



SPECIAL AND REGULAR MEETING AGENDA

Date: 2/28/2023
Time: 5:30 p.m.
Locations: [Zoom.us/join](https://zoom.us/join) – ID# 833 2365 1341 and
City Council Chambers
751 Laurel St., Menlo Park, CA 94025

NOVEL CORONAVIRUS, COVID-19, EMERGENCY ADVISORY NOTICE

Consistent with Cal. Gov. Code §54953(e), and in light of the declared state of emergency, and maximize public safety while still maintaining transparency and public access, members of the public can listen to the meeting and participate using the following methods.

How to participate in the meeting

- Submit a written comment online up to 1-hour before the meeting start time:
city.council@menlopark.gov
Please include the agenda item number you are commenting on.
- Access the meeting real-time online at:
[Zoom.us/join](https://zoom.us/join) – Meeting ID 833 2365 1341
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(669) 900-6833
Meeting ID 833 2365 1341
Press *9 to raise hand to speak
- Watch meeting:
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Channel 26
 - City Council Chambers

Note: City Council closed sessions are not broadcast online or on television and public participation is limited to the beginning of closed session.

Subject to Change: Given the current public health emergency and the rapidly evolving federal, state, county and local orders, the format of this meeting may be altered or the meeting may be canceled. You may check on the status of the meeting by visiting the city website menlopark.gov. The instructions for logging on to the webinar and/or the access code is subject to change. If you have difficulty accessing the webinar, please check the latest online edition of the posted agenda for updated information (menlopark.gov/agendas).

According to City Council policy, all meetings of the City Council are to end by midnight unless there is a super majority vote taken by 11:00 p.m. to extend the meeting and identify the items to be considered after 11:00 p.m.

A. Call To Order

B. Roll Call

C. Agenda Review

D. Public Comment

Under “Public Comment,” the public may address the City Council on any subject not listed on the agenda. Each speaker may address the City Council once under public comment for a limit of three minutes. You are not required to provide your name or City of residence, but it is helpful. The City Council cannot act on items not listed on the agenda and, therefore, the City Council cannot respond to non-agenda issues brought up under public comment other than to provide general information.

E. Presentations and Proclamations

- E1. Proclamation: Recognizing Fran Dehn
Not a California Environmental Quality Act (CEQA) project.

F. Study Session

- F1. Provide direction on the development of a “Streetaries” outdoor dining program including program elements, design standards, fee schedule and street closures ([Staff Report #23-050-CC](#)) ([Attachment](#))
Not a CEQA project.
- F2. Provide direction on whether to pursue fully grade separated alternatives for Caltrain grade separation project ([Staff Report #23-048-CC](#)) ([Attachment](#))
Not a CEQA project.

G. Consent Calendar

- G1. Accept the City Council meeting minutes for January 31 and February 7, 2023 ([Attachment](#))
Not a CEQA project.
- G2. Award a construction contract to Anderson Pacific Engineering Construction, Inc. for the Chrysler Stormwater Pump Station Improvement project ([Staff Report #23-038-CC](#))
Determine this action is exempt from CEQA pursuant to CEQA Guidelines §15302 (Replacement and Reconstruction Projects).
- G3. Authorize the city manager to enter into a contract with Rincon Consultants Inc. to prepare the environmental analysis for the proposed Life Sciences development project at 980-1030 O’Brien Drive for the amount of \$137,459 and future augments as may be necessary to complete the environmental review for the proposed project ([Staff Report #23-039-CC](#))
Not a CEQA project.
- G4. Award a construction contract to Radius Earthwork Inc. for the Ravenswood Avenue Resurfacing project ([Staff Report #23-040-CC](#))
Determine this action is categorically exempt under CEQA Guidelines §15301 and §15304 exemption for existing facilities.

- G5. Adopt a resolution supporting the City's shuttle program for application for the San Mateo County Shuttle Program fiscal year 2023-24 and 2024-25 and authorize the city manager to enter into funding agreements ([Staff Report #23-041-CC](#))
Not a CEQA project.
- G6. Adopt a resolution to accept and appropriate a San Mateo County 2023 Summer Enrichment Grant in the total amount of \$13,000 to support and expand summer camp enrichment programs for children at the Belle Haven Youth Center ([Staff Report #23-046-CC](#))
Not a CEQA project.

H. Regular Business

- H1. Identify a preferred aquatics operator and authorize the city manager to negotiate an agreement for an aquatics operator at Burgess Pool and the future Menlo Park Community Campus aquatics center; and form an ad hoc City Council subcommittee to advise the agreement negotiation process ([Staff Report #23-047-CC](#)) ([Attachment](#))
Not a CEQA project.
- H2. Appoint a City Council liaison to the Finance and Audit Committee ([Staff Report #23-045-CC](#))
Not a CEQA project.

I. City Council Initiated Items

- I1. Direction on City Council role in San Mateo County related animal control efforts ([Staff Report #23-042-CC](#))
Not a CEQA project.

J. Informational Items

- J1. City Council agenda topics: March 14 – 28, 2023 ([Staff Report #23-051-CC](#))
Not a CEQA project.
- J2. Annual City Council priority setting workshop March 18, 2023 ([Staff Report #23-043-CC](#))
Not a CEQA project.
- J3. Proposed process and timeline to develop a focused addendum to the Parks and Recreation Facilities Master Plan to include pickleball ([Staff Report #23-044-CC](#))
Not a CEQA project.
- J4. Expiration of local emergencies ([Staff Report #23-049-CC](#))

K. City Manager's Report

L. City Councilmember Reports

M. Closed Session

- M1. CONFERENCE WITH LEGAL COUNSEL—EXISTING LITIGATION
Paragraph (1) of subdivision (d) of Section 54956.9)

Name of case: Tobias Kunze and Liliana Kunze Briseno v. City of Menlo Park (Case No. 3-CIV-00590)
Not a CEQA project.

N. Adjournment to Closed Session

At every regular meeting of the City Council, in addition to the public comment period where the public shall have the right to address the City Council on any matters of public interest not listed on the agenda, members of the public have the right to directly address the City Council on any item listed on the agenda at a time designated by the chair, either before or during the City Council's consideration of the item.

At every special meeting of the City Council, members of the public have the right to directly address the City Council on any item listed on the agenda at a time designated by the chair, either before or during consideration of the item. For appeal hearings, appellant and applicant shall each have 10 minutes for presentations.

If you challenge any of the items listed on this agenda in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City of Menlo Park at, or before, the public hearing.

Any writing that is distributed to a majority of the City Council by any person in connection with an agenda item is a public record (subject to any exemption under the Public Records Act) and is available by request by emailing the city clerk at jaherren@menlopark.gov. Persons with disabilities, who require auxiliary aids or services in attending or participating in City Council meetings, may call the City Clerk's Office at 650-330-6620.

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STAFF REPORT

City Council

Meeting Date:

2/28/2023

Staff Report Number:

23-050-CC

Study Session:

Provide direction on the development of a “Streetaries” outdoor dining program including program elements, design standards, fee schedule and street closures

Recommendation

Staff recommend that the City Council review the proposed “Streetaries” outdoor dining program including the following items:

1. Draft ordinance adding Chapter 13.30 Streetaries Outdoor Dining Areas to Title 13 (Streets, Sidewalks and Utilities) and draft ordinance Amending Sections 13.18.10 and 13.18.20 of Chapter 13.18 (Use of Public Right of Way) (Attachment A)
2. Draft Streetaries Outdoor Dining Design Standards (Attachment B)
3. Draft Streetaries Outdoor Dining fee schedule (Attachment C)

In addition, staff recommend that the City Council provide feedback on next steps for road closures enacted on portions of Santa Cruz Avenue and Ryans Lane during the pandemic that enabled outdoor dining in streets. See Attachment D for potential transitional road reopening options.

No action is required by the City Council for this study session. Feedback on the proposed Streetaries outdoor dining program is a preliminary step in introducing the ordinances, design standards, and other necessary program elements at a future City Council meeting.

Policy Issues

In June 2020, the City Council adopted an urgency ordinance to establish a temporary outdoor use permit (TOUP) program as well as to close portions of Santa Cruz Avenue and Ryans Lane for pedestrian use. This ordinance is set to expire with the end of the California COVID-19 State of Emergency Order. Governor Gavin Newsom announced in October 2022 that the COVID-19 State of Emergency Order will expire by the end of February 2023. The decision to establish the TOUP program was exempt from California Environmental Quality Act (CEQA) due to the temporary emergency nature of the decision.

The proposed Streetary outdoor dining program establishes a long-term policy including design standards, and permitting, operational, and maintenance regulations for outdoor dining in the public right-of-way. The goal of the proposed Streetary outdoor dining program is to maintain the economic activity gained by businesses that implemented parklets while strengthening safety standards, creating a consistent design aesthetic, and ensuring fairness in the way the public right-of-way is used.

Background

The City Council's actions to mitigate the economic impacts of COVID-19 on the local economy helped many businesses remain viable while operating in a safe manner. The City Council has considered the topic of outdoor dining, parklets and outdoor use permits multiple times and adopted a number of urgency ordinances since the start of the COVID-19 pandemic as summarized below.

- 2015 Santa Cruz Street Café Pilot Program: In 2015, the City Council approved the Santa Cruz Street Café pilot program to allow merchants to convert street parking to parklets for outdoor uses. The program included city approved designs by Ian Moore Designs and the City Council agreed to a cost-sharing structure that paid for part of the base design of the parklets and for installation of the improvements. Owners of the parklets signed license agreements that expired in 2021 and are now operating on a month-to-month basis through the TOUP program.
- June 19, 2020 – Urgency Ordinance No. 1070: Allowed staff to implement the TOUP program and close portions of Santa Cruz Avenue for pedestrian use.
- July 16, 2020 – Urgency Ordinance No. 1071: Expanded the uses eligible for a TOUP.
- September 8, 2020 – Urgency Ordinance No. 1072: Extended the TOUP and street closure through February 28, 2021.
- October 6, 2020 – Urgency Ordinance No. 1073: Expanded the TOUP program to include:
 - Install “walk your bike” signs on closed portions of Santa Cruz Avenue
 - Allow use of travel lanes without limitations to time of day or day of week
 - Close a portion of Ryans Lane between Crane Street and Escondido Lane
- February 23, 2021 – Ordinance No. 1085: Extended TOUP program and street closure through January 2022.
- January 25, 2022: Extended the TOUP program to allow parklets/street cafes in off-street parking spaces and to remain on a month-to-month basis; parklets/street cafes in off-street parking spaces set to expire once the COVID-19 State of Emergency Order is lifted.
- January 10, 2023: Staff presented draft outdoor dining design standards as an informational item to the City Council and public.

Analysis

Existing outdoor uses

There are 12 food service establishments with existing outdoor uses. In addition, there are five retail/personal service businesses with existing outdoor uses. Seven of the 17 existing parklets were built as part of the Santa Cruz Street Café Pilot Program (five of the pilot parklets expanded when the TOUP program was established) and the rest were added after the TOUP program was established.

Table 1: Food service establishments		
Food service establishment name	Address	Outdoor use description / status
1. Amici's	880 Santa Cruz Ave.	Sidewalk café and seating in on-street parking
2. Bistro Vida	641 Santa Cruz Ave.	Santa Cruz Street café parklet plus covered structure and extended parklet beyond frontage, and seating in closed travel lane
3. Cafe Zoe	1929 Menalto Ave.	Outdoor dining in public/private parking spaces
4. Camper	898 Santa Cruz Ave.	Santa Cruz Street café parklet plus new parklet on University Drive with covered structure
5. Carpaccio	1122 Crane St	Outdoor dining on closed section of Ryans Lane adjacent to restaurant
6. Coffeebar	1149 Chestnut St	Outdoor dining on Chestnut Avenue on-street parking and in parking plaza #2 (5-spots)
7. Bon Marche Farmer's Market	643 Santa Cruz Ave.	Weekly farmers market event on Wednesdays from 3 p.m. – 8 p.m.
8. Galata	827 Santa Cruz Ave.	Santa Cruz Street café parklet plus covered structure and extended parklet, and sidewalk café
9. Left Bank	635 Santa Cruz Ave.	Santa Cruz Street café parklet plus sidewalk café and seating and umbrellas in closed travel lane
10. Mademoiselle Colette	812-816 Santa Cruz Ave.	Santa Cruz Street café parklet
11. Roma	820 Santa Cruz Ave.	Santa Cruz Street café parklet plus covered structure and extended parklet
12. Stacks	600 Santa Cruz Ave.	Parklet in on-street parking

Table 2: Retail businesses		
Food service establishment name	Address	Outdoor use description / status
1. Art Ventures Gallery	888 Santa Cruz Ave.	Art sculptures in on-street parking
2. Charisma Salon	1143 Crane St	Tents covering seating and tables on parking spots in public parking lot
3. Fleet Feet	859 Santa Cruz Ave.	Parklet platform with cement planter barriers, sometimes used to place clothes and shoes, otherwise used for events
4. Gitane	855 Santa Cruz Ave.	Parklet platform with cement planter barriers and seating
5. Harvest	639 Santa Cruz Ave.	Santa Cruz Street Café Parklet used to display furniture from store

There are other businesses in Menlo Park with outdoor seating in some capacity approved through other zoning processes to which the proposed Streetary program would not apply.

Business outreach

The City’s economic development consultant, HdL Companies (HdL), communicated with many of the owners or representatives for the businesses listed above. Outreach regarding the proposed outdoor dining program was initially conducted by email in December 2022 and followed up with in-person visits to businesses. At that time, HdL spoke with the owners/managers of Art Ventures, Carpaccio, Camper and

Café Zoe. Before the City Council meeting January 10, 2023, HdL sent a notice of the outdoor dining agenda item to TOUP owners. On Tuesday, January 17, 2023, Assistant City Manager Stephen Stolte and HdL Senior Advisor Kirstin Hinds conducted in-person visits to downtown TOUP owners and spoke with Camper, Carpaccio, Bistro Vida, Gitane and Fleet Feet. HdL also spoke with CoffeeBar and Left Bank over the phone in the following weeks.

The following bullet points summarize the feedback on the proposed outdoor dining program received from businesses with existing outdoor uses:

- They are in favor of establishing a longer-term outdoor dining program.
- A few restaurants indicated they are not fully recovered from the pandemic financially, but outdoor dining helped them stay open while customers chose outdoor dining over indoor.
- Businesses indicated their customers continue to choose outdoor seating over indoor, even in the cooler winter months.
- Several inquired about a cost sharing aspect, such as providing barriers or waiving permit fees. Two businesses also asked for assistance in identifying what they need to do to their existing parklets to comply with the proposed design criteria.
- Retail businesses with outdoor uses provided mixed feedback, though there was general agreement that outdoor dining should continue. One retailer indicated that they enjoy the outdoor use/parklet space in place of parking and would like to see retail included in a longer-term program, another enjoys using their parklet on a weekly or monthly basis for events but does not need to use it all of the time, and another said they have not used their outdoor space.
- Businesses on Santa Cruz Avenue expressed interest in keeping the street closed because it allows them to expand business operations, though they acknowledged that it would be okay if the City reopened the street. Outside of this business outreach, city staff have heard interest from various groups and individuals in street reopening.
- Carpaccio expressed interest in being able to continue operating an outdoor dining parklet in Ryans Lane. A few neighboring businesses have expressed interest in reopening Ryans Lane.

Streetary outdoor dining program

“Streetary” or “Streetaries” are defined in the proposed ordinance as an outdoor eating area that operates within and uses the public right-of-way directly adjacent to the food service establishment street frontage. The Streetary outdoor dining program would establish guidelines, requirements, and operational regulations for outdoor dining in the public right-of-way, including on sidewalks and in public parking spaces on the side of the street or in public parking lots.

The proposed Streetary program intends to:

1. Activate the public realm in Downtown Menlo Park and other commercial areas.
2. Maintain physical and visual access to Menlo Park businesses.
3. Provide safe, attractive, and accessible spaces for outdoor use.
4. Provide accessible amenity areas for private businesses during operating hours.
5. Support the economic vitality and growth of businesses and the City.

The following are key features of the proposed Streetary outdoor dining program:

- Since the Streetary ordinance pertains to outdoor dining in the public right-of-way, the public works department is set as the program lead. Public works will manage permit intake, circulation of applications to other departments and agencies (planning and building divisions, Menlo Park Fire District), final sign off/approval, and program enforcement. The planning division will review applications for design

compliance. When construction is involved, the building division will review Streetary permits and conduct inspections.

- Outdoor uses will be an extension of a food service establishment's operation for private use only and limited to outdoor dining.
- To streamline the review process for new Streetary applications, the proposed guidelines include Streetary parklet site plan and design templates for use by applicants. Applicants can create their own Streetary designs as long as the proposed designs are within the design criteria outlined in the guidelines. Sidewalk cafes can be unique to the business but must be set up in accordance with the required setbacks and furnishing and fixture design criteria.
- Application for Streetary permits will require both business and property owner consent. In situations where a shared Streetary is desired, all businesses and property owners fronting the Streetary must provide consent.
- Streetaries will be limited to the business's storefront. Existing parklet owners who obtained approval from the City to expand beyond their store frontage will be permitted to remain through and exception in the Streetary ordinance.
- Fabric awnings, umbrellas, and other soft retractable material is encouraged to provide shade and limited weather protection while also allowing light in. Applications for overhead structures may be permitted but will require engineered drawings. These overhead structures go beyond the template parameters and require more staff time to review and process.
- Hours of operation for outdoor business activity must coincide with the business's hours of operation, except outdoor business activity shall not take place beyond 11 p.m. on weeknights, midnight Saturday, and 10 p.m. Sunday.
- Streetary Permits are non-transferable, and any new business requesting to use a previous businesses' Streetary must reapply for a permit.
- Maintenance of the Streetary area and utilities/drainage is clearly laid out and the permittee will be required to ensure their space is safe, free of debris, grime and graffiti, and landscaping or plants must be maintained and in good health.
- The initial permit term will be three years, followed by an annual renewal component. As a condition of the issuance of a Streetary encroachment permit, after the initial three-year term, the permittee will be required to renew their permit annually by submitting insurance and confirmation that the Streetary is in the same condition as when it was approved. Any Streetary may be subject to inspection by the City on an annual or as needed basis.
- Barriers will be standardized through safety standards set forth in the design standards, as well as the limitation of exterior materials (painted/finished wood or concrete planters, steel planters or perforated steel railings; colors and materials should be compatible with the façade of the adjacent building/restaurant.)

Fee structure

Below is a proposed fee structure for the Streetary outdoor dining program, which staff recommend be waived until January 1, 2025. Justification for the proposed fees is described below.

Table 3: Proposed “Streetary” outdoor dining permit fees: parklet			
“Streetary” parklet	First year	Annual renewal	Notes
Streetary permit - parklets	\$1,725	\$250	First year permit application same as administrative permit fee for outdoor dining on private properties. Applicants may be subject to a surcharge for a structural review. Renewal fee covers administration.
Use of space (estimated at \$4.24/SF for 360 SF, total may vary)	\$1,526.40	\$1,500	Use of space recommended cost is \$4.24/SF for an estimated average of 360 SF per 2-stalls. Total use of space may vary depending on total area occupied by the business. Renewal fee covers use of space.
Recommended cost for Streetary parklets	\$3,251.40	\$1,750	Staff recommend fees are waived until January 1, 2025, if business complies by the end of the transitional period described below.

Table 3: Proposed “Streetary” outdoor dining permit fees: sidewalk café			
“Streetary” sidewalk café	First year	Annual renewal	Notes
Streetary permit - sidewalk cafes (stand-alone)	\$810	\$250	First year permit application same as a major encroachment permit fee. Renewal fee covers administration.
Use of space (estimated at \$4.24/SF for 150 SF, total may vary)	\$636	\$600	Use of space recommended cost is \$4.24/SF for an estimated 150 SF per sidewalk café. Total use of space may vary depending on total area occupied by the business. Renewal paid annually for use of space
Recommended Cost for Streetary sidewalk cafes	\$1,446	\$850	Staff recommend fees are waived until January 1, 2025, if business is in compliance by the end of the transitional period described below.

Justification for fees

- First year permit fee: The first year fee covers permit review and design compliance review. The first year Streetary parklet permit fee is based on the cost of an administrative permit fee for outdoor dining on private property, due to similarities in considerations and review. The first year Streetary sidewalk café fee is based on the major encroachment permit fee.
- First year use of space: In determining a first year use of space fee, HdL researched retail rent per square foot in Menlo Park. Of the properties for lease, the average rent was \$4.24 per square foot. The average size of a two-stall parklet (which is the maximum size of the parklet templates) is 360 square-feet, bringing the estimated first year use of space fee for a parklet to \$1,526.40. The average size of a sidewalk café is approximately 150 square feet, and using the same price per square foot, the estimated first year use of space fee for a sidewalk café comes to \$636. Total use of space fees may vary depending on the total square footage of the proposed Streetary area per business. The use fee will help offset any additional administrative efforts to implement the program.
- Annual renewals: The annual permit renewal fee of \$250 includes annual submittal of insurance and confirmation that the Streetary complies by the Streetary owner. The annual use of space renewal fees for Streeteries are rounded down to \$1,500 for parklets and \$600 for sidewalk cafes but are still above \$4 per square foot of space.
- Proposed fee waiver: Small businesses have still not recovered to pre-pandemic cash flow and revenue margins, particularly for restaurants that were operated at a small percentage of normal capacity for much of the last few years. To help Menlo Park businesses continue to recover from the pandemic and

manage increased costs of doing business, staff recommend that fees be waived until January 1, 2025, for businesses who are in compliance with the Streetary program by the end of the transitional period as described below. On and after January 1, 2025, first year permit fees will apply to new Streetary applicants and the annual renewal fees will apply to existing Streetaries once their initial 3-year term is reached.

In considering a potential fee structure, staff reviewed the outdoor dining and parklet program fee structures established by several cities in the area, summarized in Attachment C. The Streetary program proposes fees that are lower than the average permit fees for the comparison cities. In comparable cities, the average amount for permit and first year use fees are \$4,084.40, and the average amount for renewal and cleaning fees is \$4,273.91. Several of the comparison cities waive permit or use fees for at least one year. Some cities set cleaning fees, however Menlo Park's proposed Streetary program sets cleaning as a responsibility of permittee through the operational and maintenance standards.

Transitional period for businesses with existing outdoor dining

A transitional period is proposed for the implementation of a new Streetary program that would apply to existing outdoor dining parklet and sidewalk café owners, starting from the adoption date of the Streetary program.

- One-month period for staff to create Streetary application and provide notice to existing permit holders: Immediately following the adoption of the program, staff will build the Streetary permit application in Menlo Park's online permitting system, Accela. HdL will also begin communication and outreach to existing parklet owners. Once the application is created and accessible on Accela, HdL will send notice to the existing parklet owners.
- Three-month open application period: Once the permit application available, existing outdoor dining parklet and sidewalk café owners will be allowed three months to apply for a Streetary Permit. During that time, HdL will provide application assistance.
- Six-month compliance period for Streetary permit applicants: For existing outdoor dining parklet and sidewalk café owners that apply for outdoor dining within the open application period, the proposed transition timeline includes a six-month period to allow for improvements or construction to meet the new guidelines. A waiver or release from businesses may be required during the transitional period between expiration of the existing TOUP permit and the effective date of the new Streetary permit. If the necessary improvements are not completed within the six-month compliance period, outdoor uses must be removed. First year permit fee waivers will apply to businesses in compliance by the end of this period.
- Seven-day compliance period for non-applicants: If an application is not submitted by the end of the three-month open application period, the parklets, sidewalk cafes, or other outdoor uses that are not allowed through other City programs must be removed within seven days of the end of the open application period. HdL will communicate this deadline to businesses, and the public works department will coordinate removal of any existing barriers.

Consideration of future grant funding for transitional period

Staff are researching a possible grant funding opportunity to help businesses with existing parklets transition to meet the Streetary outdoor dining program design standards, similar to grants distributed through the TOUP program, for City Council consideration in the future.

Transitional period for street reopening

The Menlo Park City Council relied on its emergency authorities to adopt the Downtown street closures with the understanding that closures were temporary and would need to be reassessed and adjusted periodically. The emergency authorities are tied to the COVID-19 State of Emergency Order, which is set to expire. The City Council continues to have the authority to maintain street closures beyond the emergency period for up to 18 months.

It is important to note that some businesses support street reopening and others that currently benefit from street closures would generally prefer continued street closure. Staff recommend that the City Council consider a transitional period for street reopening so that businesses can remove items from the streets, and the City can clean, remove barriers and assess safety. The Fire District also has barriers in the roadway that would need to be removed. Below are options for a transition to reopening for Santa Cruz Avenue and Ryans Lane. Additional options beyond the below proposals may need to be reviewed by the Complete Streets Commission.

Santa Cruz Avenue

There are three existing parklets on Santa Cruz Avenue at Left Bank, Harvest and Bistro Vida. Left Bank and Bistro Vida expanded their outdoor use into the travel lane. Reopening the street will require the travel lane to be free and clear of all items. Additionally, on a weekly basis the Bon Marche Farmer's Market has occupied the 600-block Santa Cruz Avenue street closure in front of Walgreens and Starbucks. The City may not be able to accommodate a weekly closure for the Bon Marche Farmer's Market due to limited staff capacity to remove and replace barriers on a weekly basis. Three proposed options for a transitional period to reopening the street include:

1. Reduce the size of the street closure for four months to allow for a bicycle travel lane during transition. A reduction in the size of the street closure would require tents, tables, and chairs to be removed from the street, and barriers to be relocated to allow for six feet of bicycle travel. This would allow Bon Marche Farmer's Market to continue operations on a weekly basis, at a slightly smaller capacity, during the transitional period.
2. Maintain current street closures for four months and prepare for reopening at the end of the transitional period. This option would allow businesses to maintain operations for a period of time while staff determine capacity for cleanup, inspections and removal of barriers. This option would also allow Bon Marche Farmer's Market to continue operations during the transitional period.
3. Reopen the street to vehicular traffic by a specific date. In this case, staff would need to prioritize the reopening of the streets by setting a deadline for businesses to clear out their items, and then plan for the cleaning, inspections for safety and removal of barriers. It is not likely that the Bon Marche Farmer's Market would continue operations on a weekly basis following the reopening of Santa Cruz Avenue due to staff capacity to remove and replace barriers on a weekly basis.

Ryans Lane

The Ryans Lane closure has allowed Carpaccio to expand dining into the travel lane, as there is no parking in front of the business on Crane Street to accommodate an outdoor dining parklet. Carpaccio has expressed interest in continuing their outdoor dining in Ryans Lane. Staff have also received interest from two nearby businesses to reopen Ryans Lane. A new restaurant will be locating in the former Ann's Coffee Shop space at 772 Santa Cruz Avenue, and may need access to Ryans Lane for construction purposes. Additionally, Carpaccio would need the consent of the adjacent new businesses to continue outdoor dining operations in Ryans Lane. Since Ryans Lane would not be able to easily accommodate a bicycle lane, two proposed options for a transitional period to reopening the street include:

1. Maintain current street closures for four months and prepare for reopening at the end of the transitional period. This option would allow Carpaccio to maintain operations for a period of time while staff determine capacity for cleanup, inspections and removal of barriers.

2. Reopen the street to vehicular traffic by a specific date. In this case, staff would need to prioritize the reopening of the streets by setting a deadline for businesses (Carpaccio in this instance) to clear out their items, and then plan for the cleaning, inspections for safety and removal of barriers.

Graphics of the potential transitional periods for street reopening are included in this packet as Attachment D.

Alignment with Specific Plan

In alignment with the El Camino Real and Downtown Specific Plan Section D – Public Space, the proposed outdoor dining activity guidelines are specific to outdoor dining and the standards identify the functional zones of the sidewalk and parking / travel lane to help establish locations for parklets and sidewalk cafes. The El Camino Real and Downtown Specific Plan identifies a need to improve the pedestrian realm on Santa Cruz Avenue, which includes sitting and outdoor dining options in functional sidewalk zones. The plan establishes sidewalk functional zones, ensuring a pedestrian clear zone and more pleasant and functional sidewalks. It redistributes the right-of-way between traffic lanes, on-street parking, and sidewalks in order to focus on an enhanced pedestrian experience while still accommodating vehicular circulation and on-street parking. Per the Santa Cruz Avenue Sidewalks section (D10) of the Specific Plan, Activities outside the building, such as outdoor dining, can enliven adjacent sidewalks. These are desirable attributes in areas with high levels of activity such as the downtown and station area.

Impact on City Resources

Staff are proposing that the Streetary permit fees be waived until January 1, 2025, for the existing parklets that come into compliance by the end of the transitional period described above. Staff are also proposing an initial permit term of three years, followed by an annual renewal, which would require the first renewal payment in 2026 for existing parklets that apply for a Streetary permit in 2023. At most, staff anticipate 12 applicants, which would mean the City could subsidize around \$40,000 in permit fees (\$3,251.40 per applicant) for the first year and \$21,000 in annual renewal fees for the next two years of the program during the transitional / fee waiver period.

The City of Menlo Park provides services and infrastructure that contribute to quality-of-life for all Menlo Park residents. Streetary permit fees would be subsidized at a mid-level of cost recovery in acknowledgement that the program benefits participating businesses, activates the public realm, and supports economic vitality of businesses and Menlo Park as a whole.

Environmental Review

This study session is not a project within the meaning of the CEQA Guidelines §§ 15378 and 15061(b)(3) as it will not result in any direct or indirect physical change in the environment. The ordinances and design standards necessary to the proposed Streetary program are separately evaluated by staff for CEQA.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

- A. Draft ordinances adding Chapter 13.30 (“Streetaries” Outdoor Dining Areas) to Title 13 (Streets,

Sidewalks and Utilities) and amending Sections 13.18.10 and 13.18.20 of Chapter 13.18 (Use of Public Right of Way)

- B. Draft “Streetaries” outdoor dining design standards
- C. Draft Streetaries outdoor dining fee structure
- D. Graphic of potential transitional period for street reopening

Report prepared by:

Kirstin Hinds, Senior Advisor, HdL Companies
Stephen Stolte, Assistant City Manager

ORDINANCE NO. XXXX**E OF THE CITY COUNCIL OF THE CITY OF MENLO PARK ADDING CHAPTER 13.30 (“STREETARIES” OUTDOOR DINING AREAS) TO TITLE 13 (STREETS, SIDEWALKS AND UTILITIES) OF THE MENLO PARK MUNICIPAL CODE; AMENDING SECTIONS 13.18.10 and 13.18.20 OF CHAPTER 13.18 (USE OF PUBLIC RIGHT OF WAY), OF THE MENLO PARK MUNICIPAL CODE RELATING TO OUTDOOR DINING AREAS IN THE PUBLIC RIGHT-OF-WAY**

WHEREAS, the City Council has determined that outdoor eating areas within certain of the City’s rights-of-way provide economic vitality to the City and businesses, create community gathering spaces, contribute to the enjoyment of public spaces, and increase opportunities for more enjoyable pedestrian travel in the City; and

WHEREAS, the City Council desires to permit outdoor eating areas within parking spaces and sidewalks of the public right-of-way – known as “Streetaries” – through Streetary Encroachment Permits; and

WHEREAS, in May 2016 the City Council approved outdoor dining in the public right-of-way as part of the Santa Cruz Street Café Pilot Program; and

WHEREAS, seven merchants currently operate outdoor dining facilities authorized by the Santa Cruz Street Café Pilot Program;

WHEREAS, between June 19, 2020 and February 23, 2021, in response to the COVID-19 Pandemic and the California COVID-19 State of Emergency Order, the City Council approved Urgency Ordinances No. 1070, 1071, 1072, 1073, and 1085 establishing a Temporary Outdoor Use Permit Program and closing portions of Santa Cruz Avenue and Ryans Lane for pedestrian use; and

WHEREAS, twelve total merchants currently operate outdoor dining facilities authorized by the Santa Cruz Street Café Pilot Program or the Temporary Outdoor Use Permit Program; and

WHEREAS, the City Council desires to transition pre-existing outdoor dining established into longer term facilities once current permits expire; and

WHEREAS, on _____ 2023, staff presented Draft Streetary Guidelines to the City Council setting forth staff’s recommendations for design and development standards, operational standards, fees, and other related requirements for new Streetary Encroachment Permits that would apply after current permits expire; and

WHEREAS, the City Council has determined that the Public Works Director should be the City’s designated representative to issue Streetary Encroachment Permits because of their locations within sidewalks and right of way currently used for vehicular parking; and

WHEREAS, the City Council aims to balance the desire for outdoor dining in the public right-of-way with adequate public parking and pedestrian and bicycle access; and

WHEREAS, pursuant to Vehicle Code section 22507(a), cities may, by ordinance or resolution, restrict or otherwise remove parking from city streets during all or certain hours of the day; and

WHEREAS, the proposed Ordinance to add Chapter 13.30 (“Streetaries” Outdoor Dining Areas) to Title 13 and amend corresponding sections of Title 13 of the Menlo Park Municipal Code relating to outdoor dining in the public right-of-way through Streetary Encroachment Permits qualifies for the exemptions set forth in CEQA Guidelines Sections 15304 and 15305 and statutory exemption 15183; and

WHEREAS, the City Council held a duly noticed public hearing, considered all public comments on the revisions and related CEQA exemptions, and determined that the amendments are consistent with the General Plan and that the revisions would be internally consistent with all other provisions of the Menlo Park Municipal Code.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MENLO PARK DOES ORDAIN AS FOLLOWS:

Division 1. The above findings are adopted and incorporated herein.

Division 2. Addition of Chapter 13.30 to the Menlo Park Municipal Code.

Chapter 13.30 – “Streetaries” Outdoor Dining Areas is hereby adopted and made a part of Title 13 (Streets, Sidewalks and Utilities) of the Menlo Park Municipal Code as set forth in Exhibit A, attached hereto and incorporated herein by this reference.

Division 3. Amendments of Section 13.18.10 of Chapter 13.18 (USE OF PUBLIC RIGHTS-OF-WAY) of the Menlo Park of Menlo Park Municipal Code.

Section 13.18.10 (DEFINITIONS) of Chapter 13.18 (USE OF PUBLIC RIGHTS-OF-WAY) of Title 13 (STREETS, SIDEWALKS, AND UTILITIES) of the Menlo Park Municipal Code is hereby amended to add subsection (10) to section 13.18.010 as set forth below. Deletions are in ~~strike through~~, and additions are in underline.

13.18.010 Definitions.

For the purposes of this chapter, the following terms, phrases, words and their derivations shall have the meaning given in this section.

...
(10) “Streetary” or “streetaries” shall mean an outdoor eating area that operates within and uses parking spaces and/or sidewalks within the public right-of-way directly adjacent to the food service establishment street frontage.

Division 4. Amendments of Section 13.18.20 of Chapter 13.18 (USE OF PUBLIC RIGHTS-OF-WAY) of the Menlo Park of Menlo Park Municipal Code.

Section 13.18.20 (PERMIT REQUIRED) of Chapter 13.18 (USE OF PUBLIC RIGHTS-OF-WAY) of Title 13 (STREETS, SIDEWALKS, AND UTILITIES) of the Menlo Park Municipal Code is hereby amended to add subsection (d) to section 13.18.20 as set forth below. Deletions are in ~~strike through~~, and additions are in underline.

13.18.020 Permit required.

...

(d) Notwithstanding subsections (a) and (b), above, an encroachment permit shall not be required for streetaries that have obtained and maintain a streetary encroachment permit pursuant to section 13.30.020.

Division 5. Compliance with California Environmental Quality Act (CEQA).

The City Council finds that this Ordinance is exempt from the provisions of the CEQA pursuant to Sections 15304 (Class 4) and 15305 (Class 5).

Class 4 Categorical exemptions (Minor Alterations to Land) *consist* of minor public or private alterations in the condition of land, water, and/or vegetation which do not involve removal of healthy, mature, scenic trees except for forestry or agricultural purposes, including “minor temporary uses of land having negligible or no permanent effects on the environment . . .” (CEQA Guideline § 15304(e).) This ordinance will involve the temporary, non-permanent use of land; all uses authorized pursuant to this ordinance would involve temporary, non-permanent improvements in the public right of way including temporary structures, tables and seating.

