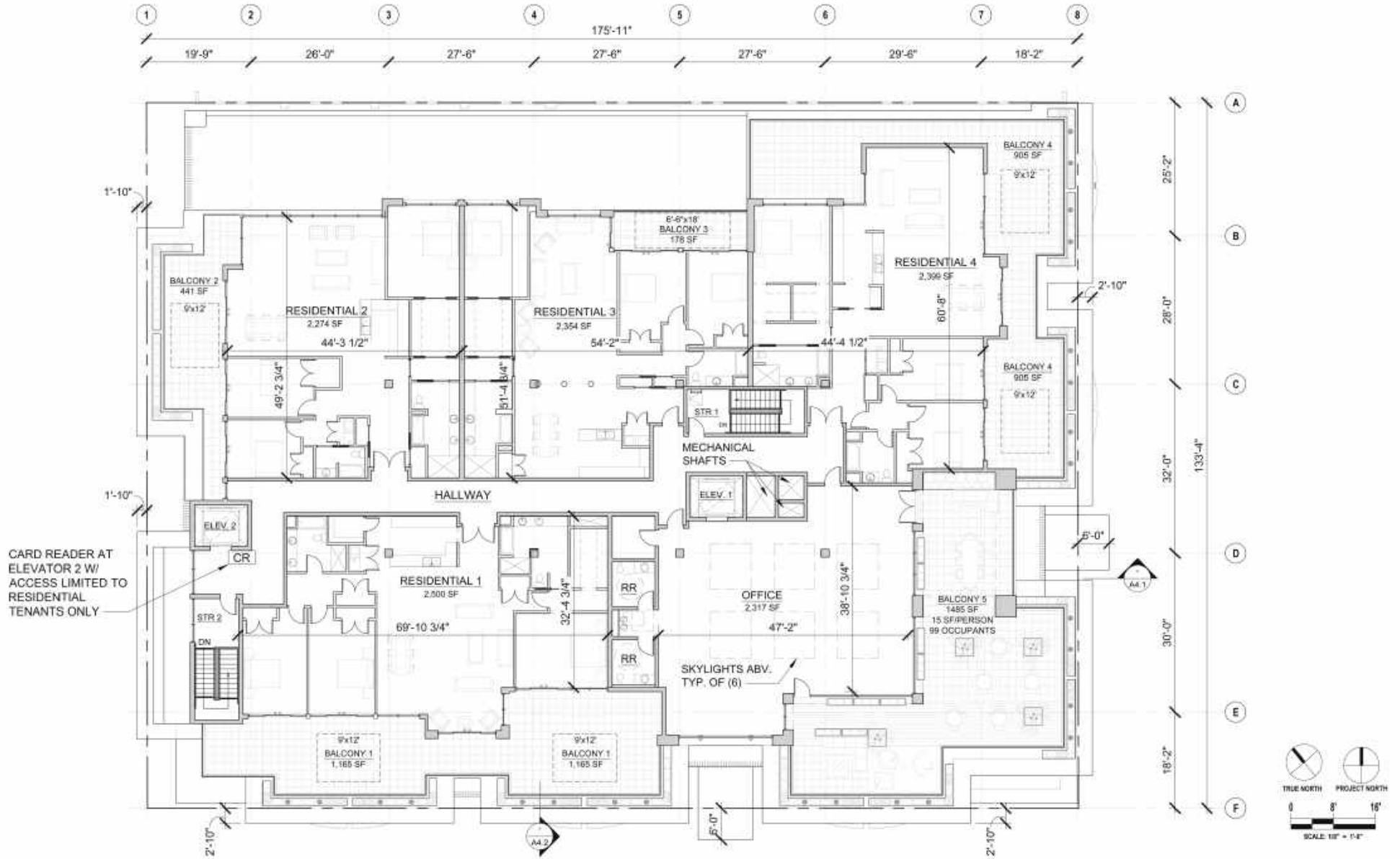


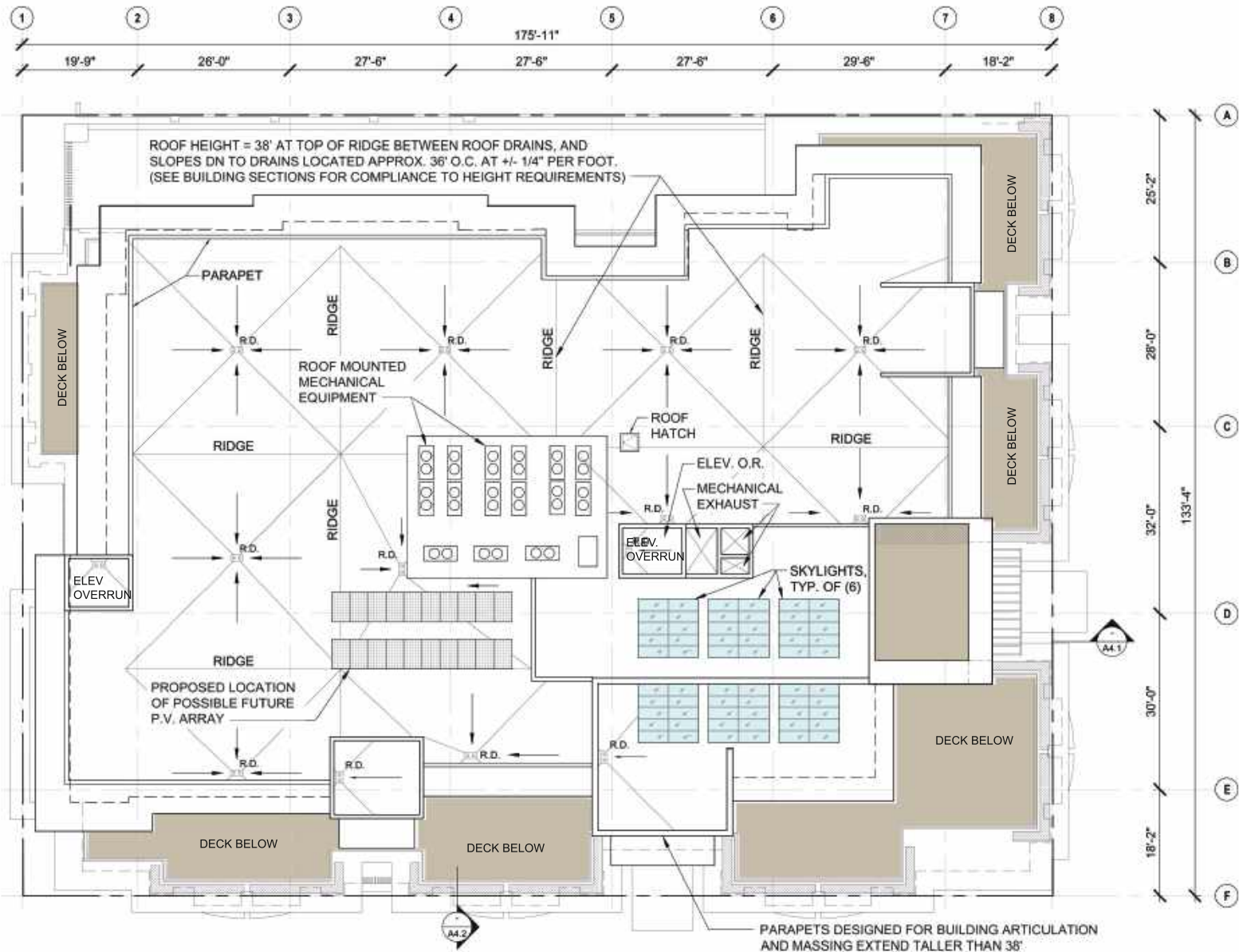
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## THIRD FLOOR PLAN

A 1.3  
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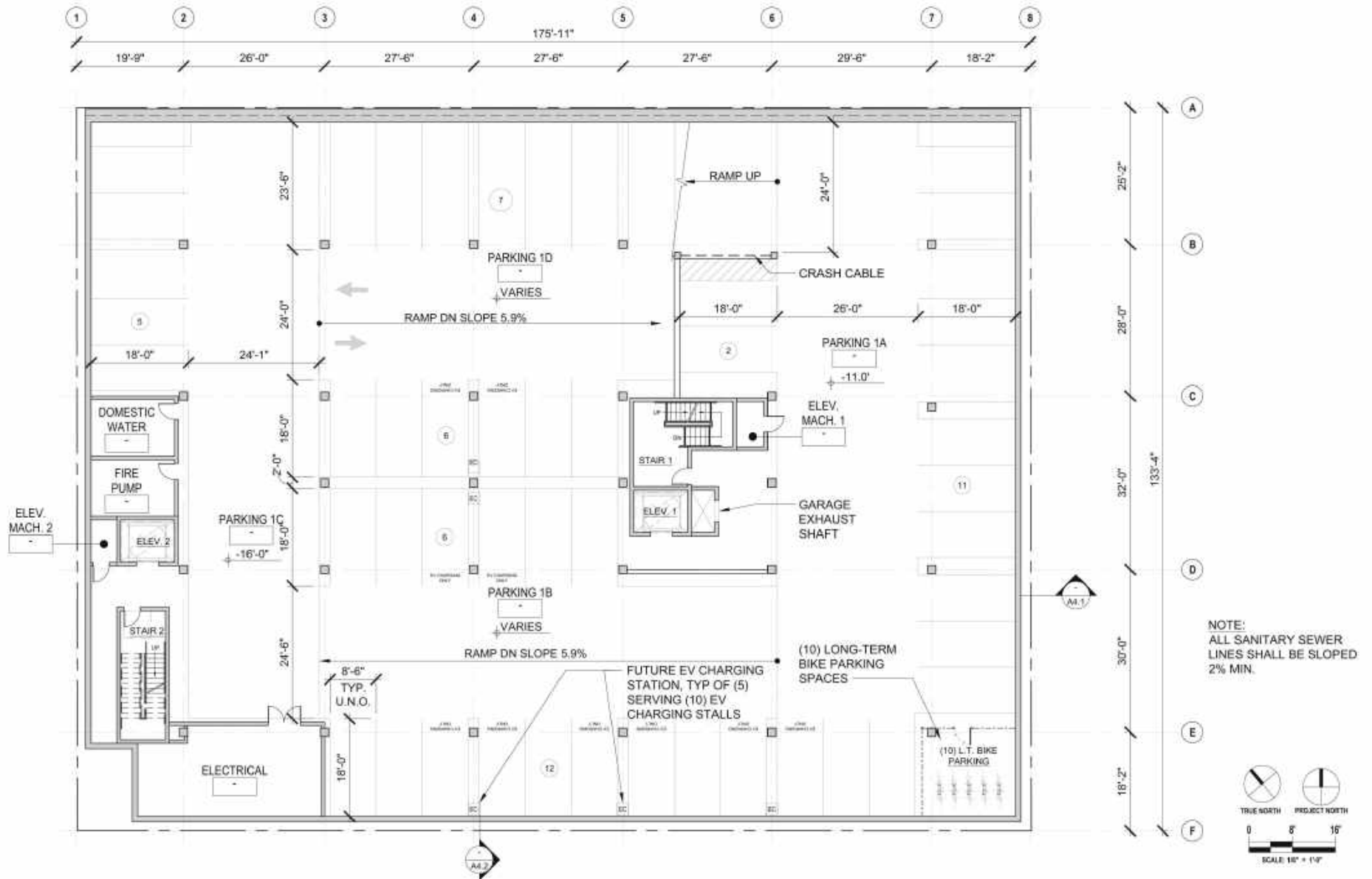
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## ROOF PLAN

A 1.4  
November 25, 2019





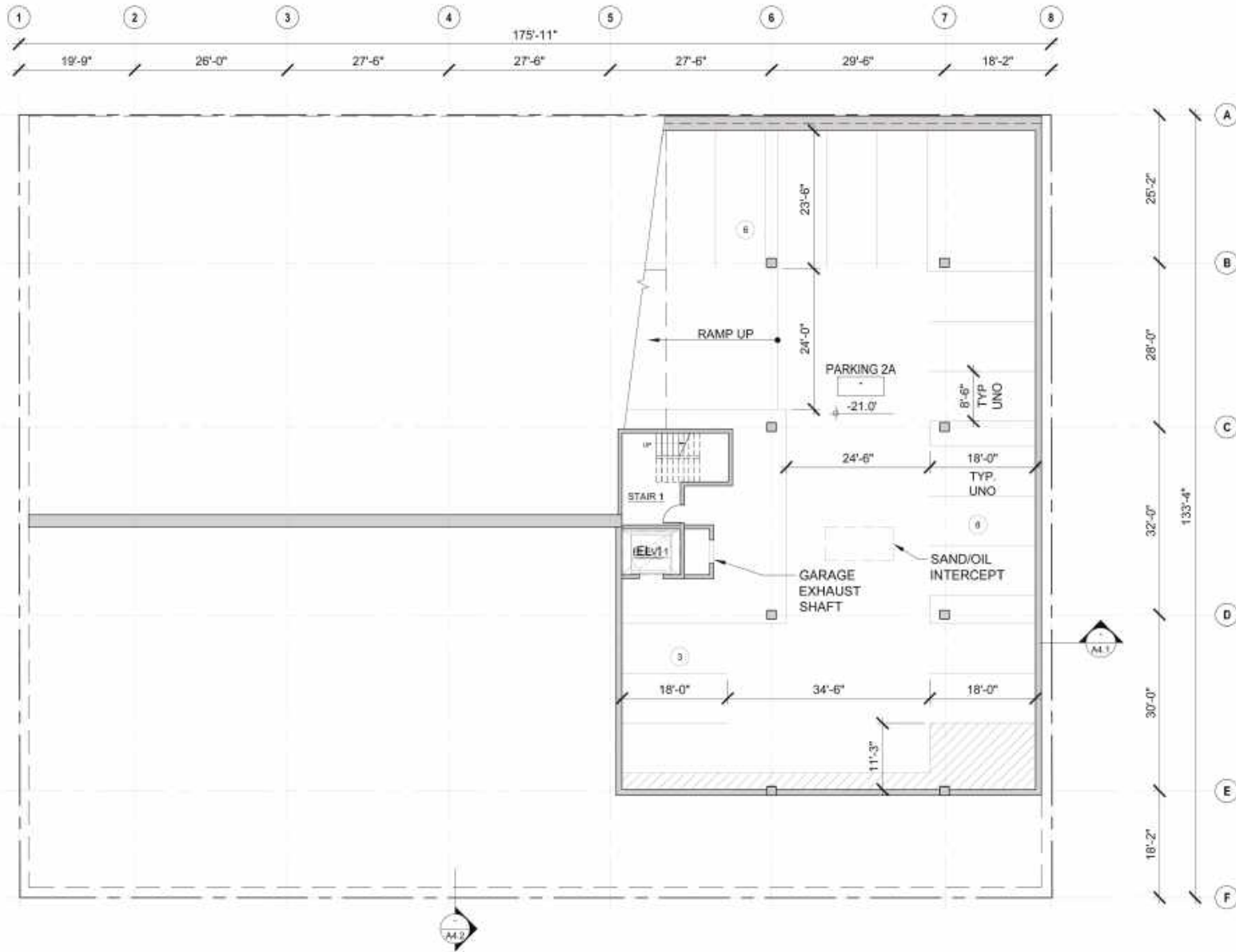
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## PARKING LEVEL P1

A 1.5  
November 25, 2019





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## PARKING LEVEL P2

A 1.6  
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### SANTA CRUZ RENDERING

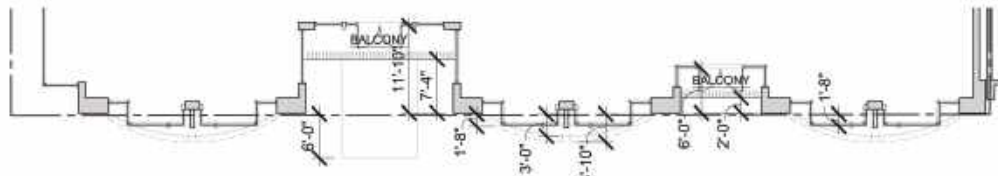
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November 25, 2019



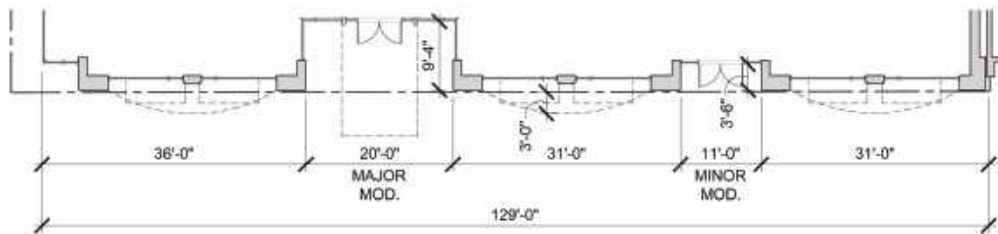


- + 37'-9" (EL: 113.495') ROOF HEIGHT (MPPC E.3.2)
- + 27'-8" (EL: 103.28') THIRD FLOOR
- + 15'-0" (EL: 90.78') SECOND FLOOR
- 0'-3" (EL: 75.54') FIRST FLOOR LEVEL 1A
- 0'-3 3/4" (EL: 75.495') AVERAGE NATURAL GRADE

EAST ELEVATION



SECOND FLOOR



FIRST FLOOR



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## SANTA CRUZ ELEVATION & MODULATION ANALYSIS

A 2.2  
November 25, 2019







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## CHESTNUT STREET RENDERING

A 2.3  
November 25, 2019

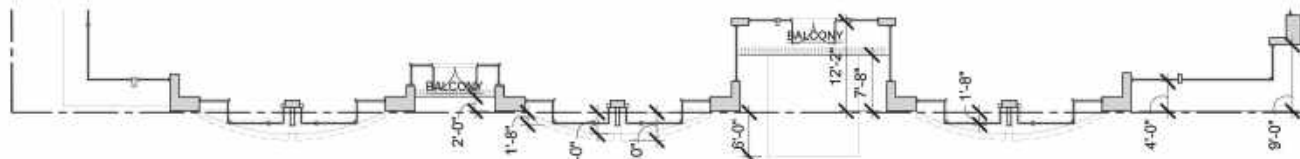




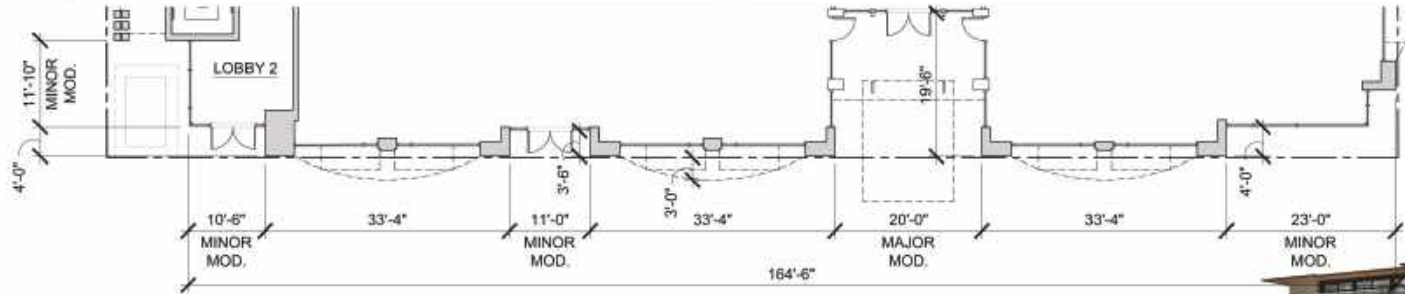


- + 37'-9" (EL: 113.495') ROOF HEIGHT (MPPC E.3.2)
- + 27'-6" (EL: 103.28') THIRD FLOOR
- + 15'-0" (EL: 90.78') SECOND FLOOR
- 0'-0" (EL: 75.78') FIRST FLOOR LEVEL 1B
- 0'-3" (EL: 75.54') FIRST FLOOR LEVEL 1A
- 0'-3 3/4" (EL: 75.495') AVERAGE NATURAL GRADE

SOUTH ELEVATION



SECOND FLOOR



FIRST FLOOR



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## CHESTNUT STREET ELEVATION & MODULATION ANALYSIS

A 2.4  
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## CHESTNUT LANE RENDERING

A 2.5  
November 25, 2019





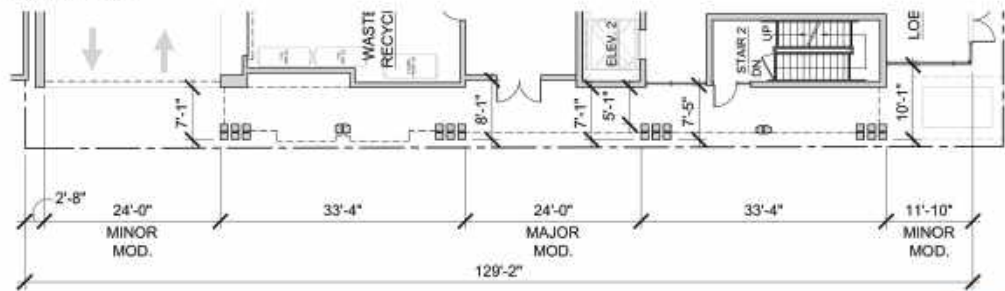


WEST ELEVATION

- + 37'-9" (EL: 113.495')  
ROOF HEIGHT  
(MPPC E.3.2)
- + 27'-6" (EL: 103.28')  
THIRD FLOOR
- + 15'-0" (EL: 90.78')  
SECOND FLOOR
- 0'-0" (EL: 75.78')  
FIRST FLOOR  
LEVEL 1A
- 0'-3 3/4" (EL: 75.495')  
AVERAGE NATURAL  
GRADE



SECOND FLOOR



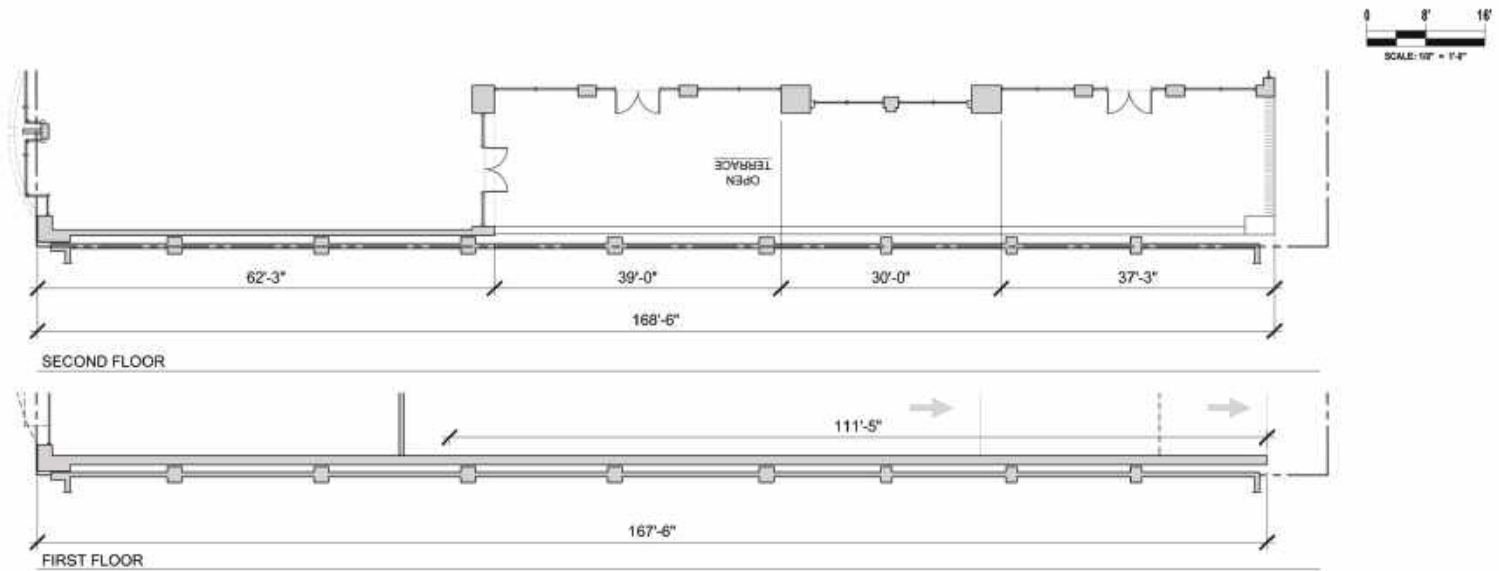
FIRST FLOOR







NORTH ELEVATION



CLEAR GLASS  
GUARDRAIL W/  
STAINLESS  
STEEL TOP RAIL

HIGH-PERFORMANCE  
CLEAR GLASS

LIGHT BEIGE STONE



WARM GRAY STONE

DARK BROWN METAL PANEL ROOF  
OVERHANG / SUNSHADE

STAINED WOOD SOFFIT, TYP. AT 3rd  
FLOOR SOFFITS

DARK GRAY METAL PLANTERS

MEDIUM BEIGE STONE

DARK BROWN METAL GUARDRAIL

DARK BROWN METAL PANEL FASCIA

DARK BROWN MULLIONS & DOORS

MEDIUM BEIGE STONE

EAST ELEVATION

STAINED WOOD  
SOFFIT, TYP. AT 3rd  
FLOOR SOFFITS

LIGHT BEIGE STONE

ROLL-UP GARAGE  
DOOR, DARK BROWN  
METAL TO MATCH  
WINDOW MULLIONS

MEDIUM BEIGE STONE



WARM GRAY STONE

CLEAR GLASS GUARDRAIL W/  
STAINLESS STEEL TOP RAIL

DARK BROWN METAL PANEL  
SUNSHADES & FASCIA

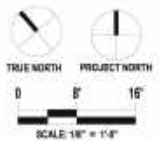
DARK BROWN METAL GUARDRAIL

DARK BROWN METAL PANELS

HIGH-PERFORMANCE CLEAR GLASS

DARK BROWN METAL DOORS

WEST ELEVATION





**SOUTH ELEVATION SUMMARY**

GROSS SURFACE AREA	2,487 SF
OPAQUE SURFACES	843 SF = 34%
TRANSPARENT SURFACES	1,639 SF = 66%

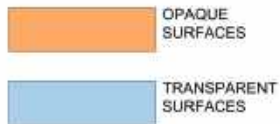
**EAST ELEVATION SUMMARY**

GROSS SURFACE AREA	1,995 SF
OPAQUE SURFACES	767 SF = 38%
TRANSPARENT SURFACES	1,288 SF = 62%

**WEST ELEVATION SUMMARY**

GROSS SURFACE AREA	672 SF
OPAQUE SURFACES	324 SF = 48%
TRANSPARENT SURFACES	348 SF = 52%

**FACADE TREATMENT**







**SOUTH ELEVATION SUMMARY**

PRIMARY FACADE AREA	2,038 SF
PROJECTION AREA	662 SF
PERCENT OF FACADE AREA	32.5%

**EAST ELEVATION SUMMARY**

PRIMARY FACADE AREA	1,919 SF
PROJECTION AREA	646 SF
PERCENT OF FACADE AREA	33.7%

**FACADE PROJECTIONS**

- PRIMARY BUILDING FACADE
- FACADE PROJECTION





LIGHT BEIGE STONE

WARM GRAY STONE

MEDIUM BEIGNE STONE



DARK BROWN METAL

HIGH-PERFORMANCE CLEAR GLASS

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MATERIALS AND COLOR BOARD



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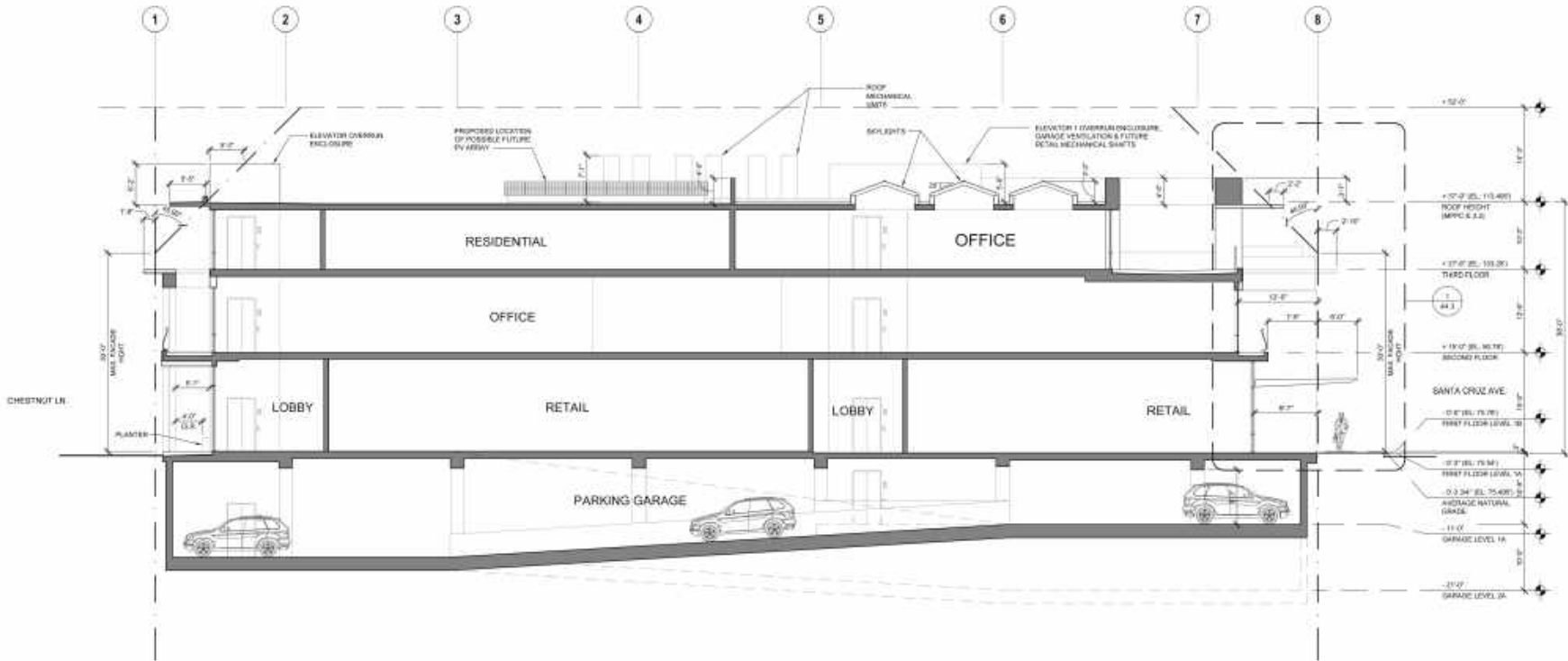
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MATERIAL & COLOR BOARD

A 3.4  
November 25, 2019







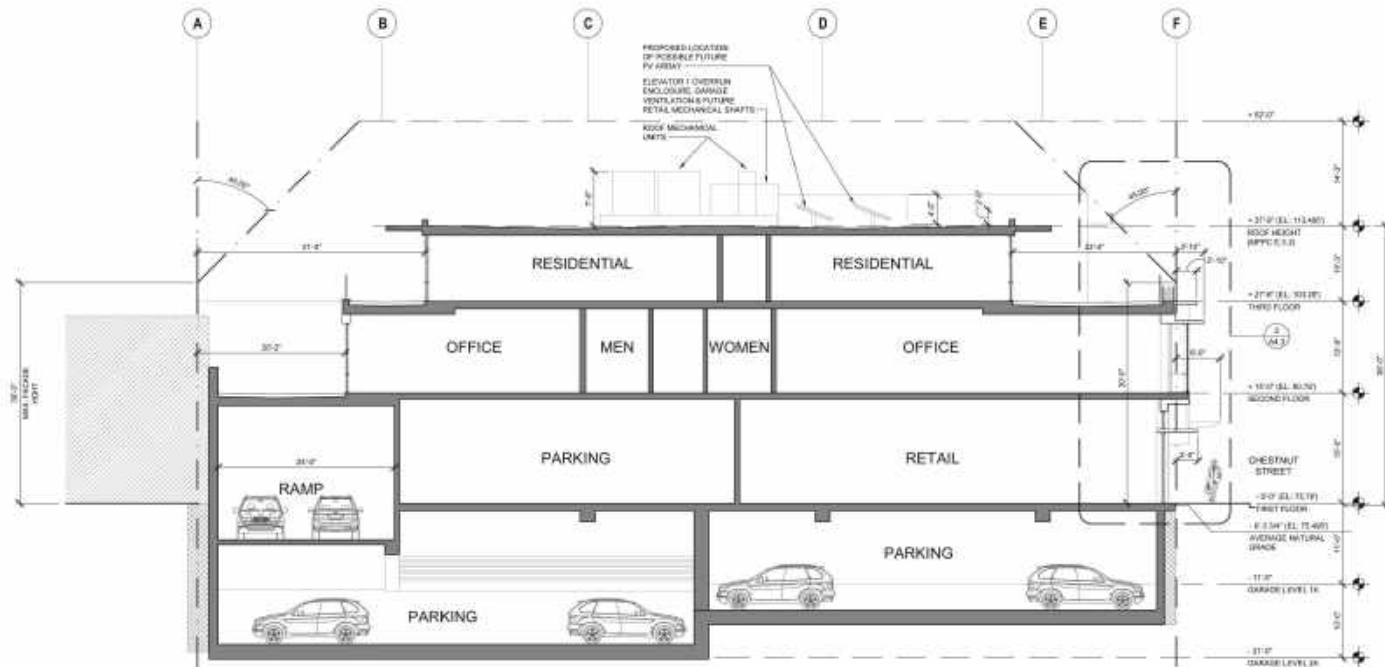
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## BUILDING SECTIONS & HEIGHT ANALYSIS

A 4.1  
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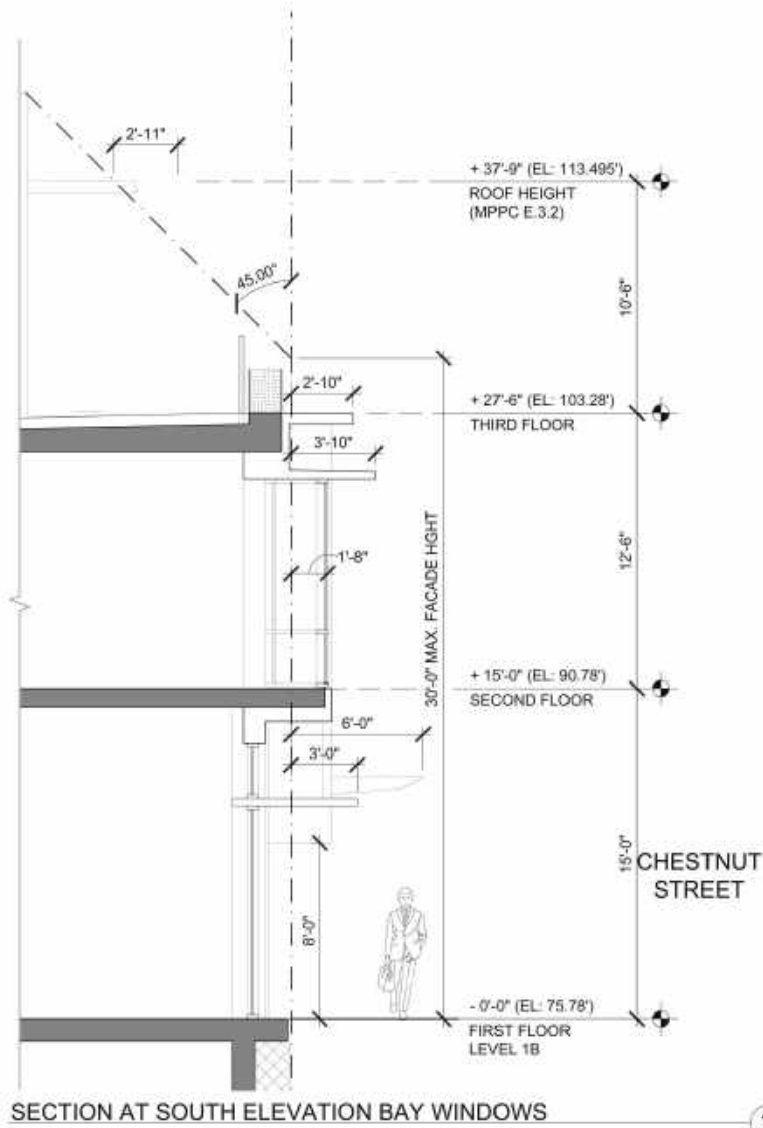
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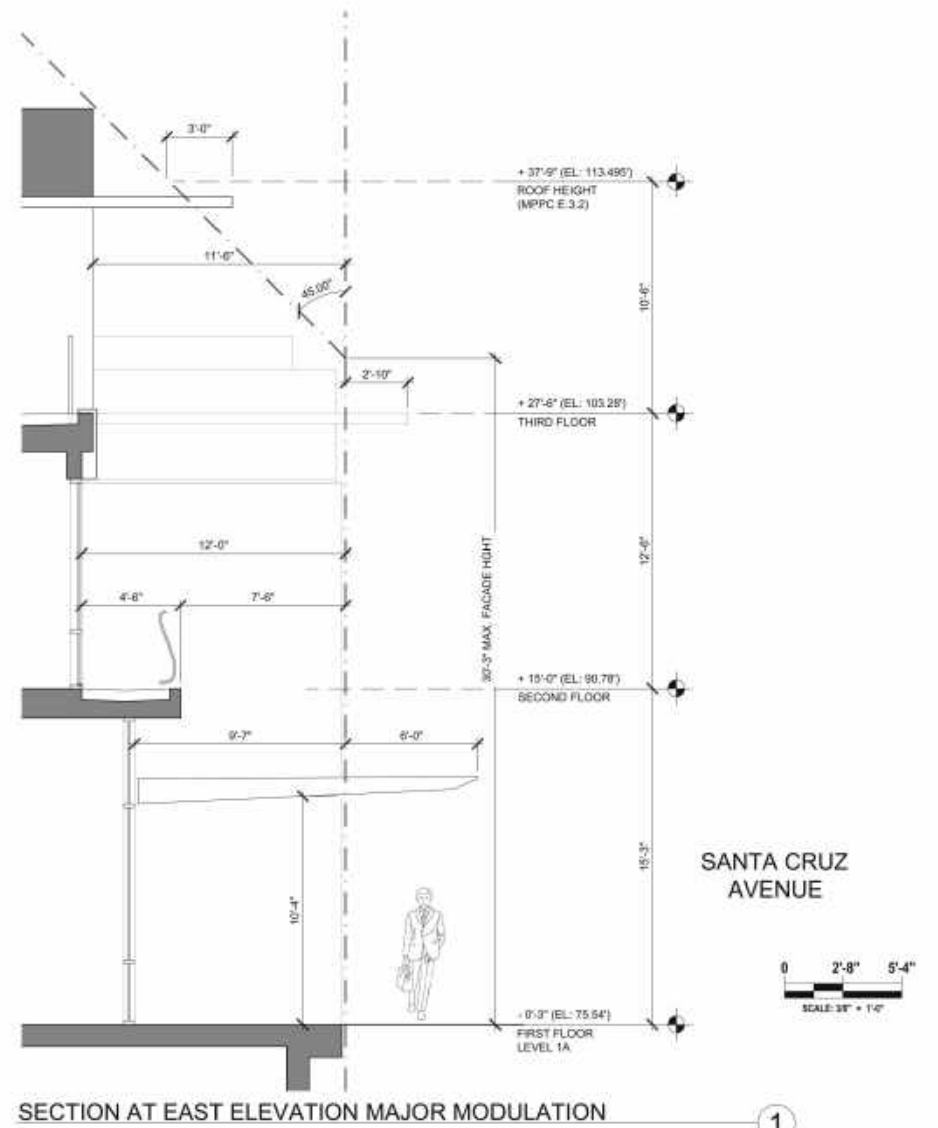
## BUILDING SECTIONS & HEIGHT ANALYSIS

A 4.2  
November 25, 2019

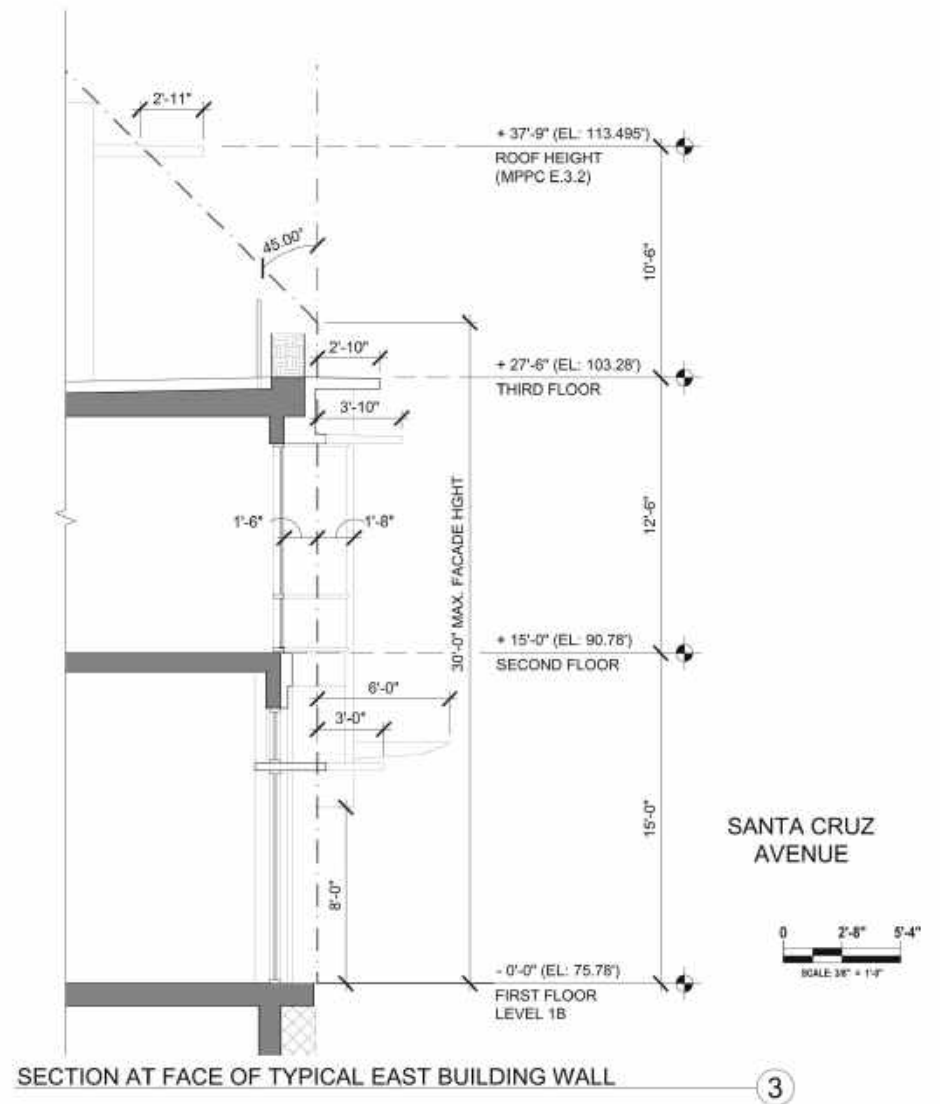


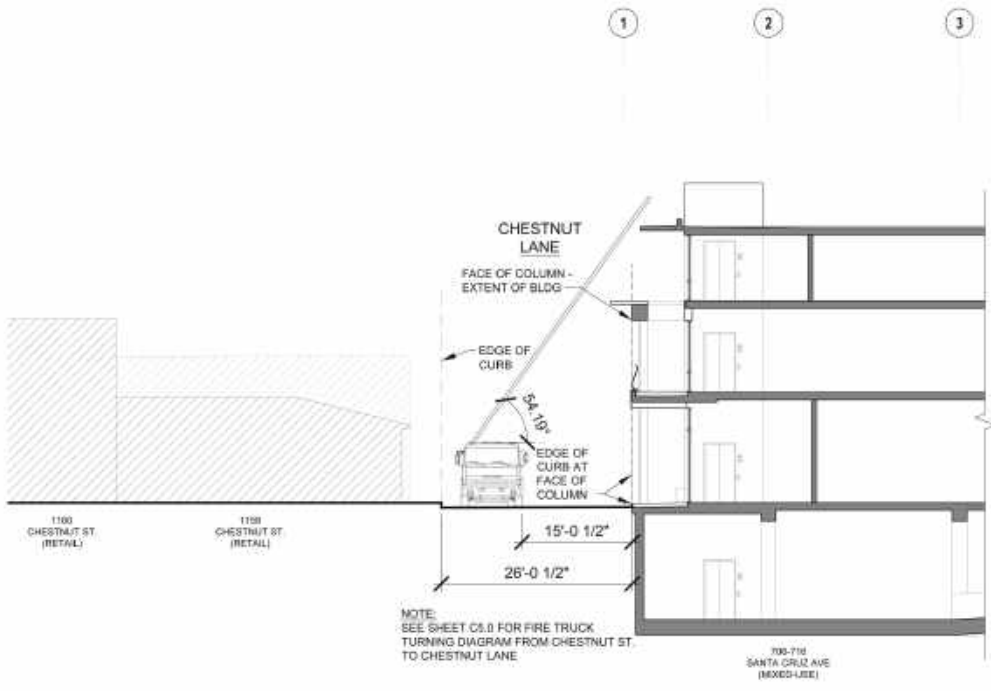


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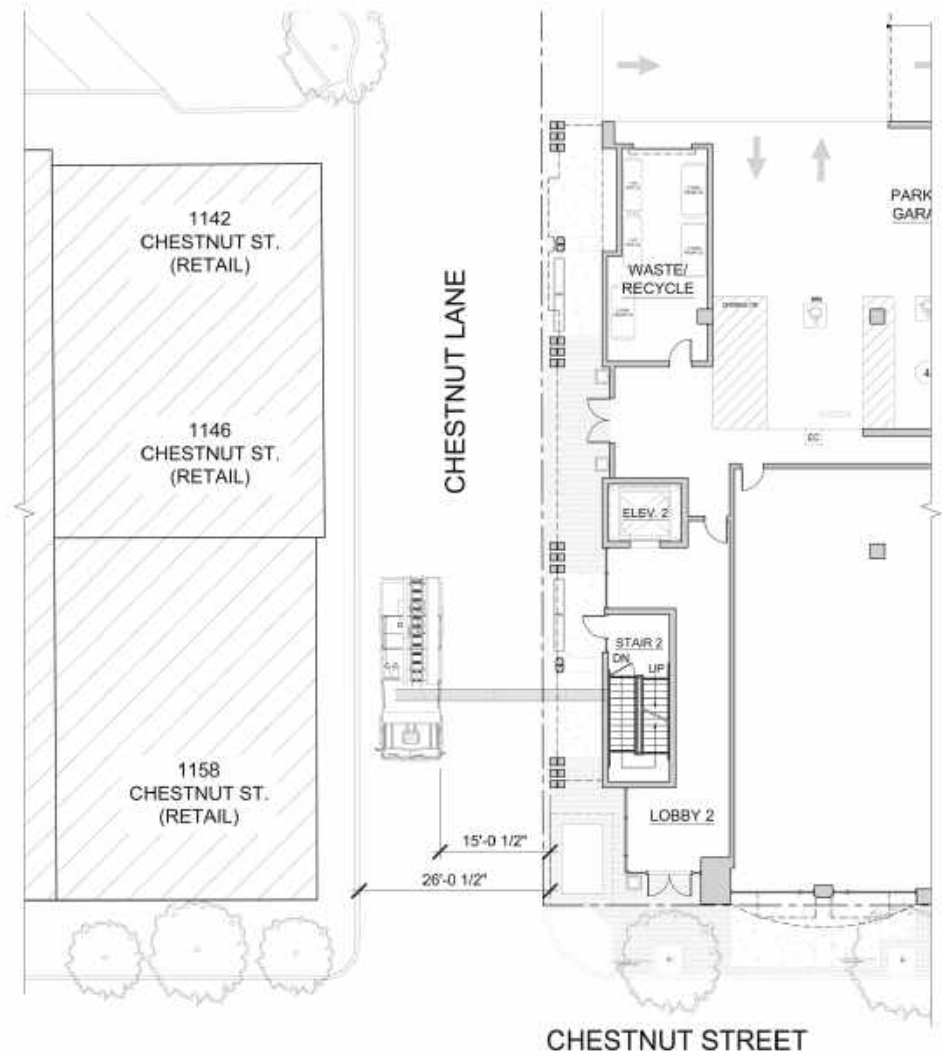


1





CHESTNUT LANE FIRE TRUCK ACCESS DIAGRAM - PARTIAL SECTION



CHESTNUT LANE FIRE TRUCK ACCESS DIAGRAM - PARTIAL SITE PLAN



## ELECTRICAL SYMBOL LIST

NOTE: This is a summary symbol list and not all items listed may be used.

### Abbreviations

A	AMPERE AMPER
APP	ABOVE FINISHED FLOOR
AU	AUTHORITY HAVING JURISDICTION
AV	AVAILABLE INTERRUPTING CAPACITY
AW	AWAY FROM WALL SURFACE
BAC	BUILDING AUTOMATION SYSTEM
C	CONDUIT, CLOSURE, CONTROL
DA	DABLE
CAF	CASEWORK
CFD	CONTRACTOR FURNISHED CONTRACTOR INSTALLED
CFI	CONTRACTOR FURNISHED OWNER INSTALLED
CLB	CLAREN
COORD	COORDINATE
CU	COPPER
DM	DIMMER
DMN	DIMENSION
DN	DOWN
DRW	DRAWING
EA	EACH
EAT	ELECTRICAL METALLIC TUBING
EM	ELECTRICAL NON-METALLIC TUBING
FA	FINE ALUMIN
FI	FIRE ALARM
FL	FLOOR FINISH
GRND	GROUND
GRD	GROUND FAULT CURRENT INTERRUPTER
GRN	GROUND FAULT INTERRUPTER
GRN	GROUND FAULT PROTECTION
HR	HARDWARE
HT	HEIGHT
IE	IDENTIFICATION
IEE	INSTITUTE OF ELECTRICAL AND ELECTRONICS ENGINEERS
IN	INCH INCHES
IN	INCHES
IN	INCHES
KAR	KILOVAULT AMPERES
KB	KILOVAULT AMPERES
KB	KILOVAULT AMPERES
LED	LIGHT EMITTING DIODE
LV	LOW VOLTAGE
M	MOTOR
MEM	MEMBER CIRCUIT AMPERES
MGR	MISCELLANEOUS
MOOP	MOVABLE OVERCURRENT PROTECTION
MR. NTE	MOVABLE REBUILT
NR	NOT APPLICABLE
NEC	NATIONAL ELECTRICAL CODE
NECA	NATIONAL ELECTRICAL CONTRACTORS ASSOCIATION
NEC	NATIONAL ELECTRICAL SAFETY CODE
NIS	NOT TO SCALE
NO	NO CENTER
OC	OWNER FURNISHED CONTRACTOR INSTALLED
OFD	OWNER FURNISHED OWNER INSTALLED
PH	PHONE
PVC	POLYVINYL CHLORIDE
POWER	POWER

RFI	REQUEST FOR INFORMATION
RM	ROOM
S	SWITCH
SH	SHIELD
STD	STANDARD
TBD	TO BE DETERMINED
TYP	TYPICAL
UN	UNDERWATER LABORATORIES
UNH	UNLESS OTHERWISE NOTED
V	VOLTS VOLTAGE
W	WITH
W/O	WITHOUT
WP	WEATHERPROOF
WTR	WATERPROOF

### Connections / Equipment

	HEAVY DUTY PLUG DISCONNECT SWITCH
	JUNCTION BOX
	WALL MOUNTED JUNCTION BOX
	MOTOR CONNECTION
	NON-FUSED DISCONNECT SWITCH
	TRANSFORMER

### General

	DETAIL NUMBER AND SHEET LOCATION
	EQUIPMENT IDENTIFICATION
	NOTES

### Lighting

	EXIT SIGN CEILING MOUNTED, ARROWS INDICATE DIRECTION OF EXIT
	EXIT SIGN WALL MOUNTED, ARROWS INDICATE DIRECTION OF EXIT
	RECESSED 1 X 4 LUMINAIRE
	RECESSED 2 X 4 LUMINAIRE
	RECESSED 2 X 2 LUMINAIRE
	RECESSED ADJUSTABLE OR WALL WASH LUMINAIRE
	RECESSED LUMINAIRE
	SURFACE OR PENDANT MOUNTED 1 X 4 LUMINAIRE
	SURFACE OR PENDANT MOUNTED 2 X 4 LUMINAIRE
	WALL MOUNTED 4' WIDE LUMINAIRE
	WALL MOUNTED LUMINAIRE

### Miscellaneous

	BRANCH CIRCUIT BREAKER, ARROWS INDICATE DIRECTION TO PANEL, WITH DIRECTION INDICATED, NONE FOR #1 AND #2 AND NUMBERED AFTER NOTES
	PHASE
	POLYVINYL CHLORIDE
	POWER

	BRANCH PANEL
	FLUSH WALL MOUNTED BRANCH PANEL
	UNFINISHED PANEL
	MAIN DISTRIBUTION PANEL / SUB DISTRIBUTION PANEL

### Recessed

	CONDUIT CONCEALED IN WALL OR CEILING SPACE
	CONDUIT MOUNTED BELOW FLOOR / GRATE
	CONDUIT EASED DOWN
	CONDUIT FLASHED UP
	CONDUIT MOUNTED ON WALL OR CEILING
	CONDUIT FLASHED OUT WITH END CAP OR INSULATED PLASTIC BUSHING

### Switches and Receptacles

	DUPLEX RECEPTACLE, INCLUDE LETTERS INDICATE MULTIPLE OPTIONS: A = CLOCK RECEPTACLE B = CLOCK RECEPTACLE C = FLUSH CEILING MOUNTED D = AMBERANCE E = RECEPTACLE PREDEFINED BY MANUFACTURER F = GROUND FAULT CURRENT INTERRUPTER G = HOSPITAL GRADE H = CHILD PROOF MOUNT COVER I = ISOLATED GROUND J = RECEPTACLE MOUNTED WITH COVER CAPS, VERIFY RECEPTACLE LENGTH K = HALF SWITCHED BY OCCUPANCY SENSOR RELAY L = FULLY SWITCHED BY OCCUPANCY SENSOR RELAY M = WIPER MOUNTED N = TRANSFER RESISTANT SWITCHING RECEPTACLE O = TRANSFER RESISTANT SWITCHING RECEPTACLE WITH WEATHER RESISTANT RECEPTACLE
	DUPLEX RECEPTACLE, FLUSH MOUNT
	DOUBLE DUPLEX RECEPTACLE, INCLUDE LETTERS INDICATE MULTIPLE OPTIONS FOR DUPLEX RECEPTACLE FOR DUPLEX
	EQUIPMENT ELECTRICAL CONNECTION
	SPECIAL PURPOSE RECEPTACLE, LETTER CODE INDICATES RECEPTACLE CONFIGURATION: L = LOCKING M = MOUNTING N = NON-LOCKING P = PULL-OUT R = RECEPTACLE WITH LOCKING COVER S = SPECIAL PURPOSE RECEPTACLE WITH LOCKING COVER T = TYPICAL U = ULTRASONIC V = VOLTAGE W = WIRELESS X = X-RAY Y = YIELDING Z = ZENER
	CEILING MOUNTED OCCUPANCY SENSOR P = PASSIVE INFRARED S = SENSING TECHNOLOGY U = ULTRASONIC, 300-3000 RANGE V = ULTRASONIC, WALL MOUNTED W = (LOW) RANGE X = (HIGH) RANGE
	WALL MOUNTED OCCUPANCY SENSOR P = PASSIVE INFRARED S = SENSING TECHNOLOGY U = ULTRASONIC, 300-3000 RANGE V = ULTRASONIC, WALL MOUNTED W = (LOW) RANGE X = (HIGH) RANGE
	WALL MOUNTED OCCUPANCY SENSOR WITH INTEGRAL 'OFF' SWITCH S = SENSING TECHNOLOGY U = ULTRASONIC, 300-3000 RANGE V = ULTRASONIC, WALL MOUNTED W = (LOW) RANGE X = (HIGH) RANGE
	SINGLE POLE SWITCH 1 = SINGLE POLE SWITCH 2 = DOUBLE POLE SWITCH 3 = THREE WAY SWITCH 4 = FOUR WAY SWITCH Y = 3-WAY / 4-WAY SWITCH, LUMINAIRE CONTROL DESIGNATION Z = DIMMER A = DIMMER CONTROL B = DIMMER CONTROL C = DIMMER CONTROL D = DIMMER CONTROL E = DIMMER CONTROL F = DIMMER CONTROL G = DIMMER CONTROL H = DIMMER CONTROL I = DIMMER CONTROL J = DIMMER CONTROL K = DIMMER CONTROL L = DIMMER CONTROL M = DIMMER CONTROL N = DIMMER CONTROL O = DIMMER CONTROL P = DIMMER CONTROL Q = DIMMER CONTROL R = DIMMER CONTROL S = DIMMER CONTROL T = DIMMER CONTROL U = DIMMER CONTROL V = DIMMER CONTROL W = DIMMER CONTROL X = DIMMER CONTROL Y = DIMMER CONTROL Z = DIMMER CONTROL

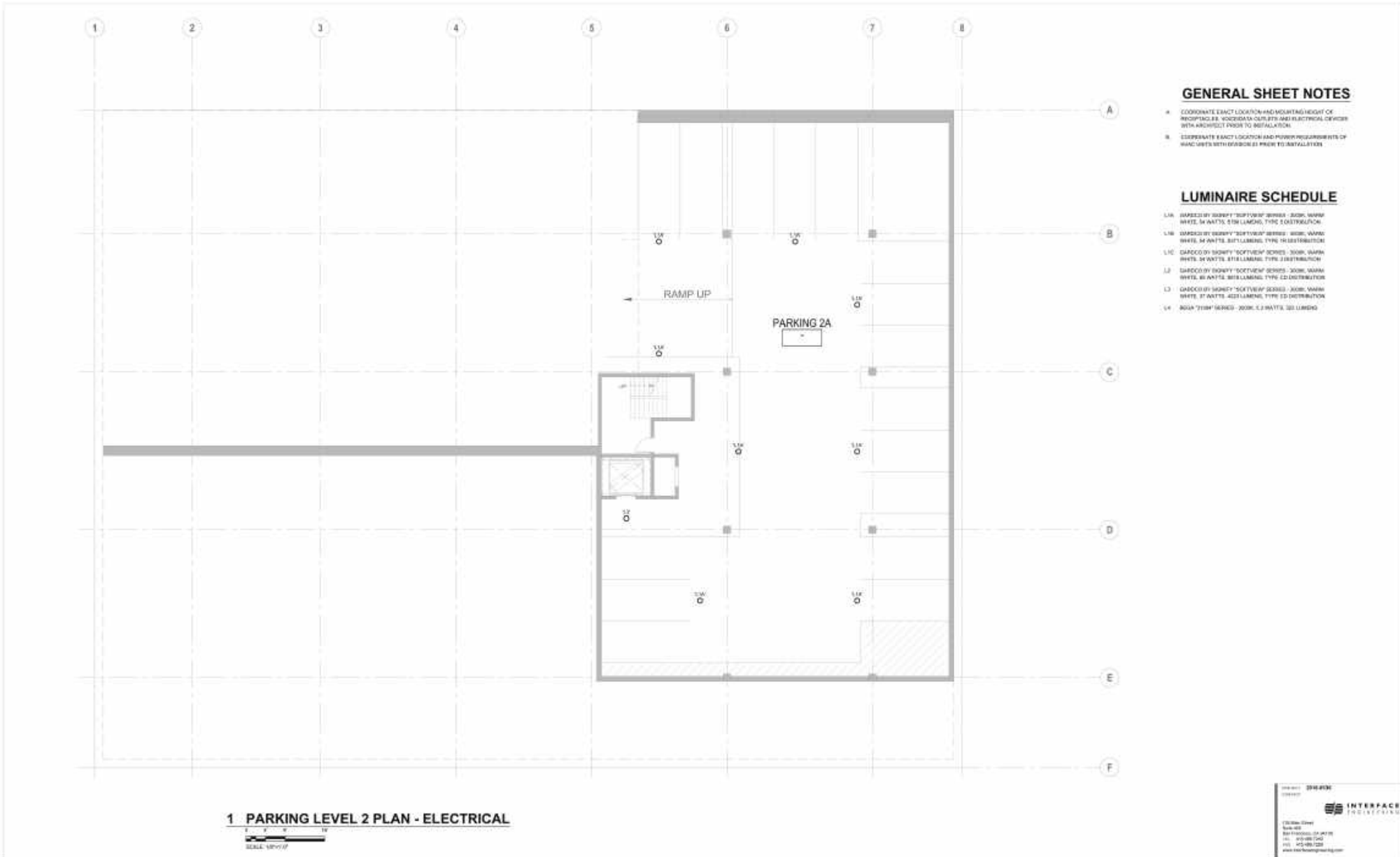
## GENERAL ELECTRICAL NOTES

1. DO NOT COMMENCE INSTALLATION OF ELECTRICAL SYSTEMS AND EQUIPMENT WITHOUT RELATED SHOP DRAWING APPROVALS.
2. COORDINATE WITH OWNER SO THAT WORK CAN BE SCHEDULED NOT TO INTERRUPT OPERATIONS, NORMAL ACTIVITIES, BUSINESS ACCESS, ACCESS TO ELEVATOR SHAFTS. THE OWNER SHALL COORDINATE TO THE BEST OF THEIR ABILITY TO ADJUST IN A COORDINATED SCHEDULE, BUT SHALL REMAIN THE FINAL AUTHORITY AS TO THE ORDER OF WORK.
3. COORDINATE THE EXACT LOCATION OF ALL UTILITIES AND EQUIPMENT PRIOR TO COMMENCEMENT OF WORK. COMPENSATE THE OWNER FOR DAMAGES CAUSED BY THE FAILURE TO LOCATE AND PRESERVE UTILITIES. REPLACE DAMAGED ITEMS WITH NEW MATERIAL, TO MATCH ORIGINAL.
4. LOCATE ALL CONDUIT LOCATIONS IN CONCRETE WALLS OR PARTITION WALLS. CONDUIT SPACING MAY BE ADJUSTED IN PLACE. REMOVE CONDUITS FROM WALLS AND CONDUITS WITH CORRESPONDING SYSTEM AND TERMINATION POINT. CUT AND CAP ABANDONED CONDUIT. DO NOT EXTEND ABOVE FINISHED FLOOR.
5. REMOVE ABANDONED WIRES TO LEAVE WITH CLEAR.
6. PROVIDE GROUND COVER PLATE FOR ABANDONED FLUSH RECEPTACLE.
7. MAINTAIN ACCESS TO EXISTING ELECTRICAL INFRASTRUCTURE WHICH REMAINS ACTIVE. VERIFY METALLIZATION OR PROVIDE ACCESS PANELS AS APPROPRIATE.
8. OBTAIN REMOVED UTILITIES, WIRING DEVICES, PANELS, BATTERIES AND EQUIPMENT TO THE OWNER. IF OWNER CHOOSES TO RETURN THESE ITEMS, RETURN SUCH ITEMS TO OWNER, CAREFULLY REPAIRED AND DISPOSE OF ITEMS REJECTED BY OWNER FROM PROJECT SITE AND IN A LEGAL MANNER.
9. PROVIDE SCHEDULE AND MAINTENANCE FOR ELECTRICAL EQUIPMENT IN BATTED WALLS, SLABS AND CEILING MOUNT DEVICES AND EQUIPMENT IN ACCORDANCE WITH ESTABLISHED CODES AND SPECIFICATIONS.
10. REFER TO SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
11. DIMENSIONS AND SPECIFICATIONS EQUIPMENT EACH OTHER REQUIREMENT BY EITHER WORKERS REQUIREMENT BY BOTH.
12. CONNECT EQUIPMENT AND DEVICES FURNISHED UNDER OTHER DIVISIONS OF THE CONTRACT BY OWNER OR BY OTHER CONTRACTORS.
13. UNLESS OTHERWISE NOTED, PROVIDE CONCEALED AND FLUSH MOUNTED INSTALLATION OF DEVICES AND EQUIPMENT IN WALLS.
14. PROVIDE SEPARATE EQUIPMENT (GROUNDING CONDUCTOR IN 30 VOLT, 150 VOLT) WIRE CABLES.
15. FOR 120 VOLT, 15 AMP CIRCUITS, WIRE CIRCUIT BREAKER FROM PANELS NEAR TO FURTHEST DEVELOPMENT OF THE CIRCUIT. PROVIDE 40 AMP CIRCUIT BREAKER.
16. RUN ELECTRICAL CONDUIT CONCEALED AND PARALLEL TO ALL LONG LINES. VERIFY WITH ARCHITECT.
17. RECEPTACLE OUTLETS SHALL BE MARKED WITH SEE SECTION 210.7.
18. LIGHTS, SWITCHES AND CONTROL MOUNTINGS SHALL COMPLY WITH CBC SECTION 410.
19. PROVIDE ALL ELECTRICAL EQUIPMENT TO RESIST A MINIMUM FORCE THAT MAY BE APPLIED. COMPLY WITH ILLINOIS REQUIREMENTS.
20. INSTALL COMPLETE SYSTEM OF CONDUCTORS IN WIRING SYSTEM THROUGHOUT BUILDING FOR FEEDERS, BRANCH CIRCUITS, ETC.
21. PROVIDE UNBATTERED HOT CONDUCTORS TO EMERGENCY WALK DOWNWAY OR EMERGENCY LUMINAIRES TO PREVENT SHOCK HAZARD TO BATTERY OPERATION WHEN LUMINAIRES ARE SWITCHED TO THE EMERGENCY POSITION.
22. INSTALLATION OF UTILITY TRANSFORMER, UTILITY SERVICE CONDUITS, WALLS, GROUNDS, ETC. SHALL BE VERIFIED AND COORDINATED WITH UTILITY COMPANY PRIOR TO INSTALLATION. ALL WORK SHALL CONFORM WITH ALL UTILITY COMPANY REGULATIONS, REGULATIONS AND STANDARDS. THE PROVIDED UTILITY COMPANY TRAILER-CRANE LOCATION, SERVICE, FEEDER ROUTING, WALK LOCATION AND SIZE ARE SUBJECT TO UTILITY COMPANY ENGINEERING REVIEW AND APPROVAL. AT THE TIME OF THE ISSUANCE OF THESE DOCUMENTS, THIS ENGINEERING HAS NOT BEEN OBTAINED. CONTRACTOR SHALL COORDINATE AND VERIFY ALL THE NECESSARY UTILITY REQUIREMENTS FOR THE PROJECT WITH UTILITY COMPANY PRIOR TO COMMENCING WORK.
23. ALL WORK ON SERVICE CONDUCTORS, FEEDERS AND OTHER SUCH EQUIPMENT SHALL BE DONE ONE (1) HOUR EACH CONDUCTOR, FEEDER AND EQUIPMENT ARE TO BE ENERGIZED. THE CONTRACTOR SHALL HAVE AN ELECTRICAL SAFETY AND LOCK OUT/TAG OUT PROCEDURE IN PLACE PRIOR TO COMMENCEMENT OF WORK.
24. ELECTRICAL CONTRACTOR SHALL COORDINATE ALL CONDUIT THROUGH WITH OTHER TRADES AND THE UTILITY COMPANY TO AVOID CONFLICT.
25. MAKE USE FOR EXTERIOR BELLING BRANCH CONDUIT SHALL BE IT.
26. OCCUPANCY SENSOR NOTES:
  1. WALL SENSORS
    - a. SENSOR MUST HAVE CLEAR VIEW OF OCCUPANTS WHERE SENSOR WILL BE LOCATED. SUBSTITUTE WITH SMALL ROOM CEILING SENSOR.
    - b. USE MANUFACTURER SPECIFICATION REGARDING PLACING SENSORS AWAY FROM STRONG AIR FLOW, HEAVY FURNITURE, LOCATION OF EACH LIGHTING FIXTURE, LUMINAIRE, RECEPTACLE OR SENSORS.
    - c. BE AWARE OF INTERFERENCE WITH CEILING AND SENSORS (MOUNTING) FULL LIGHTS, AIRFLOW, TWO TOUCHLESS SWITCH OPERATED FROM NEAR.
    - d. PRIOR TO METAL WORK, REMOVE FACTORY TRAYS AND LAYOUT ADJUSTMENT. IF LOCAL ADJUST TO CORRECT LIGHTING DIRECTION, CONTACT FACTORY REPRESENTATIVE.

## SHEET INDEX

01.1	SYMBOL LIST AND GENERAL NOTES - ELECTRICAL
02.1	FLOORING FINISH LEVEL / FLOOR PLAN - ELECTRICAL
02.2	FIRST FLOOR PLAN - ELECTRICAL
02.3	SECOND FLOOR PLAN - ELECTRICAL
02.4	THIRD FLOOR PLAN - ELECTRICAL
02.5	FOURTH FLOOR PLAN - ELECTRICAL
03.1	WALLS, LBS, DIMENSIONS

3010-0530  
  
 150 Main Street  
 Suite 400  
 Menlo Park, CA 94025  
 Tel: 650.323.7000  
 Fax: 650.323.7001  
 www.interfaceengineering.com



**GENERAL SHEET NOTES**

- 4. COORDINATE EXACT LOCATION AND MOUNTING HEIGHT OF RECEPTACLE, VOICEDATA OUTLETS AND ELECTRICAL DEVICES WITH ARCHITECT PRIOR TO INSTALLATION.
- 5. COORDINATE EXACT LOCATION AND POWER REQUIREMENTS OF HVAC UNITS WITH ENGINEER PRIOR TO INSTALLATION.

**LUMINAIRE SCHEDULE**

- L1/A DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 54 WATTS, 818 LUMENS, TYPE 3 DISTRIBUTION
- L1/B DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 54 WATTS, 818 LUMENS, TYPE 19 DISTRIBUTION
- L1/C DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 54 WATTS, 818 LUMENS, TYPE 22 DISTRIBUTION
- L1/D DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/E DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/F DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/G DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/H DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/I DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/J DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/K DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/L DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/M DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/N DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/O DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/P DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/Q DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
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- L1/U DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/V DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/W DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/X DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/Y DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION
- L1/Z DARGCO BY SIGNIFY "SOFTVIEW" SERIES - 300W, 3000K, 5000K WHITE, 81 WATTS, 868 LUMENS, TYPE 22 DISTRIBUTION

**1 PARKING LEVEL 2 PLAN - ELECTRICAL**



PROJECT: 2019-0101  
 DATE: 11/25/19  
**INTERFACE**  
 ENGINEERING  
 1700 Main Street  
 Suite 400  
 San Francisco, CA 94115  
 Tel: 415-485-1242  
 Fax: 415-485-1238  
 www.interface-engineering.com

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**706 SANTA CRUZ AVE. MENLO PARK**

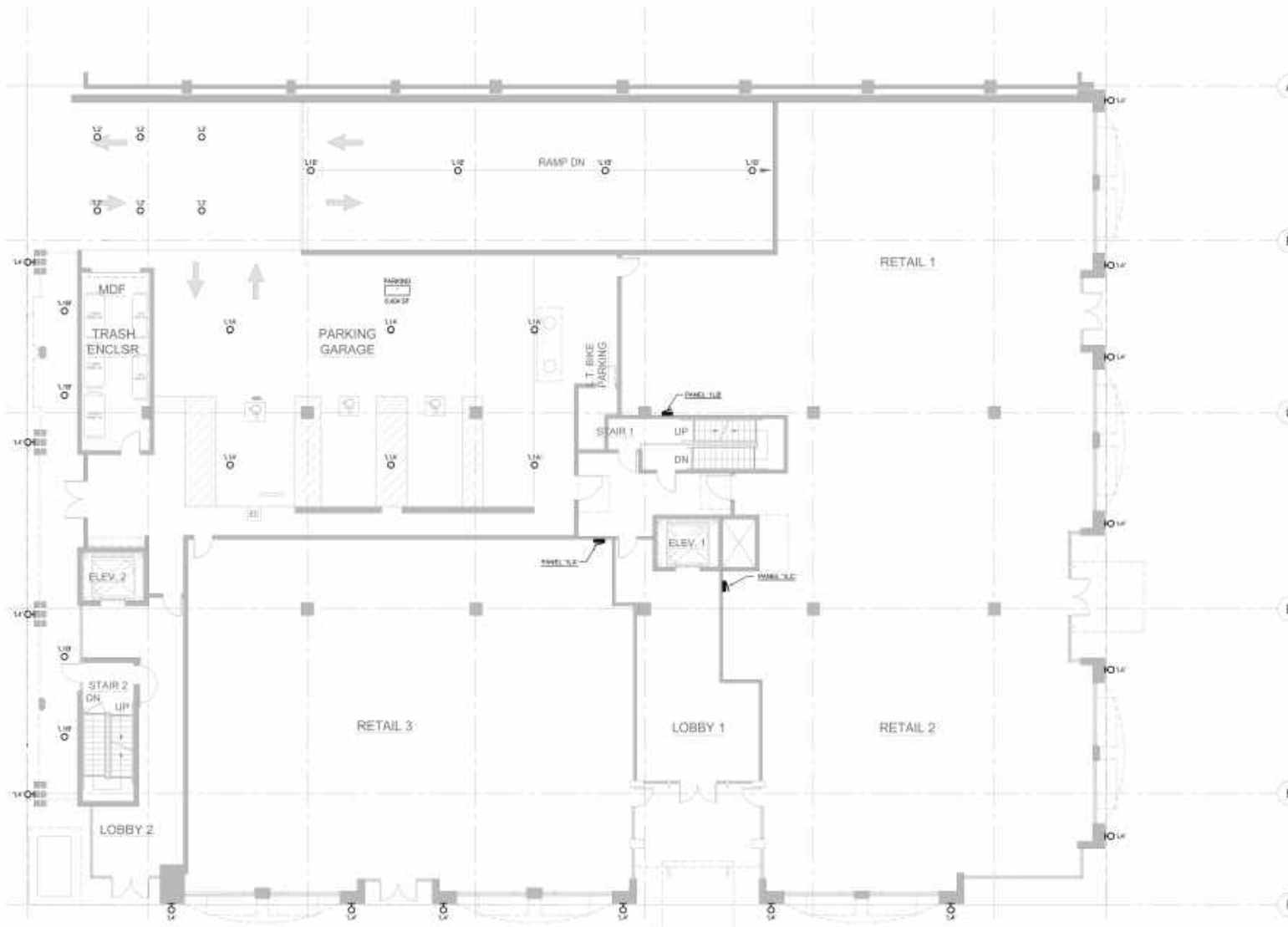
Architectural Review  
 706 Santa Cruz Ave., LLC.