Class 5 categorical exemptions (Minor Alterations in Land Use Limitations) covers projects that consist of minor alterations in land use limitation in areas with an average slope of less than 20%, which do not result in any changes in land use or density including but not limited to issuance of minor encroachment permits. This ordinance will result in the issuance of licenses and/or permits for use and encroachment into the public right of way.

The proposed project is consistent with these exemptions, because the requested action will not result in a direct or reasonably foreseeable change in the environment and because there is no possibility that the activity in question may have a significant effect on the environment.

Division 6. Severability.

The City Council hereby declares every section, paragraph, sentence, cause, and phrase of this ordinance is severable. If any section, paragraph, sentence, clause, or phrase of this ordinance is for any reason found to be invalid or unconstitutional, such invalidity or unconstitutionality shall not affect the validity or constitutionality of the remaining sections, paragraphs, sentences, clauses, or phrases.

Division 7. Publication; Effective Date.

This Ordinance shall be published once, in full or in summary form, after its final passage, in a newspaper of general circulation, published, and circulated in the City of Menlo Park, and shall be in full force and effect thirty (30) days after its final passage. If published in summary form, the summary shall also be published within fifteen (15) days after the adoption, together with the names of those City Councilmembers voting for or against same, in a newspaper of general circulation published and circulated in the City of Menlo Park, County of San Mateo, State of California.

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INTRODUCED on the ___ day of ___, 2023.

PASSED AND ADOPTED as an ordinance of the City of Menlo Park at a regular meeting of said City Council on the ___ day of ___, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

APPROVED:

Jen Wolosin, Mayor

ATTEST:

Judi A. Herren, City Clerk

Exhibits:

- A. "Streetaries" Outdoor Dining Areas is hereby adopted and made a part of Title 13 (Streets, Sidewalks and Utilities)

Chapter 13.30 – “STREETARIES” OUTDOOR EATING AREAS

13.30.010 - Definitions.

For the purposes of this chapter, the following words and phrases shall have the meaning set forth below. Unless in direct conflict with terms defined herein, other words and phrases shall be interpreted as defined in the Streets and Highways Code:

- A. “Director” shall mean the Public Works Director or their designee.
- B. “Food service establishment” shall mean businesses primarily engaged in serving prepared food and/or beverages for consumption on or off the premises.
- C. “Parking spaces within the public right-of-way” shall mean the designated on-street or off-street area where parking of vehicles does not violate any posted parking restriction and the use of the space does not interfere with the safety nor the passage of persons and vehicles.
- D. “Permittee” shall mean the permit holder of a valid streetary encroachment permit issued by the city.
- E. “Sidewalk” shall mean any public right-of-way designated for pedestrian access.
- F. “Streetary” or “streetaries” shall mean an outdoor eating area that operates within and uses parking spaces and/or sidewalks within the public right-of-way directly adjacent to the food service establishment street frontage.

13.30.020 - Permit required.

Any person desiring to erect, construct, place or maintain an encroachment upon any City sidewalk and/or existing parking spaces within the public right-of-way for a streetary must first obtain an annual streetary encroachment permit pursuant to this chapter. Each applicant for an annual streetary encroachment permit shall comply with the requirements of this chapter, any other applicable laws, and any regulations and policies adopted pursuant to this chapter.

Permits shall only be issued to applicants with a valid business license issued by the city to operate a food service establishment directly adjacent to the parking spaces within the right-of-way for which a streetary encroachment permit is sought.

All persons operating and/or maintaining streetaries as of the effective date of this ordinance shall be required to obtain a streetary encroachment permit and pay applicable fees no later than December 1, 2023. Failure to obtain a streetary encroachment permit within this time period shall constitute a violation of this chapter and may subject the current streetary operator to closure and/or enforcement pursuant to this chapter.

13.30.030 - Permit application.

The director of public works (the “director”) shall establish an application form for an annual streetary encroachment permit, including any application materials deemed necessary to enable complete review of the application. The application shall be valid for an initial term of three years, and then accompanied by an annual fee, in an amount set forth by separate resolution of the City Council.

The director shall transmit a copy of each application and renewal application for a streetary encroachment permit to the director of community development (the “planning director”). The planning director shall review the application for compliance with all applicable design and development standards.

13.30.040 - Fees.

Each permit application shall be accompanied by a permit application fee. Prior to permit issuance, the permittee shall pay an annual encroachment lease fee and a one-time, refundable deposit. Fees shall be in amounts set forth by separate resolution of the City Council.

13.30.050 - Where permitted.

- A. Streetaries are permitted in existing parking spaces within the right-of-way. The stopping, parking and/or standing of vehicles shall be prohibited in all locations and existing parking spaces within the City where a streetary encroachment permit has been issued.
- B. Streetaries are permitted on sidewalks within the right-of-way.

13.30.060 - Location criteria.

- A. Notwithstanding subsections A and B, above, streetaries shall not be permitted along streets with (1) peak hour parking restrictions or (2) speed limits greater than 25 miles per hour.
- B. Notwithstanding subsection A, above, streetaries shall not be permitted in parking spaces that (1) are Americans with Disabilities Act (ADA) designated, (2) are designated for no parking (red curb), passenger loading zones (white curb), commercial loading zones (yellow curb), limited parking zones (green) and/or any other colored curb zones with restrictions on driveways, ADA ramps, or entrances to parking lots or city-owned parking facilities, (3) would block or obstruct any fire hydrant, fire sprinkler or standpipe hose, or other public safety infrastructure, (4) would obstruct utility access panels, manhole covers, storm drains, valves, or any other type of utility assets, or (5) would obstruct any bicycle facility, or (6) provide bicycle parking, unless the bicycle parking can be reasonably relocated within 300 feet.
- C. Maximum parking stalls. A maximum of two parking spaces shall be used for each streetary.
- D. Adjacency to storefront. Streetaries may only be permitted in parking spaces and on sidewalks within the right-of-way that are directly in front of the permittee's food service establishment. If a food service establishment fronts more than one but less than two parking spaces and fronts more than fifty percent (50%) of the second parking space, the permittee is eligible to apply for two parking spaces for use as a streetary.
- E. Notwithstanding subsection E, above, the Director at their sole discretion may consider an exception to the adjacency to storefront requirement if: (1) the proposed Streetary was in operation as an outdoor food service establishment for six months prior to the effective date of this ordinance, (2) the proposed Streetary had obtained and at all times maintained all required permits and licenses to operate as an outdoor food service establishment, and (3) the Director determines that the location of the proposed Streetary will not be detrimental to the health, safety, peace, morals, comfort or general welfare of persons or adjacent businesses as evidenced by the permittee obtaining written consent from the establishments fronting the proposed location and providing to the City.
- F. Travel lane setback. Streetaries shall have an eighteen inch (18") minimum setback from the travel lane measured from the parking striping adjacent to the travel lane. In cases where no striping exists, the maximum length of the streetary measured from the face of the curb shall be determined by the director or their designee.
- G. Side setback. Streetaries shall have a two-foot minimum side setback to each wheel stop located at each end of the parking space.
- H. Utilities. A minimum clearance of four feet (4') from either side of utility access panels, manhole covers, storm drains, street valves, or any other type of utility assets will be required to allow for maintenance access. Streetaries proposed under overhead utility lines

shall meet the minimum vertical distance requirements as established by the California Public Utility Commission. Streetaries that block the outlet of a sidewalk underdrain shall ensure the outlet is functional and flowing. Permittees shall take a thorough inventory of utility access covers in the proposed streetary area by checking under parked cars. Permittees shall provide for access to any city or public utility company that may have underground conduits beneath the constructed streetary. Access to utilities may require that a permittee temporarily remove all or a portion of the constructed streetary. Permittees shall be responsible for the cost of removing, re-installing and restoring any damage to the streetary.

13.30.070 - Design and development standards.

The City Council shall, by separate resolution, adopt design and development standards regulating the form, design, safety, and maintenance of streetaries. The director may, from time to time, make minor modifications to the design and development standards.

13.30.080 - Criteria for issuance.

- A. The director, in acting upon any application for a streetary encroachment permit, shall either approve, approve with conditions, or deny the issuance of a permit based on the following principles and standards:
 - a. That the proposed use of the parking spaces and/or sidewalks within the public right-of-way is in compliance with all applicable provisions of this chapter;
 - b. That the proposed use of the parking spaces and/or sidewalks within the public right is so arranged as to ensure the protection of public health, safety and general welfare, and prevent interference with users of the sidewalks, streets and holders of other permits; and
 - c. That the proposed use will properly comply with the provisions and development standards prescribed in this chapter, or as prescribed by the director and/or planning director.
- B. The streetary encroachment permit may be subject to additional conditions where the city has documented any violation of this chapter, other applicable laws and regulations, or the streetary is detrimental to the health, safety, peace, morals, comfort or general welfare of persons residing or working in the neighborhood or is detrimental or injurious to property and improvements in the neighborhood.
- C. Deviations from Adopted Standards. Where the director determines that (1) a strict application of standards set forth in this ordinance cannot be met by a proposed streetary, and (2) the streetary, as proposed, will not be detrimental to the health, safety, peace, morals, comfort or general welfare of persons residing or working in the neighborhood or injurious to property and improvements in the neighborhood, the director may grant exceptions or minor modifications to the standards set forth in this ordinance.

13.30.090 - Limited to food service establishments.

All permits issued pursuant to this chapter shall be limited to streetaries established in conjunction with and abutting food service establishments.

13.30.100- Operational standards.

- A. No live entertainment or amplified music. No live entertainment or amplified music shall be permitted in streetaries established pursuant to this chapter without first obtaining all required permits including but not limited to those issued pursuant to Chapter 8.06 (Noise).
- B. No outdoor food preparation, flames, heating. Outdoor food preparation, food heating mechanisms, cooking and open flames, hotpots, candles, open flames, and barbecues shall not be permitted in streetaries.
- C. Alcohol service. Alcohol service shall be permitted subject to the acquisition of all required local and State permits and licenses.
- D. Noise and disruptive behavior. Permittees shall be responsible for ensuring their patrons minimize noise and disruptive behavior while using their streetary space.
- E. Site maintenance. Streetaries shall be maintained free of litter, refuse and debris. The area shall be scrubbed and mopped daily by the permittee to remove any food or drink stains. Such cleaning shall be in accordance with the City's Storm Water Management Program per Chapter 7.42 of the Menlo Park Municipal Code, which prohibits any discharge other than rainwater into the storm water drainage system.
- F. Streetaries shall adhere the following site maintenance requirements:
 - a. The permittee is required to keep the streetary area safe, free of debris, grime, and graffiti, and to keep any plants in good health.
 - b. Outdoor seating shall be scrubbed and mopped to remove any food or drink stains on a daily basis and the sidewalk shall be power washed quarterly by the permittee. Such cleaning shall be in accordance with the City's Storm Water Management and Discharge Control Program, which prohibits any discharge other than storm water into the storm water drainage system.
 - c. Streetaries and enclosures shall be designed in a manner that does not negatively impact existing drainage patterns. Features shall be incorporated into the design that allows water to flow freely off the street surface and along any existing gutter.
 - d. The permittee shall be required to clean tree grates, the gutter, and drainage inlets. Business and/or property owners shall be required to remove any debris that accumulates against their streetary. The areas on, under, and around the streetary must be clear of leaves and debris, which may require the restaurant operator or property owner to blow underneath the streetary or enclosure. The permittee shall also provide cleanout access upon request.
 - e. The permittee is required to maintain the pedestrian access clearances in sidewalk café and curb extension area and keep these areas safe, clean, and free of debris.
 - f. Streetaries shall not block access necessary to maintain the canopy of City maintained street trees.
 - g. Failure to maintain the cleanliness, safety, and accessibility of a streetary or café may subject the permittee to violations and fines. If maintenance issues are not resolved, a streetary permittee may be required by the City to remove the streetary at the permittee's expense.
- G. Sidewalk Cleaning. The permittee shall comply with all State and local regulations related to waste disposal including the National Pollutant Discharge Elimination System (NPDES) and the California Storm Water Quality Association (CASQA) as follows:
 - a. Pollution Prevention (visit www.cabmphandbooks.com for more information)
 - i. Use dry cleaning methods whenever practical for surface cleaning activities.
 - ii. Use the least toxic materials available (e.g. water-based paints, gels or sprays for graffiti removal).
 - b. Surface Cleaning
 - i. Regular broom (dry) sweep the streetary, sidewalk, plaza, and parking areas to minimize cleaning with water.

- ii. Dry cleanup first (sweep, collect, and dispose of debris and trash) when cleaning sidewalks or plazas, then wash with or without soap.
 - iii. Block the storm drain or contain runoff when cleaning with product
- H. ADA compliance. Streetary seating areas must, at all times, comply with all requirements of the ADA and provide sufficient clearance and walkway space to allow safe access and egress. For multi-level streetaries, a minimum of one level of ADA access shall be required.
- I. Hours of operation. The streetary shall adhere to the same approved hours of operation as the associated food service establishment business, with the following limitations:
 - a. For streetaries that are within 150-foot horizontal or vertical distance from residences: Streetaries shall not commence operations prior to 7 a.m. on Saturdays, Sundays, and holidays; Streetaries shall not commence operations prior to 6 a.m. Monday through Friday; Streetaries shall not operate after 10 p.m. on Fridays and Saturdays; Streetaries shall not operate after 9 p.m. Sunday through Thursday and holidays. For purposes of this section, "holidays" shall have the same meaning as set forth in chapter 8.13 (Noise) of title 8.
- J. Insufficient usage. Streetaries must demonstrate adequate usage in order to contribute to the economic vitality of the city. The city requires these streetaries to be set up and readily available for use during the operational hours of the food service establishment associated with the streetary, except in inclement weather and subject to the hours of operation limitations under subsection G. The city has the right to notify any streetary in writing of insufficient usage and invoke the enforcement procedures set forth in this chapter.

13.30.110 - Indemnification and insurance.

As a condition of the issuance of an annual streetary encroachment permit, the permittee shall defend, indemnify and hold harmless the City of Menlo Park and shall present, along with each application or renewal application for an annual permit, evidence of liability insurance in a form acceptable to the director.

13.30.120 – Enforcement.

- A. Any person who violates this chapter shall be guilty of a misdemeanor and may be subject to any and all enforcement proceedings or remedies, including the imposition of penalties as authorized by law. The provisions of this chapter are cumulative to any other remedies authorized by law. Any streetary may be subject to inspection by the city on an annual basis or as needed to ensure compliance with this chapter and permits issued pursuant to this chapter.
- B. The city retains the right to revoke a streetary encroachment permit at any time where a violation of this chapter has occurred and/or revocation is necessary to protect the public interest, health, safety and/or welfare of the community. If the director believes that a permittee is in violation of this chapter, the director may issue a notice of violation to the permittee. The notice of violation shall be served on the permittee, either in person or by first class mail. The notice shall indicate that the permit is subject to termination unless, within 30 days of service of the notice of violation, the violation is corrected or a hearing pursuant to this chapter is requested in writing.
- C. If the director or their designee determines that a permittee will not be able to continue to meet the requirements of this chapter because of a proposed public highway right-of-way improvement, the director or their designate shall notify the permittee, either in person or by first class mail, that the permit will be terminated within 30 days of service of the notice of termination.

13.30.130 – Appeals

Any person aggrieved by the director's action on a streetary encroachment permit application or revocation may appeal by submitting a written appeal to the city manager, along with any applicable appeal fee as provided in the city's master fee schedule, within fifteen (15) days of the date on which the permit was issued or on which the application was denied or on which the permit was revoked. The city manager, or their designee, shall hold a hearing on the appeal within thirty (30) days of receipt of the appeal, and shall give the applicant and the appellant at least ten (10) days' written notice of the time of the hearing. The decision by the city manager or their designee on the appeal shall be final.



City of Menlo Park “Streetaries” Outdoor Dining Design Standards



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STREETARY PROGRAM

STREETARY PROGRAM OVERVIEW

In 2015, the City of Menlo Park approved the Santa Cruz Street Café Program and assisted seven businesses in building semi-permanent parklets. The initial terms of the Santa Cruz Street Café program agreements expired in 2021 and the agreement holders were able to continue operating in the parklets on a month to month basis. The City's Temporary Outdoor Use Permit (TOUP) program began in 2020 in response to the COVID-19 Pandemic and California State of Emergency order. The TOUP Program was tied to duration of the COVID-19 California State of Emergency Order, which expired February 28, 2023.

To continue outdoor dining operations, the City of Menlo Park implemented the Streetary program for outdoor eating areas, which is an expansion of past outdoor use and parklet programs with improved design standards for the operation of outdoor dining installations in the Downtown Business District and other commercial areas of the City in the form of parklets and sidewalk cafes. Allowing for Streetaries provides an opportunity for businesses to expand their operations outdoors onto public streets or private spaces.

A "Streetary" refers to an outdoor eating area that operates within and uses public parking spaces and/or sidewalks within the public right of way directly adjacent to a restaurant or café or other food service establishment's street frontage. Streetaries are generally allowed within two areas of the right-of-way: (1) the sidewalk area between the curb and the building or parcel frontage and (2) parking areas, generally the parking spaces between the curb and travel lane, as well as parking stalls in public or private parking lots.

Streetaries are generally permitted in the El Camino Real and Downtown Specific Plan / Downtown Business District area and most other commercially zoned areas for restaurant and dining space uses where the speed limit is 25 miles per hour or less. However, Streetaries are not allowed where bike/pedestrian safety and vehicle travel would be impacted, as well as along El Camino Real because it is a State-controlled roadway and is not under the City's jurisdiction.

STREETARY GOALS

The City of Menlo Park Streetary Program goals and intentions are as follows:

1. Activate the public realm in Downtown Menlo Park and other commercial areas; and
2. Maintain physical and visual access to Menlo Park businesses; and
3. Provide safe, attractive, and accessible spaces for outdoor use; and
4. Provide accessible amenity areas for private businesses during operating hours; and
5. Support the economic vitality and growth of businesses and the City.

SECTION 1. STREETARY PARKLETS

PARKLET STREETARY OVERVIEW

PURPOSE

The purpose of the Streetary parklet standards is to ease the approval process for parklets by establishing parklet template design solutions for an applicant to meet the design, locational, and permitting requirements of the City.

GUIDELINES

The Streetary parklet design standards provide design guidelines and requirements for Menlo Park businesses to use in the preparation of an application to construct a parklet on a public parking space under City control in any commercial district where outdoor uses are permitted. A parklet is a sidewalk extension, typically installed on public parking spaces, that provides more space and amenities for outdoor dining (such as seating or greenery area) operated and maintained by the business establishment operating the Streetary.

RELATION TO SIDEWALK CAFÉ STREETARIES

The Streetary Program includes parklets and sidewalk cafes to facilitate additional outdoor dining and social gathering spaces in the public realm. Businesses that will only implement a sidewalk cafe should refer to the sidewalk café Streetary overview and may proceed to sidewalk café Streetary section of this document on page 21. Businesses that desire to implement a parklet should follow the parklet Streetary guidelines contained in this section. Where both a parklet and an adjoining sidewalk cafe are planned, both sections should be consulted.

PERMIT REQUIREMENTS

An Streetary encroachment permit is required for all parklet installations and sidewalk cafes, including those that were in place prior to the creation of these guidelines. **Streetary Permit application packets, as described below, must be submitted prior to the construction of any new parklet or the modification of an existing parklet previously permitted under the Santa Cruz Street Café or Temporary Outdoor Use Permit pilot programs.**

All permits will be reviewed by the Planning and Engineering Divisions and other relevant City departments, prior to approval. Permits for use of sidewalk and public parking plazas shall be revocable upon 72-hour notice if the City determines the sidewalk space or public parking plazas are needed for other non-emergency uses, such as expanded walking space on sidewalks for events or additional parking in public parking plazas.

PARKLET STREETARY SUBMITTAL REQUIREMENTS CHECKLIST

PERMIT SUBMITTAL REQUIREMENTS

The following shall be submitted to the **City of Menlo Park Public Works Department** as part of the Streetary:

- Permit fee payment
- Site Plan (scaled - i.e., 1/8" = 1'10" | dimensioned - i.e., 5'0" clearance path) (refer to site plan checklist)
- Elevation drawing (refer to elevation drawing checklist)
- Identification of Parklet Design Components (refer to Design Components Checklist)
- Photograph of proposed design components
- Photograph of proposed Streetary location (showing the front and side views along the business frontage)
- Required insurance forms (see below)
- Traffic control plan (for installation/construction)

INSURANCE REQUIREMENTS

- Completed Hold Harmless Agreement (review requirements listed)
- Insurance Endorsement Form
- Insurance Certificate (must name City as additional insured, see Hold Harmless Form)

Insurance Category	Minimum Limits
Worker's compensation	Statutory Minimum
Employer's Liability	\$1,000,000.00 per accident for bodily injury or disease
Commercial General Liability	\$1,000,000.00 (\$2,000,000.00 for parklets) per occurrence for bodily injury, personal injury, and property damage

Streetary permit(s) are revocable. Compliance with Streetaries (Chapter 13.30) and the design / program guidelines in this document is required.

SITE PLAN CHECKLIST

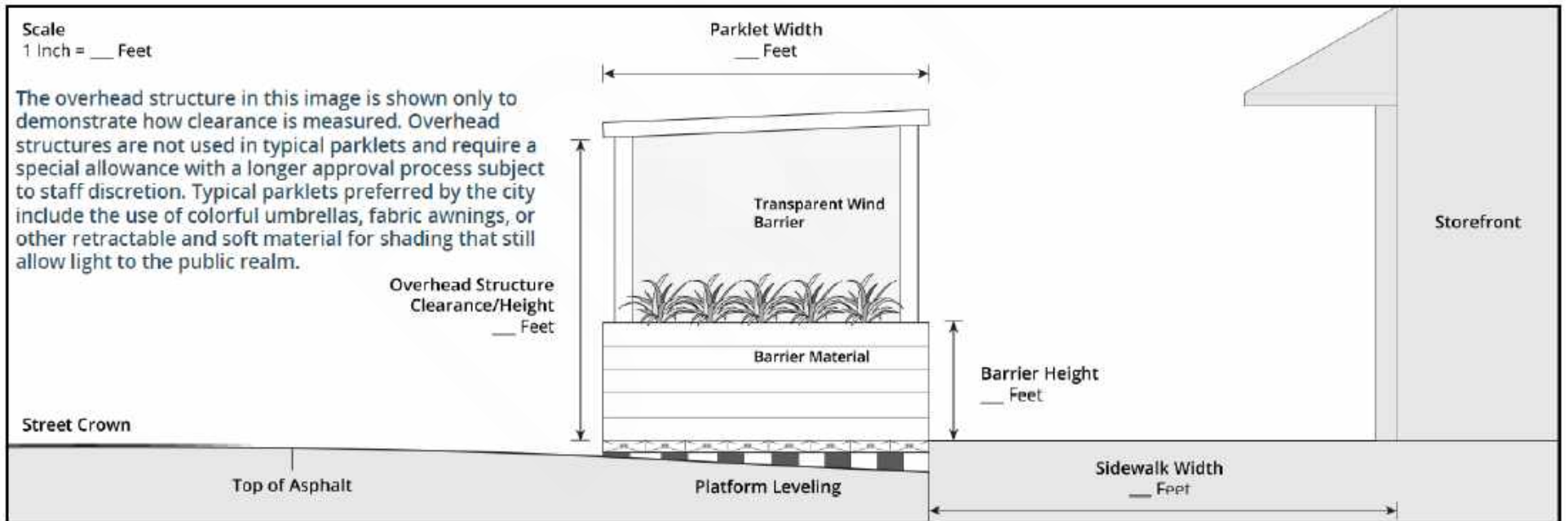
The Site Plan Checklist provides information required to be shown on the applicant's submitted site plan. The Checklist includes information for the existing site conditions for the desired parking space and parklet location requirements to verify that the proposal is suitable along the business frontage. Example site plans that include the Checklist items are provided in the following section for each parklet template.

EXISTING SITE CONDITIONS		PROPOSED PARKLET STREETARY INFORMATION	
Applicant building and adjacent businesses (with entry doors shown)	<input type="checkbox"/>	Proposed parklet footprint and dimensions	<input type="checkbox"/>
Existing sidewalk width(s)	<input type="checkbox"/>	Egress plan and accessibility compliance	<input type="checkbox"/>
Existing curb cuts and driveways	<input type="checkbox"/>	Number of parking spaces that the parklet will occupy	<input type="checkbox"/>
Adjacent bicycle lane or auto traffic lane	<input type="checkbox"/>	Parklet setbacks:	
Existing parking spaces, ADA spaces, accessible routes and loading zones with dimensions	<input type="checkbox"/>	- Minimum 3-feet from adjacent parking spaces	<input type="checkbox"/>
Existing parking meters and meter numbers	<input type="checkbox"/>	- Minimum 2-foot from adjacent bicycle lane or auto traffic lane	<input type="checkbox"/>
Other existing sidewalk features (e.g., fire hydrants, streetlights, planters, bike racks, bus shelters, outdoor dining, trees, signs, etc.)	<input type="checkbox"/>	- Minimum 2-feet from fronting driveways (if applicable)	<input type="checkbox"/>
		Distance from the proposed parklet to the nearest crosswalk or intersection	<input type="checkbox"/>
		Location, height, impact rating, and materials of barriers, railings, and/or enclosures	
All color curb zones (red, yellow, green, white, blue)	<input type="checkbox"/>	Location and spacing of parklet tables, chairs, umbrellas/fabric sails, and other furnishings	<input type="checkbox"/>
Scale	<input type="checkbox"/>	Additional bike racks adjacent to the parklet (if applicable)	<input type="checkbox"/>
North arrow	<input type="checkbox"/>	Lighting (if applicable) <i>no new foundations are allowed</i>	<input type="checkbox"/>
Name and location of adjoining streets or alleys	<input type="checkbox"/>	Portable heaters and power source (if applicable)	<input type="checkbox"/>
Adjacent existing parklets	<input type="checkbox"/>	Café counter: a long flat-topped fixture for dining (if applicable)	<input type="checkbox"/>
Height of building at proposed location and adjacent buildings	<input type="checkbox"/>	Benches (if applicable)	<input type="checkbox"/>

PARKLET STREETARY ELEVATION CHECKLIST

The Parklet Elevation Checklist provides information required to be shown on the Parklet Applicant's submitted parklet elevation drawing.

PROPOSED PARKLET BARRIER AND STRUCTURE INFORMATION		PROPOSED PARKLET PLATFORM AND OTHER INFORMATION	
Parklet width	<input type="checkbox"/>	Platform Support	<input type="checkbox"/>
Barrier Height	<input type="checkbox"/>	Platform Leveling (i.e., pedestal or other means)	<input type="checkbox"/>
Barrier Materials (including planters, railings, and soft posts (if applicable))	<input type="checkbox"/>	Platform Anchorage	<input type="checkbox"/>
		Drainage Clearance	<input type="checkbox"/>
Overhead Structure (if approved as a special allowance)	<input type="checkbox"/>	Existing Asphalt and street crown	<input type="checkbox"/>
Overhead Structure minimum 7-foot clearance	<input type="checkbox"/>	Sidewalk Width	<input type="checkbox"/>
Overhead Structure Height	<input type="checkbox"/>	Storefront	<input type="checkbox"/>



OUTDOOR PARKLET STREETARY DESIGN STANDARDS

SELECT A PARKLET STREETARY TEMPLATES

Each template reflects conditions that generally exist for on-street parking spaces in commercial areas: parallel parking spaces, diagonal parking spaces, and special condition parklets. The applicant shall select a parklet template based on the existing conditions along the site frontage.

Special condition parklets allow for unique or site-specific conditions. Each special condition template has a unique icon that is used throughout these guidelines to indicate where a program requirement is specific to that special condition parklet.

Once the applicant has selected a parklet template, the applicant may then select an option for each of the design components listed on page 12. These include enclosure or platform materials, seating options, and other considerations. Parklets shall meet all additional design requirements starting on page 13. Please see the Appendix for additional parklet template renderings and site plans.

A-1 PARALLEL SPACE PARKLET (1 PARKING SPACE)

The Parallel Parklet template occupying one parking space is a compact parklet that provides an intimate outdoor dining setting.

This template limits the maximum parklet area to one parking space and can be utilized in areas where available on-street parking spaces are limited.

Parklets proposed for perpendicular parking spaces can use a modified version of this template.

Parklet Streetary Template Options

- A. Parklet in parallel parking space(s):
 - 1. A-1 One Space
 - 2. A-2 Two Space
- B. Parklet in a diagonal parking space
- C. Special Condition Parklets:
 - 1. C-1 Parklet/Sidewalk Café Combination
 - 2. C-2 Parklet/Sidewalk Café Curb Extension
 - 3. C-3 Parklet/Pedestrian Street Combination

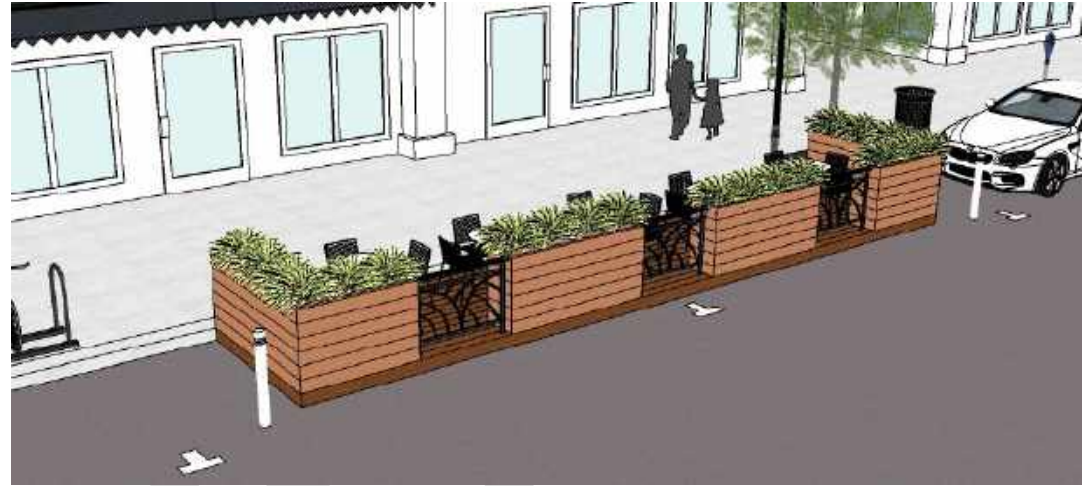


A-2 PARALLEL SPACE PARKLET (2 PARKING SITES)

The Parallel Parklet template occupying two parking spaces provides a larger outdoor dining setting.

This template limits the maximum parklet area to two parallel parking spaces.

Parklets proposed for perpendicular parking spaces can also use a modified version of this template.



B-DIAGONAL SPACE PARKLET

The Diagonal Space Parklet template provides a larger outdoor dining setting.

When including the necessary setbacks, this template would need three spaces.



C-1 PARKLET/SIDEWALK CAFÉ COMBINATION

The Parklet/Sidewalk Cafe Combination template is for conditions where an applicant has the opportunity to incorporate both a sidewalk cafe and a parklet for a combined space.

This Special Condition template can be used in either a parallel parking or diagonal parking site.

The combined space should be designed as a comprehensively planned singular space.



C-2 PARKLET/SIDEWALK CAFÉ CURB EXTENSION COMBINATION

The Parklet/Sidewalk Cafe Curb Extension Combination template is provided for sites where a business would like to incorporate a sidewalk cafe on an adjacent curb extension with a parklet.

This Special Condition template can be used in either a parallel or diagonal parking site adjacent to an intersection.

The combined space should be designed as a comprehensively planned singular space.



C-3 PARKLET/PEDESTRIAN STREET COMBINATION

The Parklet/Pedestrian Street Combination template is designed with flexibility for parklets that are proposed on streets the City has permanently closed to vehicular through-traffic to enable pedestrian access and circulation.

This template shall not be used where streets are only temporarily closed.

Expanded seating areas would not be allowed unless the supplementary seating is specifically reflected in plans submitted and approved by the City.



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SELECT PARKLET DESIGN COMPONENTS

Each of the Parklet templates can be constructed with a series of components of a variety of materials to ensure quality design and an enhancement of the public realm. The applicant shall select one or more of the appropriate features from the Required Design Components, below, and then select additional, Optional Design Components. The design should be compatible with the façade of the adjacent building/restaurant. The applicant may propose to use an alternative material that may be approved upon review by City staff.

REQUIRED DESIGN COMPONENTS		OPTIONAL DESIGN COMPONENTS**	
Platform <i>Select one:</i>		Shading <i>Select one:</i>	
Brick pavers _____	Concrete Pavers _____	Umbrella (Fire resistant) _____	Fabric sail shade (Fire resistant) _____
Wood boards _____	Other: _____		
Platform Support <i>Select one:</i>		Lighting	
Wood platform support _____	Steel-frame platform support _____	String Lights _____	Light post _____ (No new foundations allowed)
Barriers <i>Select one:</i>		Overhead Structures as a Special Condition <i>Select one of the following:</i>	
Wood planter* _____	Concrete Planter* _____	Pergola _____	Solid Roof _____
Steel planter _____	Perforated steel Railing _____	Tent/Membrane Structure _____	Other: _____
Tables and Seating		Other	
Movable café tables and chairs _____	Metal benches _____	Additional planters _____	Bike rack _____
Wooden benches _____	Café counter _____	Portable heaters _____	Other: _____

**Wood and Concrete Planters must be painted or finished, and colors should be compatible with the façade of the adjacent building.*

***Fuel fired heating devices are prohibited inside of cloths, umbrellas, tents, canopies, and membrane structures (electric is okay). Heating devices must be CSFM rated.*

DETAILED DESIGN STANDARDS

This section provides detailed requirements and standards that are applicable to all parklet templates. Parklet design elements and materials consist of the following elements, generally: (A) Location and Setback; (B) Platforms; (C) Railings or enclosures; and (D) Overhead Structures as a Special Allowance. Furnishing & Fixture Design Standards and General Operation & Maintenance Requirements for parklets and sidewalk cafes are at the end of the document.

These requirements are established to maintain safety, accessibility, and an enhanced design that connects the public realm. The applicant's compliance with these requirements shall be shown on the applicant's site plan, submitted as part of the parklet permit application.

A. LOCATION AND SETBACK CRITERIA

Streetaries are generally allowed in most commercial districts along the curbside on public streets where on-street parking spaces exist. However, parklets are not allowed along El Camino Real because it is a State-controlled roadway and is not under the City's jurisdiction.