**PARKING LEVEL 2 PLAN - ELECTRICAL**

E 2.0  
 November 25, 2019







**1 FIRST FLOOR PLAN - ELECTRICAL**

SCALE: 1/8"=1'-0"

**GENERAL SHEET NOTES**

- 1. COORDINATE EXACT LOCATION AND MOUNTING HEIGHT OF RECEPTACLES, VOICE/DATA OUTLETS AND ELECTRICAL DEVICES WITH ARCHITECT PRIOR TO INSTALLATION.
- 2. COORDINATE EXACT LOCATION AND POWER REQUIREMENTS OF HVAC UNITS WITH DIVISION 23 PRIOR TO INSTALLATION.

**LUMINAIRE SCHEDULE**

- 1.1A. GARDCO BY SONYFY "SOFTVIEW" SERIES - 300W, WARM WHITE, 84 WATTS, 170 LUMENS, TYPE 3 DISTRIBUTION
- 1.1B. GARDCO BY SONYFY "SOFTVIEW" SERIES - 300W, WARM WHITE, 84 WATTS, 170 LUMENS, TYPE 19 DISTRIBUTION
- 1.1C. GARDCO BY SONYFY "SOFTVIEW" SERIES - 300W, WARM WHITE, 84 WATTS, 170 LUMENS, TYPE 3 DISTRIBUTION
- 1.2. GARDCO BY SONYFY "SOFTVIEW" SERIES - 300W, WARM WHITE, 84 WATTS, 170 LUMENS, TYPE 3 DISTRIBUTION
- 1.3. GARDCO BY SONYFY "SOFTVIEW" SERIES - 300W, WARM WHITE, 84 WATTS, 170 LUMENS, TYPE 3 DISTRIBUTION
- 1.4. BELLA "TRIM" SERIES - 400W, 5.3 WATTS, 82 LUMENS

NOTE:  
 UPLIGHTING NOT ALLOWED IN SPECIFIC PLAN AREA.  
 ADDITIONAL REVIEW OF BUILDING MOUNTED LIGHTS  
 WILL BE REQUIRED AT THE BUILDING PERMIT STAGE.

2019-0018  
 PROJECT  
**INTERFACE**  
 ENGINEERING  
 110 Main Street  
 Suite 400  
 San Francisco, CA 94102  
 Tel: 415.486.2244  
 Fax: 415.486.2245  
 www.interfaceengineering.com

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**706 SANTA CRUZ AVE. MENLO PARK**  
 Architectural Review  
 706 Santa Cruz Ave., LLC.

**FIRST FLOOR PLAN - ELECTRICAL**

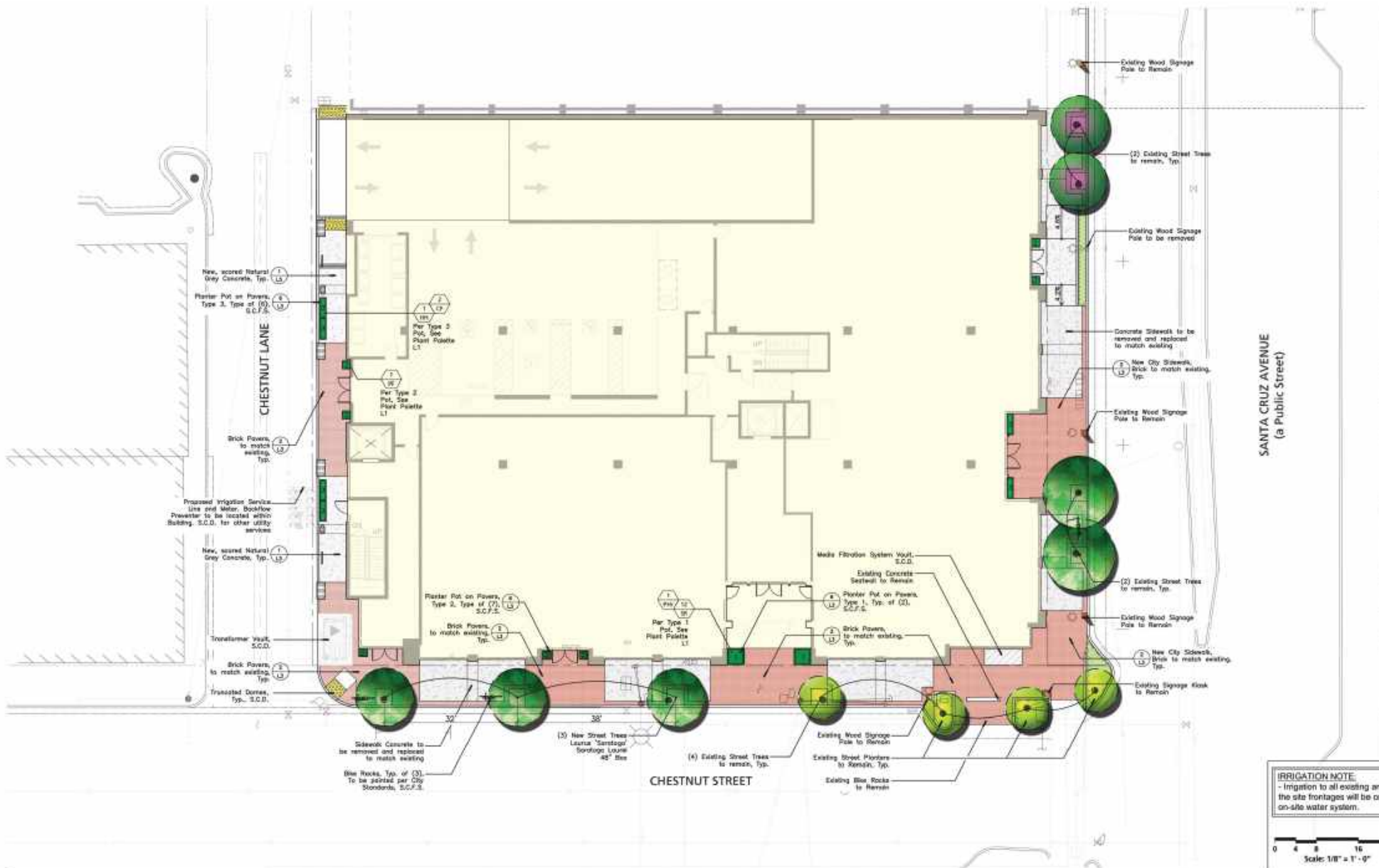
E 2.2  
 November 25, 2019



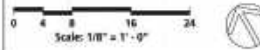








**IRRIGATION NOTE**  
 - Irrigation to all existing and new trees along the site frontages will be connected to the on-site water system.



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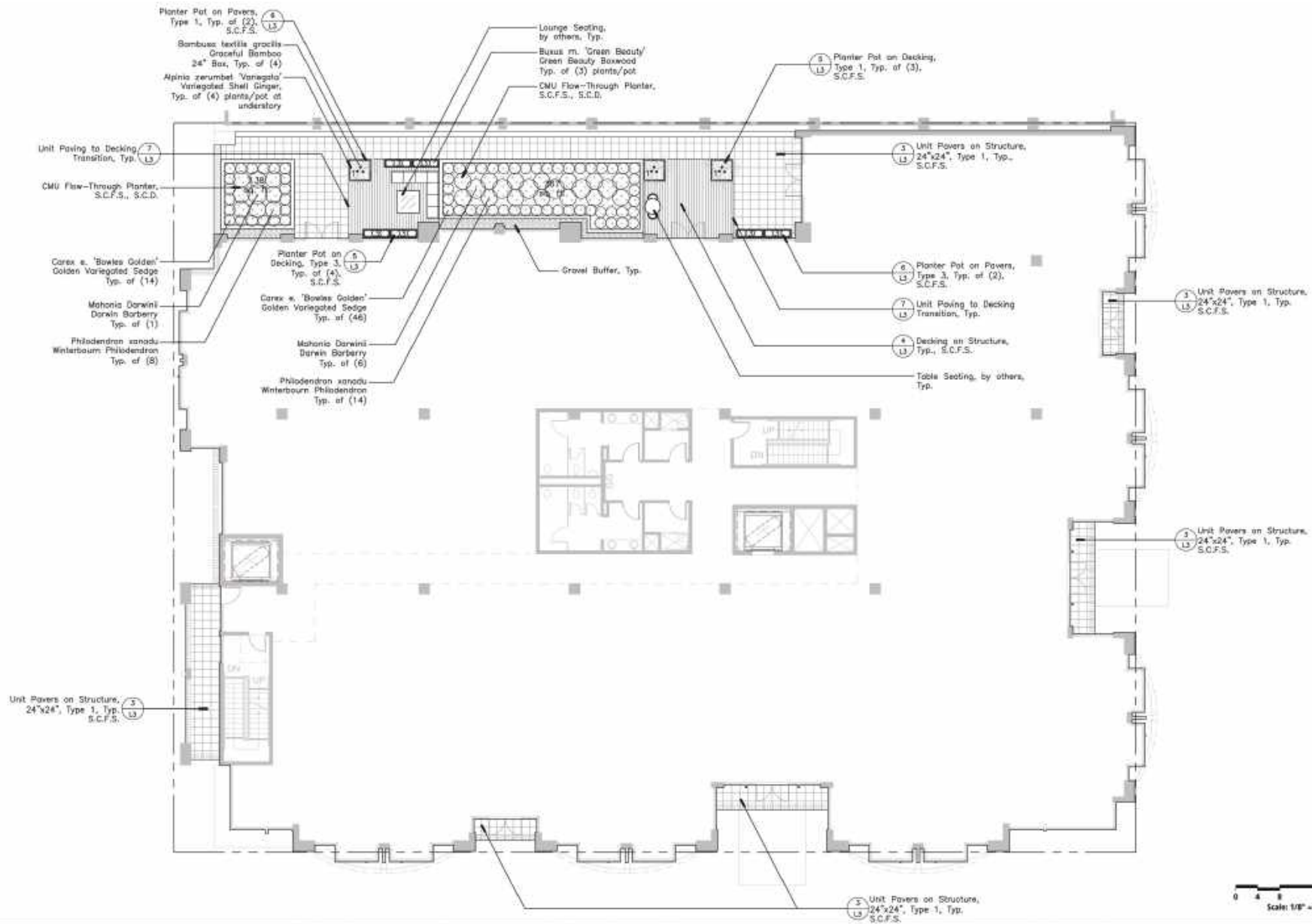
Architectural Review  
 706 Santa Cruz Ave., LLC.



## LEVEL 1- LANDSCAPE PLAN

L 2.1  
 November 25, 2019





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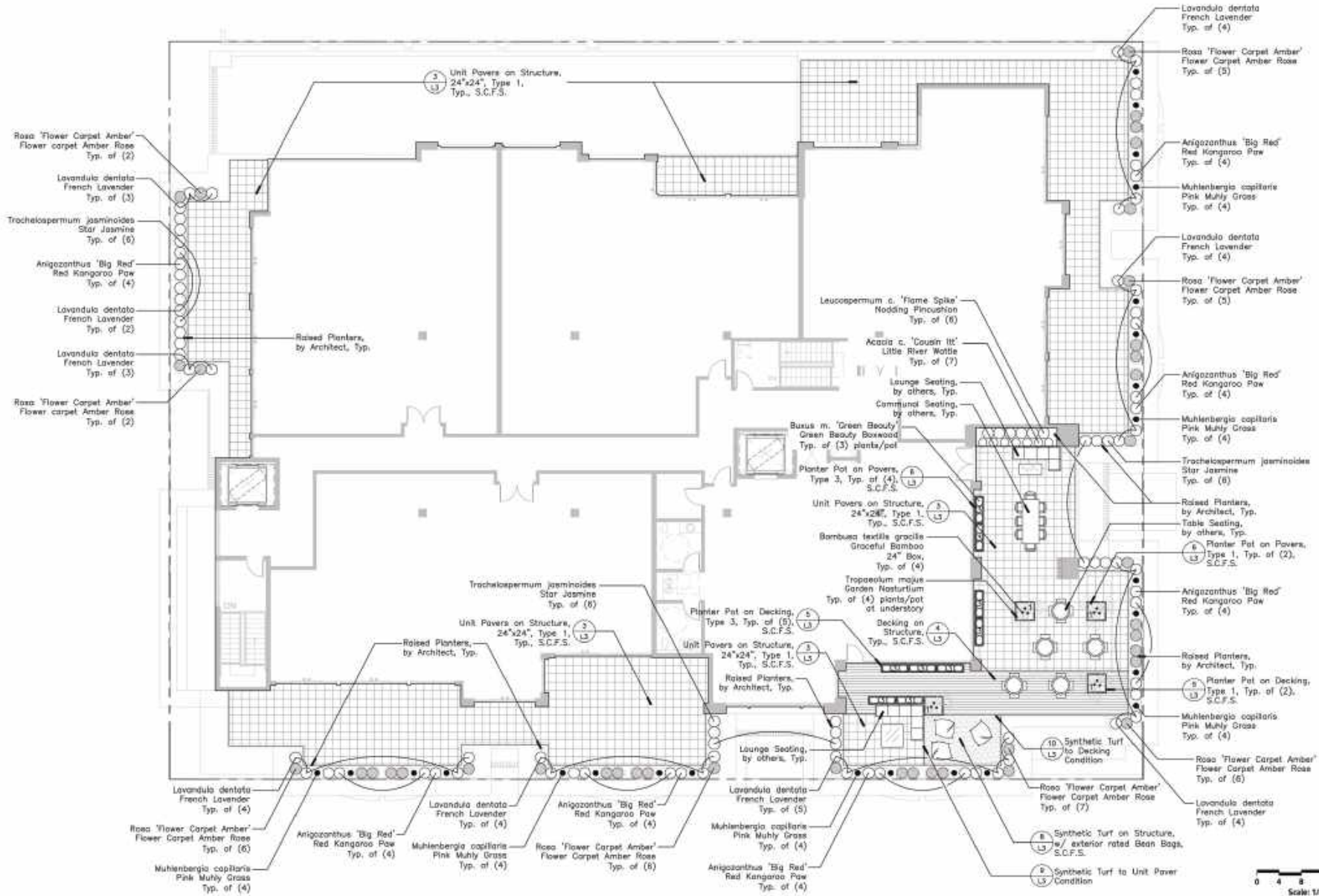


**LEVEL 2- LANDSCAPE PLAN**

L 2.2  
 November 25, 2019







FORM4 ARCHITECTURE • THE GUZZARDO PARTNERSHIP • KIER & WRIGHT • ADAPTURE • INTERFACE ENGINEERING

# 706 SANTA CRUZ AVE. MENLO PARK

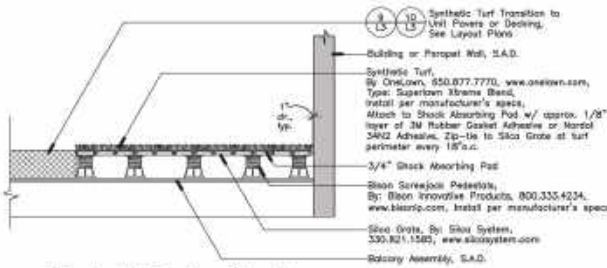
Architectural Review  
706 Santa Cruz Ave., LLC.



## LEVEL 3- LANDSCAPE PLAN

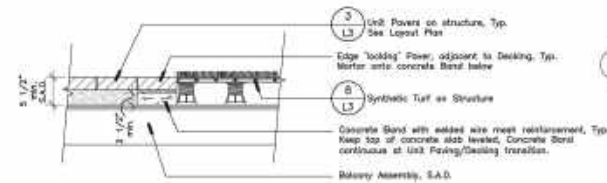
L 2.3  
November 25, 2019





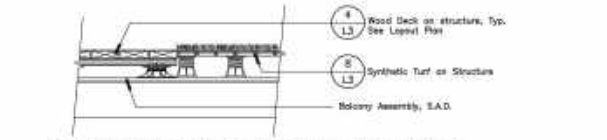
**8 Synthetic Turf on Structure**

Scale: 1/2" = 1'-0"



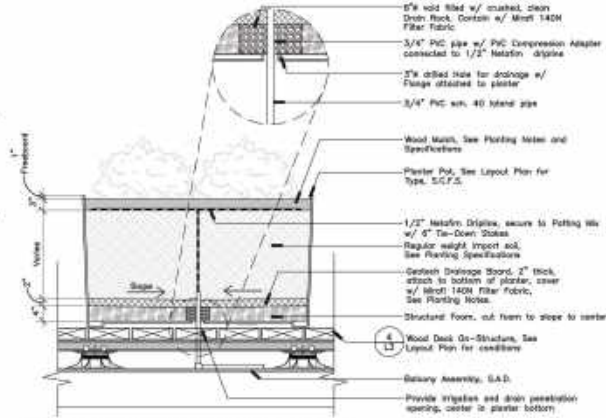
**9 Synthetic Turf to Unit Paver Condition**

Scale: 1" = 1'-0"



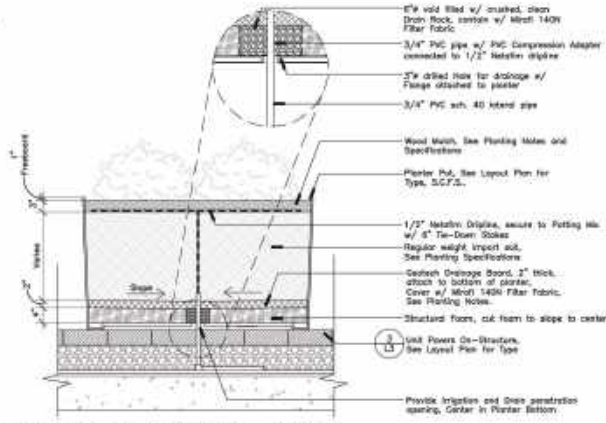
**10 Synthetic Turf to Decking Condition**

Scale: 1" = 1'-0"



**5 Planter Pot on Decking Condition**

Scale: 3/4" = 1'-0"



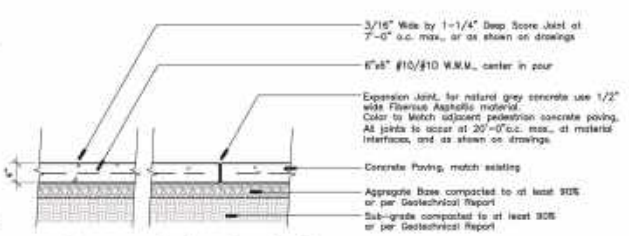
**6 Planter Pot on Pavers Condition**

Scale: 3/4" = 1'-0"



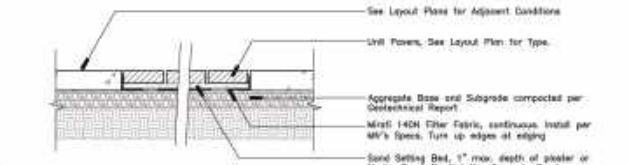
**7 Unit Paving to Decking Condition**

Scale: 1" = 1'-0"



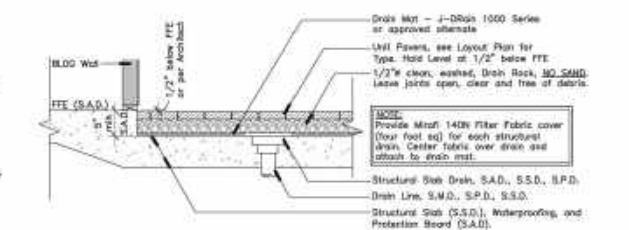
**1 Concrete Paving On-Grade**

Scale: 1" = 1'-0"



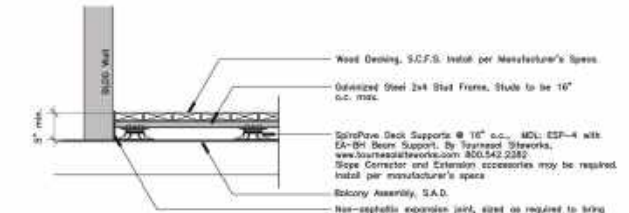
**2 Unit Pavers - On Grade**

Scale: 1" = 1'-0"



**3 Unit Pavers On-Structure**

Scale: 1/2" = 1'-0"



**4 Wood Deck - On Structure**

Scale: 1" = 1'-0"



EXISTING TREE INVENTORY - 11/15/2018					
Tag No.	Botanical Name	Common Name	Trunk Diameter (in.)	Remove	Retain
1	Tilia cordata	Limeleaf Linden	35	x	
2	Lagerstrœmia indica	Crape Myrtle	7	x	
3	Lagerstrœmia indica	Crape Myrtle	5	x	
4	Philadelphus emblicifolius	Victorian Blue	11	x	
5	Philadelphus emblicifolius	Victorian Blue	10	x	
6	Tilia cordata	Limeleaf Linden	12	x	
7	Pyrus calleryana	Flowering Pear	2	x	
8	Pyrus calleryana	Flowering Pear	2	x	
9	Pyrus calleryana	Flowering Pear	3	x	
10	Philadelphus emblicifolius	Victorian Blue	16		x
11	Philadelphus emblicifolius	Victorian Blue	13		x
12	Ulmus parviflorus californicus	California Elm Tree	23		x
13	Rapanea grandiflora	Bluebird Magnolia	10		x
14	Crataegus calycata	Cardi. Thorn	20	x	
15	Platanus occidentalis	Old World Sycamore	2		x

EXISTING AND PROPOSED TREES	
Total Existing Trees	15
Total Existing Trees to be Removed	5
Total Existing Trees to Retain	10
Total Proposed Trees	12
Net Total Trees for Project	22

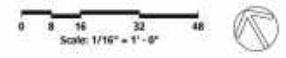
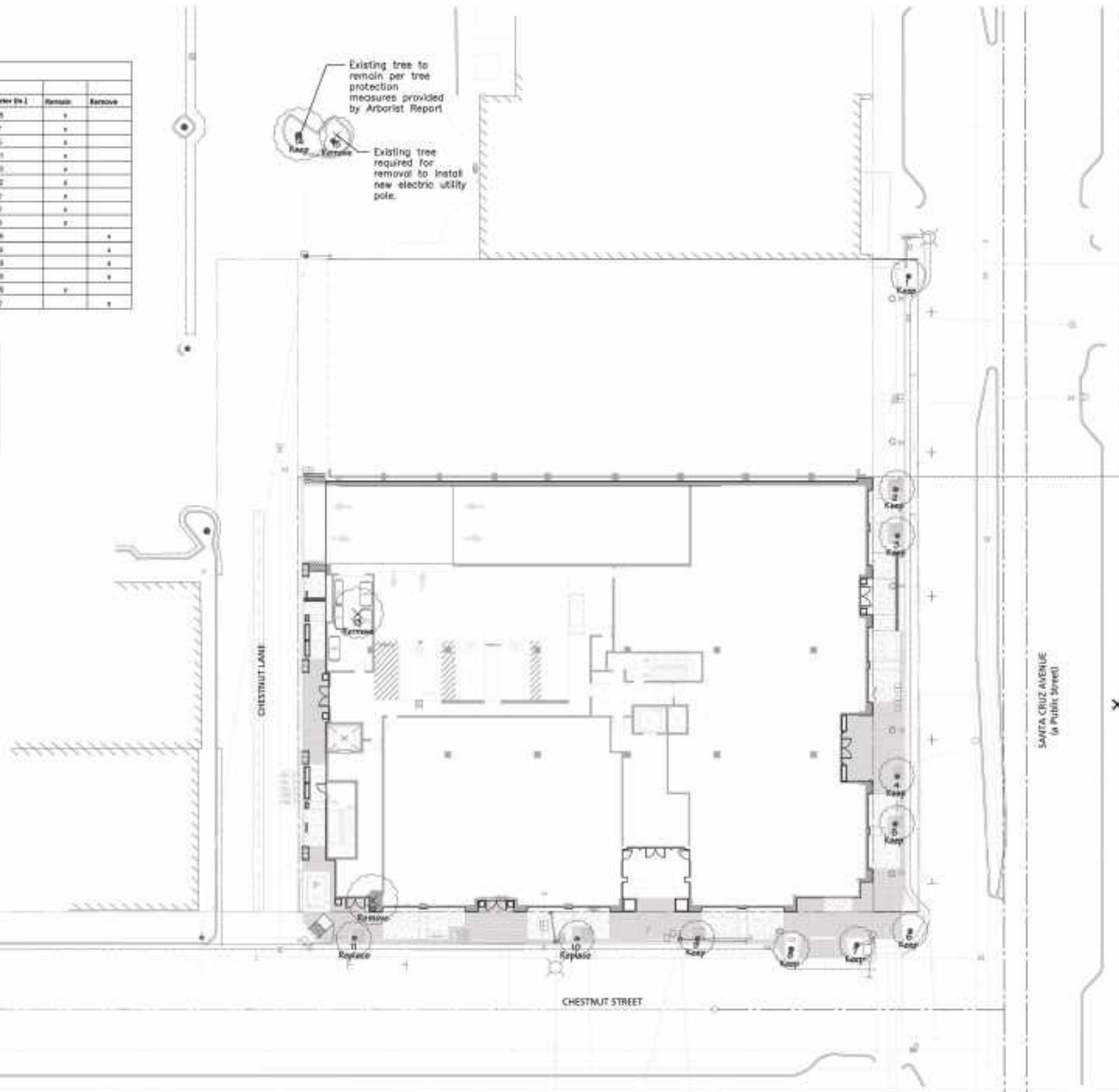
**TREE DISPOSITION LEGEND**

 Existing Tree to be removed  
 Existing Tree to Retain

NOTE: Tree Disposition Plan has been prepared based on Arborist Report prepared by Donald L. Ralston, the Arborist Report Number is 18015 for tree evaluation details.

Existing tree to remain per tree protection measures provided by Arborist Report

Existing tree required for removal to install new electric utility pole.





**LEGEND**

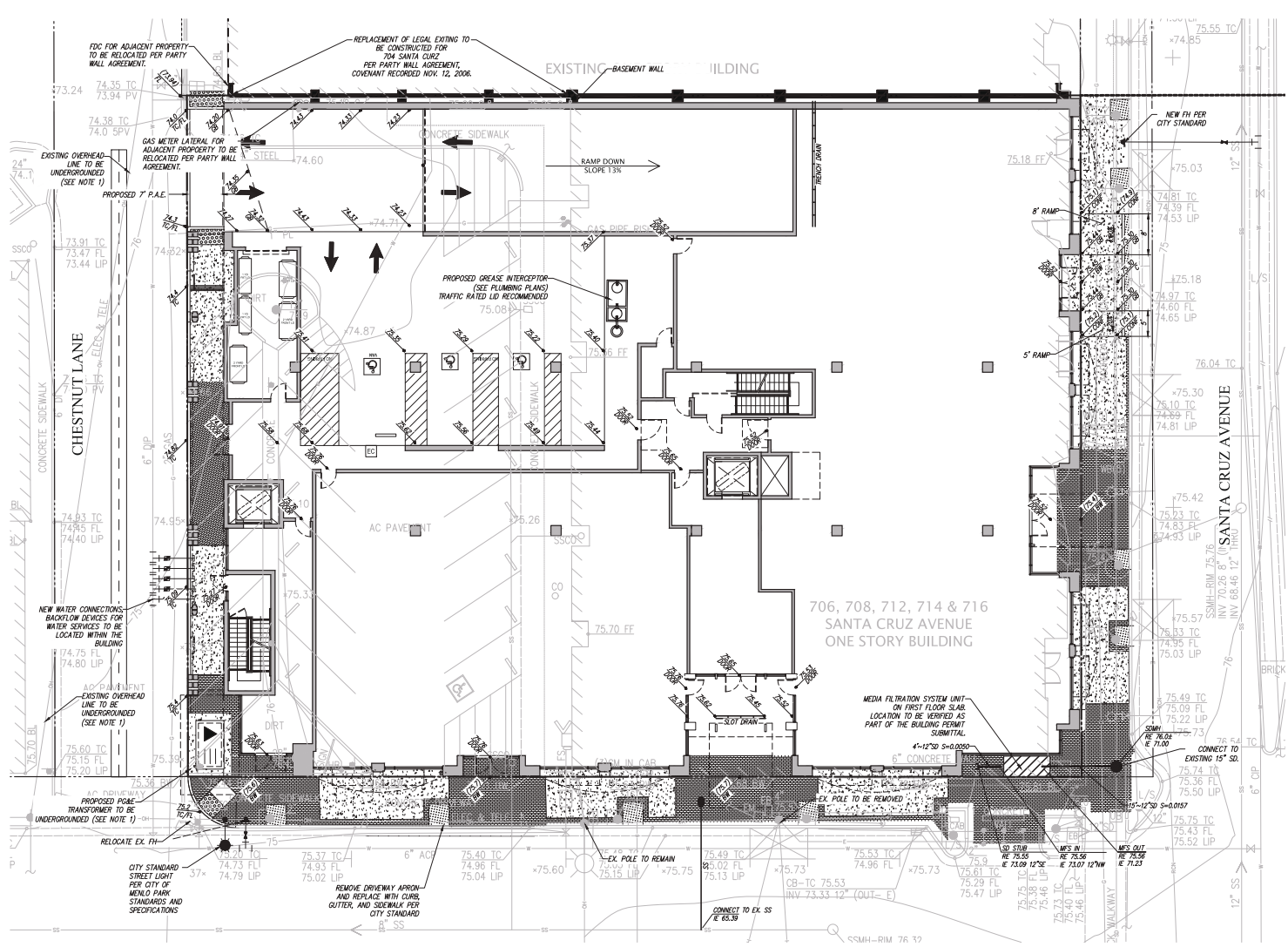
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ADJACENT PROPERTY LINE	---
CENTERLINE	---
EASEMENT	---
NON-ACCESS	---
PUMP	⊕
STORM DRAIN MANHOLE	⊕
CATCH BASIN	⊕
MEDIA FILTRATION SYSTEM	⊕
SANITARY SEWER MANHOLE	⊕
FIRE HYDRANT(FHN)/WET BARREL TYPE WITH ONE 4 1/2 INCH CONNECTION AND TWO 2 1/2 INCH CONNECTIONS	⊕
GATE VALVE	I
WATER METER	I
POST INDICATOR VALVE	I
FIRE DISTRICT CONNECTION	I
SPRINKLER SYSTEM RISER	I
BACK FLOW PREVENTER	I
CHECK VALVE	I
REDUCER	I
LIGHT	⊕
GAS METER	⊕
TRANSFORMER	⊕
SPOT ELEVATION	⊕
HANDICAP STALL	⊕
CURB	---
CURB & GUTTER	---
RED CURB	---
RIDGE	---
SANITARY SEWER	---
STORM DRAIN GRAVITY FLOW	---
STORM DRAIN FORCED FLOW	---
DOMESTIC WATER SERVICE	---
FORCE MAIN	---
FIRE SERVICE	---
WATER	---
ELECTRIC	---
GAS	---
JOINT TRENCH	---
PCC SIDEWALK	---
TRUNCATED DOMES	---
BIO-TREATMENT PLANTING	---
3" GRIND & OVERLAY	---

**ABBREVIATIONS**

BU	BUBBLE UP
COMM.	COMMERCIAL
DWS	DOMESTIC WATER SERVICE
EX	EXISTING
FDC	FIRE DEPARTMENT CONNECTION
FH	FIRE HYDRANT
FL	FLOW LINE
FS	FIRE SERVICE
INV	INVERT ELEVATION
IRR	IRRIGATION
OVD	OVERFLOW DRAIN
R	RIDGE
RES.	RESIDENTIAL
RM	RIM ELEVATION
SD	STORM DRAIN
SS	SANITARY SEWER
TW	TOP OF WALL
VERT.	VERTICAL
W	WITH

**NOTES**

- ALL OVERHEAD COMMUNICATION LINES ARE TO BE UNDERGROUND PER CITY AND LOCAL UTILITY COMPANYS STANDARDS AND SPECIFICATIONS. ALL UTILITIES TO BE UNDERGROUNDED ALONG THE PROJECT FRONTAGE SHALL BE TO THE SATISFACTION OF THE CITY OF MENLO PARK AND THE FIRE DISTRICT.
- PUBLIC SIDEWALK ACCESS TO ADJACENT PROPERTY TO BE COORDINATED WITH THE CITY OF MENLO PARK AS PART OF PERMIT DRAWINGS.
- DESIGN AND CONFIGURATION OF THE ALLEY-TYPE DRIVE AISLE ON CHESTNUT LANE SHALL TO BE COORDINATED WITH THE CITY OF MENLO PARK AND ISSUED AS PART OF PERMIT DRAWINGS.
- EXISTING STREET LIGHTS SHALL BE REPAIRSHED AND PAINTED PER CITY OF MENLO PARK STANDARDS AND SPECIFICATIONS. STREET LIGHTS SHALL BE UPGRADED TO LED FIXTURES.
- ALL EXISTING UTILITIES WITHIN PUBLIC SIDEWALK SHALL BE PROTECTED IN PLACE UNLESS NOTED OTHERWISE.
- BACKFLOW DEVICES SHALL BE LOCATED ON THE GARAGE FLOOR ELECTRICAL ROOM.
- REFER TO SHEET 2.0 FOR ON-SITE DESIGN INFORMATION.
- IRRIGATION TO ALL EXISTING AND NEW TREES ALONG THE SITE FRONTAGES WILL BE CONNECTED TO THE ON-SITE WATER SYSTEM.



**NOTES**

THE PROJECT WILL UNDERGROUND THE OVERHEAD UTILITIES ON CHESTNUT LANE AND CHESTNUT STREET. THE SCOPE OF THE WORK WILL BE DEFINED PRIOR TO THE APPROVAL OF THE FINAL MAP TO THE SATISFACTION OF THE FIRE DISTRICT AND THE CITY OF MENLO PARK.

NO.	REVISION
NO.	REVISION
<p><b>KIER &amp; WRIGHT</b>          CIVIL ENGINEERS &amp; SURVEYORS, INC.          2855 Collier Canyon Road          Menlo Park, CA 94025          Phone (650) 245-8788          Fax (650) 245-8796          www.kierwright.com</p>	
<p><b>PRELIMINARY GRADING, DRAINAGE, UTILITY PLAN OF GROUND FLOOR</b>          706 SANTA CRUZ AVE          FOR  <b>FORM 4 ARCHITECTURE</b></p>	
<p>CALIFORNIA          MENLO PARK.</p>	
DATE	MAY 2019
SCALE	AS SHOWN
DESIGNER	ST
DRAFTER	RS
JOB NO.	A14009-2
SHEET	C2.0
OF	9 SHEETS



**LEGEND**

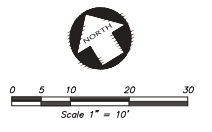
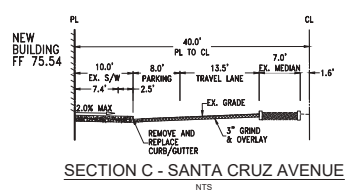
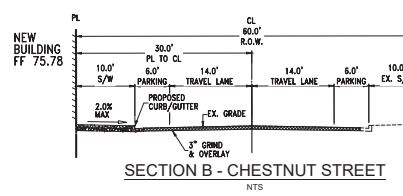
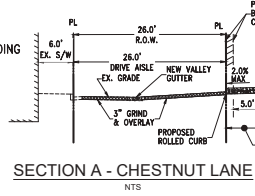
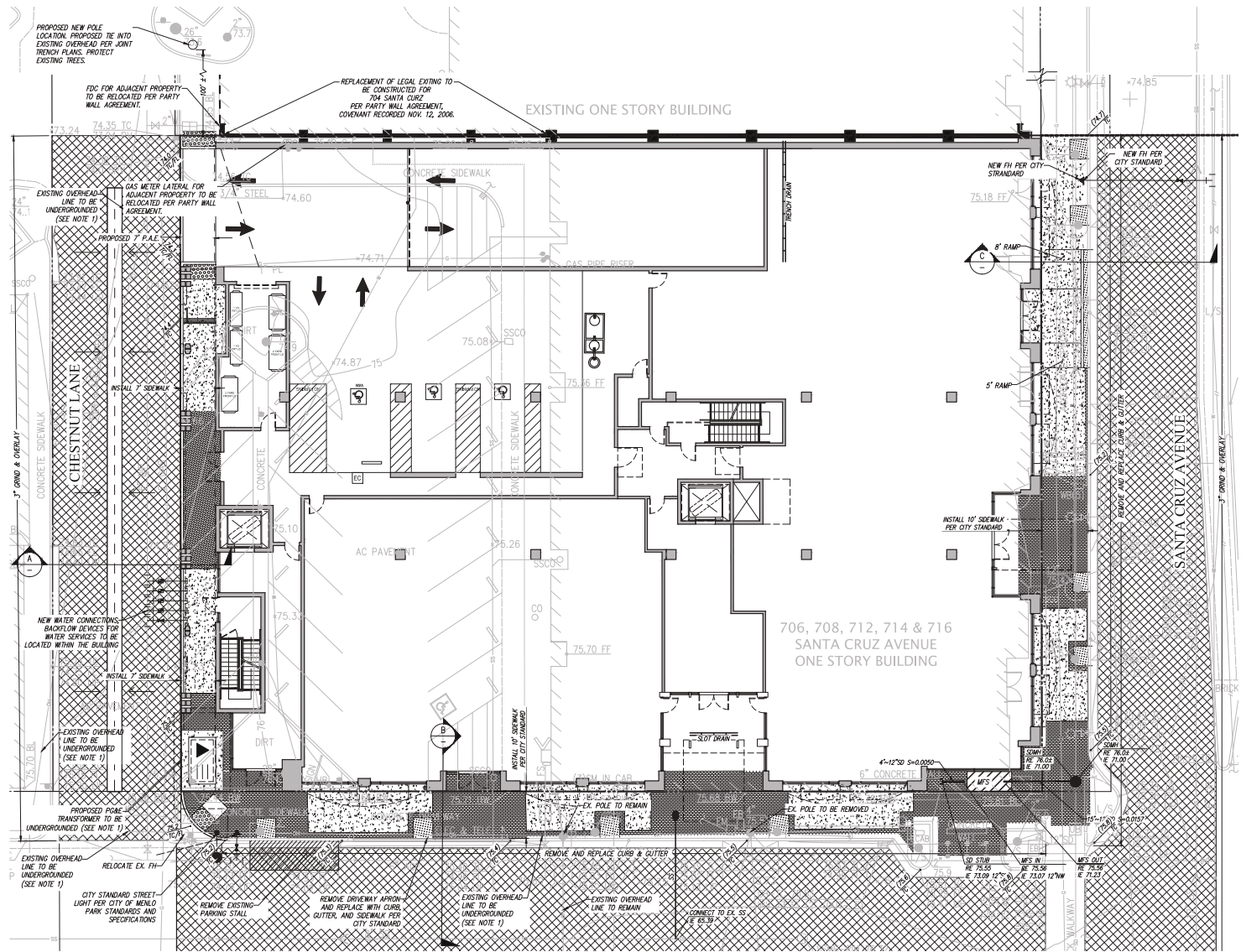
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ADJACENT PROPERTY LINE	----
CENTERLINE	=====
EASEMENT	-----
NON-ACCESS	-----
PUMP	⊕
STORM DRAIN MANHOLE	⊕
CATCH BASIN	⊕
MEDIA FILTRATION SYSTEM	⊕
SANITARY SEWER MANHOLE	⊕
FIRE HYDRANT(FH)/WET BARREL	⊕
TYPE WITH ONE 4 1/2 INCH CONNECTION AND TWO 2 1/2 INCH CONNECTIONS	⊕
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WATER METER	W
POST INDICATOR VALVE	PIV
FIRE DISTRICT CONNECTION	FD
SPRINKLER SYSTEM RISER	S
BACK FLOW PREVENTER	BFP
CHECK VALVE	C
REDUCER	R
LIGHT	L
GAS METER	G
TRANSFORMER	T
SPOT ELEVATION	100.00
HANDICAP STALL	H
CURB	—
CURB & GUTTER	—
RED CURB	—
RIDGE	—
SANITARY SEWER	SS
STORM DRAIN GRAVITY FLOW	SD
STORM DRAIN FORCED FLOW	SDFM
DOMESTIC WATER SERVICE	DW
FORCE MAIN	FM
FIRE SERVICE	FS
WATER	W
ELECTRIC	E
GAS	G
JOINT TRENCH	JT
PCC SIDEWALK	—
TRUNCATED DOMES	—
BIO-TREATMENT PLANTING	—
3" GRIND & OVERLAY	—

**ABBREVIATIONS**

BU	BUBBLE UP
COMM	COMMERCIAL
DWS	DOMESTIC WATER SERVICE
EX	EXISTING
FDC	FIRE DEPARTMENT CONNECTION
FH	FIRE HYDRANT
FL	FLOW LINE
FS	FIRE SERVICE
INV	INVERT ELEVATION
IRR	IRRIGATION
OVD	OVERFLOW DRAIN
R	RIDGE
RES.	RESIDENTIAL
RM	RIM ELEVATION
SD	STORM DRAIN
SS	SANITARY SEWER
TW	TOP OF WALL
VERT.	VERTICAL
W	WITH

**NOTES**

- ALL OVERHEAD COMMUNICATION LINES ARE TO BE UNDERGROUND PER CITY AND LOCAL UTILITY COMPANY'S STANDARDS AND SPECIFICATIONS. ALL UTILITIES TO BE UNDERGROUNDED ALONG THE PROJECT FRONTAGE SHALL BE TO THE SATISFACTION OF THE CITY OF MENLO PARK AND THE FIRE DISTRICT.
- PUBLIC SIDEWALK ACCESS TO ADJACENT PROPERTY TO BE COORDINATED WITH THE CITY OF MENLO PARK AS PART OF PERMIT DRAWINGS.
- DESIGN AND CONFIGURATION OF THE ALLEY-TYPE DRIVE AISLE ON CHESTNUT LANE SHALL TO BE COORDINATED WITH THE CITY OF MENLO PARK AND ISSUED AS PART OF PERMIT DRAWINGS.
- EXISTING STREET LIGHTS SHALL BE REFURBISHED AND PAINTED PER CITY OF MENLO PARK STANDARDS AND SPECIFICATIONS. STREET LIGHTS SHALL BE UPGRADED TO LED FIXTURES.
- ALL EXISTING UTILITIES WITHIN PUBLIC SIDEWALK SHALL BE PROTECTED IN PLACE UNLESS NOTED OTHERWISE.
- BACKFLOW DEVICES SHALL BE LOCATED ON THE GARAGE FLOOR ELECTRICAL ROOM.
- REFER TO SHEET 2.0 FOR ON-SITE DESIGN INFORMATION.
- IRRIGATION TO ALL EXISTING AND NEW TREES ALONG THE SITE FRONTAGES WILL BE CONNECTED TO THE ON-SITE WATER SYSTEM.

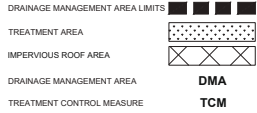


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<b>KIER &amp; WRIGHT</b> CIVIL ENGINEERS & SURVEYORS, INC. 2858 Collier Canyon Road Menlo Park, CA 94025 Phone (650) 245-8788 Fax (650) 245-8796 www.kierwright.com	
<b>PRELIMINARY OFFSITE IMPROVEMENT PLANS</b> OF <b>706 SANTA CRUZ AVE</b> FOR <b>FORM 4 ARCHITECTURE</b>	
CALIFORNIA MENLO PARK.	
DATE	MAY 2019
SCALE	AS SHOWN
DESIGNER	ST
DRAFTER	RS
JOB NO.	A14009-2
SHEET	C2.1
OF	9 SHEETS





**LEGEND**



**IMPERVIOUS/PERVIOUS TABLE**

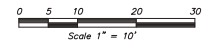
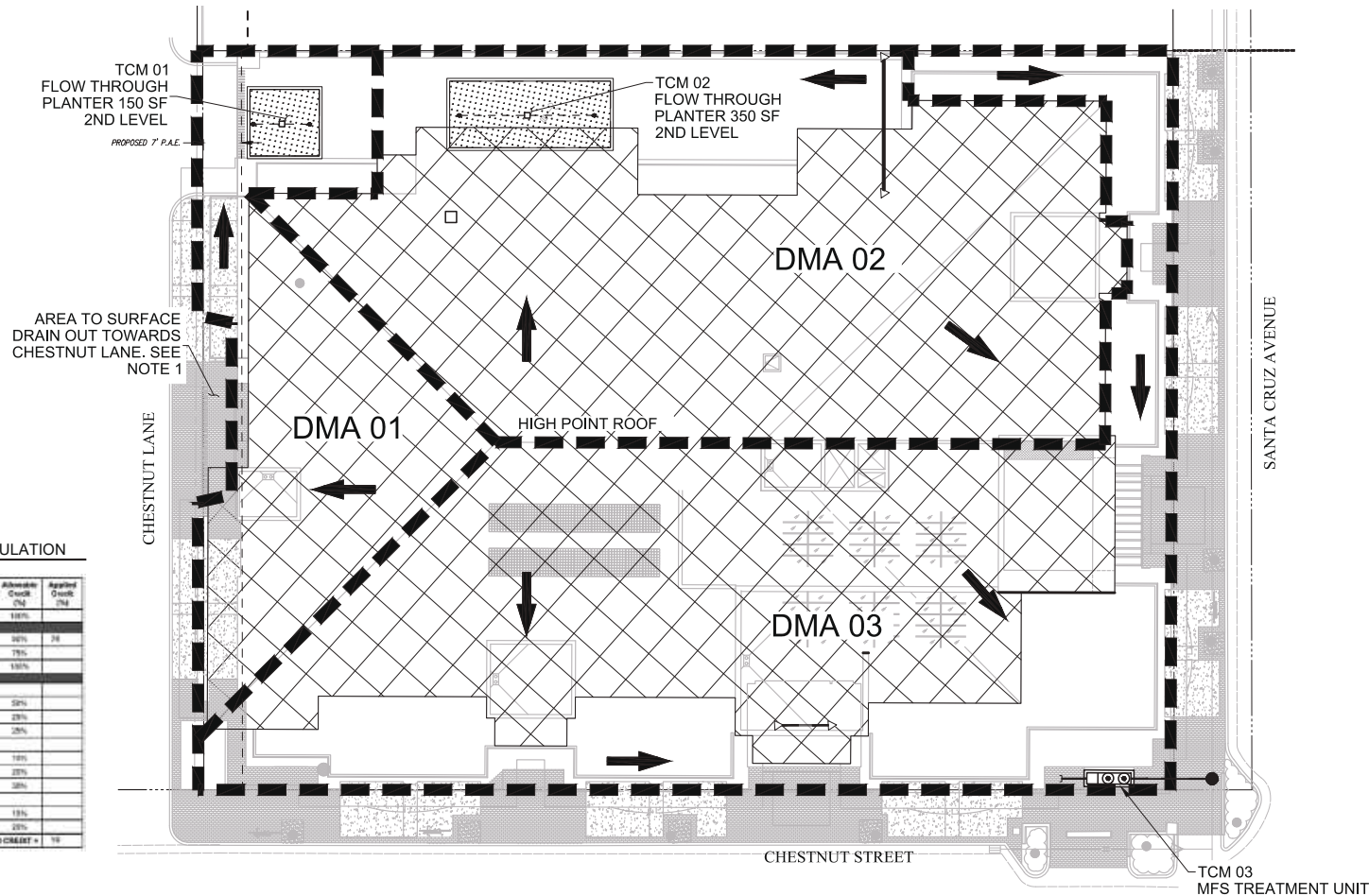
EXISTING SITE		
	AREA (S.F.)	% TOTAL
IMPERVIOUS	22966	97.9
PERVIOUS	489	2.1
TOTAL	23455	100.0
C-FACTOR	0.89	
PROPOSED SITE		
	AREA (S.F.)	% TOTAL
IMPERVIOUS	22955	97.9
PERVIOUS	500	2.1
TOTAL	23455	100.0
C-FACTOR	0.89	

**NOTES**

- SMALL AREA NEAR MAIN ENTRY TO BUILDING SHALL DRAIN OUT TOWARDS CHESTNUT LANE (<200 SF). THIS AREA WILL BE AFFECTED BY TIRE WASH AND SMALL AMOUNTS OF RAIN THAT FALL UNDER THE 2ND FLOOR TERRACE AREA.

**LID TREATMENT REDUCTION CREDIT CALCULATION**

Category	Impervious Area (Creases/Roofload/Seals)	Risk Coverage (%)	Percent Density of P.A.E.	Density Criteria	Admissible Credit (%)	Applied Credit (%)
A			M.A.		100%	
B	0.5E	90%	≥ 1	Rate ≤ 30 (Cubic) or P.A.E. ≤ 2.1 Rate ≤ 75 (Cubic) or P.A.E. ≤ 3.1 Rate ≤ 150 (Cubic) or P.A.E. ≤ 4.1	90% 75% 100%	75
C				Location credit (select one): Within 10' edge of street curb: 25% Within 15' edge of street curb: 25% Within a planned PCA: 25% Density credit (select one): Rate ≤ 30 (Cubic) or P.A.E. ≤ 2.1: 10% Rate ≤ 60 (Cubic) or P.A.E. ≤ 4.1: 20% Rate ≤ 150 (Cubic) or P.A.E. ≤ 8.1: 25% Parking credit (select one): 4.10% asphalt surface parking: 15% As to other paving: 25%		
<b>TOTAL TMD CREDIT =</b>					<b>75</b>	



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 <b>KIER &amp; WRIGHT</b> CIVIL ENGINEERS & SURVEYORS, INC. 2850 Collier Canyon Road Menlo Park, CA 94025-1951 Phone (650) 245-8788 Fax (650) 245-8776 www.kierwright.com							
<b>PRELIMINARY STORM WATER MANAGEMENT PLAN</b> OF <b>706 SANTA CRUZ AVE</b> FOR <b>FORM 4 ARCHITECTURE</b> MENLO PARK, CALIFORNIA							
DATE		MAY 2019					
SCALE		AS SHOWN					
DESIGNER		ST					
DRAFTER		RS					
JOB NO.		A14009-2					
SHEET		<b>C3.0</b>					
OF		9 SHEETS					

**MFS UNIT CALCULATIONS**

MFS TREATED IMPERVIOUS AREA 10,702 S.F.  
 SITE TREATED IMPERVIOUS AREA 22,755 S.F.  
 MFS TREATMENT 47%

MEDIA FILTER SIZING		
DMA #	10702	A= [0.24565] cfs
C Value	0.9	Weighted C Value
Area* (S.F.)	10,702	
0.8	0	0.800
0.7	0	
0.1	0	

\*Input Values by hand or use Table at the bottom of the spreadsheet.

D=Carla  
 D=2044221

Manufacturer: K/S/Inc/Oldcastle  
 Cartridge Height: 18 in.  
 Cartridge Media (if applicable): Perk Filter  
 G.U.D. Cartridge Treatment Floors (CTF): 18.2 gpm/cartridge

# Cartridges = [2] @ 149 gpm @ 2 / CTF  
 # Cartridges = [1] @ 6625 (round up)  
 # Cartridges Required = [2]  
 Treatment Flow Rate Capacity = [0.48434] cfs

**MEDIA FILTER INSPECTION & MAINTENANCE**

NO.	MAINTENANCE TASK	FREQUENCY OF TASK
1	INSPECT FOR STANDING WATER, SEDIMENT, TRASH AND DEBRIS.	MONTHLY DURING RAINY SEASON
2	REMOVE ACCUMULATED TRASH AND DEBRIS IN THE UNIT DURING ROUTINE INSPECTIONS.	MONTHLY DURING RAINY SEASON, OR AS NEEDED AFTER STORM EVENTS
3	INSPECT TO ENSURE THAT THE FACILITY IS DRAINING COMPLETELY WITHIN FIVE DAYS AND PER MANUFACTURER'S SPECIFICATIONS.	ONCE DURING THE WET SEASON AFTER MAJOR STORM EVENT
4	REPLACE THE MEDIA PER MANUFACTURER'S INSTRUCTIONS OR AS INDICATED BY THE CONDITION OF THE UNIT.	PER MANUFACTURER'S SPECIFICATIONS
5	INSPECT OUTLETS TO ENSURE PROPER DRAINAGE.	MONTHLY DURING RAINY SEASON, OR AS NEEDED AFTER STORM EVENTS

**SITE DESIGN MEASURES**

1. PARKING ON TOP OF OR UNDER BUILDING.

**SOURCE CONTROL MEASURES**

1. MAINTENANCE (PAVEMENT SWEeping, CATCH BASIN CLEANING, GOOD HOUSEKEEPING).
2. STORM DRAIN LABELING
3. INTERIOR PARKING STRUCTURES

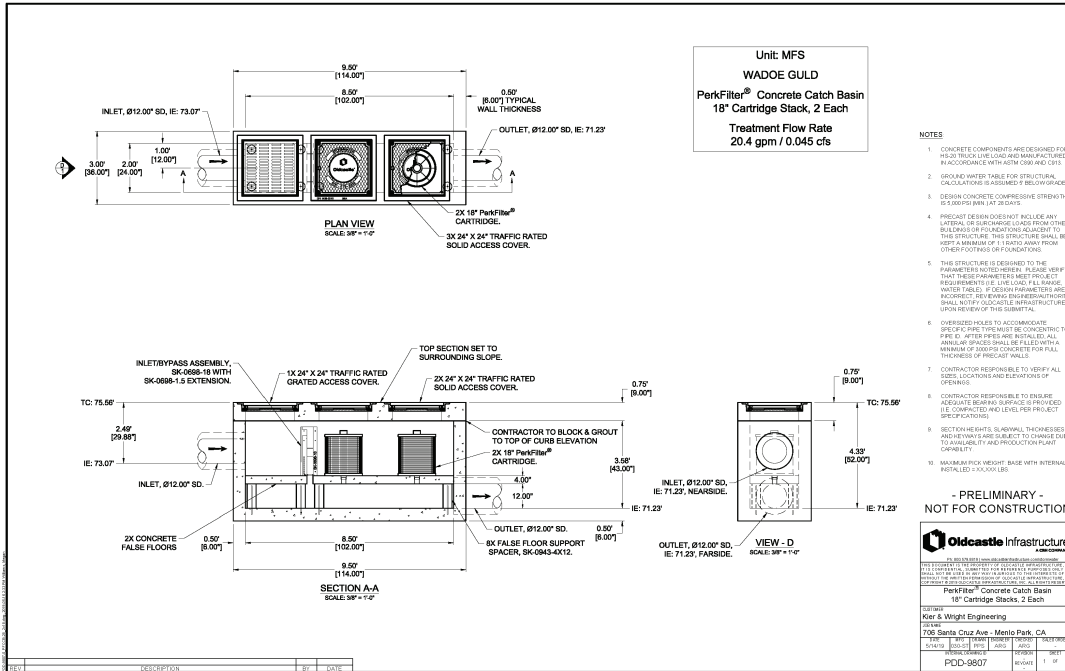
**LID TREATMENT CALCULATIONS (4% RULE)**

AREA	TREATMENT CALCULATIONS					
	IMPERVIOUS AREA (SF)	PERVIOUS AREA (SF)	TOTAL (SF)	PERVIOUS AREA * 0.1 (SF)	EFFECTIVE IMPERVIOUS AREA (SF)	TREATMENT AREA REQUIRED (4% RULE) (SF)
DMA 01	3,340	150	3,490	15.0	3,355	134
DMA 02	8,713	350	9,063	35.0	8,748	350

**FLOW THROUGH INSPECTION & MAINTENANCE**

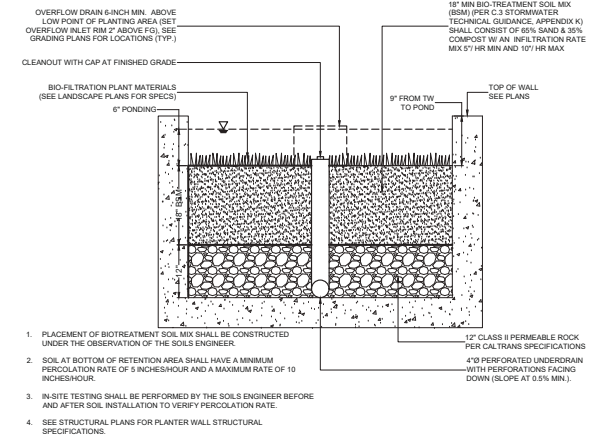
NO.	MAINTENANCE TASK	FREQUENCY OF TASK
1	INSPECT THE PLANTER SURFACE AREA, INLETS AND OUTLETS FOR OBSTRUCTIONS AND TRASH. CLEAR ANY OBSTRUCTIONS AND REMOVE TRASH.	QUARTERLY
2	INSPECT PLANTER FOR STANDING WATER. IF STANDING WATER DOES NOT DRAIN WITHIN 2-3 DAYS, THE SURFACE BIOTREATMENT SOIL SHOULD BE TILLED OR REPLACED WITH THE APPROVED SOIL MIX AND REPLANTED. USE THE CLEANOUT RISER TO CLEAR ANY UNDERDRAINS OF OBSTRUCTIONS OR CLOGGING MATERIAL.	QUARTERLY
3	CHECK FOR ERODED OR SETTLED BIOTREATMENT SOIL MEDIA. LEVEL SOIL WITH RAKE AND REMOVE/REPLANT VEGETATION AS NECESSARY.	QUARTERLY
4	MAINTAIN THE VEGETATION AND IRRIGATION SYSTEM. PRUNE AND WEED TO KEEP FLOW-THROUGH PLANTER NEAT AND ORDERLY IN APPEARANCE.	QUARTERLY
5	EVALUATE HEALTH AND DENSITY OF VEGETATION. REMOVE AND REPLACE ALL DEAD AND DISEASED VEGETATION. REMOVE EXCESSIVE GROWTH OF PLANTS THAT ARE TOO CLOSE TOGETHER.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
6	USE COMPOST AND OTHER NATURAL SOIL AMENDMENTS AND FERTILIZERS INSTEAD OF SYNTHETIC FERTILIZERS, ESPECIALLY IF THE SYSTEM USES AN UNDERDRAIN.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
7	INSPECT THE OVERFLOW PIPE TO MAKE SURE THAT IT CAN SAFELY CONVEY EXCESS FLOWS TO A STORM DRAIN. REPAIR OR REPLACE ANY DAMAGED OR DISCONNECTED PIPING. USE THE CLEANOUT RISER TO CLEAR UNDERDRAINS OF OBSTRUCTIONS OR CLOGGING MATERIAL.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
8	INSPECT THE ENERGY DISSIPATOR AT THE INLET TO ENSURE IT IS FUNCTIONING ADEQUATELY, AND THAT THERE IS NO SCOUR OF THE SURFACE MULCH. REMOVE ANY ACCUMULATION OF SEDIMENT.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
9	INSPECT AND, IF NEEDED, REPLACE WOOD MULCH. IT IS RECOMMENDED THAT 2" TO 3" OF COMPOSTED ARBOR MULCH BE APPLIED ONCE A YEAR.	ANNUALLY, BEFORE THE RAINY SEASON BEGINS
10	INSPECT SYSTEM FOR EROSION OF BIOTREATMENT SOIL MEDIA. LOSS OF MULCH, STANDING WATER, CLOGGED OVERFLOWS, WEEDS, TRASH AND DEAD PLANTS. IF USING ROCK MULCH, CHECK FOR 3" OF COVERAGE.	ANNUALLY AT THE END OF THE RAINY SEASON AND/OR AFTER LARGE STORM EVENTS
11	INSPECT SYSTEM FOR STRUCTURAL INTEGRITY OF WALLS, FLOW SPREADERS, ENERGY DISSIPATORS, CURB CUTS, OUTLETS AND FLOW SPLITTERS.	ANNUALLY AT THE END OF THE RAINY SEASON AND/OR AFTER LARGE STORM EVENTS

NON-LID TREATMENT CONTROL MEASURE SUMMARY TABLE												
DMA#	TCM #	TREATMENT TYPE	DRAINAGE AREA (AC)	DRAINAGE AREA (SF)	IMPERVIOUS AREA (SF)	PERVIOUS AREA (SF)	TREATMENT FLOWRATE REQUIRED (CFS)	TREATMENT FLOWRATE CAPACITY (CFS)	# of Cartridges Required	# of Cartridges Provided	Media Type	Cartridge Height (inches)
DMA 03	TCM 03	MFS UNIT	0.25	10,702	10,702	0	0.044	0.045	2	2	PERK FILTER	18"



**MEDIA FILTRATION SYSTEM DETAIL**

1



**FLOW THROUGH PLANTER DETAIL**

2

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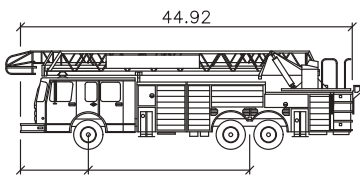


**LEGEND**

- PROPERTY LINE
- ADJACENT PROPERTY LINE
- CENTERLINE
- EASEMENT
- NON-ACCESS
- PUMP
- STORM DRAIN MANHOLE
- CATCH BASIN
- MEDIA FILTRATION SYSTEM
- SANITARY SEWER MANHOLE
- FIRE HYDRANT/FH-IN-WET BARREL TYPE WITH ONE 4 1/2 INCH CONNECTION AND TWO 2 1/2 INCH CONNECTIONS.
- GATE VALVE
- WATER METER
- POST INDICATOR VALVE
- FIRE DISTRICT CONNECTION
- SPRINKLER SYSTEM RISER
- BACK FLOW PREVENTER
- CHECK VALVE
- REDUCER
- LIGHT
- GAS METER
- TRANSFORMER
- SPOT ELEVATION
- HANDICAP STALL
- CURB
- CURB & GUTTER
- RED CURB
- RIDGE
- SANITARY SEWER
- STORM DRAIN GRAVITY FLOW
- STORM DRAIN FORCED FLOW
- DOMESTIC WATER SERVICE
- FORCE MAIN
- FIRE SERVICE
- WATER
- ELECTRIC
- GAS
- JOINT TRENCH
- PCC SIDEWALK
- TRUNCATED DOMES
- BIO-TREATMENT PLANTING

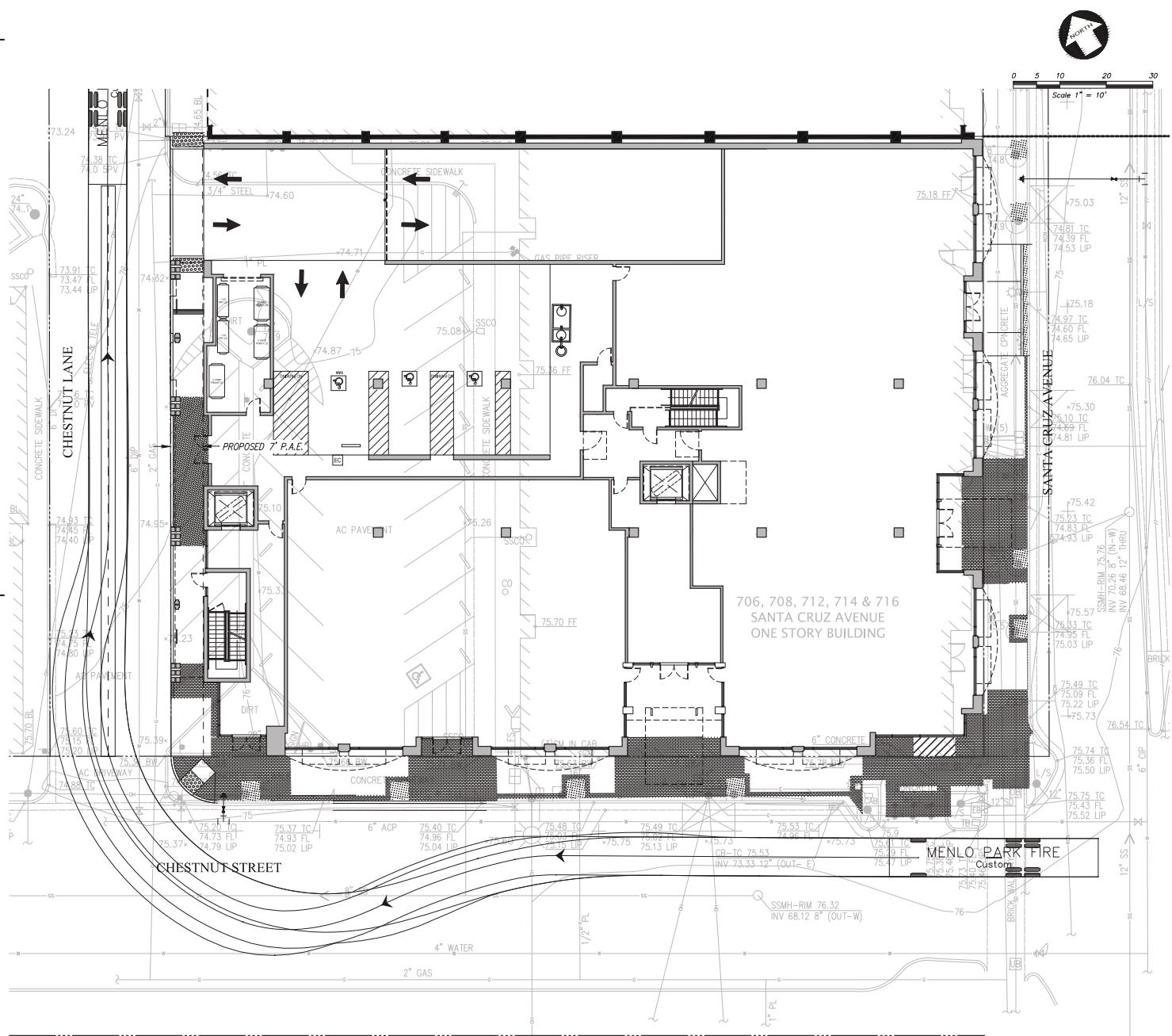
**ABBREVIATIONS**

- BU BUBBLE UP
- COMM. COMMERCIAL
- DWS DOMESTIC WATER SERVICE
- EX EXISTING
- FDG FIRE DEPARTMENT CONNECTION
- FH FIRE HYDRANT
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- INV INVERT ELEVATION
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- OVD OVERFLOW DRAIN
- R RIDGE
- RES. RESIDENTIAL
- RM RM ELEVATION
- SD STORM DRAIN
- SS SANITARY SEWER
- TW TOP OF WALL
- VERT. VERTICAL
- WT WITH



**MENLO PARK FIRE**

Width	: 8.25
Track	: 8.25
Lock to Lock Time	: 6.0
Steering Angle	: 25.4



	<p><b>KIER &amp; WRIGHT</b>          CIVIL ENGINEERS &amp; SURVEYORS, INC.          2850 Collier Canyon Road          Menlo Park, CA 94025          Phone (650) 245-8788          Fax (650) 245-8796          www.kierwright.com</p>	CALIFORNIA <b>MENLO PARK FIRE TURNING</b> <b>OF</b> <b>706 SANTA CRUZ AVE</b> <b>FOR</b> <b>FORM 4 ARCHITECTURE</b> MENLO PARK, CA
DATE	MAY 2019	
SCALE	AS SHOWN	
DESIGNER	ST	
DRAFTER	RS	
JOB NO.	A14009-2	
SHEET	<b>C5.0</b>	
OF	9 SHEETS	

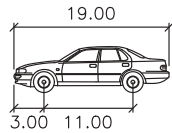


**LEGEND**

- PROPERTY LINE
- ADJACENT PROPERTY LINE
- CENTERLINE
- EASEMENT
- NON-ACCESS
- PUMP
- STORM DRAIN MANHOLE
- CATCH BASIN
- MEDIA FILTRATION SYSTEM
- SANITARY SEWER MANHOLE
- FIRE HYDRANT/FH-IN-WET BARREL TYPE WITH ONE 4 1/2 INCH CONNECTION AND TWO 2 1/2 INCH CONNECTIONS.
- GATE VALVE
- WATER METER
- POST INDICATOR VALVE
- FIRE DISTRICT CONNECTION
- SPRINKLER SYSTEM RISER
- BACK FLOW PREVENTER
- CHECK VALVE
- REDUCER
- LIGHT
- GAS METER
- TRANSFORMER
- SPOT ELEVATION
- HANDICAP STALL
- CURB
- CURB & GUTTER
- RED CURB
- RIDGE
- SANITARY SEWER
- STORM DRAIN GRAVITY FLOW
- STORM DRAIN FORCED FLOW
- DOMESTIC WATER SERVICE
- FORCE MAIN
- FIRE SERVICE
- WATER
- ELECTRIC
- GAS
- JOINT TRENCH
- PCC SIDEWALK
- TRUNCATED DOMES
- BIO-TREATMENT PLANTING

**ABBREVIATIONS**

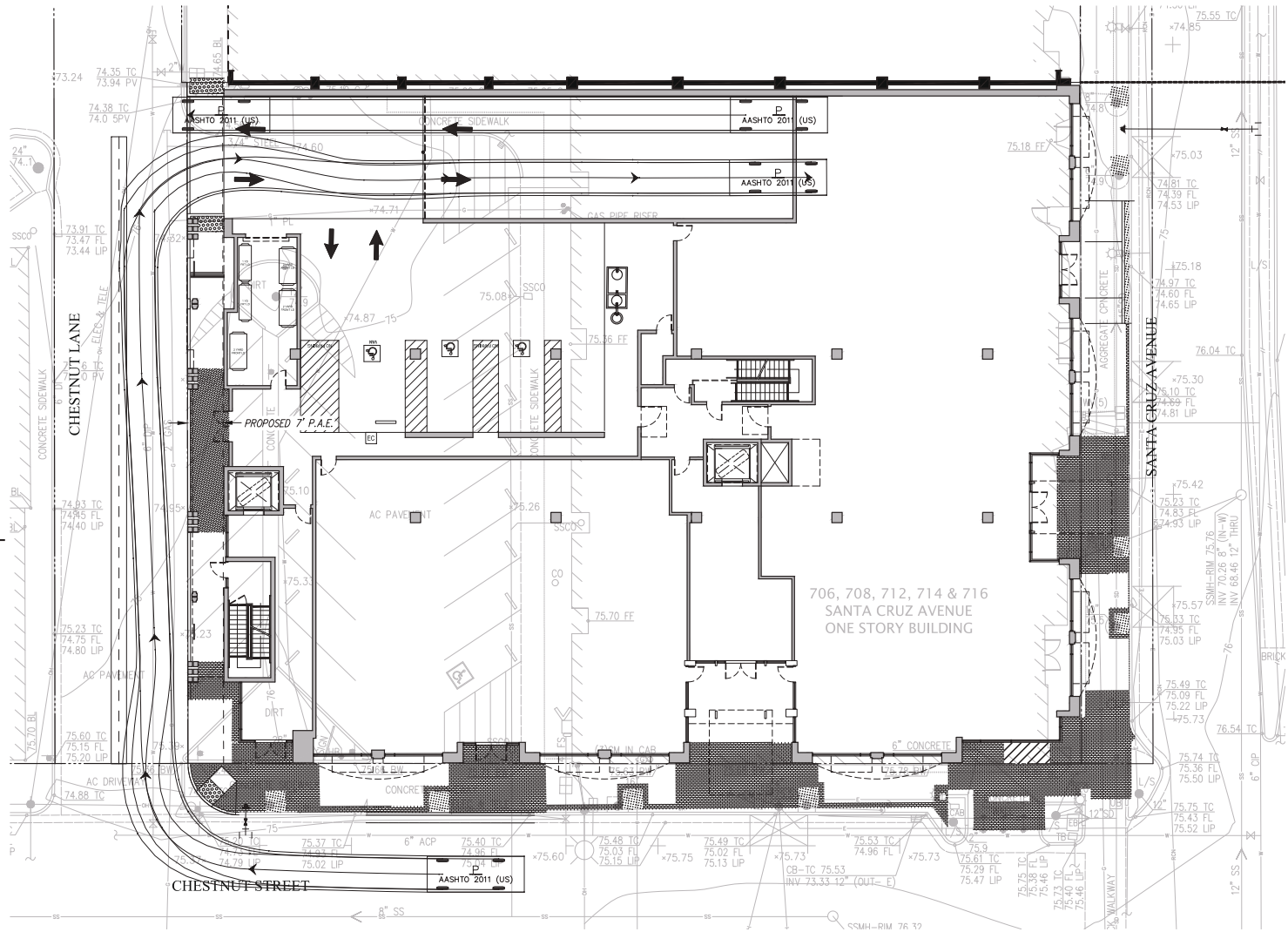
- BU BUBBLE LIP
- COMM COMMERCIAL
- DWS DOMESTIC WATER SERVICE
- EX EXISTING
- FOC FIRE DEPARTMENT CONNECTION
- FH FIRE HYDRANT
- FL FLOW LINE
- FS FIRE SERVICE
- INV INVERT ELEVATION
- IRR IRRIGATION
- OVD OVERFLOW DRAIN
- R RIDGE
- RES RESIDENTIAL
- RM RM ELEVATION
- SD STORM DRAIN
- SS SANITARY SEWER
- TW TOP OF WALL
- VERT VERTICAL
- WT WITH



P  
 Width : 7.00 feet  
 Track : 6.00  
 Lock to Lock Time : 6.0  
 Steering Angle : 31.6



0 5 10 20 30  
 Scale 1" = 10'



<p><b>PASSENGER VEHICLE TURNING</b>          OF  <b>706 SANTA CRUZ AVE</b>          FOR  <b>FORM4 ARCHITECTURE</b></p>	<p>CALIFORNIA  <b>MENLO PARK.</b></p>
DATE	MAY 2019
SCALE	AS SHOWN
DESIGNER	ST
DRAFTER	RS
JOB NO.	A14009-2
SHEET	C5.1
OF	9 SHEETS