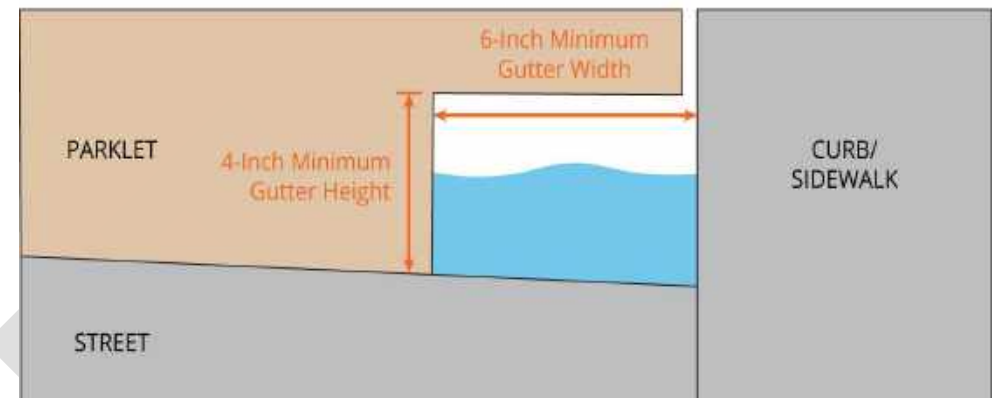
1. **Permitted areas within City:** Streetaries are intended for outdoor dining in the El Camino Real and Downtown Specific Plan /Downtown Business District, as well as other commercially zoned areas where outdoor uses are permitted. Streetaries are only permitted on roads with speed limits less than 25mph. Parklets are not allowed in ADA/handicap parking spaces or red curb zones.
2. **Parklet Size**
 - A typical parklet occupies no more than 360 square feet (the space of 2 parallel parking spaces). Parklets that occupy more than 360 square feet may be approved at the City's discretion.
 - A parklet located in an angled space shall occupy a minimum of 3 diagonal spaces to allow for sufficient buffering to vehicles parking in adjacent parking spaces. This may be reduced to 2 diagonal parking spaces where a parklet is proposed in combination with a curb extension.
3. **Parklet Setback Requirements**
 - 2.5-feet from driveway flare
 - 2-feet from the travel lane (1-foot from the travel lane may be acceptable where the travel lane is 11 or 12-feet in width), measured from the parking striping adjacent to the travel lane. Where the parking striping does not exist, the applicant shall confirm with City staff the width of the travel lane measured from the street centerline and provided the setback from the identified travel lane edge. The 2-foot setback must be kept clear at all times, unless occupied by reflective delineator posts or their equal
 - 2-feet from a bike lane
 - 3-feet from the side to the adjacent parking space, parallel or diagonal. Where parking striping does not exist, the applicant shall confirm with City staff the width and location of each parking space and shall measure the setback from the identified parking space location.

4. **Crosswalk & Bulb-out Setback:** When located near an intersection, parklets must be located at least 20-feet from the nearest boundary (edge of the crosswalk that is closest to the parklet) of a crosswalk at the nearest intersection or street corner. A curb extension (commonly referred to as a “bulb-out”), some other physical barrier that would protect the parklet in a corner location, or other specific site condition may allow the City to consider variations from the 20-foot minimum requirement.
5. **Other Location Criteria:**
- **Utilities:** Parklets may not be constructed over or obstruct utility access panels, manhole covers, storm drains, survey monuments, or fire hydrants. Be sure to take a thorough inventory of utility access covers in your proposed parklet area by working with city staff to determine utility locations, surveying your proposed parklet area, and checking under parked cars.
 - A minimum clearance of 8-feet from storm drains and manhole covers will be required to allow for maintenance access.
 - Parklets proposed under overhead utility lines will be required to meet the minimum distance requirements as established by the CPUC.
 - Parklets that block the outlet of a sidewalk underdrain will be required to ensure the outlet is functional and flowing.
 - Parklet sponsors must provide for access to any City or utility company that may have underground conduits beneath the constructed parklet at all times, or immediately following a request for access. No notice can be guaranteed for emergency access to underground utilities. For planned access, 72-hour notice will be issued to the parklet sponsor. Access to utilities may require that a parklet sponsor temporarily remove all or a portion of the constructed parklet at the parklet sponsor/owner’s expense.
 - **Fire Hydrants:** Fire hydrants shall not be blocked and parklet owners must maintain a 3 foot radius clearance around physical hydrant and 15 feet access to hydrant (7 1/2 feet in each direction linear along street).
 - **Fire District Connections (FDCs):** to maintain access to FDCs, five-foot wide openings should be provided between every two adjacent parklets (and no more than 75-feet apart).

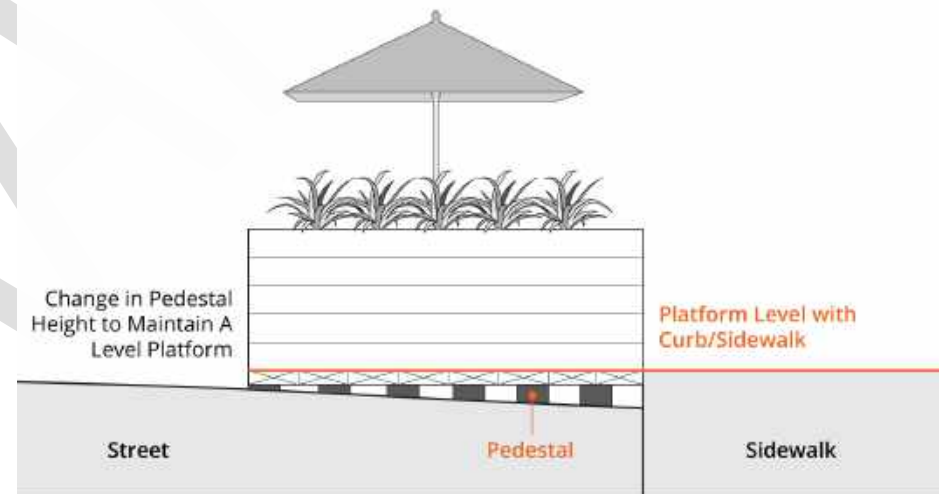
B. PARKLET PLATFORM DESIGN CRITERIA

- Materials:** Platforms must be constructed with durable, fire resistant materials and be able to withstand the effects of the outdoor environment. **Examples:** naturally durable wood, preservative-treated wood, or other engineered material suitable for exterior conditions.
 - Concrete platforms are **not** permitted; however, you may use concrete pavers on a platform structure.
 - Surface materials must be textured or treated with a non-skid coating to ensure a safe walking surface. Loose particles, such as sand and loose stone, are **not** permitted.
 - Applicants interested in utilizing fabric sails or other architectural features which may “catch” wind, must provide proof of wind loads.
- Maintain access to utilities:** Parklets must be designed to provide access to site-specific utilities, such as storm sewer covers, utility cabinets, and underground vaults, by employing in their design and construction access panels or removable pavers.
 - A minimum 15-foot clearance shall be maintained from a fire hydrant and all other fire appurtenances.
- Platforms must be flush and even with the sidewalk and must not leave a gap greater than one-half (1/2) an inch, nor a vertical separation greater than one-quarter (1/4) inch.
- A parklet platform or it’s enclosure shall not be mounted on wheels.
- Drainage, Ventilation and Rodent Proofing:** Platforms shall allow curbside draining flow with the following:
 - A four-inch (4”) height by six-inch (6”) width minimum clear gutter space shall be provided along the entire length of the proposed platform.
 - Openings under the platform shall be screened with corrosion-resistant material with a maximum one quarter inch mesh to prevent rodent access and debris buildup beneath the platform and in the gutter.

CURBSIDE DRAINAGE

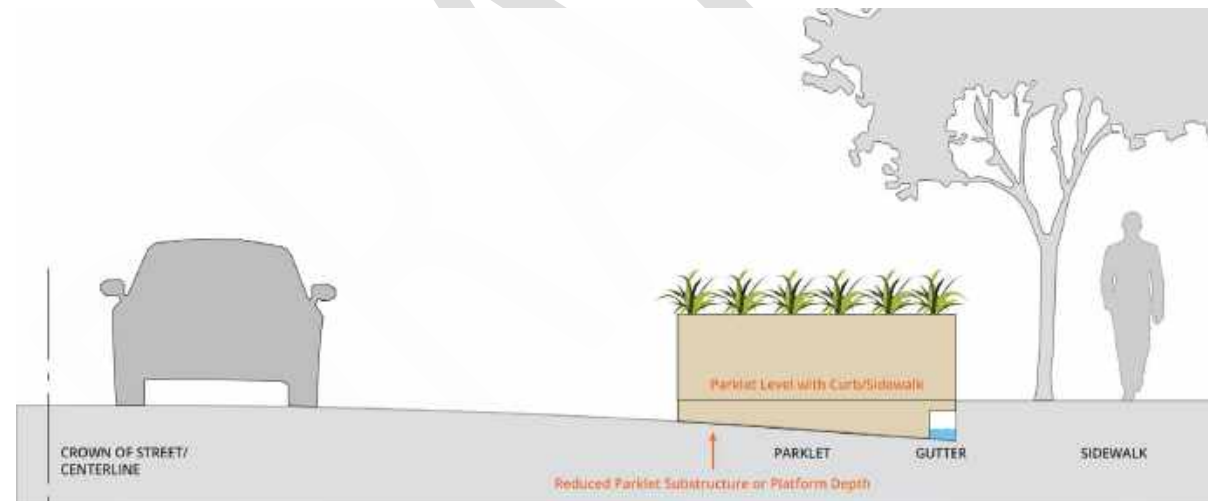


PEDESTAL SYSTEM LEVELING



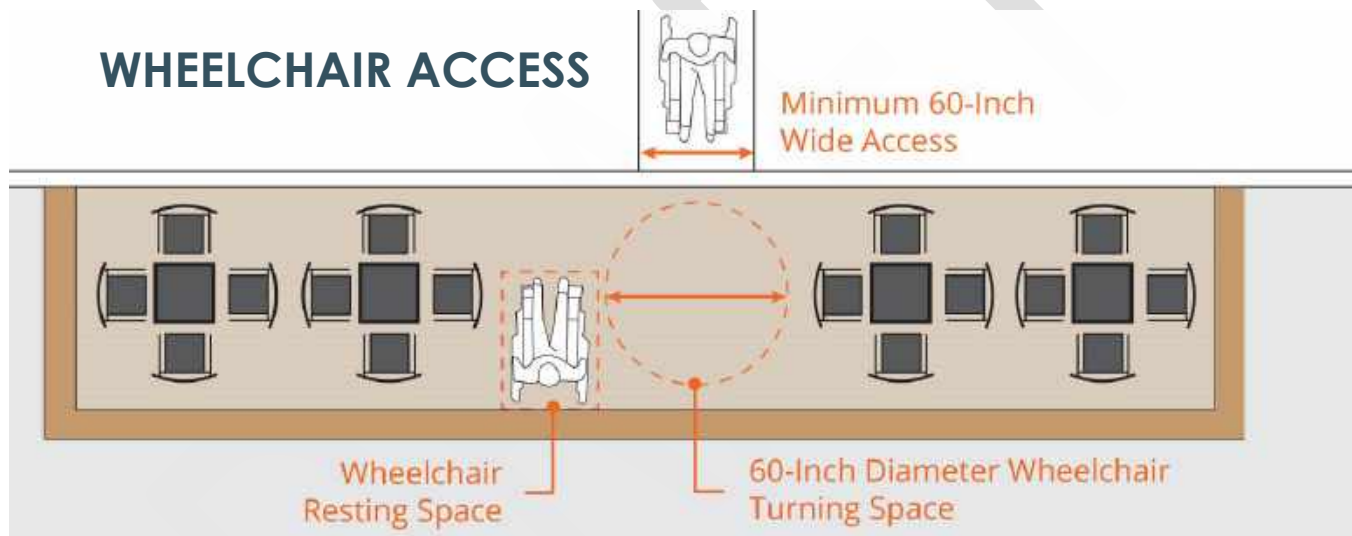
- The parklet permittee shall clean covers or screens to prevent any blockage of flow in the gutter.
 - All parklets shall provide access through the parklet platform or threshold to the gutter adjacent to the curb. Access may be provided through removable panels, pavers, or other means.
6. **Bolting Not Allowed:** At no time may structures be bolted or affixed in any way to the roadway or any structure (including but not limited to buildings, fire hydrants, street trees, streetlight, parking meters, or traffic poles, etc.).
- An exception to this is that wheel stops for traffic protection must be bolted to the roadway as described in the Traffic Protection section of these Guidelines. However, in case of removal, the establishment shall be responsible for repairing the pavement holes at the permit holder's expense.
7. **Sub-structure:** Designs for the sub-structure of a parklet vary and depend on the slope of the street and overall design for the structure. The sub-structure must accommodate the crown of the road and provide a level surface for the parklet.
- "Bison pedestals" spaced under the surface and of different heights are a common application. Another method is to provide steel sub-structure and angled beams.

SUBSTRUCTURE OR PLATFORM LEVELING



8. **ADA Accessibility:** All accessibility elements of the proposed platform shall be designed, constructed and/or conform to the applicable provisions, rules, regulations and guidelines of the California Building Code and Americans with Disabilities Act.
- **Accessible Platform Surface:** The portion of the parklet platform connected by the accessible path of travel to the wheelchair turning space and wheelchair resting space must be level. The accessible platform surface maximum cross slope (perpendicular to the sidewalk or curb) and running slope (parallel to the curb) cannot exceed 2%. Platform surface shall be made of slip resistant material.

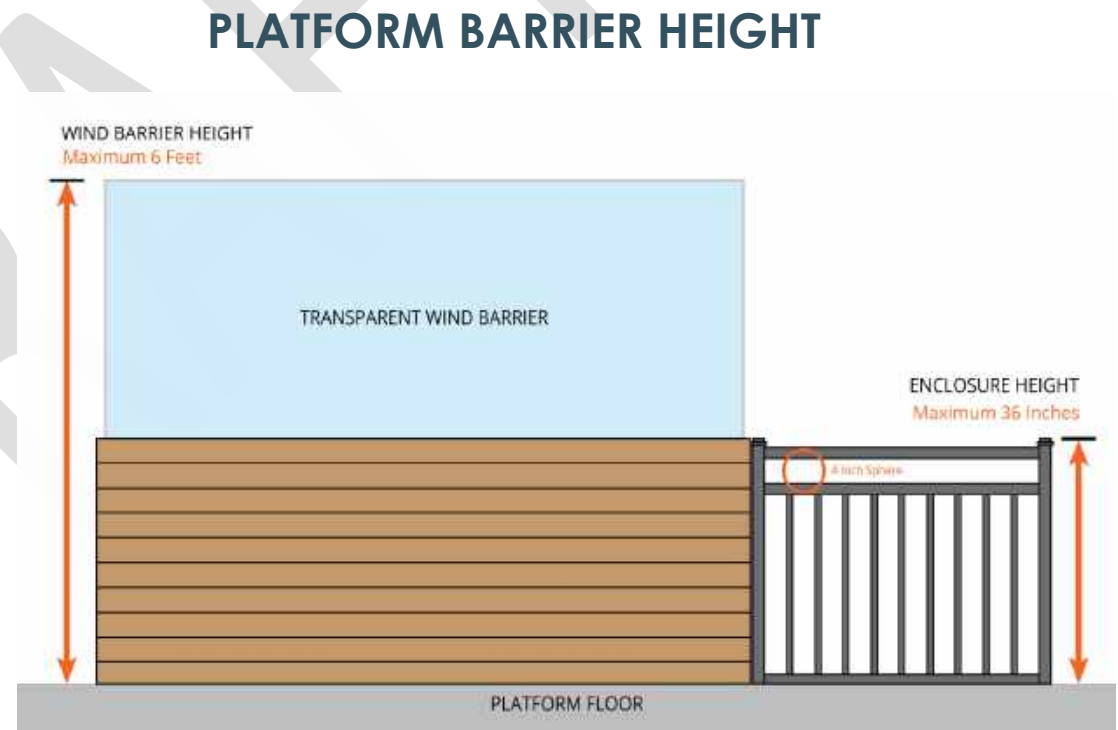
- **Accessible entry:** Shall be a minimum of 48 inches wide.
- **Accessible path of travel:** It must connect the sidewalk to the accessible entry, platform surface, wheelchair turning space and wheelchair resting space.
- **Wheelchair turning space:** Shall be 60 inches in diameter and located entirely within the platform; a 12-inch maximum overlap on the curb and sidewalk is acceptable.
- **Wheelchair landing:** A 30- by 48-inch clear floor area. It's permitted to overlap with the Wheelchair Turning Space by 24-inches maximum in any orientation.



C. BARRIERS, RAILINGS, OR ENCLOSURES

The parklet railing or enclosure marks the boundary between the parklet and the street or sidewalk. Traffic barriers are required to surround the parklet on three sides to protect from oncoming traffic and patrons inadvertently entering the vehicular way. It should serve as a safe edge while also being visually appealing, permeable, and inviting. The following standards and guidelines should inform your design:

1. **Force / Impact Rating:** Barriers, Railings or Enclosures must be stable and sturdy enough not to fall over or be pushed over (must withstand 250-lbs of force).
2. **Edge Buffers:** the parklet should have an edge as a buffer from the street. This can take the form of planters, railing, cabling, or some other appropriate enclosure.
3. **Height:** The height should not exceed 36-inches from the parklet platform floor to the top of the railing.
 - Wind barriers taller than 36-inches shall incorporate transparent materials like acrylic, plexus-glass, plastic films. Wind barriers shall be installed and engineered to withstand wind loads, in conformance with any necessary building permits. The height of the railing, including the transparent materials, should not extend more than 6 feet from the floor platform, unless it is designed within an overhead structure per requirements for overhead structures.
 - Openings in rails must prevent passage of 4-inch sphere.
4. **Visibility:** The enclosure should not block the view of conflicting movements of traffic, including pedestrian traffic, nor block the view of traffic control devices such as traffic signs, traffic signals, and other traffic warning devices.
 - Property addresses need to remain visible. If needed, post reflective visible addresses on structures if blocking from street view on building.
5. **Reflectors:** All railings/enclosures must have retro-reflective reflectors or retro-reflective tape on the corners of the parklet facing the travel lane such that they are visible by vehicle traffic at night.
6. **Posts or Bollards:** Parklets shall have vertical elements that make them visible to traffic, such as flexible posts or bollards both with retroreflective tape. One possible measure are safe hits posts (as shown in the picture below). These vertical elements shall be placed 2 feet



from the edge of the parklet on both sides of the parklet not adjacent to the travel lane. Structural bollards may be required if deemed necessary by the city.

7. **Design:** Barriers may be made up of or encased by one of the following materials: (1) wood planters, (2) concrete planters, (3) steel planter, or (4) perforated steel railings. Wood and concrete planters must be finished or painted. The colors and materials should be compatible with the design, materials, and colors of the façade of the adjacent building/restaurant frontage.
8. To accommodate seasonal conditions, the applicant may propose alternative platform and wind barrier materials and designs for review and approval by City.

D. OVERHEAD STRUCTURES

Overhead structures may require a longer review and approval process subject to staff discretion. Typical parklets preferred by the city include the use of umbrellas, fabric awnings, or other retractable and soft material for shading that still allow light to the public realm in colors that are compatible with the façade of the adjacent building/restaurant.

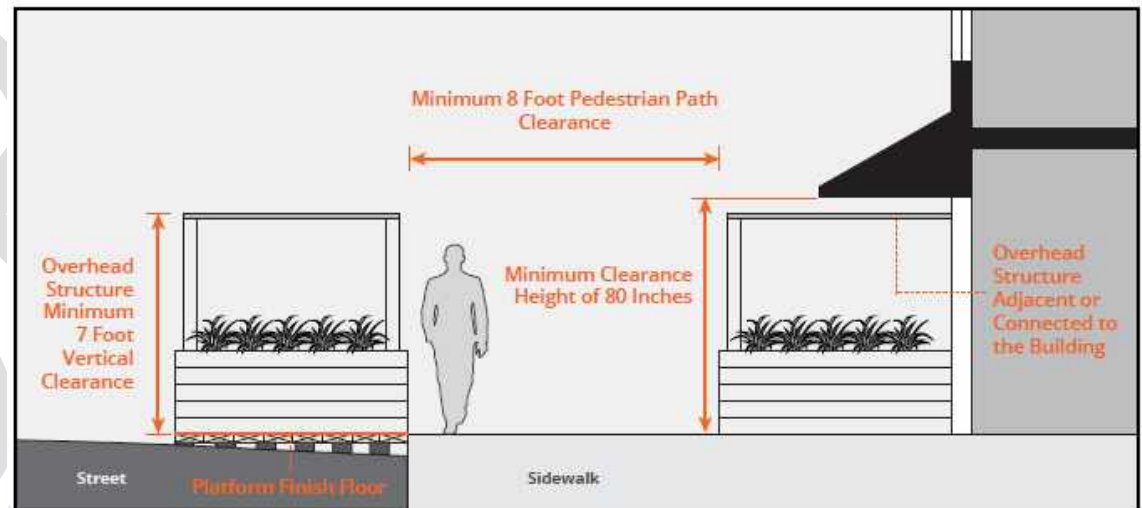
1. Installation of any roof or overhead structure is subject to review by the city.

2. A site plan and elevation drawing of the parklet shall be provided with a structural engineer or architect's stamp if the applicant proposes to include an overhead structure, such as a pergola or other affixed roof structure. Umbrellas or fabric sail shades are not considered overhead structures. However, umbrellas and fabric sail shades must be identified in the site plan for the city to review for compliance with standards such as sight lines, height, wind/fire resistance, etc.

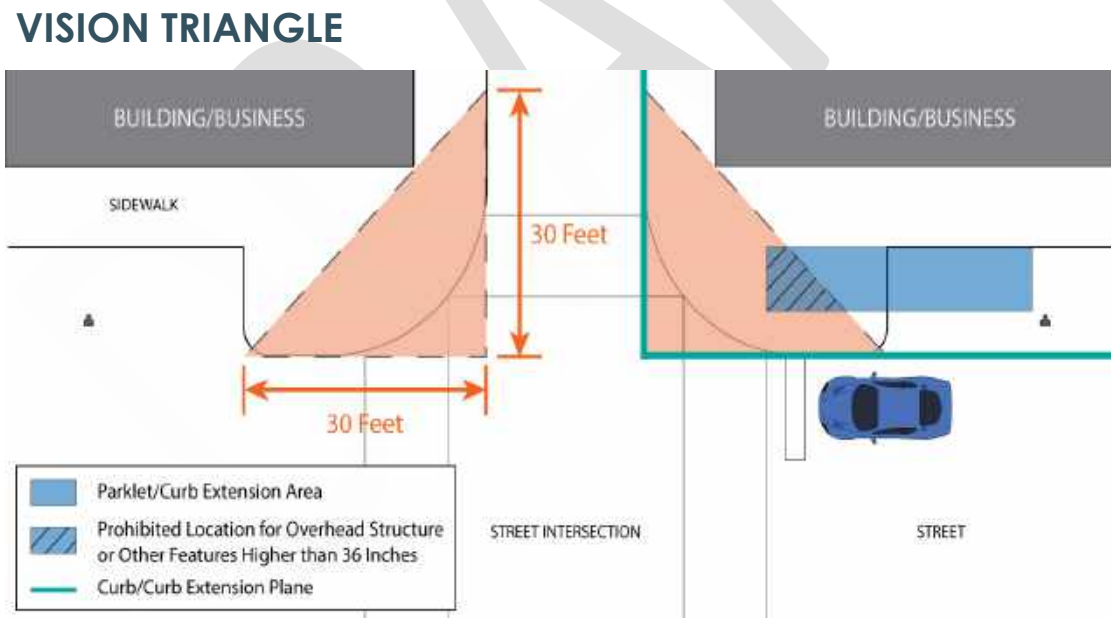
3. The City will review any proposed overhead structures to ensure that sufficient visibility for pedestrians, traffic, businesses, and other site-specific conditions, are properly maintained. The city may require modifications or deny the proposed overhead structure.

4. The minimum vertical clearance for an overhead structure within the parklet footprint, a sidewalk cafe, or curb extension is 7 feet above the finished floor level.

OVERHEAD STRUCTURE



5. The maximum vertical height for an overhead structure within a parklet footprint shall not exceed 10-feet or the height of the first floor top of plate height of restaurant, whichever is shorter in height.
6. In order to ensure motorist sight lines at street intersections, overhead structures or other parklet feature higher than 36 inches are prohibited within a 30-foot vision triangle measured from the intersection of the curb or curb extension line.
7. Parklets with overhead structures may only be placed where a minimum 8-foot pedestrian clearance is provided along the pedestrian path. No portion of the structure may encroach within this clearance.
8. Overhead structures shall maintain a visual connection to the street by designing the structure to provide some openness with transparent wind barriers required in C.4. on page 32 or with openings in the enclosure. Shading materials should be soft to reduce prominence and retractable so it can be pulled back when not in use or when more light is desired in the space.
9. Any fabric, membrane shade, or other weather protection, including umbrellas and tents of any size, must be CSFM listed and constructed of fire-resistant treated material as per the California Code of Regulations, Title 19, Division 1, Section 315(a). Heating devices within overhead structures shall meet requirements in the Furnishings and Fixtures section starting on page 24.



SECTION 2. SIDEWALK CAFÉ STREETARY PROGRAM

SIDEWALK CAFÉ STREETARY OVERVIEW

PURPOSE

The purpose of these guidelines is to help clarify and provide standards for sidewalk cafes uses in order to enhance the quality and safety of the pedestrian experience and reinforce a sense of place and economic vitality. These guidelines and requirements are established to achieve the Streetary goals by maintaining safety, accessibility, and an enhanced design that connects the public realm. Compliance with all applicable sections of these guidelines is required at all times.

DEFINITION OF A SIDEWALK CAFÉ

An outdoor area located on the public right-of-way within the sidewalk area which is used to provide more space and amenities for outdoor dining operated and maintained by the business establishment conducting the outdoor dining.

SIDEWALK CAFÉ AREA

The sidewalk cafe area can be defined as one or a combination of:

1. The space delineated by the facade of the building to the adjacent street, perpendicular to the public right-of-way.
2. The recessed entries immediately adjacent to the outdoor sidewalk cafes.
3. Certain alleys determined by the city that are adjacent to the operating business.

SIDEWALK CAFÉ AREA CLEARANCE REQUIREMENTS

The cafe operators are required to maintain all minimum distances and clearance requirements at all times: *Minimum 5-foot pedestrian path clearance; min. 5-foot entrance clearance radius; and min. 5-foot pedestrian path at crosswalks from edge of curb at all times.* City staff will perform compliance inspections. Application submittals are required to comply with all Americans with Disabilities Act (ADA) requirements.

MOVEABLE IMPROVEMENTS

The sidewalk cafe area contains various sidewalk patterns which affect the placement of improvements such as tables and chairs in the public right-of-way. The installation of these improvements is considered temporary in nature. The operator shall locate moveable flower boxes or planters adjacent to the street in such a way as to visually define and contain the sidewalk cafe area. The sidewalk cafe improvements shall be set up during the applicable hours of operation.

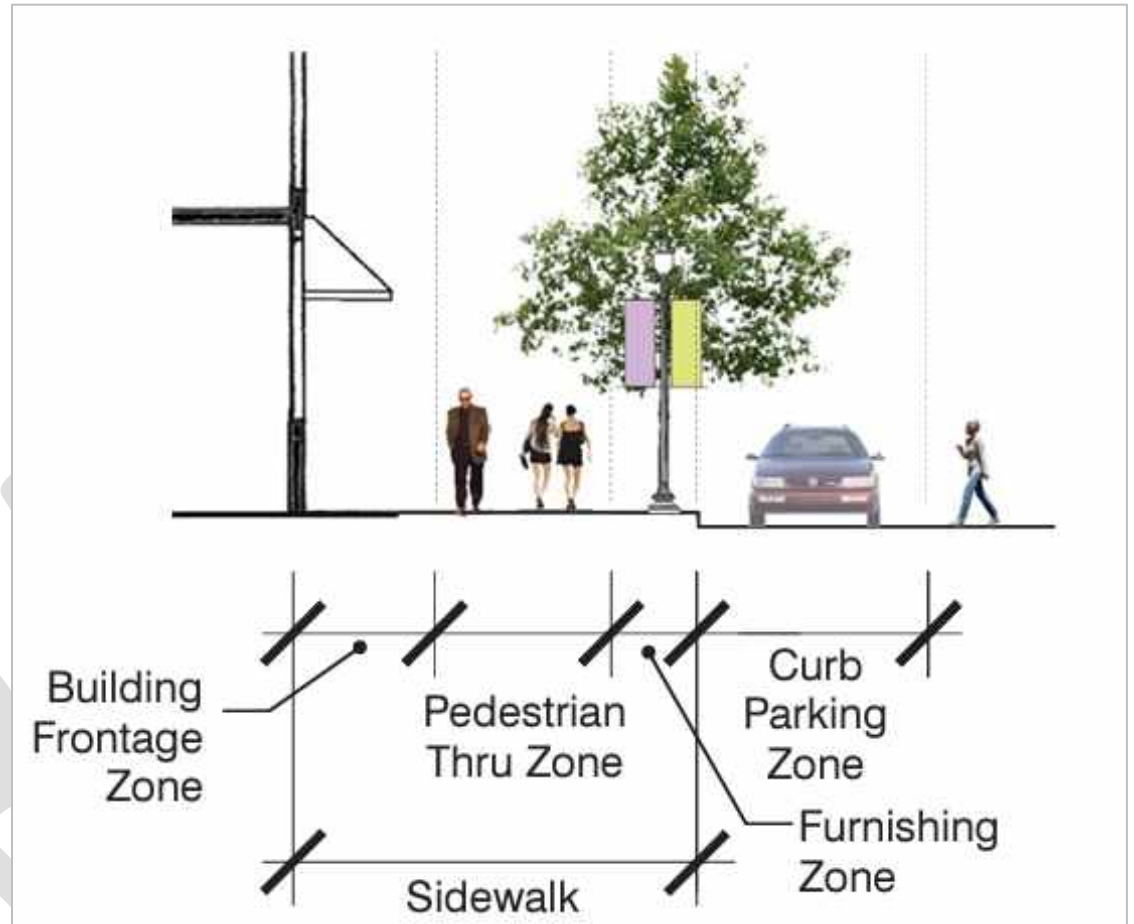
PERMANENT IMPROVEMENTS

Unless the applicant obtains a building permit, the operator shall not make any permanent changes, such as building structures or bolting tables to the ground. Permanent improvements within sidewalk areas must be in conformance with the applicable provisions and guidelines of the California Building Code and the Americans with Disabilities Act (ADA) accessibility requirements, and all other life, health, and safety requirements.

SIDEWALK CAFÉ ZONES

Any sidewalk cafe area may be viewed as containing several functional zones which must be respected at all times. The El Camino Real and Downtown Specific Plan* establishes sidewalk functional zones, ensuring a pedestrian clear zone and more pleasant and functional sidewalks. Understanding these zones will assist the applicant in determining the best layout for their sidewalk cafe area. These zones include:

- **Sidewalks:** The area dedicated to pedestrian activity made up of the building frontage, pedestrian thru, and furnishing zones, and adjacent to the curb parking or street traffic zone.
- **Building Frontage Zone:** The area of a public sidewalk located between the pedestrian thru zone and adjacent building or property line, assuming the sidewalk dimension allows for it. Depending on the location of the building, a frontage zone may accommodate outdoor seating and planting.
- **Pedestrian Thru Zone:** Allows for unimpeded pedestrian circulation, free of all obstruction, including utility boxes and fences for outdoor dining.
- **Furnishing Zone:** The buffer between the pedestrian thru zone and curb parking/street traffic. The furnishings zone accommodates public amenities such as street trees, streetlamps, benches, bike racks, kiosks, news racks, mailboxes, transit shelters, public art, plantings, utility poles and utility boxes. In some cases, the furnishings zone is also used for outdoor seating and dining by shops, cafes, and restaurants.
- **Curb Parking Zone:** The interface between the roadway and sidewalk and accommodates vehicular parking, and in most cases parklets.



**Please note that not all sidewalks within the Specific Plan are wide enough to incorporate each of the aforementioned zones. Sidewalk cafes should be planned according to the sidewalk zones and width in front of the building / restaurant.*

SIDEWALK CAFÉ SUBMITTAL REQUIREMENTS CHECKLIST

PERMIT SUBMITTAL REQUIREMENTS

The following shall be submitted to the **City of Menlo Park Public Works Department** as part of the Streetary Permit Application, submitted through Menlo Park's online permitting platform, Accela:

- Permit fee payment
- Site Plan (scaled - I.e., 1/8" = 1'10" | dimensioned - I.e., 5'0" clearance path) (refer to site plan checklist)
- Identification of Parklet Design Components (refer to Design Components Checklist)
- Photograph of proposed design components
- Photograph of proposed Streetary location (showing the front and side views along the business frontage)
- Required insurance forms (see below)
- Traffic control plan (for installation/construction)

INSURANCE REQUIREMENTS

- Completed Hold Harmless Agreement (review requirements listed)
- Insurance Endorsement Form
- Insurance Certificate (must name City as additional insured, see Hold Harmless Form)

Insurance Category	Minimum Limits
Worker's compensation	Statutory Minimum
Employer's Liability	\$1,000,000.00 per accident for bodily injury or disease
Commercial General Liability	\$1,000,000.00 (\$2,000,000.00 for parklets) per occurrence for bodily injury, personal injury, and property damage

Streetary permit(s) are revocable. Compliance with Streetaries (Chapter 13.30) and the design / program guidelines in this document is required.

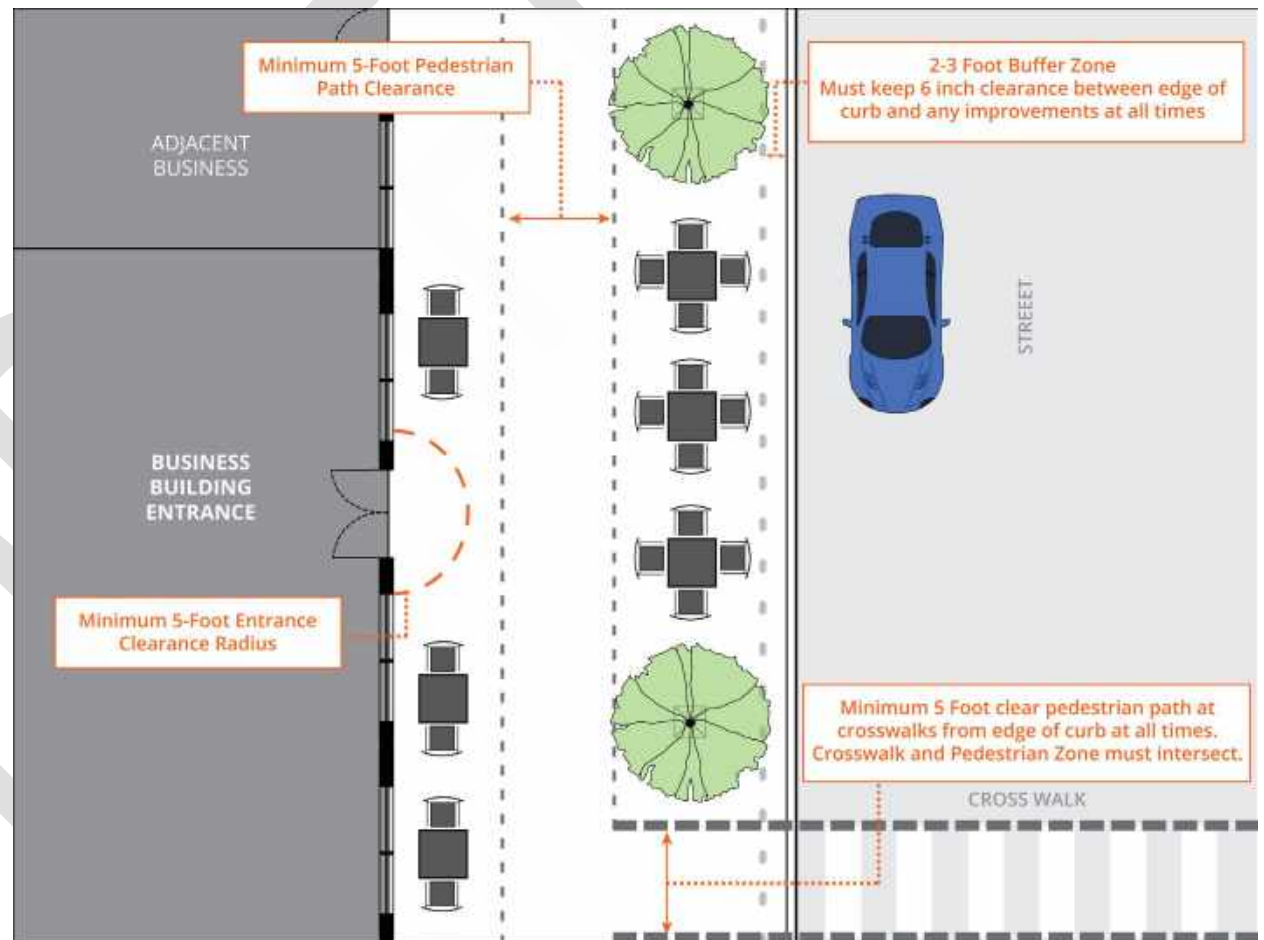
FURNISHINGS & FIXTURE DESIGN STANDARDS

STREETARY FURNISHINGS & FIXTURE DESIGN STANDARDS

FURNISHINGS AND FIXTURES DESIGN STANDARDS

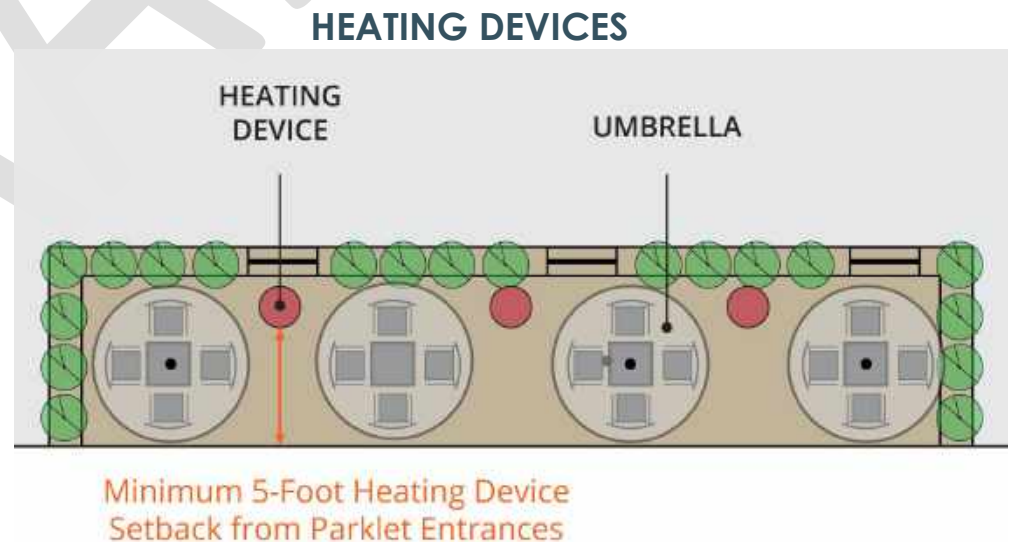
All furnishings and fixtures in Streetaries shall comply with the California Building Code, California Fire Code, and the Americans with Disabilities Act accessibility requirements.