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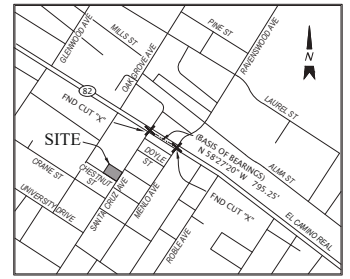
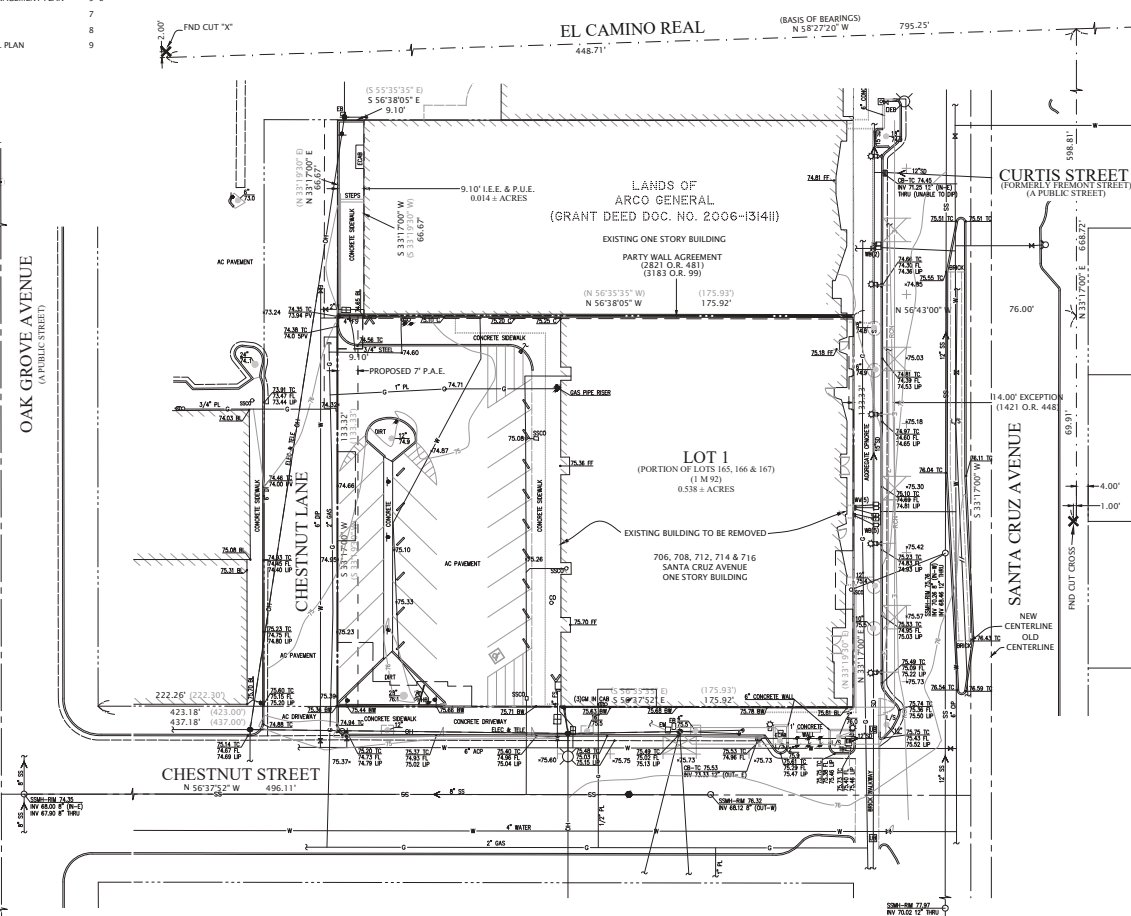


# VESTING TENTATIVE MAP

BEING A ONE LOT SUBDIVISION FOR CONDOMINIUM PURPOSES  
 NOT TO EXCEED FOUR (4) RESIDENTIAL UNITS & NOT TO EXCEED FOUR (4) COMMERCIAL PARCELS  
 WITH RIGHTS RESERVED FOR NOT TO EXCEED TEN (10) COMMERCIAL CONDOMINIUMS  
 706-716 SANTA CRUZ AVENUE, MENLO PARK, CALIFORNIA

## SHEET INDEX

EXISTING CONDITIONS	1
PROPOSED CONDITIONS LOWER LEVEL	2
PROPOSED CONDITIONS GROUND LEVEL	3
CONCEPTUAL GRADING, DRAINAGE & UTILITY PLAN	4
PRELIMINARY STORM WATER MANAGEMENT PLAN	5-6
VEHICLE ACCESS PLAN	7
FIRE ACCESS PLAN	8
CONCEPTUAL EROSION CONTROL PLAN	9



## NOTES

- RECORD OWNERS: 706-716 SANTA CRUZ AVENUE, LLC, 700 SANTA CRUZ AVENUE, MENLO PARK, CA 94025
- SUBDIVIDER: 706-716 SANTA CRUZ AVENUE, LLC, 700 SANTA CRUZ AVENUE, MENLO PARK, CA 94025, PHONE: (415) 260-9008, CONTACT: VASILE C. OROS
- MAP PREPARED BY: KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC., 3350 SCOTT BOULEVARD, BUILDING 22, SANTA CLARA, CA 95054, PHONE: (408) 727-6665, CONTACT: JIMMY R. VICIL, LS 6256, 071-102-250, RETAIL, OFFICE/RESIDENTIAL, SP-ECR/D, EL CAMINO REAL/DOWNTOWN SPECIFIC PLAN, NO CHANGE, MIXED-USE, 1, NOT TO EXCEED FOUR (4), NOT TO EXCEED FOUR (4)
- A.P.N.
- EXISTING USE:
- PROPOSED USE:
- EXISTING ZONING:
- PROPOSED ZONING:
- GENERAL PLAN:
- PROPOSED NUMBER OF LOTS:
- PROPOSED NUMBER OF RESIDENTIAL UNITS:
- PROPOSED NUMBER OF COMMERCIAL PARCELS:
- MAXIMUM NUMBER OF POSSIBLE COMMERCIAL CONDOMINIUM UNITS: TEN (10)
- TOTAL ACRES: 0.538 - ACRES
- ALL DISTANCES ARE APPROXIMATE
- NO NEW STREET NAMES PROPOSED
- THIS TENTATIVE MAP WAS PREPARED FROM INFORMATION FURNISHED IN A PRELIMINARY TITLE REPORT PREPARED BY OLD REPUBLIC TITLE COMPANY, ORDER NUMBER 0626029434-JG, DATED MARCH 21, 2017.
- UTILITY CONNECTION AND SIZING INFORMATION IS NOT SHOWN AT THIS TIME DUE TO SCHEMATIC NATURE OF SITE PLAN.
- FLOOD ZONE NOTE: THE SUBJECT PROPERTY IS SHOWN ON THE FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NUMBER 060321 0308 E, DATED OCTOBER 16, 2012, AS BEING LOCATED IN FLOOD ZONE "X". AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.
- DISK SET IN MASSIVE STRUCTURE, 0.1 MILE SOUTHWEST OF SOUTHERN PACIFIC COMPANY RAILROAD STATION, AT THE INTERSECTION OF SANTA CRUZ AVENUE AND EL CAMINO REAL AT THE ELLIOT BUILDING, IN THE TOP PROJECTION OF FOUNDATION. ELEVATION: 73.95 FEET (DATUM) NAVD 1988
- BASIS OF BEARINGS: THE BEARING OF NORTH 58°27'20" WEST TAKEN ON THE MONUMENT LINE OF EL CAMINO REAL AS SHOWN ON THAT CERTAIN PARCEL MAP FILED FOR RECORD ON FEBRUARY 14, 1991 IN BOOK 64 OF PARCEL MAPS AT PAGES 57-58, SAN MATEO COUNTY RECORDS WAS TAKEN AS THE BASIS OF ALL BEARINGS SHOWN HEREON.
- UTILITIES: CITY OF MENLO PARK: SANITARY SEWER: WEST BAY SANITARY DISTRICT; WATER: CALIFORNIA WATER COMPANY; GAS: PACIFIC GAS & ELECTRIC COMPANY; ELECTRIC: PACIFIC GAS & ELECTRIC COMPANY; TELEPHONE: AT&T; CABLE: COMCAST; FIRE: MENLO PARK FIRE PROTECTION DISTRICT

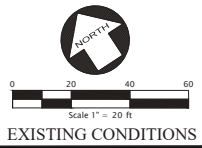
## ABBREVIATIONS

AC	ASPHALTIC CONCRETE
BL	BUILDING
BW	BACK OF WALK
CB	CATCH BASIN
DOC.	DOCUMENT
EB	ELECTRIC BOX
ECAB	ELECTRIC CABINET
FL	FLOW LINE
FND	FOUND
FS	FIRE SPRINKLER ALARM
HB	HOSEBOX
I.E.E.	INGRESS/EGRESS EASEMENT
LIP	LIP OF GUTTER
NO.	NUMBER
O.R.	OFFICIAL RECORD
P.A.E.	PEDESTRIAN ACCESS EASEMENT
P.U.E.	PUBLIC UTILITY EASEMENT
SSCO	SANITARY SEWER CLEAN OUT
SMH	SANITARY SEWER MANHOLE
TB	TELEPHONE BOX
TC	TOP OF CURB
WB	WATER BOX

## LEGEND

PROPERTY LINE	---	GAS METER	⊙	INDEX CONTOUR	—15—
ADJACENT PROPERTY LINE	---	UTILITY POLE W/ GUY WIRE	⊙	CURB	=====
CENTERLINE	---	VALVE	⊙	CURB & GUTTER	=====
TIE LINE	---	CATCH BASIN / DROP INLET	⊙	CONCRETE	=====
MONUMENT LINE	---	WATER METER	⊙	SANITARY SEWER	SS
BUILDING LINE WITH DOOR	---	FIRE DEPARTMENT CONNECTION	⊙	STORM DRAIN	SD
BUILDING OVERHANG	---	UTILITY BOX (SIZE VARIES)	⊙	WATER	W
LIGHT	⊙	SIGN	⊙	RECLAIMED WATER	RW
FIRE HYDRANT	⊙	WATER W/ SIZE AND ELEVATION	⊙	GAS	G
STORM DRAIN MANHOLE	⊙	SPOT ELEVATION	⊙	UNDERGROUND ELECTRIC	OE
MANHOLE	⊙	AERIAL SPOT ELEVATION	⊙	OVERHEAD	OH
CLEAN OUT	⊙	CONTOUR	⊙	LIGHTING CONDUIT	LC

PREPARED BY: *J.R. Vicil*  
 PRINT NAME: JIMMY R. VICIL, PLS 6256  
 TITLE: ASSOCIATE  
 DATE: 9-5-2019



SHEET 9 OF 9  
 REVISION  
 KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.  
 3350 Scott Boulevard, Building 22  
 Santa Clara, California 95054  
 (408) 727-6665  
 (408) 727-3661  
 CALIFORNIA  
 VESTING TENTATIVE MAP  
 FOR: 706-716 SANTA CRUZ AVENUE, LLC.  
 706-716 SANTA CRUZ AVENUE  
 MENLO PARK  
 DATE: SEPT., 2019  
 SCALE: 1" = 20'  
 DESIGNER: JR  
 DRAFTER: JF  
 JOB: A14009-2  
 SHEET: C1.0





**LEGEND**

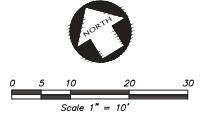
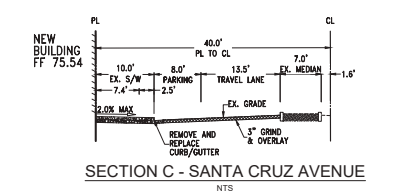
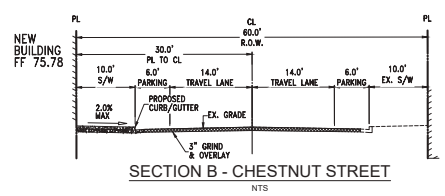
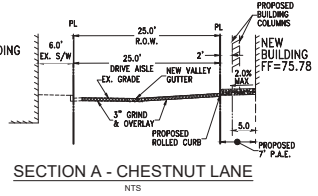
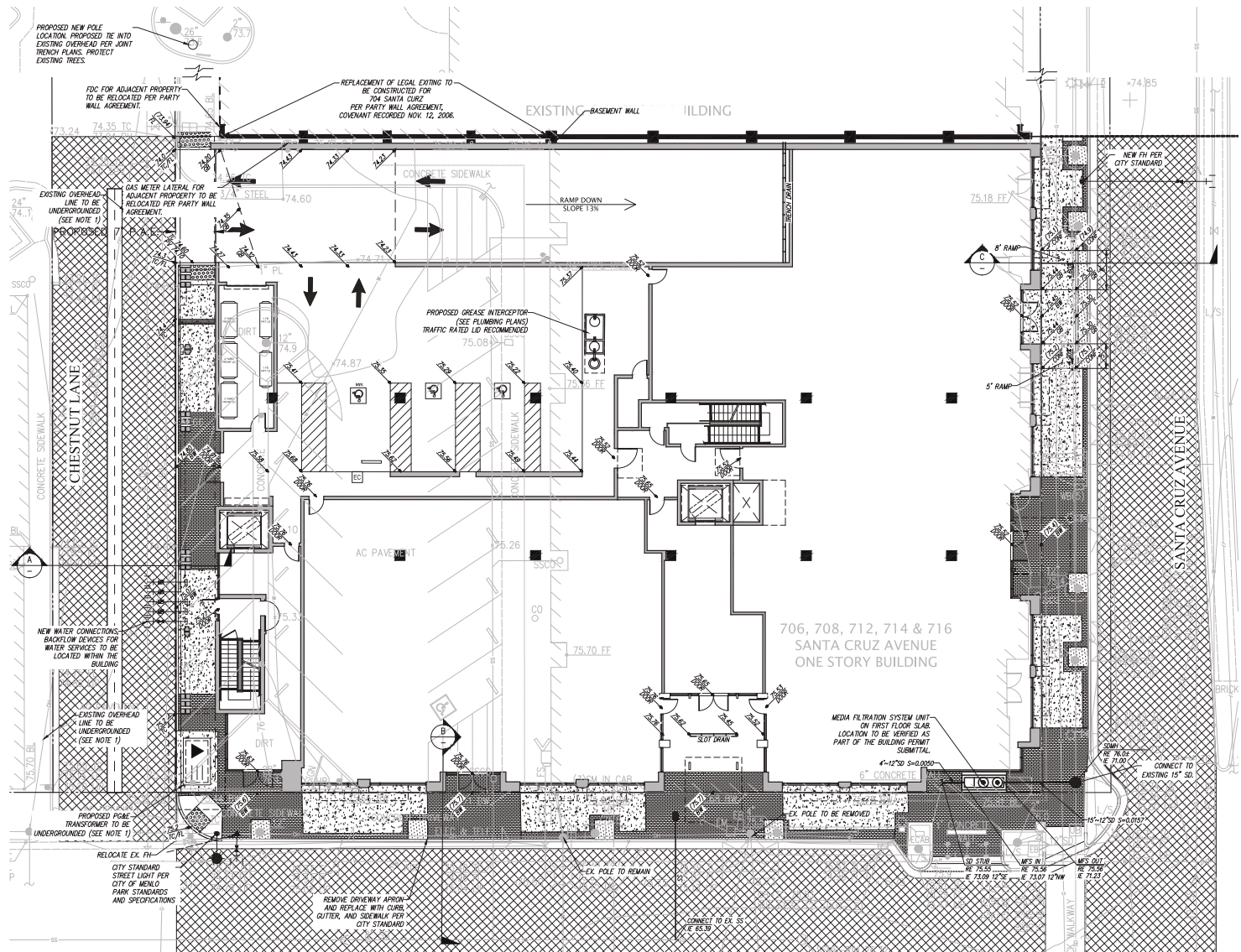
PROPERTY LINE	---
ADJACENT PROPERTY LINE	----
CENTERLINE	=====
EASEMENT	-----
NON-ACCESS	-----
PUMP	⊕
STORM DRAIN MANHOLE	⊙
CATCH BASIN	⊙
MEDIA FILTRATION SYSTEM	⊙
SANITARY SEWER MANHOLE	⊙
FIRE HYDRANT(FHN)/WET BARREL TYPE WITH ONE 4 1/2 INCH CONNECTION AND TWO 2 1/2 INCH CONNECTIONS	⊙
GATE VALVE	I
WATER METER	W
POST INDICATOR VALVE	P
FIRE DISTRICT CONNECTION	▲
SPRINKLER SYSTEM RISER	▲
BACK FLOW PREVENTER	▲
CHECK VALVE	N
REDUCER	▲
LIGHT	⬤
GAS METER	⊕
TRANSFORMER	⊕
SPOT ELEVATION	▲
HANDICAP STALL	⊕
CURB	—
CURB & GUTTER	—
RED CURB	—
RIDGE	—
SANITARY SEWER	SS
STORM DRAIN GRAVITY FLOW	SD
STORM DRAIN FORCED FLOW	SDFM
DOMESTIC WATER SERVICE	DW
FORCE MAIN	FM
FIRE SERVICE	FS
WATER	W
ELECTRIC	E
GAS	G
JOINT TRENCH	JT
PCC SIDEWALK	▨
TRUNCATED DOMES	▨
BIO-TREATMENT PLANTING	▨
3" GRIND & OVERLAY	▨

**ABBREVIATIONS**

BU	BUBBLE UP
COMM.	COMMERCIAL
DWS	DOMESTIC WATER SERVICE
EX	EXISTING
FDC	FIRE DEPARTMENT CONNECTION
FH	FIRE HYDRANT
FL	FLOW LINE
FS	FIRE SERVICE
INV	INVERT ELEVATION
IRR	IRRIGATION
OVD	OVERFLOW DRAIN
R	RIDGE
RES.	RESIDENTIAL
RM	RIM ELEVATION
SD	STORM DRAIN
SS	SANITARY SEWER
TW	TOP OF WALL
VERT.	VERTICAL
W	WITH

**NOTES**

- ALL OVERHEAD COMMUNICATION LINES ARE TO BE UNDERGROUND PER CITY AND LOCAL UTILITY COMPANY'S STANDARDS AND SPECIFICATIONS. ALL UTILITIES TO BE UNDERGROUNDED ALONG THE PROJECT FRONTAGE SHALL BE TO THE SATISFACTION OF THE CITY OF MENLO PARK AND THE FIRE DISTRICT.
- PUBLIC SIDEWALK ACCESS TO ADJACENT PROPERTY TO BE COORDINATED WITH THE CITY OF MENLO PARK AS PART OF PERMIT DRAWINGS.
- DESIGN AND CONFIGURATION OF THE ALLEY-TYPE DRIVE AISLE ON CHESTNUT LANE SHALL TO BE COORDINATED WITH THE CITY OF MENLO PARK AND ISSUED AS PART OF PERMIT DRAWINGS.
- EXISTING STREET LIGHTS SHALL BE REFURBISHED AND PAINTED PER CITY OF MENLO PARK STANDARDS AND SPECIFICATIONS. STREET LIGHTS SHALL BE UPGRADED TO LED FIXTURES.
- ALL EXISTING UTILITIES WITHIN PUBLIC SIDEWALK SHALL BE PROTECTED IN PLACE UNLESS NOTED OTHERWISE.
- BACKFLOW DEVICES SHALL BE LOCATED ON THE GARAGE FLOOR ELECTRICAL ROOM.
- REFER TO SHEET 2.0 FOR ON-SITE DESIGN INFORMATION.
- IRRIGATION TO ALL EXISTING AND NEW TREES ALONG THE SITE FRONTAGES WILL BE CONNECTED TO THE ON-SITE WATER SYSTEM.



NO.	REVISION
NO.	REVISION
NO.	REVISION
NO.	REVISION

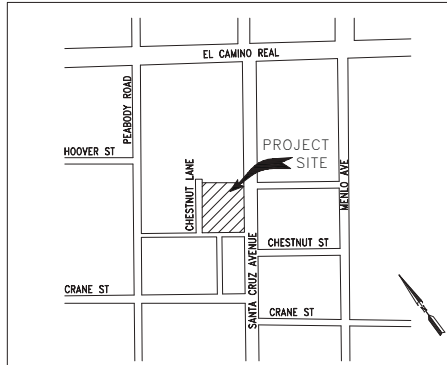
**KIER & WRIGHT**  
 CIVIL ENGINEERS & SURVEYORS, INC.  
 2858 Collier Canyon Road  
 Menlo Park, CA 94025  
 Phone (650) 245-8788  
 Fax (650) 245-8796  
 www.kierwright.com

PROPOSED CONDITIONS GROUND LEVEL OF  
**706 SANTA CRUZ AVE**  
 FOR  
**FORM 4 ARCHITECTURE**  
 CALIFORNIA  
 MENLO PARK.

DATE: MAY 2019  
 SCALE: AS SHOWN  
 DESIGNER: ST  
 DRAFTER: RS  
 JOB NO.: A14009-2  
 SHEET: 3  
 OF 4 SHEETS







VICINITY MAP  
N. T. S.

WORK RESPONSIBILITY  
JOINT TRENCH

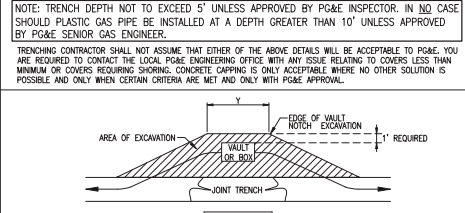
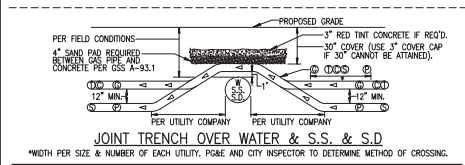
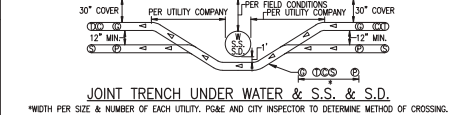
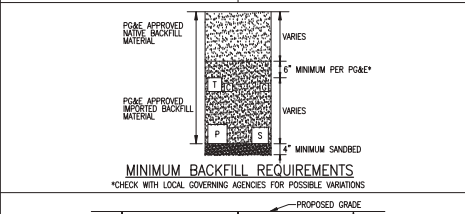
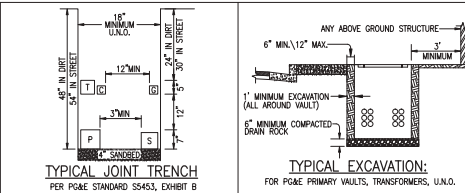
TRENCHING Excavate & Backfill.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
GAS MATERIAL Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
ELECTRIC CABLE Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
ELECTRIC CONDUIT Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
ELECTRIC BOXES Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
ELECTRIC TRANSFORMER PADS Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
ELECTRIC SWITCHGEAR & TRANSFORMER Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
TELEPHONE CONDUIT Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
TELEPHONE CABLE Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
TELEPHONE SPICE BOXES Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
TELEPHONE S&J PAD Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
C.A.T.V. CONDUIT Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
C.A.T.V. SPICE BOXES Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
C.I.E.C. FIBER CONDUIT Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
C.I.E.C. FIBER SPICE BOXES Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR
DIRECTIONAL DRILL / JACK AND BORE Supply & Install.....	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	PG&E LOCAL CALITY	CONTRACTOR

● SYMBOL DESIGNATES THE WORK TO BE PERFORMED BY THE RESPECTIVE CONTRACTOR & UTILITY COMPANIES.  
○ NOT APPLICABLE UNLESS OTHERWISE SPECIFIED.  
\* PG&E TO PULL CABLE INTO ENERGIZED ENCLOSURES

THESE PLANS WERE PREPARED IN CONJUNCTION WITH THE FOLLOWING PLANS:

CIVIL IMPROVEMENT PLANS/GRADING PLANS	03-18-2018	PRELIMINARY
ARCHITECTURAL ELECTRONIC FILE	08-26-2019	PRELIMINARY
APPLICANT DESIGN (GAS)		
APPLICANT DESIGN (ELECTRIC)		
TELEPHONE		
C.A.T.V.		
LANDSCAPE	08-28-2019	PRELIMINARY
LIGHT LOCATIONS		

**VIZION UTILITY PARTNERS is not responsible for any substantial changes or relocations.** OTHER UTILITIES SHOWN ARE APPROXIMATED AND BASED ON FIELD SURVEY AND AVAILABLE UTILITY INFORMATION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION AND EXTENT OF UTILITIES PRIOR TO THE COMMENCEMENT OF WORK. PHYSICAL VERIFICATION OF UTILITY LOCATIONS SHALL BE PERFORMED BY CAREFUL PROBING OR HAND DIGGING IN ACCORDANCE WITH ARTICLE 6 OF THE CALIFORNIA CONSTRUCTION SAFETY ORDERS.



BOX EXCAVATION	NOTE:
PRIMARY BOX SIZE 7' x 7'	DISTANCE VARIES PER FIELD CONDITIONS
4'-6" x 8'-6"	11'

ELECTRIC CONDUIT MINIMUM BEND RADIUS			
CONDUIT DIAMETER	VERTICAL RADIUS	HORIZONTAL RADIUS	
3"	24"	36"	
4"	24"	36"	
6"	36"	60"	

NOTE: 315° MAX BENDS IN ANY SECONDARY CONDUIT RUN 200' OR LESS.  
300° MAX BENDS IN ANY PRIMARY CONDUIT RUN.

TYPICAL GAS METER REQUIREMENTS*						
METER TYPE	LOAD (SFPH)	DELIVERY PRESSURE* (PSIG)	PAD SIZE (INCHES)	MIN. WIDTH REQUIRED TO METER X (INCHES)	DISTANCE FROM RISER TO FINISHED WALK (INCHES)	MIN. HOUSING/NE STUB OUT (INCHES)
TYPICAL RESIDENTIAL	0-350	0-600	N/A UNLESS USING FLEX-HOSE METER	24	6 TO 9	4
400 TO 1000	351-1,400	0.25	N/A UNLESS USING FLEX-HOSE METER	30	6 TO 9	6
1.5M OR 3M ROTARY	1,401-5,000	APPROVED BY PG&E	40 X 36 X 4	52	20	VARIES
5M OR 7M ROTARY	3,001-7,000	APPROVED BY PG&E	78 X 36 X 4	90	20	VARIES
11M OR 16M ROTARY	7,001-16,000	APPROVED BY PG&E	94 X 36 X 4	106	20	VARIES

\*ACTUAL METER-SET CONFIGURATIONS MAY DIFFER DEPENDING ON FIELD CONDITIONS AND RESTRICTIONS. FOR GAS METER DETAILS, SEE SECTION 2 OF CURRENT PG&E GAS SERVICE REQUIREMENTS GREENBOOK BY PG&E. PG&E STANDARD METER SPACING REQUIREMENTS DO NOT INCLUDE CLEARANCE FOR EXHAUSTION VALVES OR OTHER ADDITIONAL SAFETY EQUIPMENT. SEE NOTE 17 UNDER GENERAL NOTES ON SHEET AT-1.

\*\*DELIVERY PRESSURE TO BE CONFIRMED IN BUILDING PERMITS AND MECHANICAL PLANS. PG&E WARRANTS SITE AUTHORITY TO DETERMINE IF THE ELEVATED DELIVERY-PRESSURE SERVICE IS AVAILABLE AT A SPECIFIC LOCATION.

GENERAL NOTES:

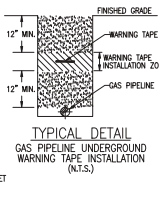
1. THE PREFERRED TRENCH LOCATION IS IN A PUBLIC UTILITY EASEMENT (P.U.E.).
2. ALL DEPTHS AND RESULTING COVER REQUIREMENTS ARE MEASURED FROM FINAL GRADE.
3. COVER, CLEARANCES AND SEPARATION SHALL BE AS GREAT AS PRACTICABLE UNDER THE CIRCUMSTANCES, BUT UNDER NO CIRCUMSTANCES SHALL BE LESS THAN THE MINIMUM COVER CLEARANCE AND SEPARATION REQUIREMENTS SET FORTH IN GENERAL ORDER 128 AND 4907R 192.321, 4907R 192.325, AND 4907R 192.371. ALL FACILITIES SHALL BE MAINTAINED IN PLACE OR RELOCATED TO OTHER AREAS SHALL BE TAKEN TO ENSURE NO MOTION OF THE FACILITIES. DIMENSIONAL REQUIREMENTS FOR SHADING, LEVELING, AND BACKFILLING SHALL BE DETERMINED SUBSEQUENT TO COMPLETION.
4. TRENCH DIMENSIONS SHOWN ARE TYPICAL. TRENCH SIZES AND CONFIGURATIONS MAY VARY DEPENDING UPON OCCUPANCY AND/OR FIELD CONDITIONS. TRENCH SIZE AND CONFIGURATION MUST AT ALL TIMES BE CONSTRUCTED IN A MANNER THAT ENSURES PROPER CLEARANCES AND SEPARATIONS. TRENCHES SHALL BE TAKEN TO ENSURE NO MOTION OF FACILITIES AND CONFIGURATIONS AS SHOWN IN THIS EXHIBIT MUST BE DESIGNED TO ENSURE THIS REQUIREMENT.
5. IT IS PREFERRED TO HAVE NON-PG&E OWNED STREETLIGHTS AT A LEVEL OTHER THAN THE GAS OR ELECTRIC LEVEL. NON-PG&E OWNED STREETLIGHTS MAY BE AT THE ELECTRIC LEVEL OF THE TRENCH AS LONG AS MINIMUM CLEARANCES ARE PROVIDED AND COMPLY WITH ALL SPECIAL NOTES FOR A JOINT TRENCH WITH A SECOND ELECTRIC UTILITY.
6. NON-UTILITY FACILITIES ARE NOT ALLOWED IN ANY JOINT UTILITY TRENCH, E.G., IRRIGATION CONTROL LINES, BUILDING FIRE ALARM SYSTEMS, PRIVATE TELEPHONE SYSTEMS, OUTDOOR ELECTRICAL CABLE, ETC.
7. WHEN COMMUNICATION DUCTS ARE INSTALLED, A MINIMUM OF 12" RADIAL SEPARATION SHALL BE MAINTAINED FROM GAS FACILITIES. EXCEPTION: WITH MUTUAL AGREEMENT, 4" MIN. DIAMETER OR SMALLER GAS PIPES SHALL BE INSTALLED, THE SEPARATION MAY BE REDUCED TO NOT LESS THAN 6 INCHES.
8. PROVIDE SEPARATION FROM TRENCH WALL AND OTHER FACILITIES SUFFICIENT TO ENSURE PROPER COMPACTION.
9. MAINTAIN PROPER SEPARATION BETWEEN POE FACILITIES AND "NET" UTILITY LINES AS DESCRIBED IN UO STANDARD 55453. THE MINIMUM ALLOWABLE HORIZONTAL SEPARATION BETWEEN COMPANY FACILITIES AND "NET" FACILITIES IS 3" WITH A MINIMUM 1" OF UNDISTURBED EARTH OR THE INSTALLATION OF A SUITABLE BARRIER BETWEEN THE FACILITIES. IF ANY 2-DIMENSIONAL SEPARATION CANNOT BE ATTAINED BETWEEN "NET" UTILITIES AND COMPANY FACILITIES, A VARIANCE MAY BE APPROVED BY THE LOCAL INSPECTION SUPERVISOR AND SUBMITTED TO THE SERVICE PLANNING SUPPORT PROGRAM MANAGER FOR APPROVAL. SEPARATIONS OF 7" OR LESS ARE NOT PERMISSIBLE AND WILL NOT BE ALLOWED. THE COMPANY MAY AGREE TO MAINTAIN A MINIMUM 3" SEPARATION AT THE REQUEST OF AN APPLICANT IF WARRANTED AND THE NEED IS JUSTIFIED. THE REQUEST FOR A VARIANCE MUST:
  - BE MADE IN WRITING AND SUBMITTED TO THE COMPANY AIDE DURING THE PLANNING AND DESIGN PHASE OF THE PROJECT.
  - CLEARLY DESCRIBE THE CONDITIONS NECESSITATING THE VARIANCE.
  - INCLUDE A PROPOSED BARRIER AND INCLUDE BARRIER BETWEEN THE "NET" UTILITIES AND COMPANY FACILITIES IN THE EVENT 1" OF UNDISTURBED EARTH CANNOT BE MAINTAINED. NET DRAIN LINES CONNECTED TO DRAINPOITS ON BUILDINGS ARE CONSIDERED A "NET" UTILITY FOR THE PURPOSES OF THIS STANDARD.
10. SEPARATIONS SHALL BE MAINTAINED AT ABOVE COVER TERMINATION POINTS.
11. PROCEDURES FOR APPROVING NATIVE BACKFILL FOR SHADING OF POE GAS FACILITIES:
  - RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF 3 LOCATIONS PER 1,000' OF TRENCH. 100% OF THE SAMPLE MUST PASS THROUGH A 1/2" SEIVE AND 75% MUST PASS THROUGH A #4 SCREEN. ADDITIONAL SAMPLES MUST BE TAKEN IF EXISTING CONDITIONS SHOWN ARE TO BE TAKEN AT THE DISCRETION OF THE POE REPRESENTATIVE ON SITE.
  - THE SOILS MUST NOT CONTAIN ANY ROCKS THAT HAVE SHARP EDGES OR THAT MAY OTHERWISE BE ABRASIVE.
  - THE SOILS MUST NOT CONTAIN CLOSERS LARGER THAN 1/2" IF TO BE USED AS SHADING, BEDDING OR LEVELING.
  - COMPACTION REQUIREMENTS MUST MEET ANY APPLICABLE PG&E, FEDERAL, STATE, COUNTY, OR LOCAL REQUIREMENTS. AT ALL TIMES THE OVER SATURATION OF NATIVE SOILS BE USED TO ACHIEVE THESE REQUIREMENTS.
  - THE SEIVES AND SCREENS SHALL BE:
    - 1/2" SEIVE: 8" DIAMETER BY 2' DEEP, STAINLESS STEEL WESH SCREEN.
    - #4 SCREEN: 8" DIAMETER BY 2' DEEP, STAINLESS STEEL WESH SCREEN.
12. PROCEDURES FOR APPROVING NATIVE BACKFILL FOR SHADING AT POE ELECTRIC FACILITIES:
  - RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF 3 LOCATIONS PER 1,000' OF TRENCH. ADDITIONAL SAMPLES SHALL BE TAKEN IF EXISTING CONDITIONS SHOWN ARE TO BE TAKEN AT THE DISCRETION OF THE POE REPRESENTATIVE ON SITE.
  - SHADING LARGER ROCK, PAWING MATERIAL, CORNERS, SHARPLY ANGULAR SUBSTANCES, OR CORROSIVE MATERIAL SHALL NOT BE PLACED IN THE TRENCH WHERE SUCH MATERIAL MAY DAMAGE THE CONDUITS AND/OR PREVENT PROPER COMPACTION OF SOIL AROUND THE CONDUIT OR THE TRENCH WALLS.
  - NATIVE SOILS NOT CONTAINING CLOSERS NOT TO EXCEED 6" IN DIAMETER MAY BE INCLUDED IN THE SHADING MATERIAL PROVIDED THE CLOSERS ARE READILY BREAKABLE BY HAND. NOTE: SOILS CONSISTING PRIMARILY OF ADOBE, HARD COMPACT (GENE) CLAY, AND SAND MUST NOT BE USED AS SHADING MATERIAL.
  - AT NO TIME SHALL THE OVER SATURATION OF NATIVE SOILS BE USED TO ACHIEVE THESE REQUIREMENTS.
  - CONTACT THE ENGINEERING DOCUMENT REVIEWER FOR MORE INFORMATION ON PAGE 2.
13. COMPETENT NATIVE SOILS ARE PREFERRED TO BE USED FOR SHADING, BEDDING, AND BACKFILL THROUGHOUT THE TRENCH.
14. NATIVE SOILS EXCEEDING 1/2" MINUS AND/OR WHERE GAS IS TO BE PLACED AT THE BOTTOM OF A TRENCH IN AREAS THAT EXCEED 1/2" MINUS AND/OR WHERE GAS IS TO BE PLACED AT THE BOTTOM OF A TRENCH IS CONSIDERED TO CONSIST OF "HARD" NATIVE IMPORT MATERIAL. 1/2" MINUS IMPORT MATERIAL SHALL BE USED FOR SHADING AND/OR BEDDING OF GAS FACILITIES.
15. HARD NATIVE IMPORT MATERIAL IS PER COST ENGINEERING GUIDELINE 4128.
16. IF A LEVELING COURSE IS REQUIRED FOR GAS FACILITIES, THE USE OF NATIVE SOILS IS PREFERRED, BUT IF 1/2" MINUS CONDITIONS ARE NOT ATTAINABLE WITH THE NATIVE SOILS, THEN THE USE OF PG&E APPROVED IMPORT MATERIALS IS REQUIRED. BEDDING UNDER GAS FACILITIES WILL BE A MINIMUM OF 1/2" OF COMPACTED 1/2" MINUS NATIVE SOILS OR PG&E APPROVED IMPORT MATERIAL.
17. THE USE OF ANY IMPORTED MATERIAL FOR BACKFILLING PURPOSES SHALL BE LIMITED TO THOSE SITUATIONS WHEN NATIVE SOILS TO NOT ALLOW FOR REQUIRED COMPACTION.
18. THE APPLICANT IS RESPONSIBLE FOR THE REMOVAL OF EXCESS SPILL AND ASSOCIATED COSTS.
19. SEPARATION BETWEEN GAS FACILITIES AND ELECTRIC FACILITIES MAY BE REDUCED TO 6" WHEN CROSSING.
20. SERVICE SADDLES ARE THE PREFERRED SERVICE FITTINGS FOR USE THROUGHOUT THE JOINT TRENCH PROJECT, ALL PROJECTS WILL BE DESIGNED AND ESTIMATED USING SERVICE SADDLES. HOWEVER, SERVICE TEES MAY BE USED IF ALL CLEARANCES, SEPARATION, AND COVERAGE REQUIREMENTS ARE MAINTAINED.
21. CONTRACTOR TO INCREASE METER SPACING AS NECESSARY WHEN EXHAUSTION VALVES OR OTHER ADDITIONAL SAFETY EQUIPMENT ARE REQUIRED. EXHAUSTION VALVES ARE REQUIRED IN SOME AREAS AND ARE NOT PART OF PG&E/UTON STANDARD SPACING REQUIREMENTS DO NOT INCLUDE CLEARANCE FOR EXHAUSTION VALVES.

TRANSFORMER CLEARANCE REQUIREMENTS:

1. ABOVE ANY SINGLE PHASE TRANSFORMER LOCATION, MAINTAIN 20' UNOBSTRUCTED OVERHEAD CLEARANCE OVER TRANSFORMER WALK/PAD.
2. ABOVE ANY THREE PHASE TRANSFORMER LOCATION, MAINTAIN 30' UNOBSTRUCTED OVERHEAD CLEARANCE OVER TRANSFORMER WALK/PAD.

GAS PIPELINE UNDERGROUND WARNING TAPE NOTES:

1. A WARNING TAPE IS TO BE INSTALLED IN OPEN TRENCH INSTALLATION OVER GAS PIPELINES IN BOTH TRENCHES AND UNBROKEN TRENCHES. INCLUDES TRENCHES, BELL HOLES, EXCAVATIONS FOR REPAIR PURPOSES AND RISER REQUIREMENTS. THE WARNING TAPE IS INSTALLED AT THE BOTTOM OF DIGGING IN THE "TOLERANCE ZONE" TO STRIKE THE WARNING TAPE PRIOR TO THE PENETRATION. WHEN THE WARNING TAPE IS PENETRATED AND DAMAGED WITH EXCAVATING EQUIPMENT IT STRIKES WITHOUT BREAKING, THIS ALERTING THE EXCAVATOR OF THE GAS FACILITY LOCATION.
2. INSTALL 6" WIDE WARNING TAPE ABOVE THE GAS PIPELINE AT LEAST 12" BELOW GRADE, AND NO CLOSER THAN 12" FROM THE PIPE. INSTALLATION SHOULD BE MADE BETWEEN THE FINISHED WALK AND THE TRENCH WALL AS POSSIBLE. INSTALL THE TAPE ALONG THE LENGTH OF THE EXCAVATION. EXCEPT WHEN JOINT TRENCH DESIGN DOES NOT ALLOW FOR INSTALLATION OF WARNING TAPE WITH THE WARNING TAPE INSTALLATION ZONE, THE WARNING TAPE IS TO BE MAINTAINED 6" ABOVE THE GAS PIPELINE, AND NOT THE FACILITY ABOVE THE PIPE.
3. WARNING TAPE SHALL BE BRIGHTLY COLORED YELLOW AND MARKED "CAUTION: GAS LINE BURIED BELOW. MARKING SHALL BE PERFORMED AT INSTALLATION.
4. WARNING TAPE SHALL BE STORED IN SUCH A MANNER THAT LIMITS ULTRAVIOLET (UV) EXPOSURE.



PG&E PM#: \_\_\_\_\_  
ELECTRIC: \_\_\_\_\_  
GAS: \_\_\_\_\_

DESIGN CHANGE COMPONENT  
ANY CHANGES TO THIS DESIGN MUST BE APPROVED BY  
PG&E GAS AIDE

CONSTRUCTION NOTES:

1. ALL TRENCHING, BACKFILLING AND INSTALLATION BY CONTRACTOR MUST COMPLY WITH PG&E UO STANDARD 55453 (EFFECTIVE DATE 7-5-2006).
2. ALL WORK MUST COMPLY WITH P.O. & E. TELEPHONE, C.A.T.V., STANDARDS AND PRACTICES. ALL WORK MUST BE INSPECTED AND APPROVED BY RESPECTIVE INSPECTORS. RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF THREE LOCATIONS PER 1,000' OF TRENCH. 100% OF THE SAMPLE MUST PASS THROUGH A 1/2" SEIVE AND 75% MUST PASS THROUGH A #4 SCREEN. ADDITIONAL SAMPLES MUST BE TAKEN IF EXISTING SOIL CONDITIONS CHANGE AND IT IS TO BE AT THE DISCRETION OF THE POE REPRESENTATIVE ON SITE. THE SOILS MUST NOT CONTAIN ANY ROCKS THAT HAVE SHARP EDGES OR THAT MAY OTHERWISE BE ABRASIVE. THE SOILS MUST NOT CONTAIN CLOSERS LARGER THAN 1/2" IF TO BE USED AS SHADING, BEDDING OR LEVELING MATERIALS. CONTRACTOR REQUIREMENTS MUST MEET ANY APPLICABLE PG&E, FEDERAL, STATE, COUNTY OR LOCAL REQUIREMENTS. ANY NATIVE SOILS OR IMPORT MATERIALS USED MUST NOT VIOLATE THESE REQUIREMENTS.
3. BACKFILL SHALL BE APPROVED BY THE UTILITY COMPANIES AND THE CITY. COMPACTION WILL BE TESTED AND PASSED BY THE SOILS ENGINEER.
4. IF SOIL IS NOT ROCK FREE, ADD 4" DEPTH OF TRENCH FOR SAND BEDDING.
5. VERIFY SPICE BOX EXCAVATION SIZES WITH SUPPLIERS).
6. THE TRENCHING CONTRACTOR SHALL COORDINATE THE UTILITY COMPANIES' INSTALLATION. THE TRENCHING CONTRACTOR TO PLACE CONNECTING CONDUIT WITHIN 5' OF BUILDING EXTERIOR WALL.
7. CONTRACTOR SHALL MAKE HIMSELF FAMILIAR WITH THE PROJECT IMPROVEMENT PLANS AND CONDUCT HIS WORK ACCORDINGLY.
8. IT IS THE TRENCHING CONTRACTOR'S RESPONSIBILITY TO PROTECT IN PLACE ALL EXISTING FACILITIES. NO EXTRA PAYMENT WILL BE CONSIDERED FOR CROSSING OTHER SYSTEMS.
9. VIZION UTILITY PARTNERS ASSUMES NO RESPONSIBILITY FOR THE PROJECT CONDITIONS. THESE DRAWINGS WERE PREPARED USING DATA SUPPLIED BY PG&E, TELEPHONE, C.A.T.V., IMPROVEMENT PLANS AND THE CITY'S VARIOUS "AS BUILT" INFORMATION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PHYSICALLY REVIEW THE PROJECT PRIOR TO SUBMITTING HIS BID.
10. CONTRACTOR WILL COMPLY WITH ALL LAWS, ORDINANCES AND REGULATIONS. CONTRACTOR SHALL BE FAMILIAR WITH O.S.H.A. INDUSTRIAL SAFETY PROCS AND SHALL CONDUCT HIS WORK ACCORDINGLY. WHEN WORKING NEAR ENERGIZED OR "HOT" EQUIPMENT, THE UTILITY OWNER SHALL BE NOTIFIED TO SUPPLY THE APPROPRIATE MAN POWER, PUBLIC SAFETY AND TRAFFIC CONTROL MEASURES ARE THE CONTRACTOR'S RESPONSIBILITY.
11. THE CONTRACTOR SHALL PROTECT CONSTRUCTION STAKING. HE SHALL COORDINATE STAKING WITH THE PROJECT'S CIVIL ENGINEER.
12. CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (USA) 2 WORKING DAYS PRIOR TO START OF WORK.
13. CONTRACTOR SHALL NOTIFY INSPECTORS OF ANY POTENTIAL CONFLICTS PRIOR TO START OF WORK.
14. THIS PLAN IS TO BE USED FOR SOLE PURPOSE OF DIGGING THE JOINT TRENCH. SEE PG&E, AT&T, AND COMCAST PLANS FOR EXACT SIZE AND NUMBER OF CONDUITS INSTALLED IN THE JOINT TRENCH. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THE CORRECT NUMBER, SIZE AND TYPES OF CONDUITS ARE INSTALLED PER THE ENGINEERED PLANS BY EACH UTILITY COMPANY.
15. NOTE PLANS ISSUED AT THE PRE-CONSTRUCTION MEETING MAY BE SUBJECT TO REVISIONS. IF FINAL PLANS FROM EACH UTILITY COMPANY WERE NOT AVAILABLE AT THE START OF CONSTRUCTION.
16. WATER, SEWER, IRRIAS, SANITARY WASTE, FUELS INCLUDING DIESEL AND GASOLINE, LIQ. PROPANE AND OTHER VOLATILE HEAVIER THAN AIR GASES, SPRINKLER IRRIGATION, STEAM AND OTHER "NET" FACILITIES SHALL MAINTAIN A MINIMUM OF THREE FEET FROM THE NEAREST OUTER SURFACE OF PG&E FACILITIES WITH NO LESS THAN ONE FOOT OF EARTH SOIL BARRIER BETWEEN THE ADJACENT SIDES OF THE INDIVIDUAL TRENCHES.
17. IN THE EXTRAORDINARY CASE THAT THE MINIMUM THREE FOOT HORIZONTAL SEPARATION CANNOT BE ATTAINED BETWEEN "NET" UTILITIES AND COMPANY DRILL FACILITIES, A VARIANCE MAY APPROVED BY THE LOCAL INSPECTION SUPERVISOR AND SUBMITTED TO THE SERVICE PLANNING SUPPORT PROGRAM MANAGER FOR APPROVAL.
18. ALL METER PANELS: INDIVIDUAL RESIDENTIAL OR NONRESIDENTIAL APPLICANTS WITH A METER PANEL RATING OF ANY SIZE, INSTALLED INSIDE A METER ROOM OR OTHER STRUCTURE, MUST FOLLOW ALL OF THE REQUIREMENTS DESCRIBED BELOW.
  1. INSTALL, OWN AND MAINTAIN A SEPARATE, NOMINAL, 2-INCH DIAMETER CONDUIT WITH FULL TAPES INSIDE OF THE METER PANEL OR SWITCHBOARD AT THE TOP OF THE METER SECTION.
  2. CONDUIT THE 2-INCH DIAMETER CONDUIT OR FULL TAPES EXTEND TO THE OUTSIDE OF THE BUILDING A MINIMUM OF 6 FEET AND A MAXIMUM OF 10 FEET ABOVE GRADE, THE OPEN END OF THE CONDUIT THAT IS EXPOSED TO THE OUTSIDE OF THE BUILDING MUST BE PROTECTED BY A TEMPORARY CAP OR PLUG.
  3. DO NOT USE THE CONDUIT, THE CONDUIT IS FOR PG&E'S METERING EQUIPMENT ONLY.
19. THIS JOINT TRENCH PLAN WAS PREPARED BASED ON TOPOGRAPHICAL SURVEY AS PROVIDED BY A CIVIL ENGINEER. THE CONTRACTOR IS CAUTIONED THAT EXPLORATORY WORK IS NECESSARY TO DETERMINE THE ACTUAL LOCATION OF ANY EXISTING UTILITIES. VIZION UTILITY PARTNERS STRONGLY RECOMMENDS THAT ALL UTILITIES BE PHYSICALLY LOCATED ON THE SITE BEFORE THE ONSET OF SITE WORK. SUBSTRUCTURE LOCATIONS MAY REQUIRE FIELD ADJUSTMENT TO COMPENSATE FOR ACTUAL EXISTING UTILITY LOCATIONS.

STRUCTURE VERIFICATION STAMP

DEVELOPER  
PLEASE NOTE AND SIGN

ALL PG&E ENCLOSURES AND BOXES HAVE BEEN SET TO WORK ACCORDING TO GRADE STAKES PROVIDED BY DEVELOPER'S ENGINEER. ALL COSTS TO RELOCATE OR RE-ADJUST BOXES AT A LATER DATE WILL BE BELIEVED TO THE DEVELOPER. PLEASE HAVE YOUR SITE VERIFY THE CORRECT GRADE OF ALL ENCLOSURES OR BOXES, AND SIGN AND DATE DRAWING.

THANK YOU

SIGNED: \_\_\_\_\_  
DATE: \_\_\_\_\_

UTILITY APPROVALS			FOR VIZION USE ONLY		
UTILITY	APPROVED BY	DATE	Q1 REVIEW	INITIALS	DATE
PG&E ELECTRIC					
PG&E GAS					
AT&T (PHONE)					
COMCAST (CATV)					
CITY ENGINEER					

DEVELOPER:  
706-716 SANTA CRUZ AVE LLC  
700 SANTA CRUZ AVE  
MENLO PARK, CA 94025  
VASILE C. OROS  
T: 415-260-0608  
EMAIL: VOR011@AOL.COM

SHEET INDEX  
JT-1 JOINT TRENCH TITLE SHEET  
JT-2 JOINT TRENCH INTENT

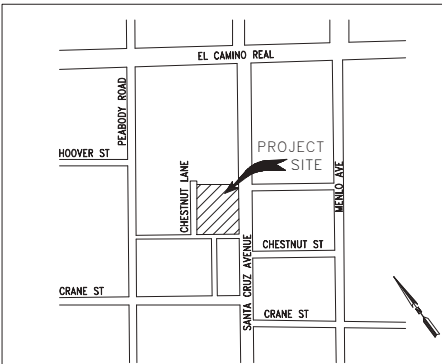
PROJ NO: 19-969  
SCALE: N.T.S.  
PREP: J. KLEIN  
DESIGNED BY: J. CAZARES  
CHECKED BY:  
DATE: 09-05-2019  
DRAWING NO: JT-1  
SHEET: 1 of 2

REGISTERED PROFESSIONAL ENGINEER  
No. 26422  
Exp. 03-31-20  
CIVIL  
STATE OF CALIFORNIA

DATE: \_\_\_\_\_  
 REVISION: \_\_\_\_\_  
 TITLE BLOCK  
 JOINT TRENCH TITLE SHEET  
 706 SANTA CRUZ AVENUE  
 RELOCATION  
 706-716 SANTA CRUZ AVE LLC  
 MENLO PARK  
 Vizion Utility Partners  
 VISION ENGINEERS, CONSULTANTS & STREETLIGHT DESIGN  
 1015 TOWNSEND DRIVE SUITE 101, MENLO PARK, CA 94025  
 PROJECT NO: 19-969  
 SCALE: N.T.S.  
 PREP: J. KLEIN  
 DESIGNED BY: J. CAZARES  
 CHECKED BY:  
 DATE: 09-05-2019  
 DRAWING NO: JT-1  
 SHEET: 1 of 2







**VICINITY MAP**  
N. T. S.

**WORK RESPONSIBILITY JOINT TRENCH**

TRENCHING Excavate & Backfill	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
GAS MATERIAL Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
ELECTRIC CABLE Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
ELECTRIC CONDUIT Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
ELECTRIC BOXES Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
ELECTRIC TRANSFORMER PADS Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
ELECTRIC SWITCHGEAR & TRANSFORMER Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
TELEPHONE CONDUIT Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
TELEPHONE CABLE Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
TELEPHONE SPICE BOXES Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
TELEPHONE S&I PAD Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
C.A.T.V. CONDUIT Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
C.A.T.V. SPICE BOXES Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
C.I.E.C. FIBER CONDUIT Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
C.I.E.C. FIBER SPICE BOXES Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR
DIRECTIONAL DRILL / JACK AND BORE Supply & Install	PG&E ELECTRIC	PG&E GAS	PG&E C.A.T.V.	PG&E TELEPHONE	PG&E LOCALITY	CONTRACTOR

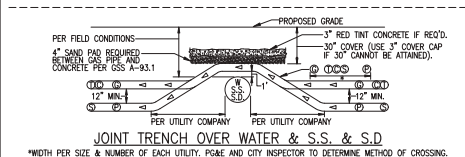
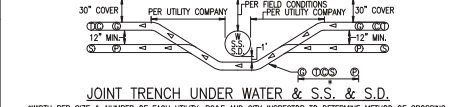
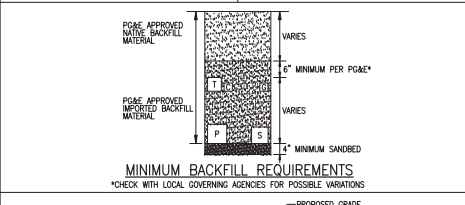
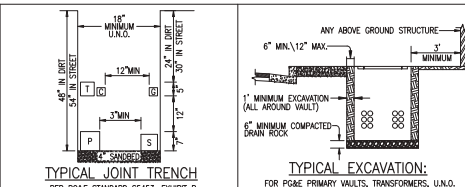
● SYMBOL DESIGNATES THE WORK TO BE PERFORMED BY THE RESPECTIVE CONTRACTOR & UTILITY COMPANIES.  
○ NOT APPLICABLE UNLESS OTHERWISE SPECIFIED.  
\* PG&E TO PULL CABLE INTO ENERGIZED ENCLOSURES

THESE PLANS WERE PREPARED IN CONJUNCTION WITH THE FOLLOWING PLANS:

CIVIL IMPROVEMENT PLANS/GRADING PLANS	03-18-2018	PRELIMINARY
ARCHITECTURAL/ELECTRONIC FLOOR	10-24-2019	PRELIMINARY
APPLICANT DESIGN (GAS)		
APPLICANT DESIGN (ELECTRIC)		
TELEPHONE		
C.A.T.V.		
LANDSCAPE	08-28-2019	PRELIMINARY
LIGHT LOCATIONS		

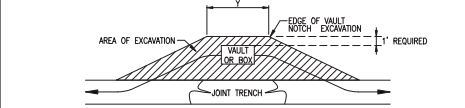
**VIZION UTILITY PARTNERS is not responsible for any schedule changes or reservations.**

OTHER UTILITIES SHOWN ARE APPROXIMATE AND BASED ON FIELD SURVEY AND AVAILABLE UTILITY INFORMATION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION AND EXTENT OF UTILITIES PRIOR TO THE COMMENCEMENT OF WORK. PHYSICAL VERIFICATION OF UTILITY LOCATIONS SHALL BE PERFORMED BY CAREFUL PROBING OR HAND DIGGING IN ACCORDANCE WITH ARTICLE 6 OF THE CALIFORNIA CONSTRUCTION SAFETY ORDERS.



NOTE: TRENCH DEPTH NOT TO EXCEED 5' UNLESS APPROVED BY PG&E INSPECTOR. IN NO CASE SHOULD PLASTIC GAS PIPE BE INSTALLED AT A DEPTH GREATER THAN 10' UNLESS APPROVED BY PG&E SENIOR GAS ENGINEER.

TRENCHING CONTRACTOR SHALL NOT ASSUME THAT EITHER OF THE ABOVE DETAILS WILL BE ACCEPTABLE TO PG&E. YOU ARE REQUIRED TO CONTACT THE LOCAL PG&E ENGINEERING OFFICE WITH ANY ISSUE RELATING TO COVERS LESS THAN MINIMUM OR COVERS REQUIRING SHORING. CONCRETE CAPPING IS ONLY ACCEPTABLE WHERE NO OTHER SOLUTION IS POSSIBLE AND ONLY WHEN CERTAIN CRITERIA ARE MET AND ONLY WITH PG&E APPROVAL.



**ELECTRIC CONDUIT MINIMUM BEND RADIUS**

CONDUIT DIAMETER	VERTICAL RADIUS	HORIZONTAL RADIUS
3"	24"	36"
4"	24"	36"
6"	36"	54"
8"	48"	72"

NOTE: 315° MAX BENDS IN ANY SECONDARY CONDUIT RUN 200' OR LESS.  
300° MAX BENDS IN ANY PRIMARY CONDUIT RUN.

**TYPICAL GAS METER REQUIREMENTS\***

METER TYPE	DELIVERY PRESSURE (PSIG)	PAD SIZE (INCHES)	MIN. WIDTH REQUIRED FOR METER (INCHES)	DISTANCE FROM RISER TO FINISHED SLUR OUT (INCHES)	MIN. HOUSING/SLUR OUT (INCHES)
TYPICAL RESIDENTIAL	0-350	0.25	N/A UNLESS USING FLEX-HOSE METER	24	6 TO 9
400 TO 1000	351-1,400	0.25	N/A UNLESS USING FLEX-HOSE METER	30	6 TO 9
1,401-3,000	1,401-2,400	0.25	N/A UNLESS USING FLEX-HOSE METER	30	6 TO 9
1.5M OR 3M ROTARY	3,001-7,000	APPROVED BY PG&E	40 X 36 X 4	52	20
5M OR 7M ROTARY	7,001-16,000	APPROVED BY PG&E	78 X 36 X 4	90	20
11M OR 16M ROTARY	17,001-16,000	APPROVED BY PG&E	94 X 36 X 4	106	20

\*ACTUAL METER-SET CONFIGURATIONS MAY DIFFER DEPENDING ON FIELD CONDITIONS AND RESTRICTIONS. FOR GAS METER DETAILS, SEE SECTION 2 OF CURRENT PG&E GAS SERVICE REQUIREMENTS GREENBOOK OR PG&E GAS STANDARD METER SPACING REQUIREMENTS DO NOT INCLUDE CLEARANCE FOR EXHAUSTION VALVES OR OTHER ADDITIONAL SAFETY EQUIPMENT. SEE NOTE 17 UNDER GENERAL NOTES ON SHEET JT-1.

\*\*DELIVERY PRESSURE TO BE CONFIRMED BY BUILDING MECHANICAL PLANS. PG&E MAINTAINS SILE AUTHORITY TO DETERMINE IF THE ELEVATED DELIVERY-PRESSURE SERVICE IS AVAILABLE AT A SPECIFIC LOCATION.

**GENERAL NOTES:**

- THE PREFERRED TRENCH LOCATION IS IN A PUBLIC UTILITY EASEMENT (P.U.E.).
- ALL DEPTHS AND RESULTING COVER REQUIREMENTS ARE MEASURED FROM FINAL GRADE.
- CONTRACTOR SHALL VERIFY AND OBTAIN PERMISSION FROM THE JURISDICTIONAL OWNERS OF THE CIRCUMSTANCES, BUT UNDER NO CIRCUMSTANCES SHALL BE LESS THAN THE MINIMUM COVER, CLEARANCE AND SEPARATION REQUIREMENTS SET FORTH IN GENERAL ORDER 128 AND 4920R 192.371, 4920R 192.372, AND 4920R 192.373. ALL FACILITIES SHALL BE MAINTAINED IN PLACE OR REPAIRED. OTHER MEASURES SHALL BE TAKEN TO ENSURE NO MOTION OF THE FACILITIES. DIMENSIONAL REQUIREMENTS FOR SHADING, LEVELING, AND BACKFILLING SHALL BE DETERMINED SUBSEQUENT TO COMPLETION.
- TRENCH DIMENSIONS SHOWN ARE TYPICAL. TRENCH SIZES AND CONFIGURATIONS MAY VARY DEPENDING UPON OCCUPANCY AND/OR FIELD CONDITIONS. TRENCH SIZE AND CONFIGURATION MUST AT ALL TIMES BE CONSTRUCTED IN A MANNER THAT ENSURES PROPER CLEARANCES AND SEPARATIONS ARE MAINTAINED THROUGHOUT THE TRENCH WIDTH AND CONFIGURATIONS AS SHOWN IN THIS EXHIBIT MUST BE DESIGNED TO EXCEED THIS REQUIREMENT.
- IT IS PREFERRED TO HAVE NON-PG&E OWNED STREETLIGHTS AT A LEVEL OTHER THAN THE GAS OR ELECTRIC LEVEL. NON-PG&E OWNED STREETLIGHTS MAY BE AT THE ELEVATED LEVEL OF THE TRENCH AS LONG AS MINIMUM CLEARANCES ARE PROVIDED AND COMPLY WITH ALL SPECIAL NOTES FOR A JOINT TRENCH WITH A SECOND ELECTRIC TRENCH.
- NON-UTILITY FACILITIES ARE NOT ALLOWED IN ANY JOINT UTILITY TRENCH, E.G., IRRIGATION CONTROL LINES, BUILDING FIRE ALARM SYSTEMS, PRIVATE TELEPHONE SYSTEMS, OUTDOOR ELECTRICAL CABLE, ETC.
- WHEN COMMUNICATION DUCTS ARE INSTALLED, A MINIMUM OF 12" RADIAL SEPARATION SHALL BE MAINTAINED FROM GAS FACILITIES. EXCEPTING WITH MUTUAL AGREEMENT, WHEN 4-INCH DIAMETER OR SMALLER GAS PIPES IS INSTALLED, THE SEPARATION MAY BE REDUCED TO NOT LESS THAN 6 INCHES.
- PROVIDE SEPARATION FROM TRENCH WALL AND OTHER FACILITIES SUFFICIENT TO ENSURE PROPER COMPACTION.
- MAINTAIN PROPER SEPARATION BETWEEN PO&E FACILITIES AND "NET" UTILITY LINES AS DESCRIBED IN UO STANDARD 55453. THE MINIMUM ALLOWABLE HORIZONTAL SEPARATION BETWEEN COMPANY FACILITIES AND "NET" FACILITIES IS 3" WITH A MINIMUM 1" OF UNDISTURBED EARTH OR THE INSTALLATION OF A SUITABLE BARRIER BETWEEN THE FACILITIES. IF 3" IS NOT MAINTAINED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A BARRIER. A VARIANCE MAY BE APPROVED BY THE LOCAL INSPECTION SUPERVISOR AND SUBMITTED TO THE SERVICE PLANNING SUPPORT PROGRAM MANAGER FOR APPROVAL. SEPARATIONS OF 0" OR LESS ARE NOT PERMISSIBLE AND WILL NOT BE ALLOWED. THE COMPANY MAY ASK TO INCREASE THE MINIMUM 3" SEPARATION REQUIREMENT AT THE REQUEST OF AN APPLICANT IF WARRANTED AND THE NEED IS JUSTIFIED. THE REQUEST FOR A WAIVER MUST:
  - BE MADE IN WRITING AND SUBMITTED TO THE COMPANY AHEAD DURING THE PLANNING AND DESIGN PHASE OF THE PROJECT.
  - CLEARLY DESCRIBE THE CONDITIONS NECESSITATING THE WAIVER.
  - INCLUDE A PROPOSED BARRIER AND INCLUDE BARRIER BETWEEN THE "NET" UTILITIES AND COMPANY DRY FACILITIES IN THE EVENT 1" OF UNDISTURBED EARTH CANNOT BE MAINTAINED. NOTE: DRAIN LINES CONNECTED TO DOWNSPOUTS ON BUILDINGS ARE CONSIDERED A "NET" UTILITY FOR THE PURPOSES OF THIS STANDARD.
- SEPARATIONS SHALL BE MAINTAINED AT ABOVE GROUND TERMINATION POINTS.
- PROCEDURES FOR APPROVING NATIVE BACKFILL FOR SHADING OF PO&E GAS FACILITIES:
  - RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF 3 LOCATIONS PER 1,000' OF TRENCH. 100% OF THE SAMPLE MUST PASS THROUGH A 1/2" SIEVE AND 75% MUST PASS THROUGH A #4 SCREEN. ADDITIONAL SAMPLES MUST BE TAKEN IF EXISTING CONDITIONS CHANGE AND ARE TO BE TAKEN AT THE DISCRETION OF THE PG&E REPRESENTATIVE ON SITE.
  - THE SOILS MUST NOT CONTAIN ANY ROCKS THAT HAVE SHARP EDGES OR THAT MAY OTHERWISE BE ABRASIVE.
  - THE SOILS MUST NOT CONTAIN CLOSERS LARGER THAN 1/2" IF TO BE USED AS SHADING, BEDDING OR LEVELING.
  - COMPACTION REQUIREMENTS MUST MEET ANY APPLICABLE PG&E, FEDERAL, STATE, COUNTY OR LOCAL REQUIREMENTS. AS AT NO TIME SHALL THE OVER SATURATION OF NATIVE SOILS BE USED TO ACHIEVE THESE REQUIREMENTS. THE SIEVES AND SCREENS SHALL BE:
    - 1/2" SIEVE: 8" DIAMETER BY 2" DEEP, STAINLESS STEEL WIRE SCREEN.
    - #4 SCREEN: 8" DIAMETER BY 2" DEEP, STAINLESS STEEL WIRE SCREEN.
- PROCEDURES FOR APPROVING NATIVE BACKFILL FOR SHADING AT PO&E ELECTRIC FACILITIES:
  - RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF 3 LOCATIONS PER 1,000' OF TRENCH. ADDITIONAL SAMPLES SHALL BE TAKEN IF EXISTING CONDITIONS CHANGE AND ARE TO BE TAKEN AT THE DISCRETION OF THE PG&E REPRESENTATIVE ON SITE.
  - SHADING LARGE ROCK, PAWING MATERIAL, CORNERS, SHARPLY ANGULAR SUBSTANCES, OR CORROSIVE MATERIAL SHALL NOT BE PLACED IN THE TRENCH WHERE SUCH MATERIAL MAY DAMAGE THE CONDUITS AND/OR PREVENT PROPER COMPACTION OVER OR AROUND THE CONDUIT.
  - NATIVE SOILS CONTAINING CLOSERS NOT TO EXCEED 6" IN DIAMETER MAY BE INCLUDED IN THE SHADING MATERIAL PROVIDED THE CLOSERS ARE EASILY BREAKABLE BY HAND. NOTE: SHADING SHALL CONSIST PRIMARILY OF ADG, HARD COMPOST (GENSL CLAY, SAND & GUM) SHALL NOT BE USED AS SHADING MATERIAL.
  - AT NO TIME SHALL THE OVER SATURATION OF NATIVE SOILS BE USED TO ACHIEVE THESE REQUIREMENTS. REFER TO ENGINEERING DOCUMENT NUMBER 13 ON PG&E 4.
- COMPETENT NATIVE SOILS ARE PREFERRED TO BE USED FOR SHADING, BEDDING, AND BACKFILLING THROUGHOUT THE TRENCH.
- WHERE NATIVE SOILS EXCEED 1/2" MINUS AND/OR WHERE GAS IS TO BE PLACED AT THE BOTTOM OF A TRENCH IN AREAS THAT EXCEED 1/2" MINUS SOIL CONDITIONS, OR WHERE THE BOTTOM OF A TRENCH IS CONSIDERED TO CONSIST OF HARD SOIL, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SHADING AND/OR BEDDING OF GAS FACILITIES.
- POST APPROVED IMPORT MATERIAL IS PER C&E ENGINEERING GUIDELINE 4128.
- IF A VARIANCE IS REQUIRED FOR GAS FACILITIES, THE USE OF NATIVE SOILS IS PREFERRED, BUT IF 1/2" MINUS CONDITIONS ARE NOT ATTAINABLE WITH THE NATIVE SOILS, THEN THE USE OF PG&E APPROVED IMPORT MATERIALS IS REQUIRED. BEDDING UNDER GAS FACILITIES WILL BE A MINIMUM OF 3" OF COMPACTED 1/2" MINUS NATIVE SOILS OR PG&E APPROVED IMPORT MATERIAL.
- POST APPROVED IMPORT MATERIAL IS PER NOTE 12. THIS APPLIES TO LEVELING COURSES AS WELL AS SHADING. THE MINIMUM PG&E APPROVED BEDDING MATERIAL MAY BE INCREASED AT THE DISCRETION OF PG&E WHEN WARRANTED BY EXISTING FIELD CONDITIONS (E.G., ROCKY SOILS, HARD PAW, ETC.).
- THE USE OF ANY IMPORTED MATERIAL FOR BACKFILLING PURPOSES SHALL BE LIMITED TO THOSE SITUATIONS WHEN NATIVE SOILS DO NOT ALLOW FOR REQUIRED COMPACTION.
- THE APPLICANT IS RESPONSIBLE FOR THE REMOVAL OF EXCESS SPOIL AND ASSOCIATED COSTS.
- SEPARATION BETWEEN GAS FACILITIES AND ELECTRIC FACILITIES MAY BE REDUCED TO 6" WHEN CROSSING.
- SERVICE SADDLES ARE THE PREFERRED SERVICE FITTINGS FOR USE THROUGHOUT THE JOINT TRENCH PROJECT. ALL PROJECTS WILL BE DESIGNED AND ESTIMATED USING SERVICE SADDLES. HOWEVER, SERVICE TEES MAY BE USED IF ALL CLEARANCES, SEPARATION, AND COVERAGE REQUIREMENTS ARE MAINTAINED.
- CONTRACTOR TO INCREASE METER SPACING AS NECESSARY WHEN EXHAUSTION VALVES OR OTHER ADDITIONAL SAFETY EQUIPMENT ARE REQUIRED. EXHAUSTION VALVES OR OTHER ADDITIONAL SAFETY EQUIPMENT ARE NOT PART OF PG&E/UDON SPACING REQUIREMENTS DO NOT INCLUDE CLEARANCE FOR EXHAUSTION VALVES.

**TRANSFORMER CLEARANCE REQUIREMENTS:**

- ABOVE ANY SINGLE PHASE TRANSFORMER LOCATION, MAINTAIN 20' UNOBSTRUCTED OVERHEAD CLEARANCE OVER TRANSFORMER VAULT/PAD.
- ABOVE ANY THREE PHASE TRANSFORMER LOCATION, MAINTAIN 30' UNOBSTRUCTED OVERHEAD CLEARANCE OVER TRANSFORMER VAULT/PAD.

**GAS PIPELINE UNDERGROUND WARNING TAP NOTES:**

- A WARNING TAP IS TO BE INSTALLED IN OPEN TRENCH INSTALLATION OVER GAS PIPELINES IN BOTH TRENCHES AND UNBROKEN TRENCHES. INCLUDES TRENCHES, BELL HOLES, EXCAVATIONS FOR REPAIR PURPOSES AND RISER RELATIONS. THE WARNING TAP IS INTENDED FOR EXHAUSTION DIGGING IN THE "TOLERANCE ZONE" TO STRIKE THE WARNING TAP PRIOR TO THE PIPELINE. WHEN THE WARNING TAP IS STRUCK AND SHARDED WITH EXCAVATING EQUIPMENT, IT STRETCHES WITHOUT BREAKING, THIS ALERTING THE EXCAVATOR OF THE FACILITY LOCATION.
- INSTALL 6" WIDE WARNING TAP ABOVE THE GAS PIPELINE AT LEAST 12" BELOW GRADE, AND NO CLOSER THAN 12" FROM THE PIPE. INSTALLATION SHALL PROCEED FROM THE TRENCH END TO THE PIPE AND THE TAP AS POSSIBLE. INSTALL THE TAP ALONG THE LENGTH OF THE TRENCH. THE WARNING TAP SHALL BE INSTALLED IN EACH PIECE OF TAP. EXCEPTING: WHEN A JOINT TRENCH DESIGN DOES NOT ALLOW FOR INSTALLATION OF WARNING TAP WITH THE WARNING TAP INSTALLATION ZONE, THE WARNING TAP MAY BE BORED TO 6" ABOVE THE GAS PIPELINE, AND BORED THE FACILITY ABOVE THE PIPE.
- WARNING TAP SHALL BE BRIGHTLY COLORED YELLOW AND MARKED "CAUTION: GAS LINE BELOW" WITH A SIMILAR MARKING.
- WARNING TAP SHALL BE STORED IN SUCH A MANNER THAT LIMITS ULTRAVIOLET (UV) EXPOSURE.

**TYPICAL DETAIL**  
GAS PIPELINE UNDERGROUND WARNING TAP INSTALLATION (N.T.S.)

**CONSTRUCTION NOTES:**

- ALL TRENCHING, BACKFILLING AND INSTALLATION BY CONTRACTOR MUST COMPLY WITH PG&E UO STANDARD 55453 (EFFECTIVE DATE 7-5-2006).
- ALL WORK MUST COMPLY WITH P.G. & E. TELEPHONE, C.A.T.V., STANDARDS AND PRACTICES. ALL WORK MUST BE INSPECTED AND APPROVED BY RESPECTIVE INSPECTORS. RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF THREE LOCATIONS PER 1,000' OF TRENCH. 100% OF THE SAMPLES MUST PASS THROUGH A 1/2" SIEVE AND 75% MUST PASS THROUGH A #4 SCREEN. ADDITIONAL SAMPLES MUST BE TAKEN IF EXISTING SOIL CONDITIONS CHANGE AND IS TO BE AT THE DISCRETION OF THE PG&E REPRESENTATIVE ON SITE. THE SOILS MUST NOT CONTAIN ANY ROCKS THAT HAVE SHARP EDGES OR THAT MAY OTHERWISE BE ABRASIVE. THE SOILS MUST NOT CONTAIN CLOSERS LARGER THAN 1/2" IF TO BE USED AS SHADING, BEDDING OR LEVELING MATERIALS. COMPACTION REQUIREMENTS MUST MEET ANY APPLICABLE PG&E, FEDERAL, STATE, COUNTY OR LOCAL REQUIREMENTS. ANY NATIVE SOILS OR IMPORT MATERIALS USED MUST NOT HINDER THESE REQUIREMENTS.
- BACKFILL SHALL BE APPROVED BY THE UTILITY COMPANIES AND THE CITY. COMPACTION WILL BE TESTED AND PASSED BY THE SOILS ENGINEER.
- IF SOIL IS NOT ROCK FREE, ADD 4" DEPTH OF TRENCH FOR SAND BEDDING.
- VERIFY SPICE BOX EXCAVATION SIZES WITH SUPPLIERS).
- THE TRENCHING CONTRACTOR SHALL COORDINATE THE UTILITY COMPANIES' INSTALLATION. THE TRENCHING CONTRACTOR TO PLACE CONNECTING CONDUIT WITHIN 5' OF BUILDING OVERWALL.
- CONTRACTOR SHALL MAKE HIMSELF FAMILIAR WITH THE PROJECT IMPROVEMENT PLANS AND CONDUCT HIS WORK ACCORDINGLY.
- IT IS THE TRENCHING CONTRACTOR'S RESPONSIBILITY TO PROTECT IN PLACE ALL EXISTING FACILITIES. NO EXTRA PAYMENT WILL BE CONSIDERED FOR CROSSING OTHER SYSTEMS.
- VIZION UTILITY PARTNERS ASSUMES NO RESPONSIBILITY FOR THE PROJECT CONDITIONS. THESE DRAWINGS WERE PREPARED USING DATA SUPPLIED BY PG&E, TELEPHONE, C.A.T.V., IMPROVEMENT PLANS AND THE CITY'S WIDIOUS "AS BUILT" INFORMATION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PHYSICALLY REVIEW THE PROJECT PRIOR TO SUBMITTING HIS BID.
- CONTRACTOR WILL COMPLY WITH ALL LAWS, ORDINANCES AND REGULATIONS. CONTRACTOR SHALL BE FAMILIAR WITH O.S.H.A., INDUSTRIAL SAFETY ORDERS AND SHALL CONDUCT HIS WORK ACCORDINGLY. WHEN WORKING NEAR ENERGIZED OR "HOT" EQUIPMENT, THE UTILITY OWNER SHALL BE NOTIFIED TO SUPPLY THE APPROPRIATE MAN POWER. PUBLIC SAFETY AND TRAFFIC CONTROL MEASURES ARE THE CONTRACTOR'S RESPONSIBILITY.
- THE CONTRACTOR SHALL PROTECT CONSTRUCTION STAKING. HE SHALL COORDINATE STAKING WITH THE PROJECT'S CIVIL ENGINEER.
- CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (USA) 2 WORKING DAYS PRIOR TO START OF WORK.
- CONTRACTOR SHALL NOTIFY INSPECTORS OF ANY POTENTIAL CONFLICTS PRIOR TO START OF WORK.
- THIS PLAN IS TO BE USED FOR SOLE PURPOSE OF DIGGING THE JOINT TRENCH. SEE PG&E, AT&T, AND COMCAST PLANS FOR EXACT SIZE AND NUMBER OF CONDUITS INSTALLED IN THE JOINT TRENCH. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THE CORRECT NUMBER, SIZE AND TYPES OF CONDUITS ARE INSTALLED PER THE ENGINEERED PLANS BY EACH UTILITY COMPANY.
- NOTE: PLANS ISSUED AT THE PRE-CONSTRUCTION MEETING MAY BE SUBJECT TO REVISIONS. IF FINAL PLANS FROM EACH UTILITY COMPANY WERE NOT AVAILABLE AT THE START OF CONSTRUCTION:
  - WATER, SEWER, GAS, RAINWATER WASTE, FUELS (INCLUDING DIESEL, GAS AND GASOLINE), OIL, PROPANE AND OTHER VOLATILE HEAVIER THAN AIR GASES, STEAM, HEAT, AND OTHER "NET" FACILITIES SHALL MAINTAIN A MINIMUM OF THREE FEET FROM THE NEAREST EXPOSED SURFACE OF PG&E FACILITIES WITH NO LESS THAN ONE FOOT OF EARTH SOIL BARRIER BETWEEN THE ADJACENT SIDES OF THE INDIVIDUAL TRENCHES.
  - IN THE EXTRAORDINARY CASE THAT THE MINIMUM THREE FOOT HORIZONTAL SEPARATION CANNOT BE ATTAINED BETWEEN "NET" UTILITIES AND COMPANY DRY FACILITIES, A VARIANCE MAY APPROVED BY THE LOCAL INSPECTION SUPERVISOR AND SUBMITTED TO SERVICE PLANNING SUPPORT PROGRAM MANAGER FOR APPROVAL.
- ALL METER PANELS: INDIVIDUAL RESIDENTIAL OR NONRESIDENTIAL APPLICANTS WITH A METER PANEL RATING OF ANY SIZE, INSTALLED INSIDE A METER ROOM OR OTHER STRUCTURE, MUST FOLLOW ALL OF THE REQUIREMENTS DESCRIBED BELOW.
  - INSTALL OWN AND MAINTAIN A SEPARATE, NOMINAL 2-INCH DIAMETER CONDUIT WITH FULL TAPES INSIDE OF THE METER ROOM AND FULL TAPES MUST EXTEND FROM THE OUTSIDE SURFACE OF THE BUILDING AND TERMINATE OUTSIDE.
  - THE METER PANEL OR SWITCHBOARD AT THE TOP OF THE METER SECTION.
  - CONDUIT THE 2-INCH DIAMETER CONDUIT OR FULL TAPES EXTEND THE OUTSIDE OF THE BUILDING A MINIMUM OF 8 FEET AND A MAXIMUM OF 10 FEET ABOVE GRADE, THE OPEN END OF THE CONDUIT THAT IS EXPOSED TO THE OUTSIDE MUST HAVE A REMOVABLE TEMPORARY CAP OR PLUG.
  - DO NOT USE THE CONDUIT, THE CONDUIT IS FOR PG&E'S METERING EQUIPMENT ONLY.
- THIS JOINT TRENCH PLAN WAS PREPARED BASED ON TOPOGRAPHICAL SURVEY AS PROVIDED BY A CIVIL ENGINEER. THE CONTRACTOR IS CAUTIONED THAT EXPLORATORY WORK IS NECESSARY TO DETERMINE THE ACTUAL LOCATION OF ANY EXISTING UTILITY. VIZION UTILITY PARTNERS STRONGLY RECOMMENDS THAT ALL UTILITIES BE PHYSICALLY LOCATED ON THE SITE BEFORE THE ONSET OF SITE WORK. SUBSTRUCTURE LOCATIONS MAY REQUIRE FIELD ADJUSTMENT TO COMPENSATE FOR ACTUAL EXISTING UTILITY LOCATIONS.

**STRUCTURE VERIFICATION STAMP**

DEVELOPER  
PLEASE NOTE AND SIGN

ALL PG&E ENCLOSURES AND BOXES HAVE BEEN SET TO GRADE. ACCORDING TO GRADE STAKES PROVIDED BY THE DEVELOPER'S ENGINEER. ALL COSTS TO RELOCATE OR RE-WORK BOXES AT A LATER DATE WILL BE BORNE BY THE DEVELOPER. PLEASE HAVE YOUR SIFT, VERIFY THE CORRECT GRADE OF ALL ENCLOSURES OR BOXES, AND SIGN AND DATE DRAWING. THANK YOU.

SIGNED: \_\_\_\_\_  
DATE: \_\_\_\_\_

**UTILITY APPROVALS**

UTILITY	APPROVED BY	DATE	FOR VIZION USE ONLY
PG&E ELECTRIC			Q1 REVIEW
PG&E GAS			INITIALS DATE
AT&T (PHONE)			NETW (ELEC) NETW (GAS)
COMCAST (CATV)			COMPOSITE PRE-COMMIT
CITY ENGINEER			

**DEVELOPER:**  
706-716 SANTA CRUZ AVE LLC  
700 SANTA CRUZ AVE  
MENLO PARK, CA 94025  
VASILE C. OROS  
T: 415-260-0608  
EMAIL: vros11@AOL.COM

**SHEET INDEX**

JT-1	JOINT TRENCH TITLE SHEET
JT-2	JOINT TRENCH INTENT

**DESIGN CHANGE COMPONENT**  
ANY CHANGES TO THIS DESIGN MUST BE APPROVED BY

PG&E GAS ADE

PROJ NO: 19-969  
SCALE: N.T.S.  
PREP: J. KLEIN  
DESIGN: M. HOANG  
CHECKED BY:  
LAST UPDATE: 10-29-2019  
DRAWING NO: JT-1  
SHEET: 1 OF 2

REGISTERED PROFESSIONAL CIVIL ENGINEER  
DAVID B. HOODING  
NO. 26422  
Exp. 03-31-20  
CIVIL  
STATE OF CALIFORNIA

DATE: \_\_\_\_\_  
REVISION: \_\_\_\_\_  
SHEET NO: \_\_\_\_\_

**JOINT TRENCH TITLE SHEET**  
706 SANTA CRUZ AVENUE  
NEW BUSINESS  
706-716 SANTA CRUZ AVE LLC

**Vizion Utility**  
PARTNERS  
UTILITY ENGINEERS, CONSULTANTS & STRUCTURAL DESIGN  
1000 BROADWAY, SUITE 1100, SAN FRANCISCO, CA 94103  
WWW.VIZIONUTILITY.COM

CALIFORNIA  
MENLO PARK







### VICINITY MAP

N. T. S.

### WORK RESPONSIBILITY JOINT TRENCH

TRENCHING	Excavate & Backfill	PG&E ELECTRIC	PG&E GAS	PG&E PHONE	PG&E C.A.T.V.	CONTRACTOR
GAS MATERIAL	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
ELECTRIC CABLE	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
ELECTRIC CONDUIT	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
ELECTRIC BOXES	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
ELECTRIC TRANSFORMER PADS	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
ELECTRIC SWITCHGEAR & TRANSFORMER	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
TELEPHONE CONDUIT	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
TELEPHONE CABLE	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
TELEPHONE SPICE BOXES	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
TELEPHONE S&I PAD	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
C.A.T.V. CONDUIT	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
C.A.T.V. SPICE BOXES	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install
C.I.E.C. FIBER CONDUIT	Accepted	Declined	Accepted	Declined	Accepted	Declined
C.I.E.C. FIBER SPICE BOXES	Accepted	Declined	Accepted	Declined	Accepted	Declined
DIRECTIONAL DRILL / JACK AND BORE	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install	Supply & Install

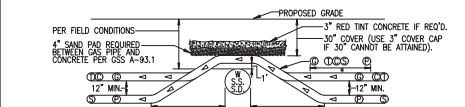
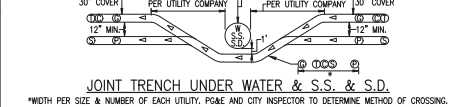
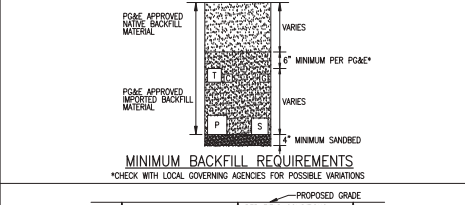
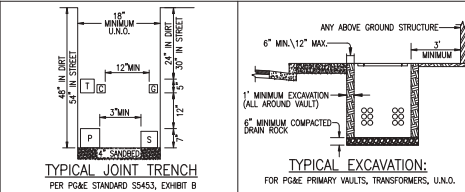
● SYMBOL DESIGNATES THE WORK TO BE PERFORMED BY THE RESPECTIVE CONTRACTOR & UTILITY COMPANIES.  
○ NOT APPLICABLE UNLESS OTHERWISE SPECIFIED  
\* PG&E TO PULL CABLE INTO ENERGIZED ENCLOSURES

THESE PLANS WERE PREPARED IN CONJUNCTION WITH THE FOLLOWING PLANS:

CIVIL IMPROVEMENT PLANS/GRADING PLANS	DATE	STATUS
ARCHITECTURAL/ELECTRIC PLANS	10-24-2019	PRELIMINARY
APPLICANT DESIGN (GAS)		RECEIVED
APPLICANT DESIGN (ELECTRIC)		RECEIVED
TELEPHONE		RECEIVED
C.A.T.V.		RECEIVED
LANDSCAPE	08-28-2019	PRELIMINARY
LIGHT LOCATIONS		RECEIVED

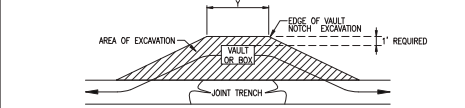
**VIZION UTILITY PARTNERS is not responsible for any schedule changes or reservations.**

OTHER UTILITIES SHOWN ARE APPROXIMATE AND BASED ON FIELD SURVEY AND AVAILABLE UTILITY INFORMATION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION AND EXTENT OF UTILITIES PRIOR TO THE COMMENCEMENT OF WORK. PHYSICAL VERIFICATION OF UTILITY LOCATIONS SHALL BE PERFORMED BY CAREFUL PROBING OR HAND DIGGING IN ACCORDANCE WITH ARTICLE 6 OF THE CALIFORNIA CONSTRUCTION SAFETY ORDERS.



NOTE: TRENCH DEPTH NOT TO EXCEED 5' UNLESS APPROVED BY PG&E INSPECTOR. IN NO CASE SHOULD PLASTIC GAS PIPE BE INSTALLED AT A DEPTH GREATER THAN 10' UNLESS APPROVED BY PG&E SENIOR GAS ENGINEER.

TRENCHING CONTRACTOR SHALL NOT ASSUME THAT EITHER OF THE ABOVE DETAILS WILL BE ACCEPTABLE TO PG&E. YOU ARE REQUIRED TO CONTACT THE LOCAL PG&E ENGINEERING OFFICE WITH ANY ISSUE RELATING TO COVERS LESS THAN MINIMUM OR COVERS REQUIRING SHORING. CONCRETE CAPPING IS ONLY ACCEPTABLE WHERE NO OTHER SOLUTION IS POSSIBLE AND ONLY WHEN CERTAIN CRITERIA ARE MET AND ONLY WITH PG&E APPROVAL.



BOX EXCAVATION	NOTE:
3' x 5'	DISTANCE VARIES PER FIELD CONDITIONS
4'-6" x 8'-6"	11'

CONDUIT DIAMETER	VERTICAL RADIUS	HORIZONTAL RADIUS
2"	24"	36"
3"	24"	36"
4"	24"	36"
6"	36"	60"

NOTE: 315° MAX BENDS IN ANY SECONDARY CONDUIT RUN 200' OR LESS.  
300° MAX BENDS IN ANY PRIMARY CONDUIT RUN.

TYPICAL GAS METER REQUIREMENTS*	DELIVERY PRESSURE (PSIG)	PAD SIZE (INCHES)	MIN. W/LS REQUIRED FOR METER X TO FINISHED W/LS (INCHES)	DISTANCE FROM RISER TO METER X (INCHES)	MIN. HOUSING/NEELED OUT (INCHES)
TYPICAL RESIDENTIAL	0-350	0.25	N/A UNLESS USING FLEX-HOSE METER	24	6 TO 9
400 TO 1000	351-1,400	0.25	N/A UNLESS USING FLEX-HOSE METER	30	6 TO 9
1,401-3,000	1,401-2,400	0.25	N/A UNLESS USING FLEX-HOSE METER	30	6 TO 9
1.5M OR 3M ROTARY	1,401-3,000	APPROVED BY PG&E	40 X 36 X 4	52	20
5M OR 7M ROTARY	3,001-7,000	APPROVED BY PG&E	78 X 36 X 4	90	20
11M OR 16M ROTARY	7,001-16,000	APPROVED BY PG&E	94 X 36 X 4	106	20

\*ACTUAL METER-SET CONFIGURATIONS MAY DIFFER DEPENDING ON FIELD CONDITIONS AND RESTRICTIONS. FOR GAS METER DETAILS, SEE SECTION 2 OF CURRENT EDITION OF GAS SERVICE REQUIREMENTS GREENBOOK BY PG&E. PG&E STANDARD METER SPACING REQUIREMENTS DO NOT INCLUDE CLEARANCE FOR EXHAUSTION VALVES OR OTHER ADDITIONAL SAFETY EQUIPMENT. SEE PLUMBING AND MECHANICAL NOTES ON SHEET JT-1.

\*\*DELIVERY PRESSURE TO BE CONFIRMED IN BUILDING PLUMBING AND MECHANICAL PLANS. PG&E MAINTAINS SOLE AUTHORITY TO DETERMINE IF THE ELEVATED DELIVERY-PRESSURE SERVICE IS AVAILABLE AT A SPECIFIC LOCATION.

### GENERAL NOTES:

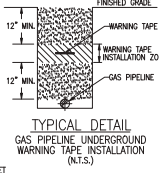
- THE PREFERRED TRENCH LOCATION IS IN A PUBLIC UTILITY EASEMENT (P.U.E.).
- ALL DEPTHS AND RESULTING COVER REQUIREMENTS ARE MEASURED FROM FINAL GRADE.
- CONDUIT SPACING AND SEPARATION SHALL BE AS GREAT AS PRACTICABLE UNDER THE CIRCUMSTANCES, BUT UNDER NO CIRCUMSTANCES SHALL BE LESS THAN THE MINIMUM COVER, CLEARANCE AND SEPARATION REQUIREMENTS SET FORTH IN GENERAL ORDER 128 AND 492P/192.371 AND 492P/192.375 AND 492P/192.371. ALL FACILITIES SHALL BE INSTALLED IN PLACE OF THE PG&E REPRESENTATIVE ON SITE. THE SOILS MUST NOT CONTAIN ANY ROCKS THAT HAVE SHARP EDGES OR THAT MAY OTHERWISE BE ABRASIVE. THE SOILS MUST NOT CONTAIN CLOSERS LARGER THAN 1/2" IF TO BE USED AS SHADING. BEDDING OR LIFTING MATERIALS COMPARISON REQUIREMENTS MUST MEET ANY APPLICABLE P.G.&E. FEDERAL, STATE, COUNTY OR LOCAL REQUIREMENTS. ANY NATIVE SOILS OR IMPORT MATERIALS USED MUST NOT HINDER THESE OPERATIONS.
- TRENCH DIMENSIONS SHOWN ARE TYPICAL. TRENCH SIZES AND CONFIGURATIONS MAY VARY DEPENDING UPON OCCUPANCY AND/OR FIELD CONDITIONS. TRENCH SIZE AND CONFIGURATION MUST AT ALL TIMES BE CONSTRUCTED IN A MANNER THAT ENSURES PROPER CLEARANCES AND SEPARATIONS ARE MAINTAINED. TRENCH WIDTH AND CONFIGURATIONS AS SHOWN IN THIS EXHIBIT MUST BE DESIGNED TO EXCEED THIS REQUIREMENT.
- IT IS PREFERRED TO HAVE NON-PG&E OWNED STREETLIGHTS AT A LEVEL OTHER THAN THE GAS OR ELECTRIC LEVEL. NON-PG&E OWNED STREETLIGHTS MAY BE AT THE ELEVATED LEVEL OF THE TRENCH AS LONG AS MINIMUM CLEARANCES ARE PROVIDED AND COMPLY WITH ALL SPECIAL NOTES FOR A JOINT TRENCH WITH A SECOND ELEVATION FACILITY.
- NON-UTILITY FACILITIES ARE NOT ALLOWED IN ANY JOINT UTILITY TRENCH, E.G., IRRIGATION CONTROL LINES, BUILDING FIRE ALARM SYSTEMS, PRIVATE TELEPHONE SYSTEMS, OUTDOOR ELECTRICAL CABLE, ETC.
- WHEN COMMUNICATION DUCTS ARE INSTALLED, A MINIMUM OF 12" RADIAL SEPARATION SHALL BE MAINTAINED FROM GAS FACILITIES. EXCEPTING WITH MUTUAL AGREEMENT, WHEN 4-INCH DIAMETER OR SMALLER GAS PIPES IS INSTALLED, THE SEPARATION MAY BE REDUCED TO NOT LESS THAN 6 INCHES.
- PROVIDE SEPARATION FROM TRENCH WALL AND OTHER FACILITIES SUFFICIENT TO ENSURE PROPER COMPACTION.
- MAINTAIN PROPER SEPARATION BETWEEN PG&E FACILITIES AND "NET" UTILITY LINES AS DESCRIBED IN UO STANDARD. MAINTAIN THE MINIMUM ALLOWABLE HORIZONTAL SEPARATION BETWEEN COMPANY FACILITIES AND "NET" FACILITIES IS 3" WITH A MINIMUM 1" OF UNDISTURBED EARTH OR THE INSTALLATION OF A SUITABLE BARRIER BETWEEN THE FACILITIES. IF A 3" SEPARATION SEPARATION CANNOT BE ATTAINED BETWEEN "NET" UTILITIES AND COMPANY FACILITIES, A VARIANCE MAY BE APPROVED BY THE LOCAL INSPECTION SUPERVISOR AND SUBMITTED TO THE SERVICE PLANNING SUPPORT PROGRAM MANAGER FOR APPROVAL. SEPARATIONS OF "0" OR LESS ARE NOT PERMISSIBLE AND WILL NOT BE ALLOWED. THE COMPANY MAY AGREE TO MAINTAIN A MINIMUM 3" SEPARATION BETWEEN "NET" UTILITIES AND COMPANY FACILITIES IF WARRANTED AND THE NEED IS JUSTIFIED. THE REQUEST FOR A VARIANCE MUST:
  - BE MADE IN WRITING AND SUBMITTED TO THE COMPANY AIDE DURING THE PLANNING AND DESIGN PHASE OF THE PROJECT.
  - CLEARLY DESCRIBE THE CONDITIONS NECESSITATING THE VARIANCE.
  - INCLUDE A PROPOSED BARRIER AND INCLUDE BARRIER BETWEEN THE "NET" UTILITIES AND COMPANY DRY FACILITIES IN THE EVENT 1" OF UNDISTURBED EARTH CANNOT BE MAINTAINED. NOTE: DRAIN LINES CONNECTED TO DOWNSPUTS ON BUILDINGS ARE CONSIDERED A "NET" UTILITY FOR THE PURPOSES OF THIS STANDARD.
- SEPARATIONS SHALL BE MAINTAINED AT ABOVE COVER TERMINATION POINTS.
- PROCEDURES FOR APPROVING NATIVE BACKFILL FOR SHADING OF PG&E GAS FACILITIES:
  - RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF 3 LOCATIONS PER 1,000' OF TRENCH. 100% OF THE SAMPLE MUST PASS THROUGH A 1/2" SIEVE AND 75% MUST PASS THROUGH A #4 SCREEN. ADDITIONAL SAMPLES MUST BE TAKEN IF EXISTING SOIL CONDITIONS CHANGE AND ARE TO BE TAKEN AT THE DISCRETION OF THE PG&E REPRESENTATIVE ON SITE.
  - THE SOILS MUST NOT CONTAIN ANY ROCKS THAT HAVE SHARP EDGES OR THAT MAY OTHERWISE BE ABRASIVE.
  - THE SOILS MUST NOT CONTAIN CLOSERS LARGER THAN 1/2" IF TO BE USED AS SHADING, BEDDING OR LEVELING.
  - COMPARISON REQUIREMENTS MUST MEET ANY APPLICABLE PG&E FEDERAL, STATE, COUNTY OR LOCAL REQUIREMENTS. AS AT NO TIME SHALL THE OVER SATURATION OF NATIVE SOILS BE USED TO ACHIEVE THESE REQUIREMENTS. THE SEVES AND SCREENING SHALL BE:
  - 1/2" SIEVE: 8" DIAMETER BY 2" DEEP, STAINLESS STEEL WESH SCREEN.
  - #4 SCREEN: 8" DIAMETER BY 2" DEEP, STAINLESS STEEL WESH SCREEN.
- PROCEDURES FOR APPROVING NATIVE BACKFILL FOR SHADING AT PG&E ELECTRIC FACILITIES:
  - RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF 3 LOCATIONS PER 1,000' OF TRENCH. ADDITIONAL SAMPLES SHALL BE TAKEN IF EXISTING SOIL CONDITIONS CHANGE AND ARE TO BE TAKEN AT THE DISCRETION OF THE PG&E REPRESENTATIVE ON SITE.
  - SHADING LARGE ROCK, PAWING MATERIAL, CORNERS, SHARPLY ANGULAR SUBSTANCES, OR CORROSIVE MATERIAL SHALL NOT BE PLACED IN THE TRENCH WHERE SUCH MATERIAL MAY DAMAGE THE CONDUITS AND/OR PREVENT PROPER COMPACTION OVER OR AROUND THE CONDUIT.
  - NATIVE SOILS CONTAINING CLOSERS NOT TO EXCEED 6" IN DIAMETER MAY BE INCLUDED IN THE SHADING MATERIAL PROVIDED THE CLOSERS ARE HEAVILY BREAKABLE BY HAND NOTE: CONSIDER PRIMARILY OF ADobe, HARD COMPOST (GENSL CLAY, AND BAY MUD) SHALL NOT BE USED AS SHADING MATERIAL.
  - AT NO TIME SHALL THE OVER SATURATION OF NATIVE SOILS BE USED TO ACHIEVE THESE REQUIREMENTS. REFER TO ENGINEERING DOCUMENT NUMBER 13 ON PG&E 4128.
- COMPETENT NATIVE SOILS ARE PREFERRED TO BE USED FOR SHADING, BEDDING, AND BACKFILL THROUGHOUT THE TRENCH.
- WHERE NATIVE SOILS EXCEED 1/2" MINUS AND/OR WHERE GAS IS TO BE PLACED AT THE BOTTOM OF A TRENCH IN AREAS THAT EXCEED 1/2" MINUS SOIL CONDITIONS, OR WHERE THE BOTTOM OF A TRENCH IS CONSIDERED TO CONSIST OF HARD SOIL, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SHADING AND/OR BEDDING OF GAS FACILITIES.
- PG&E APPROVED IMPORT MATERIALS PER CGE ENGINEERING GUIDELINE 4128.
- IF A VARIANCE IS REQUIRED FOR GAS FACILITIES, THE USE OF NATIVE SOILS IS PREFERRED, BUT IF 1/2" MINUS CONDITIONS ARE NOT ATTAINABLE WITH THE NATIVE SOILS, THEN THE USE OF PG&E APPROVED IMPORT MATERIALS IS REQUIRED. BEDDING UNDER GAS FACILITIES WILL BE A MINIMUM OF 3" OF COMPACTED 1/2" MINUS NATIVE SOILS OR PG&E APPROVED IMPORT MATERIAL.
- IF THE USE OF NATIVE SOILS IS NOT FEASIBLE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SHADING AND/OR BEDDING OF GAS FACILITIES. THE USE OF ANY IMPORTED MATERIAL FOR BACKFILLING PURPOSES SHALL BE LIMITED TO THOSE SITUATIONS WHEN NATIVE SOILS DO NOT ALLOW FOR REQUIRED COMPACTION.
- THE APPLICANT IS RESPONSIBLE FOR THE REMOVAL OF EXCESS SPOIL AND ASSOCIATED COSTS.
- SEPARATION BETWEEN GAS FACILITIES AND ELECTRIC FACILITIES MAY BE REDUCED TO 6" WHEN CROSSING.
- SERVICE SADDLES ARE THE PREFERRED SERVICE FITTINGS FOR USE THROUGHOUT THE JOINT TRENCH PROJECT. ALL PROJECTS WILL BE DESIGNED AND ESTIMATED USING SERVICE SADDLES. HOWEVER, SERVICE TEES MAY BE USED IF ALL CLEARANCES, SEPARATION, AND COVERAGE REQUIREMENTS ARE MAINTAINED.
- CONTRACTOR TO INCREASE METER SPACING AS NECESSARY WHEN EXHAUSTION VALVES OR OTHER ADDITIONAL SAFETY EQUIPMENT ARE REQUIRED. EXHAUSTION VALVES OR OTHER ADDITIONAL SAFETY EQUIPMENT ARE NOT PART OF PG&E/CONTRACTOR SPACING REQUIREMENTS DO NOT INCLUDE CLEARANCE FOR EXHAUSTION VALVES.

### TRENCHING CLEARANCE REQUIREMENTS:

- ABOVE ANY SINGLE PHASE TRANSFORMER LOCATION, MAINTAIN 20' UNOBSTRUCTED OVERHEAD CLEARANCE OVER TRANSFORMER VAULT/PAD.
- ABOVE ANY THREE PHASE TRANSFORMER LOCATION, MAINTAIN 30' UNOBSTRUCTED OVERHEAD CLEARANCE OVER TRANSFORMER VAULT/PAD.

### GAS PIPELINE UNDERGROUND WARNING TAP NOTES:

- A WARNING TAP IS TO BE INSTALLED IN OPEN TRENCH INSTALLATION OVER GAS PIPELINES IN BOTH TRENCH AND UNBROKEN FACILITIES. THIS INCLUDES TRENCHES, BELL WALES, EXCAVATIONS FOR REPAIR PURPOSES AND RISER RELATIONS. THE WARNING TAP IS INTENDED FOR EXHAUSTION DIGGING IN THE "TOLERANCE ZONE" TO STRIKE THE WARNING TAP PRIOR TO THE PIPELINE. WHEN THE WARNING TAP IS STRUCK, IT SHOULD BE SHARED WITH EXCAVATING EQUIPMENT IT STRIKES WITHOUT BREAKING, THIS ALERTING THE EXCAVATOR OF THE FACILITY LOCATION.
- INSTALL 6" WIDE WARNING TAP ABOVE THE GAS PIPELINE AT LEAST 12" BELOW GRADE, AND NO CLOSER THAN 12" FROM THE PIPE. INSTALLATION SHALL BE MADE BETWEEN THE FINISHED GRADE AND THE WARNING TAP AS FAR AS POSSIBLE. INSTALL THE TAP ALONG THE LENGTH OF THE TOLERANCE ZONE. WHEN THE WARNING TAP IS STRUCK, IT SHOULD BE SHARED WITH EXCAVATING EQUIPMENT IT STRIKES WITHOUT BREAKING, THIS ALERTING THE EXCAVATOR OF THE FACILITY LOCATION.
- WARNING TAP SHALL BE BRIGHTLY COLORED YELLOW AND MARKED "CAUTION: GAS LINE BURIED BELOW SURFACE" WITH A SIMILAR MARKING.
- WARNING TAP SHALL BE STORED IN SUCH A MANNER THAT LIMITS ULTRAVIOLET (UV) EXPOSURE.



### PG&E PM#S:

ELECTRIC:  
GAS:

DESIGN CHANGE COMPONENT  
ANY CHANGES TO THIS DESIGN MUST BE APPROVED BY  
PG&E GAS AIDE

### CONSTRUCTION NOTES:

- ALL TRENCHING, BACKFILLING AND INSTALLATION BY CONTRACTOR MUST COMPLY WITH PG&E UO STANDARD 55453 (EFFECTIVE DATE 7-5-2006).
- ALL WORK MUST COMPLY WITH P.G. & E. TELEPHONE, C.A.T.V., STANDARDS AND PRACTICES. ALL WORK MUST BE INSPECTED AND APPROVED BY RESPECTIVE INSPECTORS. RANDOM SOIL SAMPLES SHALL BE TAKEN FROM A MINIMUM OF THREE LOCATIONS PER 1,000' OF TRENCH. 100% OF THE SAMPLES MUST PASS THROUGH A 1/2" SIEVE AND 75% MUST PASS THROUGH A #4 SCREEN. ADDITIONAL SAMPLES MUST BE TAKEN IF EXISTING SOIL CONDITIONS CHANGE AND ARE TO BE TAKEN AT THE DISCRETION OF THE PG&E REPRESENTATIVE ON SITE. THE SOILS MUST NOT CONTAIN ANY ROCKS THAT HAVE SHARP EDGES OR THAT MAY OTHERWISE BE ABRASIVE. THE SOILS MUST NOT CONTAIN CLOSERS LARGER THAN 1/2" IF TO BE USED AS SHADING, BEDDING OR LIFTING MATERIALS. COMPARISON REQUIREMENTS MUST MEET ANY APPLICABLE P.G.&E. FEDERAL, STATE, COUNTY OR LOCAL REQUIREMENTS. ANY NATIVE SOILS OR IMPORT MATERIALS USED MUST NOT HINDER THESE OPERATIONS.
- BACKFILL SHALL BE APPROVED BY THE UTILITY COMPANIES AND THE CITY. COMPACTION WILL BE TESTED AND PASSED BY THE SOILS ENGINEER.
- IF SOIL IS NOT ROCK FREE, ADD 4" DEPTH OF TRENCH FOR SAND BEDDING.
- VERIFY SPICE BOX EXCAVATION SIZES WITH SUPPLIERS).
- THE TRENCHING CONTRACTOR SHALL COORDINATE THE UTILITY COMPANIES' INSTALLATION. THE TRENCHING CONTRACTOR TO PLACE CONNECTING CONDUIT WITHIN 5' OF BUILDING ELEVATION.
- CONTRACTOR SHALL MAKE HIMSELF FAMILIAR WITH THE PROJECT IMPROVEMENT PLANS AND CONDUCT HIS WORK ACCORDINGLY.
- IT IS THE TRENCHING CONTRACTOR'S RESPONSIBILITY TO PROTECT IN PLACE ALL EXISTING FACILITIES. NO EXTRA PAYMENT WILL BE CONSIDERED FOR CROSSING OTHER SYSTEMS.
- VIZION UTILITY PARTNERS ASSUMES NO RESPONSIBILITY FOR THE PROJECT CONDITIONS. THESE DRAWINGS WERE PREPARED USING DATA SUPPLIED BY PG&E TELEPHONE, C.A.T.V., IMPROVEMENT PLANS AND THE CITY'S WINDOUS "AS BUILT" INFORMATION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PHYSICALLY REVIEW THE PROJECT PRIOR TO SUBMITTING HIS BID.
- CONTRACTOR WILL COMPLY WITH ALL LAWS, ORDINANCES AND REGULATIONS. CONTRACTOR SHALL BE FAMILIAR WITH O.S.H.A., INDUSTRIAL SAFETY ORDERS AND SHALL CONDUCT HIS WORK ACCORDINGLY. WHEN WORKING NEAR ENERGIZED OR "HOT" EQUIPMENT, THE UTILITY OWNER SHALL BE NOTIFIED TO SUPPLY THE APPROPRIATE MAN POWER. PUBLIC SAFETY AND TRAFFIC CONTROL MEASURES ARE THE CONTRACTOR'S RESPONSIBILITY.
- THE CONTRACTOR SHALL PROTECT CONSTRUCTION STAKING. HE SHALL COORDINATE STAKING WITH THE PROJECT'S CIVIL ENGINEER.
- CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (USA) 2 WORKING DAYS PRIOR TO START OF WORK.
- CONTRACTOR SHALL NOTIFY INSPECTORS OF ANY POTENTIAL CONFLICTS PRIOR TO START OF WORK.
- THIS PLAN IS TO BE USED FOR SOLE PURPOSE OF DIGGING THE JOINT TRENCH. SEE PG&E, AT&T, AND COMCAST PLANS FOR EXACT SIZE AND NUMBER OF CONDUITS INSTALLED IN THE JOINT TRENCH. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THE CORRECT NUMBER, SIZE AND TYPES OF CONDUITS ARE INSTALLED PER THE ENGINEERED PLANS BY EACH UTILITY COMPANY.
- NOTE: PLANS ISSUED AT THE PRE-CONSTRUCTION MEETING MAY BE SUBJECT TO REVISIONS, IF FINAL PLANS FROM EACH UTILITY COMPANY WERE NOT AVAILABLE AT THE START OF CONSTRUCTION.
- WATER, SEWER, RISERS, SANITARY WASTE, FUELS (INCLUDING DIESEL AND GASOLINE), OIL, PROPANE AND OTHER VOLATILE HEAVIER THAN AIR GASES, STEAM, HOT WATER, AND OTHER "NET" FACILITIES SHALL MAINTAIN A MINIMUM OF THREE FEET FROM THE NEAREST UNDER SURFACE OF PG&E FACILITIES WITH NO LESS THAN ONE FOOT OF EARTH SOIL BARRIER BETWEEN THE ADJACENT SIDES OF THE INDIVIDUAL TRENCHES.
- IN THE EXTRAORDINARY CASE THAT THE MINIMUM THREE FOOT HORIZONTAL SEPARATION CANNOT BE ATTAINED BETWEEN "NET" UTILITIES AND COMPANY DRY FACILITIES, A VARIANCE MAY APPROVED BY THE LOCAL INSPECTION SUPERVISOR AND SUBMITTED TO SERVICE PLANNING SUPPORT PROGRAM MANAGER FOR APPROVAL.
- ALL METER PANELS: INDIVIDUAL RESIDENTIAL OR NONRESIDENTIAL APPLICANTS WITH A METER PANEL RATING OF ANY SIZE, INSTALLED INSIDE A METER ROOM OR OTHER STRUCTURE, MUST FOLLOW ALL OF THE REQUIREMENTS DESCRIBED BELOW.
  - INSTALL OWN AND MAINTAIN A SEPARATE, NOMINAL, 2-INCH DIAMETER CONDUIT WITH FULL TAPES INSIDE OF THE METER ROOM AND FULL TAPES MUST EXTEND FROM THE OUTSIDE SURFACE OF THE BUILDING AND TERMINATE OUTSIDE.
  - THE METER PANEL OR SWITCHBOARD AT THE TOP OF THE METER SECTION.
  - CONDUIT THE 2-INCH DIAMETER CONDUIT OR FULL TAPES EXTEND THE OUTSIDE OF THE BUILDING A MINIMUM OF 8 FEET AND A MAXIMUM OF 10 FEET ABOVE GRADE, THE OPEN END OF THE CONDUIT THAT IS EXPOSED TO THE OUTSIDE MUST HAVE A REMOVABLE TEMPORARY CAP OR PLUG.
  - DO NOT USE THE CONDUIT, THE CONDUIT IS FOR PG&E'S METERING EQUIPMENT ONLY.
- THIS JOINT TRENCH PLAN WAS PREPARED BASED ON TOPOGRAPHICAL SURVEY AS PROVIDED BY A CIVIL ENGINEER. THE CONTRACTOR IS CAUTIONED THAT EXPLORATORY WORK IS NECESSARY TO DETERMINE THE ACTUAL LOCATION OF ANY EXISTING UTILITY. VIZION UTILITY PARTNERS STRONGLY RECOMMENDS THAT ALL UTILITIES BE PHYSICALLY LOCATED ON THE SITE BEFORE THE ONSET OF SITE WORK. SUBSTRUCTURE LOCATIONS MAY REQUIRE FIELD ADJUSTMENT TO COMPENSATE FOR ACTUAL EXISTING UTILITY LOCATIONS.

### SUBSTRUCTURE VERIFICATION STAMP

DEVELOPER  
PLEASE NOTE AND SIGN  
ALL PG&E ENCLOSURES AND BOXES HAVE BEEN SET TO GRADE. ACCORDING TO GRADE STAKES PROVIDED BY THE DEVELOPER'S ENGINEER. ALL COSTS TO RELOCATE OR RE-WORK BOXES AT A LATER DATE WILL BE BORNE BY THE DEVELOPER. PLEASE HAVE YOUR SIFT, VERIFY THE CORRECT GRADE OF ALL ENCLOSURES OR BOXES, AND SIGN AND DATE DRAWING. THANK YOU!  
SIGNED: \_\_\_\_\_  
DATE: \_\_\_\_\_

### UTILITY APPROVALS

UTILITY	APPROVED BY	DATE
PG&E ELECTRIC		
PG&E GAS		
AT&T (PHONE)		
COMCAST (CATV)		
CITY ENGINEER		

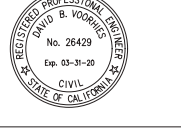
### FOR VIZION USE ONLY

Q/A REVIEW	INITIALS	DATE
NETW (ELEC)		
NETW (GAS)		
COMPOSITE		
PRE-COM		

DEVELOPER:  
706-716 SANTA CRUZ AVE LLC  
700 SANTA CRUZ AVE  
MENLO PARK, CA 94025  
VASILE C. OROS  
T: 415-260-0608  
EMAIL: vros11@AOL.COM

### SHEET INDEX

- JT-1 JOINT TRENCH TITLE SHEET
- JT-2 JOINT TRENCH INTENT



DATE: \_\_\_\_\_  
REVISION: \_\_\_\_\_  
SHEET NO: \_\_\_\_\_

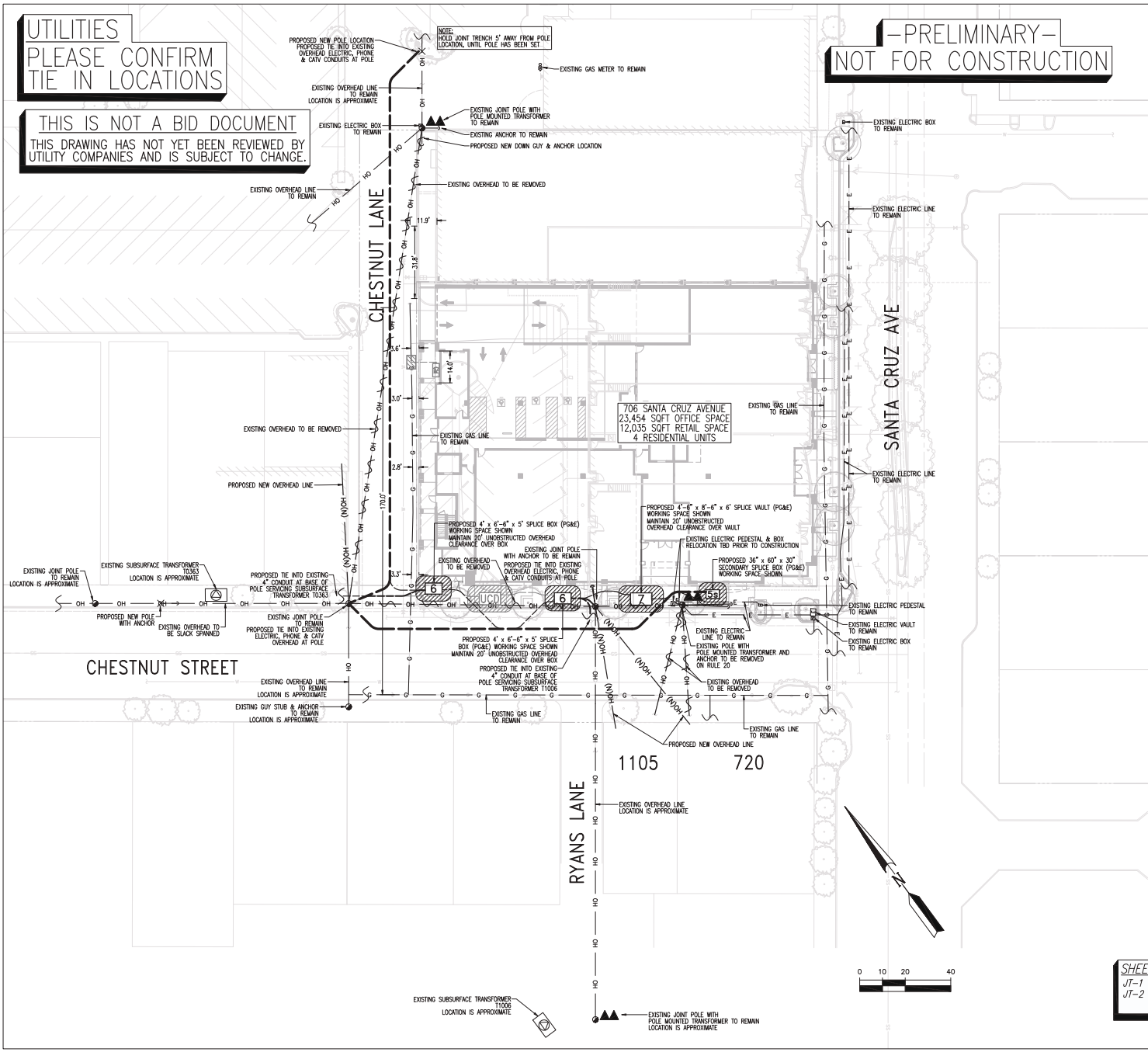
**Vizion Utility**  
PARTNERS  
UTILITY ENGINEERS, CONSULTANTS & ARCHITECTS DESIGN  
1000 RIVERVIEW DRIVE, SUITE 100, MENLO PARK, CA 94025  
(650) 321-1100  
WWW.VIZIONUTILITY.COM

PROJECT NO: 19-969  
SCALE: N.T.S.  
DESIGNER: J. KLEIN  
CHECKER: M. HOANG  
DATE: 10-28-2019  
DRAWING NO: JT-1  
SHEET: 1 OF 2

**UTILITIES**  
PLEASE CONFIRM  
TIE IN LOCATIONS

**THIS IS NOT A BID DOCUMENT**  
THIS DRAWING HAS NOT YET BEEN REVIEWED BY  
UTILITY COMPANIES AND IS SUBJECT TO CHANGE.

**-PRELIMINARY-**  
**NOT FOR CONSTRUCTION**



**NOTE TO CONTRACTOR:**  
FOR CONTRACTOR'S WORK RESPONSIBILITY,  
REFER TO JOINT TRENCH TITLE SHEET (JT-1)

**LEGEND**

- PROPOSED JOINT TRENCH
- x PROPOSED NEW POLE
- [Symbol] PROPOSED 4'-6" x 8'-6" x 6' SPLICE VAULT (PG&E) WORKING SPACE SHOWN MAINTAIN 20' UNOBSTRUCTED OVERHEAD CLEARANCE OVER VAULT
- [Symbol] PROPOSED 4' x 6'-6" x 5' SPLICE BOX (PG&E) WORKING SPACE SHOWN MAINTAIN 20' UNOBSTRUCTED OVERHEAD CLEARANCE OVER BOX
- [Symbol] PROPOSED 36" x 60" x 30" SECONDARY SPLICE BOX (PG&E) WORKING SPACE SHOWN
- E EXISTING ELECTRIC LINE
- EXISTING JOINT POLE TO REMAIN
- ▲ EXISTING POLE MOUNTED TRANSFORMER
- [Symbol] EXISTING ELECTRIC VAULT
- [Symbol] EXISTING SUBSURFACE TRANSFORMER
- EXISTING ELECTRIC PEDESTAL
- EXISTING ELECTRIC BOX
- OH EXISTING OVERHEAD LINE
- G EXISTING GAS LINE
- OH EXISTING OVERHEAD TO BE REMOVED
- ▲ EXISTING POLE MOUNTED TRANSFORMER TO BE REMOVED

SUBSTRUCTURE LOCATIONS MUST BE STAKED BY A LICENSED SURVEYOR PRIOR TO CONSTRUCTION. SEE CONSTRUCTION NOTES ON JOINT TRENCH TITLE SHEET (JT-1) REGARDING EXISTING CONDITIONS.

REFER TO PG&E UTILITY BULLETIN TD-7001B-005 DOCUMENT FOR SMART METER ANTENNA CONSTRUCTION REQUIREMENTS.

**NOTE TO COMCAST:**  
PLEASE CONFIRM WHO WILL PROVIDE CONDUIT AND VAULTS. DEVELOPER TO PROVIDE TRENCH.

**NOTE FOR UNDERGROUND ELECTRIC INSTALLATION:**  
USE OF STANDARD PVC DB-120 IS NO LONGER APPROVED BY PG&E FOR 2" CONDUIT SIZE AND BENDS. PVC DB-120 CELLULAR CORE CONDUIT CAN BE USED IN PLACE OF STANDARD PVC DB-120 CONDUIT. FOR ALL APPROVED 2" CONDUITS AND BENDS, SEE BULLETIN TD-062288B-001.

**SHEET INDEX**

JT-1	JOINT TRENCH TITLE SHEET
JT-2	JOINT TRENCH INTENT



DATE:	
REVISION:	
CHECKING:	
DATE BEFORE:	
DATE AFTER:	
<b>JOINT TRENCH INTENT</b> <b>706 SANTA CRUZ AVENUE</b> <b>WRO</b> 706-716 SANTA CRUZ AVE LLC MENLO PARK CALIFORNIA	
PROJ. NO:	19-969
SCALE:	1" = 20'
PREP:	J. KLEIN
DRAWN BY:	M. HOANG
CHECKED BY:	
LAST UPDATED:	10-28-2019
DRAWING NO.:	JT-2
SHEET:	2 of 2

DRAWING NAME: \\net\proj\2019\19-969\206\_santa\_cruz\_wro.dwg, mchd par\pdp\wp\19-969\_206.dwg  
 DATE PLOTTED: Monday, October 28, 2019 10:02:32 AM  
 PLOTTED BY: MNH HOANG

M70



# TEMPORARY EARTH RETENTION SYSTEM

## 706-716 SANTA CRUZ AVENUE

### MENLO PARK , CA

PRELIMINARY DESIGN  
NOT FOR CONSTRUCTION

FOR:

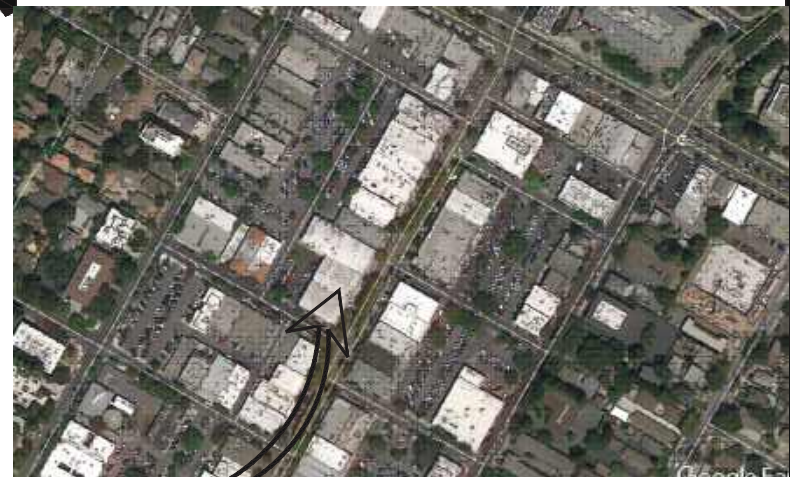


**PBA Inc.**  
Structural Engineering  
Geo-Structures • Earth Retention

410 3th Ave., Suite 100, San Rafael, CA 94903  
TEL: (415) 458-0181

www.pbandinc.com  
pba@pbandinc.com

JOB NO. 170401  
JUNE 24, 2019



PROJECT LOCATION

### SITE LOCATION PLAN

**SCOPE OF WORK**

AS A PART OF MIXED USE BUILDING 706-716 SANTA CRUZ AVENUE PROJECT IN MENLO PARK, CA, IT IS NECESSARY TO PROVIDE TIEBACK SOLDIER BEAM SHORING WALL WITH UNDERPINNING DESIGN.

**GENERAL**

- ALL CONSTRUCTION SHALL CONFORM TO THE GEOTECHNICAL ENGINEERING CIRCULAR NO. 4, PUBLICATION NO. FHWA-IF-99-015 - GROUND ANCHORS AND ANCHORED SYSTEMS PUBLISHED BY FHWA JUNE 1999, CALIFORNIA BUILDING CODE (2016 ED.), AND REGULATIONS OF CITY OF MENLO PARK, CA.
- ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE GUIDELINES ESTABLISHED IN THE "TRENCHING AND SHORING MANUAL" PUBLISHED BY OFFICE OF STRUCTURES, CALTRANS.
- THESE DRAWINGS ARE BASED ON AND MUST BE READ IN CONJUNCTION WITH 706 SANTA CRUZ AVE. MENLO PARK ARCHITECTURAL REVIEW PLAN DATED MAY 13, 2019 BY FORM4 ARCHITECTURE.
- THE DATA FOR THE ALIGNMENT OF THE WALL, ITS GEOMETRY AND ELEVATIONS ARE FOR REFERENCE ONLY AND ARE BASED ON THE INFORMATION GIVEN ON FOUNDATION PLANS. THE GENERAL CONTRACTOR IS TO VERIFY ALL DIMENSIONS, OFFSETS, ELEVATIONS AND CONDITIONS AT THE SITE AND REPORT ANY DISCREPANCIES AND LACK OF COORDINATION BETWEEN THESE DRAWINGS AND FOUNDATION PLAN TO THE EARTH RETENTION ENGINEER, PB&A INC., IN A TIMELY MANNER.
- LINES AND GRADES SHALL BE ESTABLISHED BY THE GENERAL CONTRACTOR TO INSURE PROPER HORIZONTAL ALIGNMENT OF WALL AND LOCATION OF THE SOLDIER BEAMS.
- THE GENERAL CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES AND OTHER OBSTACLES. ALL EXISTING UTILITIES SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY. GENERAL CONTRACTOR IS RESPONSIBLE TO POTHOLE ALL UTILITIES (AS NEEDED) BEFORE SHORING WALL CONSTRUCTION IS TO BEGIN. GENERAL CONTRACTOR IS TO REPORT ALL INTERFERENCES BETWEEN THE UTILITIES OR OTHER OBSTACLE WITH THE SOLDIER BEAM AND TIEBACKS TO THE SHORING ENG., PB&A IN A TIMELY MANNER.
- THE SHORING SYSTEM IS DESIGNED FOR A SURCHARGE WHICH INCLUDES REGULAR TRAFFIC LOADING AND LIGHT WEIGHT CONSTRUCTION EQUIPMENT, SUCH AS CONCRETE TRUCKS, ETC. CRANES AND DRILLED RIG EXCLUDED. HEAVY CONSTRUCTION EQUIPMENT IS TO BE KEPT A DISTANCE EQUIVALENT TO THE DEPTH OF THE EXCAVATION AWAY FROM THE EDGE OF THE RETAINING WALL.

**DESIGN CRITERIA**

THE DESIGN OF THE EARTH RETENTION SYSTEM IS BASED ON THE TEST BORING LOGS AND THE INFORMATION CONTAINED IN THE "GEOTECHNICAL INVESTIGATION FOR MIXED-USE BUILDING 706-716 SANTA CRUZ AVENUE MENLO PARK, CALIFORNIA 94025", DATED DECEMBER, 2015, PREPARED BY ROMIG ENGINEERS, INC.

**MATERIALS-SOLDIER BEAM, AND TIEBACKS:**

- TIEBACK ANCHORS SHALL BE 0.6" DIA. 7-WIRE, LOW RELAXATION 270 KSI STRAND CONFORMING TO ASTM A416.
- TIE ROD SHALL BE THREADED BAR TO CONFORM TO ASTM A722 GRADE 150.
- BAR COUPLERS SHALL DEVELOP THE FULL ULTIMATE TENSILE STRENGTH OF THE BAR AS CERTIFIED BY THE MANUFACTURER.
- CENTRALIZERS - MANUFACTURED FROM PVC PIPING.
- STRUCTURAL STEEL FOR WIDE FLANGE SOLDIER BEAMS SHALL CONFORM TO ASTM A36 OR EQUAL GRADE.
- STRUCTURAL STEEL FOR MISCELLANEOUS ITEMS SUCH AS STIFFENER AND BEARING PLATES PACKING, ANGLES, CONFORM TO THE REQUIREMENTS OF ASTM A36.
- GROUT USED IN TIEBACKS SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3,000 P.S.I. MIN. 6 SACKS OF CEMENT PER CUBIC YARD.
- ALL WELDING SHALL CONFORM TO A.W.S. STANDARDS AND BE DONE BY CERTIFIED WELDERS HAVING A CURRENT CERTIFICATION & USING E70XX ELECTRODES.
- LAGGING SHALL BE D.F. #2.
- LEAN CONCRETE MIX FOR BACKFILLING THE DRILLED HOLE BEAM EMBEDMENT BELOW THE BOTTOM OF THE EXCAVATION IS TO CONTAIN MIN. 2 SACK OF CEMENT PER CU. YD. OF CONCRETE.

**CONSTRUCTION PROCEDURE FOR TIEBACK SOLDIER BEAM SHORING**

- DRILL HOLES FOR SOLDIER BEAMS AT THE LOCATION AND TO THE DEPTH AS INDICATED ON THE PLANS.
- BACKFILL THE DRILLED HOLE BELOW AND ABOVE THE BOTTOM OF THE EXCAVATION WITH LEAN CONCRETE MIX PER NOTE 10 ABOVE.
- EXCAVATE AND PLACE LAGGING TO PREVENT LOSS OF GROUND. PLACEMENT OF LAGGING BELOW THE DEPTH OF 20' MAY CEASE, W/ WRITTEN PERMISSION FROM THE SHORING ENGINEER IN APPROPRIATE LOCATIONS AND AS GROUND CONDITIONS WARRANT.
- STEEL PLATE LAGGING MAY BE USED ABOVE TIEBACK LOCATION FOR TIEBACK BEAMS AND FOR THE ENTIRE DEPTH OF EXCAVATION FOR THE CANTILEVER BEAMS.
- CONTINUE TO EXCAVATE TO NOT MORE THAN 1'-6" BELOW THE LEVEL OF THE TIEBACK OR AS SOIL PERMITS. THEN DRILL HOLE FOR THE TIEBACKS TO THE SUGGESTED DEPTH AS NECESSARY. FILL THE DRILLED HOLE THROUGH OUT WITH GROUT TIEBACKS MAY BE REGROUTED AS NECESSARY TO ASSURE COMPLIANCE WITH THE TESTING SCHEDULE.
- EXCAVATION IS TO PROCEED IN LIFTS AS SOIL STABILITY ALLOWS, (5' MAX.). AN APPROPRIATE BERM IS TO BE CREATED SO THAT THE DRILL RIG CAN DRILL THE HOLE FOR THE TIEBACKS.
- TEST TIEBACKS NO EARLIER THAN 3 DAYS AFTER GROUTING ACCORDING TO THE PROCEDURE DESCRIBED ON THIS SHEET.
- CONTINUE EXCAVATION AND LAG.
- ANY LOSS OF GROUND FROM BEHIND LAGGING IS TO BE REPLACED WITH LEAN MIX OF CEMENT SAND SLURRY.

**TIEBACK TESTING PROCEDURE:**

EVERY TIEBACK SHALL BE PROOF TESTED. TESTING SHALL BE PERFORMED BY INCREMENTALLY LOADING THE TIEBACK IN ACCORDANCE WITH THE SCHEDULES SHOWN ON PLANS. AT EACH LOAD INCREMENT THE MOVEMENT OF THE END OF THE TIEBACK SHALL BE RECORDED TO THE NEAREST .001" WITH RESPECT TO AN INDEPENDENT FIXED REFERENCE POINT NOT ATTACHED TO THE SHORING WALL. THE ANCHOR LOAD SHALL BE MEASURED WITH A PRESSURE GAUGE CALIBRATED WITH THE JACK, WHICH SHALL BE ACCURATE ENOUGH TO READ 200 PSI CHANGES IN HYDRAULIC PRESSURE, OR A 4-KIP (4000 POUNDS) DIFFERENTIAL IN FORCE, WHICHEVER IS THE LESSER. THE PUMP SHALL BE CAPABLE OF APPLYING EACH LOAD INCREMENT IN LESS THAN ONE MINUTE, AND SHALL MAINTAIN THE TEST LOAD AS REQUIRED DUE TO CREEP OF THE TIEBACKS AND/OR MOVEMENT OF THE WALL.

FOR EACH PROOF TEST THE FOLLOWING INFORMATION SHALL BE RECORDED:

- TIEBACK NUMBER AND LOCATION.
  - INSTALLED FREE LENGTH AND BONDED LENGTH OF THE TIEBACK.
- DURING TIEBACK TESTING, THE FOLLOWING INFORMATION SHALL BE RECORDED FOR EACH LOAD INCREMENT AND CORRESPONDING OBSERVATION PERIOD:
- LOAD SEQUENCE NUMBER AND % OF DESIGN LOAD; MAXIMUM LOAD PER SCHEDULE.
  - DIAL PRESSURE AND CORRESPONDING LOAD IN KIPS (200 PSI INCREMENTS).
  - OBSERVATION PERIOD IN MINUTES AND/OR SECONDS FROM THE TIME AT WHICH THE SPECIFIED LOAD IS REACHED.
  - MOVEMENT OF THE END OF THE TIEBACKS TO THE NEAREST .001 INCH. ZERO MOVEMENT SHALL BE ASSUMED AT THE INITIAL ALIGNMENT LOAD (AL), WHICH IS SEQUENCE NUMBER 1.

**PROOF TEST**

THE PROOF TESTS SHALL BE PERFORMED BY INCREMENTALLY LOADING THE TIEBACKS IN ACCORDANCE WITH THE PROOF TEST SCHEDULE SHOWN ABOVE. AT EACH INCREMENT OF THE LOAD SHALL BE MAINTAINED UNTIL THE RATE OF TIEBACK MOVEMENT IS CLEARLY APPROACHING ZERO. THE MINIMUM OBSERVATION PERIOD SHALL BE ONE MINUTE. THE 125% DL MAXIMUM TEST LOAD SHALL BE HELD FOR AT LEAST 10 MINUTES, AND AS MUCH AS 60 MINUTES, AS DESCRIBED BELOW. FOR FINAL SEQUENCE MAXIMUM TEST LOAD, THE TIEBACK SHALL BE LOADED TO 125% DESIGN LOAD AND THE TIEBACK MOVEMENT SHALL BE RECORDED FOR OBSERVATION PERIODS OF 1, 2, 3, 4, 5, 7, AND 10 MINUTES. IF THE TOTAL ELONGATION BETWEEN 1 MINUTE AND 10 MINUTES DOES NOT EXCEED .04 INCHES, THEN THE TIEBACK SHALL BE CONSIDERED ACCEPTABLE AND THE TEST MAY BE DISCONTINUED. IF THE ELONGATION BETWEEN 1 MINUTE AND 10 MINUTES EXCEEDS .04 INCHES, THEN MONITORING OF TIEBACK MOVEMENT SHALL CONTINUE FOR AN ADDITIONAL 50 MINUTES, WITH MOVEMENTS RECORDED AT 15, 20, 25, 30, 45 AND 60 MINUTES SO THAT A CREEP CURVE CAN BE PLOTTED. AT THE END OF THE PROOF TEST THE TIEBACK SHALL BE LOCKED OFF AT LOCK-OFF LOAD, AS SPECIFIED IN THE SCHEDULE. THE MAXIMUM 125% DL TEST LOAD ELONGATION IS MEASURED FOR 60 MINUTES, THE TIEBACK WILL BE CONSIDERED ACCEPTABLE IF THE ELONGATION DOES NOT EXCEED 0.08".

PROOF TEST SCHEDULE
LOADING SEQUENCE
ALIGNMENT LOAD
0.25 DESIGN LOAD
0.50 DESIGN LOAD
0.75 DESIGN LOAD
1.00 DESIGN LOAD
1.25 DESIGN LOAD

**DRAWING LISTS**

- SH1.0 GENERAL NOTES
- SH2.0 SHORING KEY PLAN
- SH3.0 PLAN & ELEVATION
- SH3.1 PLAN & ELEVATION
- SH3.2 PLAN & ELEVATION
- SH3.3 PLAN & ELEVATION
- SH3.4 PLAN & ELEVATION
- SH3.5 PLAN & ELEVATION
- SH4.0 SECTION



PREPARED FOR: **TEMPORARY SHORING PLAN**  
 NOTES

PREPARED FOR: **TEMPORARY SHORING PLAN**  
 NOTES

PREPARED FOR: **TEMPORARY SHORING PLAN**  
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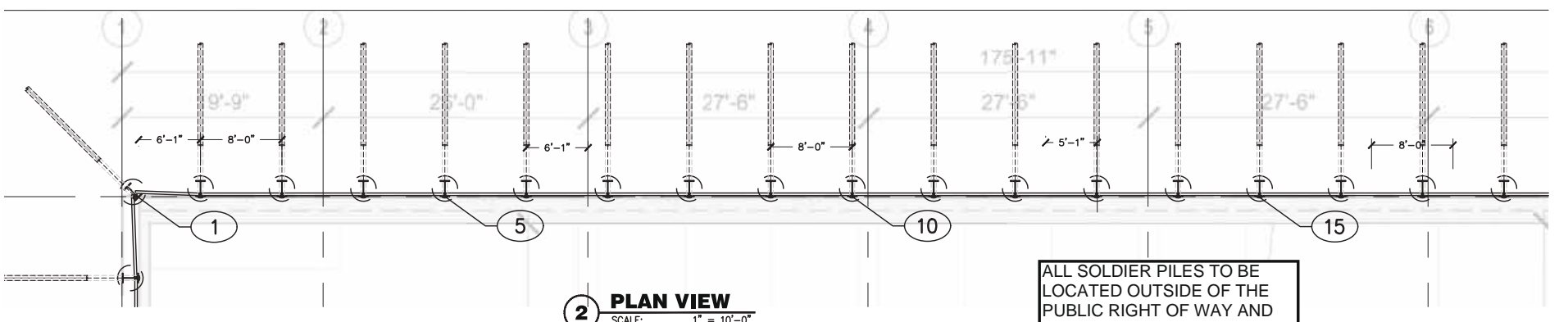
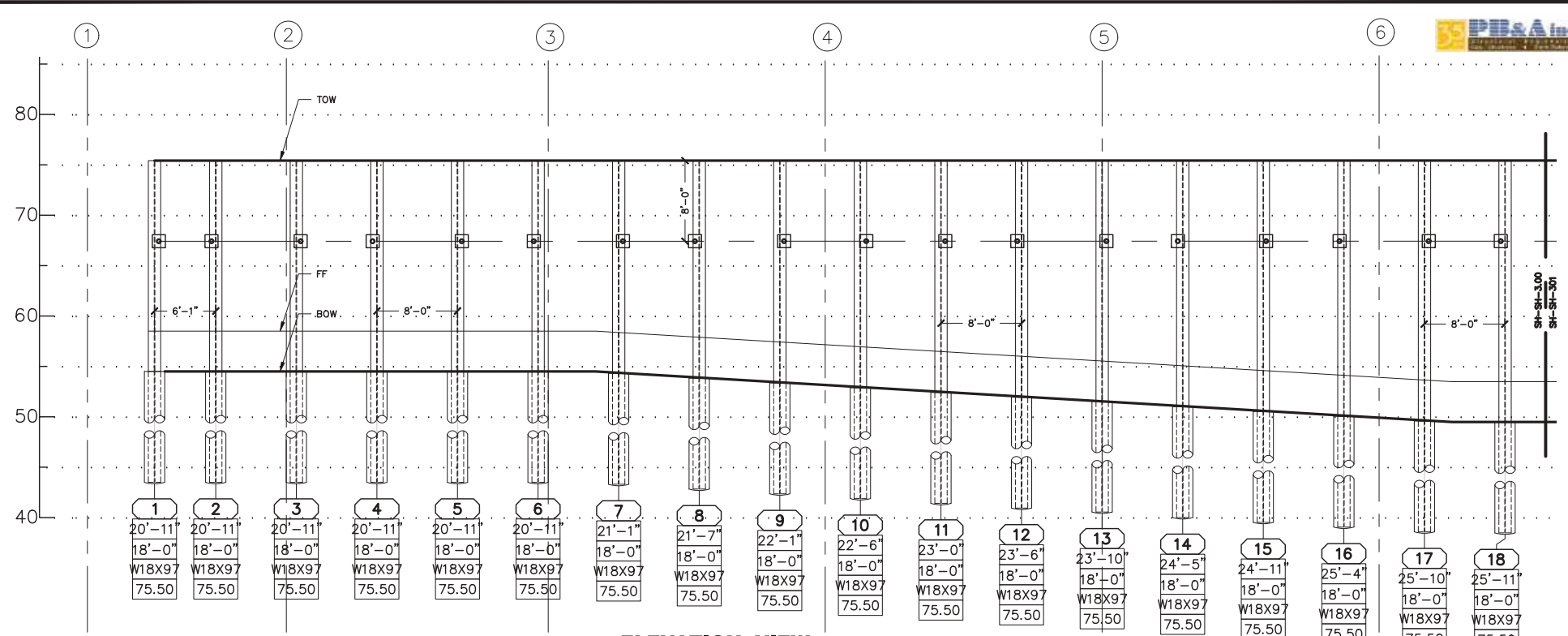
**ALL SOLDIER PILES TO BE LOCATED OUTSIDE OF THE PUBLIC RIGHT OF WAY AND WITHIN PROPERTY LINES PER CITY OF MENLO PARK REQUIREMENTS**

**PRELIMINARY DESIGN NOT FOR CONSTRUCTION**

170401  
06-24-2019

706-716 SANTA CRUZ AVENUE





ALL SOLDIER PILES TO BE LOCATED OUTSIDE OF THE PUBLIC RIGHT OF WAY AND WITHIN PROPERTY LINES PER CITY OF MENLO PARK REQUIREMENTS

PRELIMINARY DESIGN NOT FOR CONSTRUCTION

PREPARED FOR: PBA & A

PROJECT: TEMPORARY SHORING PLAN

ELEVATION

DATE: 06-24-2019

706-716 SANTA CRUZ AVENUE

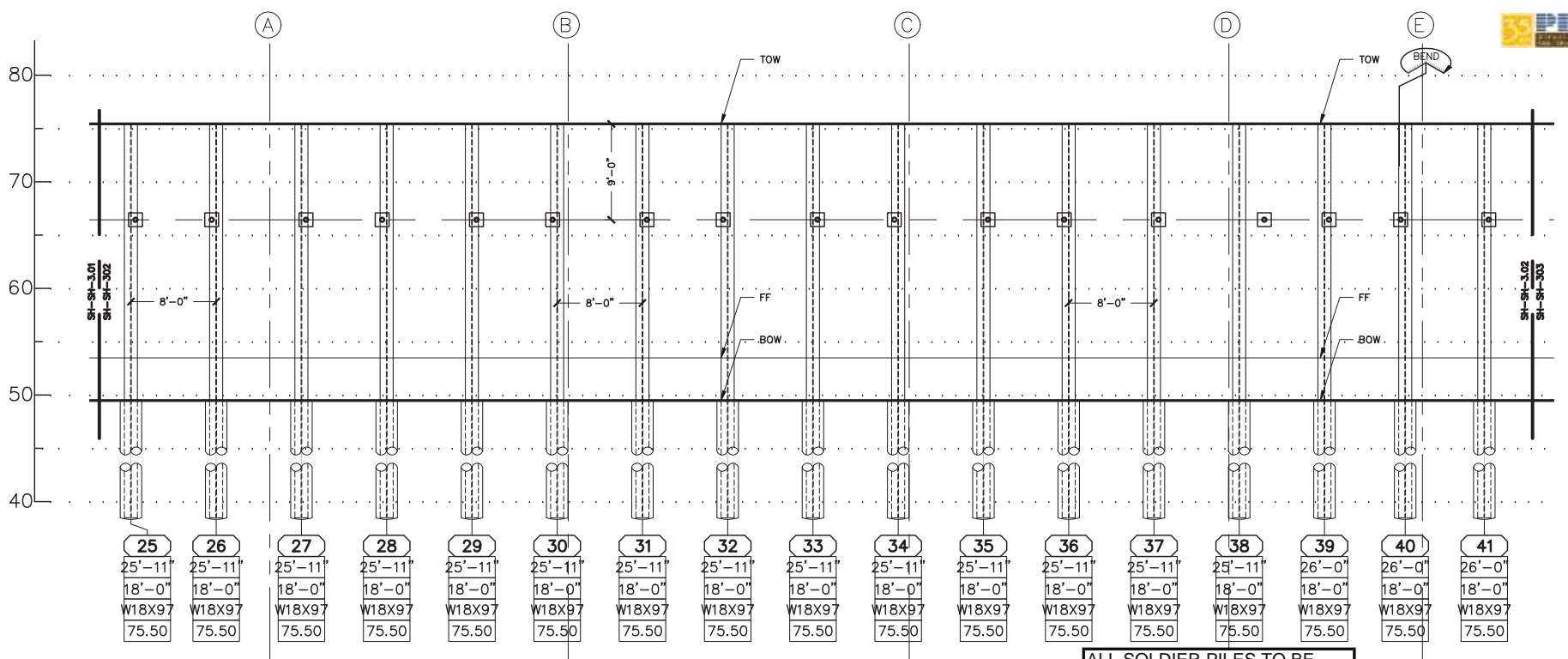
170401

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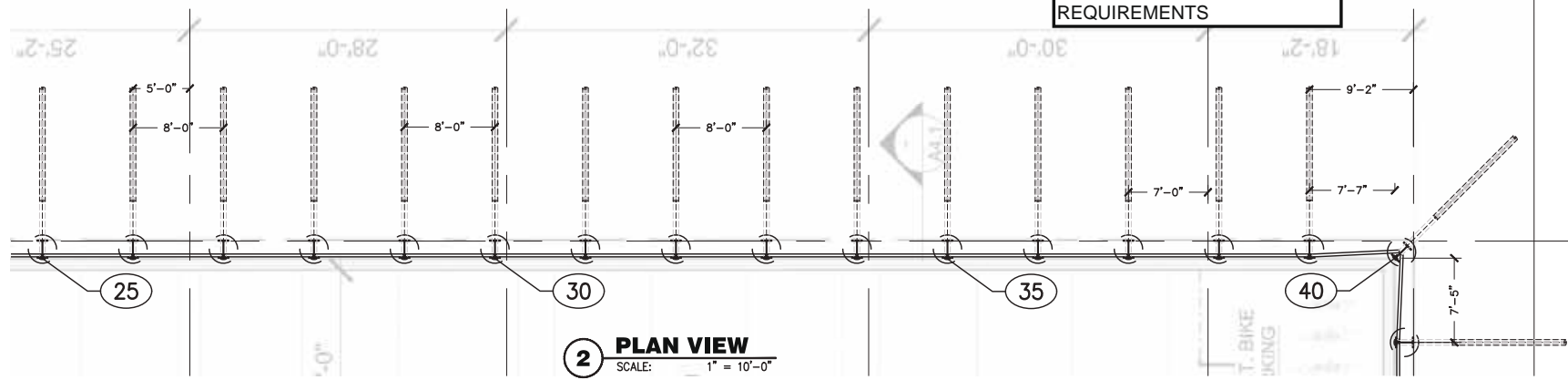




**1 ELEVATION VIEW**  
SCALE: 1" = 10'-0"

ALL SOLDIER PILES TO BE LOCATED OUTSIDE OF THE PUBLIC RIGHT OF WAY AND WITHIN PROPERTY LINES PER CITY OF MENLO PARK REQUIREMENTS

PRELIMINARY DESIGN NOT FOR CONSTRUCTION



**2 PLAN VIEW**  
SCALE: 1" = 10'-0"

170001  
06-24-2019

PREPARED FOR: [Redacted]

PROJECT NAME: [Redacted]

PROJECT NUMBER: [Redacted]