- 1. TABLES, CHAIRS, AND SEATING.** Tables and chairs shall match and be made of safe, sturdy, and durable material, such as wood, steel, plastic, and wrought iron. All furniture shall be commercial grade and manufactured for outdoor commercial use. Vinyl tablecloths are not recommended.
 - As a general recommendation, round tables should be no larger than 2½ feet wide and square tables should be no larger than 3 feet wide. Tables shall meet clearance requirements for parklets and sidewalk cafes.
 - Accessible seating shall be provided and comply with the following California Building Code Sections:
 - i. 11B-305 Clear floor space of 30" by 48" at 5% of seating



ii. 11B-306 Minimum knee and toe clearances

- **Seating in Sidewalk Cafes:** Small round or square tables shall seat no more than four people unless otherwise approved by the City, subject to compliance with any required pedestrian clearance. Use of larger tables may be granted by the City provided that all clearance requirements are met.
2. **FURNISHINGS.** Furnishings and fixtures must be designed to be weighted down (or fixed to parklet) for wind protection. Items may need to be stored inside the business during periods of high winds. Please ensure that the weights used do not create tripping hazards for pedestrians. No furniture or any parts of a parklet or sidewalk café shall be attached, chained, or in any manner affixed to any tree, post, signs, sidewalk, streetlight, fire hydrant, or other public fixture within or near the licensed area. Applicant must demonstrate that required egress/door swing, etc., from occupancies is protected for occupant load to public way.
 3. **FIRE RESISTANT MATERIALS.** Any fabric, membrane shade, or other weather protection, including umbrellas and tents of any size, needs to be constructed of fire-resistant treated material as per the California Code of Regulations, Title 19, Division 1, Section 315(a). Fire-resistant materials must have documentation of a California State Marshal's Seal on all panels, or meet NFPA 702 testing requirements by a NRTL, or meet CPAI 84 standards.
 4. **PORTABLE HEATERS & OTHER HEATING DEVICES.** Heating devices may be electric, propane, or natural gas powered, and shall meet the following requirements:
 - Heating devices are prohibited inside of tents, canopies, and membrane structures.
 - A minimum 5-foot clearance shall be provided between a building and the heating device.
 - A minimum 5-foot clearance shall be provided between combustible materials and a heating device.
 - A minimum 5-foot clearance shall be provided between exits or exit discharges and a heating device.
 - Propane or natural gas heating devices shall have a tip over switch that shuts off the flow of gas.
 - Propane or natural gas heating devices shall have a maximum of 20 pounds' capacity gas container.
 - Propane storage shall remain outside in approved area; propane storage is not allowed inside.
 - **Gas heating appliances are NOT permitted on wood-based parklets, only electrical.**



5. **SHADING & UMBRELLAS.**

- Individual canvas or other non-vinyl umbrellas of a compatible design may be permitted in the right-of-way and shall not extend over the Pedestrian Zone.
- Umbrellas shall be made for outdoor commercial use subject to City approval. Any fabric, membrane shade, or other weather protection, including umbrellas and tents of any size, needs to be constructed of fire-resistant treated material as per the California Code of Regulations, Title 19, Division 1, Section 315(a). Fire-resistant materials must have documentation of a California State Marshal's Seal on all panels, or meet NFPA 702 testing requirements by a NRTL, or meet CPAI 84 standards.
- Umbrellas must be at least 80-inches above the parklet platform or sidewalk.
- Umbrellas and shading must not block sight lines for traffic or pedestrians in crosswalks.

6. **ELECTRICAL CONNECTIONS.** Electrical service to any parklet requires a permit. All wiring and electrical cords must be exterior rated, GFCI protected, and UL listed. Electrical extension cords between buildings and a parklet are not allowed.

- Cords within the parklet must not create tripping hazards on the sidewalk.
- The use of adapters is prohibited.
- Businesses are not allowed to tap into existing City electrical connections such as twinkle light outlets or streetlight poles.
- Generators are not allowed in association with parklets.

7. **LIGHTING.** Lighting is encouraged and may be provided through separately permitted electrical connections to the building. Solar powered (small scale) or battery powered lighting is encouraged.

- Lighting shall not be directed towards the roadway to unintentionally cause glare for vehicles, bicyclists, or pedestrians.
- Light strings must be hung to allow for a minimum clearance of 10 feet above the sidewalk and the platform of the parklet.
- Candles and open flames are prohibited within a parklet.
- No new foundations are permitted for light poles.

8. **SIGNAGE.** All signage must comply with the City's Sign Design Guidelines. Non-illuminated signs are allowed provided the sign area does not exceed 6 square feet (this is in addition to the sign ordinance maximum sign area limitations). For Streetaries/parklets on a corner, two signs not exceeding 9 total square feet may be permitted. Sign copy is limited to business identification, except that if the parklet is meant to serve the general public, signage should reflect such. If the parklet is meant to serve as restaurant seating, signage should correlate in design with the signage on the primary building. Menu boards, both portable and on walls, shall be subject to sign permit approval and shall comply with the Menlo Park Sign Ordinance.

9. **LANDSCAPING AND PLANTERS.** All planters and planting areas must contain seasonal blooming live plants year round. The sidewalk café and/or parklet owner shall be responsible for the prompt removal of all empty or poorly maintained planting areas. Landscaping and planters must be maintained, kept healthy and free of litter/debris at all times.

- Neither the planter or plant material shall impede or hinder pedestrian and vehicular visibility.

- Landscaping or planter options include planter boxes within the setback area or serving as the parklet platform enclosure; hanging planters; or raised planter beds.
- Plants must be drought tolerant and/or native. Edible or fragrant plants are encouraged. Plants that have thorns, spikes, or sharp edges, or that are poisonous or invasive, are not permitted.
- ***Parklet Landscaping & Planters Standards:***
 - i. Planters are allowed within the parklet setback area.
 - ii. The height shall be between 30 and 36-inches in height.
 - iii. The height of plants within planters in parklet setback areas or planters serving as parklet platform enclosures shall not exceed 6-inches in height.
- ***Sidewalk Café Landscaping & Planters Standards:***
 - i. The cafe owner may use flower boxes or planters to define the boundaries of the cafe seating area.
 - ii. To avoid blocking the patron’s vision while seated, the combined height of the planters and live plants shall not exceed 4 feet from sidewalk grade.
 - iii. The boxes or planters in Sidewalk Cafes shall be no greater in height than 3-feet and no wider than 2 feet at the base.
 - iv. Plant materials and containers shall remain at or below shoulder height of a seated person so as to not block visibility of the seating area.
 - v. The flower boxes, planters, and storage bins shall be portable and be made of safe, durable, and attractive material such as wood or steel.

10. BARRIERS/FENCES

- ***Parklet Barriers/Fences:*** Refer to Parklet Design Standard Section C. “Platform barriers, railings, or enclosures”
- ***Sidewalk Café Barriers/Fences:*** Barriers should provide visual and physical connections with the surrounding public realm. Barriers should have a low height that allow views into and out of the space with at least one opening between barriers for an area of passage. The use of barriers should be limited and only occur where needed. All in-ground improvements require a building permit which are subject to approval by the Engineering Department. All barriers and fences shall be shown on the application site plan and are subject to approval by the City.

11. TRASH RECEPTACLES. The number, type, and location of trash receptacles shall be determined by the City and shall comply with the public right-of-way access code. Trash receptacles shall be made of durable material to match the existing facade and shall be commercial grade as determined by the City.

GENERAL OPERATION & MAINTENANCE

OPERATION AND MAINTENANCE FOR STREETARIES

- A. LIVE ENTERTAINMENT.** No live entertainment or amplified music is permitted in Streetaries without first obtaining a Special Events Permit.
- B. NO OUTDOOR FOOD PREPARATION.** Outdoor food preparation is not permitted. No heating, cooking or open flames are permitted in a parklet. Hotpots, barbecues, or other heating of food in the public right of way is not allowed.
- C. NO OPEN FLAMES.** Candles and open flames are not allowed on the parklets.
- D. ALCOHOL SERVICE.** Restaurants and food establishments licensed by the State of California will be permitted to sell and allow on premise consumption of alcoholic beverages. Establishments that serve alcoholic beverages in the parklet area shall be required to meet all requirements of the Alcoholic Beverage Control Board and any other federal, state, or local laws and regulations governing the sale and consumption of alcoholic beverages.
- E. NOISE & DISRUPTIVE BEHAVIOR.** Streetary owners are responsible for ensuring their patrons minimize noise and disruptive behavior while using their Streetary space.
- F. SEMI-PERMANENT CONSTRUCTION.**
 - 1. Noise generated by construction activities is regulated in the City of Menlo Park. Semi-permanent construction activities in a parklet or sidewalk café must adhere to the city's [noise ordinance and allowable construction hours](#).
 - 2. Traffic Control Plans are required for construction in the right-of-way. Traffic control plans must be completed in accordance with the latest edition of the "Manual of Uniform Traffic Control Devices for Construction and Maintenance Work Zones" issued by the State of California, Department of Transportation and as required in the City of Menlo Park's Guidelines for [traffic control plans](#).
- G. SITE MAINTENANCE.** All Streetaries must be maintained at all times by the Streetary owner, including the space leading to and from the parklets and sidewalk cafes.
 - 1. The permittee is required to keep the parklet or café area safe, free of debris, grime, and graffiti, and to keep any plants in good health.
 - 2. Outdoor seating shall be scrubbed and mopped to remove any food or drink stains on a daily basis and the sidewalk shall be power washed quarterly by the permittee. Such cleaning shall be in accordance with the City's [Storm Water Management and Discharge Control Program](#), which prohibits any discharge other than storm water into the storm water drainage system.
 - 3. Parklets and enclosures shall be designed in a manner that does not negatively impact existing drainage patterns. Features shall be incorporated into the design that allows water to flow freely off the street surface and along any existing gutter.
 - 4. The permittee shall be required to clean tree grates, the gutter, and drainage inlets. Business and/or property owners shall be required to remove any debris that accumulates against their parklet. The areas on, under, and around the parklets must be clear of leaves and

debris, which may require the restaurant operator or property owner to blow underneath the parklet or enclosure. The permittee shall also provide cleanout access upon request.

5. The permittee is required to maintain the pedestrian access clearances in sidewalk café and curb extension area and keep these areas safe, clean, and free of debris.
6. Parklets shall not block access necessary to maintain the canopy of City maintained street trees.
7. Failure to maintain the cleanliness, safety, and accessibility of a parklet or café may subject the permittee to violations and fines. If maintenance issues are not resolved, a parklet permittee may be required by the City to remove the parklet at the permittee's expense.

H. SIDEWALK CLEANING

The permittee shall comply with all State and local regulations related to waste disposal including the National Pollutant Discharge Elimination System (NPDES) and the California Storm Water Quality Association (CASQA) as follows:

1. **Pollution Prevention** (visit www.cabmphandbooks.com for more information)
 - Use dry cleaning methods whenever practical for surface cleaning activities.
 - Use the least toxic materials available (e.g., water-based paints, gels or sprays for graffiti removal).
2. **Surface Cleaning**
 - Regular broom (dry) sweep sidewalk, plaza, parklet, and parking areas to minimize cleaning with water.
 - Dry cleanup first (sweep, collect, and dispose of debris and trash) when cleaning sidewalks or plazas, then wash with or without soap.
 - Block the storm drain or contain runoff when cleaning with product.

I. ADA COMPLIANCE. Streetary seating areas must, at all times, comply with all requirements of the Americans with Disabilities Act (ADA) and provide sufficient clearance and walkway space to allow safe access and egress. For multi-level Streetaries, a minimum of one level of ADA access shall be required.

J. HOURS OF OPERATION. Streetaries shall adhere to the same approved hours of operation as the associated business, with the following limitations:

1. *For Streetaries that are within 150-foot horizontal or vertical distance from residences:* Streetaries shall not commence operations prior to 7 a.m. on Saturdays, Sundays, and holidays; Streetaries shall not commence operations prior to 6 a.m. Monday through Friday; Streetaries shall not operate after 10 p.m. on Fridays and Saturdays; Streetaries shall not operate after 9 p.m. Sunday through Thursday and holidays (as defined in the Menlo Park Municipal Code chapter 8.13 "Noise" of title 8).

K. USAGE OF STREETARY SPACE. Streetaries must demonstrate adequate usage in order to contribute to the economic vitality of the city. The city requires these Streetaries to be set up and readily available for use during the operational hours of the food service establishment associated with the Streetary, except in inclement weather and subject to the hours of operation.

L. FURNITURE & STORAGE.

1. Applicants may store parklet or sidewalk café tables, chairs, portable heaters, umbrellas, and other furnishings within the business, parklet, or café area. No storage containers shall be allowed in the parklet or café area, except for propane storage with approval by city staff. Restaurants and cafes may use minimal storage space (i.e., waiter stations for storage of utensils, linens, and menus) during operating business hours to store.
2. Storage of furnishings shall not be allowed along the sidewalk or any required pedestrian clearances.
3. Furnishings shall not be secured to lampposts, streetlights, trees, or any other public street furniture/infrastructure.
4. All approved furnishings shall be properly maintained and cleaned regularly.
5. Propane storage shall remain outside in approved area; propane storage is not allowed inside.

M. LANDSCAPING.

The café operator shall maintain flower boxes and planting areas unless otherwise determined by the City. All boxes, planters and planting areas shall be planted with seasonal blooming or ornamental evergreen live plants year round.

N. PUBLIC SAFETY EMERGENCIES AND REMOVAL.

1. Because a parklet may be installed above underground utilities, there may be occasions in which a parklet may need to be removed with little or no notice. In the event of a utility failure, such as a gas leak or similar threat to public safety, the city or a utility provider may remove a parklet with little or no notice. Sidewalk cafes may also be removed with little or no notice where there is a threat to public safety. The parklet or sidewalk cafe permittee is responsible for the cost of re-installing and restoring any damage to the parklet or sidewalk cafe.
2. When the permittee removes their parklet or sidewalk cafe, the permittee shall be responsible for notifying the City and removing the parklet at the permittee's expense.
3. If the business associated with a parklet or sidewalk café ceases operation, changes its business name or type of use permit, or decides to no longer maintain its parklet or sidewalk cafe for more than 30 days, the City may remove and dispose of the parklet or sidewalk cafe at the permittee's expense.
4. The permittee shall be responsible for any surface restoration required after the parklet or sidewalk cafe removal. A separate [Encroachment Permit](#) shall be required for any restoration or other work performed in the public right-of-way. Restoration of the public right-of-way shall be done by a licensed contractor, to City of Menlo Park standards and shall be completed to the satisfaction of the City Engineer.

STREETARY SITE PLAN TEMPLATES

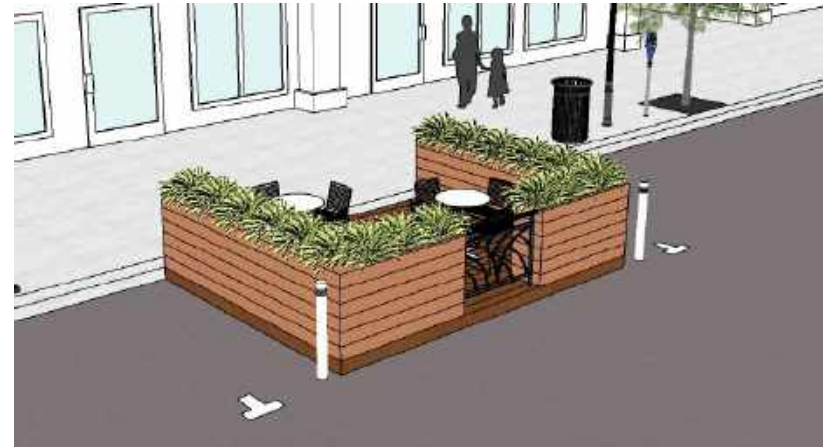
SELECT A STREETARY TEMPLATES

- Parklet in parallel parking space (s)
 - A-1 One Space
 - A-2 Two Space
- Parklet in a diagonal parking space
- Special Condition Parklet, which includes three alternative conditions
 - C-1 Parklet/Sidewalk Café Combination
 - C-2 Parklet/Sidewalk Café Curb Extension
 - C-3 Parklet/Pedestrian Street Combination

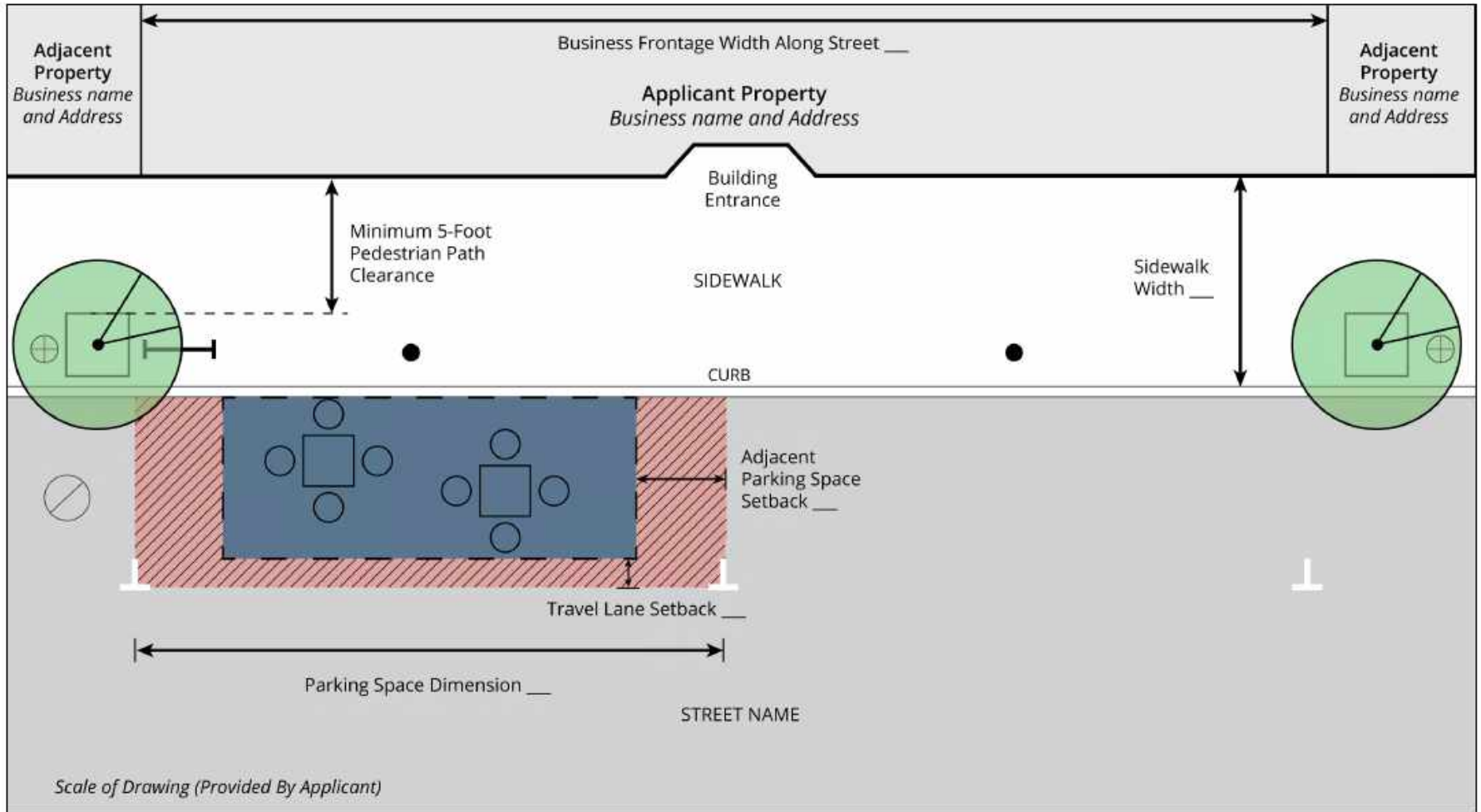
A-1 PARALLEL SPACE PARKLET

1 PARKING SPACE

- The Parallel Parklet template occupying one parking space is a compact parklet that provides an intimate outdoor dining setting.
- This template limits the maximum parklet area to one parking space and can be utilized in areas where available on-street parking spaces are limited.
- Parklets proposed for perpendicular parking spaces can use a modified version of this template.



SAMPLE SITE PLAN | A-1 PARALLEL SPACE PARKLET (1 Parking Space)



- | | | | | | | | |
|--|-----------------------|--|------------------------|--|-----------------------|--|--|
| | Existing Street Light | | Existing Utilities | | Parking Space Marking | | Proposed Parklet Location and Dimensions |
| | Existing Street Tree | | Existing Parking Meter | | Bike Rack | | Required setbacks |
| | Parklet Table | | Parklet Chair | | Fence/Barrier | | |

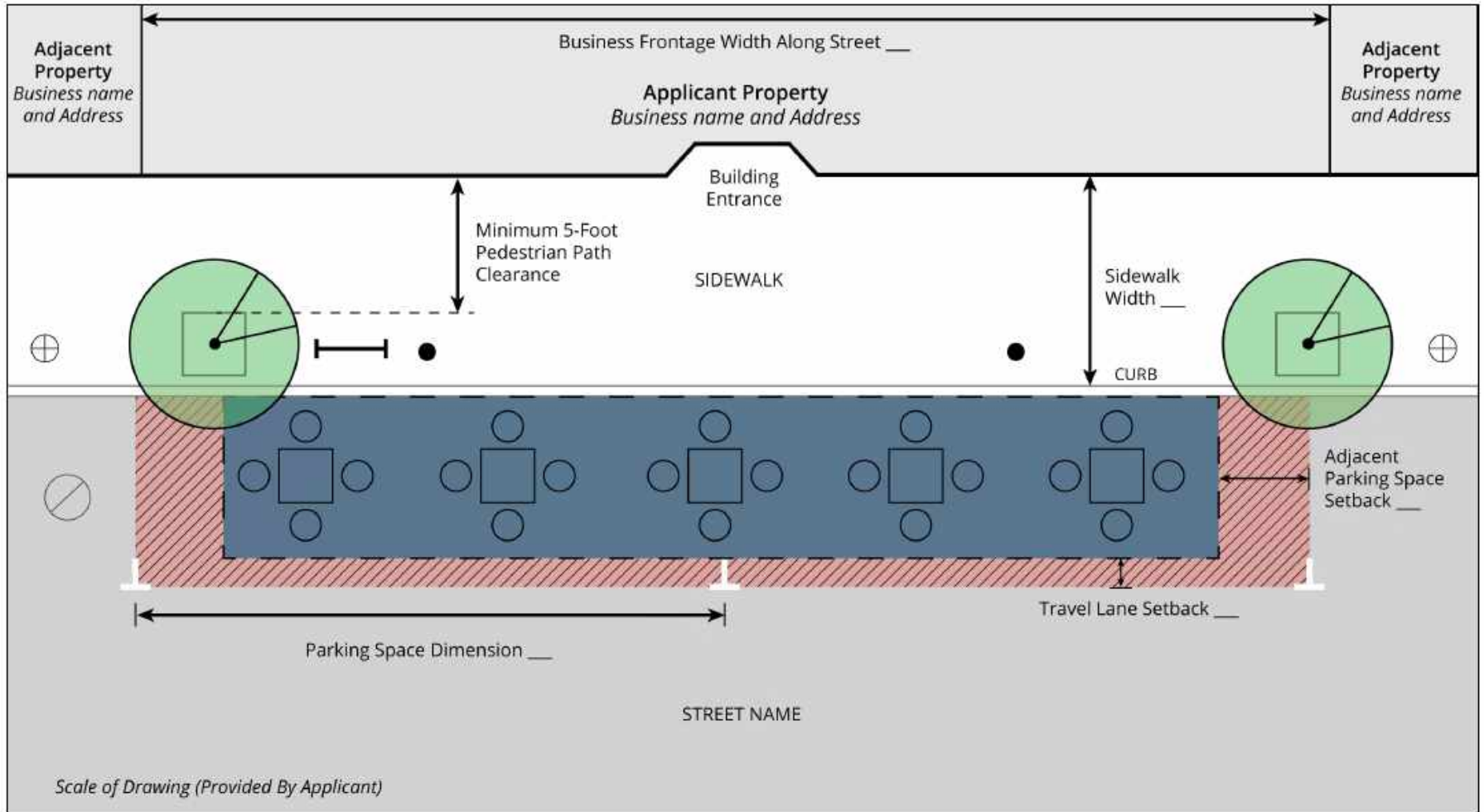
Example site plan for illustrative purposes only; complete site plan requirements provided on Page ___

A-2 PARALLEL SPACE PARKLET 2 PARKING SPACES

- The Parallel Parklet template occupying two parking spaces provides a larger outdoor dining setting.
- This template limits the maximum parklet area to two parallel parking spaces.
- Parklets proposed for perpendicular parking spaces can also use a modified version of this template.



SAMPLE SITE PLAN | A-2 PARALLEL SPACE PARKLET (2 Parking Spaces)



- | | | | | | | | |
|--|-----------------------|--|------------------------|--|-----------------------|--|--|
| | Existing Street Light | | Existing Utilities | | Parking Space Marking | | Proposed Parklet Location and Dimensions |
| | Existing Street Tree | | Existing Parking Meter | | Bike Rack | | Required setbacks |
| | Parklet Table | | Parklet Chair | | | | |

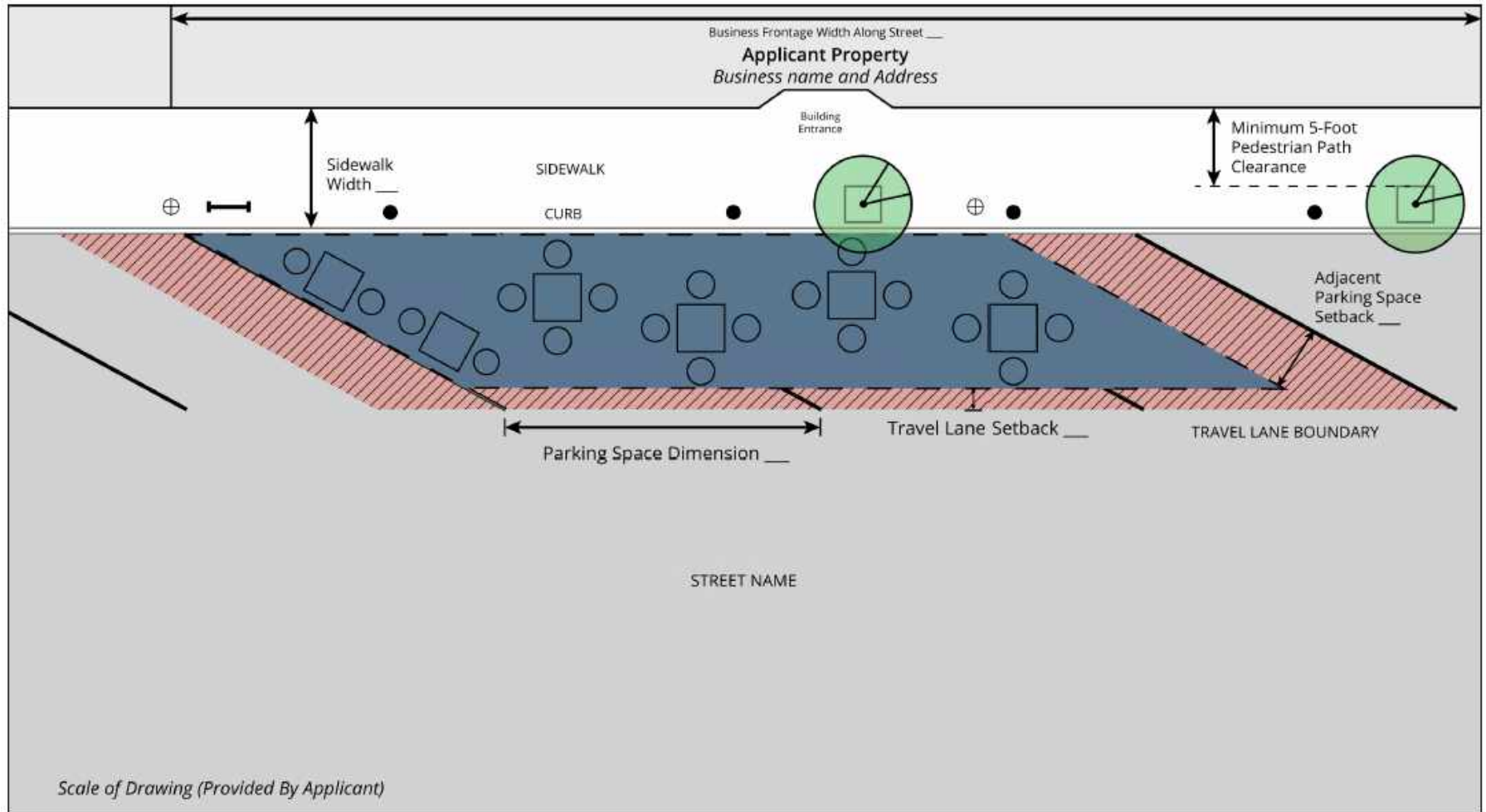
Example site plan for illustrative purposes only; complete site plan requirements provided on Page ___



B-DIAGONAL SPACE PARKLET

The Diagonal Space Parklet template provides a larger outdoor dining setting. When including the necessary setbacks, this template would need three spaces.

SAMPLE SITE PLAN | B-DIAGONAL SPACE PARKLET



- | | | | | | | | |
|--|-----------------------|--|------------------------|--|-----------------------|--|--|
| | Existing Street Light | | Existing Utilities | | Parking Space Marking | | Proposed Parklet Location and Dimensions |
| | Existing Street Tree | | Existing Parking Meter | | Bike Rack | | Required setbacks |
| | Parklet Table | | Parklet Chair | | Fence/Barrier | | |

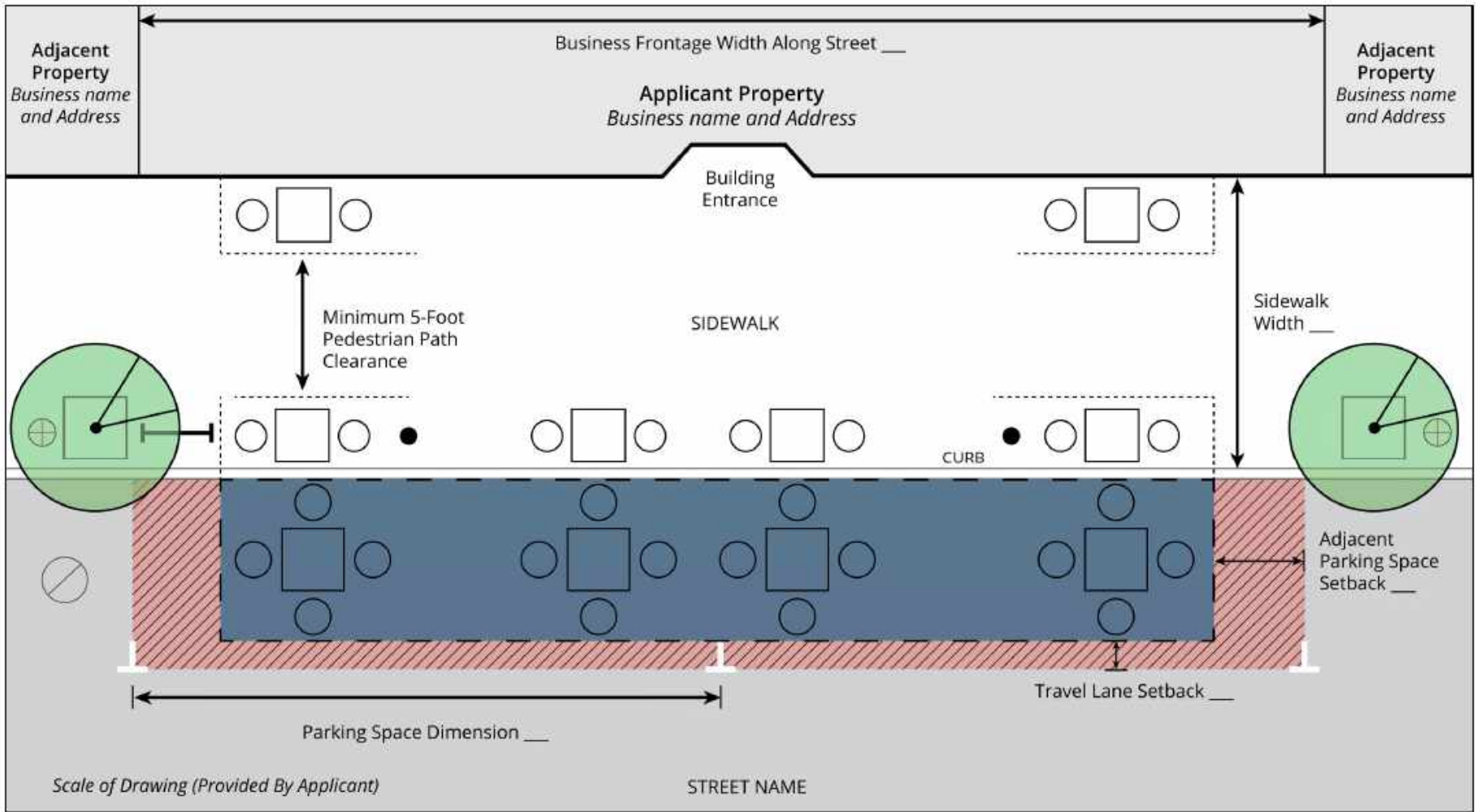
Example site plan for illustrative purposes only; complete site plan requirements provided on Page ___

C-1 PARKLET/SIDEWALK CAFÉ COMBINATION

- The Parklet/Sidewalk Cafe Combination template is for conditions where an applicant can incorporate both a sidewalk cafe and a parklet for a combined space.
- This Special Condition template can be used in either a parallel parking or diagonal parking site.
- The combined space should be designed as a comprehensively planned singular space.