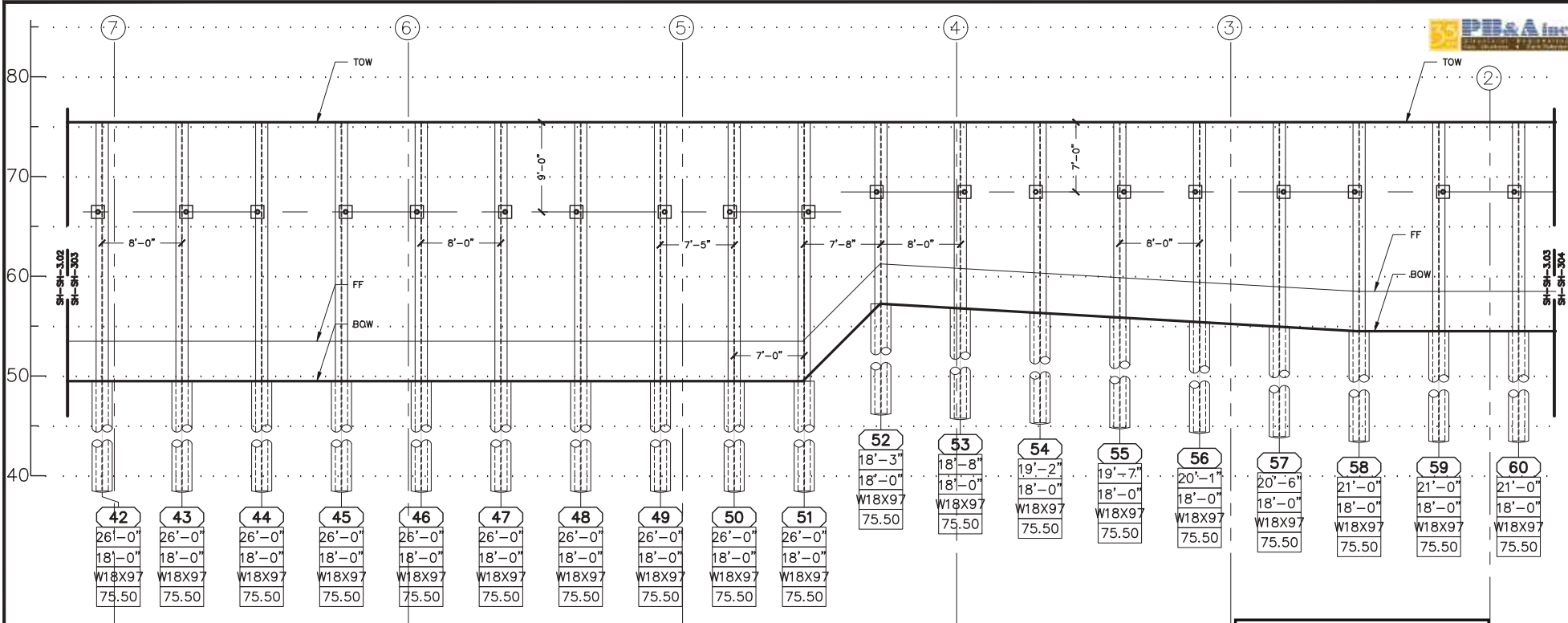
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706-716 SANTA CRUZ AVENUE

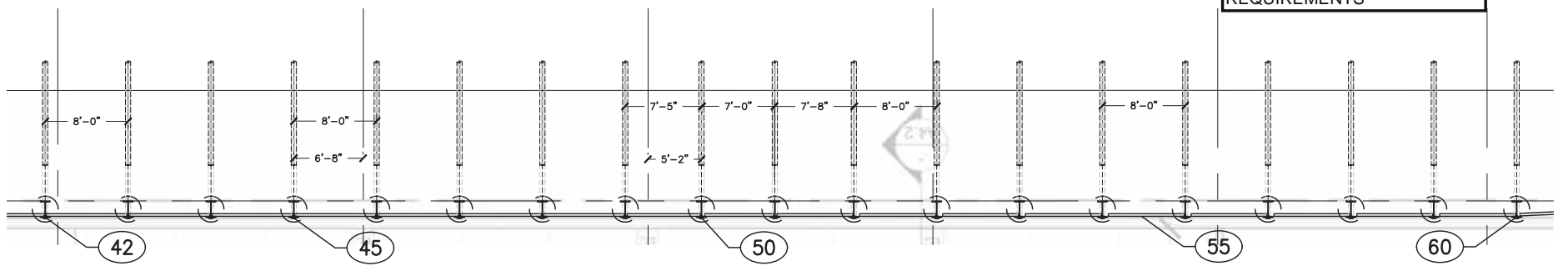
TEMPORARY SHORING PLAN  
ELEVATION

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PRINT DATE: 24 JUN 2019 - 4:54PM



ALL SOLDIER PILES TO BE LOCATED OUTSIDE OF THE PUBLIC RIGHT OF WAY AND WITHIN PROPERTY LINES PER CITY OF MENLO PARK REQUIREMENTS



PRELIMINARY DESIGN  
NOT FOR CONSTRUCTION

PREPARED FOR: PBA & A

PROJECT: TEMPORARY SHORING PLAN

ELEVATION

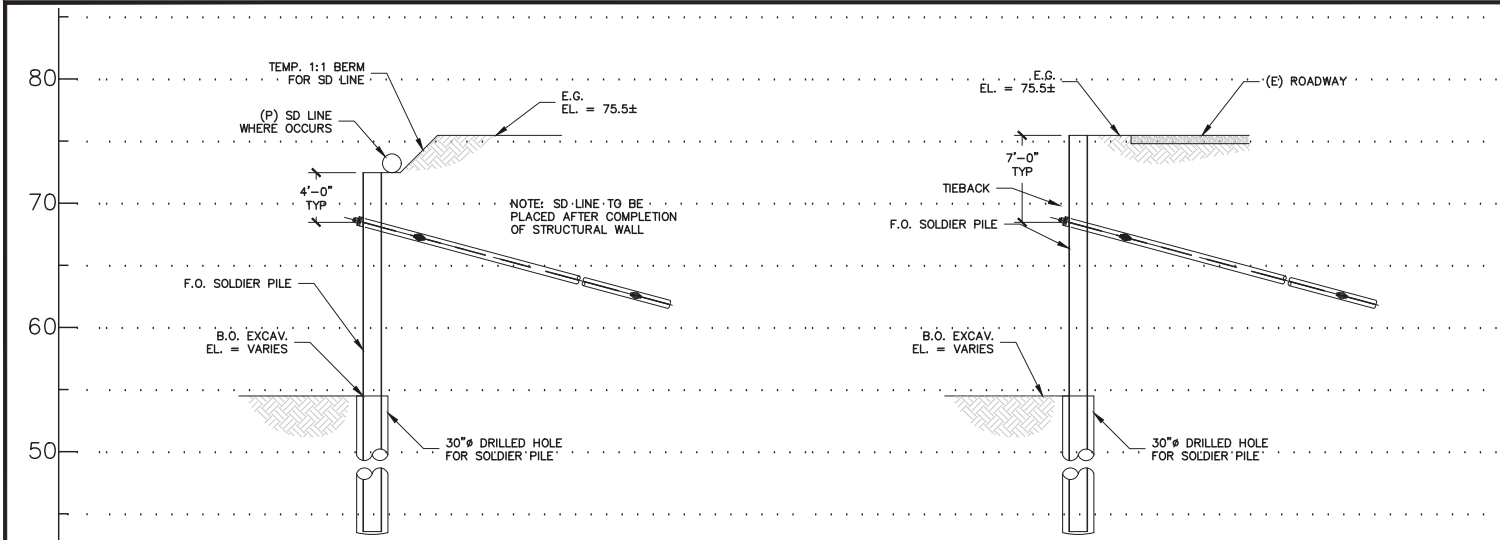
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170401 06-24-2019

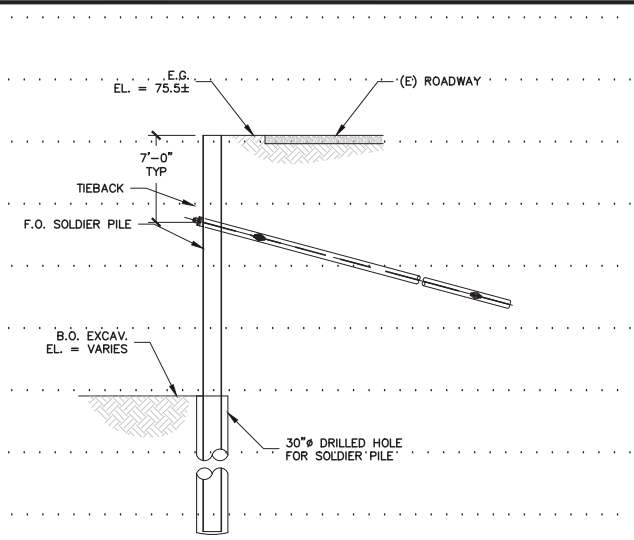
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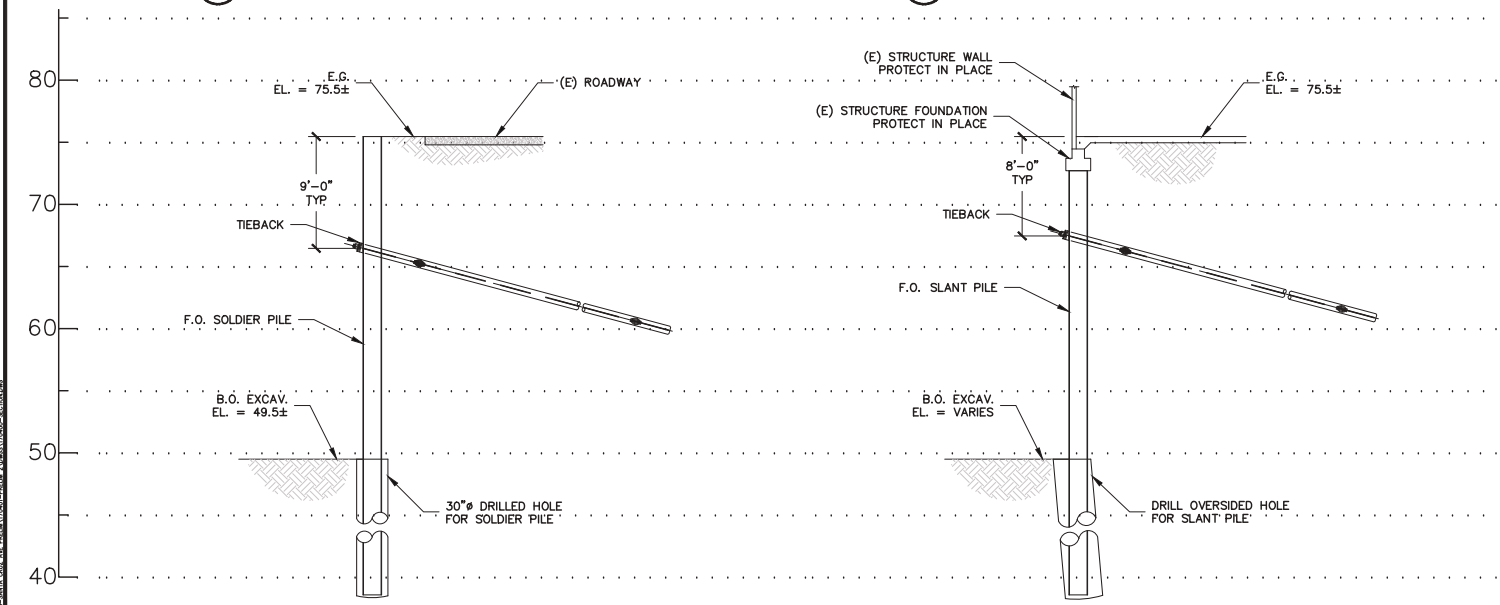




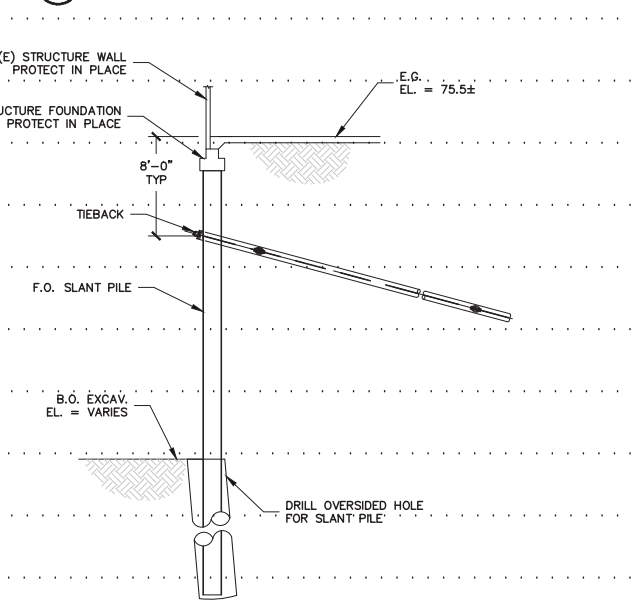
**1 TYPICAL SECTION-WITH SD**  
SCALE: 1" = 10'-0"



**2 TYPICAL SECTION-21'-0" MAX**  
SCALE: 1" = 10'-0"



**3 TYPICAL SECTION-26'-0" MAX**  
SCALE: 1" = 10'-0"



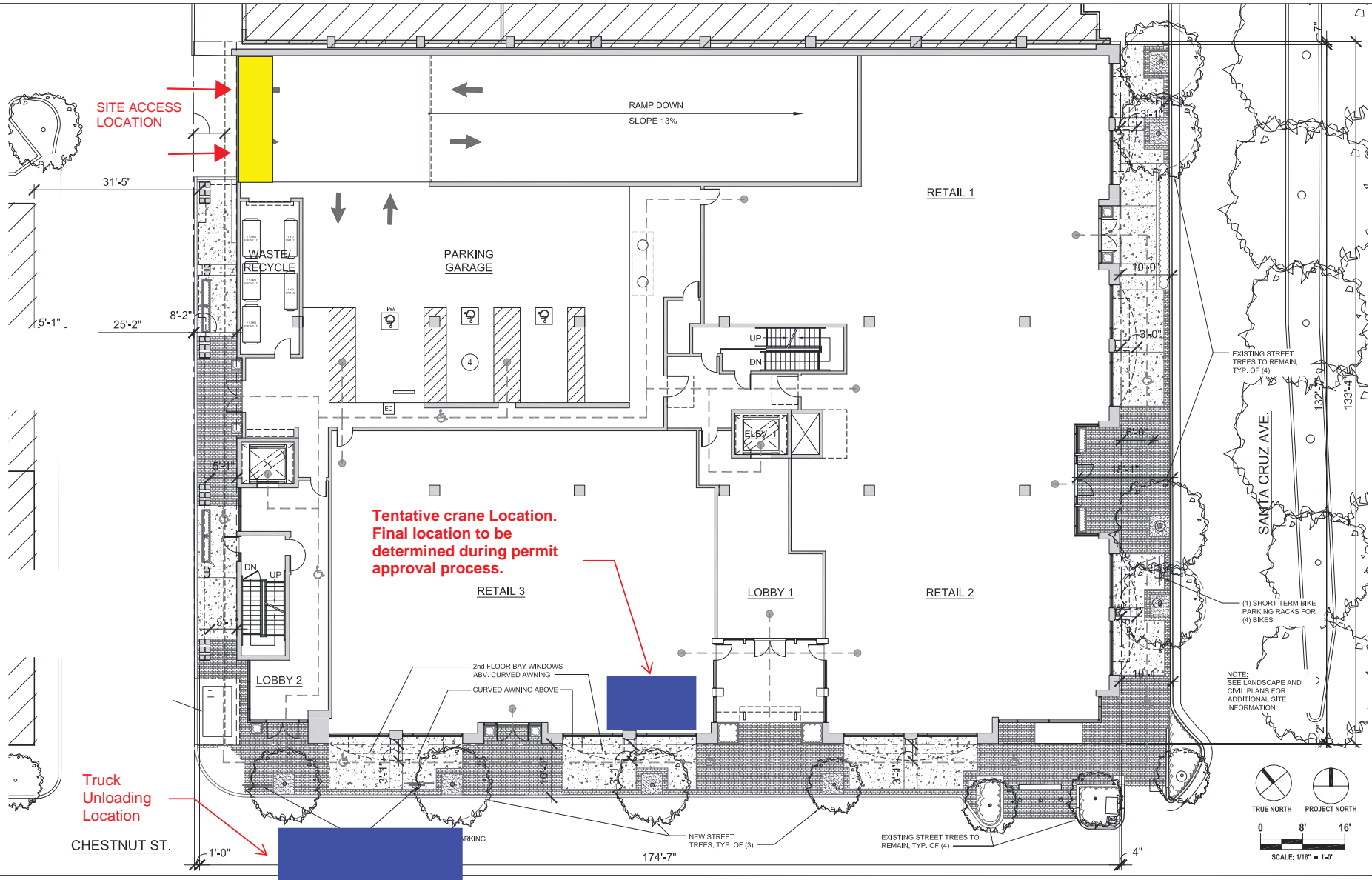
**4 TYPICAL SECTION-SLANT PILE**  
SCALE: 1" = 10'-0"

ALL SOLDIER PILES TO BE LOCATED OUTSIDE OF THE PUBLIC RIGHT OF WAY AND WITHIN PROPERTY LINES PER CITY OF MENLO PARK REQUIREMENTS

PRELIMINARY DESIGN  
NOT FOR CONSTRUCTION

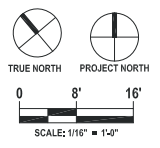
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 SECTION: [REDACTED]  
 PROJECT: [REDACTED]  
 SHEET: SM-400  
 DATE: 09-24-2019  
 706-716 SANTA CRUZ AVENUE  
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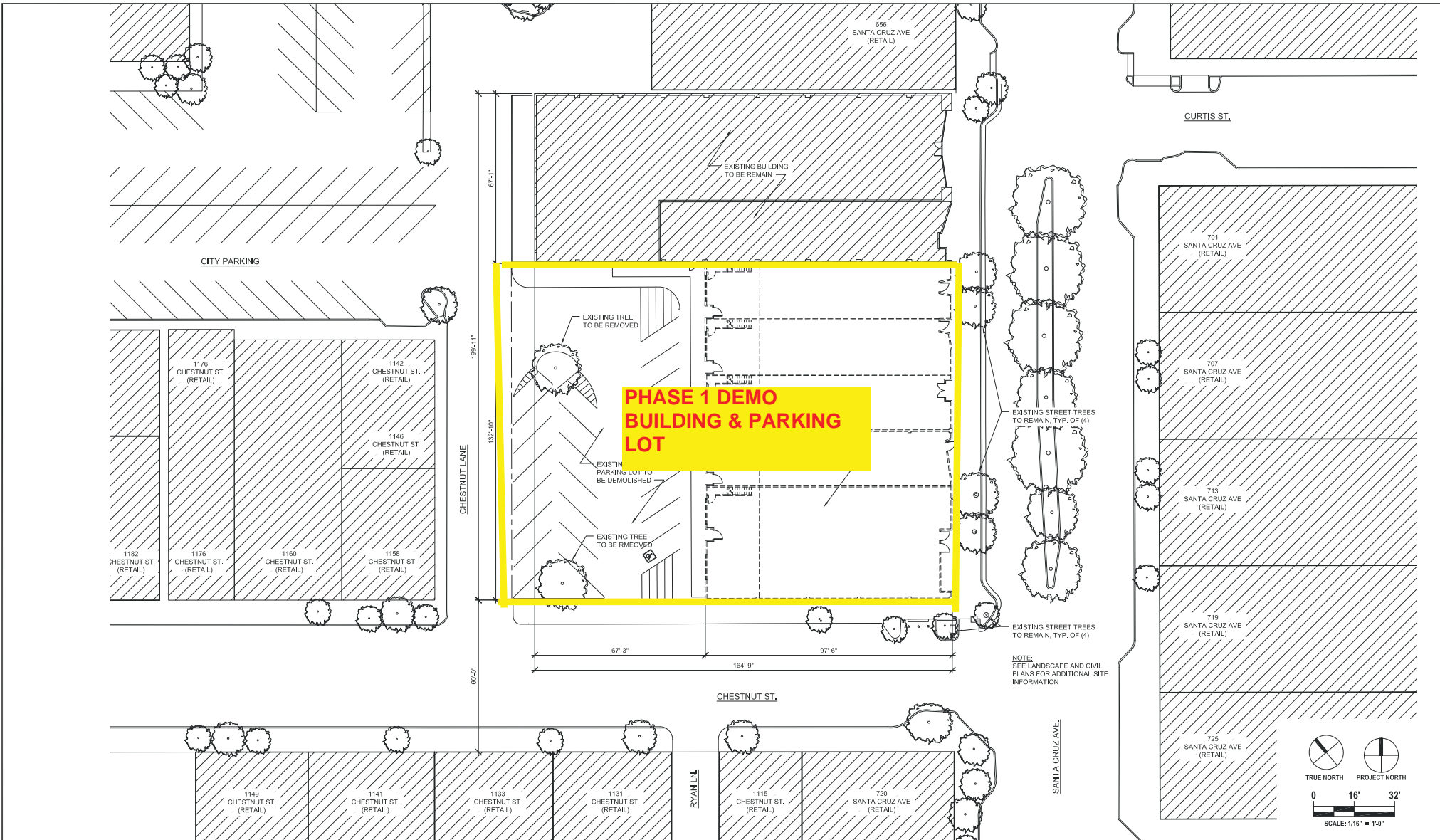
706 SANTA CRUZ AVE. MENLO PARK, CA

**\*ALL MATERIAL TO BE JUST IN TIME DELIVERY UNTIL UNDERGROUND BASEMENT IS CONSTRUCTED WITH WORKING FIRE SPRINKLERS.**



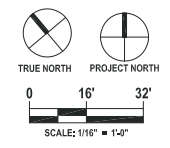
**CP-1**



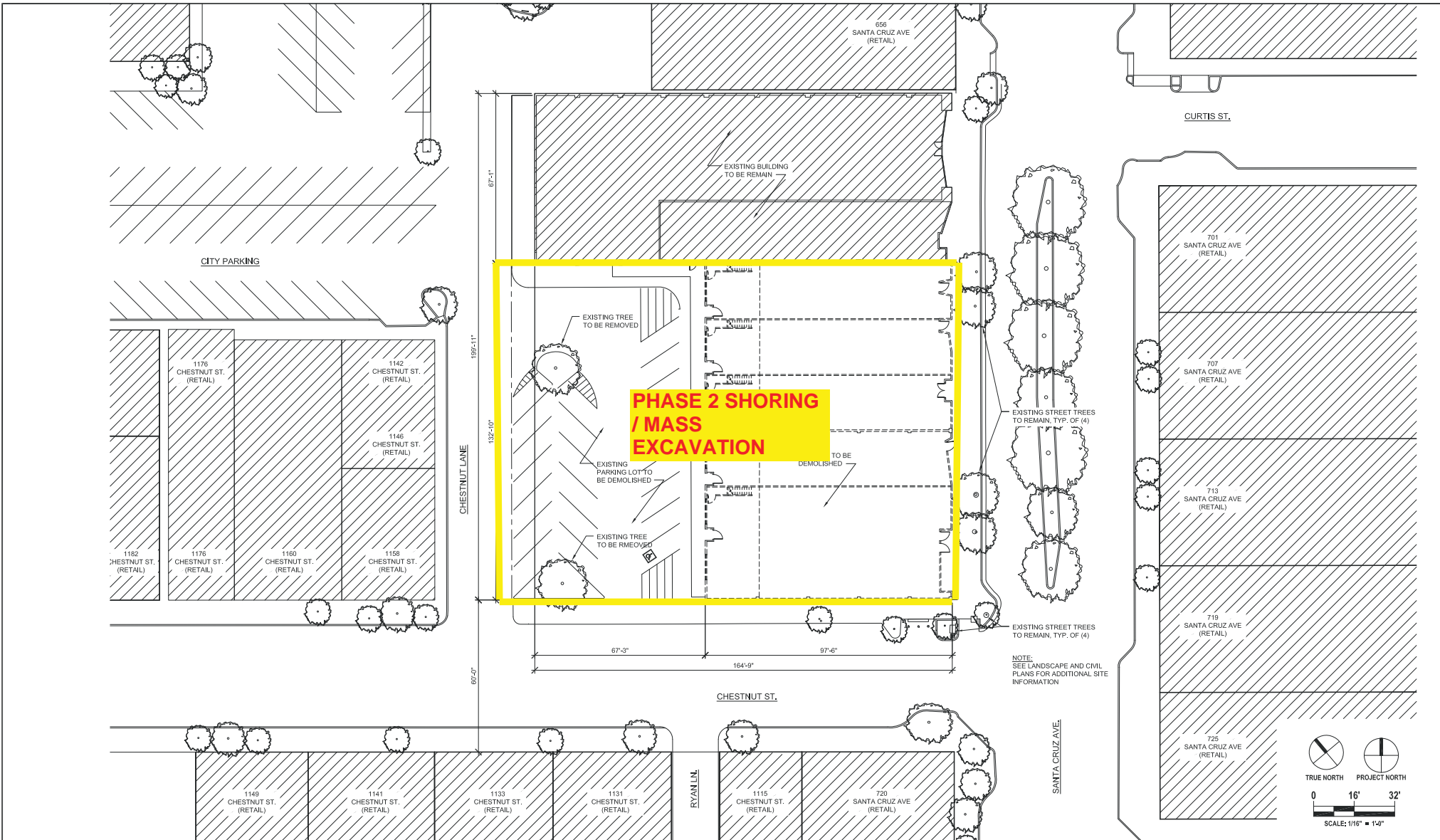


706 Santa Cruz Ave. Menlo Park, CA

**Phase 1 Demo Building & Parking Lot Area**



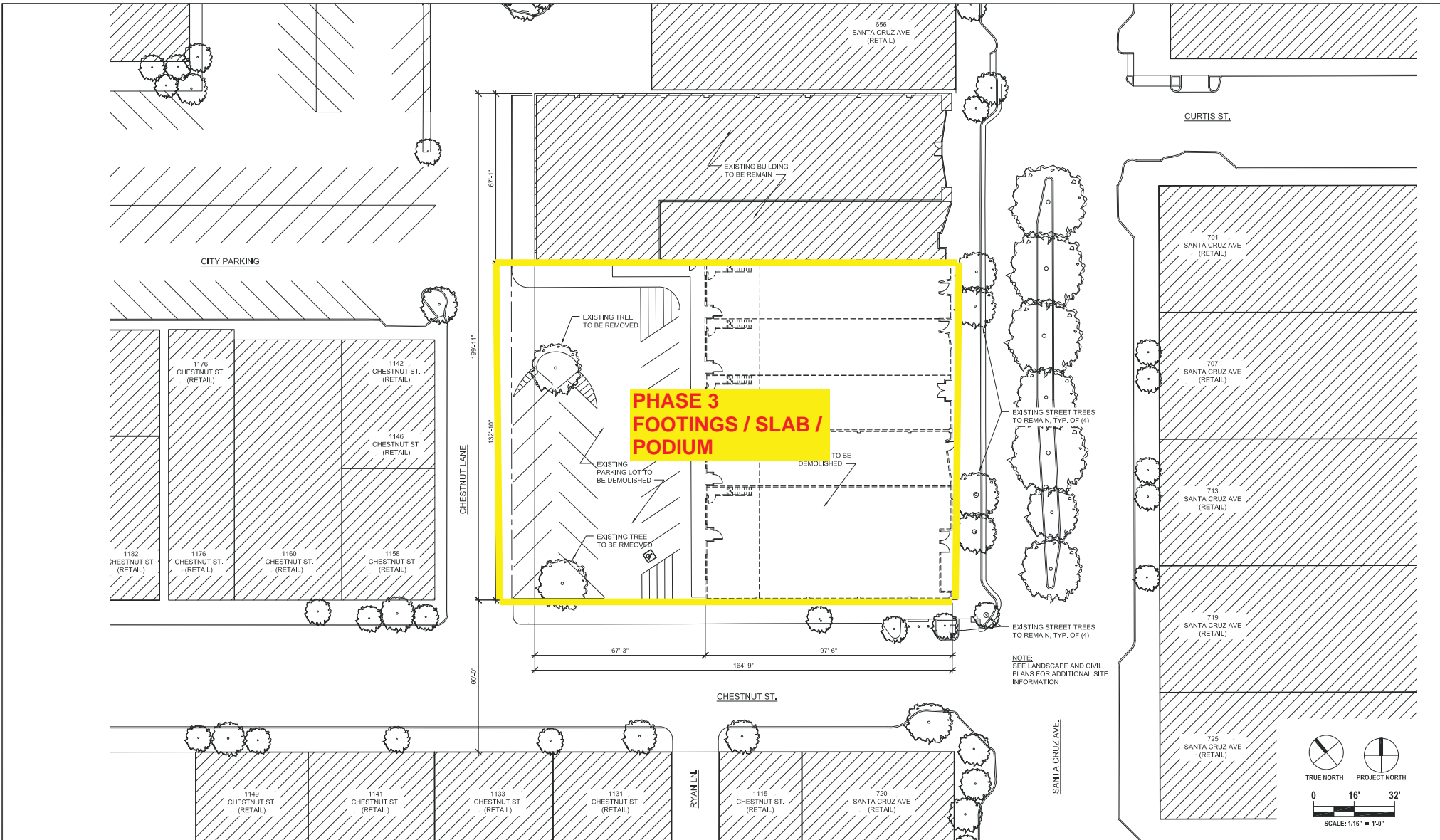
**CP-2**



706 Santa Cruz Ave. Menlo Park, CA

**PHASE 2 SHORING AND MASS EXCAVATION**

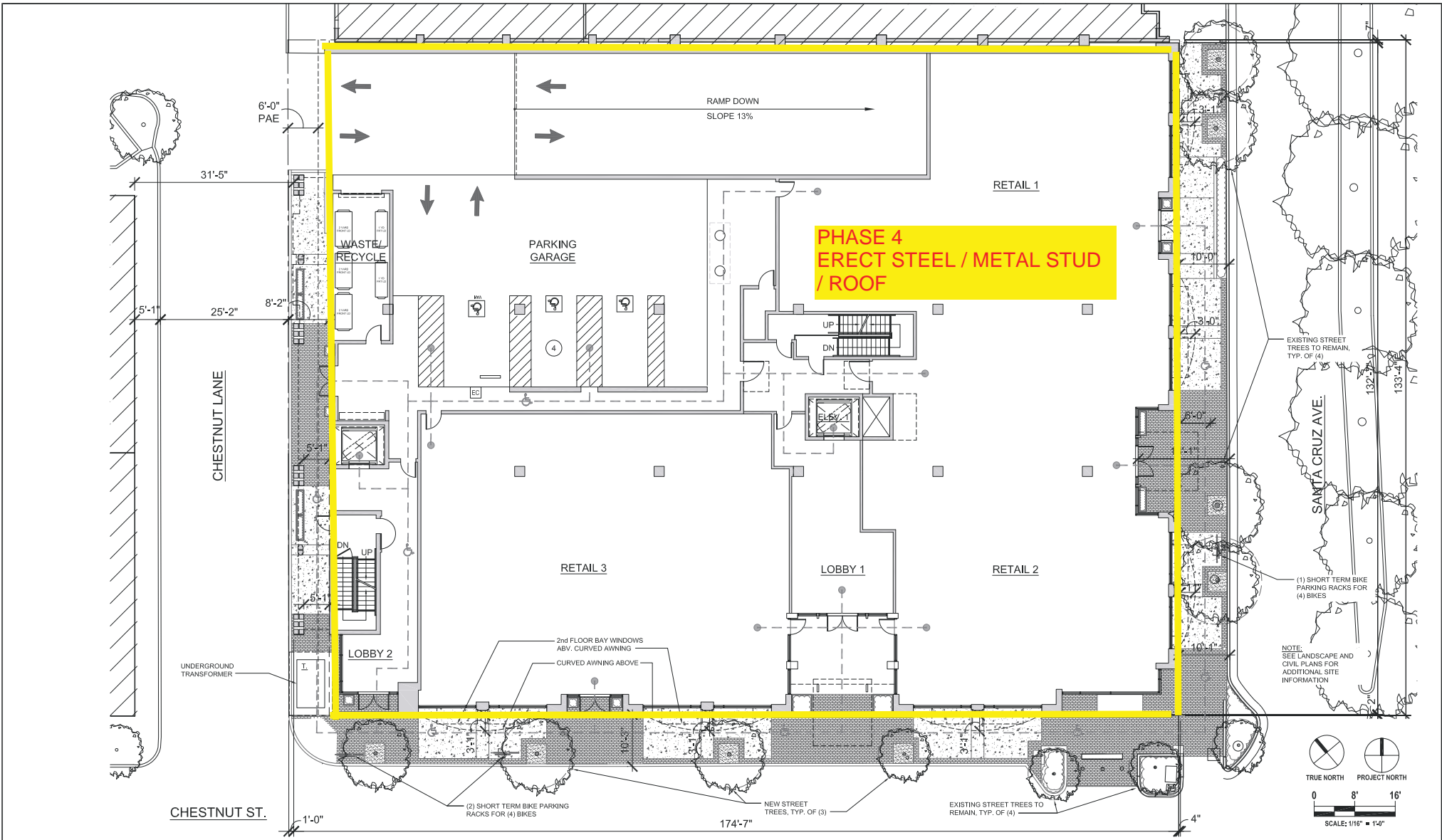
**CP-3**



706 Santa Cruz Ave. Menlo Park, CA

**PHASE 3 FOOTINGS / SLAB / PODIUM**

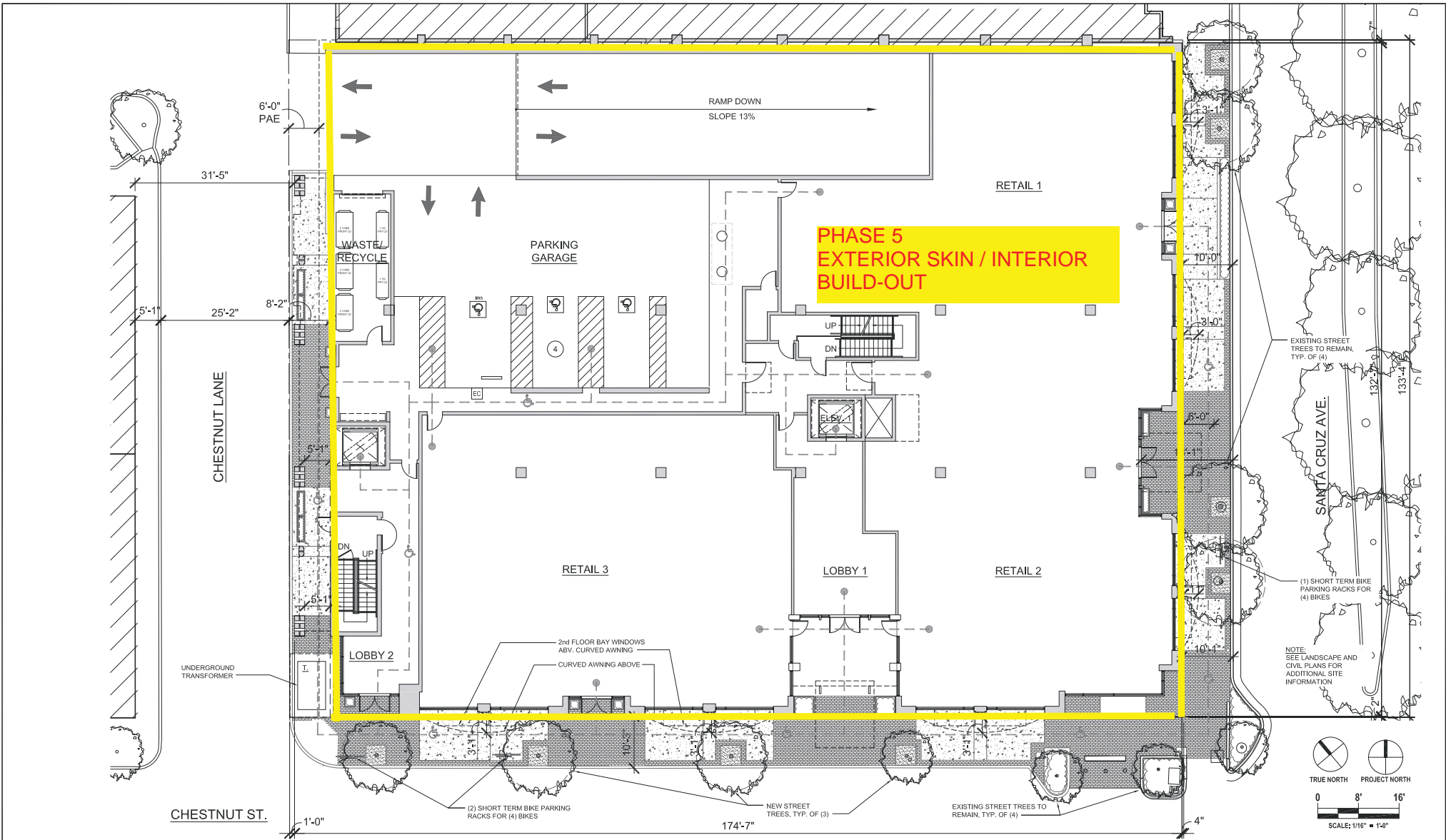
**CP-4**



706 SANTA CRUZ AVE. MENLO PARK, CA

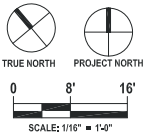
Phase 4 - Erect Steel / Metal Stud / Roof

CP-5



706 SANTA CRUZ AVE. MENLO PARK,  
CA

Phase 5 - Exterior Skin /  
Interior Build-Out



CP-6



**Preliminary Parking Management**

**Overall Notes**

The intent of this plan is to address the different phases of parking during the 706 Santa Cruz Ave. project located in Menlo Park, Ca. This is preliminary as details, means, methods and schedule durations are currently being defined as we begin the demolition and construction process.

**Phase 1 Demo**

The phase 1 demo phase of this project, all subcontractors and workers will be encouraged to park offsite at the Cal Trans station which is 0.3 miles from the jobsite. All residential areas will be off limits to our subcontractors. Also, all workers will be encouraged to carpool to the site. Ownership is also researching options / possibilities of renting nearby parking lot spaces.

**Pre-Construction and Construction Phase**

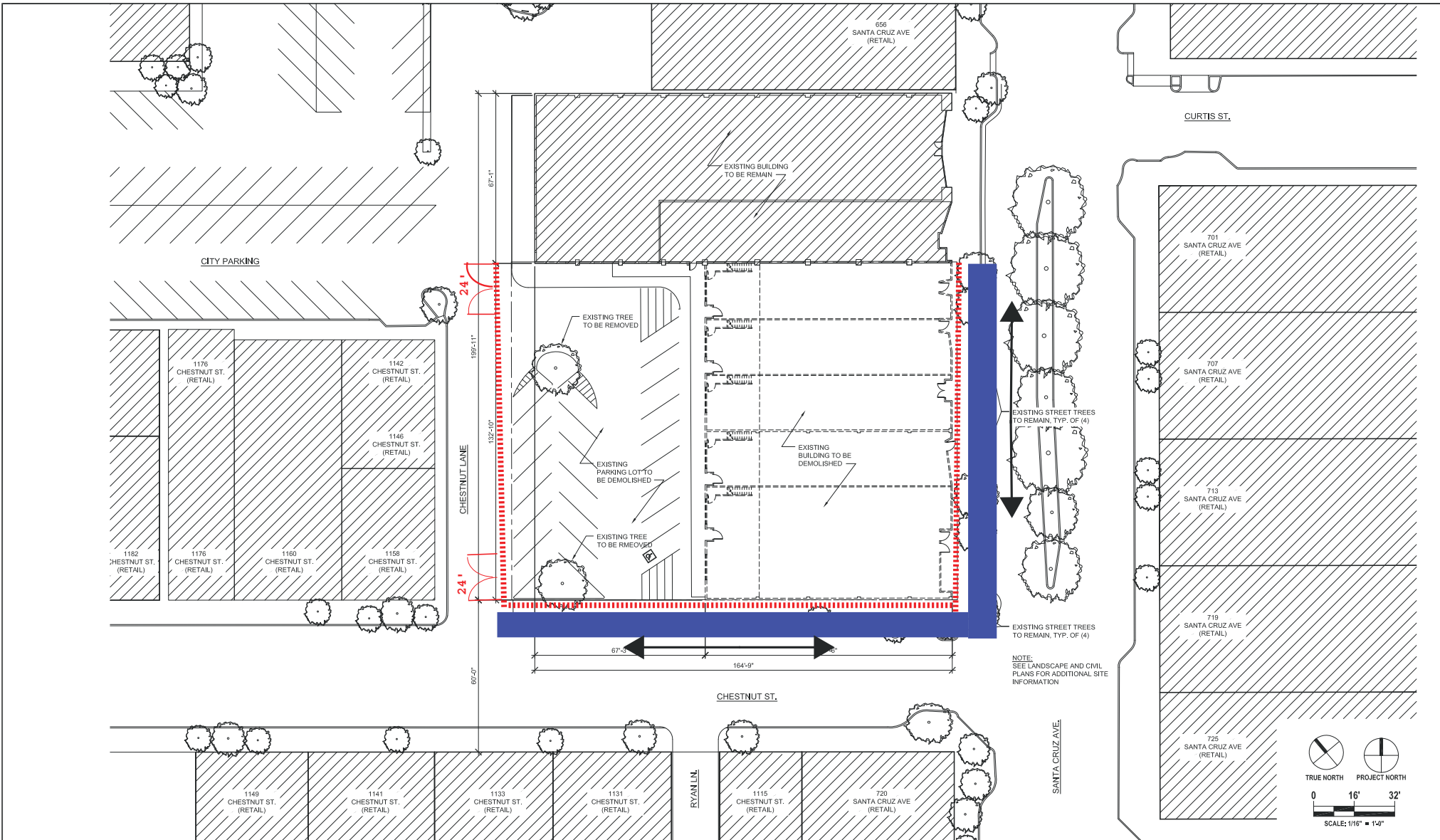
During the 16-month duration of this project SBC will encourage all workers to park offsite at the Cal Trans station which is 0.3 miles from the jobsite. Notices and maps for the Cal Trans location and fees will be posted in our jobsite trailer and will be conveyed in all SBC / subcontractor meetings. SBC will require the subcontractors to provide a monthly public transit/carpooling fee within their bids. Ownership is also researching options / possibilities of renting nearby parking lot spaces. All residential areas will be off limits. SBC will also encourage subcontractors that will be working on this project to carpool to the site.



Project: 706 Santa Cruz Ave. Menlo Park, CA

**All parking to be offsite in the Menlo Park Caltrans station parking lot located at 1120 Merrill St. Menlo Park, CA 94025**

○ Jobsite 706 Santa Cruz Ave to Caltrans Station 1120 Merrill St. Menlo Park, CA (0.4 miles)



706 Santa Cruz Ave. Menlo Park, CA

 Fencing

 Pedestrian/ Scaffold Tunnel

CP-8



**706 Santa Cruz Ave, Menlo Park**  
Project Narrative - Planning Commission Hearing

December 10, 2019

To: The City of Menlo Park Planning Division  
Ms. Kaitie Meador

From: 706 Santa Cruz Ave., LLC. and  
From4 Architecture, Robert Giannini, Architect

Subject: **706 Santa Cruz Avenue, Menlo Park**  
**Planning Commission**

Dear Kaitie:

It has been a pleasure working with you and the City as we developed a new architectural expression for the 706 Santa Cruz project that extends from the existing Ace Hardware to Chestnut Street on Santa Cruz. We began the new design by meeting with you and other key City staff and learned about the goals for this important site. Our application address the new design, and issues that had been identified in the earlier application. We also participated in a Study Session with the City on September 17, 2018 where we obtained initial comments on the design, all of which have been taken into account with this current submittal.

Following are the primary goals of the Downtown Specific Plan:

*Specific Plan Goals:*

- 1) *Encourages infill development of vacant and under-utilized lots along El Camino Real through increased intensities, coupled with strict building modulation and ground-floor setback and building profile requirements that both attenuate the mass and scale of larger buildings and create wider public sidewalks;*
- 2) *Retains the existing "village" character downtown by keeping buildings low and requiring varied building massing, including through building profile and façade modulation requirements;*
- 3) *Increases downtown activity, foot traffic and transit use through enhanced public spaces, mixed-use infill projects (including residential uses) and higher intensities of development near the commuter rail station;*
- 4) *Enhances community life through an integrated network of widened sidewalks, promenades, pocket parks and public gathering spaces; and*
- 5) *Enhances east-west connectivity across El Camino Real through crosswalk and sidewalk improvements, while accommodating north-south vehicular through-traffic, and across the railroad tracks through grade-separated pedestrian and bicycle connections.*

*Of the above goals, items 2 and 3 can be addressed by this project.*

*To quote the Specific Plan: "Buildings with minimal setbacks have a special relationship with the sidewalk and street. In these cases, buildings frame the street and form a well-defined street edge. Activities within the building, if seen, particularly at ground level, can provide visual interest and a degree of safety to passersby.*





*above: View from Santa Cruz down Chestnut Avenue - glass corner opens building to the street.*



*The building is scaled to fit in with the existing context. A strong horizontal cap at the top of the second floor makes the overall building mass appear as two story. Due to sight lines, the third floor is not readily perceived by the public.*

- a) In keeping with the above aspiration, this project strives to be a welcome neighbor, and a unique and appropriate example of the direction given in the Downtown Plan. Because people will experience it at street level it has an intimate, people level scale.

- b) We feel the building should be modern and “of its time,” but we are not advocates of stark modernism in this location. The architectural expression is what we call “warm modern.” It uses high quality, warm materials; buff color and gray stone & tile, dark brown mullions and clear glass to convey a feeling of transparency, quality, and friendliness.
- c) Wood soffits add warmth and fit in with the scale of the street. They make the buff colored limestone feel even warmer.
- d) The building is extroverted and becomes part of the street life. It wants to be a welcome participant - not trying to upstage its neighbors, but rather join in the vitality of the street.
  - To achieve that goal bay windows protrude to engage the street and add a artistic and interesting top to the storefronts below. This makes the retail presentation special. People in the second floor office become part of the action. Passersby can see activity within, and that adds life and safety to the street.
  - The third floor houses a “*residential & office village*” surrounded by deep roof decks edged with flowers. This adds an interesting third dimension where third floor residents have an overview of the street, and people on the street may enjoy activity happening on several levels.
  - High quality materials attract retailers and give the street character. The building defines the street and conveys an upscale, welcoming and safe feeling.

*From the Specific Plan: “Building massing and modulation consider both vertical and horizontal modulations. The modulation of buildings refers to change or variety across a building plane to provide distinction in the building as well as provide visual interest. Vertical modulation is the introduction of façade articulation that creates a rhythm or pattern across the façade of a building. Horizontal modulation provides visual clarity between ground floors, upper stories and roofs.”*

- e) The design works within the “major and minor” module guidelines of the specific plan as described above. The rhythm the guidelines advocate help organize the street. To add interest we especially focused on the infill of the module.
  - Each frame’s infill is conceived to be an artistic composition of clear glass, bay windows and sunshades with great attention to detail so that it creates intimate people scale.
  - Curves introduced in the composition’s sunshades break the rigidness that could be the outcome of strict modulation rules.
  - To add variety, the major modulations on Santa Cruz & Chestnut Ave. employ a different color material and scale as required by the Specific Plan. The resulting overlapping composition is a base of buff colored stone & glass wrapped around the *residential & office village*” above. The grey tile of the major modulations hint at and preview what’s happening on the third floor above.





## Chestnut Lane

*An important part of the redesign was to work closely with our neighbors on Chestnut Lane. Following are the concerns and our responses:*

- 1) *The previous design reduced Chestnut Lane by 5'.*
  - a) The project has been redesigned to keep the Lane at its current width and also enhance the pedestrian experience and make the Lane more special.
  - b) Building columns are located just behind the location of the existing curb. A covered sidewalk is created behind those columns, and much of it is buffered from the road by planters.
- 2) *Previously the garage entrance was 30 feet in width lined up with neighbor building's storefront.*
  - a) The garage entrance was moved to the far edge of our site to minimize any impact (please also see plan view next sheet). It is also set back from the street.
  - b) More importantly the geometry is now such that one drives straight down into the garage rather than having to negotiate an "S" turn once you enter. The result is that the garage opening can be City minimum width of 24'.
  - c) The simplified geometry also means that cars can more quickly enter the garage and proceed to the parking level minimizing queuing. The motion is simpler, more direct and quicker.
  - d) A roll down grill will mitigate the open void of the internal driveway beyond.



*Chestnut Lane Elevation*

*Garage entry shifted to the edge and is narrower than before.  
The movement is quicker because one drives straight in.*



### Managing Trash & Deliveries

All uses within the building allow for trash and deliveries to be staged in the concealed area behind retail.

- 1) Trash
  - a) All retail spaces open to the covered service area where the trash room is located. Trash is stored there and on trash day the cans will be rolled out of the building and loaded by the self loading trucks. This occurs every morning between 5 and 6 AM when the parking lot is empty.
  - b) Office and residential elevators also drop down into the service area where trash may be stored.
  - c) To accommodate capacity, multiple pick up days can be arranged and all can be at hours when public activity is minimal.
- 2) Deliveries
  - a) The same common service area offers a direct path for deliveries into all uses in the building.
  - b) To accommodate capacity, multiple delivery times can be arranged and all can be at hours when public activity is minimal.

### **Skylights & 3rd Floor Office**

This application includes a variance request to allow skylights on the third floor office. During the Study Session there was a concern from the Commission that ceiling heights could be low. That is true due to the requirements of the Downtown plan. It is most noticeable in the 3rd floor office which was needed in part due to the area lost to the arcade required by the City on Chestnut Lane. Adding skylights to that office will help mitigate the situation. They protrude above the roof the minimum distance needed for drainage. Due to parapets and sight lines they would not be perceived from the street. Part of this application is that variance request which itemizes the five findings required. We appreciate your consideration.

### **Signage**

It is our desire to address signage once tenants are identified and proper attention can be paid to this important feature of the building. Therefore it is not addressed in this application, and no sign would be allowed until it is reviewed and approved by the City in the future.

*Thanks very much for your attention and review of the various design aspects of this project!*

*Form4 Architecture*



*Robert Giannini  
Architect, President*

Menlo Park El Camino Real/Downtown Specific Plan  
Standards and Guidelines: Project Compliance Worksheet

706 Santa Cruz: Downtown (D) Zone – “Main Street” Overlay

Section	Standard or Guideline	Requirement	Evaluation
<b>E.3.1 Development Intensity</b>			
E.3.1.01	Standard	Business and Professional office (inclusive of medical and dental office) shall not exceed one half of the base FAR or public benefit bonus FAR, whichever is applicable.	<b>Complies:</b> Total proposed office is equal to 1.0 FAR or 23,454 sf. Allowable FAR for lot is 2.0. Refer to sheets CS0.1 & MP1.0 to MP1.3.
E.3.1.02	Standard	Medical and Dental office shall not exceed one third of the base FAR or public benefit bonus FAR, whichever is applicable.	<b>Complies:</b> No proposed medical or dental office space.
<b>E.3.2 Height</b>			
E.3.2.01	Standard	Roof-mounted mechanical equipment, solar panels, and similar equipment may exceed the maximum building height, but shall be screened from view from publicly-accessible spaces.	<b>Complies:</b> Equipment placed on roof in middle of roof area exceed maximum height by up to 7'-6" (see A0.1, A1.4, A4.1, A4.2). No visibility from publicly accessible spaces per sight line study on sheets MP-3.0 and MP-3.1.
E.3.2.02	Standard	Vertical building projections such as parapets and balcony railings may extend up to 4 feet beyond the maximum façade height or the maximum building height, and shall be integrated into the design of the building.	<b>Complies:</b> Building height 37.75' (38' allowed) plus 4' maximum increase for parapets. Façade height 27'-9" (30' allowed) plus 3'-6" for railings. Parapets and railings are integrated with design. See A2.1 to A2.7.  <b>Note:</b> Proposed skylights would exceed the height limit by approximately 3'-2" (see A4.1) and appear obscured from public view by parapets or roof edges. Since skylights increase interior volume, they have not been viewed as vertical building projections akin to parapets and railings subject to this standard. The applicant, however, is seeking a variance for the proposed skylights.
E.3.2.03	Standard	Rooftop elements that may need to exceed the maximum building height due to their function, such as stair and elevator towers, shall not exceed 14 feet beyond the maximum building height. Such rooftop elements shall be integrated into the design of the building.	<b>Complies:</b> Elevator overruns exceed height limit by approximately 6 feet 2 inches and are integrated with the massing and material usage of the building. Only the elevator overrun on the Chestnut Lane façade would be publicly visible. See A2.5.
<b>E.3.3 Setbacks and Projections within Setbacks</b>			
E.3.3.01	Standard	Front setback areas shall be developed with sidewalks, plazas, and/or landscaping as appropriate.	<b>Complies:</b> This lot has a 0' setback requirement on all sides, but there are functional setbacks and recesses at modulations and entries that have brick pavers and landscaped planters to integrate with the adjacent sidewalk area. Also, the Chestnut Lane side is treated as a covered walkway with landscape in planters. See L2.1.
E.3.3.02	Standard	Parking shall not be permitted in front setback areas.	<b>N/A:</b> No front setback permitted that would allow for parking.

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E.3.3.03	Standard	In areas where no or a minimal setback is required, limited setback for store or lobby entry recesses shall not exceed a maximum of 4-foot depth and a maximum of 6-foot width.	<b>Complies:</b> This lot has a 0' setback requirement on all sides. Store and lobby recesses have been provided at minor and major modulations, which would permit them to exceed this standard in order to meet those standards at key locations. No other locations have recesses for store or lobby entries. See A1.1, A2.2, A2.4 and A2.6.
E.3.3.04	Standard	In areas where no or a minimal setback is required, building projections, such as balconies, bay windows and dormer windows, shall not project beyond a maximum of 3 feet from the building face into the sidewalk clear walking zone, public right-of-way or public spaces, provided they have a minimum 8-foot vertical clearance above the sidewalk clear walking zone, public right-of-way or public space.	<b>Complies:</b> Second floor bay windows project 1'-8" out from the building face into the sidewalk clear walking zone and have more than 8-foot vertical clearance to the sidewalk. See A4.3, A2.2, A2.4 and A2.6.
E.3.3.05	Standard	In areas where setbacks are required, building projections, such as balconies, bay windows and dormer windows, at or above the second habitable floor shall not project beyond a maximum of 5 feet from the building face into the setback area.	<b>N/A:</b> No setbacks are required in this zoning district.
E.3.3.06	Standard	The total area of all building projections shall not exceed 35% of the primary building façade area. Primary building façade is the façade built at the property or setback line.	<b>Complies:</b> A3.3 (Projection Analysis) indicates a 33.7% projection area on the Santa Cruz side and a 32.5% projection area on the Chestnut Street side, both of which do not exceed the 35% limit.
E.3.3.07	Standard	Architectural projections like canopies, awnings and signage shall not project beyond a maximum of 6 feet horizontally from the building face at the property line or at the minimum setback line. There shall be a minimum of 8-foot vertical clearance above the sidewalk, public right-of-way or public space.	<b>Complies:</b> The maximum projection would be 6'-0" at the building entry canopies, which are at least 10'-4" above the sidewalk. Other canopies, eaves, signs, etc., do not project more than 3'-10" and are at least 8 feet above the sidewalk. See A4.3 and A4.4
E.3.3.08	Standard	No development activities may take place within the San Francisquito Creek bed, below the creek bank, or in the riparian corridor.	<b>N/A:</b> No development activities will take place within the San Francisquito Creek bed, below the creek bank, or in the riparian corridor.
<b>E.3.4 Massing and Modulation</b>			
<b>E.3.4.1 Building Breaks</b>			
E.3.4.1.01	Standard	The total of all building breaks shall not exceed 25 percent of the primary façade plane in a development.	<b>N/A:</b> Building breaks are not applicable in downtown zones.
E.3.4.1.02	Standard	Building breaks shall be located at ground level and extend the entire building height.	<b>N/A:</b> No building breaks are required.



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E.3.4.1.03	Standard	In all districts except the ECR-SE zoning district, recesses that function as building breaks shall have minimum dimensions of 20 feet in width and depth and a maximum dimension of 50 feet in width. For the ECR-SE zoning district, recesses that function as building breaks shall have a minimum dimension of 60 feet in width and 40 feet in depth.	<b>N/A:</b> No building breaks are required.
E.3.4.1.04	Standard	Building breaks shall be accompanied with a major change in fenestration pattern, material and color to have a distinct treatment for each volume.	<b>N/A:</b> No building breaks are required.
E.3.4.1.05	Standard	In all districts except the ECR-SE zoning district, building breaks shall be required as shown in Table E3.	<b>N/A:</b> No building breaks are required.
E.3.4.1.06	Standard	In the ECR-SE zoning district, and consistent with Table E4 the building breaks shall: <ul style="list-style-type: none"> <li>• Comply with Figure E9;</li> <li>• Be a minimum of 60 feet in width, except where noted on Figure E9;</li> <li>• Be a minimum of 120 feet in width at Middle Avenue;</li> <li>• Align with intersecting streets, except for the area between Roble Avenue and Middle Avenue;</li> <li>• Be provided at least every 350 feet in the area between Roble Avenue and Middle Avenue; where properties under different ownership coincide with this measurement, the standard side setbacks (10 to 25 feet) shall be applied, resulting in an effective break of between 20 to 50 feet.</li> <li>• Extend through the entire building height and depth at Live Oak Avenue, Roble Avenue, Middle Avenue, Partridge Avenue and Harvard Avenue; and</li> <li>• Include two publicly-accessible building breaks at Middle Avenue and Roble Avenue.</li> </ul>	<b>N/A:</b> No building breaks are required.
E.3.4.1.07	Standard	In the ECR-SE zoning district, the Middle Avenue break shall include vehicular access; publicly-accessible open space with seating, landscaping and shade; retail and restaurant uses activating the open space; and a pedestrian/bicycle connection to Alma Street and Burgess Park. The Roble Avenue break shall include publicly-accessible open space with seating, landscaping and shade.	<b>N/A:</b> No building breaks are required.
E.3.4.1.08	Guideline	In the ECR-SE zoning district, the breaks at Live Oak, Roble, Middle, Partridge and Harvard Avenues may provide vehicular access.	<b>N/A:</b> No building breaks are required.

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<b>E.3.4.2 Façade Modulation and Treatment</b>			
<b>E.3.4.2.01</b>	Standard	Building façades facing public rights-of-way or public open spaces shall not exceed 50 feet in length without a minor building façade modulation. At a minimum of every 50' façade length, the <b>minor vertical façade modulation</b> shall be a minimum 2 feet deep by 5 feet wide recess or a minimum 2-foot setback of the building plane from the primary building façade.	<b>Complies:</b> Each street side has at least one minor modulation in addition to the major modulations and meets this standard. See A2.2, A2.4, and A2.6 for minor and major modulations.
<b>E.3.4.2.02</b>	Standard	Building façades facing public rights-of-way or public open spaces shall not exceed 100 feet in length without a major building modulation. At a minimum of every 100 feet of façade length, a <b>major vertical façade modulation</b> shall be a minimum of 6 feet deep by 20 feet wide recess or a minimum of 6 feet setback of building plane from primary building façade for the full height of the building. This standard applies to all districts except ECR NE-L and ECR SW since those two districts are required to provide a building break at every 100 feet.	<b>Complies:</b> Each street side has one major modulation and meets this standard. See A2.2, A2.4, and A2.6 for minor and major modulations.
<b>E.3.4.2.03</b>	Standard	In addition, the major building façade modulation shall be accompanied with a 4-foot minimum height modulation and a major change in fenestration pattern, material and/or color.	<b>Complies:</b> The roof form is varied from an eave at roof surface height to a taller parapet at each major modulation to meet this standard. The color of the stone cladding also changes from the light beige to the warm grey at the major modulation. See A2.1 to A2.6.
<b>E.3.4.2.04</b>	Guideline	Minor façade modulation may be accompanied with a change in fenestration pattern, and/or material, and/or color, and/or height.	<b>Complies:</b> The roof form is varied from an eave at roof surface height to a slightly taller parapet at some minor modulations. Balconies at the modulation and minor fenestration changes or changes to the color of the stone cladding from the light beige to the warm grey are also used. Overall, the modulation distinction is subtle but still present. See A2.1 to A2.6.
<b>E.3.4.2.05</b>	Guideline	Buildings should consider sun shading mechanisms, like overhangs, <i>bris soleils</i> and clerestory lighting, as façade articulation strategies.	<b>Complies:</b> Deep eaves and horizontal sunshades are used at the upper floor and modulations. There are also curved overhangs at the bay windows to shade those windows and provide façade articulation as well as horizontal canopies at the ground floor, such as at the building corner that provide shade and articulation. See A2.1 to A2.6.
<b>E.3.4.3 Building Profile</b>			
<b>E.3.4.3.01</b>	Standard	The 45-degree building profile shall be set at the minimum setback line to allow for flexibility and variation in building façade height within a district.	<b>Complies:</b> Sections on A4.1 and A4.2 show building profiles with allowed horizontal and vertical projections out from or above the 45-degree building profile line.

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<b>E.3.4.3.02</b>	Standard	Horizontal building and architectural projections, like balconies, bay windows, dormer windows, canopies, awnings, and signage, beyond the 45-degree building profile shall comply with the standards for Building Setbacks & Projection within Setbacks (E.3.3.04 to E.3.3.07) and shall be integrated into the design of the building.	<b>Complies:</b> See note E.3.5.3.01 above. Projections such as overhangs and the elevator overrun on the Chestnut Lane side are well integrated in to the building massing.  <b>Note:</b> The Specific Plan standards do not clarify the distance horizontal projections for eave and rake overhangs can extend into the profile, but staff has typically accepted normal overhang dimensions for the architecture as being permitted by the standards to project over the line of the building profile.
<b>E.3.4.3.03</b>	Standard	Vertical building projections like parapets and balcony railings shall not extend 4 feet beyond the 45-degree building profile and shall be integrated into the design of the building.	<b>Complies:</b> Only one short parapet extends into the profile about 1 foot.
<b>E.3.4.3.04</b>	Standard	Rooftop elements that may need to extend beyond the 45-degree building profile due to their function, such as stair and elevator towers, shall be integrated into the design of the building.	<b>Complies:</b> The elevator overrun on the Chestnut Lane side extends about 6 feet above the building profile as seen on A4.1. As seen in the rendering A2.6 it is well integrated with the massing and provides height variation at the major modulation.
<b>E.3.4.4 Upper Story Façade Length</b>			
<b>E.3.4.4.01</b>	Standard	Building stories above the 38-foot façade height shall have a maximum allowable façade length of 175 feet along a public right-of-way or public open space.	<b>N/A:</b> No building stories above 38 feet.
<b>E.3.5 Ground Floor Treatment, Entry and Commercial Frontage</b>			
<b>Ground Floor Treatment</b>			
<b>E.3.5.01</b>	Standard	The retail or commercial ground floor shall be a minimum 15-foot floor-to-floor height to allow natural light into the space.	<b>Complies:</b> 15'-0" as shown on section drawings.
<b>E.3.5.02</b>	Standard	Ground floor commercial buildings shall have a minimum of 50% transparency (i.e., clear-glass windows) for retail uses, office uses and lobbies to enhance the visual experience from the sidewalk and street. Heavily tinted or mirrored glass shall not be permitted.	<b>Complies:</b> See Transparency Analysis on A3.2.  <b>Glazing note:</b> The materials board's glazing sample is noted as "High Performance Clear Glass".
<b>E.3.5.03</b>	Guideline	Buildings should orient ground-floor retail uses, entries and direct-access residential units to the street.	<b>Complies:</b> The site plan, A0.3, shows continuous retail frontage along Santa Cruz Avenue. Along Chestnut Street is retail frontage except at the building entries.
<b>E.3.5.04</b>	Guideline	Buildings should activate the street by providing visually interesting and active uses, such as retail and personal service uses, in ground floors that face the street. If office and residential uses are provided, they should be enhanced with landscaping and interesting building design and materials.	<b>Complies:</b> Retail storefronts face the street on both Chestnut Street and Santa Cruz Avenue.

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<b>E.3.5.05</b>	Guideline	For buildings where ground floor retail, commercial or residential uses are not desired or viable, other project-related uses, such as a community room, fitness center, daycare facility or sales center, should be located at the ground floor to activate the street.	<b>N/A:</b> Ground floor retail and commercial uses are proposed.
<b>E.3.5.06</b>	Guideline	Blank walls at ground floor are discouraged and should be minimized. When unavoidable, continuous lengths of blank wall at the street should use other appropriate measures such as landscaping or artistic intervention, such as murals.	<b>Complies:</b> Blank ground floor walls are minimized due to retail frontage and entries at Santa Cruz Avenue and Chestnut Street. Along Chestnut Lane a colonnade is used along with planters and material/color variation where glazing is limited. See A2.1, A2.3, and A2.5.
<b>E.3.5.07</b>	Guideline	Residential units located at ground level should have their floors elevated a minimum of 2 feet to a maximum of 4 feet above the finished grade sidewalk for better transition and privacy, provided that accessibility codes are met.	<b>N/A:</b> No residential units proposed on the ground level.
<b>E.3.5.08</b>	Guideline	Architectural projections like canopies and awnings should be integrated with the ground floor and overall building design to break up building mass, to add visual interest to the building and provide shelter and shade.	<b>Complies:</b> Canopies are used at the major entries that extend over the sidewalk. Smaller canopies and flat awning projections are used at the ground floor, including curved sunshades set below the bay windows. See A2.1 and A2.3.
<b>Building Entries</b>			
<b>E.3.5.09</b>	Standard	Building entries shall be oriented to a public street or other public space. For larger residential buildings with shared entries, the main entry shall be through prominent entry lobbies or central courtyards facing the street. From the street, these entries and courtyards provide additional visual interest, orientation and a sense of invitation.	<b>Complies:</b> The main building entry/lobby for the office spaces and side entries to the retail space is at the major modulation on the Chestnut Street side. The residential entry is more subdued in visual articulation but is on the Chestnut Street side at the building corner at Chestnut Lane. See A1.1, A2.3.
<b>E.3.5.10</b>	Guideline	Entries should be prominent and visually distinctive from the rest of the façade with creative use of scale, materials, glazing, projecting or recessed forms, architectural details, color, and/or awnings.	<b>Complies:</b> The main building entry/lobby for the office spaces and side entries to the retail space is at the major modulation on the Chestnut Street side. The large entry canopy marks the entry, and there is variation in material/color and height. The main retail entry on the Santa Cruz Avenue side has a similar design treatment. The residential entry is more subdued in visual articulation but is on the Chestnut Street side at the building corner at Chestnut Lane. See A1.1, A2.1, and A2.3.
<b>E.3.5.11</b>	Guideline	Multiple entries at street level are encouraged where appropriate.	<b>Complies:</b> Multiple entries are provided.

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E.3.5.12	Guideline	Ground floor residential units are encouraged to have their entrance from the street.	<b>N/A:</b> No ground floor residential units are proposed.
E.3.5.13	Guideline	Stoops and entry steps from the street are encouraged for individual unit entries when compliant with applicable accessibility codes. Stoops associated with landscaping create inviting, usable and visually attractive transitions from private spaces to the street.	<b>N/A:</b> No ground floor residential units are proposed.
E.3.5.14	Guideline	Building entries are allowed to be recessed from the primary building façade.	<b>Complies:</b> Major entries are recessed from 9 to 19 feet. Minor retail entries and the residential lobby entry are recessed 3.5 feet. See A2.2, A2.4.
<b>Commercial Frontage</b>			
E.3.5.15	Standard	Commercial windows/storefronts shall be recessed from the primary building façade a minimum of 6 inches	<b>Tentatively Complies:</b> Dimensions are not provided but the first-floor plan shows deep recesses up to about 1.5 feet at thicker columns/wall sections and about 6 inches at wall sections between windows. Wall section diagrams suggest similar dimensions. See A1.1, A4.3.
E.3.5.16	Standard	Retail frontage, whether ground floor or upper floor, shall have a minimum 50% of the façade area transparent with clear vision glass, not heavily tinted or highly mirrored glass.	<b>Complies:</b> See E.3.5.02.
E.3.5.17	Guideline	Storefront design should be consistent with the building’s overall design and contribute to establishing a well-defined ground floor for the façade along streets.	<b>Complies:</b> Storefronts are large glazed panels with some vertical mullion pattern used at the building corners. Glazing at the first and second floor are similar and well-integrated. See A2.1 and A2.3.
E.3.5.18	Guideline	The distinction between individual storefronts, entire building façades and adjacent properties should be maintained.	<b>Complies:</b> Storefronts have glazing, are recessed and have awnings. There is a lot of glass facing the street to allow views into the retail spaces. The storefronts/façade is distinct from the adjacent structure. See A2.1.
E.3.5.19	Guideline	Storefront elements such as windows, entrances and signage should provide clarity and lend interest to the façade.	<b>Complies:</b> The bay windows above the storefronts provide the most interest to the façade. Entries have features such as canopies to add interest. See A2.1, A2.3.
E.3.5.20	Guideline	Individual storefronts should have clearly defined bays. These bays should be no greater than 20 feet in length. Architectural elements, such as piers, recesses and projections help articulate bays.	<b>Complies:</b> Storefronts are recessed in larger bays with stone frames/piers defining the modulation. At the window line the glass is subdivided into two sections with a division between to maintain the square proportions. At the building corner a varied fenestration treatment is used with vertical proportions to the glazed units. See A2.1 to A2.4
E.3.5.21	Guideline	All individual retail uses should have direct access from the public sidewalk. For larger retail tenants, entries should occur at lengths at a maximum at every 50 feet, consistent with the typical lot size in downtown.	<b>Complies:</b> Retail entries have direct access on both Santa Cruz Avenue and Chestnut Street sides. See A2.2 and A2.4.



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E.3.5.22	Guideline	Recessed doorways for retail uses should be a minimum of two feet in depth. Recessed doorways provide cover or shade, help identify the location of store entrances, provide a clear area for out-swinging doors and offer the opportunity for interesting paving patterns, signage and displays.	<b>Complies:</b> 3.5' minimum depth of recess at retail doorways.
E.3.5.23	Guideline	Storefronts should remain un-shuttered at night and provide clear views of interior spaces lit from within. If storefronts must be shuttered for security reasons, the shutters should be located on the inside of the store windows and allow for maximum visibility of the interior.	<b>Complies:</b> No shutters are shown, nor suggested by the design.
E.3.5.24	Guideline	Storefronts should not be completely obscured with display cases that prevent customers and pedestrians from seeing inside.	<b>Tentatively Complies:</b> Drawings do not suggest that display cases would be placed at storefronts, but there is no note on the drawing to this effect. See A1.1
E.3.5.25	Guideline	Signage should not be attached to storefront windows.	<b>Tentatively Complies:</b> No signage information provided. Note: signage should not be shown on glazing. Signage will be a separate permitting process.
<b>E.3.6 Open Space</b>			
E.3.6.01	Standard	Residential developments or Mixed Use developments with residential use shall have a minimum of 100 square feet of open space per unit created as common open space or a minimum of 80 square feet of open space per unit created as private open space, where private open space shall have a minimum dimension of 6 feet by 6 feet. In case of a mix of private and common open space, such common open space shall be provided at a ratio equal to 1.25 square feet for each one square foot of private open space that is not provided.	<b>Complies:</b> Units have private balconies that meet the 80 square feet and the 6-foot minimum dimension per sheet A3.1.  Note: Open space dimensions of 6 feet and area of 80 SF are calculated at the interior, walking surface of the deck/patio.
E.3.6.02	Standard	Residential open space (whether in common or private areas) and accessible open space above parking podiums up to 16 feet high shall count towards the minimum open space requirement for the development.	<b>N/A:</b> Minimum open space not required in this zone. Only residential open space.
E.3.6.03	Guideline	Private and/or common open spaces are encouraged in all developments as part of building modulation and articulation to enhance building façade.	<b>Complies:</b> A deep entry space is provided at the major modulation on the Chestnut Street side and office and residential space at the second and third floor have terraces that articulate the building façade. See renderings A2.1, A2.3, and A2.5 and floor plans.
E.3.6.04	Guideline	Private development should provide accessible and usable common open space for building occupants and/or the general public.	<b>Complies:</b> There are large terrace areas for office users and residents. Retail workers and the public are not provided with a common usable space other than along the sidewalk.

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<b>E.3.6.05</b>	Guideline	For residential developments, private open space should be designed as an extension of the indoor living area, providing an area that is usable and has some degree of privacy.	<b>Complies:</b> On third floor large balconies extend off living and bedroom areas. See A1.3.
<b>E.3.6.06</b>	Guideline	Landscaping in setback areas should define and enhance pedestrian and open space areas. It should provide visual interest to streets and sidewalks, particularly where building façades are long.	<b>Complies:</b> Landscape in setback areas/wall recesses (planters, potted plants, etc.) provide visual interest. See L2.1.
<b>E.3.6.07</b>	Guideline	Landscaping of private open spaces should be attractive, durable and drought-resistant.	<b>Complies:</b> See L2.2 and L2.3 for landscape on upper terraces.
<b>E.3.7 Parking, Service and Utilities</b>			
<b>General Parking and Service Access</b>			
<b>E.3.7.01</b>	Guideline	The location, number and width of parking and service entrances should be limited to minimize breaks in building design, sidewalk curb cuts and potential conflicts with streetscape elements.	<b>Complies:</b> One parking/service entrance on Chestnut Lane at point least in conflict with adjacent buildings and streetscape elements. See site and area plans.
<b>E.3.7.02</b>	Guideline	In order to minimize curb cuts, shared entrances for both retail and residential use are encouraged. In shared entrance conditions, secure access for residential parking should be provided.	<b>Complies:</b> Single entry point off Chestnut Lane.
<b>E.3.7.03</b>	Guideline	When feasible, service access and loading docks should be located on secondary streets or alleys and to the rear of the building.	<b>Complies:</b> Single service entry point off Chestnut Lane. Loading activities will be required to occur in the parking plaza or Chestnut Street.
<b>E.3.7.04</b>	Guideline	The size and pattern of loading dock entrances and doors should be integrated with the overall building design.	<b>N/A:</b> No loading docks are proposed.
<b>E.3.7.05</b>	Guideline	Loading docks should be screened from public ways and adjacent properties to the greatest extent possible. In particular, buildings that directly adjoin residential properties should limit the potential for loading-related impacts, such as noise. Where possible, loading docks should be internal to the building envelope and equipped with closable doors. For all locations, loading areas should be kept clean.	<b>N/A:</b> No loading docks are proposed.
<b>E.3.7.06</b>	Guideline	Surface parking should be visually attractive, address security and safety concerns, retain existing mature trees and incorporate canopy trees for shade. See Section D.5 for more complete guidelines regarding landscaping in parking areas.	<b>N/A:</b> No surface parking lot is proposed.
<b>Utilities</b>			
<b>E.3.7.07</b>	Guideline	All utilities in conjunction with new residential and commercial development should be placed underground.	<b>Complies:</b> See note on C2.0.

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E.3.7.08	Guideline	Above ground meters, boxes and other utility equipment should be screened from public view through use of landscaping or by integrating into the overall building design.	<b>Complies:</b> The transformer has been placed underground. Gas meters have been placed on Chestnut Lane side of the building within the niche along the walkway; backflow devices have been placed inside the building. See C2.0.
<b>Parking Garages</b>			
E.3.7.09	Standard	To promote the use of bicycles, secure bicycle parking shall be provided at the street level of public parking garages. Bicycle parking is also discussed in more detail in Section F.5 “Bicycle Storage Standards and Guidelines.”	<b>Complies:</b> Two short term bike stands for 4 bikes are shown on the site plan, A0.3, on the Chestnut Street sidewalk area and 1 short term bike stand for 2 bikes is shown on Santa Cruz Avenue (6 spaces total; required 6). For long term bike parking 10 locked racks are shown on level P1, sheet A1.5, (9 spaces required)..
E.3.7.10	Guideline	Parking garages on downtown parking plazas should avoid monolithic massing by employing change in façade rhythm, materials and/or color.	<b>N/A:</b> A parking garage on a parking plaza is not proposed.
E.3.7.11	Guideline	To minimize or eliminate their visibility and impact from the street and other significant public spaces, parking garages should be underground, wrapped by other uses (i.e. parking podium within a development) and/or screened from view through architectural and/or landscape treatment.	<b>Complies:</b> Parking garage underground.
E.3.7.12	Guideline	Whether free-standing or incorporated into overall building design, garage façades should be designed with a modulated system of vertical openings and pilasters, with design attention to an overall building façade that fits comfortably and compatibly into the pattern, articulation, scale and massing of surrounding building character.	<b>N/A:</b> Garage underground.
E.3.7.13	Guideline	Shared parking is encouraged where feasible to minimize space needs, and it is effectively codified through the plan’s off-street parking standards and allowance for shared parking studies.	<b>Complies:</b> Shared parking would be used.
E.3.7.14	Guideline	A parking garage roof should be approached as a usable surface and an opportunity for sustainable strategies, such as installment of a green roof, solar panels or other measures that minimize the heat island effect.	<b>N/A:</b> Building placed on top of underground garage.
<b>E.3.8 Sustainable Practices</b>			
<b>Overall Standards</b>			
E.3.8.01	Standard	Unless the Specific Plan area is explicitly exempted, all citywide sustainability codes or requirements shall apply.	<b>Tentatively Complies:</b> The project would be required to comply at the building permit stage.
<b>Overall Guidelines</b>			
E.3.8.02	Guideline	Because green building standards are constantly evolving, the requirements in this section should be reviewed and updated on a regular basis of at least every two years.	<b>Tentatively Complies:</b> The project would be required to comply at the building permit stage.
<b>Leadership in Energy and Environmental Design (LEED) Standards</b>			

Menlo Park El Camino Real/Downtown Specific Plan  
Standards and Guidelines: Project Compliance Worksheet

**706 Santa Cruz: Downtown (D) Zone – “Main Street” Overlay**

<u>Section</u>	<u>Standard or Guideline</u>	<u>Requirement</u>	<u>Evaluation</u>
<b>E.3.8.03</b>	Standard	<p>Development shall achieve LEED certification, at Silver level or higher, or a LEED Silver equivalent standard for the project types listed below. For LEED certification, the applicable standards include LEED New Construction; LEED Core and Shell; LEED New Homes; LEED Schools; and LEED Commercial Interiors. Attainment shall be achieved through LEED certification or through a City-approved outside auditor for those projects pursuing a LEED equivalent standard. The requirements, process and applicable fees for an outside auditor program shall be established by the City and shall be reviewed and updated on a regular basis. LEED certification or equivalent standard, at a Silver level or higher, shall be required for:</p> <ul style="list-style-type: none"> <li>• Newly constructed residential buildings of Group R (single-family, duplex and multi-family);</li> <li>• Newly constructed commercial buildings of Group B (occupancies including among others office, professional and service type transactions) and Group M (occupancies including among others display or sale of merchandise such as department stores, retail stores, wholesale stores, markets and sales rooms) that are 5,000 gross square feet or more;</li> <li>• New first-time build-outs of commercial interiors that are 20,000 gross square feet or more in buildings of Group B and M occupancies; and</li> <li>• Major alterations that are 20,000 gross square feet or more in existing buildings of Group B, M and R occupancies, where interior finishes are removed and significant upgrades to structural and mechanical, electrical and/or plumbing systems are proposed.</li> </ul> <p>All residential and/or mixed use developments of sufficient size to require LEED certification or equivalent standard under the Specific Plan shall install one dedicated electric vehicle/plug-in hybrid electric vehicle recharging station for every 20 residential parking spaces provided. Per the Climate Action Plan the complying applicant could receive incentives, such as streamlined permit processing, fee discounts, or design templates.</p>	<p><b>Complies:</b> The plans show 55 points tentative for LEED Silver (50-59 points).</p> <p>Note: 11 Electric vehicle plug stalls are shown on plans.</p>
<b>Leadership in Energy and Environmental Design (LEED) Guidelines</b>			

Menlo Park El Camino Real/Downtown Specific Plan  
Standards and Guidelines: Project Compliance Worksheet

**706 Santa Cruz: Downtown (D) Zone – “Main Street” Overlay**

<u>Section</u>	<u>Standard or Guideline</u>	<u>Requirement</u>	<u>Evaluation</u>
<b>E.3.8.04</b>	Guideline	<p>The development of larger projects allows for more comprehensive sustainability planning and design, such as efficiency in water use, stormwater management, renewable energy sources and carbon reduction features. A larger development project is defined as one with two or more buildings on a lot one acre or larger in size. Such development projects should have sustainability requirements and GHG reduction targets that address neighborhood planning, in addition to the sustainability requirements for individual buildings (See Standard E.3.8.03 above). These should include being certified or equivalently verified at a LEED-ND (neighborhood development), Silver level or higher, and mandating a phased reduction of GHG emissions over a period of time as prescribed in the 2030 Challenge.</p> <p>The sustainable guidelines listed below are also relevant to the project area. They relate to but do not replace LEED certification or equivalent standard rating requirements.</p>	<b>N/A:</b> Site are below one acre.
<b>Building Design Guidelines</b>			
<b>E.3.8.05</b>	Guideline	Buildings should incorporate narrow floor plates to allow natural light deeper into the interior.	<b>Complies:</b> Narrow floor plates are proposed.
<b>E.3.8.06</b>	Guideline	Buildings should reduce use of daytime artificial lighting through design elements, such as bigger wall openings, light shelves, clerestory lighting, skylights, and translucent wall materials.	<b>Complies:</b> Skylights used.
<b>E.3.8.07</b>	Guideline	Buildings should allow for flexibility to regulate the amount of direct sunlight into the interiors. Louvered wall openings or shading devices like <i>bris soleils</i> help control solar gain and check overheating. <i>Bris soleils</i> , which are permanent sun-shading elements, extend from the sun-facing façade of a building, in the form of horizontal or vertical projections depending on sun orientation, to cut out the sun’s direct rays, help protect windows from excessive solar light and heat and reduce glare within.	<b>Complies:</b> Canopies and deep roof overhangs are used to shade numerous windows.
<b>E.3.8.08</b>	Guideline	Where appropriate, buildings should incorporate arcades, trellis and appropriate tree planting to screen and mitigate south and west sun exposure during summer. This guideline would not apply to downtown, the station area and the west side of El Camino Real where buildings have a narrower setback and street trees provide shade.	<b>Complies:</b> Street trees are provided along the sidewalks.



Menlo Park El Camino Real/Downtown Specific Plan  
Standards and Guidelines: Project Compliance Worksheet

**706 Santa Cruz: Downtown (D) Zone – “Main Street” Overlay**

<u>Section</u>	<u>Standard or Guideline</u>	<u>Requirement</u>	<u>Evaluation</u>
E.3.8.09	Guideline	Operable windows are encouraged in new buildings for natural ventilation.	<b>Complies:</b> Residential windows are operable. Retail/office windows do not appear operable. Some retail doors could be may operable at the building permit TI stage.
E.3.8.10	Guideline	To maximize use of solar energy, buildings should consider integrating photovoltaic panels on roofs.	<b>Complies:</b> Photovoltaic panels are noted on the roof plan as possible future location thereof.
E.3.8.11	Guideline	Inclusion of recycling centers in kitchen facilities of commercial and residential buildings shall be encouraged. The minimum size of recycling centers in commercial buildings should be 20 cubic feet (48 inches wide x 30 inches deep x 24 inches high) to provide for garbage and recyclable materials.	<b>Complies:</b> Recycling and compost will be provided in the building’s trash and recycling enclosure in the garage.
<b>Stormwater and Wastewater Management Guidelines</b>			
E.3.8.12	Guideline	Buildings should incorporate intensive or extensive green roofs in their design. Green roofs harvest rain water that can be recycled for plant irrigation or for some domestic uses. Green roofs are also effective in cutting-back on the cooling load of the air-conditioning system of the building and reducing the heat island effect from the roof surface.	<b>Complies:</b> No Green Roof but rainwater filtration planters are proposed at second floor terrace. See C3.0
E.3.8.13	Guideline	Projects should use porous material on driveways and parking lots to minimize stormwater run-off from paved surfaces.	<b>N/A:</b> Surface driveways or parking lots are not proposed.
<b>Landscaping Guidelines</b>			
E.3.8.14	Guideline	Planting plans should support passive heating and cooling of buildings and outdoor spaces.	<b>Complies:</b> Plants at sidewalks and on terraces, see L2.1, L2.2, L2.3.
E.3.8.15	Guideline	Regional native and drought resistant plant species are encouraged as planting material.	<b>Complies:</b> Plant list on L2.1, L2.2, L2.3
E.3.8.16	Guideline	Provision of efficient irrigation system is recommended, consistent with the City’s Municipal Code Chapter 12.44 "Water-Efficient Landscaping".	<b>Complies:</b> Reviewed by others. Water use calculations provided on C3.1.
<b>Lighting Standards</b>			
E.3.8.17	Standard	Exterior lighting fixtures shall use fixtures with low cut-off angles, appropriately positioned, to minimize glare into dwelling units and light pollution into the night sky.	<b>Complies:</b> Small sconces at all facades per E2.2, E4.1.
E.3.8.18	Standard	Lighting in parking garages shall be screened and controlled so as not to disturb surrounding properties, but shall ensure adequate public security.	<b>Complies:</b> E2.2, E4.1.
<b>Lighting Guidelines</b>			
E.3.8.19	Guideline	Energy-efficient and color-balanced outdoor lighting, at the lowest lighting levels possible, are encouraged to provide for safe pedestrian and auto circulation.	<b>Complies:</b> Lighting is proposed that would comply.

Menlo Park El Camino Real/Downtown Specific Plan  
Standards and Guidelines: Project Compliance Worksheet

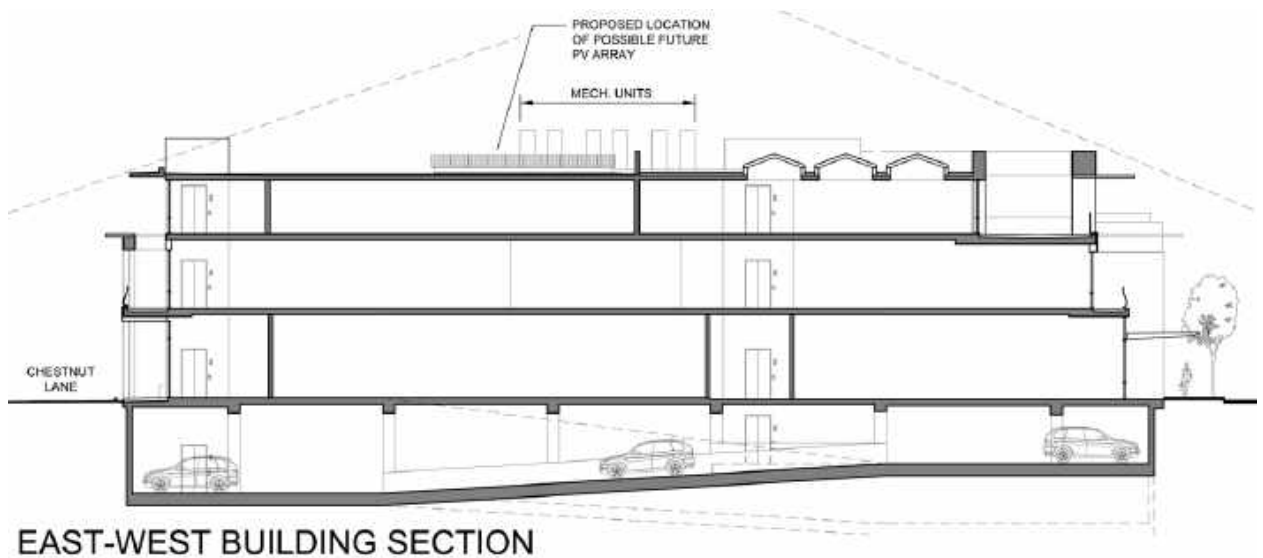
**706 Santa Cruz: Downtown (D) Zone – “Main Street” Overlay**

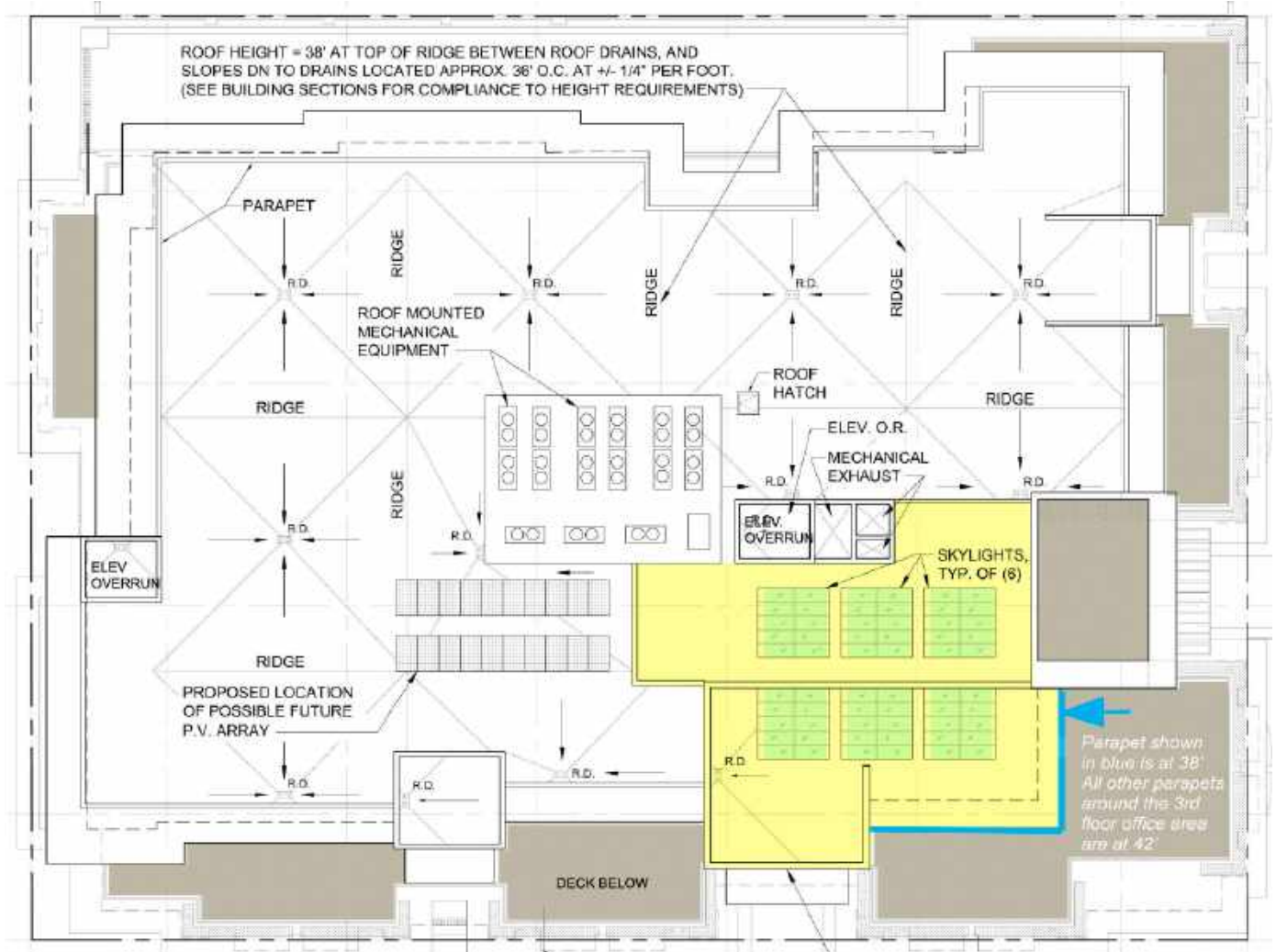
<u>Section</u>	<u>Standard or Guideline</u>	<u>Requirement</u>	<u>Evaluation</u>
<b>E.3.8.20</b>	Guideline	Improvements should use ENERGY STAR-qualified fixtures to reduce a building's energy consumption.	<b>Complies:</b> Lighting would be required to comply.
<b>E.3.8.21</b>	Guideline	Installation of high-efficiency lighting systems with advanced lighting control, including motion sensors tied to dimmable lighting controls or lighting controlled by timers set to turn off at the earliest practicable hour, are recommended.	<b>Complies:</b> : Lighting would be required to comply.
<b>Green Building Material Guidelines</b>			
<b>E.3.8.22</b>	Guideline	The reuse and recycle of construction and demolition materials is recommended. The use of demolition materials as a base course for a parking lot keeps materials out of landfills and reduces costs.	<b>Tentatively Complies:</b> Acknowledged by project architect.
<b>E.3.8.23</b>	Guideline	The use of products with identifiable recycled content, including post-industrial content with a preference for post-consumer content, are encouraged.	<b>Tentatively Complies:</b> Acknowledged by project architect.
<b>E.3.8.24</b>	Guideline	Building materials, components, and systems found locally or regionally should be used, thereby saving energy and resources in transportation.	<b>Tentatively Complies:</b> Acknowledged by project architect.
<b>E.3.8.25</b>	Guideline	A design with adequate space to facilitate recycling collection and to incorporate a solid waste management program, preventing waste generation, is recommended.	<b>Tentatively Complies:</b> Recology approved, Trash/Recycle room provided onsite.
<b>E.3.8.26</b>	Guideline	The use of material from renewable sources is encouraged.	<b>Tentatively Complies:</b> Acknowledged by project architect.

Variance Request & Findings  
706 Santa Cruz, Menlo Park, CA  
Skylights & Height

**VARIANCE REQUEST 1:**

This is a variance request to allow rooftop skylights at the proposed project located at 706 Santa Cruz, Menlo Park, CA. The following images show the skylights in section and a rendering from the Chestnut St / Santa Cruz Ave. Intersection where one would see the skylights if they were not located below the parapet height, or so far back on the roof that based on sight lines they are not visible. The following page includes the roof plan.





3rd floor office area shown in yellow

**FINDINGS:**

Menlo Park created the Downtown Plan in part to encourage a more active downtown by allowing mixed use projects. This project meets those aspirations by providing ground level retail, office on the 2nd floor, and residential, plus +/- 2,317 sf of office on the 3rd floor. The downtown plan specifies a height limit of 38' which has been interpreted to be measured to the highest element of the roof; including ridge slopes and skylights. Parapets may extend to 42' however, and roof mounted equipment can exceed even that height without being screened if sightline studies show it is not visible from the street.

In order to fit the mixed use program into 38' given the hardships mentioned on the next page, the office and residential floors are quite low by industry standards. As a way to improve the situation skylights have been located over the 3rd floor office. The skylights are not visible from the street because the parapet is as tall as the skylights (see section page 1), or so far back on the roof that based on sight lines they are not visible.. Further the skylights have been designed with the shallowest slope the manufacturer recommends.

1. **Hardship Peculiar to the Property:**

- Included on our property are 18 existing parking spaces contiguous with the City parking lot. This project is required to preserve the existing 18 parking space count in addition to meeting our particular project's parking requirements.
- A second hardship was created by the City's request to incorporate a walkway on our property for the benefit of Chestnut Lane.

2. **Necessary for the Preservation of property rights:**

- An FAR of 1.0 for office use is allowed per the Zoning Ordinance.
- The result of the above mentioned hardships, peculiar only to this property, is that some of the office program had to be located on the 3rd floor, and the costly underground parking garage had to be expanded by an extra half level.
- These hardships can be partially mitigated by maximizing the office space in order to help offset the extra parking costs. Adding office space to the 2nd floor was not possible since it already occupied the entire site. The balance of office space allowed was therefore located on the 3rd floor along with the residential.
- Providing the maximum allowed office area is necessary for the financial feasibility of the project, and to provide that office resource to Menlo Park's downtown. Office space with too low a head height will not attract tenants. Adding skylights to the 3rd floor office helps mitigate the low head height concern, and provide for the "preservation and enjoyment of substantial property rights."

3. **Not Detrimental to Public Health:**

- Based on the sight line study, the skylights are not visible from the street (see rendering p1). Eliminating them would make the goal of a vibrant downtown life through mixed use projects less feasible.
- Adding light to the 3rd floor office would improve the health & quality of life for the building tenants who are also members of the Menlo Park downtown community.
- The particular design proposed for this project provides a strong horizontal cap at the top of the second floor. This makes the overall building mass appear as two story. Due to sight lines, the third floor is not readily perceived by the public.





4. **Conditions would not be applicable to others:** Other properties can build up to 1 FAR without providing parking spaces or giving space for sidewalks. The hardships particular to this site would not apply to others.
  
5. **Condition Not Discussed:** We expect that the interpretation of the height limit to include skylights and other items hidden behind the roof parapet was not discussed or anticipated. They would have no impact to the Public.



# **ARBORIST REPORT**

**706 SANTA CRUZ AVENUE**  
**MENLO PARK, CALIFORNIA**

**Submitted to:**

706 Santa Cruz Avenue, LLC  
700 Santa Cruz Avenue  
Menlo Park, CA 94025

**Prepared by:**

David L. Babby  
*Registered Consulting Arborist<sup>®</sup> #399*  
*Board-Certified Master Arborist<sup>®</sup> #WE-4001B*

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October 26, 2018

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## EXHIBITS

<u>EXHIBIT</u>	<u>TITLE</u>
A	TREE INVENTORY TABLE (four sheets)
B	SITE MAP (one sheet)
C	PHOTOGRAPHS (four sheets)

## 1.0 INTRODUCTION

706 Santa Cruz Avenue, LLC is planning to construct a three-story building with a two-level underground parking garage at the addresses of 706, 708, 712, 714 and 716 Santa Cruz Avenue, Menlo Park; the project is titled 706 Santa Cruz Avenue, and is located at the north corner of Santa Cruz Avenue and Chestnut Street. As part of the submittal process, the owner has retained me to prepare this *Arborist Report*, and specific tasks assigned to execute are as follows:

- Visit the site, performed on 11/3/15 and 10/12/17, to identify 15 trees situated within and adjacent to the project area.
- Determine each tree's trunk diameter in accordance with Section 13.24.020 of the Menlo Park Municipal Code, rounded to the nearest inch.
- Identify which qualify as heritage trees<sup>1</sup> per City Code, as well as those within the public right-of-way and regarded as street trees.
- Ascertain each tree's health and structural integrity, and assign an overall condition rating (e.g. good, fair, poor or dead).
- Determine each tree's suitability for preservation (e.g. good, moderate or low).
- Obtain photographs; see Exhibit C.
- Document pertinent and observed health, structural and adjacent hardscape issues.
- Assign tree numbers, #1 thru 15, and include a copy of L4.0 (*Tree Disposition Plan*), dated 8/15/18, to represent the number relative to the trees' locations; see Exhibit B.
- Affix round metal tags with engraved, corresponding numbers to trees #1 thru 13.
- Review the Architectural Review plan set, dated 8/15/18, to identify potential impacts and the proposed tree disposition.
- Provide protection measures to help mitigate or avoid impacts to retained trees.
- Prepare a written report which presents the aforementioned information, and submit via email as a PDF document.

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<sup>1</sup> Section 13.24.020 of the Menlo Park Municipal Code defines a heritage tree, as it relates to this project, as any tree, other than a native California oak, being  $\geq 12'$  tall and having a trunk diameter  $\geq 15"$  at either 54" above grade or where multiple trunks divide.

## 2.0 TREE COUNT AND COMPOSITION

Fifteen (15) trees of eight various species were inventoried for this report. They are sequentially numbered as 1 thru 15, and the table below identifies their names, assigned numbers, counts and overall percentages.

NAME	TREE NUMBER(S)	COUNT	% OF TOTAL
Littleleaf linden	1 and 6	2	13%
Carob tree	14	1	7%
Chinese pistache	15	1	7%
Crape myrtle	2 and 3	2	13%
Victorian box	4, 5, 10 and 11	4	27%
California bay tree	12	1	7%
Southern magnolia	13	1	7%
Flowering pear	7, 8 and 9	3	20%
<b>Total</b>		<b>15</b>	<b>100%</b>

Specific information regarding each tree is presented within the table in **Exhibit A**. The trees' numbers and approximate locations can be viewed on the site map in **Exhibit B**, and photographs are presented in **Exhibit C**.

Trees #1, 10, 12 and 14 are defined by City Code as heritage trees.

Trees #1 thru 11 are regarded as street trees due to being situated within the public right-of-way; #1 thru 6 are along Santa Cruz Avenue, and #7 thru 11 are along Chestnut Street.

Trees #12 and 13 are located within a parking lot median between Chestnut Lane and the existing building, and #14 and 15 are located within a small parking lot island immediately north of parcel 2.



### 3.0 SUITABILITY FOR TREE PRESERVATION

Each tree has been assigned either a “good,” “moderate” or “low” suitability for preservation rating as a means to determine which qualify as suitable for incorporating into the future site development, through a process of cumulatively measuring their existing health, structural integrity, anticipated life span, location, public contribution, size, particular species, tolerance to construction impacts, growing space, regulated status, and safety to property and persons within striking distance. A description of these ratings are presented below; the good category comprises two trees, the moderate category nine, and the low category four.

**Good:** Applies to #8 and 9.

These two trees appear generally healthy and structurally stable; have no apparent, significant health issues or structural defects; present a good potential for contributing long-term to the site; and require only periodic care to maintain their longevity and structural integrity. Trees assigned this rating are the most suitable for retention and incorporating into the future development.

**Moderate:** Applies to #1-7, 12 and 13.

These trees contribute to the site but at notable levels less than those assigned a good suitability; have health and/or structural issues which may or may not be reasonably addressed and properly mitigated (in the case of #4 and 5, they will not improve); and frequent care is anticipated for their remaining lifespan. Trees assigned this rating might be worth retaining, if proper care is provided, but not at significant expense or major design revisions.

**Low:** Applies to #10, 11, 14 and 15.

These four trees should be removed, regardless of future development, due to having severely weakened and irreparable structures from advanced levels of decay and past pruning. For #10, 11 and 14, they seemingly present an unreasonable threat to persons and property below; see more detailed information regarding each within Exhibit A.

## 4.0 PROPOSED TREE DISPOSITION

A summary of the proposed tree disposition, as represented on project plans, is as follows:

- Retain in place: #1-9 and 14.
- Remove: #10-13 and 15.

Measures to help mitigate or avoid potential impacts to #1-9 and 14 are provided in the next section, and should be carefully followed throughout demolition and construction to achieve a reasonable assurance of their protection.

Information regarding the proposed removals is presented below.

Trees #10 and 11, Victorian box street trees, present a notable public safety threat due to their highly decayed, irreparable and extremely weak structures.

Tree #12, California bay, conflicts directly with constructing the underground garage and building, and there are no feasible design options available to achieve its retention.

Tree #13, Southern magnolia, is within the building footprint and appears in poor overall condition.

Tree #15, Chinese pistache, conflicts with the future installation of an electric utility pole. This tree is a replacement for a prior, declining carob removed sometime between 2012/2013, and has a suppressed, highly irregular form due to growing beneath #14's canopy.

## 5.0 TREE PROTECTION MEASURES

Recommendations presented within this section serve as protection measures to help mitigate or avoid impacts to street trees #1-9 and 14. They should be carefully followed and incorporated into project plans, and are subject to revision in the event project plans are modified. Please note that all referenced distances from trunks should be obtained the closest edge (face of) of their outermost perimeter at soil grade.

### 5.1 Design Guidelines

1. The Tree Protection Zone (TPZ) for #1 thru 6 should be as follows: up to the property line, up to existing back of curb, and 10 feet in both directions parallel to their respective streets. The TPZ for #7 thru 9 should be up to existing back of curb and 5 feet in all other directions. Tree #14's TPZ is 5 feet from its trunk.

A TPZ is where the following activities should be avoided: trenching, soil scraping, compaction, mass grading, finish-grading, overexcavation, subexcavation, swales, bioswales, storm drains, dissipaters, altering natural drainage patterns, equipment cleaning, stockpiling and dumping of materials, and equipment/vehicle operation. In the event an impact encroaches slightly within a setback, it can be reviewed on a case-by-case basis by the project arborist to determine whether measures can sufficiently mitigate the impacts to less-than-significant levels. I (hereinafter "project arborist") should be consulted in the event a TPZ cannot be achieved.

2. Show the trunk locations, diameters (as a circle to-scale) and assigned numbers on all site-related plans.
3. Utilize shoring for building the front, street portions of the underground garage (i.e. south and east walls).
4. Abandon all existing, unused lines or pipes within a TPZ, and any above-ground section should be cut off at existing soil grade (rather than being dug up and causing subsequent root damage); this provision should be specified on the demolition plan.

5. In the event tree #14 remains as plans indicate, measures needed to potentially minimize impacts include the following (provided in coordination with the City of Menlo Park's comments for the prior proposed project):
  - a. Locate the pole as far from the trunk as possible, with a minimum setback of 5 feet from trunk's nearest edge for the following: grading, trenching, excavation, compaction, and auguring to set the pole.
  - b. Route the utility connection at the pole in a direction away from the tree (e.g. radially from the pole's side opposite the trunk).
  - c. Pruning performed to accommodate auguring, lowering/setting the pole, and achieving clearance from conductors must not significantly distort or misshapen the tree's canopy, such as an entire side being removed, or a significant segment thereof. The extent of pruning required and impacts to the canopy should be determined and supervised by the City's arborist and/or designee.
6. Design and route utilities, irrigation, storm drains, dissipaters and swales beyond TPZs. Depending on the proximity to tree trunks, directional boring by at least 4 feet below existing grade may be needed, or digging within a TPZ can be manually performed using shovels (no jackhammers, and roots  $\geq 2$  inches in diameter retained and not damaged during the process). All tentative routes should be reviewed with the project arborist beforehand.
7. The erosion control design should consider that any straw wattle or fiber rolls require a maximum vertical soil cut of 2 inches for their embedment, and are established as close to canopy edges as possible (and not against a tree trunk).
8. Show the future staging area and route(s) of access on the final site plan, striving to avoid TPZs.
9. All site-related plans should contain notes referring to this report for tree protection measures.
10. Avoid specifying the use of herbicides use within a TPZ; where used on site, they should be labeled for safe use near trees. Also, liming shall not occur within 30 feet of a tree's canopy.

11. Adhere to the following additional landscape guidelines:
  - a. Establish irrigation and lighting features (e.g. main line, lateral lines, valve boxes, wiring and controllers) so no trenching occurs within a TPZ. Should this not be feasible, they may require being installed in a radial direction to a tree's trunk, and terminate a specific distance from a trunk (versus crossing past it). The routes and overall layout should be reviewed with the project arborist prior to any trenching or excavation occurring.
  - b. Avoid any tilling, ripping and compaction within TPZs.
  - c. Establish any bender board or other edging material within TPZs to be on top of existing soil grade (such as by using vertical stakes).
  - d. Utilize a 3- to 4-inch layer of coarse wood chips or other high quality mulch for new ground cover beneath canopies (avoid using gorilla hair, bark or rock, stone, gravel, black plastic or other synthetic cover).

## **5.2 Before Demolition, Grading and Construction**

12. Ensure water continues being supplied to planter areas throughout demolition and construction.
13. Tree pruning should be performed before or near the onset of demolition, to including clearing the existing and future buildings, elevating canopies (mostly through pruning away watersprouts), removing deadwood, and reducing limb/branch weight. For #14, pruning considerations are described in Section 4.0 of this report. All work must be performed under direction of the project arborist, in accordance with the most recent ANSI A300 standards, and by a California licensed tree-service contractor (D-49) having an ISA certified arborist in a supervisory role, and carrying General Liability and Worker's Compensation insurance. The City may also require a permit be issued prior to pruning occurring (due to being street trees).
14. Conduct a site meeting between the general contractor and project arborist several weeks (or more) prior to demolition for the purpose of reviewing tree fencing, shoring, routes of access, offsite improvements, demolition, staging and protection measures presented herein.



15. Install tree protection fencing prior to building demolition, and maintain throughout construction for the purpose of avoiding trunk damage and restricting access into unpaved ground within a TPZ (i.e. to protect existing planter areas and trunks). For #7 thru 9, place panels mounted by concrete blocks or metal stands to close off their *entire* planters (i.e. existing unpaved sections surrounding their trunks), whether by construction perimeter or other panel



fencing, and wrap their trunks five times with orange-plastic fencing, from the ground to where branching begins. For #1 thru 6, protection is shown above; it involves wrapping wattle around the trunk at the top and bottom of fence boards (2" by 4"), which should be vertical and extend from the ground to near the first large limb, then wrapping orange-plastic fencing around the boards three times and tie together (the sign is not needed); there are other fencing options, and can be discussed as needed. Additionally, limbs or sections of trees protruding beyond the fencing area and exposed to damage may need protecting by being wrapped with a 1- to 2-inch thick layer (about 5 to 10 layers) of orange-plastic fencing. Note that prior to the City issuing a permit, they require I provide a letter confirming fencing has been installed per this report. Also note that fencing may require modification for offsite improvement work, to be determined following consultation with the project arborist prior to such work commencing.

### 5.3 During Demolition, Grading and Construction

16. Care must be taken during demolition of existing hardscape and other features within a TPZ to avoid damaging a tree's trunk, crown and roots. Care must also be taken by equipment operators to position their equipment for avoiding trunks and branches, including the scorching of foliage. Any tree damage or injury should be reported to the project arborist for assessment.

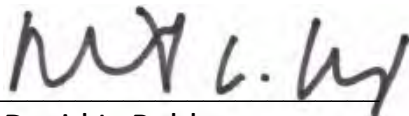
17. Demolition of the existing sidewalk shall be carefully performed to avoid, where within a TPZ, excavating into the ground and damaging roots  $\geq 1$ -inch in diameter.

18. For shoring installation, ensure placement and operation of any pile driver or drill rig is beyond canopies, and does not require the removal of large limbs during the process (this should be reviewed with the project arborist beforehand).
19. Any authorized access, digging or trenching within designated-fenced areas shall be by foot-traffic only and manually performed without the use of heavy equipment.
20. For approved trenching within a TPZ, avoid damaging or cutting roots  $\geq 1$ -inch in diameter without prior assessment by the project arborist. Should roots of this size become encountered, within one hour of exposure, wrap with burlap and keep continually moist until the covered by soil. If they are approved for severing, the root shall be cleanly severed at  $90^\circ$  to the angle of root growth against the cut line (using loppers or a sharp hand saw), and then immediately after, bury the cut end with soil.
21. Where within the specified TPZs, installation of the new sidewalk should avoid the loss of roots  $\geq 1$ -inch in diameter, and excavation performed to meet subgrade shall be manually performed using a shovel. Adhere to the above root pruning guidelines should any  $\geq 1$ -inch in diameter root be encountered during the process.
22. Spoils created during digging shall not be piled or spread on unpaved ground within a TPZ. If essential, spoils can be temporarily piled on plywood or a tarp.
23. Tree trunks shall not be used as winch supports for moving or lifting heavy loads.
24. The permanent and temporary drainage design, including downspouts, should not require water being discharged towards a tree's trunk.
25. Avoid disposing harmful products (such as cement, paint, chemicals, oil and gasoline) beneath canopies or anywhere on site that allows drainage within or near TPZs. Herbicides should not be used with a TPZ; where used on site, they should be labeled for safe use near trees.

## 6.0 ASSUMPTIONS AND LIMITING CONDITIONS

- All information presented herein reflects my observations and/or measurements obtained from the ground and project site on 11/3/15, and for trees #14 and 15, on 10/12/17.
- Condition and suitability ratings of dormant trees are subject to change once they can be observed following the growth of new leaves.
- My observations were performed visually without probing, coring, dissecting or excavating.
- The assignment pertains solely to trees listed in Exhibit A. I hold no opinion towards other trees on or surrounding the project area.
- I cannot provide a guarantee or warranty, expressed or implied, that deficiencies or problems of any trees or property in question may not arise in the future.
- No assurance can be offered that if all my recommendations and precautionary measures (verbal or in writing) are accepted and followed, that the desired results may be achieved.
- I cannot guarantee or be responsible for the accuracy of information provided by others.
- I assume no responsibility for the means and methods used by any person or company implementing the recommendations provided in this report.
- The information provided herein represents my opinion. Accordingly, my fee is in no way contingent upon the reporting of a specified finding, conclusion or value.
- The site map in Exhibit B is solely intended to represent a tree's approximate location and assigned number.
- This report is proprietary to me and may not be copied or reproduced in whole or part without prior written consent. It has been prepared for the sole and exclusive use of the parties to who submitted for the purpose of contracting services provided by David L. Babby.
- If any part of this report or copy thereof be lost or altered, the entire evaluation shall be invalid.

Prepared By:



David L. Babby

Registered Consulting Arborist® #399

Board-Certified Master Arborist® #WE-4001B

CA Licensed Tree Service Contractor #796763 (C61/D49)

Date: October 26, 2018



**EXHIBIT A:**

**TREE INVENTORY TABLE**

(four sheets)



## TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	SIZE	CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

<b>1</b>	Littleleaf linden ( <i>Tilia cordata</i> )	18	60%	40%	Fair	Moderate	X
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Comments: Street tree. High canopy with much of foliage in lower crown consisting of watersprouts. Narrow, tall form. Multiple leaders at 10' high. Christmas lights wrapped around trunk.

<b>2</b>	Crape myrtle ( <i>Lagerstroemia indica</i> )	7	60%	30%	Poor	Moderate	
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Comments: Street tree. Very high canopy and narrow form. Within a circular recessed planter covered by steel grates. Trunk is against and growing over lip of grate. Christmas lights wrapped around trunk.

<b>3</b>	Crape myrtle ( <i>Lagerstroemia indica</i> )	5	50%	30%	Poor	Moderate	
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Comments: Street tree. Very high canopy and narrow form. Within a circular recessed planter covered by steel grates. Christmas lights wrapped around trunk.

<b>4</b>	Victorian box ( <i>Pittosporum undulatum</i> )	11	40%	30%	Poor	Moderate	
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Comments: Street tree. Lollipop shaped, very high canopy. Watersprouts along lower trunk. Within a circular planter comprised of decomposed granite. Continued decline, most evident by the notable dieback along the canopy's top. Trunk grows with a slight lean towards street, and crown has asymmetrical growth away from adjacent building. Rated moderate suitability solely due to being an established street tree (otherwise has a low suitability).





### TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	SIZE	CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

5	Victorian box ( <i>Pittosporum undulatum</i> )	10	40%	30%	Poor	Moderate	
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Comments: Street tree. Within a circular planter comprised of decomposed granite. Partly buried root collar. Very sparse and declining. High, and mostly one-sided canopy towards street. Codominant leaders at 11' high. Rated moderate suitability solely due to being a street tree (otherwise has a low suitability).

6	Littleleaf linden ( <i>Tilia cordata</i> )	12	50%	40%	Poor	Moderate	
---	---	----	-----	-----	------	----------	--

Comments: Street tree. Extension cord wrapped around trunk's base, and Christmas lights around trunk. Extensive watersprouts throughout canopy. Excessive limb weight. Dieback. Branches grow against existing building and roof. Multiple leaders at 8' high.

7	Flowering pear ( <i>Pyrus calleryana</i> )	2	60%	60%	Fair	Moderate	
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Comments: Street tree. Double-staked, one having come out of ground; both stakes can be removed as tree seems sufficiently anchored. Symptoms of infection by fire blight. Old wound at trunk's base.

8	Flowering pear ( <i>Pyrus calleryana</i> )	2	70%	80%	Good	Good	
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Comments: Street tree. Double-staked, and both can be removed as tree seems more than sufficiently anchored. Symptoms of infection by fire blight.

9	Flowering pear ( <i>Pyrus calleryana</i> )	3	60%	80%	Good	Good	
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Comments: Street tree. Within a square planter containing decomposed granite.



## TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	SIZE	CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		

<b>10</b>	Victorian box ( <i>Pittosporum undulatum</i> )	16	40%	20%	Poor	Low	X
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Comments: Street tree. Outgrowing planter, and adjacent curb is broken. Canopy grows onto roof and adjacent building wall. Within a square planter containing decomposed granite. Deadwood. Extensive decay along trunk and multiple leaders. Beneath high-voltage wires (distribution).

<b>11</b>	Victorian box ( <i>Pittosporum undulatum</i> )	12	40%	20%	Poor	Low	
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Comments: Street tree. Beneath high-voltage wires (distribution). Western sycamore borer infestation. Adjacent curb is pushed out. Has a vertical column of decay along entire trunk, street side. High canopy.

<b>12</b>	California bay tree ( <i>Umbellularia californica</i> )	29	60%	40%	Fair	Moderate	X
-----------	--	----	-----	-----	------	----------	---

Comments: Three codominants stems (14, 14 and 12") at 2' high, and the 29" diameter is measured just below their union. High canopy, and a nearly 5' wide section of which grows below high-voltage wires (distribution). Watersprouts within lower crown. Has an overall poor structure with leggy form. Excessive branch weight. Within a large planter, the majority of which is comprised of decomposed granite. Abundant level of sooty mold throughout canopy. Low end of moderate suitability.

<b>13</b>	Southern magnolia ( <i>Magnolia grandiflora</i> )	13	50%	40%	Poor	Moderate	
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Comments: Within a large planter consisting of decomposed granite. Three large wounds along lower trunk. Top center section cut out. Has a notably thin canopy, due to either being overpruned or the tree progressively declining; deadwood is continually being removed.



## TREE INVENTORY TABLE

TREE/ TAG NO.	TREE NAME	SIZE	CONDITION			Suitability for Preservation (Good/Moderate/Low)	Heritage Tree
		Trunk Diameter (in.)	Health Condition (100%=Best, 0%=Worst)	Structural Integrity (100%=Best, 0%=Worst)	Overall Condition (Good/Fair/Poor/Dead)		
14	Carob tree ( <i>Ceratonia siliqua</i> )	26	60%	20%	Poor	Low	X

Comments: Within a small parking lot planter; surrounding curb, gutter and asphalt are significantly raised or buckled. Grows partially beneath high-voltage wires, and was reduced in height years ago for clearance; ensuing growth is roughly 12-15' beyond cuts. Excessive limb and branch weight, and has an asymmetrical canopy. Dead branches overhanging lot. Structure is notably weakened due to numerous large wounds and cankers along the trunk and lower crown; weak attachments between leaders, most notably one with nearly 4' of included bark; and the past and ongoing pruning required for clearance from the electrical wires results in weakly attached, rapidly growing branches throughout the tree's remaining lifespan.

15	Chinese pistache ( <i>Pistacia chinensis</i> )	2	60%	30%	Poor	Low	
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Comments: Highly suppressed growth and entirely understory to (i.e. growing beneath) #14's canopy. Crown sweeps east, and has a highly irregular form. Represents a replacement for previous declining carob tree removed at some point during 2012 to 2013.

**EXHIBIT B:**

**SITE MAP**

(one sheet)

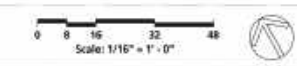
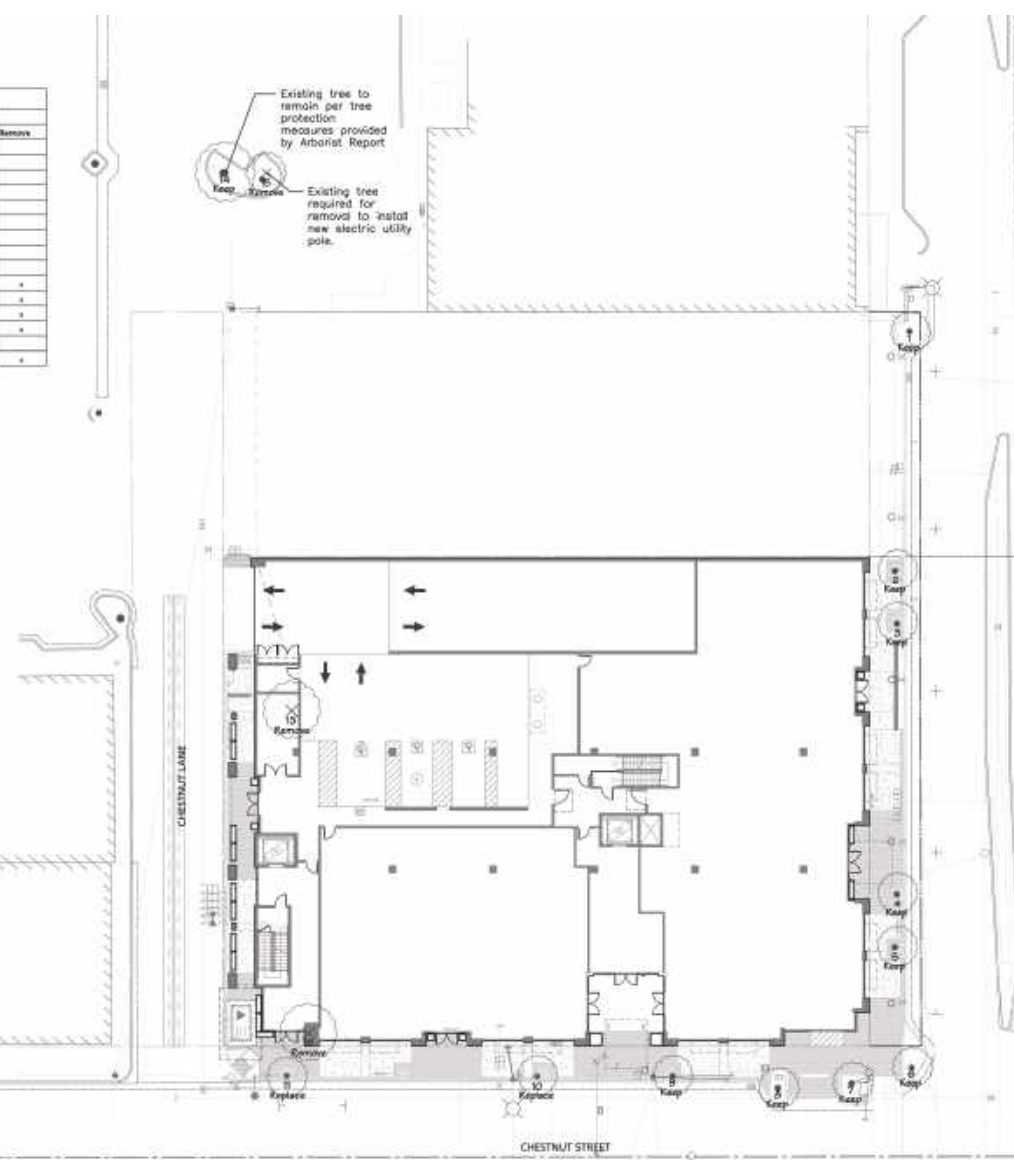
EXISTING TREE INVENTORY - 11/16/2018					
Tag No.	Botanical Name	Common Name	Trunk Diameter (in.)	Retain	Remove
1	<i>Tilia cordata</i>	Limeleaf Linden	10	x	
2	<i>Lagerströmia indica</i>	Crape Myrtle	7	x	
3	<i>Lagerströmia indica</i>	Crape Myrtle	6	x	
4	<i>Pithecolobium lanceolatum</i>	Viscayan Bee	11	x	
5	<i>Pithecolobium lanceolatum</i>	Viscayan Bee	10	x	
6	<i>Tilia cordata</i>	Limeleaf Linden	10	x	
7	<i>Pyracantha coccinea</i>	Flowering Pear	3	x	
8	<i>Pyracantha coccinea</i>	Flowering Pear	3	x	
9	<i>Pyracantha coccinea</i>	Flowering Pear	3	x	
10	<i>Pithecolobium lanceolatum</i>	Viscayan Bee	10		x
11	<i>Pithecolobium lanceolatum</i>	Viscayan Bee	10		x
12	<i>Lindleyana latifolia</i>	California Bay Tree	20		x
13	<i>Magnolia grandiflora</i>	Southern Magnolia	12		x
14	<i>Cassia alata</i>	Coast Tree	20	x	
15	<i>Platanus chinensis</i>	Chinese Platanus	2		x

EXISTING AND PROPOSED TREES	
Total Existing Trees	16
Total Existing Trees to be Removed	3
Total Existing Trees to Remain	13
Total Proposed Trees	16
<b>Net Total Trees for Project</b>	<b>30</b>

**TREE DISPOSITION LEGEND**

 Existing Tree to be removed  
 Existing Tree to Remain

NOTE: Tree Disposition Plan has been prepared based on Arbores Report prepared by David L. Reilly, Inc. Arbores Report dated November 11th, 2018 for this development project.



FORM4 ARCHITECTURE • THE GUZZARDO PARTNERSHIP • KIER & WRIGHT • ADAPTURE • INTERFACE ENGINEERING

**706 SANTA CRUZ AVE. MENLO PARK**  
 Architectural Review  
 706 Santa Cruz Ave., LLC.

**TREE DISPOSITION PLAN**

L 4.0  
 October 30, 2018





**EXHIBIT C:**  
**PHOTOGRAPHS**  
(four sheets)

**Photo Index**

**Page C-1:** Trees #1 thru 5

**Page C-3:** Trees #11 thru 13

**Page C-2:** Trees #6 thru 10

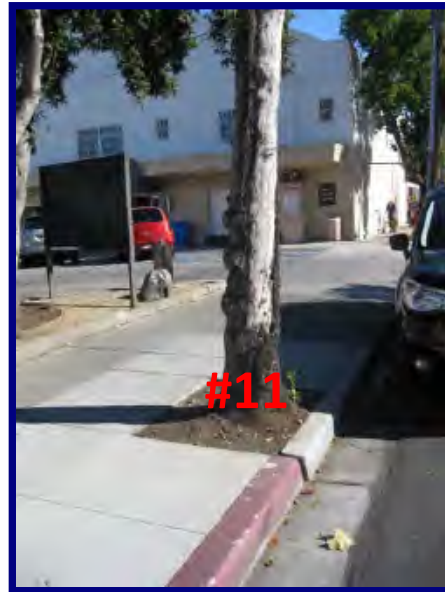
**Page C-4:** Trees #14 and 15

















**706-716 Santa Cruz  
El Camino Real/Downtown Specific Plan Program EIR – Conformance Checklist**

### **Introduction**

The City of Menlo Park (City) has developed the El Camino Real/Downtown Specific Plan (Specific Plan) to establish a framework for private and public improvements in the Specific Plan area over the coming decades. The Specific Plan addresses approximately 130 acres and focuses on the character and density of private infill development, the character and extent of enhanced public spaces, and circulation and connectivity improvements. The primary goal of the Specific Plan is to “enhance the community life, character and vitality through mixed use infill Projects sensitive to the small-town character of Menlo Park, an expanded public realm, and improved connections across El Camino Real.” The Specific Plan includes objectives, policies, development standards, and design guidelines intended to guide new private development and public space and transportation improvements in the Specific Plan area. The Plan builds upon the El Camino Real/Downtown Vision Plan that was unanimously accepted by the Menlo Park City Council on July 15, 2008.

On June 5, 2012, the City Council certified the Menlo Park El Camino Real and Downtown Specific Plan Program EIR (Program EIR). According to the Program EIR, the Specific Plan does not propose specific private developments, but establishes a maximum development capacity of 474,000 square feet of non-residential development (inclusive of retail, hotel, and commercial development), and 680 new residential units.

Form 4 Architecture on behalf of the Oros family has submitted an application for a 46,908 square foot, three-story, mixed-use project including two-levels of underground parking. The project site consists of one parcel (Assessor’s Parcel Number 071-102-250) at 706-716 Santa Cruz Avenue, which is currently occupied by existing commercial buildings and surface parking. The Project would demolish the existing commercial buildings and site improvements. The property is part of the Specific Plan area, and as such may be covered by the Program EIR analysis. The intent of this Environmental Conformity Analysis is to determine: 1) whether the Project does or does not exceed the environmental impacts analyzed in the Program EIR, 2) whether new impacts have or have not been identified, and 3) whether new mitigation measures are or are not required.

### **Existing Condition**

The subject parcel is located on the northwest corner of Santa Cruz Avenue and Chestnut Street which is part of the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district. The adjoining properties to the site include Ace Hardware store to the north, Le Boulanger restaurant to the south, Axion Learning center and several small businesses to the west and multiple small businesses to the east. The 0.54 acre (23,454 square feet) project site is currently occupied by the Juban Yakiniku House

(712 Santa Cruz Avenue) and a computer service store (708 Santa Cruz Avenue). The project site is relatively flat, rectangular shaped parcel, occupied by buildings facing Santa Cruz Avenue, with surface parking at the rear.

## **Project**

The Project includes the demolition of the existing buildings, site improvements and the construction of a three-story building including 12,049 square feet of retail, 23,454 square feet of office space, and four residential units including two-levels of underground parking. The maximum building height is 37.75' to the top of the roof and approximately 44' to the top of the elevator.

The ground floor includes at-grade parking, building lobbies, and retail space; the second level would consist of office space and the third level would consist of office space and residential units. The office space and residential units would have access from the building lobbies.

The Project includes two-levels of underground parking accessed by a two-way driveway ramp down off Chestnut Lane. A total of 69 parking spaces are proposed in surface and underground parking. Based on the parking requirements for each use, the Project is required to provide 68 total parking spaces, including replacement of 18 existing spaces, four spaces for residential use, and 46 spaces for the office use.

The ground floor consists of retail space and surface parking spaces off Chestnut Lane. Pedestrian access to the retail is provided from Santa Cruz Avenue and Chestnut Street with stair and elevator access to the underground parking.

The second level consists of office space with outdoor terraces. The third level has an office space with an outdoor terrace area and four residential units with access from an interior hall from the residential lobby area via the elevator and stairs. Each unit has a private terraced open space area.

The enclosed trash and recycle is located on Chestnut Lane. Trash and recycle containers are accessed via Chestnut Lane. Landscaping is proposed along Santa Cruz Avenue, Chestnut Lane, and Chestnut Street. Two Heritage trees are proposed to be removed due to health and development impact. One tree is considered a street tree and the other tree is located on the project site.

The Project includes Architectural Control, a Variance to allow skylights on the third floor to exceed the maximum height limit, a Vesting Tentative Map to create four residential units and one commercial area, with rights reserved to allow up to ten commercial condominiums, Heritage tree removal permits for two trees (one street tree and one on-site tree) and a Below Market Rate (BMR) Housing In-lieu fee agreement. The Planning Commission will serve as a recommending body and the City Council will be the final decision making body and take action on the proposed project.

## **Environmental Analysis**

As discussed in the introduction, this comparative analysis has been undertaken to analyze whether the Project would have any significant environmental impacts that are not addressed in the Program EIR. The comparative analysis discusses whether impacts are increased, decreased, or unchanged from the conclusions discussed in the Program EIR. The comparative analysis also addresses whether any changes to mitigation measures are required.

As noted previously, the proposal is a mixed-use Project, demolishing the existing commercial buildings and site improvements. Assuming full occupancy, the Project is estimated to generate 28 net trips in the AM peak hour and 26 net trips in the PM peak hour. Based on this level of vehicle traffic, a detailed traffic study is not required, as long as the land use assumptions on-site are consistent with those outlined in the Specific Plan. The Project is consistent with the Specific Plan land uses. The Project will be subject to the fair share contribution towards infrastructure required to mitigate transportation impacts as identified in the Downtown Specific Plan Final Environmental Impact Report.

### *Aesthetic Resources*

Impacts would be the same as the Specific Plan. The Program EIR concluded that the Project would not have a substantial adverse effect on a scenic view, vista, or designated state scenic highway, nor would the Project have significant impacts to the degradation of character/quality, light and glare, or shadows.

Implementation of the Project would result in the construction of a mixed-use development. Similar development concepts were evaluated under the Specific Plan EIR, and determined that changes to the visual character would not be substantially adverse, and the impact would be considered less than significant. The Project is subject to the Planning Commission architectural control review and approval, which includes public notice and ensures aesthetic compatibility. The Project meets the design standards and guidelines as noted in the El Camino Real/Downtown Specific Plan by breaking up the elevations, incorporating projections (terraces) and activating the street with retail on the ground floor. Therefore, the Project would not result in any impacts to the existing visual character of the site and its surroundings.

A variance is being requested to allow third floor skylights to exceed the maximum height limit. The skylights are not visible and would be concealed from view by parapet walls and would not result in any impacts to the existing visual character of the site and its surroundings.

Similar development concepts were evaluated under the Specific Plan EIR, and determined that changes to light and glare would not be substantially adverse, and the impact would be less than significant. The Specific Plan includes regulatory standards for nighttime lighting and nighttime and daytime glare. Therefore, the Project would not result in any impacts associated with substantial light or glare.

A solar analysis was prepared for the Project which demonstrated that shadows west onto the neighboring building across Chestnut Lane would be at its lowest and longest during the winter solstice and shorter during the spring and fall equinoxes. Similar development concepts were evaluated under the Specific Plan EIR, and determined that the longest shadows would occur in the morning and afternoon. In general, there are limited new shadow impacts, none of which have the potential to significantly affect in an adverse manner the use of outdoor recreational areas, public open spaces, historical resources, or substantial numbers of properties. Given the built character of the Plan area, most new shadow tends to overlap existing shadow as opposed to creating shadow where none previously existed. Furthermore, the Project itself includes design requirements that reduce shadow impacts including setbacks, and height variation that serve to limit the size of upper levels and the shadows cast by the buildings, therefore the Project would not result in any new impacts associated with shadow impacts.

As was the case with the Specific Plan, the Project would not have a substantial adverse effect on a scenic view or vista, a state scenic highway, character/quality, or light and glare impacts. Therefore, no new impacts have been identified and no new mitigation measures are required for the Project.

#### *Agriculture Resources*

Impacts would be the same as the Specific Plan. The Program EIR concluded that no impacts would result with regard to Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, or any area zoned for agricultural use or forest land.

As was the case with the Program EIR, the Project would not result in any impacts to farmland, agricultural uses, or forest land. Therefore, no new impacts have been identified and no new mitigation measures are required for the Project.

#### *Air Quality*

Impacts would be the same as the Specific Plan.

AIR-1: The Program EIR determined that emissions of criteria pollutants associated with construction would be significant, and established Mitigation Measures AIR-1a and AIR-1b to address such impacts. Mitigation Measure AIR-1a would be applied to this proposal. However, the Program EIR concluded that impacts could still be significant and unavoidable even with implementation of such mitigations. The Project would construct a three-story, mixed-use Project with two-levels of underground parking and would not involve the type of large-scale construction activities that would create additional impacts. The Project would be well below the 249 dwelling units and 277,000 square feet of commercial development construction screening threshold adopted by the Bay Area Air Quality Management District. As a result, implementation of Mitigation Measure AIR-1b is not required for this Project.

AIR-2: The Program EIR determined that the Specific Plan would have long-term emissions of criteria pollutants from increased vehicle traffic and on-site area sources that would contribute to an air quality violation (due to being inconsistent with an element of the *2010 Clean Air Plan*), and established Mitigation Measure AIR-2 requiring implementation of Mitigation Measure TR-2 regarding Transportation Demand Management (TDM) strategies to address this impact. However, the Program EIR noted that TDM effectiveness cannot be guaranteed, and concluded that the impact would be significant and unavoidable. The Project would be consistent with the Program EIR analysis, and as such would be required to implement Mitigation Measure AIR-2.

AIR-3: The Program EIR determined that the Specific Plan would increase levels of Toxic Air Contaminants (TACs) due to increased heavy duty truck traffic, but that the impacts would be less than significant. The Project would not generate an unusual amount of heavy truck traffic relative to other mixed-use developments due to the limited nature of the construction, and the Project's limited share of overall Specific Plan development would be accounted for through deduction of its totals from the Specific Plan Maximum Allowable Development.

AIR-4: The Program EIR concluded that the Specific Plan would not have a substantial adverse effect pertaining to Particulate Matter (PM<sub>2.5</sub>). The Project is consistent with the assumptions of this analysis.

AIR-5, AIR-6, AIR-7, AIR-8, AIR-10, and AIR-11: The Specific Plan determined that the introduction of sensitive receptors, specifically new residences, to an environment (near El Camino Real and the Caltrain tracks, as well as to a zone in proximity to the SRI International campus) with elevated concentrations of TACs and PM<sub>2.5</sub> could result in significant or potentially significant impacts (including in the cumulative scenario), and established Mitigation Measures AIR-5, AIR-7, and AIR-10 to bring impacts to less than significant levels. Although the project site is in proximity to the Caltrain tracks, implementing certain components of Mitigation Measure AIR-7 would reduce cancer risk to a less than significant level. Mitigation Measure AIR-5 and AIR-10 would not apply, because the project site is a sufficient distance from major roadways and the SRI International campus.

AIR-9: The Program EIR determined that the Specific Plan is fundamentally consistent with the growth projections of the Bay Area 2010 Clean Air Plan, particularly with regard to residential development. The project proposes 4 residential condominium units and commercial and office space, which is consistent with the growth projections of the Bay Area 2010 Clean Air Plan.

No new Air Quality impacts have been identified and no new mitigation measures are required for the Project.

### *Biological Resources*



Impacts would be the same as the Specific Plan. The Program EIR determined that less than significant impacts would result with regard to special status plant and wildlife species, sensitive natural communities, migratory birds, and jurisdictional waters and wetlands upon implementation of the recommended Mitigation Measures BIO-1a, BIO-1b, BIO-3a, BIO-3b, BIO-5a through BIO-5c, and BIO-6a. Mitigation Measures BIO-1a, BIO-1b, BIO-3a, BIO-3b, and BIO-5a through BIO-5c would apply to the Project, but BIO-6a would not (it is limited to Projects proposing development near San Francisquito Creek). The analysis also found that the Specific Plan would not conflict with local policies, ordinances, or plans. The Project site is fully developed and within a highly urbanized/landscaped area.

The Project site includes little wildlife habitat and essentially no habitat for plants other than the opportunity ruderal species adapted to the built environment or horticultural plants used in landscaping. The Project would not result in the take of candidate, sensitive, or special-status species.

There are 15 trees on or near the site including 13 street trees. Two Heritage trees and two non-heritage street trees are proposed to be removed, and four new trees are proposed to be planted. The Program EIR determined that no mitigation would be required with implementation of the Heritage Tree Ordinance Chapter 13.24 which requires a planting replacement at a 2:1 basis for Heritage trees on commercial Projects and at a 1:1 basis for non-heritage street trees. Since the Project has a zero setback, there is not adequate room to plant six replacement trees, therefore the applicant would be required to plant 3- 48-inch box trees and one tree in the parking plaza. The Heritage Tree Ordinance Chapter does allow the City Arborist to exercise discretion on the size and number of trees an applicant may be required to install to meet the intent of the ordinance. Additionally, the City of Menlo Park's Building Division provides "Tree Protection Specification" measures and procedures to further insure the protection of Heritage trees during construction. Compliance with these existing code requirements, guidelines, and Tree Protection Specification measures and procedures, coupled with additional tree planting, would mitigate the impact of any loss of protected trees and would constitute consistency with local ordinances designed to protect existing tree resources. The impact would be less than significant.

With implementation of the Project, construction activities would occur on an existing developed site. Therefore, as with the Program EIR, the Project would result in less than significant impacts to biological resources and no new Mitigation Measures would be required. The Project would also not conflict with local policies, ordinances, or plans, similar to the Program EIR. No new impacts have been identified and no new mitigation measures are required for the Project.

### *Cultural Resources*

Impacts would be the same as the Specific Plan. The Program EIR determined that no significant impacts to a historic resource would result with implementation of Mitigation Measure CUL-1. The analysis also concluded that the Specific Plan would result in less than significant impacts to archeological resources, paleontological resources, and

burial sites with implementation of Mitigation Measures CUL-2a, CUL-2b, CUL-3, and CUL-4. With regard to the Project site, the physical conditions, as they relate to archeological resource, have not changed in the Specific Plan area since the preparation of the Specific Plan EIR. The Project would incorporate Mitigation Measure CUL-4 through notations on plan sheets and ongoing on-site monitoring. Mitigation Measure CUL-3 would be required, as the Project would excavate beyond previously disturbed soil for the proposed two levels of below grade parking.

In compliance with Mitigation Measure CUL-1, a Historic Resource Evaluation was prepared by Preservation Architecture, dated March 2016 for the Project. The report concluded the commercial buildings were found not to be historically significant, as the buildings were constructed in 1954, it is a relatively recent commercial resource with a relatively brief and narrow commercial history. Within its commercial context, no events of importance have been identified, nor have any associated persons of potential historic importance. Therefore, the Project site does not have historical or historic potential for inclusion on the National Register of Historic Places or the California Registrar of Historical Resources.

In compliance with Mitigation Measure CUL-2a, an Archeological Resource Evaluation was prepared by Basin Research Associates, dated January 29, 2016 for the Project. The report concluded, the archival research revealed that there are no recorded cultural resources located within the study area. No traces of significant cultural materials, prehistoric or historic, were noted during the surface reconnaissance. In the event, however, that prehistoric traces are encountered, the Specific EIR requires protection activities if archaeological artifacts are found during construction.

No new impacts have been identified and no new mitigation measures are required.

### *Geology and Soils*

Impacts would be the same as the Specific Plan. The Program EIR found that no significant impacts pertaining to earthquake faults, seismic ground shaking, seismically induced hazards (e.g., liquefaction, lateral spreading, land sliding, settlement, and ground lurching), unstable geologic units, expansive soils, corrosive soils, landslides, and soil erosion would result. No Mitigation Measures are required.

The Project site is not located within an Alquist-Priolo Earthquake Fault Zone as designated by the California Geological Society, and no known active faults exist on the site. The nearest active fault to the project area is the San Andreas fault which is located approximately 4.7 miles southwest of the property. Although this is the case, the Project is in a seismically active area and, while unlikely, there is a possibility of future faulting and consequent secondary ground failure from unknown faults is considered to be low. Furthermore, the Project would comply with requirements set in the California Building Code (CBC) to withstand settlement and forces associated with the maximum credible earthquake. The CBC provides standards intended to permit structures to withstand seismic hazards. Therefore, the code sets standards for excavation, grading,

construction earthwork, fill embankments, expansive soils, foundation investigations, liquefaction potential, and soil strength loss. A Geotechnical Investigation was prepared by Romig Engineers, INC, dated December 2015 for the Project and a supplemental letter dated February 12, 2019. The report concluded the site is suitable for the proposed mixed-use development provided the recommendations in the report are followed during design and construction. No mitigation is required.

### *Greenhouse Gas Emissions*

Impacts would be the same as the Specific Plan.

GHG-1: The Program EIR determined that the Specific Plan would generate Greenhouse Gas (GHG) emissions, both directly and indirectly, that would have a significant impact on the environment. Specifically, the operational GHG using the Bay Area Air Quality District (BAAQMD) GHG Model, measured on a “GHG: service population” ratio, were determined to exceed the BAAQMD threshold. The Project’s share of this development and associated GHG emissions and service population, would be accounted for through deduction of this total from the Specific Plan Maximum Allowable Development, and as such is consistent with the Program EIR analysis. The Program EIR established Mitigation Measure GHG-1, although it was determined that the impact would remain significant and unavoidable even with this mitigation. For the Project, implementation of Mitigation Measure GHG-1 is not necessary as the BAAQMD-identified GHG Mitigation Measures are primarily relevant to City-wide plans and policies and because the City’s CAL Green Amendments have since been adopted and are applied to all projects, including this Project.

GHG-2: The Program EIR determined that the Specific Plan could conflict with AB 32 and its Climate Change Scoping Plan by exceeding the per-capita threshold cited in GHG-1. Again, the Project’s share of this development and associated GHG emissions and service population, would be accounted for through deduction of this total from the Specific Plan Maximum Allowable Development, and as such is consistent with the Program EIR analysis. The Program EIR established Mitigation Measure GHG-2a and GHG-2b, although it was determined that the impact would remain significant and unavoidable even with this mitigation.

No new impacts have been identified and no new mitigation measures are required for the Project.

### *Hazards and Hazardous Materials*

Impacts would be the same as the Specific Plan. The Program EIR determined that a less than significant impact would result in regards to the handling, transport, use, or disposal of hazardous materials during construction operations. The analysis also concluded that the Project site is not included on a list of hazardous materials sites, is not within the vicinity of an airport or private airstrip, would not conflict with an emergency response plan, and would not be located in an area at risk for wildfires. The

Specific Plan analysis determined that with implementation of Mitigation Measures HAZ-1 and HAZ-3, impacts related to short-term construction activities, and the potential handling of and accidental release of hazardous materials would be reduced to less than significant levels.

The Project would involve ground-disturbance and demolition of an existing commercial building and improvements and as such implementation of Mitigation Measures HAZ-1 and HAZ-3 would be required. Project operations would result in a mixed-used development. The Project would not handle, store, or transport hazardous materials in quantities that would be required to be regulated.

In compliance with Mitigation Measure HAZ-1, an Environmental Site Assessment Phase 1 was prepared by AEI Consultants, dated March 7, 2019 for the Project. The report concluded, no potential hazardous releases were identified therefore a Phase II was not required. Thus, Project operations would result in similar impacts as that analyzed for the Specific Plan. No new impacts have been identified and no new mitigation measures are required for the Project.

#### *Hydrology and Water Quality*

Impacts would be the same as the Specific Plan. The Program EIR found that no significant impacts pertaining to construction-related impacts (i.e., water quality and drainage patterns due to erosion and sedimentation), or operational-related impacts to water quality, groundwater recharge, the alteration of drainage patterns, or flooding would result. The City of Menlo Park Engineering Division requires a Grading and Drainage Permit and preparation of a construction plan for any construction Project disturbing 500 square feet or more of dirt. The Grading and Drainage (G&D) Permit requirements specify that the construction must demonstrate that the sediment laden-water shall not leave the site. Incorporation of these requirements would be expected to reduce the impact of erosion and sedimentation to a less-than-significant level. No Mitigation Measures are required.

#### *Land Use and Planning*

Impacts would be the same as the Specific Plan.

LU-1: The Program EIR determined that the Specific Plan would not divide an established community. The Project would involve demolition of existing building and on-site improvements. The Specific Plan would allow for taller buildings, any new development would occur along the existing grid pattern and proposed heights and massing controls would result in buildings comparable with existing and proposed buildings found in the Plan area. The proposed development consists of a construction of a three-story, mixed-use building with two-levels of underground parking and is subject to architectural review by the Planning Commission. The Project would not

create a physical or visual barrier, therefore would not physically divide a community. There are no impacts.

LU-2: The Program EIR determined that the Specific Plan would not alter the type and intensity of land uses in a manner that would cause them to be substantially incompatible with surrounding land uses or neighborhood character. The Project is an infill mixed-use development that meets the intent of the Specific Plan, and would be consistent with the General Plan. No mitigation is required for this impact, which is less than significant.

LU-3: The Program EIR determined that the Specific Plan would not conflict with the City's General Plan, Zoning Ordinance, or other land use plans or policies adopted for the purpose of mitigating an environmental effect. A variance would be requested to allow third floor skylights to exceed the maximum height limit. The General Plan and Zoning Ordinance were amended concurrent with the Specific Plan adoption, and the Project would comply with all relevant regulations. No mitigation is required for this impact, which is less than significant.

LU-4: The Program EIR determined that the Specific Plan, in combination with other plans and projects, would not result in cumulatively considerable impacts to land use. The Project, being a part of the Specific Plan area and accounted for as part of the Maximum Allowable Development, is consistent with this determination. No mitigation is required for this impact, which is less than significant.

No new impacts have been identified and no new mitigation measures are required for the Project.

#### *Mineral Resources*

Impacts would be the same as the Specific Plan. The Program EIR noted that the Project site is not located within an area of known mineral resources, either of regional or local value.

As was the case with the Specific Plan, the Project would not result in the loss of availability of a known mineral resource or mineral resources recovery site. No new impacts have been identified and no new mitigation measures are required for the Project.

#### *Noise*

Impacts would be the same as the Specific Plan.

NOI-1: The Program EIR determined that construction noise, in particular exterior sources such as jackhammering and pile driving, could result in a potentially significant impact, and established Mitigation Measures NOI-1a through NOI-1c to address such impacts. The physical conditions as they relate to noise levels have not changed



substantially in the Specific Plan area since the preparation of the Specific Plan EIR. Therefore, construction noise impacts of the Project would be less than significant, and these mitigation measures would apply (with the exception of Mitigation Measure NOI-1b, which applies to pile driving activities, which wouldn't take place as part of the Project).

NOI-3: The Program EIR determined that the Specific Plan could include the introduction of sensitive receptors (i.e., new residences) to a noise environment with noise levels in excess of standards considered acceptable under the City of Menlo Park Municipal Code (i.e., near the Caltrain tracks), as well as the introduction of sensitive receptors to substantial levels of ground borne vibration from the Caltrain tracks. Mitigation Measures NOI-3 would require detailed acoustical assessments for residential units constructed within the Specific Plan area to ensure that Title 24 interior noise level standards are achieved.

NOI-4: The Program EIR determined that the Specific Plan could include the introduction of sensitive receptors, specifically new residences, to substantial levels of ground borne vibration from the Caltrain tracks. The project area is not adjacent to the Caltrain right-of-way, which has the potential for vibration-related issues. Therefore, the proposed project would not result in any impacts related to ground borne noise or vibration.

NOI-5: The Program EIR determined that implementation of the Specific Plan, together with anticipated future development in the area in general, would result in a significant increase in noise levels in the area. The Program EIR established Mitigation Measure NOI-5 to require the City to use rubberized asphalt in future paving projects within the Plan area if it determines that it will significantly reduce noise levels and is feasible given cost and durability, but determined that due to uncertainties regarding Caltrans approval and cost/feasibility factors, the cumulative impact of increased traffic noise on existing sensitive receptors is significant and unavoidable. The proposed project's share of this development would be accounted for through deduction of this total from the Specific Plan Maximum Allowable Development.

No new Noise impacts have been identified and no new mitigation measures are required for the Project.

### *Population and Housing*

Impacts would be similar from that analyzed in the Program EIR.

POP-1: The Program EIR determined that the implementation of the Specific Plan would not cause the displacement of existing residents to the extent that the construction of replacement facilities outside of the Plan area would be required. The Project site is existing commercial buildings and includes the construction of a three-story, mixed-use development with two-levels of underground parking. Therefore, no

residents would be displaced. No mitigation is required for this impact, which is less than significant.