SAMPLE SITE PLAN | C-1 PARKLET/SIDEWALK CAFE COMBINATION



- | | | | | | | | |
|--|-----------------------|--|------------------------|--|-----------------------|--|--|
| | Existing Street Light | | Existing Utilities | | Parking Space Marking | | Proposed Parklet Location and Dimensions |
| | Existing Street Tree | | Existing Parking Meter | | Bike Rack | | Required setbacks |
| | Parklet Table | | Parklet Chair | | Fence/Barrier | | |

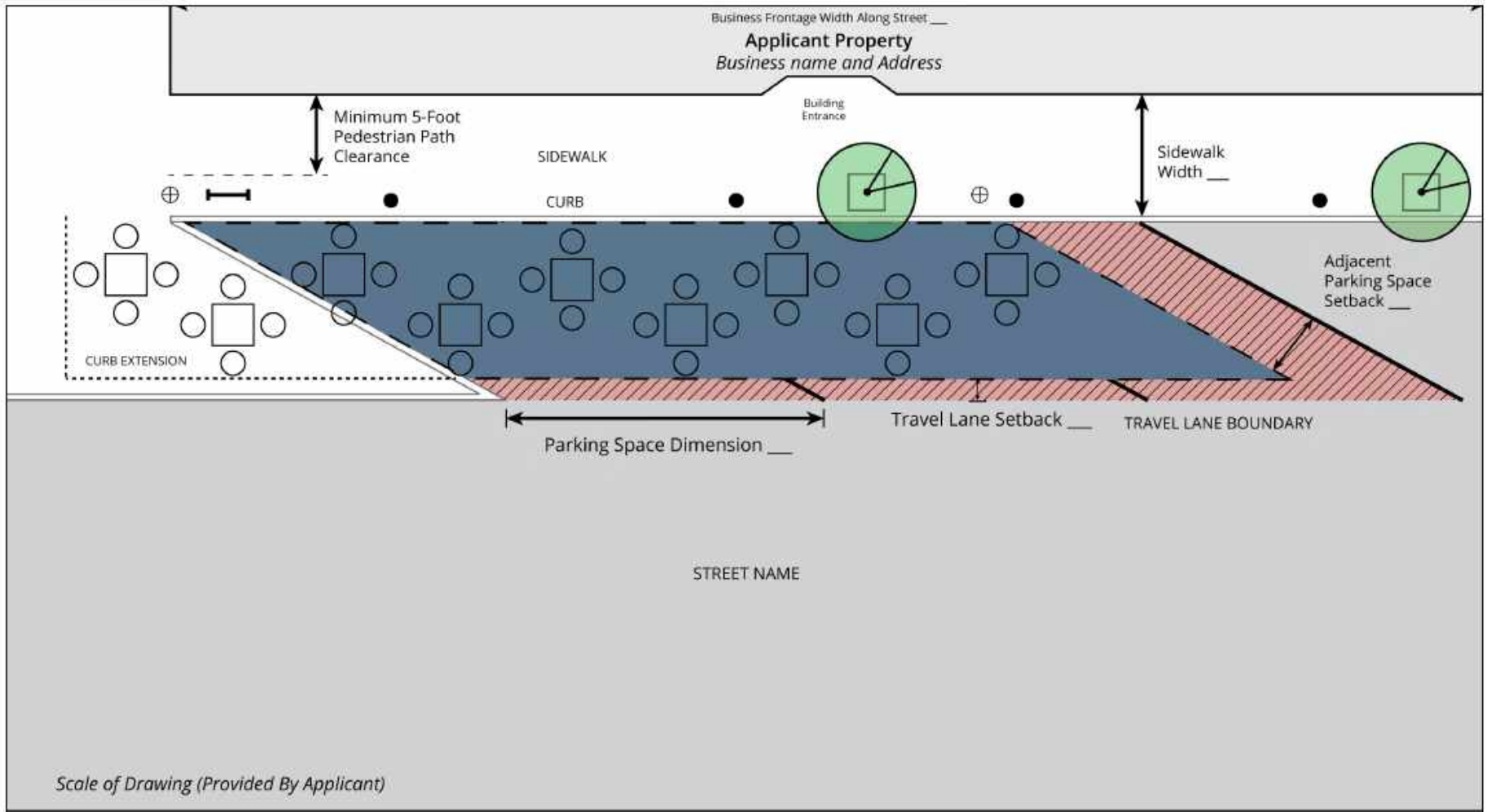
Example site plan for illustrative purposes only; complete site plan requirements provided on Page ___



C-2 PARKLET/SIDEWALK CAFÉ CURB EXTENSION COMBINATION

- The Parklet/Sidewalk Cafe Curb Extension Combination template is provided for sites where a business would like to incorporate a sidewalk cafe on an adjacent curb extension with a parklet.
- This Special Condition template can be used in either a parallel or diagonal parking site adjacent to an intersection.
- The combined space should be designed as a comprehensively planned singular space.

SAMPLE SITE PLAN | C-2 PARKLET/SIDEWALK CAFE CURB EXTENSION COMBINATION



Scale of Drawing (Provided By Applicant)

- | | | | | | | | |
|--|-----------------------|--|------------------------|--|-----------------------|--|--|
| | Existing Street Light | | Existing Utilities | | Parking Space Marking | | Proposed Parklet Location and Dimensions |
| | Existing Street Tree | | Existing Parking Meter | | Bike Rack | | Required setbacks |
| | Parklet Table | | Parklet Chair | | Fence/Barrier | | |

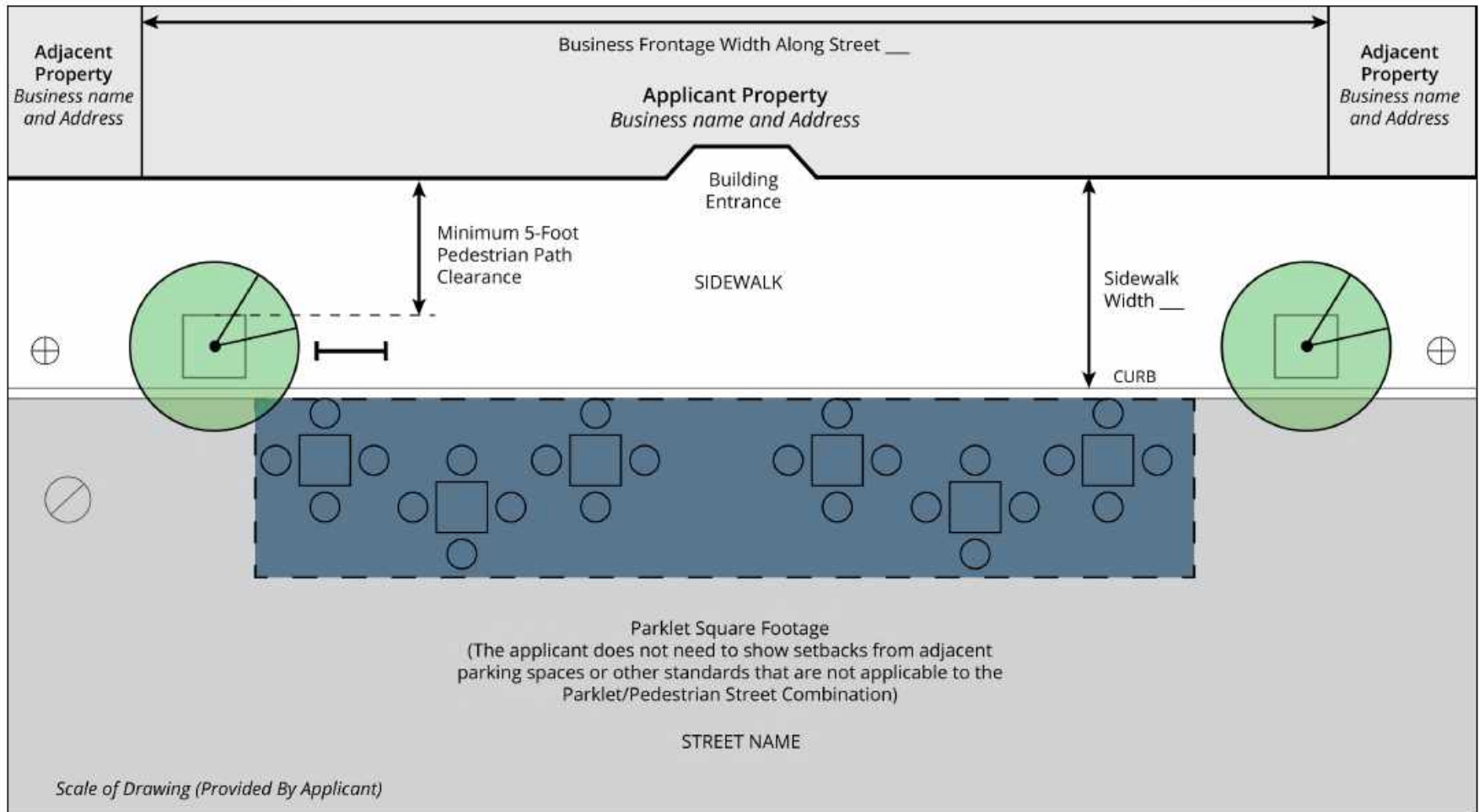
Example site plan for illustrative purposes only; complete site plan requirements provided on Page

C-3 PARKLET/PEDESTRIAN STREET COMBINATION

- The Parklet/Pedestrian Street Combination template is designed with flexibility for parklets that are proposed on streets the City has permanently closed to vehicular through-traffic to enable pedestrian access and circulation.
- This template shall not be used where streets are only temporarily closed.
- Expanded seating areas would not be allowed unless the supplementary seating is specifically reflected in plans submitted and approved by the City.



SAMPLE SITE PLAN | C-3 PARKLET/PEDESTRIAN STREET COMBINATION



- ⊕ Existing Street Light
- ⊗ Existing Utilities
- ⊥ Parking Space Marking
- Proposed Parklet Location and Dimensions
- 🌳 Existing Street Tree
- Parklet Chair
- Bike Rack
- Parklet Table

Example site plan for illustrative purposes only; complete site plan requirements provided on Page ___

Proposed Streetary Fee Structure

PROPOSED FEES FOR CITY OF MENLO PARK STREETARY OUTDOOR DINING PERMIT			
STREETARY PARKLET	1ST YEAR	ANNUAL RENEWAL	NOTES
Streetary permit - parklets	\$1,725	\$250	First year permit application same as administrative permit fee for outdoor dining on private properties. Applicants may be subject to a surcharge for a structural review. Renewal fee covers administration.
Use of space (estimated at \$4.24/sf for 360sf, total may vary)	\$1,526.40	\$1,500	Use of space recommended cost is \$4.24/SF for an estimated average of 360 SF per 2-stalls. Total Use of space may vary depending on total area occupied by the business. Renewal fee covers use of space.
Recommended cost for streetary parklets	\$3,251.40	\$1,750	Staff recommend fees are waived until January 1, 2025, if business complies by the end of the transitional period described below.
STREETARY SIDEWALK CAFÉ	1ST YEAR	ANNUAL RENEWAL	NOTES
Streetary permit - sidewalk cafes (stand-alone)	\$810	\$250	First year permit application same as a major encroachment permit fee. Renewal fee covers administration.
Use of space (estimated at \$4.24/sf for 150 sf, total may vary)	\$636	\$600	Use of space recommended cost is \$4.24/SF for an estimated 150 SF per sidewalk café. Total use of space may vary depending on total area occupied by the business. Renewal paid annually for use of space
Recommended cost for streetary sidewalk cafes	\$1,446	\$850	Staff recommend fees are waived until January 1, 2025, if business is in compliance by the end of the transitional period described below.

Outdoor Dining Fee Structures in Comparable / Nearby Cities

CITY	PERMIT FEE & PARKLET 1ST YEAR (2 STALLS/360 SF)	ANNUAL USE & CLEANING FEES	NOTES
BURLINGAME	\$431 permit fee + \$1/ every SF over 200-sf over max \$5,100 for 1 st year use of space = \$5,531.00	\$1,500 use fee \$3,000 cleaning fee = \$4,500.00	
LOS ALTOS	\$500 permit fee	\$100 use fee	
MORGAN HILL	\$716.10 permit fee \$1,000 security deposit = \$1,716.10	\$1,000 use fee	Fees waived until January 1, 2023. Permit fee is paid every 7-years. Annual use fee is only charged for parklets in the public right-of-way
MOUNTAIN VIEW (DOWNTOWN CASTRO ST)	\$769 permit fee \$2,400 for 1 st year use of space = \$3,169	\$205 use fee \$180 cleaning fee = \$385	
REDWOOD CITY	\$2,226 permit fee \$5,883.60 for 1 st year use fee = \$8,109.60	\$4,823.60 use fee (Annual use of space fee = \$10.16/SF)	Annual use fee waived until 2024
SAN CARLOS	\$539 permit fee \$1,289 for 1 st year = \$1,828	\$289 use fee \$705 cleaning fee = \$994	
SAN MATEO	\$500 permit fee \$1,000 for 1 st year use fee = \$1,500	\$500 use fee	\$500 encroachment permit application + 250/stall first year Annual renewal of \$250/stall
SAN RAFAEL	\$2,000 permit fee \$2,000 one-time deposit = \$4,000 (half refundable)	\$3,600 use fee	Permit fee waived through 2023. Annual Use Fee waived until June 1, 2023. 50% fee discount applies between June 1, 2023 and May 30, 2024. 25% fee discount applies between June 1, 2024 and May 30, 2025. No discounts or waivers beginning June 1, 2025.

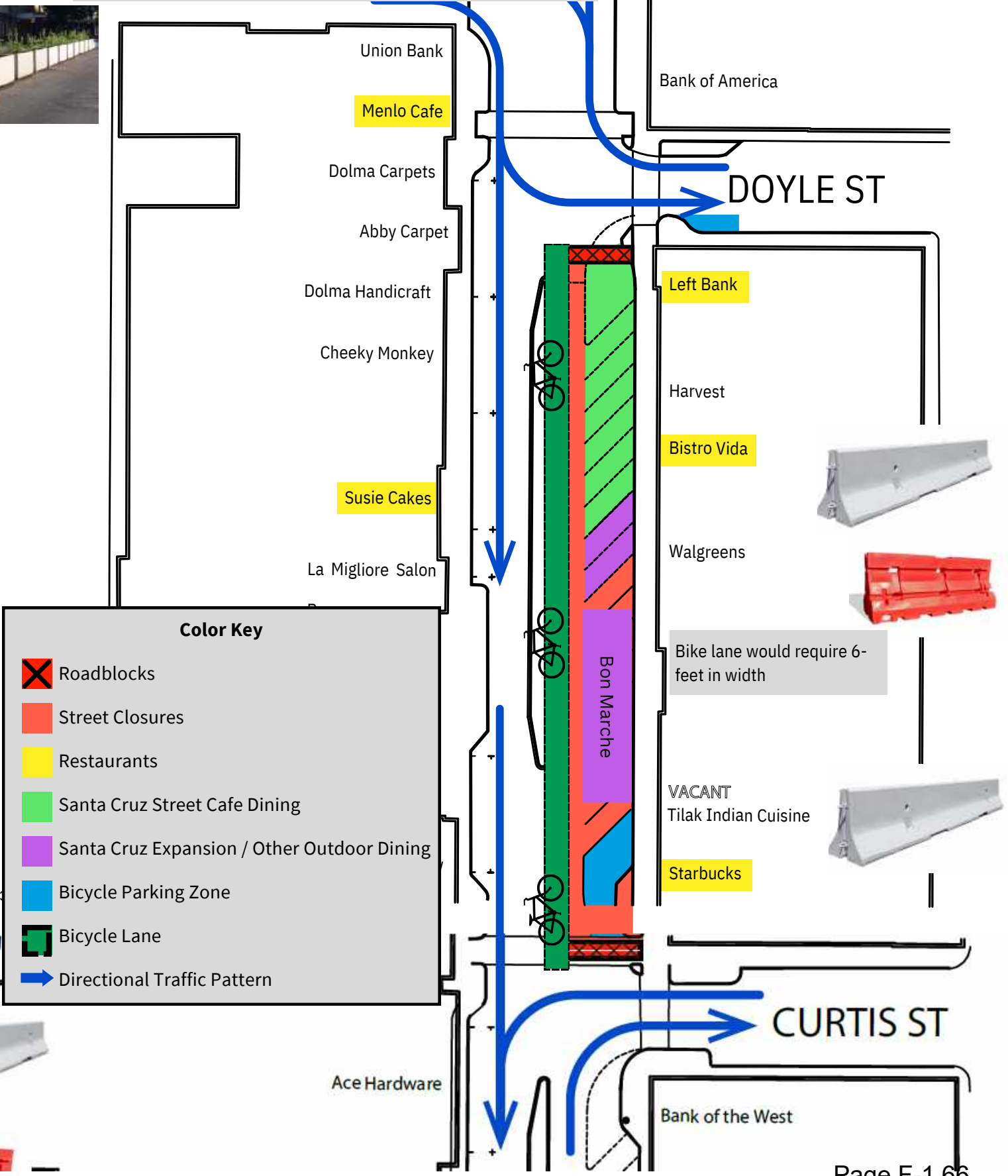
Santa Cruz Ave Reopening Option 1.

Reduce the size of the street closure for 4 months to allow for bicycle travel lane during transition.



North

PLATE 1 South



Color Key

- Roadblocks
- Street Closures
- Restaurants
- Santa Cruz Street Cafe Dining
- Santa Cruz Expansion / Other Outdoor Dining
- Bicycle Parking Zone
- Bicycle Lane
- Directional Traffic Pattern

Santa Cruz Ave Reopening Option 2.

Maintain street closures as is for 4 months and prepare for re-opening at the end of the transitional period.

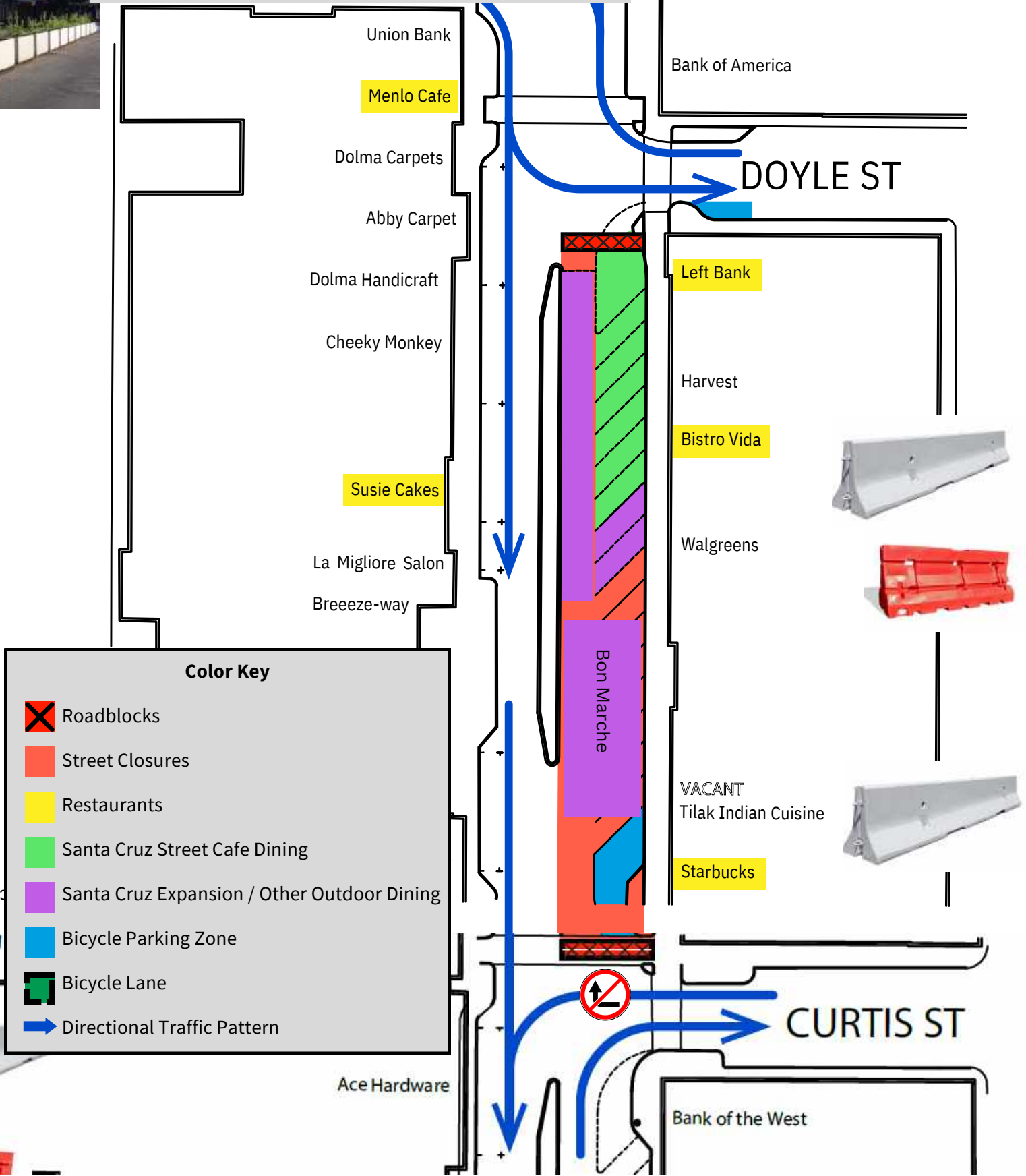
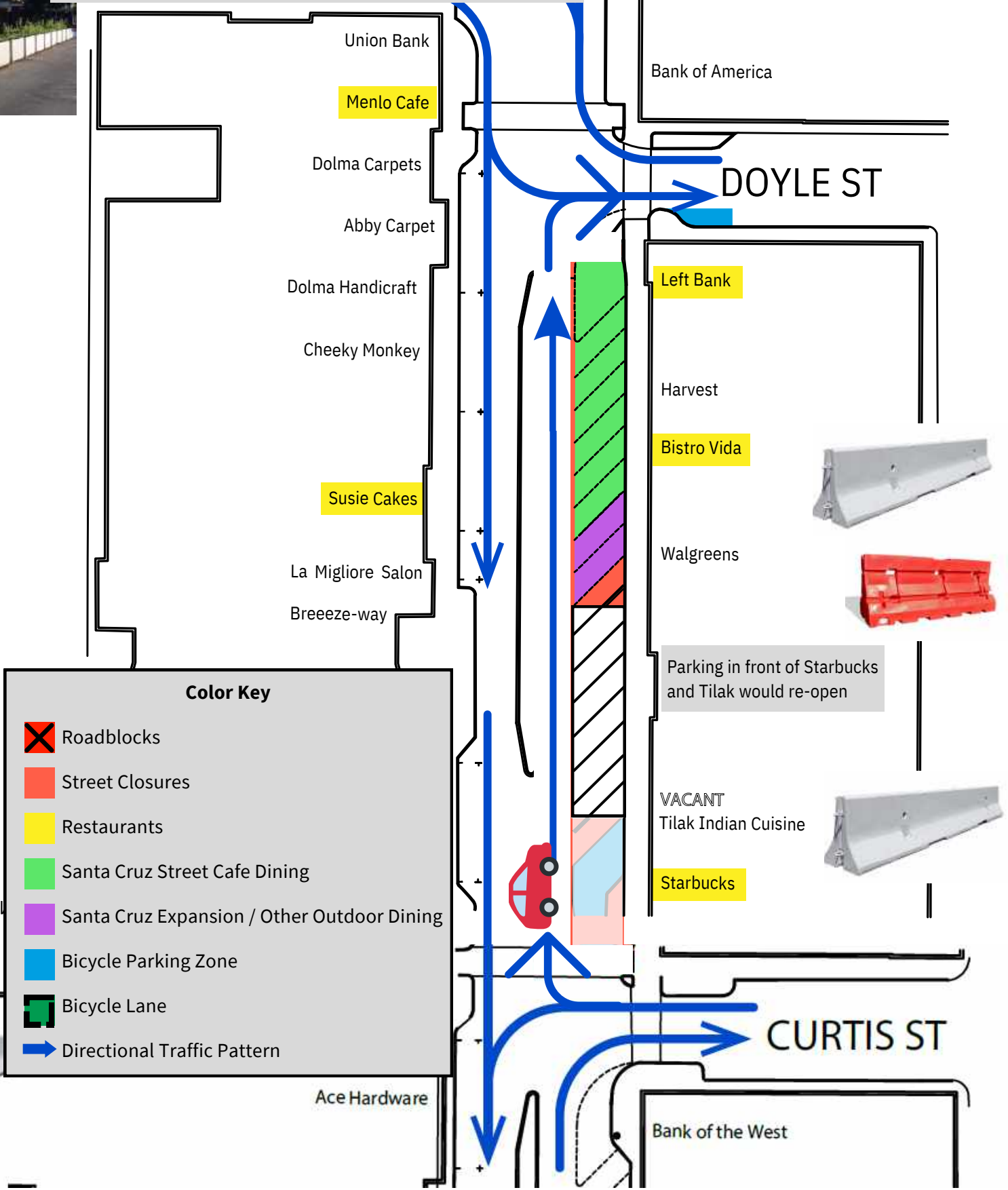


PLATE 1 South

Santa Cruz Ave Reopening Option 3.

Reopen the street to vehicular traffic by a specific date.



CHESTNUT ST

Ryans Lane Reopening Option 1.

Maintain street closures as is for 4 months and prepare for re-opening at the end of the transitional period.

North

ESCONDIDO LN

RYANS LANE

CRANE ST

Le Boulanger


Bagel Street Café

Discovery Shop

Carpaccio

Match Line

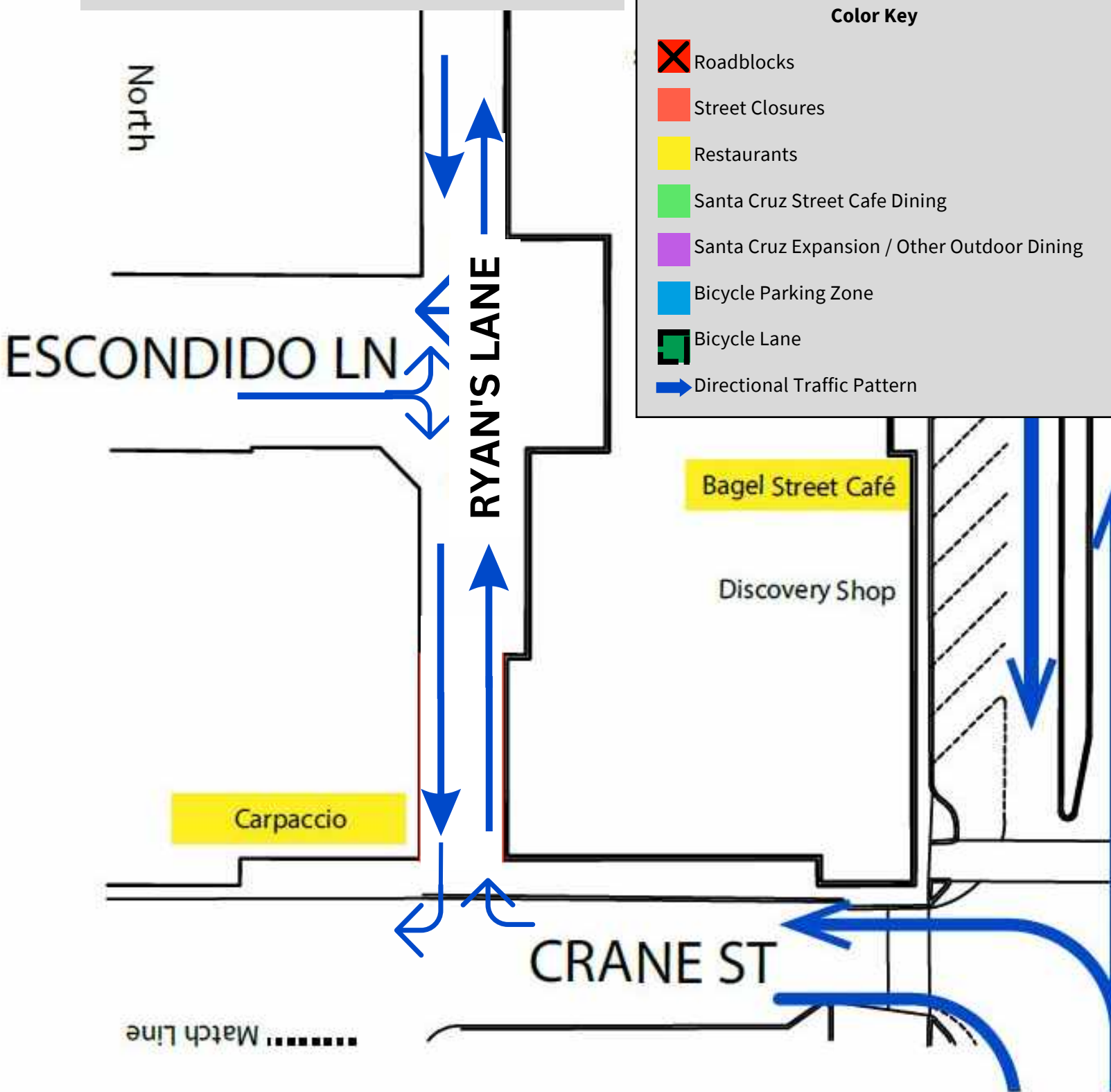
Color Key


-  Roadblocks
-  Street Closures
-  Restaurants
-  Santa Cruz Street Cafe Dining
-  Santa Cruz Expansion / Other Outdoor Dining
-  Bicycle Parking Zone
-  Bicycle Lane
-  Directional Traffic Pattern

CHESTNUT ST

Ryans Lane Reopening Option 2.

Reopen the street to vehicular traffic by a specific date.





“STREETARIES” OUTDOOR DINING PROGRAM

February 28, 2023 Study Session



AGENDA

- Study Session Purpose
- Background
- Existing Outdoor Uses
- Summary of Business Feedback
- Proposed Streetary Program
- Discussion of Santa Cruz Ave and Ryans Lane





MEETING PURPOSE

- Study Session to provide feedback on:
 - (1) Proposed Streetaries outdoor dining program
 - (2) Options for Santa Cruz Avenue and Ryans Lane
- No City Council action is required for this item; staff request feedback and direction



BACKGROUND

- **Temporary Outdoor Use Permit (TOUP) Program will expire** when the COVID-19 State of Emergency Order ends on February 28, 2023
 - January 2022: Council asked staff to research a longer-term outdoor dining program
 - January 10, 2023: Staff presented a draft outdoor dining design standard packet as an informational item
 - February 28, 2023: Study session includes the Streetary Outdoor Dining program, which establishes a long-term policy for outdoor dining in the public right-of-way



EXISTING OUTDOOR USES

- 12 food service establishments with existing outdoor uses
- 5 retail/personal service businesses with existing outdoor uses
- Note: 7 of the 17 parklets were built as part of the Santa Cruz Street Café Pilot Program, some of which expanded under TOUP



SUMMARY OF BUSINESS FEEDBACK ON A LONGER-TERM PROGRAM



Restaurants are not fully recovered from the pandemic and outdoor dining has helped them stay open

Retail businesses with outdoor uses generally agree that outdoor dining should continue

Some businesses would like to maintain the street closure but are okay with reopening

Other groups have indicated interest in opening the closed streets

Existing parklet owners are interested in a cost sharing aspect as part of a longer term program

Customers continue to choose outdoor dining

A photograph of a street scene in Menlo Park. In the foreground, there are green plants with red flowers. A black metal railing runs across the middle ground. In the background, a building has a brown oval sign that says "harvest" and a red circular sign that says "TABAC". Further down the street, there are more buildings, trees, and a person walking. The scene is brightly lit, suggesting a sunny day.

PROPOSED STREETARY PROGRAM



STREETARY OUTDOOR DINING PROGRAM

- “Streetary” or “Streetaries” are an **outdoor eating area** that operates within and uses the **public right-of-way** directly adjacent to the food service establishment street frontage.
- The program is intended to:
 1. Activate the public realm in Downtown Menlo Park and other commercial areas
 2. Maintain physical and visual access to Menlo Park businesses
 3. Provide safe, attractive, and accessible spaces for outdoor use
 4. Provide accessible amenity areas for private businesses during operating hours
 5. Support the economic vitality and growth of businesses and the City



STREETARY LOCATION CRITERIA

- Limited to outdoor dining in parking spaces and on sidewalks in the public right-of-way directly adjacent to building frontages of food service establishments.
- Additional criteria:
 - Streets that have speed limits less than 25 mph
 - Not allowed in parking spaces with peak hour parking restrictions, ADA designated spots, no parking color zones, cannot block public safety infrastructure, utility asset access, or bicycle facilities
 - Cannot be located in parking spots that provide bicycle parking unless the bicycle parking can be relocated within 300-feet
 - Must meet required travel lane and other setback requirements
 - Must provide clearance from utility access



OTHER KEY FEATURES OF THE STREETARY OUTDOOR DINING PROGRAM



- Limited to the storefronts of food service establishments
- Hours of operation must coincide with the business's hours of operation, except beyond 11 p.m. on weeknights, 12 a.m. on Saturday, and 10 p.m. on Sunday
- Maintenance of the streetary area, utilities, and drainage is clearly laid out
- The permittee will be required to ensure their space is safe, free of debris, grime, and graffiti
- Landscaping or plants must be maintained and in good health
- Permits are non-transferable



BARRIER STANDARDIZATION

- Safety standards
 - Force impact rating of 250-lbs
 - Maintenance of visibility of pedestrian, bicycle, or vehicle traffic, as well as traffic signs, signals and other traffic warning devices
 - Reflectors required on corners facing the travel lane
- Aesthetic standards
 - Materials may include wood, concrete, or steel planters, and perforated steel railings
 - Wood and concrete planters must be finished or painted
 - Colors and materials should be compatible with the façade of the adjacent building/restaurant



STREETARY DESIGN TEMPLATES



- To streamline the review process for new Streetary applications, the proposed guidelines include parklet site plan and design templates that applicants can choose from.

Parallel Parklet (1-space)



Parallel Parklet (2-space)



Diagonal Parklet





STREETARY DESIGN TEMPLATES CONTINUED



Parklet / Sidewalk Café Streetary



Parklet/Sidewalk Café Curb Extension



Parklet/Pedestrian Street Streetary*



**Parklet/Pedestrian Street Streetary only applicable for street closures*



TRANSITIONAL PERIOD FOR EXISTING OUTDOOR DINING FACILITIES

- Starting from the adoption date of the Streetary program, staff propose the following transitional period:

Through spring:

Mar-Apr

— **One-month** period to create Streetary application and notify existing permit holders

Through end of school year:

Apr-Jul

— **Three-month** open application period

Through end of 2023:

Jul-Dec

— **Seven-day** compliance (removal) period for non-applicants

— **Six-month** compliance period for Streetary permit applicants

- Total transitional period = approx. 10 months



STREETARY PERMIT TERM & FEES

- The initial permit term will be three years followed by an annual renewal.