POP-2: The Program EIR determined that the implementation of the Specific Plan would not be expected to induce growth in excess of current Projections, either directly or indirectly. The Program EIR found that full build-out under the Specific Plan would result in 1,537 new residents, well within the Association of Bay Area Governments (ABAG) Projection of 5,400 new residents between 2010 and 2030 in Menlo Park and its sphere of influence. Additionally, the Program EIR projected the new job growth associated with the new retail, commercial and hotel development to be 1,357 new jobs. The ABAG projection for job growth within Menlo Park and its sphere of influence is an increase of 7,240 jobs between 2010 and 2030. The Program EIR further determines that based on the ratio of new residents to new jobs, the Specific Plan would result in a jobs-housing ratio of 1.56, below the projected overall ratio for Menlo Park and its sphere of influence of 1.70 in 2030 and below the existing ratio of 1.78.

The Project includes the construction of a three-story, mixed-use development, with one-level of underground parking. Construction of the Project, including site preparation, would temporarily increase construction employment. Given the relatively common nature and scale of the construction associated with the Project, the demand for construction employment would likely be met within the existing and future labor market in the City and the County. The size of the construction workforce would vary during the different stages of construction, but a substantial quantity of workers from outside the City or County would not be expected to relocate permanently.

POP-3: The Program EIR determined that implementation of the Specific Plan, in combination with other plans and projects would not result in cumulatively considerable impacts to population and housing. The EIR identified an additional 959 new residents and 4,126 new jobs as a result of other pending Projects. These combined with the projection for residents and jobs from the Specific Plan equate to 2,496 new residents and 5,483 new jobs, both within ABAG Projections for Menlo Park and its sphere of influence in 2030. The additional jobs associated with the Project would not be considered a substantial increase, would continue to be within all projections and impacts in this regard would be considered less than significant. Thus, no new impacts have been identified and no new mitigation measures are required for the Project.

No new Population and Housing impacts have been identified and no new mitigation measures are required for the Project.

#### *Public Services and Utilities*

Impacts would be the same as the Specific Plan. The Program EIR concluded that less than significant impacts to public services, including fire protection, police protection, schools, parks, and other public facilities would result. In addition, the Program EIR concluded that the Project would result in less than significant impacts to utilities and service systems, including water services, wastewater services, and solid waste. No

mitigation measures were required under the Program EIR for Public Services and Utilities impacts.

The Menlo Park Fire Protection District (MPFPD) currently serves the project area. MPFPD review and approval of individual development plans is a standard part of the Project review process, ensuring that new buildings meet all relevant service requirements. MPFPD have completed an initial Project review, and have tentatively approved the Project for compliance with applicable Fire Code regulations. The Project would not intensify development over what has previously been analyzed, nor modify building standards (height, setbacks, etc.) in a way that could affect the provision of emergency services by the MPFPD. Therefore, the Project would not result in any impacts resulting in the need for new or physically altered fire facilities.

Public parks near the project area include Burgess Park, Fremont Park, and Nealon Park. Additional public facilities, such as the library and recreational facilities at the Civic Center complex are located next to Burgess Park. The project would not intensify development over what has previously been analyzed, and existing public facilities would continue to be sufficient to serve the population of the project area. Therefore, the proposed project would not result in the demand for new public parks or other public facilities.

The existing water, wastewater, electric, gas, and solid waste infrastructure is adequate to support the Project, as the mixed-use development would not exceed what was previously analyzed, which the current site was developed to support.

No new Public Services and Utilities impacts have been identified and no new mitigation measures are required for the Project.

#### *Transportation, Circulation and Parking*

Assuming full occupancy, the Project is estimated to generate 28 new trips in the AM peak hour and 26 net new trips in the PM peak hour. Based on this level of vehicle traffic, a detailed traffic study is not required, as the land use assumptions on site are consistent with those outlined in the Downtown Specific Plan. The Project is consistent with the Specific Plan land uses. The Project would be subject to the fair-share contribution towards infrastructure required to mitigate transportation impacts.

The Project is consistent with the Specific Plan land uses. The Project would be subject to the fair share contribution towards infrastructure required to mitigate transportation impacts as identified in the Downtown Specific Plan Final Environmental Impact Report.

TR-1 and TR-7: The Program EIR concluded that the Specific Plan would result in significant and unavoidable traffic impacts related to operation of area intersections and local roadway segments, in both the short-term and cumulative scenarios, even after implementation of Mitigation Measures TR-1 and TR-7. The Project would pay required

TIF (Transportation Impact Fee) and fair-share contributions as part of these mitigations.

TR-2 and TR-8: The Program EIR determined that the Specific Plan would adversely affect operation of certain local roadway segments, in both the near-term and cumulative scenarios. The Project's share of the overall Specific Plan development would be accounted for through deduction of this total from the Specific Plan Maximum Allowable Development, and as such is consistent with the Program EIR analysis.

In addition, the Project would be required through the MMRP to implement Mitigation Measure TR-2, requiring submittal and City approval of a Transportation Demand Management (TDM) program prior to Project occupancy. The goal of the TDM plan is to identify trip reduction methods to be implemented in order to reduce the number of AM and PM peak single occupant vehicle (SOV) trips that are generated by the project site. A TDM Plan was prepared by TDM Specialists, INC, dated October 4, 2019. This TDM plan is estimated to reduce the number of new SOV trips by 20 percent using a variety of infrastructure and incentive based measures such as carpooling, transit riding, bicycling, walking and telecommuting. However, this mitigation (which is also implemented through Mitigation Measure AIR-2) cannot have its effectiveness guaranteed, as noted by the Program EIR, so the impact remains significant and unavoidable.

TR-3, TR-4, TR-5, and TR-6: The Program EIR determined that the Specific Plan would not result in impacts to freeway segment operations, transit ridership, pedestrian and bicycle safety, or parking in the downtown. The Project, using a parking rate supported by appropriate data and analysis, would be consistent with this analysis, and no new impacts or mitigation measures would be projected.

No new impacts have been identified and no new mitigation measures are required for the Project.

## **Conclusion**

As discussed, the Conformance Checklist is to confirm that 1) the Project does not exceed the environmental impacts analyzed in the Program EIR, 2) that no new impacts have been identified, and 3) no new mitigation measures are required. As detailed in the analysis presented above, the Project would not result in greater impacts than were identified for the Program EIR. No new impacts have been identified and no new mitigation measures are required for the Project.

## **References**

1. Arborist Report prepared by Arbor Resources dated October 26, 2018.
2. Cultural Resource Evaluation prepared by Basin Research Associates dated January 29, 2016

3. Historic Resource Evaluation prepared by Preservation Architecture, dated March 10, 2016.
4. Phase I Environmental Site Assessment prepared by AEI Consultants, dated March 7, 2019.
5. Geotechnical Investigation prepared by Romig Engineers, INC, dated December 2015 and supplemental letter dated February 12, 2019.
6. Plans prepared by the Form 4 Architecture.
7. TDM Plan prepared by TDM Specialists dated October 4, 2019.
8. Staff site visit November 6, 2016.
9. Trip Generation Analysis prepared by Hexagon Transportation Consultants dated May 7, 2019.



El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue				
Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
AIR QUALITY				
<b>IMPACT BEING ADDRESSED: Impact AIR-1: Implementation of the Specific Plan would result in increased long-term emissions of criteria pollutants associated with construction activities that could contribute substantially to an air quality violation. (Significant)</b>				
<p><i>Mitigation Measure AIR-1a:</i> During construction of individual projects under the Specific Plan, project applicants shall require the construction contractor(s) to implement the following measures required as part of Bay Area Air Quality Management District's (BAAQMD) basic dust control procedures required for construction sites. For projects for which construction emissions exceed one or more of the applicable BAAQMD thresholds, additional measures shall be required as indicated in the list following the Basic Controls.</p> <p><u>Basic Controls that Apply to All Construction Sites</u></p> <ol style="list-style-type: none"> <li>1. All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day.</li> <li>2. All haul trucks transporting soil, sand, or other loose material off-site shall be covered.</li> <li>3. All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.</li> <li>4. All vehicle speeds on unpaved roads shall be limited to 15 mph.</li> <li>5. All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.</li> <li>6. Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points.</li> <li>7. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.</li> </ol>	<p>Exposed surfaces shall be watered twice daily.</p> <p>Trucks carrying demolition debris shall be covered.</p> <p>Dirt carried from construction areas shall be cleaned daily.</p> <p>Speed limit on unpaved roads shall be 15 mph.</p> <p>Roadways, driveways, sidewalks and building pads shall be laid as soon as possible after grading.</p> <p>Idling times shall be minimized to 5 minutes or less; Signage posted at all access points.</p> <p>Construction equipment shall be properly tuned and maintained.</p>	<p>Measures shown on plans, construction documents and on-going during demolition, excavation and construction.</p>	<p>Project sponsor(s) and contractor(s)</p>	<p>PW/CDD</p>

<b>El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue</b>				
<b>Mitigation Measure</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
8. Post a publicly visible sign with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. The BAAQMD's phone number shall also be visible to ensure compliance with applicable regulations.	Signage will be posted with the appropriate contact information regarding dust complaints.			
<b><i>Impact AIR-2: Implementation of the Specific Plan would result in increased long-term emissions of criteria pollutants from increased vehicle traffic and on-site area sources that would contribute substantially to an air quality violation. (Significant)</i></b>				
<b>Mitigation Measure AIR-2:</b> Mitigation Measure TR-2 of Section 4.13, Transportation, Circulation and Parking, identifies Transportation Demand Management (TDM) strategies to be implemented by individual project applicants, although the precise effectiveness of a TDM program cannot be guaranteed. As the transportation demand management strategies included in Mitigation Measure TR-2 represent the majority of available measures with which to reduce VMT, no further mitigation measures are available and this impact is considered to be significant and unavoidable.	See Mitigation Measure TR-2.			

El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue				
Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
<b>Impact AIR-7: Implementation of the Specific Plan would expose sensitive receptors to elevated concentrations of Toxic Air Contaminants (TACs) associated with Caltrain operations which may lead to considerable adverse health effects. (Potentially Significant)</b>				
<p><b>Mitigation Measure AIR-7:</b> The Mitigation Monitoring and Reporting Program shall require that all developments that include sensitive receptors such as residential units that would be located within approximately 1,095 feet of the edge of the Caltrain right-of-way shall undergo, prior to project approval, a screening-level health risk analysis to determine if cancer risk, hazard index, and/or PM<sub>2.5</sub> concentration would exceed BAAQMD thresholds. If one or more thresholds would be exceeded at the site of the subsequent project, the project (or portion of the project containing sensitive receptors, in the case of a mixed-use project) shall be equipped with filtration systems with a Minimum Efficiency Reporting Value (MERV) rating of 14 or higher. The ventilation system shall be designed by an engineer certified by the American Society of Heating, Refrigeration and Air-Conditioning Engineers, who shall provide a written report documenting that the system reduces interior health risks to less than 10 in one million, or less than any other threshold of significance adopted by BAAQMD or the City for health risks. The project sponsor shall present a plan to ensure ongoing maintenance of ventilation and filtration systems and shall ensure the disclosure to buyers and/or renters regarding the findings of the analysis and inform occupants as to proper use of any installed air filtration. Alternatively, if the project applicant can prove at the time of development that health risks at new residences due to DPM (and other TACs, if applicable) would be less than 10 in one million, or less than any other threshold of significance adopted by BAAQMD for health risks, or that alternative mitigation measures reduce health risks below any other City-adopted threshold of significance, such filtration shall not be required.</p>	<p>A health risk analysis shall be prepared.</p> <p>If one or more thresholds are exceeded, a filtration system shall be installed; Certified engineer to provide report documenting that system reduces health risks</p> <p>Plan developed for ongoing maintenance and disclosure to buyers and/renters.</p>	<p>Simultaneous with a building permit submittal</p>	<p>Project sponsor(s)</p>	<p>CDD</p>

El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue				
Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
BIOLOGICAL RESOURCES				
<b>Impact BIO-1: The Specific Plan could result in the take of special-status birds or their nests. (Potentially Significant)</b>				
<p><b>Mitigation Measure BIO-1a:</b> Pre-Construction Special-Status Avian Surveys. No more than two weeks in advance of any tree or shrub pruning, removal, or ground-disturbing activity that will commence during the breeding season (February 1 through August 31), a qualified wildlife biologist will conduct pre-construction surveys of all potential special-status bird nesting habitat in the vicinity of the planned activity. Pre-construction surveys are not required for construction activities scheduled to occur during the non-breeding season (August 31 through January 31). Construction activities commencing during the non-breeding season and continuing into the breeding season do not require surveys (as it is assumed that any breeding birds taking up nests would be acclimated to project-related activities already under way). Nests initiated during construction activities would be presumed to be unaffected by the activity, and a buffer zone around such nests would not be necessary. However, a nest initiated during construction cannot be moved or altered.</p> <p><b>If pre-construction surveys indicate that no nests of special-status birds are present or that nests are inactive or potential habitat is unoccupied:</b> no further mitigation is required.</p> <p><b>If active nests of special-status birds are found during the surveys:</b> implement Mitigation Measure BIO-1b.</p>	<p>A nesting bird survey shall be prepared if tree or shrub pruning, removal or ground-disturbing activity will commence between February 1 through August 31.</p>	<p>Prior to tree or shrub pruning or removal, any ground disturbing activity and/or issuance of demolition, grading or building permits.</p>	<p>Qualified wildlife biologist retained by project sponsor(s)</p>	<p>CDD</p>

**El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue**

Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
<p><b>Mitigation Measure BIO-1b:</b> Avoidance of active nests. If active nests of special-status birds or other birds are found during surveys, the results of the surveys would be discussed with the California Department of Fish and Game and avoidance procedures will be adopted, if necessary, on a case-by-case basis. In the event that a special-status bird or protected nest is found, construction would be stopped until either the bird leaves the area or avoidance measures are adopted. Avoidance measures can include construction buffer areas (up to several hundred feet in the case of raptors), relocation of birds, or seasonal avoidance. If buffers are created, a no disturbance zone will be created around active nests during the breeding season or until a qualified biologist determines that all young have fledged. The size of the buffer zones and types of construction activities restricted will take into account factors such as the following:</p> <ol style="list-style-type: none"> <li>1. Noise and human disturbance levels at the Plan area and the nesting site at the time of the survey and the noise and disturbance expected during the construction activity;</li> <li>2. Distance and amount of vegetation or other screening between the Plan area and the nest; and</li> <li>3. Sensitivity of individual nesting species and behaviors of the nesting birds.</li> </ol>	<p>If active nests are found during survey, the results will be discussed with the California Department of Fish and Game and avoidance procedures adopted.</p> <p>Halt construction if a special-status bird or protected nest is found until the bird leaves the area or avoidance measures are adopted.</p>	<p>Prior to tree or shrub pruning or removal, any ground-disturbing activities and/or issuance of demolition, grading or building permits.</p>	<p>Project sponsor(s) and contractor(s)</p>	<p>CDD</p>
<p><b>Impact BIO-3: Impacts to migratory or breeding special-status birds and other special-status species due to lighting conditions. (Potentially Significant)</b></p>				
<p><b>Mitigation Measure BIO-3a:</b> Reduce building lighting from exterior sources.</p> <ol style="list-style-type: none"> <li>a. Minimize amount and visual impact of perimeter lighting and façade up-lighting and avoid uplighting of rooftop antennae and other tall equipment, as well as of any decorative features;</li> <li>b. Installing motion-sensor lighting, or lighting controlled by timers set to turn off at the earliest practicable hour;</li> <li>c. Utilize minimum wattage fixtures to achieve required lighting levels;</li> <li>d. Comply with federal aviation safety regulations for large buildings by installing minimum intensity white strobe lighting with a three-second flash interval instead of continuous flood lighting, rotating lights, or red lighting</li> <li>e. Use cutoff shields on streetlight and external lights to prevent upwards lighting.</li> </ol>	<p>Reduce building lighting from exterior sources.</p>	<p>Prior to building permit issuance and ongoing.</p>	<p>Project sponsor(s) and contractor(s)</p>	<p>CDD</p>



<b>El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue</b>				
<b>Mitigation Measure</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p><b>Mitigation Measure BIO-3b:</b> Reduce building lighting from interior sources.</p> <p>a. Dim lights in lobbies, perimeter circulation areas, and atria;</p> <p>b. Turn off all unnecessary lighting by 11pm thorough sunrise, especially during peak migration periods (mid-March to early June and late August through late October);</p> <p>c. Use gradual or staggered switching to progressively turn on building lights at sunrise.</p> <p>d. Utilize automatic controls (motion sensors, photosensors, etc.) to shut off lights in the evening when no one is present;</p> <p>e. Encourage the use of localized task lighting to reduce the need for more extensive overhead lighting;</p> <p>f. Schedule nightly maintenance to conclude by 11 p.m.;</p> <p>g. Educate building users about the dangers of night lighting to birds.</p>	<p>Reduce building lighting from interior sources.</p>	<p>Prior to building permit issuance and ongoing.</p>	<p>Project sponsor(s) and contractor(s)</p>	<p>CDD</p>
<p><b>Impact BIO-5: The Specific Plan could result in the take of special-status bat species. (Potentially Significant)</b></p>				
<p><b>Mitigation Measure BIO-5a:</b> Preconstruction surveys. Potential direct and indirect disturbances to special-status bats will be identified by locating colonies and instituting protective measures prior to construction of any subsequent development project. No more than two weeks in advance of tree removal or structural alterations to buildings with closed areas such as attics, a qualified bat biologist (e.g., a biologist holding a California Department of Fish and Game collection permit and a Memorandum of Understanding with the California Department of Fish and Game allowing the biologist to handle and collect bats) shall conduct pre-construction surveys for potential bats in the vicinity of the planned activity. A qualified biologist will survey buildings and trees (over 12 inches in diameter at 4.5-foot height) scheduled for demolition to assess whether these structures are occupied by bats. No activities that would result in disturbance to active roosts will proceed prior to the completed surveys. If bats are discovered during construction, any and all construction activities that threaten individuals, roosts, or hibernacula will be stopped until surveys can be completed by a qualified bat biologist and proper mitigation measures implemented.</p> <p><b>If no active roosts present:</b> no further action is warranted.</p>	<p>Retain a qualified bat biologist to conduct pre-construction survey for bats and potential roosting sites in vicinity of planned activity.</p> <p>Halt construction if bats are discovered during construction until surveys can be completed and proper mitigation measures implemented.</p>	<p>Prior to tree pruning or removal or issuance of demolition, grading or building permits.</p>	<p>Qualified bat biologist retained by project sponsor(s)</p>	<p>CDD</p>

El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue				
Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
<b>If roosts or hibernacula are present:</b> implement Mitigation Measures BIO-5b and 5c.				
<b>Mitigation Measure BIO-5b:</b> Avoidance. If any active nursery or maternity roosts or hibernacula of special-status bats are located, the subsequent development project may be redesigned to avoid impacts. Demolition of that tree or structure will commence after young are flying (i.e., after July 31, confirmed by a qualified bat biologist) or before maternity colonies forms the following year (i.e., prior to March 1). For hibernacula, any subsequent development project shall only commence after bats have left the hibernacula. No-disturbance buffer zones acceptable to the California Department of Fish and Game will be observed during the maternity roost season (March 1 through July 31) and during the winter for hibernacula (October 15 through February 15). Also, a no-disturbance buffer acceptable in size to the California Department of Fish and Game will be created around any roosts in the Project vicinity (roosts that will not be destroyed by the Project but are within the Plan area) during the breeding season (April 15 through August 15), and around hibernacula during winter (October 15 through February 15). Bat roosts initiated during construction are presumed to be unaffected, and no buffer is necessary. However, the "take" of individuals is prohibited.	If any active nursery or maternity roosts or hibernacula are located, no disturbance buffer zones shall be established during the maternity roost and breeding seasons and hibernacula.	Prior to tree removal or pruning or issuance of demolition, grading or building permits	Qualified bat biologist retained by project sponsor(s)	CDD
<b>Mitigation Measure BIO-5c:</b> Safely evict non-breeding roosts. Non-breeding roosts of special-status bats shall be evicted under the direction of a qualified bat biologist. This will be done by opening the roosting area to allow airflow through the cavity. Demolition will then follow no sooner or later than the following day. There should not be less than one night between initial disturbance with airflow and demolition. This action should allow bats to leave during dark hours, thus increasing their chance of finding new roosts with a minimum of potential predation during daylight. Trees with roosts that need to be removed should first be disturbed at dusk, just prior to removal that same evening, to allow bats to escape during the darker hours. However, the "take" of individuals is prohibited.	A qualified bat biologist shall direct the eviction of non-breeding roosts.	Prior to tree removal or pruning or issuance of demolition, grading or building permits.	Qualified bat biologist retained by project sponsor(s)	CDD

El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue				
Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
CULTURAL RESOURCES				
<b>Impact CUL-1: The proposed Specific Plan could have a significant impact on historic architectural resources. (Potentially Significant)</b>				
<p><b>Mitigation Measure CUL-1:</b> Site Specific Evaluations and Treatment in Accordance with the Secretary of the Interior's Standards:</p> <p><b>Site-Specific Evaluations:</b> In order to adequately address the level of potential impacts for an individual project and thereby design appropriate mitigation measures, the City shall require project sponsors to complete site-specific evaluations at the time that individual projects are proposed at or adjacent to buildings that are at least 50 years old.</p> <p>The project sponsor shall be required to complete a site-specific historic resources study performed by a qualified architectural historian meeting the Secretary of the Interior's Standards for Architecture or Architectural History. At a minimum, the evaluation shall consist of a records search, an intensive-level pedestrian field survey, an evaluation of significance using standard National Register Historic Preservation and California Register Historic Preservation evaluation criteria, and recordation of all identified historic buildings and structures on California Department of Parks and Recreation 523 Site Record forms. The evaluation shall describe the historic context and setting, methods used in the investigation, results of the evaluation, and recommendations for management of identified resources. If federal or state funds are involved, certain agencies, such as the Federal Highway Administration and California Department of Transportation (Caltrans), have specific requirements for inventory areas and documentation format.</p>	<p>A qualified architectural historian shall complete a site-specific historic resources study. For structures found to be historic, specify treating conforming to Secretary of the Interior's standards, as applicable.</p>	<p>Simultaneously with a project application submittal.</p>	<p>Qualified architectural historian retained by the Project sponsor(s).</p>	<p>CDD STATUS COMPLETE: The historic resource evaluation, prepared by Preservation Architecture, dated March 10, 2016, concludes the commercial buildings were found not to be historically significant.</p>

<b>El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue</b>				
<b>Mitigation Measure</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p><b>Treatment in Accordance with the Secretary of the Interior's Standards.</b> Any future proposed project in the Plan Area that would affect previously recorded historic resources, or those identified as a result of site-specific surveys and evaluations, shall conform to the Secretary of the Interior's Standards for the Treatment of Historic Properties and Guidelines for Preserving, Rehabilitating, Restoring, and Reconstructing Historic Buildings (1995). The Standards require the preservation of character defining features which convey a building's historical significance, and offers guidance about appropriate and compatible alterations to such structures.</p>				
<p><b>Impact CUL-2: The proposed Specific Plan could impact currently unknown archaeological resources. (Potentially Significant)</b></p>				
<p><b>Mitigation Measure CUL-2a:</b> When specific projects are proposed that involve ground disturbing activity, a site-specific cultural resources study shall be performed by a qualified archaeologist or equivalent cultural resources professional that will include an updated records search, pedestrian survey of the project area, development of a historic context, sensitivity assessment for buried prehistoric and historic-period deposits, and preparation of a technical report that meets federal and state requirements. If historic or unique resources are identified and cannot be avoided, treatment plans will be developed in consultation with the City and Native American representatives to mitigate potential impacts to less than significant based on either the Secretary of the Interior's Standards described in Mitigation Measure CUL-1 (if the site is historic) or the provisions of Public Resources Code Section 21083.2 (if a unique archaeological site).</p>	<p>A qualified archeologist shall complete a site-specific cultural resources study.</p> <p>If resources are identified and cannot be avoided, treatment plans will be developed to mitigate impacts to less than significant, as specified.</p>	<p>Simultaneously with a project application submittal.</p>	<p>Qualified archaeologist retained by the project sponsor(s).</p>	<p>CDD STATUS COMPLETE: The cultural resource evaluation, prepared by Basin Research Associates, dated January 29, 2016, concludes that the proposed project will have no impact on cultural resources.</p>

<b>El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue</b>				
<b>Mitigation Measure</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p><b>Mitigation Measure CUL-2b:</b> Should any archaeological artifacts be found during construction, all construction activities within 50 feet shall immediately halt and the City must be notified. A qualified archaeologist shall inspect the findings within 24 hours of the discovery. If the resource is determined to be a historical resource or unique resource, the archaeologist shall prepare a plan to identify, record, report, evaluate, and recover the resources as necessary, which shall be implemented by the developer. Construction within the area of the find shall not recommence until impacts on the historical or unique archaeological resource are mitigated as described in Mitigation Measure CUL-2a above. Additionally, Public Resources Code Section 5097.993 stipulates that a project sponsor must inform project personnel that collection of any Native American artifact is prohibited by law.</p>	<p>If any archaeological artifacts are discovered during demolition/construction, all ground disturbing activity within 50 feet shall be halted immediately, and the City of Menlo Park Community Development Department shall be notified within 24 hours.</p> <p>A qualified archaeologist shall inspect any archaeological artifacts found during construction and if determined to be a resource shall prepare a plan meeting the specified standards which shall be implemented by the project sponsor(s).</p>	Ongoing during construction.	Qualified archaeologist retained by the project sponsor(s).	CDD
<b>Impact CUL-3: The proposed Specific Plan may adversely affect unidentifiable paleontological resources. (Potentially Significant)</b>				
<p><b>Mitigation Measure CUL-3:</b> Prior to the start of any subsurface excavations that would extend beyond previously disturbed soils, all construction forepersons and field supervisors shall receive training by a qualified professional paleontologist, as defined by the Society of Vertebrate Paleontology (SVP), who is experienced in teaching non-specialists, to ensure they can recognize fossil materials and will follow proper notification procedures in the event any are uncovered during construction. Procedures to be conveyed to workers include halting construction within 50 feet of any potential fossil find and notifying a qualified paleontologist, who will evaluate its significance. Training on paleontological resources will also be provided to all other construction workers, but may involve using a videotape of the initial training and/or written materials rather than in-person training by a paleontologist. If a fossil is determined to be significant and avoidance is not feasible, the paleontologist will develop and implement an excavation and</p>	<p>A qualified paleontologist shall conduct training for all construction personnel and field supervisors.</p> <p>If a fossil is determined to be significant and avoidance is not feasible, the paleontologist will develop and implement an excavation and salvage plan in accordance with SVP standards.</p>	Prior to issuance of grading or building permits that include subsurface excavations and ongoing through subsurface excavation.	Qualified archaeologist retained by the project sponsor(s).	CDD
<b>Impact CUL-4: Implementation of the Plan may cause disturbance of human remains including those interred outside of formal cemeteries. (Potentially Significant)</b>				
<p><b>Mitigation Measure CUL-4:</b> If human remains are discovered during construction, CEQA Guidelines 15064.5(e)(1) shall be followed, which is as follows:</p> <p>* In the event of the accidental discovery or recognition of any human remains in any location other than a dedicated cemetery, the following steps should be taken:</p>	<p>If human remains are discovered during any construction activities, all ground-disturbing activity within the site or any nearby area shall be halted immediately, and the County coroner must be contacted immediately and other specified procedures must be followed as applicable.</p>	On-going during construction	Qualified archeologist retained by the project sponsor(s)	CDD



**El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue**

Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
<p>1) There shall be no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent human remains until:</p> <p>a) The San Mateo County coroner must be contacted to determine that no investigation of the cause of death is required; and</p> <p>b) If the coroner determines the remains to be Native American:</p> <ol style="list-style-type: none"> <li>1. The coroner shall contact the Native American Heritage Commission within 24 hours;</li> <li>2. The Native American Heritage Commission shall identify the person or persons it believes to be the most likely descended from the deceased Native American;</li> <li>3. The most likely descendent may make recommendations to the landowner or the person responsible for the excavation work, for means of treating or disposing of, with appropriate dignity, the human remains and any associated grave goods as provided in Public Resources Code Section 5097.98; or</li> </ol> <p>2) Where the following conditions occur, the landowner or his authorized representative shall reburial the Native American human remains and associated grave goods with appropriate dignity on the property in a location not subject to further subsurface disturbance.</p> <p>a) The Native American Heritage Commission is unable to identify a most likely descendent or the most likely descendent failed to make a recommendation within 48 hours after being notified by the Commission.</p> <p>b) The descendant identified fails to make a recommendation; or</p> <p>c) The landowner or his authorized representative rejects the recommendation of the descendant, and the mediation by the Native American Heritage Commission fails to provide measures acceptable to the landowner.</p>				
<p><b>GREENHOUSE GASES AND CLIMATE CHANGE</b></p>				
<p><b><i>Impact GHG-2: The Specific Plan could conflict with applicable plans, policies or regulations of an agency with jurisdiction over the Specific Plan adopted for the purpose of reducing the emissions of GHGs. (Significant)</i></b></p>				

<b>El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue</b>				
<b>Mitigation Measure</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<b>Mitigation Measure GHG-2a:</b> All residential and/or mixed use developments of sufficient size to require LEED certification under the Specific Plan shall install one dedicated electric vehicle/plug-in hybrid electric vehicle recharging station for every 20 residential parking spaces provided. Per the Climate Action Plan the complying applicant could receive incentives, such as streamlined permit processing, fee discounts, or design templates.	Install one dedicated electric vehicle/plug-in hybrid electric vehicle recharging station for every 20 residential parking spaces	Simultaneous with project application submittal	Project sponsor(s)	CDD
<b>Mitigation Measure GHG-2b:</b> The City could implement a pilot program in the Specific Plan area to require mandatory commercial recycling, either at all buildings or, at a minimum, at newly constructed buildings. Such a program, identified in the AB 32 Scoping Plan and included in the City's Climate Action Plan (CAP) as a measure for future study, could reduce GHG emissions in the Plan area and, if successful, could be implemented citywide.	Consider feasibility of pilot program. If pilot or permanent program implemented, require commercial recycling in applicable projects	Consider feasibility of pilot program as outlined in CAP.  If adopted, simultaneous with project application submittal and ongoing.	Feasibility study: PW  If adopted: Project sponsors(s)	PW  PW
<b>HAZARDOUS MATERIALS</b>				
<b><i>Impact HAZ-1: Disturbance and release of contaminated soil during demolition and construction phases of the project, or transportation of excavated material, or contaminated groundwater could expose construction workers, the public, or the environment to adverse conditions related to hazardous materials handling. (Potentially Significant)</i></b>				

**El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue**

Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
<p><b>Mitigation Measure HAZ-1:</b> Prior to issuance of any building permit for sites where ground breaking activities would occur, all proposed development sites shall have a Phase I site assessment performed by a qualified environmental consulting firm in accordance with the industry required standard known as ASTM E 1527-05. The City may waive the requirement for a Phase I site assessment for sites under current and recent regulatory oversight with respect to hazardous materials contamination. If the Phase I assessment shows the potential for hazardous releases, then Phase II site assessments or other appropriate analyses shall be conducted to determine the extent of the contamination and the process for remediation. All proposed development in the Plan area where previous hazardous materials releases have occurred shall require remediation and cleanup to levels established by the overseeing regulatory agency (San Mateo County Environmental Health (SMCEH), Regional Water Quality Control Board (RWQCB) or Department of Toxic Substances Control (DTSC) appropriate for the proposed new use of the site. All proposed groundbreaking activities within areas of identified or suspected contamination shall be conducted according to a site specific health and safety plan, prepared by a licensed professional in accordance with Cal/OHSA regulations (contained in Title 8 of the California Code of Regulations) and approved by SMCEH prior to the commencement of groundbreaking.</p>	<p>Prepare a Phase I site assessment.</p> <p>If assessment shows potential for hazardous releases, then a Phase II site assessment shall be conducted.</p> <p>Remediation shall be conducted according to standards of overseeing regulatory agency where previous hazardous releases have occurred.</p> <p>Groundbreaking activities where there is identified or suspected contamination shall be conducted according to a site-specific health and safety plan.</p>	<p>Prior to issuance of any grading or building permit for sites with groundbreaking activity.</p>	<p>Qualified environmental consulting firm and licensed professionals hired by project sponsor(s)</p>	<p>CDD STATUS COMPLETE: An Environmental Site Assessment Phase 1 was prepared by AEI Consultants, dated March 7, 2019, no potential hazardous releases were identified and a Phase II was not required.</p>

El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue				
Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
<b>Impact HAZ-3: Hazardous materials used on any individual site during construction activities (i.e., fuels, lubricants, solvents) could be released to the environment through improper handling or storage. (Potentially Significant)</b>				
<i>Mitigation Measure HAZ-3:</i> All development and redevelopment shall require the use of construction Best Management Practices (BMPs) to control handling of hazardous materials during construction to minimize the potential negative effects from accidental release to groundwater and soils. For projects that disturb less than one acre, a list of BMPs to be implemented shall be part of building specifications and approved of by the City Building Department prior to issuance of a building permit.	Implement best management practices to reduce the release of hazardous materials during construction.	Prior to building permit issuance for sites disturbing less than one acre and ongoing during construction for all project sites	Project sponsor(s) and contractor(s)	CDD
NOISE				
<b>Impact NOI-1: Construction activities associated with implementation of the Specific Plan would result in substantial temporary or periodic increases in ambient noise levels in the Specific Plan area above levels existing without the Specific Plan and in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies. (Potentially Significant)</b>				
<i>Mitigation Measure NOI-1a:</i> Construction contractors for subsequent development projects within the Specific Plan area shall utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures, and acousticallyattenuating shields or shrouds, etc.) when within 400 feet of sensitive receptor locations. Prior to demolition, grading or building permit issuance, a construction noise control plan that identifies the best available noise control techniques to be implemented, shall be prepared by the construction contractor and submitted to the City for review and approval. The plan shall include, but not be limited to, the following noise control elements:  * Impact tools (e.g., jack hammers, pavement breakers, and rock drills) used for construction shall be hydraulically or electrically powered wherever possible to avoid noise associated with compressed air exhaust from pneumatically powered tools. However, where use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used; this muffler shall achieve lower noise levels from the exhaust by approximately 10 dBA. External jackets on the tools themselves shall be used where feasible in order to achieve a reduction of 5 dBA. Quieter procedures shall be used, such as drills rather than impact equipment, whenever feasible;	A construction noise control plan shall be prepared and submitted to the City for review. Implement noise control techniques to reduce ambient noise levels.	Prior to demolition, grading or building permit issuance Measures shown on plans, construction documents and ongoing through construction	Project sponsor(s) and contractor(s)	CDD

<b>El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue</b>				
<b>Mitigation Measure</b>	<b>Action</b>	<b>Timing</b>	<b>Implementing Party</b>	<b>Monitoring Party</b>
<p>* Stationary noise sources shall be located as far from adjacent receptors as possible and they shall be muffled and enclosed within temporary sheds, incorporate insulation barriers, or other measures to the extent feasible; and</p> <p>* When construction occurs near residents, affected parties within 400 feet of the construction area shall be notified of the construction schedule prior to demolition, grading or building permit issuance. Notices sent to residents shall include a project hotline where residents would be able to call and issue complaints. A Project Construction Complaint and Enforcement Manager shall be designated to receive complaints and notify the appropriate City staff of such complaints. Signs shall be posted at the construction site that include permitted construction days and hours, a day and evening contact number for the job site, and day and evening contact numbers, both for the construction contractor and City representative(s), in the event of problems.</p>				
<p><i>Mitigation Measure NOI-1c:</i> The City shall condition approval of projects near receptors sensitive to construction noise, such as residences and schools, such that, in the event of a justified complaint regarding construction noise, the City would have the ability to require changes in the construction control noise plan to address complaints.</p>	<p>Condition projects such that if justified complaints from adjacent sensitive receptors are received, City may require changes in construction noise control plan.</p>	<p>Condition shown on plans, construction documents and specifications. When justified complaint received by City.</p>	<p>Project sponsor(s) and contractor(s) for revisions to construction noise control plan.</p>	<p>CDD</p>
<p><b><i>Impact NOI-3: The Specific Plan would introduce sensitive receptors to a noise environment with noise levels in excess of standards considered acceptable under the City of Menlo Park Municipal Code. (Potentially Significant)</i></b></p>				
<p><i>Mitigation Measure NOI-3:</i> Interior noise exposure within homes proposed for the Specific Plan area shall be assessed by a qualified acoustical engineer to determine if sound rated walls and windows would be required to meet the Title 24 interior noise level standard of 45 dBA, Ldn. The results of each study shall be submitted to the City showing conceptual window and wall assemblies with Sound Transmission Class (STC) ratings necessary to achieve the noise reductions for the project to satisfy the interior noise criteria within the noise environment of the Plan area.</p>	<p>Interior noise exposure assessed by qualified acoustical engineer and results submitted to City showing conceptual window and wall assemblies necessary to meet City standards.</p>	<p>Simultaneous with submittal for a building permit.</p>	<p>Project sponsors(s) and contractor(s)</p>	<p>CDD</p>



El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue				
Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
TRANSPORTATION, CIRCULATION AND PARKING				
<b>Impact TR-1: Traffic from future development in the Plan area would adversely affect operation of area intersections. (Significant)</b>				
<i>Mitigation Measures TR-1a through TR-1d: (see EIR for details)</i>	Payment of fair share funding.	Prior to building permit issuance.	Project sponsor(s)	PW/CDD
<b>Impact TR-2: Traffic from future development in the Plan area would adversely affect operation of local roadway segments. (Significant)</b>				
<p><i>Mitigation Measure TR-2: New developments within the Specific Plan area, regardless of the amount of new traffic they would generate, are required to have in-place a City-approved Transportation Demand Management (TDM) program prior to project occupancy to mitigate impacts on roadway segments and intersections. TDM programs could include the following measures for site users (taken from the C/CAG CMP), as applicable:</i></p> <ul style="list-style-type: none"> <li>* Commute alternative information;</li> <li>* Bicycle storage facilities;</li> <li>* Showers and changing rooms;</li> <li>* Pedestrian and bicycle subsidies;</li> <li>* Operating dedicated shuttle service (or buying into a shuttle consortium);</li> <li>* Subsidizing transit tickets;</li> <li>* Preferential parking for carpoolers;</li> <li>* Provide child care services and convenience shopping within new developments;</li> <li>* Van pool programs;</li> <li>* Guaranteed ride home program for those who use alternative modes;</li> <li>* Parking cashout programs and discounts for persons who carpool, vanpool, bicycle or use public transit;</li> <li>* Imposing charges for parking rather than providing free parking;</li> <li>* Providing shuttles for customers and visitors; and/or</li> <li>* Car share programs.</li> </ul>	Develop a Transportation Demand Management program.	Submit draft TDM program with building permit. City approval required before permit issuance. Implementation prior to project occupancy.	Project sponsor(s)	PW/CDD

El Camino Real/Downtown Mitigation Monitoring and Reporting Program - 706 Santa Cruz Avenue				
Mitigation Measure	Action	Timing	Implementing Party	Monitoring Party
<b>Impact TR-7: Cumulative development, along with development in the Plan area, would adversely affect operation of local intersections. (Significant)</b>				
Mitigation Measures TR-7a through TR-7n: (see EIR for details)	Payment of fair share funding. The fee is calculated at \$379.40 per PM peak hour vehicle trip. The supplemental TIF is updated annually.	Prior to building permit issuance.	Project sponsor(s)	PW/CDD
<b>Impact TR-8: Cumulative development, along with development in the Plan area would adversely affect operation of local roadway segments. (Significant)</b>				
Mitigation Measure TR-8: Implement TR-2 (TDM Program).	See Mitigation Measure TR-2.			



# Transportation Demand Management Plan

## *Transportation Action Plan*



# 706-716 Santa Cruz Ave., Menlo Park

## Transportation Demand Management Plan (Transportation Action Plan)



*Prepared for:*

**706-716 Santa Cruz Ave., LLC**

*Prepared by:*



(408) 420-2411

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**ATTACHMENTS**

- List of Nearby Amenities – 0.25 miles or less from 706-716 Santa Cruz Ave (Personal services, restaurants, coffee, retail/sundry, banking, etc.)
- Local Transit Maps (SamTrans Routes)

**APPENDIX A**

Guaranteed Ride Home Program Materials  
Sample Employer Resources and Incentives

**TDM SPECIALISTS QUALIFICATIONS**



## EXECUTIVE SUMMARY

Conditions of approval for the 706-716 project include vehicle trip reduction for all net new peak-hour trips generated by the proposed project. Traffic congestion, air pollution, and inadequate parking are critical concerns for the City of Menlo Park. Traffic congestion intensifies demand on City fiscal resources for roadway construction and maintenance and increases lost time for residents and commuters. The transportation sector produces more than 50 percent of the Bay Area's air pollution, and more than 40 percent of greenhouse gas emissions.<sup>1</sup>

TDM Specialists, Inc. has prepared a Transportation Demand Management (TDM) Plan, on behalf of the applicant, for their proposed Menlo Park mixed-use development at 706-716 Santa Cruz Avenue. The design of the 706-716 Santa Cruz Avenue project meets commute-sustainable standards by incorporating select TDM elements (see list on page ii).

*The U.S. Green Building Council (USGBC) encourages and accelerates global adoption of sustainable green building and development practices through the creation and implementation of universally understood and accepted tools and performance criteria. [www.usgbc.org](http://www.usgbc.org)*

Other contributing and complementary sustainable building efforts include applicable portions of the U.S. Green Building Council's (USGBC) Leadership in Energy and Environmental Design (LEED), and California's Green Building standards. The applicant has committed to building a LEED Silver project.

### California Green Building Standards

The 2010 California Green Building Standards Code is Part 11 of the California Building Standards Code in Title 24 of the California Code of Regulations. Part 11 is also known as the CalGreen Code.

This green development approach reduces parking demand, vehicle trips, air pollution and traffic congestion, and contributes to successful greenhouse gas and carbon footprint reductions for long-term operations. Implementation and monitoring requirements stemming from AB 32 and SB 375 will require property owners,

developers, and employers to reduce greenhouse gas emissions. By implementing the TDM Plan today and reducing emissions now, this Menlo Park project will be a contributor in the mitigation process.

This TDM Plan addresses alternatives to on-site parking needs, as well as employee and resident commuter activities that reduce the number of trips spent driving alone. In addition, this plan supports the alternative transportation mode-use goals that address both traffic and air quality concerns in the City of Menlo Park. TDM measures specifically developed for the 706-716 Santa Cruz Avenue project include a variety of infrastructure and incentive-based measures such as carpooling, transit riding, bicycling, walking and telecommuting.

<sup>1</sup> Bay Area Air Quality Management District, Aaron Richardson, Public Information Officer

The project's trip reduction activities and transportation mode-use rate will be monitored annually, with the first employee and resident commute survey to be conducted one year after full occupancy of the project. An alternative transportation mode-use survey report will be submitted to the City's Director of Planning following the completion of the annual employee commuter survey.

The measures and elements contained in this plan are consistent with other well-performing TDM plans and commute programs in the Greater San Francisco Bay Area and are estimated to reduce all net new vehicle trips. A summary list of proposed commercial TDM measures for the office component includes:

### **TDM Infrastructure and Physical Measures**

- Community connectivity – pedestrian and transit-oriented design (LEED standard)
- Transit and shuttle proximity within walking distance
- Pedestrian connections
- Bicycle parking – long-term and short-term (LEED standard)
- Carpool and fuel-efficient parking spaces (LEED standard)
- Transportation commuter kiosk
- On-site project amenities (e.g., café, gym, vending)
- Nearby amenities (e.g., café, retail, restaurants, ATM/banking)

### **Commercial Programmatic TDM Measures**

- TDM tenant performance lease language
- Tenant/employer commute program training (applicant-provided)
- Commute Coordinator (assistance and outreach)
- Employee commute flier
- Kick-off commuter campaign (at 50 percent occupancy)
- Promotional programs (Bike to Work Day, Earth Day, Annual Transportation Spare the Air Fair), e-newsletters, etc.
- Carpool matching services and resources
- Bicycle route mapping and resources
- Transit trip planning and resources
- Tenant-driven TDM measures – **required per lease**
  - Transit and vanpool subsidies made available to all employees
  - Participation in the annual commute survey
  - Emergency Ride Home Program

- Tenant-driven TDM measures – **strongly encouraged**
  - SamTrans Way2Go Pass or Caltrain Go Pass
  - Pre-tax options
  - Employee commute website portal
  - Commute allowances
  - Carpool/vanpool incentive program
  - Flextime/off-peak commuting
  - Teleworking/telecommuting
  - Compressed work week
  - Employee Commute Coordinator

**Commercial TDM Commitment, Monitoring and Reporting**

- City/County Association of Governments peak-hour assessment
- Annual driveway trip hose counts
- Conduct annual five-day employee commute survey
- Prepare annual commute summary report
- No expiration of TDM Plan or programs

**Residential TDM measures:**

- Bicycle amenities and parking
- Access to transportation commuter kiosk
- Participation in on-site commuter promotional marketing
- Participation in on-site commuter events and fairs
- Participation in annual commute survey
- Resident electronic transportation resource flier
- Resident commuter resource welcome packet
- Resident free trial transit passes (SamTrans or Caltrain)

The project development and its TDM consultant prepared this TDM Plan, also referenced as a Transportation Action Plan, using guidelines and tools consistent with Commute.org’s employer and commuter programs, and with the City/County Association of Governments (C/CAG) San Mateo County Congestion Management Program. Over the past decade, the TDM consultant has worked with Commute.org staff and understood the applicable TDM measures that apply to this development project.

<p>Work with the Alliance to develop/ implement a Transportation Action Plan.</p>	<p>Ten peak hour trips will be credited.</p>	<p>This is based on staff’s best estimate.</p>
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Residential TDM details are provided in Section 18 (page 34).

## 1.0 INTRODUCTION AND PURPOSE

The comprehensive plan of commute options and on-site measures identified in this report are essential to realizing the trip reduction benefits of the project. These factors will provide the momentum to achieve desired trip reduction needs for the project.

TDM is a combination of services, incentives, facilities and actions that reduce single occupant vehicle (SOV) trips to help relieve traffic congestion, parking demand and air pollution. The TDM measures outlined herein are anticipated to result in a reduction in commuter and day-time trips.

The following is a summary of current public policy goals related to sustainability and congestion management.

### **San Francisco Bay Area Regional Commuter Benefit**

Recently approved Senate Bill (SB) 1339 requires employers with 50 or more full-time employees to have the flexibility to offer their employees one or more of the following:

- The option to pay for their transit, vanpooling or bicycling expenses with pre-tax dollars, as allowed by federal law (scheduled to be \$265 per month for transit or vanpool)
- A transit or vanpool subsidy of at least \$75 per month
- A free shuttle or vanpool operated by or for the employer
- An alternative program that provides similar benefits in reducing single-occupant vehicles

In Bay Area cities where these policies are already in place, most employers have chosen the pre-tax option. The Regional Commuter Benefit program offers substantial economic benefits to employers and employees. Employers can reduce payroll taxes (9-10 percent of subject wages), and employees can lower their commute costs by nearly 40 percent.

### **State Greenhouse Gas (GHG) Guideline and Policy Setting**

California is rated 12<sup>th</sup> to 16<sup>th</sup> as the largest emitter of carbon dioxide (CO<sub>2</sub>) and is responsible for approximately two percent of the world's CO<sub>2</sub> emissions. Below are summaries of the most pertinent State bills that address efforts to reduce GHG emissions.

Assembly Bill 32 California Climate Solutions Act of 2006 - requires that Statewide GHG emissions be reduced to 1990 levels by the year 2020. This first-in-the-world comprehensive program of regulatory and market mechanisms are designed to achieve real, quantifiable, and cost-effective reductions of GHG. AB 32 establishes the California Air Resources Board as the agency responsible for monitoring and reducing GHG emissions.

Senate Bill 375 - establishes improved land use and transportation policy supporting AB 32, providing a means for achieving the AB 32 goals for cars and light trucks through land use changes. This legislation created potentially revolutionary changes in California's regional planning processes for housing and transportation by mandating the creation of sustainable regional growth plans. These plans are expected to double the GHG emission reduction targets that local governments must meet through land use planning.

#### Climate Change Facts

- From 1990 to 2009, greenhouse gas emissions in the United States have grown by about 0.04% per year. Of U.S. greenhouse gas emissions, 87% are related to energy consumption. The U.S. accounts for about 20% of the world's total energy-related CO<sub>2</sub> emissions.
- Approximately 25% of California's greenhouse gas emissions are attributable to electricity generation, while 38% are attributed to the transportation sector.
- A solo driver, commuting by car 20 miles round-trip daily that switches to public transportation, can reduce his/her annual CO<sub>2</sub> emissions by 2.4 tons per year. This is equivalent to a 10% reduction in all greenhouse gases produced by a typical two-adult, two-car family.
- By eliminating one car and taking public transportation for all trips instead of driving, a savings of up to 30% of CO<sub>2</sub> emissions can be realized.

Source: VTA Public Transportation, VTA Combating Climate Change, January 2012

## 2.0 TRANSPORTATION DEMAND MANAGEMENT DEFINITION

The basic premise of TDM is the effective utilization of existing transportation resources. The City of Menlo Park, as is typical of other urban areas in the United States, has millions of dollars invested in roadway and public transit infrastructure. The goal of TDM is to take advantage of these major capital investments efficiently and economically. The following are basic goals that can be achieved through effective utilization of TDM measures:

- *Reduce parking demand by converting SOV trips to an alternate mode of transportation (e.g., transit, carpool or vanpool, bicycling or walking).*
- *Shift travel to less congested routes by providing traveler information systems that warn motorists about delays or alternative routes.*
- *Support other technological solutions (e.g., compressed natural gas, electric/hybrid vehicles, or other zero emission vehicles).*
- *Eliminate or shift trips from peak periods (e.g., flexible schedules, compressed work weeks, or telecommuting).*

Current economics and limited resources affect the ability to build and maintain more roads or parking structures. This reality necessitates better utilization of the existing transportation infrastructure (like adding a second shift at an existing manufacturing plant). To that end, TDM measures support the transition to a greater use of existing alternative transportation options.

### **3.0 PROJECT DESCRIPTION**

The 706-716 Santa Cruz Avenue project is a proposed mixed-use building consisting of ground floor retail space, second floor office space with ground floor entrance lobby, and four residential condominiums and a small office suite on the third floor. The third-floor residences take the form of roof top lofts commonly found in metropolitan areas. Each unit has a generous private terrace with a common terrace and rooftop garden that provides access to the stairs and elevator. On-grade parking will be preserved from the existing conditions with additional proposed below-grade parking. The project provides 69 parking stalls. The garage entrance is located off on Chestnut Lane.

The 706-716 Santa Cruz Avenue project will utilize urban design features, is near mass transit, shopping and recreation, and incorporates air quality features such as an electric charging station and bicycle storage. A project location map is shown on page 4.





The project will also include complementary sustainable building design as described in the U.S. Green Building Council's (USGBC) Leadership in Energy and Environmental Design (LEED), and California's Green Building standards. The applicant has committed to building a LEED Silver project.

**Building will be Silver LEED and may include the following measures:**

- Community Connectivity – Construct or renovate a building on a site that is located on a previously developed site, is within a ½-mile of residential area with average density of 10 units per acre net, is within a ½-mile of at least 10 basic services, and has pedestrian access between the building and the services.
- Public Transportation Access – Locate the project within a ½-mile walking distance (measured from a main building entrance) of an existing or planned and funded commuter rail, light rail or subway station.
- Bicycle Storage – Provide secure bicycle racks and/or storage within 200 yards of a building entrance for 3% or more of all building users (calculated on average for the year).
- Parking Capacity – Provide preferred parking for carpools or vanpools, marked as such, for 3% of total parking spaces.

***Sustainable Site LEED Credits:***

***Credit 2: Development Density and Community Connectivity (5 Points)***

***Credit 4.1: Public Transportation Access (6 Points)***

***Credit 4.2: Bicycle Storage (1/2 Point)***

***Credit 4.4: Parking Capacity (2 Points)***

This TDM Plan is designed to address employee and resident vehicle trips associated with a mixed-use project and contains the appropriate measures and elements that are consistent with other regional commute programs.

A comprehensive array of alternative transportation mode-use strategies is presented in the remaining report as outlined in four sections:

- I. TDM Infrastructure and Physical Measures
- II. Programmatic TDM Measures
- III. TDM Commitment, Monitoring and Reporting
- IV. Residential TDM Measures

The remainder of this TDM Plan defines the measures proposed specifically for the 706-716 Santa Cruz Avenue project.



## SECTION I – TDM INFRASTRUCTURE AND PHYSICAL MEASURES

The following physical infrastructure measures are designed to support alternative transportation commuters. These measures are TDM components that will be installed or built during the construction of the project. A TDM Site Map is shown on page 17.

### 4.0 COMMUNITY CONNECTIVITY

The project will become a pedestrian-friendly and transit-oriented mixed-use project that embraces Menlo Park's goals and policies. Some of the pedestrian and transit-oriented design features include orienting the building toward transit stops and tying into adjacent bicycle and pedestrian circulation facilities. This type of connectivity provides a high-level of pedestrian, bicycle and transit access for the project and meets the criteria for *LEED Credit 2: Development Density and Community Connectivity*.

### 5.0 TRANSIT PROXIMITY

The 706-716 Santa Cruz Avenue project will be located within walking distance (measured from a main building entrance) of the existing Menlo Park Caltrain commuter rail station. This station meets the LEED criteria, and is located approximately 0.20 mile (a four-minute walk) from the project. There are eight SamTrans transit resources within the same distance.

#### A5.103 Site Selection

**CalGreen Section: A5.103.1 Community Connectivity.** Where feasible, locate project on a previously developed site within a ½-mile radius of at least 10 basic services, readily accessible by pedestrians, including but not limited to, one each of a bank, place of worship, convenience grocery, daycare, cleaners, fire station, barber shop, hardware store, laundry, library, medical clinic, dental clinic, senior care facility, park, pharmacy, post office, restaurant (two may be counted), school, supermarket, theater, community center, fitness center, museum or farmers market.

#### SS Credit 2: Development Density and Community Connectivity

##### Intent

To channel development to urban areas with existing infrastructure, protect greenfields, and preserve habitat and natural resources.

##### Requirements – Community Connectivity

Construct or renovate a building on a site that is located on a previously developed site, is within a ½-mile of residential area with an average density of 10 units per acre net, is within ½-mile of at least 10 basic services, and has pedestrian access between the building and the services.

An advantage for this project is its very near proximity to local SamTrans bus transit services. In addition, the free local Menlo Park Midday Shuttle is located within easy walking distance from the site.

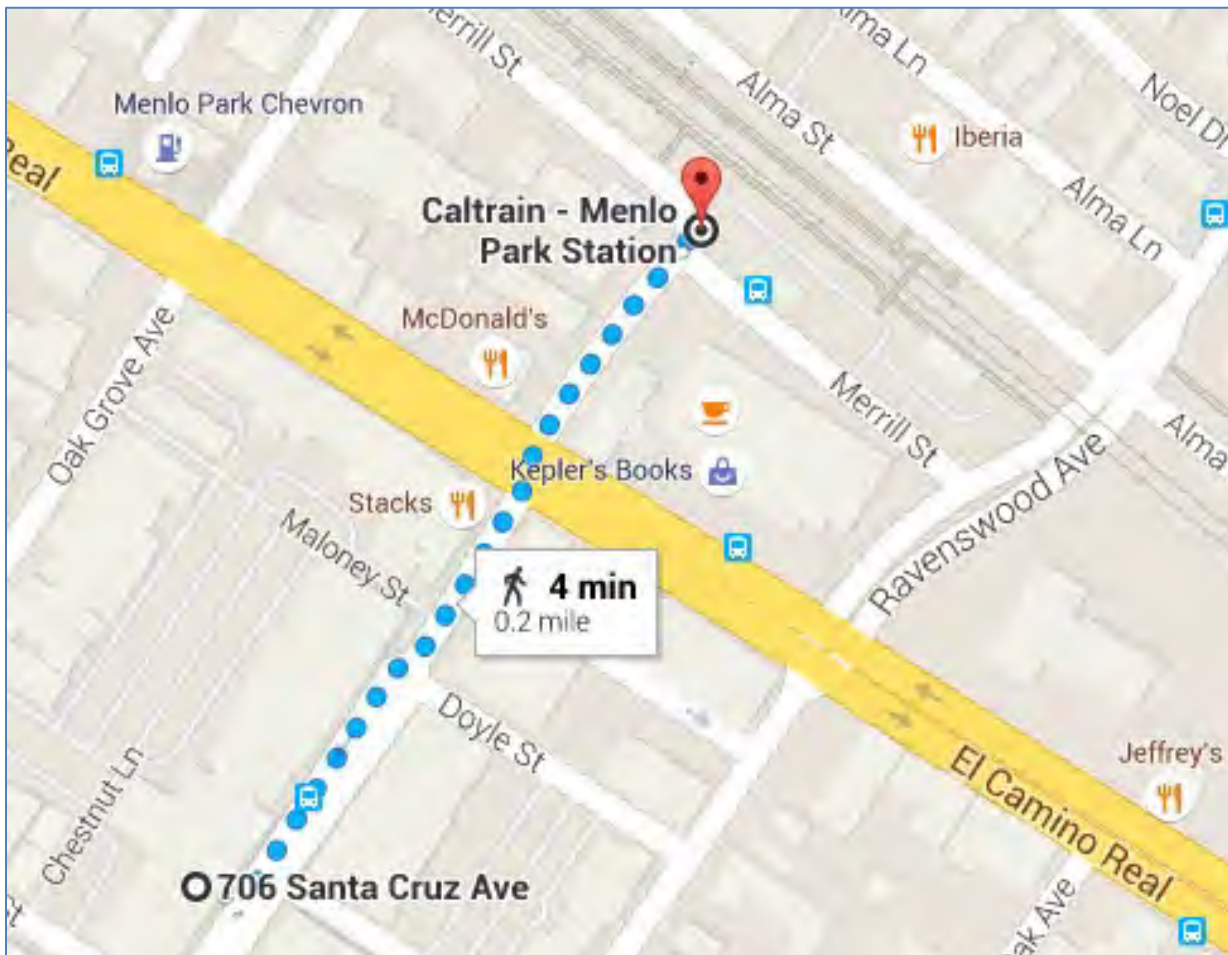
Transit services total more than 89 trips per day, providing good transit connectivity for future employees and residents at the site. A transit access table, shown on page 8, identifies the number of transit trips provided for occupants of the project. This high level of transit access meets the criteria for *LEED Credit 4.1: Public Transportation Access*.

**SS Credit 4.1: Alternative Transportation—Public Transportation Access**

**Intent**  
To reduce pollution and land development impacts from automobile use.

**Requirements – Transit Proximity**  
Locate the project within ½-mile walking distance (measured from a main building entrance) of an existing or planned and funded commuter rail, light rail, bus or subway station.

**Walking Route Map to Transit**



A SamTrans transit map and the Menlo Park Midday Shuttle map are shown on page 9. Other transit maps for the local area are provided as attachments.

## 706-716 Santa Cruz Avenue Transit Resources

Route #	Span of Service	# of Trips/Weekday	Communities Served
82* Samtrans	5 Days/Week 8:00 a.m.	3	Bay/Marsh, Bay/Harmon, Coleman/Menlo Oaks, Santa Monica/San Andreas, Merrill/Santa Cruz, <b>Santa Cruz/Curtis</b> , Hillview School, Laurel/Glenwood, Middlefield/ Santa Margarita
83* Samtrans	5 Days/Week 7:53 a.m.	6	Bay/Ringwood, Bay/Menlo Oaks, Durham/Laurel, Marmona/Robin, Merrill/Santa Cruz, <b>Santa Cruz/Curtis</b> , Hillview School, Laurel/Glenwood
84* Samtrans	5 Days/Week 8:03 a.m.	3	Encinal/Middlefield, Middlefield/Lane, Merrill/Santa Cruz, <b>Santa Cruz/Curtis</b> , Hillview School, Laurel/Glenwood, Middlefield/Santa Margarita
86* Samtrans	5 Days/Week 7:28 a.m.** - 3:29 p.m.	4	Indian Crossing, La Mesa/Alpine, Sharon Park/Sharon, Santa Cruz/Merrill, <b>Santa Cruz/Curtis</b> , Menlo Atherton High
286 Samtrans	5 Days/Week 7:31 a.m. - 5:20 p.m.	8	Monte Rosa/Eastridge, Menlo Park Caltrain, <b>Santa Cruz/Curtis</b> , Ringwood/Arlington
Menlo Park Caltrain Station	7 Days/Week 5:04 a.m. - 12:56 a.m.	65	<i>Other Transit Connections:</i> Marsh Road and Willow Road Shuttles Stanford Marguerite BOH
<b>Total VTA Bus Trips/Weekday</b>		<b>89</b>	

\* School-day Only

\*\* Mondays, Tuesdays, Thursdays, and Fridays

All buses and trains are lift equipped for handicapped, elderly, or those in need.

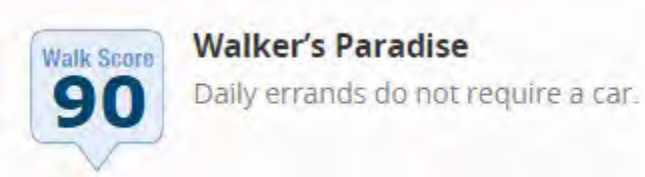






## 6.0 PEDESTRIAN AMENITIES

Safe, convenient and well-lit pedestrian paths surround the project, and will provide the most direct route to the nearest shuttle or transit connection from the project.



This score means that most errands can be accomplished on foot. The creation of a pedestrian-oriented environment ensures access between public areas and private development, while strengthening pedestrian and bicycle connections.

Lighting, landscaping, and building orientation will be designed to enhance pedestrian safety. According to WalkScore.com, the project is a “Walker’s Paradise” site, scoring 90 out of 100. This

Pedestrian continuity will also be enhanced by:

- Locating most of the parking below grade.
- Recessing door and window features of the building to further the walkable area of the sidewalks.
- Incorporating landscaped areas to serve visitors and passersby at the entry to the building.
- Installing planters on the property adjacent to the public right-of-way.



## 7.0 BICYCLE FACILITIES AND CONNECTIONS

The project is surrounded by bicycle connections in the City of Menlo Park, including bicycle connections to regional bicycle facilities along Valparaiso Avenue and Wallea Drive. Although the City of Menlo Park supports a range of excellent bicycle facilities, some sections of El Camino Real are rated for extreme caution. A Bicycle Map of Menlo Park is provided on page 12. A copy of the Mid-Peninsula Bicycle Map is provided on page 13.

### Bicycle Storage – Long-Term and Short-Term

A total of 22 Class I and Class II secure bicycle parking facilities will be provided on-site, at no charge for bicycle commuters.

### Long-Term Bicycle Parking

Ten Class I (long-term) secure and covered bicycle parking may include bicycle lockers or a bicycle room. Sample photos of Class I bicycle parking options are shown below.





**Short-Term Bicycle Parking**

Below are examples of Class II (short-term) bicycle racks. The 12 Class II secure bicycle racks will be “U racks” or equivalent and must secure the frame and both wheels. Three Class II racks will be located near the building entrance within constant visual range, unless it is demonstrated that they create a public hazard or locating them there is otherwise infeasible. If space is unavailable near building entrances, the racks must be designed so that the lock is protected from physical assault.



*Partial SS Credit 4.2: Alternative Transportation—Bicycle Storage*

**Intent**  
To reduce pollution and land development impacts from automobile use.

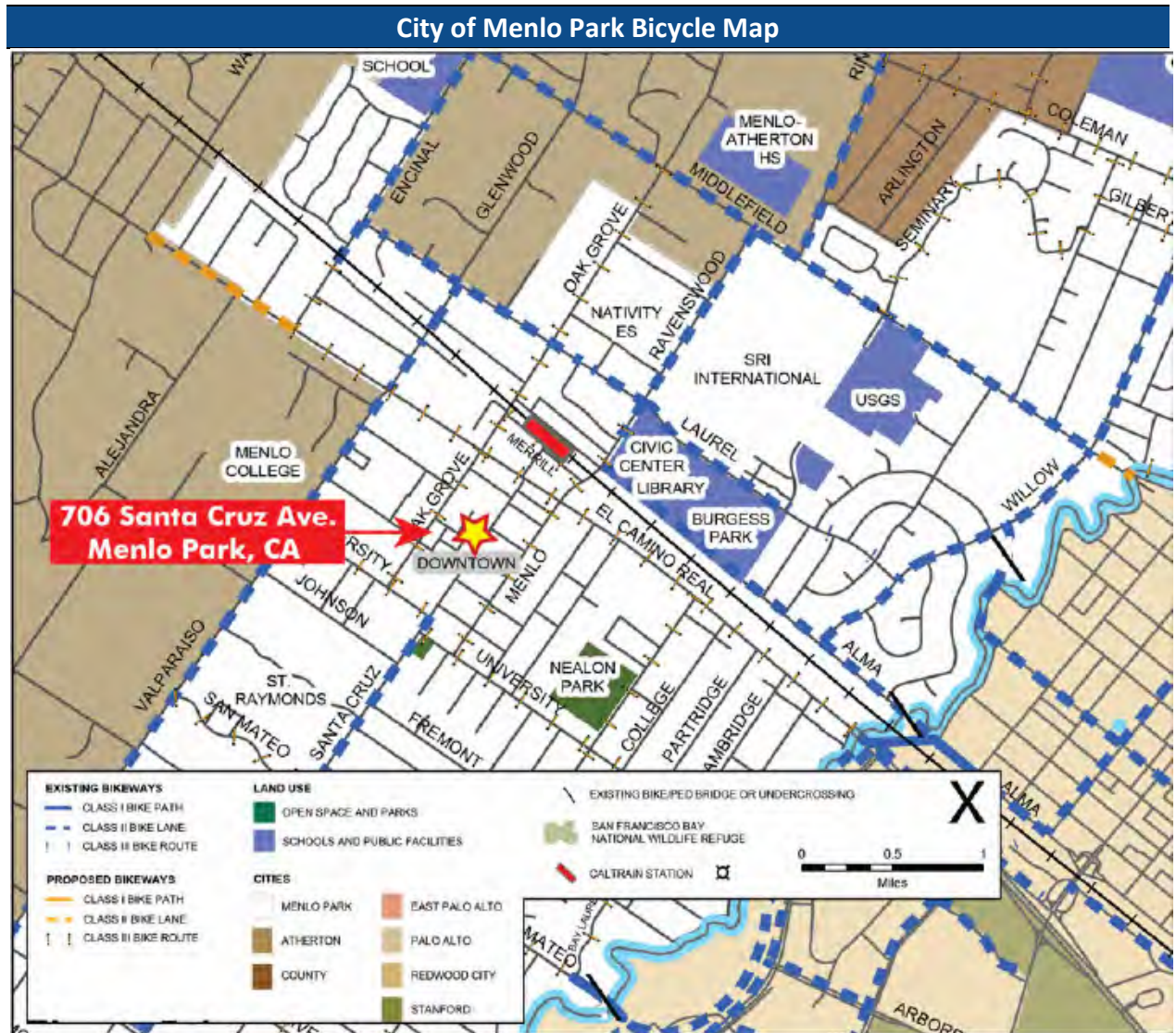
**Requirements**

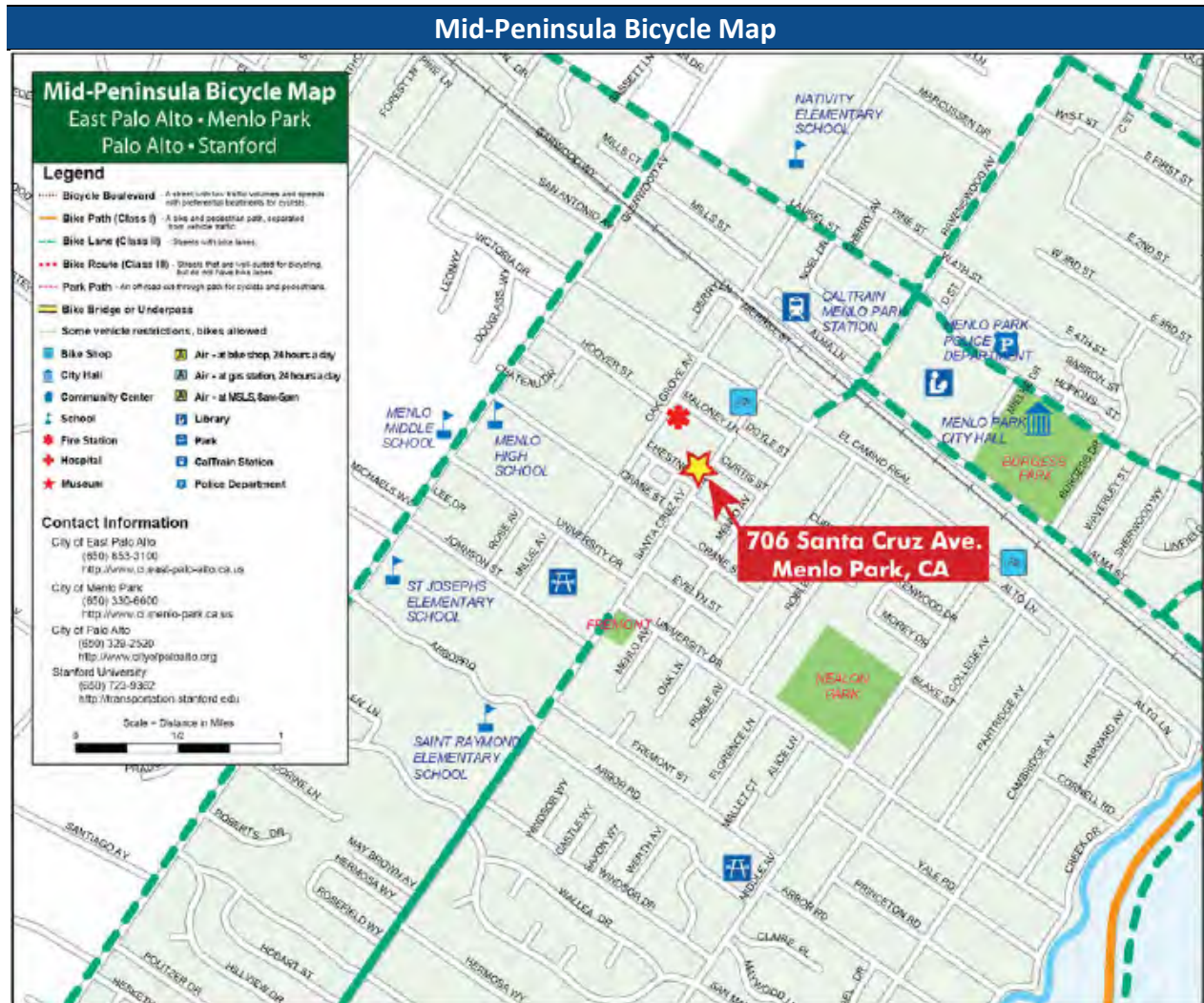
- Provide secure bicycle racks and/or storage within 200 yards of a building entrance for 3% or more of all building users.

These bicycle measure clarify the applicable C/CAG peak-hour mitigation measures designed for the project.

<p>Secure bicycle storage</p>	<p>One peak hour trip will be credited for every 3 new bike lockers/racks installed and maintained. Lockers/racks must be installed within 100 feet of the building.</p>	<p>Experience has shown that bicycle commuters will average using this mode one-third of the time, especially during warmer summer months.</p>
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## 8.0 PARKING MANAGEMENT

The willingness to participate in employee ridesharing and the measurable level of actual participation, is directly linked to parking convenience, availability and parking cost.

### Carpool and Clean-fuel Vehicle Designations

Carpool and clean-fuel vehicle parking spaces are an excellent incentive that sends a clear message to employees that alternative transportation is not only important, but also provides benefits to those who use it.

Upon completion and implementation of this TDM Plan, and in accordance with LEED standards, there will be two designated carpool or clean-fuel vehicle parking spaces (approximately three percent of total allocated parking spaces).

If carpool parking spaces become occupied by non-carpoolers, these parking spaces may require policy development, employee registration, and permits.



#### SS Credit 4.4: Alternative Transportation—Parking Capacity

##### Intent

To reduce pollution and land development impacts from automobile use

##### Requirements

- Size parking capacity to meet, but not exceed the minimum local zoning requirements.
- Provide preferred parking for carpools or vanpools for 3% of the total parking spaces.

The carpool parking measure meets the *Sustainable Site LEED Credit 4.4: Alternative Transportation: Parking Capacity*.

### Preferential Parking Space Placement

One effective means of encouraging employees to carpool and/or use a clean-fuel vehicle is to reserve the preferred parking spaces (premium, convenient locations close to buildings in the shade or within 100 feet of building entrances) for the exclusive use of carpool, vanpools and clean-fuel vehicles.



The applicant will be responsible for striping the parking space pavement and providing appropriate signage for preferential parking at the site.





### 9.0 TRANSPORTATION AND COMMUTE INFORMATION KIOSK

An information board or kiosk will be in the building in a common gathering area (e.g., lobby employee entrance, break or lunch room). The kiosk will contain transportation information such as transit schedules, SamTrans, VTA, Caltrain, shuttle schedules, bike maps and ride-matching materials. Information will be updated periodically by the project Commute Coordinator. The kiosk may be wall-mounted or freestanding.



C/CAG allows peak-hour trip credits to apply for this TDM measure.

<p>Operation of a commute assistance center, offering on site, one stop shopping for transit and commute alternatives information, preferably staffed with a live person to assist building tenants with trip planning.</p>	<p>One peak hour trip will be credited for each feature added to the information center; and an additional one peak hour trip will be credited for each hour the center is staffed with a live person, up to 20 trips per each 200 tenants. Possible features may include:                  Transit information                  brochure rack                  Computer kiosk connected</p>	<p>This is based on staff's best estimate. Short of there being major disincentives to driving, having an on site TDM program offering commute assistance is fundamental to an effective TDM program.</p>
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### 10.0 PROJECT AMENITIES

Amenities provide employees with a full-service work environment. Eliminating or reducing the need for an automobile to make midday trips increases non-drive-alone rates. Many times, employees perceive their dependence upon the drive-alone mode because of errands and activities they must carry out in different locations. By reducing this dependence through the provision of services and facilities at the work site, an increase in alternative mode usage for commute-based trips should be realized. A list of on-site amenities for the project may include:

#### On-site Amenities

- Secure bicycle parking and racks
- Carpool and vanpool parking
- Transportation and commute kiosk
- Wireless Internet (Wi-Fi) access
- Restaurant, café, or vending kiosk

#### Nearby Amenities

- Restaurants, cafes/delis, coffee
- Shipping and postal services
- Daycare and preschool
- Car sharing opportunities
- Retail, grocery, personal services and gifts
- Fitness, entertainment, health and beauty
- Banks and ATMs

A more detailed list of nearby amenities and personal services within a ¼-mile walk from the project site is provided as an attachment.

While some amenities may be on-site at the development project, a significant number of amenities will be very nearby. C/CAG has allowed peak-hour vehicle trip credits to be prorated for this TDM measure based on amenity locations that are off-site but still nearby.

Provision of on-site amenities/accommodations that encourage people to stay on site during the workday, making it easier for workers to leave their automobiles at home.	Five peak hour trips will be credited for each feature added to the job site. Possible features may include: banking grocery shopping clothes cleaning exercise facilities child care center	This is based on staff's best estimate.
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## SECTION II – PROGRAMMATIC TDM MEASURES

The following programmatic measures are designed to enhance the success of the TDM program and, upon implementation, create the “Commute Program.” These measures are TDM components that will be required of tenants and employers as part of their occupancy agreements, and they represent various promotions and outreach activities of the project’s Commute Program.

### 11.0 TENANT SERVICES, MANAGEMENT AND COMMUTER OUTREACH

An active Commute Coordinator, cooperative property management, and involved tenant-employers will generate positive impacts toward the success of the TDM goals and elements that are implemented. Commute programs and benefits must be presented to the employees in a comprehensive and proactive manner along with other employee programs. This can be done via participation in, and support of, employee orientation forums or transportation fairs, transportation kiosk postings, employee newsletters, management bulletins, e-mails, and related activities.

#### Tenant Performance and Lease Language – TDM Requirements

For all commercial tenants, the applicant will draft lease language or side agreements that require the identification of a designated employer contact responsible for compliance and implementation of the TDM program (including offering programs such as transit subsidies to all employees, annual survey and reporting, and registration in the carpool parking program).

The applicant will require a tenant to provide one point of contact for implementation of this plan. The tenant/employer designated contact will coordinate closely with the project Commute Coordinator; maintain on-site TDM programs, employee education and marketing; administer the annual surveys; and provide information continuity for the building owner/landlord and the City of Menlo Park. Features identified in the lease will also include the following TDM components:

- Tenant-driven TDM measures – **required per lease**
  - Transit and vanpool subsidies made available to all employees
  - Participation in the annual employee commute survey
  - Emergency Ride Home program for employee commuters
- Tenant-driven TDM measures – **strongly encouraged**
  - SamTrans Way2Go pass or Caltrain Go Pass
  - Pre-tax options
  - Employee commute website portal

- Commute allowances and/or subsidies
- Carpool/vanpool incentive program
- Flextime/off-peak commuting
- Teleworking/telecommuting
- Employee Commute Coordinator

The lease agreement language may also identify the commercial tenant's share of potential penalties for failure to achieve an acceptable alternative mode-use rate, failure to participate in the annual employee commute survey, or failure to submit the annual report. The building management will be responsible for project-wide tenant performance.

The lease language may be worded as follows:

*Tenant hereby agrees to designate one of its employees to act as a liaison with the Landlord to facilitate and coordinate such programs as may be required by governmental agencies to reduce the traffic generated by the 706-716 Santa Cruz Avenue project, as required by the City of Menlo Park, as part of conditions of approval and to encourage the use of public transportation and ridesharing, including providing transit subsidies for all employees (or SamTrans Way2Go annual pass, Caltrain GoPass, etcetera), implementing an emergency ride home program, and participating in the annual employee survey.*

### **Initial Tenant/Employer Commute Program Training**

As needed, the applicant or property management will provide TDM and commute program training and commute program start-up assistance for their tenants. A TDM resource representative will provide tenant training, planning assistance, and annual monitoring and survey reporting.

The overarching goals of this support function are to reduce commute trips for employees, formalize tenant commute programs, and assist with employee marketing and outreach. The TDM resource representative may assist building management in the preparation of tenant materials for new employee orientation, production of kick-off events, coordination of carpool parties, development of commuter e-news articles, support with employee assistance, and coordination of the annual transportation fair.

Tenants will be encouraged to participate in local and regional area commuter promotional marketing and events via Connect San Mateo<sup>2</sup>. The City of San Mateo recently launched the [Connect San Mateo](http://www.connectsanmateo.com) program, a partnership with Commute.org and SamTrans, to offer residents an interactive and user-friendly website for commuters to explore the numerous alternative transportation options available within the City of San Mateo. The goal of the program is to

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<sup>2</sup> [www.connectsanmateo.com](http://www.connectsanmateo.com)



increase awareness of mobility options and alternatives to single-occupancy vehicles, reducing vehicle miles traveled and greenhouse gas emissions. Incentives include gift cards for carpoolers and free trial transit passes.



### Commute Coordinator/Commuter Assistance

The applicant may also provide a project Commute Coordinator whose primary responsibility will be implementing the TDM plan. The Commute Coordinator may be a part-time or outsourced coordinator who manages the commute program. The Commute Coordinator will be responsible for providing commute assistance to employees, producing on-site transportation fairs and promotional events, collaborating with 511 to maximize rideshare resources, conducting the annual survey, and producing the annual commute report. TDM industry data demonstrates that having a Commute Coordinator increases alternative mode use.

The Commute Coordinator will provide the following services:

- Promote trip reduction and air quality strategies to employees at the project site.
- Be the main point of contact for employer contacts and employees who wish to commute using an alternative.
- Conduct annual employee surveys and provide reports to the City of Menlo Park, including commute patterns, mode splits and TDM program success (the process includes annual surveying of employees, tabulation of data and provision of results in report format).
- Evaluate survey results for alternative transportation potential and/or changes to the current program.
- Catalog all existing incentives that encourage employees to utilize alternative transportation programs.
- Work with local agencies such as Caltrain, SamTrans, 511 Rideshare, and the Bay Area Air Quality Management District (BAAQMD).
- Post informational materials on transportation kiosks in employee common areas, as well as disperse alternative program information to employees via designated employer contacts, posters, fliers, banners, campus newsletter, new employee orientation, and etcetera.

- Participate in the BAAQMD Spare the Air program. Spare the Air day notices will be forwarded to employees to discourage driving alone to work.
- Coordinate and manage various aspects of the plan that require periodic updating or monitoring, such as carpool parking, bicycle locker assignments and transit schedule updates.
- As needed, the applicant or property management will provide 706-716 Santa Cruz Avenue TDM (and commute) program training and commute program start-up assistance for tenants. A TDM resource representative or consultant can provide tenant training, planning assistance, and annual monitoring and survey reporting.



The applicant will provide Commute Coordinator staffing and employee outreach and training for commute programs and management.

### **Employee Commuter Flier**

All future employees will be provided with an employee commuter flier. This flier will include (but is not limited to) information about carpool parking, transit opportunities, bicycle routes and on-site amenities and resources. Fliers will be made available at the commute resources kiosks and integrated with tenant/employer information. Fliers can also be incorporated with the new employee packets. Below is a sample employee commuter flier.

# 706 Santa Cruz Avenue COMMUTER PROGRAMS

**COMMUTER SUPPORT** – Find transportation and commuter information below.

<p><b>Transit and Shuttle Services</b></p> <ul style="list-style-type: none"> <li><a href="#">Menlo Park FREE Midday Shuttle Service</a></li> <li><a href="#">Menlo Park Caltrain Station Map</a></li> <li><a href="#">Menlo Park Caltrain Real Time Mobile Tracking</a></li> <li><a href="#">SamTrans Routes to/from Caltrain</a></li> <li><a href="#">Free Trial Transit Passes</a></li> <li><a href="#">Transit Trip Planner</a></li> <li><a href="#">511 Transit Trip Tracker</a></li> </ul>	<p><b>Carpool, Vanpool, and Ride-Matching Services</b></p> <ul style="list-style-type: none"> <li><a href="#">Scoop</a> Carpool Matching app</li> <li><a href="#">First Scoop</a> ride free or driver bonus</li> <li><a href="#">Waze</a> Carpool Matching app</li> <li><a href="#">First Waze</a> ride free or driver bonus - promo code <b>SAARIZahf</b></li> <li><a href="#">Commute.org</a> Carpool Matching site</li> <li><a href="#">Other Carpool Matching apps</a></li> <li><a href="#">\$50 Carpool Incentive (a one-time reward)</a></li> <li><a href="#">511.org Carpool Rewards</a></li> <li><a href="#">Commute.org Vanpool Incentives</a></li> </ul>
<p><b>Bicycle Parking and Facilities</b></p> <ul style="list-style-type: none"> <li>Secure Bicycle Parking (<a href="#">registration form</a>)</li> <li><a href="#">San Mateo County Bike Map</a></li> <li><a href="#">Santa Clara County Bikeways Map</a></li> <li><a href="#">Regional City Bike Maps</a></li> <li><a href="#">Find a Bike Buddy to share the ride</a></li> <li><a href="#">511.org BikeMapper 3.1 BETA</a></li> <li><a href="#">Silicon Valley Bicycle Coalition</a></li> <li><a href="#">Bicycle Resource Guide</a></li> </ul>	<p><b>Commuter Incentives and Services</b></p> <ul style="list-style-type: none"> <li>Commute.org <a href="#">Commuter Rewards</a></li> <li>511.org <a href="#">Commuter Rewards</a></li> <li>Bay Area <a href="#">Share the Air Alert Notices</a></li> <li>Menlo Park <a href="#">Commuter Assistance</a></li> </ul>

**Kick-off Commuter Campaign**

At 50 percent occupancy of the new facility, the applicant will host a commute alternative kick-off celebration or employee marketing campaign. Transportation service providers, such as Caltrain, SamTrans, 511, and bicycle representatives, will be promoted via posters and exhibit booths. To encourage employee participation in the event, the applicant and tenants will provide food (e.g., popcorn, ice cream, hot dogs and/or other refreshments).



### Promotional Programs and Employee Outreach

Throughout the year, as appropriate, the project Commute Coordinator will maintain employee awareness by hosting other transportation fairs. As lunchtime events, these informal fairs will highlight transit and trip-planning services, rideshare matching and other commute opportunities at the new site. The transportation fairs will bring together transit and transportation providers (Caltrain, SamTrans and VTA), bicycle advocates, and ride-matching organizations.

Other on-site events and promotions may include Bike-to-Work Week, Earth Day, Caltrain Day, or an annual Transportation Spare the Air Fair. During the year, various transit and rideshare organizations will be invited to set up a marketing booth during lunchtime at a central location within the building to promote the alternative commute options available to employees. Free trial transit passes will be available for first-time riders. Periodic on-site staffed information tables will also be recommended throughout the year in concert with other employer events such as health fairs, benefits fairs, and etcetera.



Periodic rideshare articles or emails will be written by the project Commute Coordinator for internal employee newsletters (if desired), with ongoing highlights of alternative commuters and their successes. Internal company notices and incentive promotions should attract the attention of commuters, generate excitement about the use of commute alternatives, and reward those who rideshare. These promotions are often sponsored in conjunction with the Regional Rideshare Program or the BAAQMD.

The project Commute Coordinator will register with the BAAQMD for the Spare the Air program to receive regional air quality forecast bulletins about poor and unhealthy air quality days. These direct e-mail updates will be forwarded to all employees to encourage the use of alternative transit during peak advisory periods.

### 12.0 CARPOOL AND VANPOOL RIDE-MATCHING PROMOTIONS

Carpooling will be strongly encouraged at the project. The regional and local rideshare program provides individuals with a computerized list of other commuters near their employment and residential ZIP code, along with the closest cross street, phone number and hours commuters are available to commute to and from work.



The prospective carpooler will also be given a list of existing carpools and vanpools from their residential area that they may be able to join should vacancies exist. To the right is a sample screen shot of this online ride-matching resource.

Commute.org also offers a carpool incentive program. Employees who form a new carpool with two or more people or add a new member to an existing carpool can each receive a \$100 carpool incentive. An image of the carpool incentive program is shown below.



**JOIN TODAY AND START CARPOOLING!**

**C/CAG Carpool 2.0 Rewards Program** The Carpool 2.0 program rewards all forms of carpooling, from matching with your co-workers to using carpool apps to find carpool partners. Carpool 2.0 aims to increase local carpool ridership during peak travel periods, therefore reducing single occupancy vehicles, traffic congestion, greenhouse gas (GHG) emissions and the need for parking within San Mateo County. By carpooling, commuters can save time, money, and the environment.

**How Does Carpool 2.0 Work?** Carpoolers will receive up to **\$100** in e-gift cards and get to choose from over 70 retailers and charities! When carpool trips are tracked through the STAR Commute Tracker app or by connecting a Scoop or Waze Carpool account to a STAR account ([my.commute.org](http://my.commute.org)) to auto-track carpool trips, commuters can unlock rewards. For every 10 carpool days tracked, commuters can claim a \$25 e-gift card reward, up to 4 times. Carpool commutes must start or end in San Mateo County to qualify for this reward.

**How Do I Receive My Reward?** In the Commuter Tracker app or STAR desktop ([my.commute.org](http://my.commute.org)). Click 'Join Program' under rewards. After every 10 carpool days tracked, click 'Claim' to request reward. Once request is approved, an email will be sent to access reward.

**The Program is Live on STAR!** Carpool 2.0 rewards are available now on STAR ([my.commute.org](http://my.commute.org))








### 13.0 BICYCLE RESOURCES

Bicycle commuters looking to find a riding partner can log on to [bicycling.511.org/](http://bicycling.511.org/) for more information. The 511 system provides significant resources for bicycle commuters including:

- ◆ Free Bike Buddy matching
- ◆ Bicycle maps
- ◆ Safe bicycle route mapping
- ◆ Location of lockers
- ◆ How to take your bike on public transit
- ◆ How to take your bike across Bay Area toll bridges
- ◆ How to ride safely in traffic
- ◆ Tips on commuting
- ◆ Tips for bike selection
- ◆ Links to bicycle organizations
- ◆ Bike to Work Day
- ◆ Other bicycle resources (e.g., \$20 monthly pre-tax payroll deduction, etc.)



CycleTracks: Bicycle trip tracking tool



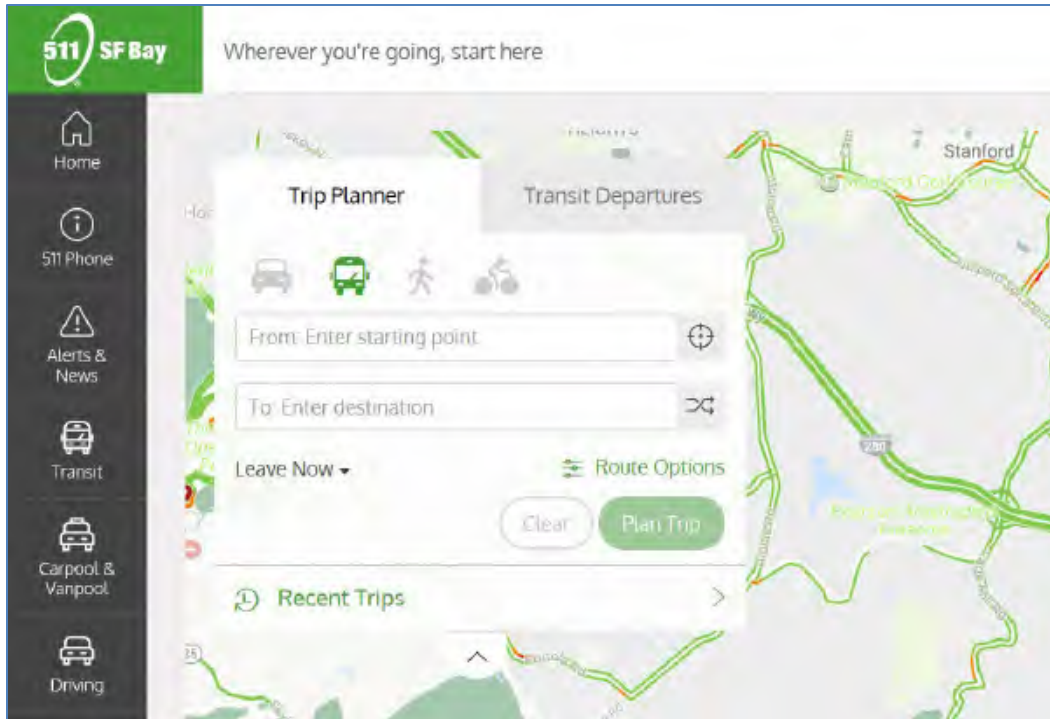
iBikeChallenge: Records your bike trips, tracks miles, calories burned, gas money saved, and pollution prevented

Additional bicycle resources are provided by **Commute.org** such as a free one-hour, on-site Bike and Pedestrian Safety Program for employees. This workshop teaches commuters about bicycling and walking as a safe and stress-relieving commute mode, traffic laws for bicyclists and pedestrians, and bicycle maintenance tips. It also offers a drawing for free bicycle-related prizes.


### 14.0 TRANSIT TRIP PLANNING RESOURCES

Online transit trip planning services are a useful tool for planning public transit trips. The greater San Francisco Bay Area is currently serviced by the 511.org which provides a useful tool for planning public transit trips. The 511 trip planner can build an itinerary that suits the need of the transit user.

The itinerary identifies the fastest commute with the least amount of transfers or the cheapest fares. The trip planner, by default, will generate the fastest itinerary between the origin and destination. This free service can be found online at [www.511.org](http://www.511.org).



Other Transit Resources include online applications and mobile device applications.

 **Dadnab** Dadnab.com enables commuters to plan transit trips in the Bay Area using text messaging from a mobile phone by converting information from the 511 Transit Trip Planner to a text message. By sending a text message with origin, destination and optional arrival or departure time, Dadnab's reply will tell commuters what buses or trains to take at which locations and times.

Google has also collaborated with select regional transit agencies to provide a public transit planner for riders of VTA, SamTrans, AC Transit and BART. This free service can be found online at [www.google.com/transit](http://www.google.com/transit).

## 15.0 EMERGENCY RIDE HOME PROGRAM

All commercial project tenants will implement a free guaranteed emergency ride home (GRH) program for their employees who use alternative forms of transportation. All employees who commute to work using transit, bicycle, walking, carpool or vanpool will be guaranteed a free ride home in case of a personal emergency or when they unexpectedly must work late, thereby missing the last bus, train or their normal carpool home.



The GRH program will provide employees with peace of mind that comes from knowing that if a child or loved one becomes ill or injured during the day, the employee can get to them quickly. The GRH program has proven very successful, as it removes one of the major objections’ employees must giving up their private automobile, especially those with young families. Tenants, and all their employees, may participate in the free emergency ride home program administered by the Alliance (formerly the Peninsula Traffic Congestion Relief Alliance). To participate in the GRH program, commuters must first register on Commute.org’s STAR platform. STAR users can access their GRH reimbursement options after logging a qualifying trip.

The GRH program reimburses commuters who carpool, vanpool, take transit, shuttle, bike, or walk to work or to a participating college in San Mateo County up to \$60 per event, up to four times per calendar year. GRH program participants decide how to get home (e.g. taxi, ride-hailing app, public transit, or combination) and complete the reimbursement process after they are home safely. If public transit is used as the GRH ride, Commute.org will give a \$5 Starbucks e-Card bonus.

**Eligible reasons for GRH trip:**

- Personal or family illness or emergency
- Home emergency
- Eldercare or daycare emergency
- Bicycle theft or breakdown
- Unforeseen change of work schedule (requires confirmation from supervisor)
- Inclement weather (for walkers/bicyclists)
- Carpool partner emergency resulted in loss of ride home

**GRH does not cover the following trips or reasons for reimbursement:**

- Transit delays
- Natural disasters
- Personal errands or appointments
- Ride to work
- Carpool app provider cannot find a match to get the commuter home
- Taking an Uber or Lyft to work is not a qualifying alternative commute mode to work
- Non-emergency side trips
- Business-related travel
- Transportation to a doctor or hospital resulting from an on-the-job injury (GRH cannot be used to replace an employer’s legal responsibility under workers’ compensation regulations.)

<p>Join the Alliance’s guaranteed ride home program.</p>	<p>Two peak hour trips will be credited for every 2 slots purchased in the program.</p>	<p>Experience shows that when a Guaranteed Ride Home Program is added to a TDM program, average ridership increases by about 50%.</p>
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**16.0 TRANSPORTATION MANAGEMENT ASSOCIATION PARTICIPATION**

As San Mateo County’s TMA-like entity, Commute.org is available to help employers and property managers develop or enhance their commuter programs. The goal is to encourage employees and tenants to make smart transportation choices: carpooling; vanpooling; taking a bus, train, shuttle or ferry; biking; and walking. Programs Representatives are available to aid employers with all Commute.org (and 511.org) programs.

The project development will be work with Commute.org to create a site-specific program once tenants are identified and ready to occupy the site.

Participate in/create/sponsor a Transportation Management Association.	Five peak hour trips will be credited.	Generally acceptable TDM practices (based on research of TDM practices around the nation and reported on the Internet).
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## SECTION III – COMMITMENTS, MONITORING AND REPORTING

### 17.0 COMMITMENTS, MONITORING AND REPORTING

The intent of TDM planning is to reduce SOV trips and, in so doing, lessen resulting parking issues, traffic congestion and mobile source-related air pollution. Menlo Park requires all net, new peak-hour trips are mitigated. A comprehensive program of TDM measures and incentives can reduce parking demand, traffic and air pollution, creating a more sustainable employment environment, while freeing up valuable land for higher and better uses.

*According to the California Air Pollution Control Officers Association (CAPCOA)'s newest greenhouse gas document, Quantifying Greenhouse Gas Mitigation Measures,<sup>3</sup> a subsidized or discount transit program provides the maximum potential to reduce up to 30 percent of commute vehicle trips. A ride-share program could reduce the commute vehicle trips up to 15 percent. A program to limit the parking supply or to charge work place parking could reduce up to 12 percent of commute vehicle trips, and providing trip end facilities or a cash-out program could likely reduce the commute vehicle trips by five and six percent, respectively. Although the reductions are not additive, a combination of measures would have a global maximum cap likely ranging from 20 to 40 percent.<sup>4</sup>*

It is important to ensure TDM measures are implemented and effective. Therefore, a monitoring program may be necessary. Because the TDM Program is performance based (looking for project alternative mode-use and corresponding trip reductions), an annual commute program evaluation (the annual employee five-day commute survey) will allow the applicant and the City to assess the effectiveness of the unique program designed for the project.

#### **City/County Association of Governments of San Mateo County (C/CAG) Guidelines**

C/CAG requires the applicant to implement TDM programs that have the capacity to reduce the demand for new peak-hour trips. These programs, once implemented, must be ongoing for the occupied life of the development. The local jurisdiction must also agree to maintain data available for monitoring by C/CAG that supports the ongoing compliance with the agreed-to trip reduction measures. The estimated C/CAG trip credit accounting for the development is provided in page 30.

The C/CAG trip credit accounting also meets the City of San Mateo's requirement to provide a quantifiable checklist of vehicle trip reduction measures.

<sup>3</sup> Available at: <http://www.capcoa.org/wp-content/uploads/2010/11/CAPCOA-Quantification-Report-9-14-Final.pdf>

<sup>4</sup> 1<sup>st</sup> Admin Draft of Draft Environmental Impact Report, State Clearinghouse #2012082007, City of Mountain View



The May 7, 2019, updated traffic assessment letter, prepared by Hexagon Transportation Consultants, Inc., estimated the project, to generate 40 trips during the AM peak hour, and 75 trips during the PM peak hour. Below is an excerpt from the Traffic assessment.

Project trip generation was estimated by applying to the size and uses of the development the appropriate trip generation rates obtained from the ITE Trip Generation Manual, 10th Edition. Based on average trip generation rates for a general office building (Land Use 710), retail space (shopping center, Land Use 820), and multi-family housing (low-rise) (Land Use 220) the proposed development would generate a total of 709 daily trips, with 40 trips (30 inbound and 10 outbound) occurring during the AM peak hour and 75 trips (27 inbound and 48 outbound) occurring during the PM peak hour (see Table 1).

The existing retail building's trip generation can be credited against the proposed mixed-use development. The retail building's trip generation was estimated based on the same rates. Based on ITE rates, the existing retail building is generating, or could be generating, a total of 482 daily trips with 12 trips occurring during the AM peak hour, and 49 trips occurring in the PM peak hour.

After subtracting the existing use trip credit, the project is estimated to produce a net increase of 227 daily trips, with an increase of 28 trips (23 inbound and 5 outbound) during the AM peak hour and an increase of 26 trips (4 inbound and 22 outbound) during the PM peak hour. [Net, new peak-hour trips total 54.]

Below is the trip generation Table 1 from the Hexagon traffic letter which shows the total, and net, new total peak-hour trips for AM and PM periods.

**Table 1**  
**Trip Generation Estimates for the 706 Santa Cruz Avenue Mixed-Use Development**

Land Use	Size	Daily			AM Peak Hour			PM Peak Hour			
		Rate	Trips	Rate	In	Out	Total	Rate	In	Out	Total
<b>Proposed Uses</b>											
Office Space <sup>1</sup>	23.45 ksf	9.74	228	1.16	23	4	27	1.15	4	23	27
Retail <sup>2</sup>	11.96 ksf	37.75	451	0.94	7	4	11	3.81	22	24	46
Apartment <sup>3</sup>	4 d.u.	7.32	29	0.46	0	2	2	0.56	1	1	2
<b>Subtotal</b>			<b>709</b>		<b>30</b>	<b>10</b>	<b>40</b>		<b>27</b>	<b>48</b>	<b>75</b>
<b>Existing Uses</b>											
Retail <sup>2</sup>	(12.76) ksf	37.75	(482)	0.94	(7)	(5)	(12)	3.81	(23)	(26)	(49)
<b>Net New Trips</b>			<b>227</b>		<b>23</b>	<b>5</b>	<b>28</b>		<b>4</b>	<b>22</b>	<b>26</b>
<b>Notes:</b>											
ksf = 1,000 square feet.											
d.u. = Dwelling Unit											
<sup>1</sup> General Office Building (Land Use 710) based on ITE's <i>Trip Generation Manual, 10th Edition</i> , average rates.											
<sup>2</sup> Shopping Center (Land Use 820) based on ITE's <i>Trip Generation Manual, 10th Edition</i> , average rates.											
<sup>3</sup> Multi-Family Housing (Low-Rise) (Land Use 220) based on ITE's <i>Trip Generation Manual, 10th Edition</i> , average rates.											

Selected TDM project measures were assessed using the C/CAG trip credit accounting criteria. The C/CAG trip credit accounting below shows that 82 peak-hour trips will be mitigated by the project's TDM measures. This accounting meets the City of San Mateo's requirement to provide a checklist of trip reduction measures that integrate with the C/CAG mitigation criteria.

TDM Measures	Quantity	Credit Ratio	Trip Credit
Bicycle Parking - long-Term (Class I) (10)			
Bicycle Parking - Short-Term (Class II) (12)			
Total Bicycle Storage	22	0.33	7
Preferential carpool and EV/clean fuel parking	11	0.5	5.5
Locate residential development within 1/3 mile of fixed rail	27	1	27
Transportation Information Board/Kiosk(s)	1	5	5
Nearby amenities	1	5	5
TMA Participation (via Commute.org)	1	5	5
Developer-Provided Tenant Training and Resources	1	1	1
Guaranteed Emergency Ride Home program	10	1	10
Annual Employee Commute Survey	0.5	3	1.5
TDM/Transportation Action Plan	1	10	10
Additional Credit for combination of any 10 elements	1	5	5
<b>Total C/CAG Trip Credits</b>			<b>82</b>

The number of guaranteed ride home credits (10) applied in the above matrix are based on the combined number of overall bicycle parking facilities, carpool parking spaces, residence location within 1/3 mile of transit (of which all trips are mitigated), and other TDM measures that are assumed to generate at least ten commuters (or more) who will be eligible for the GRH program.

**Annual Driveway Hose Counts**

The project will prepare and provide annual driveway counts, in addition to the employee survey (discussed below). The counts will be prepared by an independent, licensed consultant and paid for by the property owner, or tenant. The driveway counts and resulting data shall be included in the annual TDM Commute Survey Report.

**Annual Five-day Employee Commute Survey**

A five-day commute survey will be completed each year to evaluate and ensure the success of the TDM measures. The applicant will encourage, support and participate in the promotion and marketing of the annual employee survey. Employees who do not participate in the commute survey will be counted as drive-alone or SOV commuters by default. Consequently, this default mechanism will produce conservative results.

Survey data can be used to focus TDM marketing and the efforts of the Commute Coordinator to maintain the project’s commitment to reduce vehicle trips at the site. Below is a sample of a survey question to gauge employees’ typical daily commute activities.

**6. How did you GET TO WORK LAST WEEK, (select the primary transportation method you used.) If you were out of the office, please describe your "typical" weekly commute activity.**

Commute Modes	
Monday	
Tuesday	Drove alone to worksite
Wednesday	Rode as a passenger in a carpool (did not drive)
Thursday	Carpooled with an employee/colleague
Friday	Vanpooled (5+ people)
	Rode transit (bus, shuttle, train, etc.)
	Biked to work
	Walked/jogged to work
	Teleworked/worked remotely
	Rode motorcycle/scooter
	Did not work this day

**Annual Summary Report**

Each year, the applicant and Commute Coordinator, via employee survey data, will prepare an annual TDM summary report to be submitted to the City to document the effectiveness of the TDM Plan in achieving alternative mode-uses and trip reduction efforts. The TDM summary report will include a determination of historical employee commute methods provided by information obtained from a survey of all employees working in the buildings. The summarized results from the employee survey will provide both quantitative data (e.g., mode split) and qualitative data (e.g., employee perception of the alternative transportation programs).

<b>Employee Commute Mode Summary</b>	<b>Percent</b>
Carpooler (driver and/or passenger)	8.34%
Transit and/or Shuttle Users	4.34%
Bicycle	2.90%
Walker/Pedestrian	1.14%
Telecommuter	2.59%
Motorcycle/moped	0.29%
Vanpooler	0.00%
Did not work this day	0.63%
<b>Total</b>	<b>20.23%</b>

The initial annual employee survey (and subsequent surveys) will be conducted in the fourth quarter of each year.

Survey Employees to examine use and best practices.	Three peak hour trips will be credited for a survey developed to be administered twice yearly	This is based on staff's best estimate with the goal of finding best practices to achieve the mode shift goal.
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**No Expiration of TDM Plan or Programs**

All measures in this TDM Plan will continue to be implemented by the applicant on an ongoing basis. There is no expiration of this Plan. Periodic on-site auditing may be conducted by the City of Menlo Park to ensure that measures in this Plan are being implemented.

## SECTION IV – RESIDENTIAL TDM MEASURES

### 18.0 RESIDENTIAL TDM MEASURES

Residents of this mixed-use project will enjoy access to the on-site commuter and transportation resources within the commercial areas of the building. Residents can take advantage of the various commuter features that are offered to the employees at the site. These include:

- Bicycle amenities and parking
- Access to the transportation commuter kiosk
- Participation in San Mateo Scoop ride matching campaign
- Participation in [Connect San Mateo](#) and [Commute.org](#) resources (including GRH)
- Participation in the on-site commuter promotional marketing
- Participation in the on-site commuter events and fairs
- Participation in the annual commute survey

Additional residential TDM measures may include features that are designed specifically for future residents of the building.

1. Resident electronic transportation resource flier
2. Resident commuter welcome resource packet
3. Resident free trial transit passes

Below are the details of these three TDM measures.

#### Electronic Resident Transportation Resource Flier

An electronic resident commuter resource flier like the employee flier will be created. The flyer will highlight nearby transit opportunities, and provide resource links to ridesharing, bicycle, commuter and car sharing resources. The property manager will email residents the electronic transportation flier for easy access to commuter links.

#### Resident Welcome Commuter Resource Packet

Prior to occupancy, all residents will receive an information packet containing on-site commuter amenities (e.g., bicycle parking, commuter kiosk) and alternative transportation opportunities. The Resident Commuter Packet will include transit and local shuttle maps and schedules, bicycle maps, and trip planning resources.

#### Resident Free Trial Transit Pass

In addition to the resident commuter resource materials, a five-pack of free trial SamTrans transit day passes will be provided to each new residential tenant (if requested). Alternatively, the resident may choose an 8-ride Caltrain ticket. These trial transit passes will allow residents to try the transit service for one week.

	Local, KX, 292, 397 Into San Francisco			
	Cash	Clipper	Day Pass	Monthly Pass
<b>Adult</b> (Age 19 through 64)	\$2.25	\$2.05	\$5.50	\$65.60



According to CCAG, locating residential development within a one-third mile of a fixed rail passenger station provides significant trip-reduction benefits. All trips from the four residential units, totaling 23 daily trips, will be considered credited due to the location of the development.

Locate residential development within one-third mile of a fixed rail passenger station.	All trips from a residential development within one-third mile of a fixed rail passenger station will be considered credited due to the location of the development.	This is based on staff's best estimate.
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**19.0 CONCLUSION**

The 706-716 Santa Cruz Avenue TDM Plan was developed to meet the specific needs for the project, considering logistical resources and opportunities of the site. From conception, the applicant has been committed to an integrated project design that enhances pedestrian and community opportunities.

This TDM Plan describes elements, measures and actions that commit the applicant to implementation and achieve a meaningful reduction in vehicle trips. The orientation of TDM features for this project will increase opportunities for pedestrian, bicycle, carpool, transit and shuttle uses.

The TDM Plan requires implementation of measures, performance, and directs the applicant and future employers to incorporate programs and employee benefits and create a formal commute program. Commute program marketing, ongoing promotions, annual survey and reporting and a Commute Coordinator will provide the synergism needed to create an effective and successful program for future project employees.

Annual monitoring via surveys will provide the documentation needed to demonstrate effectiveness, reduction of 54 net, new peak-hour vehicle trips, and requires the applicant to identify additional TDM measures and programs they would implement if the goal is not achieved.

The applicant is committed to reducing trips and increasing alternative transportation mode-uses. This TDM Plan provides the details of the applicant's commitment to the City of Menlo Park and designated responsibility for implementation.

The 706-716 Santa Cruz Avenue project supports the City of Menlo Park's policy of focusing clustered development along major transportation corridors, as well as reinforces the City of Menlo Park's Green goals and practices. By balancing air quality with economic growth, the 706-716 Santa Cruz Avenue project will help Menlo Park thrive as a community. It is projects like these that will contribute to the City of Menlo Park's future livelihood.














## **ATTACHMENTS**

*List of Nearby Amenities – 0.25 miles or less from 706-716 Santa Cruz Avenue  
(Personal services, restaurants, coffee, retail/sundry, banking, etc.)*

*Local Transit Maps (SamTrans Routes)*

**List of Nearby Amenities – Located 0.25 miles or less from 706-716 Santa Cruz Avenue**

**Restaurants, Cafes/Delis, Coffee, and Bakeries**




-  **Juban Yakiniku House**  
712 Santa Cruz Avenue, Menlo Park, CA – (650) 473-6458 (43 ft. away)
-  **Starbucks**  
643-693 Santa Cruz Avenue, Menlo Park, CA – (650) 323-5118 (128 ft. away)
-  **Una Mas Mexican Grill**  
683 Santa Cruz Avenue, Menlo Park, CA – (650) 853-1200 (167 ft. away)
-  **La Boulanger**  
720 Santa Cruz Avenue, Menlo Park, CA – (650) 322-5528 (213 ft. away)
-  **SusieCakes**  
642 Santa Cruz Avenue, Menlo Park, CA – (650) 324-2252 (220 ft. away)
-  **Bistro Vida**  
641 Santa Cruz Avenue, Menlo Park, CA – (650) 462-1686 (282 ft. away)
-  **Bagel Street Café**  
746 Santa Cruz Avenue, Menlo Park, CA – (650) 328-8809 (328 ft. away)
-  **Shiok! Singapore Kitchen**  
1137 Chestnut Street, Menlo Park, CA – (650) 838-9448 (367 ft. away)
-  **Ann's Coffee Shop**  
772 Santa Cruz Avenue, Menlo Park, CA – (650) 322-0043 (371 ft. away)
-  **Gerry's Cakes**  
1141 Chestnut Street, Menlo Park, CA – (650) 326-6282 (410 ft. away)
-  **Left Bank Menlo Park Brasserie**  
635 Santa Cruz Avenue, Menlo Park, CA – (650) 473-6543 (459 ft. away)
-  **Carpaccio**  
1120 Crane Street, Menlo Park, CA – (650) 322-1211 (486 ft. away)
-  **The Refuge**  
1143 Crane Street, Menlo Park, CA – (650) 319-8197 (0.10 miles away)
-  **Café Del Sol**  
1010 Doyle Street #1, Menlo Park, CA – (650) 326-2501 (0.10 miles away)
-  **McDonald's**  
1100 El Camino Real, Menlo Park, CA – (650) 321-1813 (0.10 miles away)
-  **Trellis Restaurant**  
1077 El Camino Real, Menlo Park, CA – (650) 326-9028 (0.10 miles away)
-  **Stacks**  
600 Santa Cruz Avenue, Menlo Park, CA – (650) 838-0066 (0.10 miles away)
-  **Menlo Café**  
620 Santa Cruz Avenue #A, Menlo Park, CA – (650) 321-6666 (0.10 miles away)
-  **Su Hong to Go**  
630 Menlo Avenue, Menlo Park, CA – (650) 322-4631 (0.10 miles away)
-  **Galata Mediterranean Grill**  
827 Santa Cruz Avenue, Menlo Park, CA – (650) 325-7900 (0.10 miles away)
-  **Subway**  
809 Santa Cruz Avenue, Menlo Park, CA – (650) 330-1692 (0.10 mile away)
-  **Angelo Mio**  
820 Santa Cruz Avenue, Menlo Park, CA – (650) 323-3665 (0.10 miles away)

- ✚ **Mama Coco Cocina Mexicana**  
1081 El Camino Real, Menlo Park, CA – (650) 272-6634 (0.10 miles away)
- ✚ **Quiznos**  
604 Santa Cruz Avenue, Menlo Park, CA – (650) 326-0820 (0.10 miles away)
- ✚ **Posh Bagel**  
869 Santa Cruz Avenue, Menlo Park, CA – (650) 329-8592 (0.20 miles away)
- ✚ **Round Table Pizza**  
1225 El Camino Real, Menlo Park, CA – (650) 321-6861 (0.20 miles away)
- ✚ **Phil's Kitchen: Hawaiian Barbeque & Chinese Specialty Take-Out**  
625 Oak Grove Avenue #B, Menlo Park, CA – (650) 561-4296 (0.20 miles away)
- ✚ **Sultana**  
1149 El Camino Real, Menlo Park, CA – (650) 322-4343 (0.20 miles away)
- ✚ **Café Borrone**  
1010 El Camino Real, Menlo Park, CA – (650) 327-0830 (0.20 miles away)
- ✚ **Applewood Pizza**  
1001 El Camino Real, Menlo Park, CA – (650) 324-3486 (0.20 miles away)
- ✚ **Peet's Coffee & Tea**  
899 Santa Cruz Avenue, Menlo Park, CA – (650) 325-8989 (0.20 miles away)
- ✚ **LB Steak Menlo Park**  
898 Santa Cruz Avenue, Menlo Park, CA – (650) 321-8980 (0.20 miles away)
- ✚ **Amici's East Coast Pizzeria**  
880 Santa Cruz Avenue, Menlo Park, CA – (650) 329-8888 (0.20 miles away)
- ✚ **Café Borrone**  
1010 El Camino Real, Menlo Park, CA – (650) 327-0830 (0.20 miles away)
- ✚ **Akasaka**  
925 El Camino Real, Menlo Park, CA – (650) 325-0444 (0.30 miles away)
- ✚ **Jan's Deli**  
1004 Alma Street, Menlo Park, CA – (650) 321-9372 (0.30 miles away)
- ✚ **Draeger's Supermarkets Deli**  
1010 University Drive, Menlo Park, CA – (650) 948-7204 (0.30 miles away)
- ✚ **Jason's Café**  
1246 El Camino Real, Menlo Park, CA – (650) 321-3300 (0.30 miles away)
- ✚ **J&J Hawaiian BBQ**  
1170 Alma Street, Menlo Park, CA – (650) 323-6137 (0.30 miles away)
- ✚ **Bradley's Funky Franks**  
1195 Merrill Street, Menlo Park, CA – (650) 391-9634 (0.30 miles away)
- ✚ **Bradley's Fine Diner**  
1165 Merrill Street, Menlo Park, CA – (650) 494-4342 (0.30 miles away)
- ✚ **Jan's Deli**  
1004 Alma Street, Menlo Park, CA – (650) 321-9372 (0.30 miles away)
- ✚ **Iberia Restaurant**  
1026 Alma Street, Menlo Park, CA – (650) 325-8981 (0.30 miles away)

## Health, Beauty & Fitness

-  **Accent on Eyewear**  
729 Santa Cruz Avenue, Menlo Park, CA – (650) 324-8888 (213 ft. away)
-  **La Migliore Aveda Concept Salon**  
644 Santa Cruz Avenue, Menlo Park, CA – (650) 321-1100 (226 ft. away)
-  **Aida Custom Cosmetics**  
1146 Chestnut Street, Menlo Park, CA – (650) 327-9882 (397 ft. away)
-  **Home Care Services for Aging Adults**  
1150 Chestnut Street, Menlo Park, CA – (650) 328-1050 (417 ft. away)
-  **Nuffer Fitness**  
1149 Chestnut Street #2, Menlo Park, CA – (650) 417-0983 (469 ft. away)
-  **Euro Skin Care**  
1176 Chestnut Street, Menlo Park, CA – (650) 328-6089 (499 ft. away)
-  **Wellfit AJ Personal Trainer**  
1019 El Camino Real, Menlo Park, CA – (650) 906-3003 (0.20 miles away)
-  **Elizabeth's Skincare Studio**  
681 Oak Grove Avenue, Menlo Park, CA – (650) 324-3223 (0.20 miles away)
-  **Empowerment Fitness**  
1019 El Camino Real, Menlo Park, CA – (650) 575-2772 (0.20 miles away)
-  **SBM Fitness**  
1019 El Camino Real, Menlo Park, CA – (650) 999-0532 (0.20 miles away)
-  **Pharmaca Integrative Pharmacy**  
871 Santa Cruz Avenue, Menlo Park, CA – (650) 618-6300 (0.20 miles away)
-  **Simpsons Family Barber Shop**  
1181 El Camino Real, Menlo Park, CA – (650) 853-9913 (0.20 miles away)
-  **Susan's Nails**  
1285 El Camino Real, Menlo Park, CA – (650) 289-0207 (0.30 ft. away)
-  **Menlo Park Acupuncture Clinic**  
530 Oak Grove Avenue #7, Menlo Park, CA – (650) 326-9391 (0.30 miles away)
-  **Veronika Gold (Psychotherapist)**  
530 Oak Grove Avenue #104, Menlo Park, CA – (650) 422-2418 (0.30 miles away)

## Retail

-  **ACE Hardware**  
700 Santa Cruz Avenue, Menlo Park, CA – (650) 325-2515 (52 ft. away)
-  **Yves Delorme**  
656 Santa Cruz Avenue, Menlo Park, CA – (650) 324-3502 (92 ft. away)
-  **Bow Wow Meow**  
654 Santa Cruz Avenue, Menlo Park, CA – (650) 323-2845 (115 ft. away)
-  **Goodwill**  
711 Santa Cruz Avenue, Menlo Park, CA – (650) 324-9380 (220 ft. away)
-  **Mike's Camera Inc.**  
715 Santa Cruz Avenue, Menlo Park, CA – (650) 323-7701 (220 ft. away)
-  **Walgreens**  
643 Santa Cruz Avenue, Menlo Park, CA – (650) 321-1530 (223 ft. away)
-  **Cheeky Monkey Toys**  
640 Santa Cruz Avenue, Menlo Park, CA – (650) 328-7975 (279 ft. away)






- ✚ **Harvest Furniture**  
639 Santa Cruz Avenue, Menlo Park, CA – (650) 325-7733 (302 ft. away)
- ✚ **Angela**  
1129 Chestnut Street, Menlo Park, CA – (650) 323-7410 (302 ft. away)
- ✚ **K.C. Goldsmiths**  
1148 Chestnut Street, Menlo Park, CA – (650) 325-9276 (390 ft. away)
- ✚ **Penzeys Spices**  
771 Santa Cruz Avenue, Menlo Park, CA – (650) 853-1785 (397 ft. away)
- ✚ **The Shop – Junior League of Palo Alto**  
785 Santa Cruz Avenue, Menlo Park, CA – (650) 328-7467 (486 ft. away)
- ✚ **Milana C (Boutique)**  
1158 Chestnut Street, Menlo Park, CA – (650) 321-6600 (486 ft. away)
- ✚ **Red Lantern Cycles**  
640 Menlo Avenue, Menlo Park, CA – (650) 853-3051 (0.10 miles away)
- ✚ **Fleet Feet Menlo Park**  
859 Santa Cruz Avenue, Menlo Park, CA – (650) 325-9432 (0.10 miles away)
- ✚ **Trader Joe's**  
720 Menlo Avenue, Menlo Park, CA – (650) 323-2134 (0.10 miles away)
- ✚ **Gray's Paint**  
717 Oak Grove Avenue, Menlo Park, CA – (650) 322-2238 (0.10 miles away)
- ✚ **Peninsula Window Fashions & Design**  
1047 El Camino Real, Menlo Park, CA – (650) 853-9000 (0.10 miles away)
- ✚ **Isabella Boutique**  
640 Menlo Avenue, Menlo Park, CA – (408) 738-2980 (0.10 miles away)
- ✚ **Head Over Heels**  
887 Santa Cruz Avenue, Menlo Park, CA – (650) 325-2400 (0.20 miles away)
- ✚ **Josef Boutique**  
883 Santa Cruz Avenue, Menlo Park, CA – (650) 353-7550 (0.20 miles away)
- ✚ **Relax the Back Menlo Park**  
1198 El Camino Real, Menlo Park, CA – (650) 325-2225 (0.20 miles away)
- ✚ **Feldman's Books**  
1170 El Camino Real, Menlo Park, CA – (650) 326-5300 (0.20 miles away)
- ✚ **Kepler's Books**  
1010 El Camino Real, Menlo Park, CA – (650) 324-4321 (0.20 miles away)
- ✚ **Draeger's Supermarkets Deli**  
1010 University Drive, Menlo Park, CA – (650) 948-7204 (0.30 miles away)
- ✚ **7-Eleven**  
525 Oak Grove Avenue, Menlo Park, CA – (650) 325-7007 (0.30 miles away)
- ✚ **Farnad (Tailor)**  
1160 University Drive, Menlo Park, CA – (650) 325-1200 (0.30 miles away)
- ✚ **Mallet Sports**  
885 Oak Grove Avenue, Menlo Park, CA – (650) 521-0639 (0.30 miles away)
- ✚ **Dancer Dejour**  
1283 El Camino Real, Menlo Park, CA – (650) 321-4000 (0.30 miles away)
- ✚ **Dressed Room**  
1014 Alma Street, Menlo Park, CA – (650) 752-6687 (0.30 miles away)






## Transportation & Shipping

-  **Menlo Park Chevron**  
1200 El Camino Real, Palo Alto, CA – (650) 4239 (0.20 miles away)
-  **FedEx Office Print & Ship Center**  
1194 El Camino Real, Menlo Park, CA – (650) 321-4202 (0.20 miles away)
-  **US Post Office**  
655 Oak Grove Avenue, Menlo Park, CA – (650) 321-0954 (0.20 miles away)
-  **Menlo Atherton Auto Repair**  
1279 El Camino Real, Menlo Park, CA – (650) 325-1280 (0.30 miles away)
-  **Post N' More**  
1259 El Camino Real, Menlo Park, CA – (650) 326-6254 (0.30 miles away)
-  **M&R Automotive – Menlo Park**  
1281 El Camino Real, Menlo Park, CA – (650) 325-3900 (0.30 miles away)



## Entertainment

-  **Color Me Mine**  
602 Santa Cruz Avenue, Menlo Park, CA – (650) 328-4486 (489 ft. away)
-  **Peabody Fine Art Gallery**  
603 Santa Cruz Avenue, Menlo Park, CA – (650) 322-2200 (0.10 miles away)
-  **Menlo Park Academy of Dance**  
1163 El Camino Real, Menlo Park, CA – (650) 323-5292 (0.20 miles away)

## Bank & ATM

-  **Bank of the West**  
701 Santa Cruz Avenue, Menlo Park, CA – (650) 328-4530 (144 ft. away)
-  **Chase Bank**  
650 Santa Cruz Avenue, Menlo Park, CA – (650) 853-2655 (161 ft. away)
-  **Bank of America**  
633 Santa Cruz Avenue, Menlo Park, CA – (650) 687-0883 (436 ft. away)
-  **U.S. Bank**  
1105 El Camino Real, Menlo Park, CA – (650) 617-8330 (0.10 miles away)
-  **Citibank**  
620 Santa Cruz Avenue, Menlo Park, CA – (650) 353-2769 (0.10 miles away)

## Daycare

-  **Brilliant Babies**  
1075 Curtis Street, Menlo Park, CA – (650) 321-0770 (177 ft. away)
-  **Kirk House Preschool**  
950 Santa Cruz Avenue, Menlo Park, CA – (650) 323-8667 (0.30 miles away)

# ROUTE 82



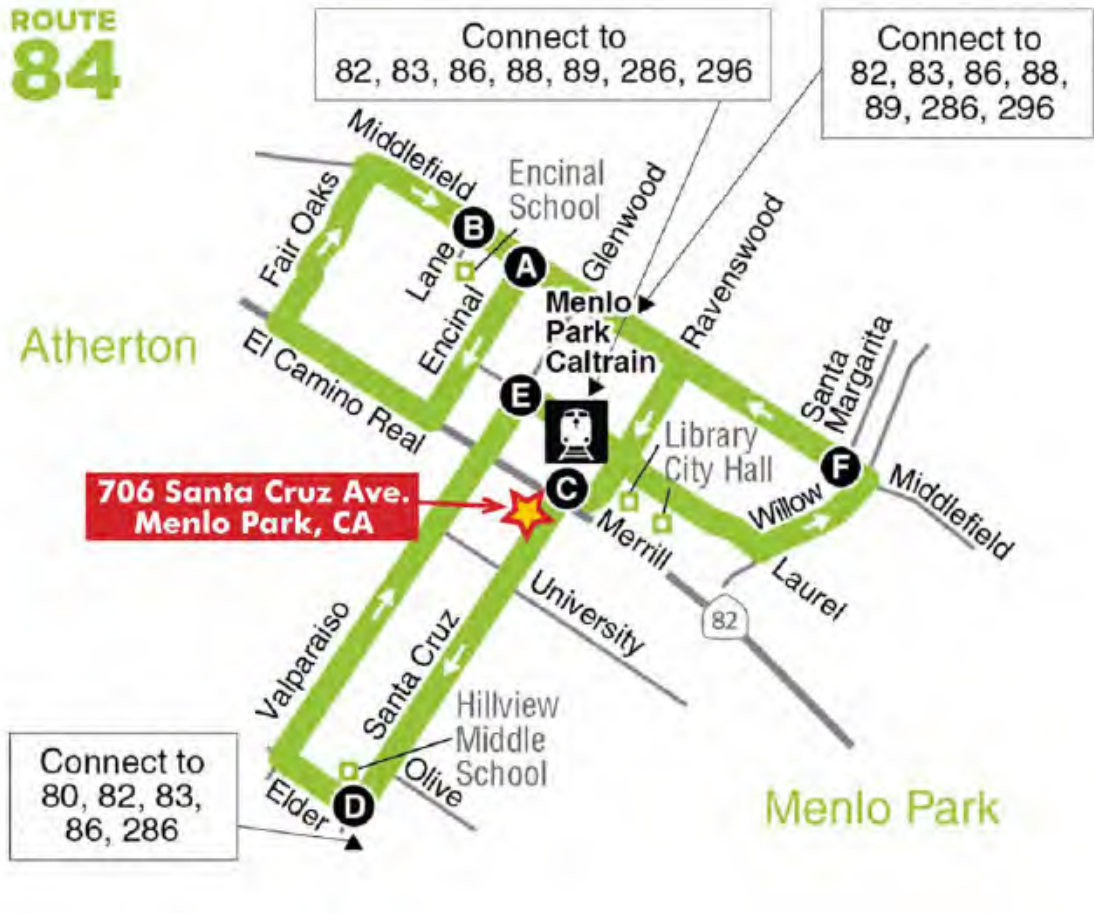
# ROUTE 83

Atherton



**ROUTE  
84**

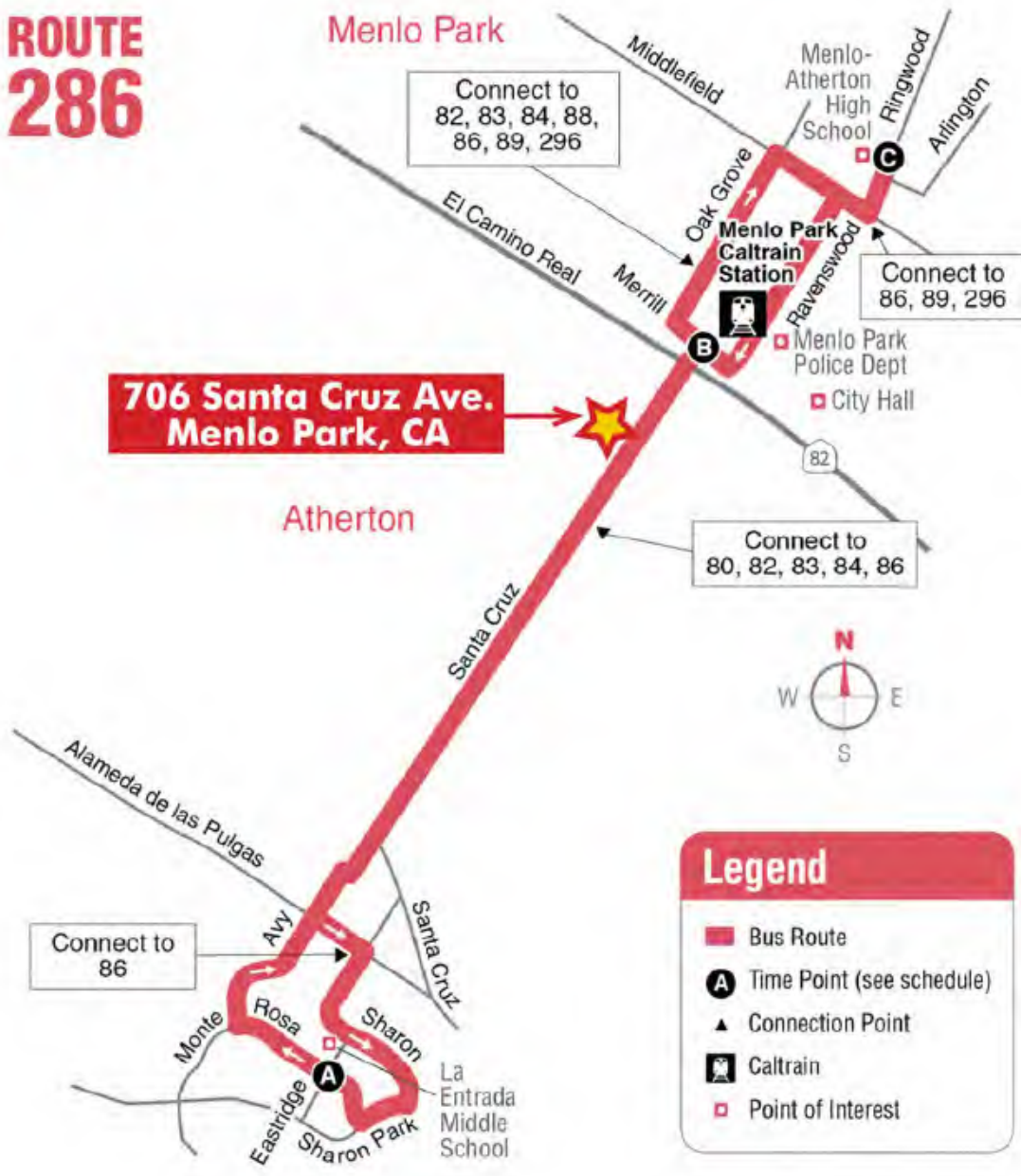
Atherton







# ROUTE 286



## **APPENDIX A**

*Guaranteed Ride Home Program Materials  
Sample Employer Resources and Incentives*

## APPENDIX A – SAMPLE OF EMPLOYER RESOURCES AND INCENTIVES

A description of employer commuter information, incentives and resources are provided in the proceeding pages.

### **Management Priority**

The support and involvement of senior management has a significant positive impact on the success of the TDM goals and elements that are implemented.

Alternative mode programs must be presented to the employees in a comprehensive and proactive manner along with any other employee programs. This can be done via participation and support of employee orientation forums or transportation fairs, transportation kiosk posting, employee newsletters, management bulletins, e-mails, etcetera.

From a practical standpoint, management support must be twofold:

- 1) Upper and middle management will encourage alternative modes whenever possible.
- 2) Managers and supervisors will be supportive of employees who try out alternative modes, even if it means initial minor adjustments to their work schedule.

TDM should be viewed as a big picture process. This includes explaining the area's air quality problems, and describing how fighting air pollution is part of being a good corporate citizen. It is important that the employees recognize the benefits on a personal and community level to see how they benefit from better air quality and less traffic congestion on the highways and the surrounding neighborhoods, less parking hassles, cost savings for employees, etcetera.

### **Business Savings (Employer Resource Tool)**

Another good resource is the Business Savings Calculator provided by the Best Workplaces for Commuters, established by the U.S. Environmental Protection Agency (EPA):

<http://www.bestworkplaces.org/resource-center/business-savings-calculator/>

The U.S. EPA developed this web-based Calculator to estimate the financial, environmental, traffic-related, and other benefits of joining the program.

Based on the information that employers enter into the calculator (describing how their organizations will meet the National Standard of Excellence for commuter benefits), this fast and easy-to-use tool produces a variety of estimates including:

- **Employee recruiting and retention.** The estimated savings from reduced employee turnover.
- **Employer taxes.** The savings employers would realize in reduced payroll taxes if they select transit passes or vanpool benefits as a way of meeting the National Standard of Excellence.
- **Employee taxes.** The income tax savings employees would realize if they choose transit passes or vanpool benefits as a way of meeting the National Standard of Excellence.

- **Total financial benefits.** The total financial savings from parking facilities, taxes, and other financial impacts.
- **Employee productivity and stress.** The estimated improvement in employee productivity and reduction in employee stress (calculations that are based in part on a recent study in Southern California).
- **Safety.** The decrease in fatalities, injuries, and lost work time that result when the number of vehicle trips is reduced.

Then, an overview of total costs and benefits divides the impacts related to commuter programs into four sections:

1. **Direct Costs and Savings:** Direct costs and savings for the employer and employees include financial impacts that occur directly as a result of the commuter benefits.
2. **Potential Facility Cost Savings:** Potential savings include reducing parking and office space costs.
3. **Recruitment and Productivity Benefits:** These business benefits can be substantial but may not appear as direct outlays or cost savings for an employer.
4. **Community Benefits:** Community benefits include reduced traffic, energy consumption, and emissions.

#### **Commuter Choice – Pre-Tax Options**

As of January 2016, the federal Commuter Choice option for tax-free salary payroll deduction is up to \$255 per month per employee for vanpool and rail transit pass fares through a voucher program (Commuter Check). Employees can now deduct up to \$3,060 a year from their salary as a pre-tax payroll deduction. This program encourages non-drive-alone commute trips. Employers also receive a tax savings as a benefit of this program.

The applicant will encourage tenants to offer this pre-tax option to their employees who utilize other transit resources such as VTA, Caltrain, or vanpools.

The federal law allows employers to give their workers up to \$255 each month for transit or vanpool commuting costs as a tax-free benefit. It allows employers to give employees the option to use payroll deductions to avoid paying taxes on up to \$255 a month in commuting costs. Alternatively, employers can share these costs with their workers by paying part of their monthly commuting costs and allowing workers to pay the balance using pre-tax dollars. Either way, both employers and their employees save money by participating in this simple plan.<sup>5</sup>

Direct transit or commute subsidies can be a set dollar amount or a percentage of the monthly costs of transportation. Employment sites that offer transit or commute subsidies generally tend to have higher levels of alternative mode-use. Subsidies can be provided in tandem with the pre-tax option.

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<sup>5</sup> [www.apta.com/research/info/online/paystoride.cfm](http://www.apta.com/research/info/online/paystoride.cfm)



A federal \$20 per month tax-free payroll deduction is available to bicycle commuters. Bicycle commuters can deduct up to \$240 per year in pre-tax bicycle expenses.

This information can be found in the Internal Revenue Code Section 132 (F), as amended by TEA-21, Title IX, Section 910.

### **Emergency Ride Home Program**

Tenants may implement an Emergency Guaranteed Ride Home (GRH) program for employees who use alternative forms of transportation. Employees who commute to work using transit, bicycle, or carpool or vanpool will be guaranteed a free ride home in the case of a personal emergency, or when they unexpectedly must work late thereby missing the last bus or their normal carpool home. The GRH program has proven very successful as it removes one of the major objections employees must giving up their private automobile, especially those with young families.

Other employer resources and TDM training resources include:



Association for Commuter Transportation (ACT)<sup>6</sup> – ACT supports individual mobility management professionals and organization members in their efforts to reduce traffic congestion, conserve energy, and improve air quality. The applicant may encourage tenants to join the local Northern California Chapter of ACT.

U.S. EPA Best Workplaces for Commuters (BWC)<sup>7</sup> is an innovative membership program that provides qualified employers and project sites with national recognition and an elite designation for offering outstanding commuter benefits, such as free or low-cost bus passes, strong telework programs, carpool matching, and vanpool subsidies. The applicant may encourage tenants to join as employer BWC worksites.



### **Carpool Incentive Programs<sup>8,9</sup>**

- **Free Preferential Parking for Carpools and Vanpools** – Parking for carpools and vanpools will be provided to commuters free of charge.
- **Carpool Rideshare Rewards** –Eligible carpools can earn gas or gift cards for every five days carpooled, up to \$100 over three months. *Rideshare Reward\$ for carpools are available from 511 for a limited period each year (typically in spring) and are provided on a first-come, first-served basis until funds are depleted (typically in June).*
- **Carpool (HOV) Lanes** – Carpool lanes, also known as high-occupancy vehicle (HOV) lanes, can reduce your commute time. To drive in HOV lanes during your commute, you

<sup>6</sup> [www.actweb.org](http://www.actweb.org)

<sup>7</sup> [www.bestworkplaces.org](http://www.bestworkplaces.org)

<sup>8</sup> [www.commute.org](http://www.commute.org)

<sup>9</sup> [www.511.org](http://www.511.org)

must be in a carpool, vanpool, public transit vehicle, or riding a motorcycle. Single-occupant hybrid vehicles are also permitted in carpool lanes during designated commute hours. HOV lanes vary in their hours of operation and the minimum number of people per car. A list of HOV hours of operation and required number passengers can be found at [www.rideshare.511.org](http://www.rideshare.511.org). A violation of the HOV lane use can result in a minimum \$381 fine. During non-commute hours, carpool lanes revert to general traffic use.

- **Park and Ride Lots** – There are 150 free park and ride lots conveniently located throughout the Bay Area, where you can meet carpool partners or your vanpool in a central location. Many lots also feature easy access to transit connections. You do not even need a car to use a park and ride lot, as many lots also offer bike lockers. Park and ride lot amenities and facilities vary, as does the availability of security. Use common sense and good judgment when choosing a lot and securing your vehicle. Vehicle safety is neither guaranteed nor implied by the 511 Regional Rideshare Program. Locations listed on this site may be operated by government agencies, private businesses or community organizations. You are encouraged to visit the lot before using it to review any posted information and call the lot operator for overnight/extended parking restrictions.<sup>10</sup>
- **San Mateo County Commuters (Only) You Pool, We Pay!** – Employees working at 706-716 Santa Cruz Avenue who live in or commute through San Mateo County, can participate in the “You Pool, We Pay!” program offered by the Commute.org. When employees form a new carpool with two or more people over the age of 18, or add a new member to an existing carpool, all carpool participants receive a \$50 gas or gift card incentive.

### Vanpool Incentive Programs

- **\$500 Gas Cards - New Vanpool Formation Incentive** – Newly formed vanpools are eligible to receive up to \$900 for starting a vanpool. Vanpooling is a less expensive, relaxing way to get to work, and the 511 Rideshare program offers the perfect incentive to start a vanpool – cash savings! The vanpool reward provides \$500 in gas cards to new vans that meet all eligibility requirements and successfully complete three to nine consecutive months of operation.

The gas cards are offered on a first-come, first-served basis, until the funds are exhausted. Employers and/or individuals who start a new vanpool may be eligible to receive the gas cards, which will be awarded to the party designated to handle the vanpool’s finances.<sup>11</sup>

- **\$300 Vanpool Seat Subsidy** – The 511 Regional Rideshare Program also offers a vanpool seat subsidy in the form of gas cards. The seat subsidy will provide \$100 per month, with

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<sup>10</sup> <http://rideshare.511.org/511maps/PandRText.asp>

<sup>11</sup> [http://rideshare.511.org/vanpooling/vanpool\\_incentives.asp](http://rideshare.511.org/vanpooling/vanpool_incentives.asp)

a limit of three months per van during the program year, to help cover the fare of a lost participant. The gas cards will be offered to eligible vans on a first-come, first-served basis, until the funds are exhausted.

- **San Mateo County (Only) \$500 New Vanpool Participant Rebates** – As an incentive for vanpooling, the Alliance will pay half of the cost for the first three months of vanpooling, up to \$100 per month per employee. New vanpool groups (or the driver) that stay on the road for at least six months can receive a one-time rebate of \$500. This one-time incentive is provided for those who join a new vanpool in the last six months and have not vanpooled for a three-month period before joining a new van.

### **Discounted Bridge Toll**

Commuters can save time and commute toll-costs by carpooling, vanpooling, or taking transit over one of the Bay Area's eight bridges during peak commute hours. Specific Bay Area bridge toll information can be found at [511.org](http://511.org). Discounted tolls are only available for carpools, hybrids, and hybrids with FasTrak, and when in designated HOV lane(s).

### **Transit Planning and Resources**

Online transit trip planning services are a useful tool for planning public transit trips. The greater San Francisco Bay Area is currently serviced by the [511.org](http://511.org) which is a useful tool for planning public transit trips. The Trip Planner can build an itinerary that suits the need of the transit user. An itinerary can be built that can identify the fastest commute, with the least amount of transfers or the cheapest fares. By default, the trip planner will generate the fastest itinerary between the origin and destination. This free service can be found online at [511.org](http://511.org).

### **Other Transit Planners**



Google has collaborated with select regional transit agencies to provide a public transit planner for riders of AC Transit and BART. This free service can be found online at [www.google.com/transit](http://www.google.com/transit).



Dadnab.com enables you to plan your transit trips in the Bay Area using text messaging from your mobile phone, by converting information from the 511 Transit Trip Planner to a text message. Send a text message with your origin, destination, and optional arrival or departure time, and Dadnab's reply will tell you what buses or trains to take, which locations and times, to assist you in reaching your destination.

### San Mateo County Commuters (Only) Try Transit Program

Commute.org offers a Try Transit Program that provides a limited number of free transit tickets to people who are interested in trying public transit to get to work. These tickets are meant for people who are new to transit. Commuters requesting tickets must work, live in, or drive through San Mateo County. Transit ticket options include:

- One BART ticket
- Three round-trip Caltrain tickets
- Water Ferry tickets
- Six one-way SamTrans tickets

### 511 Commuter Calculator

The *511 Commute Calculator* is a 511-sponsored online calculator that helps determine the hard cost of commuting by driving alone. The form asks for the number of miles traveled to work and what is paid for parking and gas, then the calculator estimates the commuting costs and vehicle CO<sub>2</sub> emissions. This free service can be found online at <https://511.org/carpool-vanpool/benefits/calculator>. This calculator may be linked with a commuter resource or HR page for employee use.

The screenshot shows a web interface titled "Benefits : Calculate Savings". It features a navigation bar with "Promotions", "County Specific", and a prominent green "Calculate Savings" button. Below the navigation bar, a heading reads "See how much lower your costs and CO2 emissions could be!". The main section is titled "Let's estimate your current commute costs" and contains several input fields:

Daily round trip commute	Amount	/mi
Work days per month	20	/mo
Your car's miles per gallon	Amount	/mpg
Price you pay for gas	\$ Amount	/gal
Daily bridge toll	\$ Amount	/day
Daily parking cost	\$ Amount	/day
Maintenance cost/mile	\$ 0.0609	/mo

At the bottom right of the form, there are two buttons: a grey "Reset" button and a green "Calculate" button.

# **TDM SPECIALISTS, INC. QUALIFICATIONS**





A Transportation Demand Management Company

**We are planners and technical experts** focused on development projects and improving employee mobility options. Our Transportation Demand Management (TDM) planning solutions reduce vehicle traffic, parking demand, greenhouse gases, and air pollution impacts. We work successfully with developers, employers, and government agencies to get TDM Plans approved and projects entitled. We also implement and manage on-site commuter programs and achieve required TDM goals.

Our TDM practitioners provide full-service commute and traffic mitigation, sustainable LEED planning, and air quality conformity. Serving as an extension of client staff, we provide a broad range of services to get the job done efficiently while meeting the unique needs of the client and specific jurisdiction.

**“We have finished the review of the Draft TDM. First let me say, that was the best TDM I have ever seen! The best by a large margin...a fantastic TDM Plan. Thank you so much.”**

*Steve Lynch, AICP, Senior Planner, City of Santa Clara, California*

### Transportation Demand Management

TDM Specialists develop Transportation Demand Management plans, traffic mitigation plans, and sustainable programs that address green commuting, mobility, and constrained parking issues. The purpose of TDM is to promote more efficient utilization of existing transportation facilities, reduce traffic congestion and mobile source emissions, and ensure that projects are designed in ways to maximize the potential for alternative transportation use.

### Commute Program Implementation

We have a proven track record of getting employees out of their cars. As projects are built and occupied, TDM Specialists can develop the structure, outreach and promotions necessary to implement and manage employee Commute Programs. The initial start-up, implementation, and ongoing management of the Commute Program are designed to meet TDM or trip reduction objectives and requirements. The overarching goal of a Commute Program is to enhance the quality of life and reduce commute trips for project employees.

*Quality of life improvements can enhance employee recruitment, morale and retention, and increase productivity that create positive benefits for businesses.*

### Sustainable Air Quality and Greenhouse Gas (GHG) Solutions

TDM Specialists successfully implements trip reduction programs tailored to fit the project, and can typically reduce employee trips to the site by 30 percent. This results in reduced drive-alone trips and complies with requirements to reduce project GHG impacts. We coordinate the mechanisms to calculate and report these results to appropriate agencies.

Contact:  
**Elizabeth L. Hughes**  
Senior Transportation Manager

**TDM Specialists, Inc.**  
5150 Fair Oaks Blvd, Suite 101-264  
Carmichael, CA 95608

**(408) 420-2411**  
elizabeth.hughes@tdmspecialists.com



*A Transportation Demand Management Company*

## Areas of Expertise

### Traffic Mitigation

TDM/TSM Mitigation Plans  
 TDM Employer Training  
 Commute Program Development  
 Commute Program Management  
 Commute Program Audits  
 Commuter Surveys  
 Transportation Fairs and Events  
 Car Management Strategies  
 Shuttle Programs  
 TMA Management

### Parking Mitigation

Parking Demand Reduction  
 Parking Management Strategies  
 Parking Constraints Solutions

### Entitlement

Project Support  
 Strategic Counsel  
 Critical Response Support  
 Environmental (EIR) Mitigation  
 (Air Quality and Transportation)

### Sustainability

Greenhouse Gas Emission Reductions  
 Supporting LEED Components  
 Air Quality Mitigation Plans

### TDM Applications

- Office or R&D buildings
- Corporate Headquarters/Campus
- Master Plan projects
- Specific Plans
- Business Parks
- Hospitals/Medical Offices
- Retail/Shopping Centers
- Residential (multi family, single family, hi-rise, etc.)
- Special Events
- Recreation
- Universities and Colleges
- Warehouse and Manufacturing
- Airports and Transit Stations

### Development, Property Management and Employer Projects

- Facebook
- Genentech
- NVIDIA
- SAP Labs
- Intel Folsom
- Intel Santa Clara
- Nokia
- Yahoo! Inc.
- NetApp
- VMware
- McClellan Business Park
- Juniper Networks
- Sunnyvale City Center
- Marvell
- Access/Palm Source
- Alexandria Real Estate Equities
- Oyster Point Business Park
- Metro Air Park
- Raley Field
- Moffett Park Business and Transportation Association
- Intuitive Surgical
- The Allen Group
- Spieker Properties
- HCP, Inc.
- Granite Regional Park
- Hyatt Place Hotel – So. San Francisco
- So. San Francisco Business Center
- Masonic Homes of California
- Fairview River Landing
- Donahue Schriber
- BioMed Realty Trust
- Panattoni Development
- Taylor Properties Development Co.
- SKS Investments, LLC
- Shorestein
- LBA Realty
- Jones Lang LaSalle
- California Farm Bureau
- California Highway Patrol
- Separovich • Domich
- Newell Real Estate Advisors
- LinkedIn
- Menlo Equities, LLC
- TMG Partners
- The Minkoff Group
- Arnell Enterprises, Inc.
- The Pollock Financial Group
- Wolff Enterprises

### Municipal & Agency Locations

- Sacramento Area Council of Governments
- California Highway Patrol
- County of Sacramento, Dept. of Human Services
- City of South San Francisco
- City of Mountain View
- City of Santa Clara
- City of Sunnyvale
- State of California, Dept. of General Services
- San Mateo City/County Association of Governments
- City of Union City
- Cal PERS
- Cal STRS
- Ogden City, UT
- City of Brisbane
- Grand Rapids Interurban Transit, MI
- City of Citrus Heights
- University of California San Diego West Campus
- Sacramento County International Airport

### Biotech, Pharmaceutical and Hospital Projects

- Genentech
- Amgen
- Rigel
- Takeda
- Onyx Pharmaceutical
- University of California San Diego, East Campus Medical Center
- Sutter Medical Center, Sacramento
- Mercy General Hospital
- Mercy San Juan Medical Center
- Enloe Medical Center
- Intuitive Surgical
- Blood Source
- Eclipsys, MA
- Counsyl, Inc.
- Theravance, Inc.