Streetary Parklet	1st year	Annual Renewal (after 3-year term)
Parklet Permit	\$1,725	\$250
Use of Space	\$1,526.40	\$1,500.00
Recommended Cost	\$3,251.40	\$1,750.00
Streetary Sidewalk Café	1st year	Annual Renewal (after 3-year term)
Sidewalk Café Permit	\$810	\$250
Use of Space	\$636	\$600
Recommended Cost	\$1,446	\$850



FEE WAIVER & POTENTIAL GRANT PROGRAM



- Waive Streetary permit fees until January 1, 2025 for businesses who meet the transitional period deadline
 - Estimating up to 12 initial applications from existing businesses
 - City would be subsidizing an estimated amount of \$82,000, or about \$6,800 per applicant (first year: \$40,000 total, or \$3,251.40 per applicant; second and third years: \$21,000 in renewal fees per year)
- Staff are researching a potential grant program to help existing parklets comply with Streetary design standards



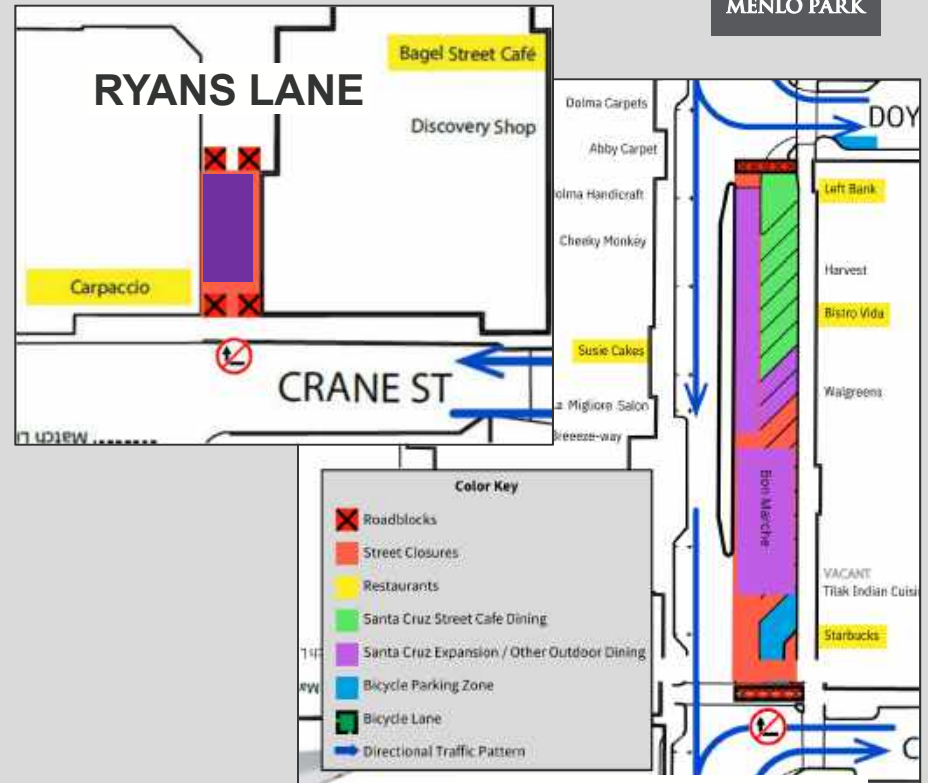
USE OF SANTA CRUZ AVE & RYANS LANE





CURRENT STREET CLOSURES

- Santa Cruz Ave and Ryans Lane **closures are set to expire** with the end of the COVID-19 State of Emergency Order
- Travel lanes would need to be cleaned and inspected before potential reopening



SANTA CRUZ AVE

CURRENT SANTA CRUZ AVE STREET CLOSURE



- 3 existing parklets plus Bon Marche Farmer's Market expanded into the travel lane



Street closure at 635 Santa Cruz Ave



Left Bank - 635 Santa Cruz Ave



Harvest - 639 Santa Cruz Ave



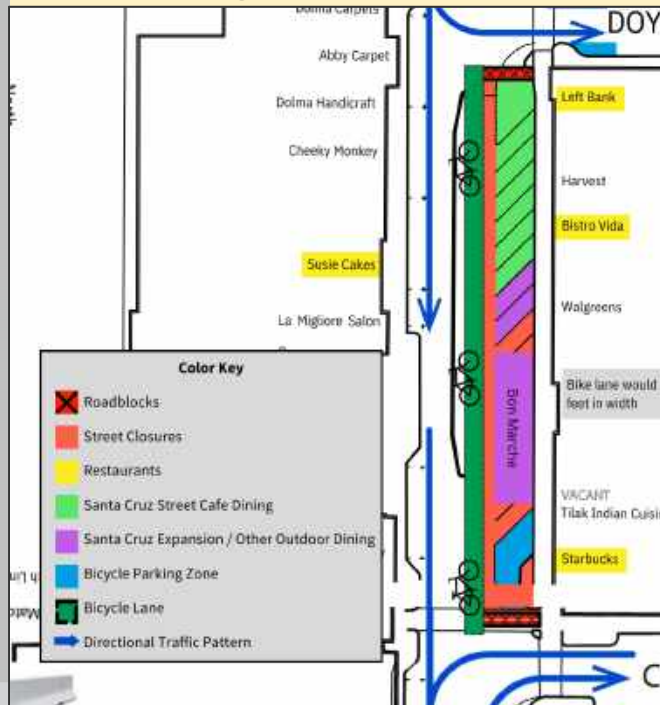
Bistro Vida - 641 Santa Cruz Ave



Street closure at 693 Santa Cruz Ave

SANTA CRUZ AVENUE

Option 1: Allow only bicycle use during 4-month period

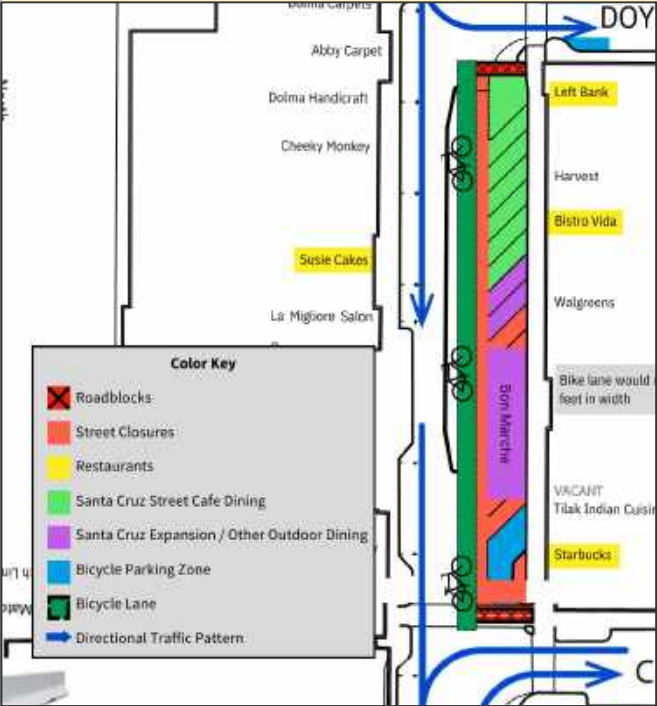


During 4-month transitional period:

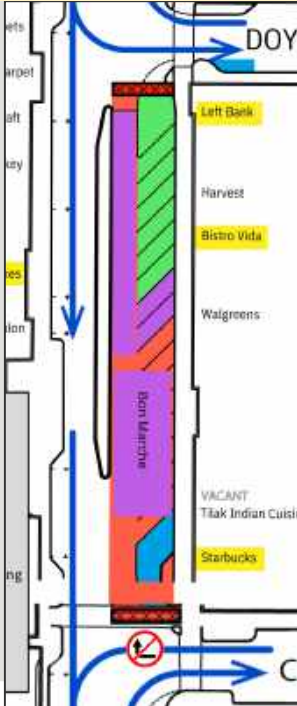
- City would study amendment to General Plan Circulation Element
- City would study environmental impacts of street closure (CEQA)

SANTA CRUZ AVENUE OPTIONS

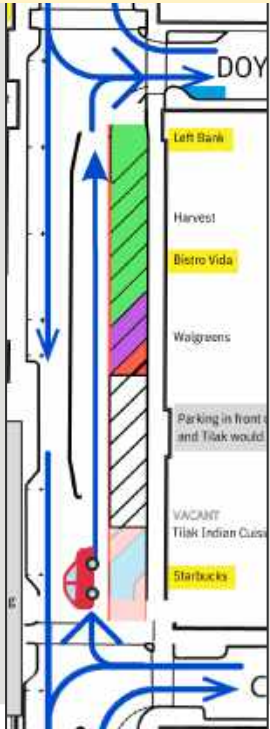
Option 1: Allow only bicycle use during 4-month period



Option 2: Maintain closure for 4-months



Option 3: Reopen street





CURRENT RYANS LANE STREET CLOSURE

- Carpaccio operates a parklet in the travel lane and they have expressed interest in maintaining their outdoor dining
- Nearby businesses indicated interest in reopening Ryans Lane
- A new restaurant will be locating at 772 Santa Cruz Avenue and they may also need access to Ryans Lane for construction
- City would need to determine no risks for construction at adjacent businesses



Carpaccio on Ryans Lane

RYANS LANE



Option 1: Maintain closure for 4-month period



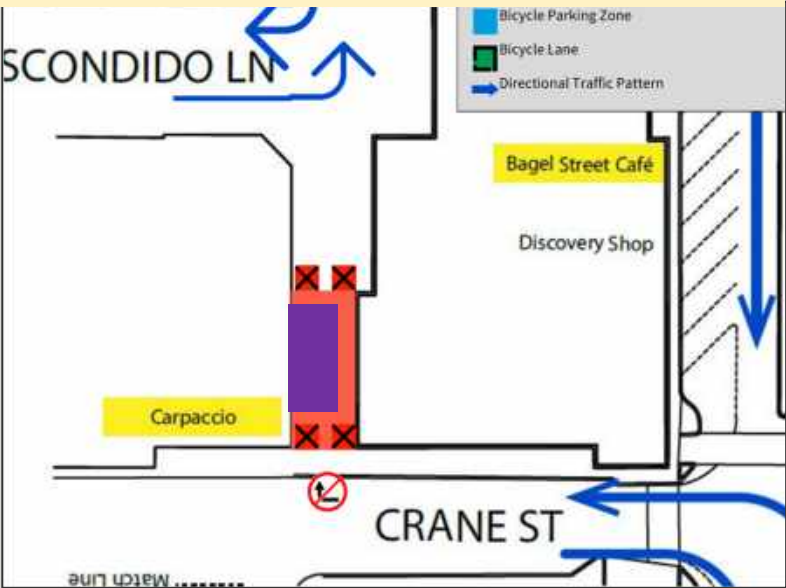
During 4-month transitional period:

- City would study amendment to General Plan Circulation Element
- City would conduct study on environmental impacts of street closure (CEQA)

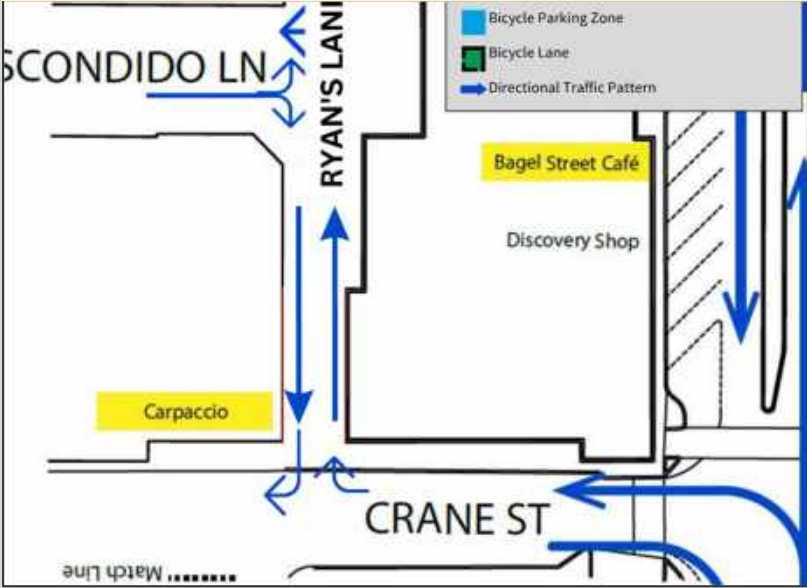
RYANS LANE



Option 1: Maintain closure for 4-month period



Option 2. Reopen street





CITY COUNCIL FEEDBACK

- Staff requests the City Council provide:
 - (1) Feedback on the proposed Streetaries outdoor dining program
 - (2) Direction on next steps for street closures on Santa Cruz Avenue and Ryans Lane



THANK YOU



STAFF REPORT

City Council

Meeting Date:

2/28/2023

Staff Report Number:

23-048-CC

Study Session:

Provide direction on whether to pursue fully grade separated alternatives for Caltrain grade separation project

Recommendation

Staff requests direction from the City Council on whether to pursue fully grade separated alternatives for the Caltrain grade separation project or proceed with the previously approved hybrid alternative of partially raising the railroad tracks and partially lowering the cross streets. If City Council directs staff to continue evaluating fully elevated alternatives, staff also seeks direction on which fully elevated alternatives to pursue.

Policy Issues

The project is a City Council priority and is consistent with the City Council rail policy and with the general plan circulation element goals to increase mobility options to reduce traffic congestion and greenhouse gas emissions; increase safety; improve Menlo Park's overall health, wellness, and quality of life through transportation enhancements; support local and regional transit that is efficient, frequent, convenient and safe; provide a range of transportation choices for the Menlo Park community; and to promote the safe use of bicycles as a commute alternative and for recreation.

Background

Since 2016, the City has been developing a project study report (PSR) for grade separation with support from AECOM. The project was funded by a grant from the San Mateo County Transportation Authority (SMCTA.) Over 50 meetings were held for the project and feedback received was incorporated into the project analysis.

In 2019, the City adopted a preferred alternative of a hybrid alternative (Alternative C), which would partially raise the rail and partially lower the streets at Ravenswood Avenue, Oak Grove Avenue and Glenwood Avenue. In making this decision, the City Council also directed staff to evaluate fully elevated alternatives due to potentially reduced cost and construction impacts.

Shortly after this, the City put the project on hold due to staffing reductions that resulted from the onset of the COVID-19 pandemic. Despite being on hold, staff submitted an updated application to the California Public Utilities Commission (CPUC) Section 190 program in late 2021, which provides funding for grade separation. While the total amount of funding provided by this program is limited (up to \$15 million), it does help the City potentially leverage funding from other State and Federal sources. The City's most recent application to the CPUC was ranked fourth in the State among submitted applications. Only Burlingame has submitted an application along the Caltrain corridor that was ranked higher (first.)

On October 18, 2022, the City Council authorized an amendment to the City's agreement with AECOM to

evaluate fully elevated alternatives. The scope of work for the amendment includes:

- Identification of potential fully elevated grade profiles (i.e., the height of the rail as it travels through the City.) This includes review of these profiles and selection of up to two for more detailed study.
- Feasibility assessment of fully elevated options. This includes preliminary engineering work and development of renderings of the selected fully elevated options. This step will include public meetings to review the fully elevated alternatives, including the cost, construction impacts and renderings.
- City Council review of preferred alternative. After completion of the feasibility study, City Council could reaffirm the preferred alternative for grade separation, select a new alternative, or request further study. If the City Council chose to reaffirm the current alternative, no further work on the current scope of work would be completed.
- Additional studies. The current scope of work includes more detailed noise/vibration, real estate analysis, and optional architectural evaluation tasks that can be conducted if the City Council would like to further evaluate a fully elevated option.

At the October 18, 2022 meeting, members of the public expressed concerns about a fully elevated option adjacent to residential neighborhoods and requested additions to the AECOM scope of work. Some of the scope of work requests have been incorporated into the work conducted to date and others may require further direction to modify the scope of work if the City Council directs staff to continue evaluating a fully elevated alternative.

Analysis

Staff have been working with AECOM to identify several potential rail profiles for a fully elevated alternative (Attachment A.) These alternatives span the full range of possibilities, including full elevation across all four existing crossings, as well as options that only elevate the rail over select crossings. Five basic profile options have been identified. The currently preferred hybrid alternative (Alternative C) is also illustrated in Attachment A) for comparison.

Table 1 identifies these fully elevated alternatives, along with the status of each road crossing:

- Fully elevated – no change in current grade of road, rail elevated over the road
- Hybrid – partially raised rail and partially lowered road
- Ped/bike only – road remains at current grade with sufficient clearance (at least 10 feet) for people walking and bicycling, but road would be closed to vehicles
- Closed – grade level would not allow the crossing to remain open

Unless noted, these options follow Caltrain design standards, specifically the use of a maximum 1 percent change in grade.

Table 1 – Fully elevated grade separation options					
Option	Ravenswood	Oak Grove	Glenwood	Encinal	Notes
1 – Yellow dashed	Fully elevated	Fully elevated	Fully elevated	Fully elevated	• Conforms well outside City limits in Atherton
2 – Red dashed	Fully elevated	Fully elevated	Fully elevated	Closed or ped/bike only (lower 9 feet)	• Conforms outside City limits in Atherton
3 – Red	Fully elevated	Fully elevated	Fully elevated	Closed or ped/bike only (lower 11 feet)	• Exceeds Caltrain maximum grade requirements (1.2%)
4 – Black	Fully elevated	Fully elevated	Hybrid (lower 4 feet) or ped/bike only	Closed or ped/bike only (lower 12 feet)	• Meets Caltrain grade requirements • Entirely within City limits
5 – Green	Fully elevated	Fully elevated	Hybrid (lower 5 feet) or ped/bike only (lower 1 foot)	Closed or ped/bike only (lower 17 feet)	• Exceeds Caltrain maximum grade requirements (1.4%) • Less visual impacts north of Glenwood

During the October 2022 meeting, the City Council received feedback from residents about the proposed scope of work. Staff have met with residents in December 2022 to understand their requests and has worked with AECOM to incorporate feedback into the current item. Specifically, residents asked that staff provide a comparison between the profile of current structures on the east side of the railroad tracks (assuming the tracks travel generally north-south on the Peninsula), compared to profile of the rail. In addition to the summary of the rail profiles, Attachment B includes a zoomed-in look at the rail profiles between Oak Grove Avenue and the City limit with the heights of existing structures shown. The figure also shows, using a white outline, where the structures are located in the plan view.

AECOM has also identified some examples of elevated railroad structures that have been constructed in recent years (Attachment C.) Within the Bay Area and nearby, there were no examples of fully elevated railroad structures adjacent to residential areas. Pictures were taken at locations between intersections to provide a viewpoint of how a fully elevated structure might appear from the properties immediately adjacent to the tracks. Future work under the current scope of work includes renderings or photo simulations that may better communicate the potential visual impact of fully elevated alternatives.

Comparison of fully elevated and hybrid options

Residents have expressed concerns about the visual impact of a fully elevated rail alternative in close proximity to residents, especially north of Oak Grove Avenue. Generally, there are one and two story multifamily buildings between Oak Grove Avenue and Glenwood Avenue immediately adjacent to the railroad tracks with access from Mills Street. North of Glenwood Avenue, there are generally single family homes to the City limits immediately adjacent to the east side of the tracks. On the west side of the railroad tracks, there are few residences immediately adjacent to the tracks – generally a street (Garwood Way, Merrill Street Stone Pine Lane) or an easement for the San Francisco Public Utilities Commission pipeline separate the tracks from residences and other buildings.

While proximity to homes is an understandable concern, a fully elevated grade separation option could have potential advantages for the City, including:

- Reduced construction impact. A fully elevated grade separation can be constructed without closing the road under the structure. This significantly reduces construction time and disruption. This is especially significant for a crossing like Ravenswood Avenue, which is one of the highest traffic streets in the City that is not a Caltrans facility. While some of the options retain hybrid crossings at Glenwood or Encinal Avenue, these options would have substantially less impact because of the significantly reduced use of

these crossings.

- Reduce utility impacts. A fully elevated grade crossing could avoid substantial utility impacts. Potential utility relocations for a hybrid option could include the San Francisco Public Utility Commission's Hetch Hetchy water line, sanitary sewers, storm drains and others. The current Alternative C (Hybrid) estimated utility relocation at approximately \$20 million in 2018.
- Reduced flooding risk. Climate change is leading to more extreme weather, including more frequent occurrences of storms that we experienced in January 2023. Undercrossings require use of pumps to ensure that the crossing remains clear of water. Sustained use, such as was experienced this year, puts the pumps at high risk of failure. In addition, the water must be pumped to a location for discharge or for temporary holding before discharge.
- Property impacts. A fully elevated railroad would require fewer property impacts such as easements for construction or acquisition. Because of the grade change of the roadways, properties near the railroad may have their access restricted or changed, potentially requiring acquisition.

In addition to greater visual impact, a fully elevated alternative would also have an impact on the in-progress Middle Avenue pedestrian and bicycle undercrossing. The Middle undercrossing has independent utility and is anticipated to be completed well in advance of grade separation. It also provides a way to mitigate the impacts of grade separation for people walking and bicycling. During construction of grade separation, the challenges crossing the railroad tracks will increase and the Middle undercrossing will provide an alternative. If a fully elevated alternative is pursued, the City may eventually consider replacing the Middle undercrossing with an at grade bicycle/pedestrian crossing once the grade separation project is completed.

Options

Staff is seeking direction on whether to continue evaluating fully elevated alternatives or continue to pursue the adopted hybrid grade separation alternative (Alternative C.)

If the City Council directs staff to further evaluate fully elevated options for grade separation, staff recommends advancing two specific profiles:

1. Option 4 (black line) – fully elevated over Ravenswood Avenue and Oak Grove Avenue, with Glenwood Avenue implemented as a hybrid or converted to bike/pedestrian only and Encinal Avenue closed
2. Option 2 or 3 (red lines) – fully elevated over Ravenswood Avenue, Oak Grove Avenue, and Glenwood Avenue that either conforms just outside the City limits (in Atherton) or exceeds the maximum Caltrain grade to conform within the City limits.

The other options represent concepts that provide fewer benefits than the two recommended options and/or are more challenging to implement due the need to receive an exception to Caltrain's design standards, which cannot be guaranteed. Option 1 (yellow line) would have significant impacts to the northern part of the City and in Atherton. Option 5 (green line) exceeds the current Caltrain grade but provides modest differences in elevation for individuals living adjacent to the tracks (2-4 feet.)

Impact on City Resources

There is no impact on City resources.

Environmental Review

This action is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §§ 15378 and 15061(b)(3) as it will not result in any direct or indirect physical change in the

environment.

The results of the current scope of work will identify required environmental reviews and studies required to advance the project. Environmental reviews and studies will be completed as part of the next phase of work.

Public Notice

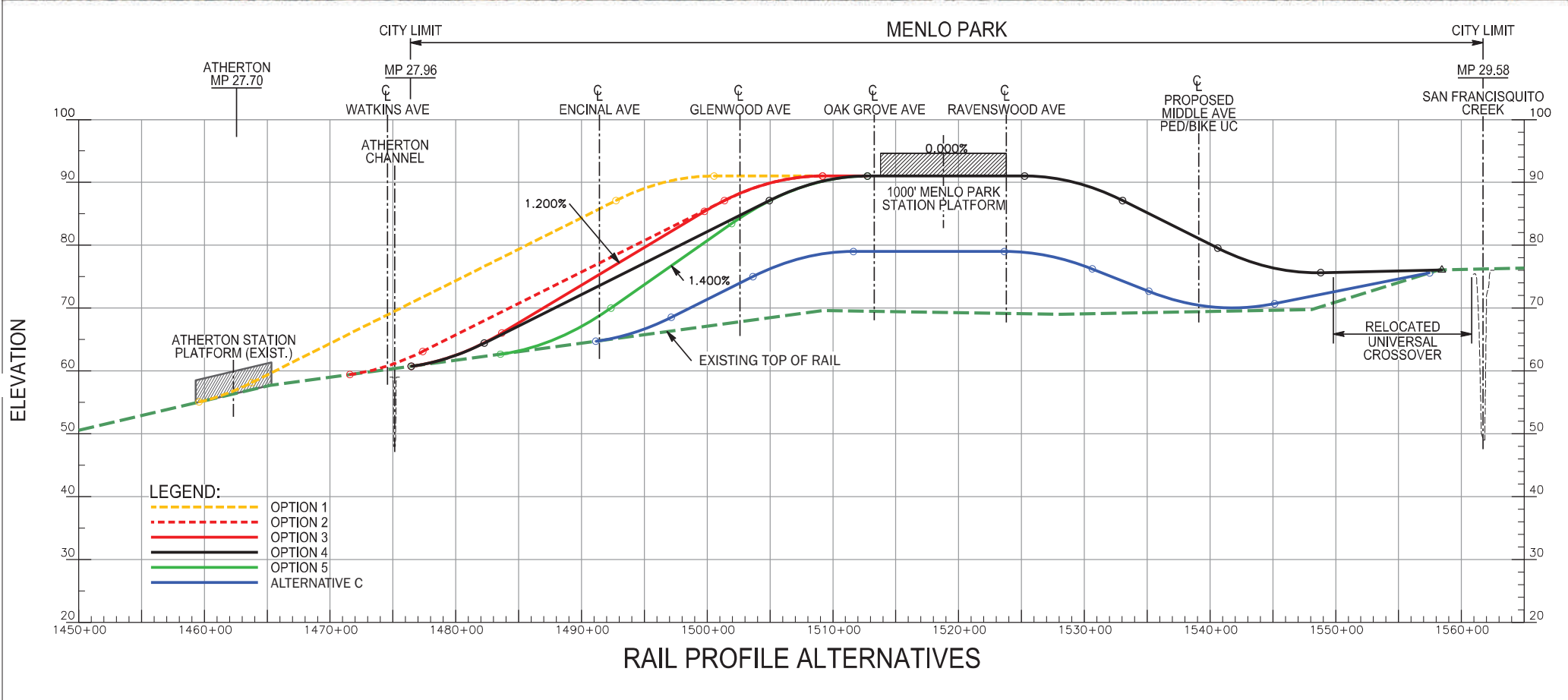
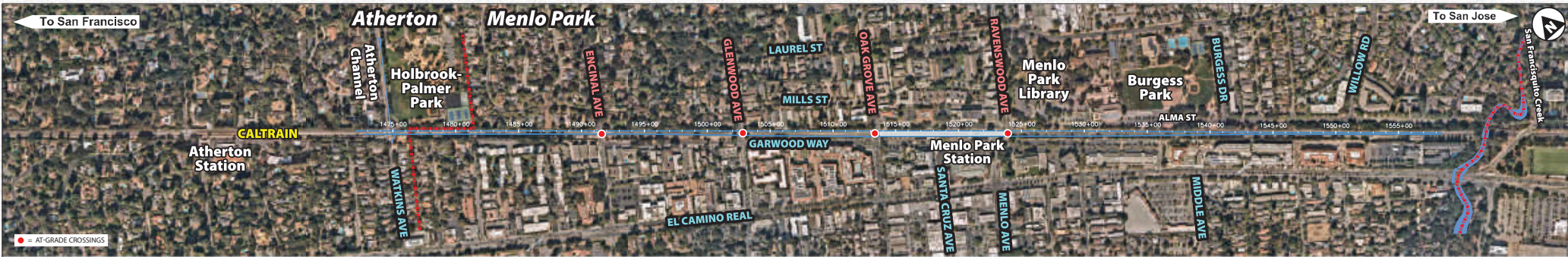
Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

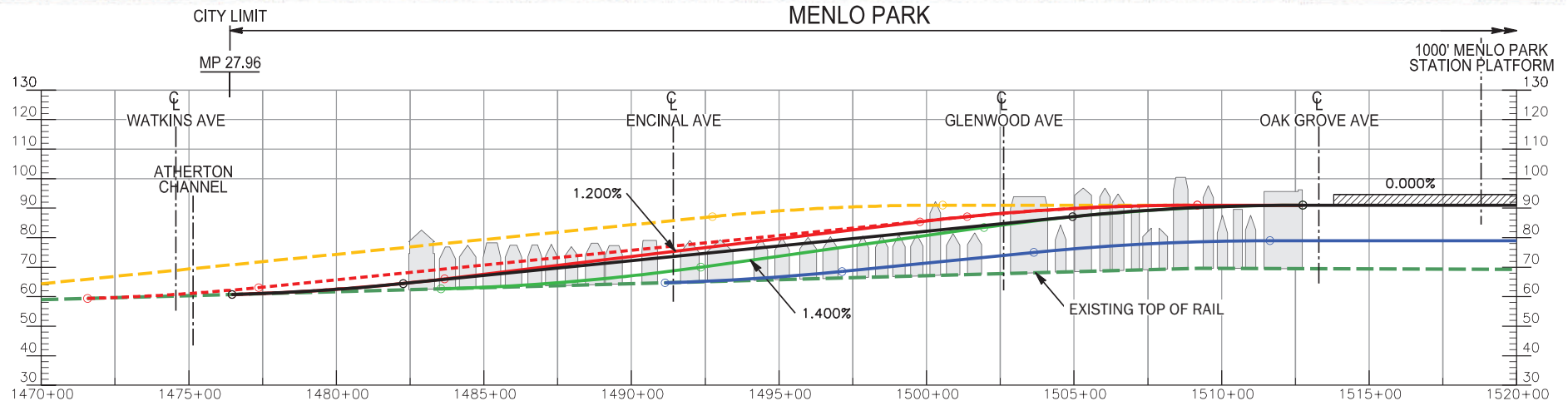
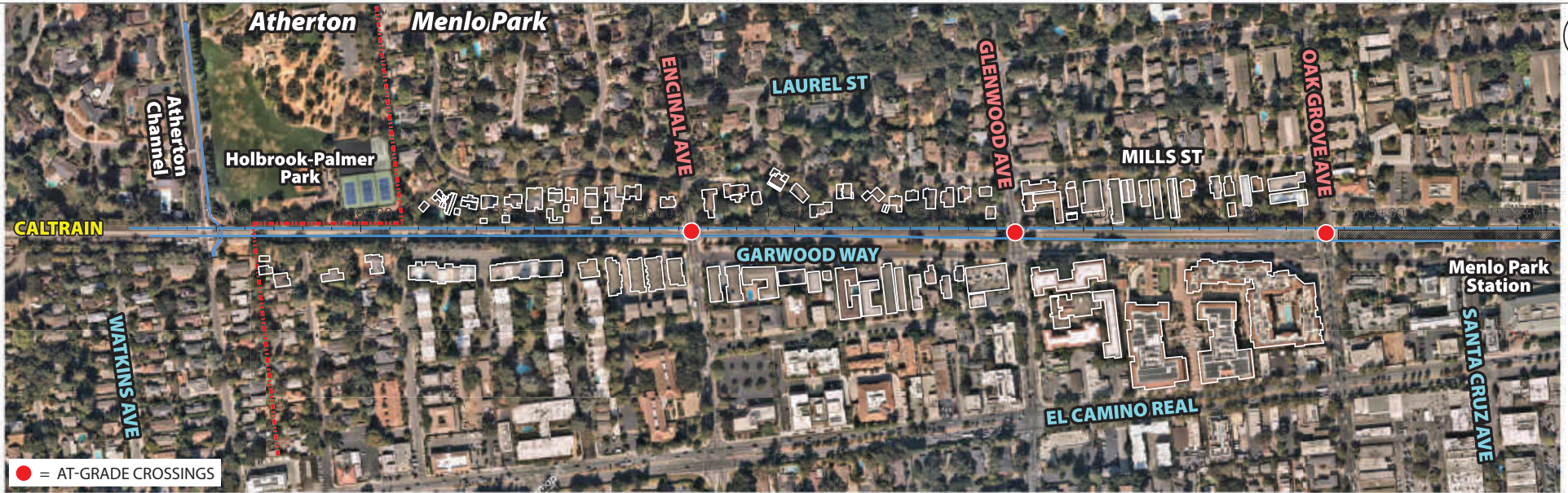
Attachments

- A. Fully elevated railroad profile options
- B. Fully elevated railroad profile options with height of current structures shown
- C. Examples of fully elevated railroad structures

Report prepared by:

Hugh Louch, Assistant Public Works Director – Transportation





- LEGEND:**
- OPTION 1
 - OPTION 2
 - OPTION 3
 - OPTION 4
 - OPTION 5
 - ALTERNATIVE C



Menlo Park Grade Separation Project

Examples of Elevated Rail on Retaining Wall
February 28, 2023

Delivering a better world

 [aecom.com](https://www.aecom.com)

Elevated Rail Toronto, Canada



Image Courtesy of Terre Armee Internationale

Light Rail Charlotte, NC



Image Courtesy of Terre Armee Internationale

Elevated Rail Japan



Elevated High-Speed Train South Korea



Image Courtesy of Terre Armee Internationale

Elevated Rail Australia



6

Menlo Park Grade Separation Project

Elevated Rail Staten Island, NY



Image Courtesy of Google

7

Menlo Park Grade Separation Project

Elevated Rail City of Industry City, CA

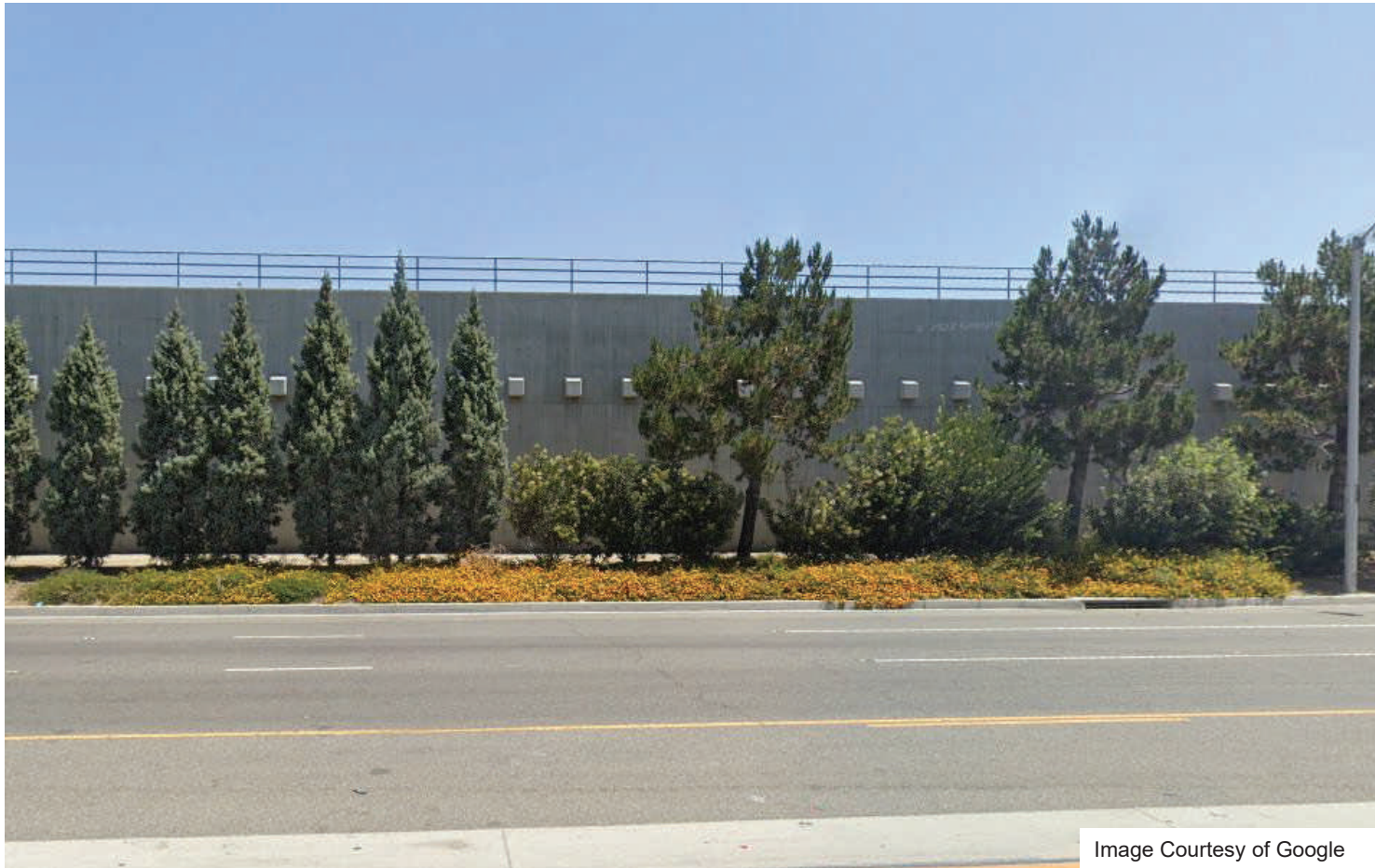


Image Courtesy of Google

Elevated Rail City of Industry City, CA



Image Courtesy of Google

Elevated Rail Westbury, NY



Image Courtesy of Google

Elevated Rail Westbury, NY



Image Courtesy of Google

Hillsdale Station San Mateo, CA



12

Menlo Park Grade Separation Project

Approach to San Bruno Station San Bruno, CA



Approach to San Carlos Station San Carlos, CA



Image Courtesy of Google

“Green Wall” Unknown Location (Not elevated rail, but potential green treatment on wall façade)



Image Courtesy of Terre Armee Internationale

Menlo Park Grade Separation Project

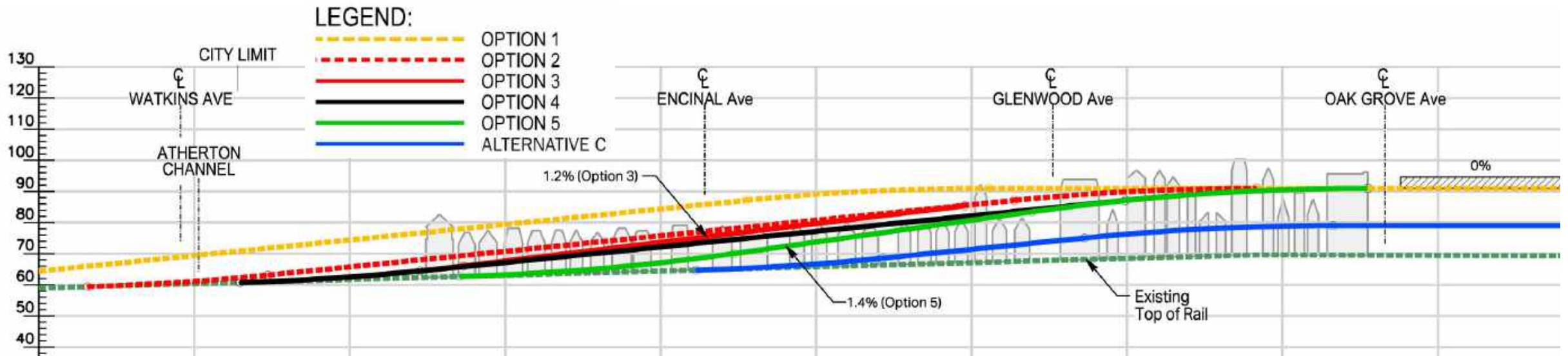
Fully Elevated Grade Separation Options

February 28, 2023






Fully Elevated Grade Separation Options



Fully Elevated Grade Separation Options



Summary of Fully Elevated Options

Topic	1 	2 	3 	4 	5 
Number of fully elevated grade separations (Improved Connectivity)	4	3	3	2	2
Design variance from Caltrain	✓	✓	X	✓	X
Trackwork in Atherton	X	X	✓	✓	✓
Encinal open to motor vehicles (See Note 1)	✓	X	X	X	X
Extent of visual impacts	XXX	XX	XX	XX	X
Glenwood road lowering required?	None	None	None	4 feet	5 feet
Utility relocations, driveway & property impacts at Glenwood	✓	✓	✓	X	X

Notes:

1. All options that close Encinal to motor vehicles can be kept open for peds/bikes via an undercrossing/tunnel structure.
2. Costs to be determined (TBD).

Fully Elevated vs. Hybrid Option (Alternative C)

- **Reduced construction impacts**
- **Less utility relocation required**
- **Reduced flooding risk**
- **Property impacts**
- **Greater visual impact**
- **Impact to future Middle Ave ped/bike undercrossing**
- **Construction costs**

Requested Direction from City Council

Question 1 – Should staff continue evaluating fully elevated grade separation?

Question 2 – If staff continue evaluating fully elevated grade separations, which profile(s) should be evaluated?

Option 4 → Feasible, but requires lowering of Glenwood

✓ **Recommended**

Option 2 → Feasible, but trackwork extends into Atherton
(or)

Option 3 → Requires Caltrain approval of a 1.2% grade

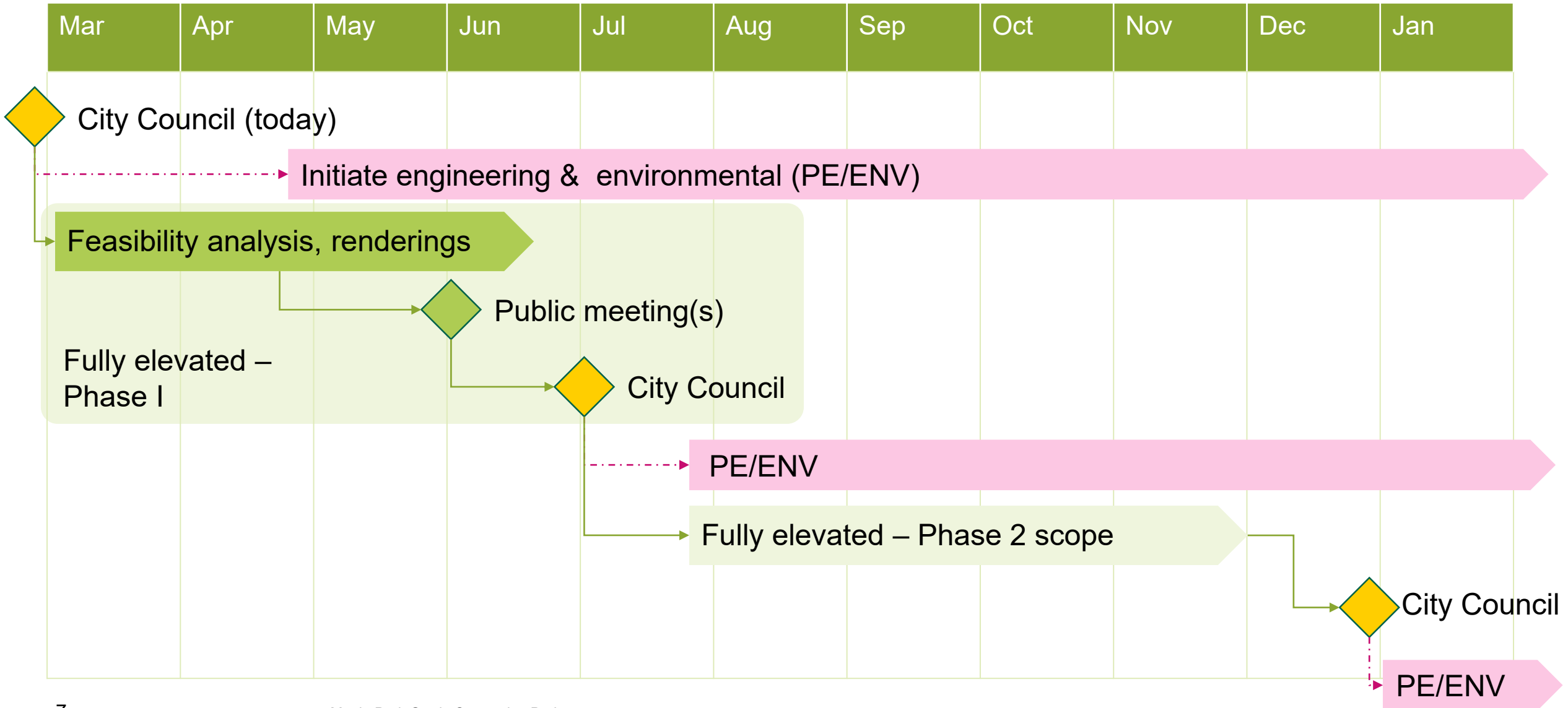
? **Consider Option 2 or 3**
(Or Option 3 if it is not feasible to extend trackwork into Atherton)

Option 1 → Impact to Atherton + Greatest Visual/Noise Impact

✗ **Not recommended**

Option 5 → Difficult to obtain Caltrain Approval of a 1.4% Grade

Next Steps and Schedule





Questions?



SPECIAL MEETING MINUTES – DRAFT

Date: 1/31/2023
Time: 6:00 p.m.
Locations: Teleconference and
City Council Chambers
751 Laurel St., Menlo Park, CA 94025

Special Session

A. Call To Order

Mayor Wolosin called the meeting to order at 6:02 p.m.

B. Roll Call

Present: Combs, Doerr, Nash, Taylor, Wolosin
Absent: None
Staff: City Manager Justin I. C. Murphy, City Attorney Nira F. Doherty, Assistant to the City Manager/City Clerk Judi A. Herren, Deputy City Clerk Sarah Sandoval

C. Public Hearing

- C1. Consider 1) the Planning Commission's recommendation to adopt a resolution to certify the subsequent environmental impact report, adopt California Environmental Quality Act findings, adopt a Statement of Overriding Considerations for significant and unavoidable impacts for the Housing Element Update project, and approve a mitigation and monitoring program; and 2) the Planning Commission and Housing Commission's recommendations to adopt a resolution to amend the General Plan to update the Housing Element for the 2023 to 2031 planning period (Staff Report 23-024-CC)

Principal Planner Tom Smith made the presentation (Attachment).

Mayor Wolosin opened the public hearing.

- Nestor Flores spoke in support of locally hired union labor in Menlo Park.
- Isabel Pardo spoke in support of locally hired union labor in Menlo Park.
- Bryan Shields spoke in support of encouragement language for locally hired union labor in the Housing Element.
- Lynne Bramlett spoke in support of reevaluating the City's planning process and reexamining the noise, open space, conservation, safety, and environmental justice elements of the Housing Element.
- Jenny Michel in support of affordable housing, tenant protections, and the use of City owned parking lots for additional housing.
- Kathleen Daly spoke in support of affordable housing.
- Pam Jones spoke in support of interpretation services at public meetings and the inclusion of Climate Resilient Communities recommendations in the Housing Element.
- Brittani Baxter spoke in support of affordable housing and walkable amenities in Menlo Park.

Mayor Wolosin closed the public hearing.

The City Council received clarification on the Department of Housing and Community Development (HCD) requirements for the Housing Element, air quality mitigation measures in the environmental impact report (EIR), historic buildings, and multimodal transportation improvements.

The City Council discussed the anti-displacement strategy, encouragement language for union labor, water efficiency, preservation of green spaces and parks, removal of certain Office-zoned sites located near Marsh Road from the list of proposed sites, Planning and Housing Commissions' recommendations, and affordable housing on City-owned parking lots as potential modifications to the Housing Element.

The City Council directed adding language related to encouraging use of union labor, retaining proposed anti-displacement language, retaining the proposed site inventory list, adding an explanation as to why the civic center and parks were not selected as housing sites, use of Planning and Housing Commissions' recommendations with the exception of modified timelines related to development of affordable housing on City-owned parking lots downtown, and transit incentives.

ACTION: Motion and second (Doerr/ Nash), to adopt a resolution to certify the subsequent EIR that analyzes potential environmental impacts of the proposed Housing Element Update project and makes California Environmental Quality Act (CEQA) findings to address impacts, including a statement of overriding considerations for significant and unavoidable environmental effects that would result from the proposed project, and to approve a mitigation monitoring and reporting program for the proposed project to mitigate impacts to less than significant with mitigation or reduce significant impacts, adopting environmental findings pursuant to the CEQA, adopting a statement of overriding considerations, and adopting a mitigation monitoring and reporting program, passed 4-1 (Combs dissenting).

ACTION: Motion and second (Nash/ Wolosin), to adopt a resolution to repeal in its entirety the 2015 to 2023 Housing Element of the General Plan and adopt in full the new text comprising the 2023 to 2031 Housing Element of the General Plan including modifications to the goals, policies, and programs language as discussed by the Council, passed 4-1 (Combs dissenting).

D. Adjournment

Mayor Wolosin adjourned the meeting at 8:33 p.m.

Sarah Sandoval, Deputy City Clerk



SPECIAL MEETING MINUTES – DRAFT

Date: 2/7/2023
Time: 6:00 p.m.
Locations: Teleconference

Special Session

A. Call To Order

Mayor Wolosin called the meeting to order at 6:05 p.m.

B. Roll Call

Present: Doerr, Nash, Taylor, Wolosin
Absent: Combs
Staff: City Manager Justin I. C. Murphy, City Attorney Nira F. Doherty, Assistant to the City Manager/City Clerk Judi A. Herren

C. Closed Session

C1. CONFERENCE WITH LEGAL COUNSEL—EXISTING LITIGATION

Paragraph (1) of subdivision (d) of §54956.9)

Name of case: David Fogel et al. v. City of Menlo Park, Case No. 21-CIV-06674
Not a California Environmental Quality Act (CEQA) project.

No reportable actions.

C2. CONFERENCE WITH LEGAL COUNSEL—ANTICIPATED LITIGATION

Significant exposure to litigation pursuant to paragraph (3) of subdivision (d) of §54956.9: (one case)

(Communications shall be available for public inspection pursuant to Government Code §54957.5.)
Not a CEQA project.

No reportable actions.

D. Adjournment

Mayor Wolosin adjourned the meeting at 8:16 p.m.

Judi A. Herren, Assistant to the City Manager/City Clerk



STAFF REPORT

City Council

Meeting Date: 2/28/2023

Staff Report Number: 23-038-CC

Consent Calendar: Award a construction contract to Anderson Pacific Engineering Construction, Inc. for the Chrysler Stormwater Pump Station Improvement project; determine this action is exempt from California Environmental Quality Act pursuant to CEQA Guidelines section 15302 (Replacement and Reconstruction Projects)

Recommendation

Staff recommends that the City Council award a \$10,870,000 contract to Anderson Pacific Engineering Construction, Inc. (Attachment A) for the Chrysler Stormwater Pump Station Improvement project, approve contingency in the amount of \$1,631,000 (held by the City), approve construction administration fees in the amount of \$260,000, and determine this action is exempt from California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines section 15302.

Policy Issues

This project is consistent with the City’s goal of maintaining and enhancing its municipal infrastructure. The project is included in the fiscal year 2022-23 capital improvement program (CIP) and is also identified as a recommended hazard mitigation action item for Menlo Park in the adopted multijurisdictional San Mateo County Local Hazard Mitigation Plan.

Background

The Chrysler Stormwater Pump Station is located at 1395 Chrysler Drive in Menlo Park, California (at the corner of Chrysler Drive and Bayfront Expressway.) The pump station is also adjacent to lands owned by Bohannon Development Group (Bohannon.) The pump station drains approximately 297 surface acres in an area roughly bounded by Marsh Road, Bayfront Expressway, Chilco Street, and the Dumbarton railroad tracks (Attachment B.) The existing facility is designed to discharge a 10-year rain event and consists of two pumps, a below-ground wet well, and a building that houses the pumps and electrical room. Stormwater is pumped to a Caltrans-owned ditch on the opposite side of Bayfront Expressway and empties into Flood Slough near the Bedwell Bayfront Park entrance.

The pump station was built in 1958 and has reached the end of its useful life. In 2015, the City contracted with Schaaf & Wheeler to provide construction bid documents for a replacement pump station that could discharge a 100-year rain event. The new pump station is elevated two feet above the Federal Emergency Management Agency (FEMA) designated 100-year base flood elevation and includes three new, larger pumps designed to discharge flows associated with sea level rise in San Francisco Bay. This project is included in the City’s CIP with partial funding from a \$5 million federal FEMA grant (awarded to the City April 24, 2020.) The grant requires the project to be substantially complete by April 2025 and, to date, \$134,270 has been utilized for the project’s design phase. The remaining \$4,865,730 in grant monies are

eligible for construction funding.

The new pump station requires a larger building footprint that entails a mutual exchange of property, the provision of easements, and construction of exterior cladding and landscaping with Bohannon. On December 8, 2020, the City Council authorized the city manager to negotiate and execute a funding agreement with Bohannon that addressed the aforementioned project needs. General Plan and Zoning Map Amendments were subsequently approved for this action November 16, 2021 and December 7, 2021, respectively.

On August 5, 2022, staff advertised the project and received six bids on September 27, 2022. All six bids were found to be non-responsive pursuant to the Public Contracting Code and were rejected by the City Council on November 1, 2022. The project was re-advertised for construction bids on November 8, 2022; three bids were received as further described in the Analysis section.

On December 7, 2022 the City executed a professional services agreement, with 4Leaf Inc. (a consultant on the City’s master agreement list.) The agreement included services for construction management, inspections, and FEMA grant administration in anticipation of construction.

On February 14, 2023, the City Council authorized the city manager to execute the first amendment to the funding agreement and all deeds and easements with Bohannon, thus effectuating the property swap and exchange of easements required for the project. To date, staff is finalizing the real estate transaction with Bohannon and actively monitoring its progress to ensure its completion prior to the project’s award of contract. In the event that the close of escrow for this transaction is not completed by February 28, 2023, staff will continue the award of construction contract to the next City Council meeting on March 14, 2023.

Analysis

On November 8, 2022, staff advertised the project for construction bidding with an engineer’s estimate of \$10,000,000. The project was advertised on the City website and posted twice in a local newspaper. The project specifications stipulated that the contract award would be based on the base bid total and the bidders’ responsiveness to the project documents. On December 13, 2022, the City received three bids for the project as summarized in Table 1.

Table 1: Bid results (Engineer’s estimate \$10,000,000)	
Contractor	Base bid total
Mitchell Engineering	\$10,585,355
Anderson Pacific Engineering Construction, Inc.	\$10,870,000
Anvil Builders	\$11,280,000

Although Mitchell Engineering had the apparent lowest base bid, staff discovered omissions in their proposal violating the Public Contracting Code. The omissions constituted grounds for finding their bid non-responsive and staff distributed a bid rejection notice to Mitchell Engineering. On December 20, 2022, a response to the notice of bid rejection was received from Mitchell Engineering and their protest was considered to be without merit upon review by the city attorney. Consequently, Anderson Pacific Engineering Construction was determined to be the lowest bidder. Staff found Anderson Pacific to be experienced with projects involving a similar scope of work and determined the contractor to be responsive

and responsible per the Public Contracting Code. Anderson Pacific’s bid is 9 percent higher than the engineer’s estimate. Staff attributes this to inflationary cost increase. Subject to the City Council’s award of contract, the project is tentatively scheduled for construction from April 2023 to April 2025.

Impact on City Resources

The project is included in the fiscal year 2022-23 CIP with \$11,850,000 in available funding from the general capital and highway user’s (gas) tax funds. Construction costs, including contingencies and administration fees, total \$12,761,000 per Table 2 and the project has sufficient funding this fiscal year. Staff will seek a future appropriation of \$911,000, as part of the fiscal year 2023-24 CIP adoption, to meet the project’s construction funding gap. Sufficient funding is available in the fiscal year 2022-23 CIP to award the base construction contract. The project is eligible for up to \$4,865,730 in FEMA grant funding. Staff will apply for the reimbursement of qualifying grant monies on a quarterly basis which will be returned to the general capital fund.

Table 2: Construction budget	
Item	Value
Construction subtotal	\$10,870,000
Contingency (15%)	\$1,631,000
Construction administration	\$260,000
Construction total	\$12,761,000
Available funding	\$11,850,000
Future appropriation request in FY 2023-24 CIP	\$911,000
Estimated FEMA grant reimbursement	(\$4,865,730)
Construction net total	\$7,895,270

Staff recommends that the City Council award a \$10,870,000 construction contract to Anderson Pacific Engineering Construction, Inc., approve construction contingency (held by the City) in the amount of \$1,631,000, and approve construction administration fees in the amount of \$260,000 for the project.

Environmental Review

On May 2, 2017, the City Council determined that project is categorically exempt under Class 2 (Section 15302, “Replacement or Reconstruction”) of the current CEQA Guidelines. Additionally, FEMA performed a National Environmental Policy Act review of the project and determined it was Categorical Excluded under Department of Homeland Security Categorical Exclusion N7 (Federal Assistance for Structure and Facility Upgrades.)

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

- A. Construction contract
- B. Drainage area

Report prepared by:
Michael Fu, Senior Civil Engineer

Report reviewed by:
Tanisha Werner, Assistant Public Works Director - Engineering

CONSTRUCTION AGREEMENT

City Manager's Office
 701 Laurel St., Menlo Park, CA 94025
 tel 650-330-6620



Agreement #:

**AGREEMENT FOR SERVICES BETWEEN
 THE CITY OF MENLO PARK AND FIRST PARTY**

THIS CONSTRUCTION CONTRACT ("Contract") is made and entered into this ____ day of _____, _____ ("Execution Date") by and between the CITY OF MENLO PARK, a California municipal corporation, ("City") and Anderson Pacific Engineering Construction, Inc. ("Contractor").

RECITALS

- A. Contractor is a California Corporation duly organized and in good standing in the State of California, License Number 245215. Contractor represents and warrants that it has the background and experience set forth in the Contractor's responses to the notice inviting bids.
- B. Contractor represents that it is duly licensed by the State of California and has the background, knowledge, experience and expertise to perform the obligations set forth in this Contract.
- C. On November 8, 2022, the City issued a Notice to Contractors inviting bids for the Project. A copy of the Contractor's Bid proposal and List of Subcontractors is attached herein and incorporated by this reference.
- D. The City desires to retain Contractor as an independent contractor to provide the construction and other services identified in this Contract for the Project upon the terms and conditions contained herein.

AGREEMENT

NOW, THEREFORE, in consideration of performance by the parties of the promises, covenants and conditions contained herein, the parties hereby agree as follows:

1. DEFINITIONS. Capitalized terms used throughout the Contract Documents shall have the meanings set forth in this Contract and/or the Special Provisions. If there is a conflict between the definitions in this Contract and the Special Provisions, the definitions in this Contract shall prevail.
2. PROJECT. The project is the construction of Chrysler Stormwater Pump Station Improvements Project, CPR002 ("Project"). The work includes all labor, materials, equipment, services, permits, licenses and taxes, and all other things necessary for Contractor to perform its obligations and complete the Project, including, without limitation, any Change Orders executed by City and Contractor in

accordance with the requirements of the Contract Documents (“Work”).

3. CONTRACT DOCUMENTS.

3.1 List of Documents. The Contract Documents (sometimes collectively referred to as “Agreement” or “Bid Documents”) consist of the following documents which are on file with the Public Works Department and are hereby incorporated by reference.

- 1) Change Orders
- 2) Field Orders
- 3) Contract
- 4) Bidding Addenda
- 5) Special Provisions
- 6) Project Plans and Drawings
- 7) Technical Specifications
- 8) City Standard Details
- 9) State of California Department of Transportation Specifications, 2006 Edition (Cal Trans specifications)
- 10) Notice to Contractors
- 11) Contractor's Bid
- 12 Bidder Certifications, Questionnaire and Statements
- 13) Reports listed in the Contract Documents
- 14) City of Menlo Park Waste Management Form, Waste Management Daily Transport Report
- 15) City of Menlo Park Truck Route Map and Regulations
- 16) Performance, Payment and Maintenance Bonds

3.2 Order of Precedence. For the purposes of construing, interpreting and resolving inconsistencies between and among the provisions of this Contract, the Contract Documents shall have the order of precedence as set forth in the preceding section. If a claimed inconsistency cannot be resolved through the order of precedence, the City shall have the sole power to decide which document or provision shall govern as may be in the best interests of the City.

4. PERMITS. Contractor, at its sole expense, shall obtain and maintain during the term of this Contract, all appropriate permits, licenses and certificates that may be required in connection with the performance of the Work, including, but not limited to, a City business license.

5. DEPARTMENT OF INDUSTRIAL RELATIONS. Contractor and any subcontractor performing Work on this Project shall be registered with the Department of Industrial Relations (“DIR”) pursuant to Labor Code Section 1725.5. No contractor or subcontractor may be listed on a bid proposal for a public works project unless registered with the DIR pursuant to Labor Code Section 1725.5, with limited exceptions from this requirement for bid purposes only under Labor Code Section 1771.1(a). This Project is subject to compliance monitoring and enforcement by the DIR. It is the responsibility of the Contractor to ensure all DIR requirements and regulations are met and stay current. For more information, see <http://dir.ca.gov/Public-Works/SB854.html>.

6. TERM. This Contract is effective on the Execution Date set forth in the initial paragraph of this Contract and shall remain in effect until the Project has been satisfactorily completed by

Contractor, unless earlier terminated pursuant to the terms of this Contract.

7. TIME OF COMPLETION. Time is of the essence with respect to all time limits set forth in the Contract Documents. Contractor shall commence the Work on the date specified in the City's Notice to Proceed. Contractor shall diligently prosecute the Work to Substantial Completion within the working days specified per the contract documents and the City's Notice to Proceed ("Contract Time"). The Contract Time may only be adjusted for extensions of time approved by the City and agreed to by Change Order executed by City and Contractor in accordance with the requirements of the Contract Documents.

8. COMPENSATION. The City agrees to compensate Contractor for its satisfactory completion of the Work in compliance with the Contract Documents for the not to exceed amount of Ten million and eight hundred seventy thousand (\$10,870,000) ("Contract Sum"). Payment shall be as set forth in the Plans, Special Provisions and/or Technical Specifications. The Contract Sum may only be adjusted by Change Orders issued, executed and satisfactorily performed by Contractor in accordance with the requirements of the Contract Documents. The Contract Sum shall be adjusted (upward or downward) only to account for Change Orders. The Contract Sum is and shall be full compensation for all Work performed by Contractor. The Contract Sum shall cover all losses arising out of the nature of the Work or from the elements or any unforeseen difficulties or obstructions which may arise or be encountered in performance of the Work until its Acceptance by the City, all risks connected with the Work and any and all expenses incurred due to the suspension or discontinuance of the Work.

9. STANDARD OF PERFORMANCE. As a material inducement to the City to enter into this Contract, Contractor hereby represents and warrants that it has the qualifications and experience necessary to undertake the Work to be provided and the Project to be completed pursuant to this Contract. Contractor agrees that the Work shall be performed by qualified, experienced and well-supervised personnel. The Work performed pursuant to this Contract shall be performed in a manner consistent with the standard of care under California law applicable to those who specialize in providing such services for projects of the type, scope and complexity of the Project.

10. COMPLIANCE WITH LAW. This Project constitutes a public work within the meaning of California Labor Code Section 1720 et. seq. and is subject to prevailing wage laws. The Work performed by Contractor pursuant to this Contract shall be provided in accordance with all ordinances, resolutions, statutes, rules and regulations of the City, and any federal, state or local governmental agency having jurisdiction in effect at the time the work is rendered.

11. REPRESENTATIVE. Scott Schumacher is hereby designated as the project manager/superintendent/foreman of Contractor authorized to act on its behalf with respect to the Work specified in this Contract. It is expressly understood that the experience, knowledge, capability and reputation of Anderson Pacific Engineering Construction, Inc. were a substantial inducement for City to enter into this Contract. Therefore, Scott Schumacher shall be responsible during the term of this Contract for directing all activities of Contractor and devoting sufficient time to personally supervise the services hereunder. The representative may not be changed by Contractor without the express written approval of the City.

12. LIQUIDATED DAMAGES.

12.1 Entitlement. City and Contractor acknowledge and agree that if Contractor fails to fully and satisfactorily complete the Work within the Contract Time, the City will suffer, as a result of Contractor's failure, substantial damages which are both extremely difficult and impracticable to ascertain. Such damages may include, but are not limited to: (a) loss of public confidence in the City and its contractors; (b) loss of public use of public facilities; and (c) extended disruption to public.

12.2 Daily Amount. City and Contractor have reasonably endeavored, but failed, to ascertain the actual damage that the City will incur if the Contractor fails to achieve Substantial Completion of the Work within the Contract Time. Therefore, the parties agree that in addition to all other damages to which the City may be entitled other than delay damages, in the event the Contractor shall fail to achieve Substantial Completion of the Work within the Contract Time, Contractor shall pay City as liquidated damages the amount of Five Hundred Dollars (\$500) per day for each calendar day after the expiration of the Contract Time until Contractor achieves Substantial Completion of the Work. The liquidated damages amount is not a penalty, but a reasonable estimate of the amount of damages the City will suffer.

12.3 Apportionment. Such liquidated damages shall be subject to reduction for delays for which Contractor is entitled to receive an extension of time under the Contract Documents ("Apportionment"). Such Apportionment shall not be affected by the fact that liquidated damages may not be applied for periods of time during which delays have occurred that are caused by both City and Contractor. It is agreed that the liquidated damages shall not be applied for portions of the Work completed prior to the expiration of the Contract Time.

12.4 Exclusive Remedy. City and Contractor acknowledge and agree that this Section 11, Liquidated Damages, shall be the City's only remedy for delay damages caused by the Contractor's failure to achieve Substantial Completion of the Work within the Contract Time.

12.5 Damages upon Abandonment. In the event that the Contractor either abandons the Work or is terminated for default in accordance with the provisions of this Contract, City shall have the right, in its sole discretion exercised by written notice issued either before or after Substantial Completion, to elect to either assert or waive its right to liquidated damages. If City elects to assert its right to liquidated damages, then the liquidated damages shall be calculated from expiration of the Contract Time to the date that Substantial Completion of the Work is achieved by the City or its replacement contractor employed to complete Contractor's performance. If City elects to waive its right to liquidated damages, then Contractor shall be liable to the City, in lieu of the liquidated damages, for all actual Losses (as defined in the General Conditions) proximately resulting from Contractor's failure to complete the Work within the Contract Time.

12.6 Other Remedies. The parties further acknowledge and agree that the City is entitled to any and all available legal and equitable remedies City may have where City's Losses are caused by any reason other than Contractor's failure to achieve Substantial Completion of the Work within the Contract Time.

13. INDEPENDENT CONTRACTOR. Contractor is, and shall at all times remain as to the City, a wholly independent contractor and not an agent or employee of the City. Contractor shall receive no premium or enhanced pay for work normally understood as overtime, nor shall Contractor receive holiday pay, sick leave, administrative leave, or pay for any other time not actually worked. The intention of the parties is that Contractor shall not be eligible for benefits and shall receive no compensation from the City except as expressly set forth in this Contract. Contractor shall have no power to incur any debt, obligation, or liability on behalf of the City or otherwise act on behalf of the City as an agent. Neither the City, nor any of its agents shall have control over the conduct of Contractor, any of Contractor's employees, or any subcontractors, except as set forth in this Contract. Contractor shall at no time, or in any manner, represent that it or any of its agents or employees or subcontractors are in any manner employees of the City. Contractor agrees to pay all required taxes on amounts paid to Contractor under this Contract, and to indemnify and hold the City harmless from any and all taxes, assessments, penalties, and interest asserted against the City by reason of the independent contractor relationship created by this Contract. Contractor shall fully comply with the worker's compensation law regarding Contractor, Contractor's employees and subconsultants. Contractor further agrees to indemnify and hold the City harmless from any failure of Contractor and any subconsultants to comply with applicable worker's compensation laws.

14. CONFLICT OF INTEREST. Contractor covenants that it presently has no interest and shall not acquire any interest, direct or indirect, which may be affected by the work to be performed by Consultant under this Contract, or which would conflict in any manner with the performance of its services hereunder. Contractor further covenants that, in performance of this Contract, no person having any such interest shall be employed by it. Furthermore, Contractor shall avoid the appearance of having any interest which would conflict in any manner with the performance of the work pursuant to this Contract. Contractor agrees not to accept any employment during the term of this Contract which is or may make Contractor financially interested, as provided in California Government Code Sections 1090 and 87100, in any decision made by the City on any matter in connection with which Contractor has been retained pursuant to this Contract. However, nothing herein shall preclude Contractor from accepting other engagements with the City.

15. INDEMNIFICATION.

15.1 To the fullest extent permitted by law, Contractor shall indemnify, defend, with independent counsel approved by the City, and hold harmless the City, and its elective or appointive boards, officers, employees agents and volunteers ("Indemnatee") from and against any and all claims, losses, or liability that may arise out of or result from damages to property or personal injury received by reason of, or in the course of work performed under this Contract due to the acts or omissions of Contractor or Contractor's officers, employees, agents or subcontractors. The indemnification provisions survive completion of the Work or the termination of this Contract. The acceptance of such services shall not operate as a waiver of such right of indemnification. Notwithstanding the foregoing, nothing contained herein shall be construed as obligating Contractor to indemnify any Indemnatee for any claims, losses or liability resulting from the sole

or active negligence or willful misconduct of the Indemnatee. Contractor shall pay City for any costs incurred in enforcing this provision.

15.2 The City does not and shall not waive any rights that they may possess against Contractor because of the acceptance by the City or the deposit with the City of any insurance policy or certificate required pursuant to this Contract. This hold harmless and indemnification provision shall apply regardless of whether or not any insurance policies are determined to be applicable to the claim, demand, damage, liability, loss, cost or expense.

15.3 Pursuant to Public Contract Code Section 9201, the City shall timely notify Contractor upon receipt of any third-party claim relating to the Contract.

16. ASSIGNABILITY. The parties agree that the experience and qualifications of Contractor as set forth in the Contractor's Bid are material considerations for the City entering into this Contract. Consultant shall not assign or transfer any interest in this Contract, without the prior written consent of the City, and any attempt by Contractor to do so shall be void and of no effect and a breach of this Contract. For purposes of this section, the sale, assignment, transfer or other disposition of any of the issued and outstanding capital stock of Contractor or of any general partner or joint venturer or syndicate member of Contractor, if a partnership or joint venture or syndicate or co-tenancy exists, which shall result in changing the control of Contractor, shall be construed as an assignment of this Construction Contract. Control means more than fifty percent (50%) of the voting power of the corporation or other entity.

17. INSURANCE AND BOND REQUIREMENTS.

17.1 Prior to the commencement of any Work, the Contractor shall provide the City with evidence that it has obtained the insurance required by this Section and all bonds, including, but not limited to, payment and performance bonds, required in the Special Provisions. Failure to obtain and maintain the required insurance and bonds to so shall be deemed a material breach of this Contract.

17.2 Insurance Requirements. Contractor shall obtain the following insurance.

A. Worker's Compensation and Employer's Liability Insurance: The CONTRACTOR shall have in effect during the entire life of this Contract workers' compensation and Employer's Liability Insurance providing full statutory coverage. In signing this Contract, the CONTRACTOR makes the following certification, required by Section 18161 of the California Labor Code: "I am aware of the provisions of Section 3700 of the California Labor Code which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with the provisions of the Code, and I will comply with such provisions before commencing the performance of the work of this Contract."

B. Commercial General Liability Insurance: The CONTRACTOR shall take out and maintain during the life of this Contract such Bodily Injury Liability and Property Damage Liability Insurance (Commercial General Liability Insurance) on an occurrence basis as shall protect it while performing work covered by this Contract from any and all claims for damages for bodily

injury, including accidental death, as well as claims for property damage which may arise from the CONTRACTOR's operations under this Contract, whether such operations be by CONTRACTOR or by any sub-consultant or by anyone directly or indirectly employed by either of them. The amounts of such insurance shall be not less than two million dollars (\$2,000,000) per occurrence and four million dollars (\$4,000,000) in aggregate, or four million dollars (\$4,000,000) combined single limit bodily injury and property damage for each occurrence. CONTRACTOR shall provide the City with acceptable evidence of coverage, including a copy of all declarations of coverage exclusions.

C. Automobile Liability Insurance: CONTRACTOR shall maintain Automobile Liability Insurance pursuant to this Contract in an amount of not less than one million dollars (\$1,000,000) for each accident combined single limit or not less than one million dollars (\$1,000,000) for any one (1) person, and one million dollars (\$1,000,000) for any one (1) accident, and Three Hundred Thousand Dollars, (\$300,000) property damage.

17.3 CITY and its subsidiary agencies, and their officers, agents, employees and servants shall be named as additional insured on any such policies of Commercial General Liability and Automobile Liability Insurance, (but not for the workers' compensation), which shall also contain a provision that the insurance afforded thereby to the CITY, its subsidiary agencies, and their officers, agents, employees, and servants shall be primary insurance to the full limits of liability of the policy, and that if the CITY, its subsidiary agencies and their officers and employees have other insurance against a loss covered by a policy, such other insurance shall be excess insurance only.

17.4 In the event of the breach of any provision of this Section, or in the event any notice is received which indicates any required insurance coverage will be diminished or canceled, CITY, at its option, may, notwithstanding any other provision of this Contract to the contrary, immediately declare a material breach of this Contract and suspend all further work pursuant to this Contract.

17.5. Before the execution of this Contract, any deductibles or self-insured retentions must be declared to and approved by CITY.

18. SUSPENSION. The City may, at any time and from time to time, without cause, order Contractor, in writing ("Suspension Order"), to suspend, delay, or interrupt the Work in whole or in part for such period of time, up to an aggregate of fifty percent (50%) of the Contract Time, as City may determine, with such period of suspension to be computed from the date of the Suspension Order. Upon receipt of a Suspension Order, Contractor shall, at City's expense, comply with its terms and take all reasonable steps to minimize costs allocable to the Work covered by the Suspension Order during the period of work stoppage. Within the period of the above noted aggregate time, or such extension to that period as is agreed upon by Contractor and City, City shall either cancel the Suspension Order or delete the work covered by the Suspension Order by issuing a Change Order. If a Suspension Order is canceled or expires, Contractor shall resume and continue with the Work. A Change Order will be issued to cover any adjustments of the Contract Sum or the Contract Time necessarily caused by such suspension.

19. BOOKS AND RECORDS. Contractor shall keep full and detailed accounts and exercise such controls as may be necessary for proper financial management under this Contract in accordance with generally accepted accounting principles and practices consistently applied. City and City's accountants shall be afforded access at all times during normal business hours, to inspect, audit and copy Contractor's records, books, estimates, take-offs, cost reports, ledgers, schedules, correspondence, instructions, drawings, receipts, subcontracts, purchase orders, vouchers, memoranda and other data relating to this Project, and Contractor shall preserve these for a period of three years after the later of (i) final payment or (ii) final resolution of all Contract Disputes and other disputes or for such longer period as may be required by law. Contractor's compliance with any request by City pursuant to this Section 18 shall be a condition precedent to filing or maintenance of any legal action or proceeding by Contractor against City and to Contractor's right to receive further payments under the Contract Documents. Any failure by Contractor to provide access to its business records for inspection or copying by City shall be specifically enforceable by issuance of a writ or a provisional or permanent mandatory injunction by a court of competent jurisdiction based on affidavits submitted to such court, without the necessity of oral testimony.

20. WAIVER. Waiver by either party of any breach or violation of any one or more terms or conditions of this Contract shall not be deemed to be a waiver of any other term or condition contained herein or a waiver of any subsequent breach or violation of the same or any other term or condition. Acceptance by the City of the performance of any work by the Contractor shall not be deemed to be a waiver of any term or condition of this Contract. In no event shall the City's making of any payment to Contractor constitute or be construed as a waiver by the City of any breach of this Contract, or any default which may then exist on the part of Contractor, and the making of any such payment by the City shall in no way impair or prejudice any right or remedy available to the City with regard to such breach or default.

21. DEFAULT. In the event the City determines, in its sole discretion, that Contractor has failed or refused to perform any of the obligations set forth in the Contract Documents, or is in breach of any provision of the Contract Documents, the City may give written notice of default to Contractor in the manner specified for this giving of notices in this Contract. Except for emergencies, Contractor shall cure any default in performance of its obligations under the Contract Documents within two (2) business days after receipt of written notice. However, if the breach cannot be reasonably cured within such time, Contractor will commence to cure the breach within two (2) days and will diligently and continuously prosecute such cure to completion within a reasonable time, which shall in no event be later than ten (10) days after receipt of such written notice.

22. CITY RIGHTS AND REMEDIES.

22.1 Remedies Upon Default. In the event that Contractor fails to cure any default of this Contract within the time period set forth in Section 20, then City may pursue any remedies available under law or equity, including, without limitation, the following: (1) the City may, without terminating the Contract, delete certain portions of the Work, reserving to itself all rights to losses related thereto; (2) the City may, without terminating the Contract, engage others to

perform the Work or portion of the Work that has not been performed by the Contractor and withhold the cost thereof to City from future payments to the Contractor, reserving to itself all rights to Losses related thereto; or (3) the City may, without terminating the Contract and reserving to itself all rights to Losses related thereto, suspend all or any portion of this Construction Contract for as long a period of time as City determines, in its sole discretion, appropriate, in which event City shall have no obligation to adjust the Contract Sum or Contract Time, and shall have no liability to Contractor for damages if City directs Contractor to resume Work; (4) the City may terminate all or any part of this Contract for default, reserving to itself all rights of Losses related thereto; or (5) the City may, without terminating the Contract and reserving to itself all rights to Losses related thereto, exercise its rights under the Performance Bond.

22.2 Additional Provisions. All of City's rights and remedies under this Contract are cumulative, and shall be in addition to those rights and remedies available in law or in equity. Designation in the Contract Documents of certain breaches as material shall not be construed as implying that other breaches not so designated are not material nor shall such designations be construed as limiting City's right to terminate the Contract, or the exercise of its other rights or remedies for default, to only material breaches. City's determination of whether there has been noncompliance with the Contract so as to warrant exercise by City of its rights and remedies for default under the Contract, shall be binding on all parties. No termination or action taken by City after such termination shall prejudice any other rights or remedies of City provided by law or equity or by the Contract Documents upon such termination; and City may proceed against Contractor to recover all liquidated damages and Losses suffered by City.

22.3 Delays by Sureties. Without limitation to any of City's other rights or remedies under the law, City has the right to suspend the performance by Contractor's sureties in the event of any of the following: (1) failure of the sureties to begin Work within a reasonable time in such manner as to insure full compliance with the Contract within the Contract Time; (2) abandonment of the Work; (3) if at any time City is of the opinion the Work is unnecessarily or unreasonably delayed; (4) willful violation of any terms of the Contract; (5) failure to perform according to the Contract Documents; or (6) failure to follow instructions of City for its completion within the Contract Time. City will serve notice of such failure upon the sureties and in the event the sureties neglect or refuse to cure the breach within the time specified in such notice, City shall have the power to suspend the performance or any part thereof of the sureties.

22.4 Damages to the City. The City will be entitled to recovery of all Losses under law or equity in the event of Contractor's default under the Contract Documents. In the event that City's Losses arise from Contractor's default under the Contract Documents, City shall be entitled to withhold monies otherwise payable to Contractor until Final Completion, as defined in the General Conditions, of the Project. If City incurs Losses due to Contractor's default, then the amount of Losses shall be deducted from the amounts withheld. Should the amount withheld exceed the amount deducted, the balance will be paid to Contractor or its designee upon Final Completion of the Project. If the Losses incurred by City exceed the amount withheld, Contractor shall be liable to City for the difference and shall promptly remit same to City.

22.5 Termination of the Contract for Default. Without limitation to any of City's other rights or

remedies at law or in equity, and reserving to itself all rights to Losses related thereto, City shall have the right to terminate this Contract, in whole or in part, upon the failure of Contractor to promptly cure any default. City's election to terminate the Contract for default shall be communicated by giving Contractor a written notice of termination in the manner specified for the giving of notices in the Contract. Any notice of termination given to Contractor by City shall be effective immediately, unless otherwise provided therein.

22.6 Termination Without Cause. City shall have the option, at its sole discretion and without cause, of terminating this Contract in part or in whole by giving thirty (30) days written notice to Contractor. Contractor agrees to accept such sums as allowed under this Section as its sole and exclusive compensation and waives any claim for other compensation or Losses, including, but not limited to, loss of anticipated profits, loss of revenue, lost opportunity, or other consequential, direct, indirect or incidental damages of any kind.

22.7 Compensation. Following termination without cause and within forty-five (45) days after receipt of a billing from Contractor seeking payment of sums authorized by this Section, City shall pay to Contractor as its sole compensation for performance of the Work the following: (1) the amount of the Contract Sum allocable to the portion of the Work properly performed by Contractor as of the date of termination, less sums previously paid to Contractor; (2) reasonable costs of Contractor and its Subcontractors and Sub-subcontractors for demobilizing and administering the close-out of its participation in the Project (including, without limitation, all billing and accounting functions, not including attorney or expert fees) for a period of no longer than thirty (30) days after receipt of the notice of termination in an amount not to exceed the daily sum payable to Contractor for Compensable Delays; (3) previously unpaid cost of any items delivered to the Project Site which were fabricated for subsequent incorporation in the Work.

22.8 Subcontractors. Contractor shall include provisions in all of its subcontracts, purchase orders and other contracts permitting termination for convenience by Contractor on terms that are consistent with this Contract and that afford no greater rights of recovery against Contractor than are afforded to Contractor under this Section.

22.9 Contractor's Duties Upon Termination. Upon receipt of a notice of termination for default or for convenience, Contractor shall, unless the notice directs otherwise, do the following: (1) immediately discontinue the Work to the extent specified in the notice; (2) place no further orders or subcontracts for materials, equipment, services or facilities, except as may be necessary for completion of such portion of the Work as is not discontinued; (3) provide to City a description, in writing no later than fifteen (15) days after receipt of the notice of termination, of all subcontracts, purchase orders and contracts that are outstanding, including, without limitation, the terms of the original price, any changes, payments, balance owing, the status of the portion of the Work covered and a copy of the subcontract, purchase order or contract and any written changes, amendments or modifications thereto, together with such other information as City may determine necessary in order to decide whether to accept assignment of or request Contractor to terminate the subcontract, purchase order or contract; (4) promptly assign to City those subcontracts, purchase orders or contracts, or portions thereof, that City elects to accept by assignment and cancel, on the most favorable terms reasonably possible, all subcontracts,

purchase orders or contracts, or portions thereof, that City does not elect to accept by assignment; and (5) hereafter do only such Work as may be necessary to preserve and protect Work already in progress and to protect materials, plants, and equipment on the Project Site or in transit thereto.

23. CONTRACTOR'S RIGHTS AND REMEDIES. Contractor may terminate this Construction Contract for cause only upon the occurrence of one of the following: (1) the Work is stopped for sixty (60) consecutive days, through no act or fault of Contractor, any subcontractor or any employee or agent of Contractor or any subcontractor, due to issuance of an order of a court or other public authority other than City having jurisdiction or due to an act of government, such as a declaration of a national emergency making material unavailable; or (2) if the City does not make payment of sums that are not in good faith disputed by the City and does not cure such default within ninety (90) days after receipt of notice from Contractor, then upon an additional thirty (30) days' notice to City, Contractor may terminate the Contract.

23.1 Damages to Contractor. In the event of termination for cause by Contractor, City shall pay Contractor the sums provided for in Section 21 above. Contractor agrees to accept such sums as its sole and exclusive compensation and agrees to waive any claim for other compensation or Losses, including, but not limited to, loss of anticipated profits, loss of revenue, lost opportunity, or other consequential, direct, indirect and incidental damages, of any kind.

24. NOTICES. Any notices or other communications required or permitted to be given under this Contract shall be given in writing by personal delivery, by a recognized courier service, or by U.S. mail, postage prepaid, and return receipt requested, addressed to the respective parties as follows:

To City:

Assistant Public Works Director - Engineering
City of Menlo Park
City Hall, 701 Laurel St.
Menlo Park, CA 94025

To Contractor:

Anderson Pacific Engineering Construction, Inc.
1370 Norman Avenue
Santa Clara, CA 95054

25. Notice shall be deemed communicated on the earlier of actual receipt or 48 hours after deposit in the U.S. mail, or the date of delivery shown on deliverer's receipt. In the event of any change of address, the moving party is obligated to notify the other party of the change of address in writing within a reasonable period of time.

In addition, copies of all Claims by Contractor under this contract shall be provided to the City Attorney as follows:

To City Attorney:

City Attorney
Burke, Williams & Sorensen, LLP
181 Third Street, Suite 200
San Rafael, CA 94901

All claims shall be delivered personally or sent by certified mail.

26. **NON-DISCRIMINATION AND EQUAL EMPLOYMENT OPPORTUNITY.** In the performance of this Contract, Contractor shall not discriminate against any employee, subcontractor or applicant for employment because of race, color, creed, religion, sex, marital status, sexual orientation, national origin, ancestry, age, physical or mental handicap, or medical condition. Contractor will take affirmative action to ensure that employees are treated without regard to race, color, creed, religion, sex, marital status, sexual orientation, national origin, ancestry, age, physical or mental handicap, or medical condition.

27. **CONTRACT DOCUMENTS AND PRECEDENCE.** The Contract Documents shall consist of the following documents. In case of inconsistencies between Contract Documents, the documents are listed in order of precedence.

28. **PUBLIC WORKS CLAIMS.** This Contract is subject to Public Contracts Code Section 9204 governing contractor claims.

29. **ATTORNEYS' FEES; VENUE.** In the event that any party to this Contract commences any legal action or proceeding to enforce or interpret the provisions of this Contract, the prevailing party in such action or proceeding shall be entitled to recover reasonable attorneys' fees and other costs incurred in that action or proceeding, in addition to any other relief to which the successful party may be entitled. The venue for any litigation shall be San Mateo County.

30. **COOPERATION.** In the event any claim or action is brought against the City relating to Contractor's performance or services under this Agreement, Contractor shall render any reasonable assistance and cooperation which City might require.

31. **NUISANCE.** Contractor shall not maintain, commit, nor permit the maintenance or commission of any nuisance in connection with the performance of services under this Contract.

32. **GOVERNING LAW.** This Contract shall be construed in accordance with and governed by the laws of the State of California.

33. **COMPLETE AGREEMENT; SEVERABILITY.** This Contract, and any other documents incorporated herein by reference, represent the entire and integrated agreement between the City and Contractor. This Contract supersedes all prior oral and written negotiations, representations or agreements. No prior oral or written understanding shall be of any force or

effect with respect to those matters covered hereunder. This Contract may only be modified by a written amendment duly executed by the parties to this Contract. In case a provision of this Contract is held to be invalid, illegal or unenforceable, the validity, legality and enforceability of the remaining provisions shall not be affected.

34. COUNTERPARTS. This Contract may be signed in multiple counterparts, which shall, when executed by all the parties constitute a single binding contract.

Signatures on next page.

IN WITNESS WHEREOF, the parties hereto have executed this agreement on the day and year first above written.

FOR FIRST PARTY:

Signature

Date

Printed name

Title

Tax ID#

APPROVED AS TO FORM:

Nira F. Doherty, City Attorney

Date

FOR CITY OF MENLO PARK:

Justin I. C. Murphy, City Manager

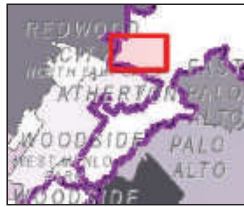
Date

ATTEST:

Judi A. Herren, City Clerk

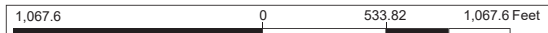
Date

Chrysler Stormwater Pump Station - Drainage Area



Legend

1:6,406



NAD_1983_StatePlane_California_III_FIPS_0403_Feet
City of Menlo Park GIS

This map is for reference purposes only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable. The City of Menlo Park and its staff shall not be held responsible for errors or omissions. Please contact City staff for the most current information.

Notes



STAFF REPORT

City Council

Meeting Date:

2/28/2023

Staff Report Number:

23-039-CC

Consent Calendar:

Authorize the city manager to enter into a contract with Rincon Consultants Inc. to prepare the environmental analysis for the proposed Life Sciences development project at 980-1030 O'Brien Drive for the amount of \$137,459 and future augments as may be necessary to complete the environmental review for the proposed project

Recommendation

Staff recommends that the City Council authorize the city manager to execute the contract for environmental review services for the proposed project at 980-1030 O'Brien Drive, attached hereto as Attachment A, with Rincon Consultants Inc. (Rincon) for the amount of \$137,459 and future augments as may be necessary to complete the environmental review for the proposed project based on the proposed scope and budget (Attachment B.)

Policy Issues

City Council Resolution No. 6479 authorizes the city manager to execute agreements necessary to conduct City business up to a stated award authority level which adjusts annually based on changes in the construction cost index. The current award authority is \$86,000. While the project applicant is responsible for the full cost of preparing the required environmental analysis for a project, and no taxpayer funds are being used for said purpose, the City Council retains discretion for all agreements exceeding the award authority delegated to the city manager.

The Planning Commission will be the acting body for the project entitlements (e.g., architectural control, use permit) and environmental review for the proposed project. The proposed project also includes a lot merger that would be reviewed by the engineering division.

Authorizing the city manager to enter into a contract with Rincon would allow the City to conduct the environmental review for the project proposal. Approval of the environmental review contract does not imply an endorsement of a project, but rather initiates the process to identify potential environmental impacts of the project for consideration during entitlement review. Policy implications of development projects are considered on a case-by-case basis.

Background

The applicant proposes to construct a new three-story life sciences/research and development (R&D) building with a ground floor commercial space. The building would be approximately 67,688 square feet in size, inclusive of an approximately 5,800 square foot commercial space on the ground floor, in the life science (L-S) zoning district located at 980-1030 O'Brien Drive.

As part of the project, two of the existing buildings, located at 1020 and 1030 O'Brien Drive, would be demolished, and the buildings and uses at 980-990 and 1010 O'Brien Drive, including the community hub sponsored by Meta, would be retained. The applicant proposes to construct the proposed building utilizing the development potential from all four parcels (e.g., gross floor area.) All development regulations would be comprehensively evaluated across the entire project site rather than on a parcel-by-parcel basis and two out of the four parcels would be merged to facilitate the development of the proposed building. The proposed building would utilize development potential transferred from the 980-990 and 1010 O'Brien Drive parcels to the merged parcel. This is a non-bonus level development project. Select plan sheets from the project plans are included in Attachment C. A location map identifying the project site is included in Attachment D. Additional information on the proposed project is available on the city-maintained project page (Attachment D.)

Environmental review process overview

Every discretionary action requires environmental review pursuant to the California Environmental Quality Act (CEQA.) One of the purposes of CEQA is to inform decision-makers and the public about the potential significant environmental effects of a proposed project. The first step in the environmental review process is for the City to determine if the proposed project would have a possible significant environmental effect or if the proposed project would qualify for a statutory or categorical exemption. Statutory exemptions are projects specifically excluded from CEQA consideration as defined by the State Categorical exemptions are made up of classes of projects that generally are considered not to have potential impacts on the environment. If a project falls within one of these classes, and no exceptions apply, it is exempt from the provisions of CEQA and no further environmental review is required. If the City determines that a potential significant impact could occur, then an exemption would not be applicable and additional environmental review would be required.

Based on preliminary review of the proposed project, the proposed project may qualify for the Class 32 infill categorical exemption (CEQA Guideline Section 15332.) The Class 32 infill categorical exemption exempts infill development within urbanized areas if it meets certain criteria. For a proposed project to qualify under class 32 exemption, it must meet the following criteria:

- The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations.
- The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.
- The project site has no value as habitat for endangered, rare or threatened species.
- Approval of the project would not result in any significant effects relating to traffic, noise, air quality or water quality.
- The site can be adequately served by all required utilities and public services.

Consistent with the requirements of CEQA, for the proposed project technical studies are necessary to substantiate these criteria, including but not limited to transportation, air quality, noise and historical resources. The transportation analysis would be prepared in compliance with the City's TIA Guidelines, which include a vehicle miles traveled (VMT) threshold for compliance with CEQA and a non-CEQA level of service (or congestion) analysis for compliance with the City's general plan.

Following City Council authorization of the environmental consultant contract, the consultant will analyze the project to determine if the project meets the criteria for the Class 32 categorical exemption and prepare a categorical exemption report.

CEQA also mandates that projects which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified shall not require additional environmental review, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site. If a Class 32 exemption is determined to not be appropriate, then additional analysis may be needed to determine whether a §15183 exemption would apply.

If through the project analysis the consultant and staff determine that there is a reasonable possibility that the project would have a significant effect, then per §15300.2 of the CEQA Guidelines, the City would further evaluate the possible environmental effects of the project through the appropriate level of environmental review (e.g., negative declaration, mitigated negative declaration or an EIR.)

Analysis

As part of the environmental review consultant selection process, staff typically requests proposals from multiple environmental consulting firms. For the proposed project, staff released a request for proposals (RFP) to 33 firms and received proposals from four firms: Dudek, Rincon, SWCA and ICF Jones and Stokes Inc. Three of the four firms selected subconsultants to prepare the transportation impact analysis. A brief comparison of the scopes is provided in Table 1 below. Three of the four scopes include optional tasks that have been incorporated into the total budget identified in Table 1. Through the environmental analysis, the City will determine if those specific tasks are necessary for the analysis of the project. The four scopes are included in this staff report as Attachments B, F, G and H.

Table 1: Comparison of environmental review scopes and budgets			
Firm	Subconsultants	Experience/other key factors	Project total cost
Dudek	In-house	Projects referenced in scope are of EIR projects in and around the Bay Area, and one notice of exemption (NOE) of similar level or scale.	\$164,329
Rincon	Hexagon	Proposed project manager has experience with class 32 exemption preparation for four projects.	\$137,459
SWCA	Kittleson	Projects referenced in scope are of EIR projects in California.	\$161,319
ICF	Hexagon	Projects referenced in scope are of projects in and around the Bay Area, with four NOE of similar level or scale.	\$218,899

City staff evaluated the four scopes and reviewed them with the applicant team, who is responsible for the full cost of the preparation of the required environmental analyses. Staff and the applicant team believe that each consultant has the necessary experience. However, staff recommends that the City Council select Rincon for the environmental review contract for the following reasons:

1. Rincon has extensive environmental review experience, including the preparation of categorical exemption documents for cities throughout the Bay Area;
2. Rincon’s proposal assumes a quicker project timeline. However, the scope and depth of the analysis remains appropriate for the proposed project; and

3. Selecting Rincon would allow for a continued diversification of environmental reviews and would further develop the City's working relationship with the firm.

Staff has shared its recommendation of Rincon with the applicant, who agrees with the recommendation. As stated previously, it may be determined that additional technical analyses are required or that the project would not qualify for a categorical exemption. Staff recommends that the City Council provide the city manager the authority to approve future contract augmentations, if needed. The draft contract indicating Rincon as the primary consultant and the proposed budget are included as Attachments A and B respectively.

Next steps

A study session for the proposed project is scheduled for the March 13 Planning Commission meeting. Following authorization of the contract for the environmental analysis, the City's consultant would begin the environmental analysis to confirm if the project qualifies for a categorical exemption.

Impact on City Resources

The applicant is required to pay all planning, building and public works permit fees, based on the City's master fee schedule, to fully cover the cost of staff time spent on the review of the project. The applicant is also required to bear the cost of the environmental review. For the environmental review and other supporting studies required by the City, the applicant deposits money with the City and the City pays the consultants. Notwithstanding, the scope and content of the environmental review is determined by the City in its sole discretion and the City is the final decision maker on the adequacy of the document.

Environmental Review

An environmental analysis, currently scoped as a categorical exemption, will be prepared for the proposed project in compliance with CEQA.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Upon receipt of the application submittal, the City sent an application submittal notice and mailed notices of the March 13 study session were sent to a quarter mile radius February 24. Additionally, a public meeting notice will be published in the Examiner March 3, identifying that the Planning Commission will hold a study session on the item at its meeting March 13.

Attachments

- A. Consultant services agreement with Rincon
- B. Environmental review scope and budget proposal from Rincon
- C. Project plans (select sheets)
- D. Hyperlink –Project page: menlopark.gov/Government/Departments/Community-Development/Projects/Under-review/1030-OBrien-Drive
- E. Location map
- F. Environmental review scope and budget proposal from Dudek
- G. Environmental review scope and budget proposal from SWCA Environmental Consultants
- H. Environmental review scope and budget proposal from ICF Jones and Stokes Inc.

Staff Report #: 23-039-CC

Report prepared by:
Fahteen Khan, Associate Planner

Report reviewed by:
Kyle Perata, Planning Manager
Deanna Chow, Assistant Community Development Director

CONSULTANT AGREEMENT

City Manager's Office
701 Laurel St., Menlo Park, CA 94025
tel 650-330-6620



Agreement #:
AGREEMENT FOR SERVICES BETWEEN THE CITY OF MENLO PARK AND RINCON CONSULTANTS INC.
THIS AGREEMENT made and entered into at Menlo Park, California, this _____, by and between the CITY OF MENLO PARK, a Municipal Corporation, hereinafter referred to as "CITY," and RINCON CONSULTANTS INC., hereinafter referred to as "FIRST PARTY."
<p>WITNESSETH:</p> <p>WHEREAS, R&M Properties, proposes to demolish two of the four existing, one-story commercial buildings on four contiguous parcels located at 980-990, 1010, 1020 and 1030 O'Brien Drive (Assessor's Parcel numbers 055-422-090, 055-422-100, 055-422-060, and 025-434-010 respectively), Menlo Park, and to construct one new three-story R&D building totaling approximately 67,688 square feet, with 23,643 square feet of covered parking in the Life Science, (LS) zoning district, hereafter referred to as the "Project", and</p> <p>WHEREAS, the City has determined that under the California Environmental Quality Act and its applicable guidelines the Project may fall under Class 32 Infill Categorical Exemption; and</p> <p>WHEREAS, FIRST PARTY is licensed to perform said services and desires to and does hereby undertake to perform said services.</p> <p>NOW, THEREFORE, IN CONSIDERATION OF THE MUTUAL COVENANTS, PROMISES AND CONDITIONS of each of the parties hereto, it is hereby agreed as follows:</p>
1. SCOPE OF WORK
In consideration of the payment by CITY to FIRST PARTY, as hereinafter provided, FIRST PARTY agrees to perform all the services as set forth in Exhibit "A," Scope of Services.
2. SCHEDULE FOR WORK
<p>FIRST PARTY's proposed schedule for the various services required pursuant to this agreement will be as set forth in Exhibit "A," Scope of Services. CITY will be kept informed as to the progress of work by written reports, to be submitted monthly or as otherwise required in Exhibit "A." Neither party shall hold the other responsible for damages or delay in performance caused by acts of God, strikes, lockouts, accidents or other events beyond the control of the other, or the other's employees and agents.</p> <p>FIRST PARTY shall commence work immediately upon receipt of a "Notice to Proceed" from CITY. The "Notice to Proceed" date shall be considered the "effective date" of the agreement, as used herein, except as otherwise specifically defined. FIRST PARTY shall complete all the work and deliver to CITY all project related files, records, and materials within one month after completion of all of FIRST PARTY's activities required under this agreement.</p>

3. PROSECUTION OF WORK

FIRST PARTY will employ a sufficient staff to prosecute the work diligently and continuously and will complete the work in accordance with the schedule of work approved by the CITY. (See Exhibit "A," Scope of Services).

4. COMPENSATION AND PAYMENT

- A. CITY shall pay FIRST PARTY an all-inclusive fee that shall not exceed \$137,459 as described in Exhibit "A," Scope of Services. All payments shall be inclusive of all indirect and direct charges to the Project incurred by FIRST PARTY. The CITY reserves the right to withhold payment if the City determines that the quantity or quality of the work performed is unacceptable.
- B. FIRST PARTY's fee for the services as set forth herein shall be considered as full compensation for all indirect and direct personnel, materials, supplies and equipment, and services incurred by FIRST PARTY and used in carrying out or completing the work.
- C. Payments shall be monthly for the invoice amount or such other amount as approved by CITY. As each payment is due, the FIRST PARTY shall submit a statement describing the services performed to CITY. This statement shall include, at a minimum, the project title, agreement number, the title(s) of personnel performing work, hours spent, payment rate, and a listing of all reimbursable costs. CITY shall have the discretion to approve the invoice and the work completed statement. Payment shall be for the invoice amount or such other amount as approved by CITY.
- D. Payments are due upon receipt of written invoices. CITY shall have the right to receive, upon request, documentation substantiating charges billed to CITY. CITY shall have the right to perform an audit of the FIRST PARTY's relevant records pertaining to the charges.

5. EQUAL EMPLOYMENT OPPORTUNITY

- A. FIRST PARTY, with regard to the work performed by it under this agreement shall not discriminate on the grounds of race, religion, color, national origin, sex, handicap, marital status or age in the retention of sub-consultants, including procurement of materials and leases of equipment.
- B. FIRST PARTY shall take affirmative action to insure that employees and applicants for employment are treated without regard to their race, color, religion, sex, national origin, marital status or handicap. Such action shall include, but not be limited to the following: employment, upgrading, demotion, or transfer; recruitment advertising; layoff or termination; rates of pay or other forms of compensation and selection for training including apprenticeship.
- C. FIRST PARTY shall post in prominent places, available to employees and applicants for employment, notices setting forth the provisions of this non-discrimination clause.
- D. FIRST PARTY shall state that all qualified applications will receive consideration for employment without regard to race, color, religion, sex, national origin, marital status or handicap.
- E. FIRST PARTY shall comply with Title VI of the Civil Rights Act of 1964 and shall provide such reports as may be required to carry out the intent of this section.
- F. FIRST PARTY shall incorporate the foregoing requirements of this section in FIRST PARTY's agreement with all sub-consultants.

CONTINUED ON NEXT PAGE

6. ASSIGNMENT OF AGREEMENT AND TRANSFER OF INTEREST

- A. FIRST PARTY shall not assign this agreement, and shall not transfer any interest in the same (whether by assignment or novation), without prior written consent of the CITY thereto, provided, however, that claims for money due or to become due to the FIRST PARTY from the CITY under this agreement may be assigned to a bank, trust company, or other financial institution without such approval. Notice of an intended assignment or transfer shall be furnished promptly to the CITY.
- B. In the event there is a change of more than 30 percent of the stock ownership or ownership in FIRST PARTY from the date of this agreement is executed, then CITY shall be notified before the date of said change of stock ownership or interest and CITY shall have the right, in event of such change in stock ownership or interest, to terminate this agreement upon notice to FIRST PARTY. In the event CITY is not notified of any such change in stock ownership or interest, then upon knowledge of same, it shall be deemed that CITY has terminated this agreement.

7. INDEPENDENT WORK CONTROL

It is expressly agreed that in the performance of the service necessary for compliance with this agreement, FIRST PARTY shall be and is an independent contractor and is not an agent or employee of CITY. FIRST PARTY has and shall retain the right to exercise full control and supervision of the services and full control over the employment, direction, compensation and discharge of all persons assisting FIRST PARTY in the performance of FIRST PARTY's services hereunder. FIRST PARTY shall be solely responsible for its own acts and those of its subordinates and employees.

8. CONSULTANT QUALIFICATIONS

It is expressly understood that FIRST PARTY is licensed and skilled in the professional calling necessary to perform the work agreed to be done by it under this agreement and CITY relies upon the skill of FIRST PARTY to do and perform said work in a skillful manner usual to the profession. The acceptance of FIRST PARTY's work by CITY does not operate as a release of FIRST PARTY from said understanding.

9. NOTICES

All notices hereby required under this agreement shall be in writing and delivered in person or sent by certified mail, postage prepaid or by overnight courier service. Notices required to be given to CITY shall be addressed as follows:

Deanna Chow
 Community Development
 City of Menlo Park
 701 Laurel St.
 Menlo Park, CA 94025
 650-330-6733
 dmchow@menlopark.gov

Notices required to be given to FIRST PARTY shall be addressed as follows:

Abe Leider, Principal
 Rincon Consultants, Inc.
 449 15th Street, Suite 303
 Oakland, CA 94612
 510-834-4455
 aleider@rinconconsultants.com

Provided that any party may change such address by notice, in writing, to the other party and thereafter notices shall be addressed and transmitted to the new address.

10. HOLD HARMLESS

The FIRST PARTY shall defend, indemnify and hold harmless the CITY, its subsidiary agencies, their officers, agents, employees and servants from all claims, suits or actions that arise out of, pertain to, or relate to the negligence, recklessness, or willful misconduct of the FIRST PARTY brought for, or on account of, injuries to or death of any person or damage to property resulting from the performance of any work required by this agreement by FIRST PARTY, its officers, agents, employees and servants. Nothing herein shall be construed to require the FIRST PARTY to defend, indemnify or hold harmless the CITY, its subsidiary agencies, their officers, agents, employees and servants against any responsibility to liability in contravention of Section 2782.8 of the California Civil Code.

CONTINUED ON NEXT PAGE

11. INSURANCE

- A. FIRST PARTY shall not commence work under this agreement until all insurance required under this Section has been obtained and such insurance has been approved by the City, with certificates of insurance evidencing the required coverage.
- B. There shall be a contractual liability endorsement extending the FIRST PARTY's coverage to include the contractual liability assumed by the FIRST PARTY pursuant to this agreement. These certificates shall specify or be endorsed to provide that thirty (30) days' notice must be given, in writing, to the CITY, at the address shown in Section 9, of any pending cancellation of the policy. FIRST PARTY shall notify CITY of any pending change to the policy. All certificates shall be filed with the City.
1. Workers' compensation and employer's liability insurance:
The FIRST PARTY shall have in effect during the entire life of this agreement workers' compensation and Employer's Liability Insurance providing full statutory coverage. In signing this agreement, the FIRST PARTY makes the following certification, required by Section 18161 of the California Labor Code: "I am aware of the provisions of Section 3700 of the California Labor Code which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with the provisions of the Code, and I will comply with such provisions before commencing the performance of the work of this agreement" (not required if the FIRST PARTY is a Sole Proprietor).
 2. Liability insurance:
The FIRST PARTY shall take out and maintain during the life of this agreement such Bodily Injury Liability and Property Damage Liability Insurance (Commercial General Liability Insurance) on an occurrence basis as shall protect it while performing work covered by this agreement from any and all claims for damages for bodily injury, including accidental death, as well as claims for property damage which may arise from the FIRST PARTY's operations under this agreement, whether such operations be by FIRST PARTY or by any sub-consultant or by anyone directly or indirectly employed by either of them. The amounts of such insurance shall be not less than one million dollars (\$1,000,000) per occurrence and one million dollars (\$1,000,000) in aggregate, or one million dollars (\$1,000,000) combined single limit bodily injury and property damage for each occurrence. FIRST PARTY shall provide the CITY with acceptable evidence of coverage, including a copy of all declarations of coverage exclusions. FIRST PARTY shall maintain Automobile Liability Insurance pursuant to this agreement in an amount of not less than one million dollars (\$1,000,000) for each accident combined single limit or not less than one million dollars (\$1,000,000) for any one (1) person, and one million dollars (\$1,000,000) for any one (1) accident, and Three Hundred Thousand Dollars, (\$300,000) property damage.
 3. Professional liability insurance:
FIRST PARTY shall maintain a policy of professional liability insurance, protecting it against claims arising out of the negligent acts, errors, or omissions of FIRST PARTY pursuant to this agreement, in the amount of not less than one million dollars (\$1,000,000) per claim and in the aggregate. Said professional liability insurance is to be kept in force for not less than one (1) year after completion of services described herein.
- C. CITY and its subsidiary agencies, and their officers, agents, employees and servants shall be named as additional insured on any such policies of Commercial General Liability and Automobile Liability Insurance, (but not for the Professional Liability and workers' compensation), which shall also contain a provision that the insurance afforded thereby to the CITY, its subsidiary agencies, and their officers, agents, employees, and servants shall be primary insurance to the full limits of liability of the policy, and that if the CITY, its subsidiary agencies and their officers and employees have other insurance against a loss covered by a policy, such other insurance shall be excess insurance only.
- D. In the event of the breach of any provision of this Section, or in the event any notice is received which indicates any required insurance coverage will be diminished or canceled, CITY, at its option, may, notwithstanding any other provision of this agreement to the contrary, immediately declare a material breach of this agreement and suspend all further work pursuant to this agreement.
- E. Before the execution of this agreement, any deductibles or self-insured retentions must be declared to and approved by CITY.

12. PAYMENT OF PERMITS/LICENSES

Contractor shall obtain any license, permit, or approval if necessary from any agency whatsoever for the work/services to be performed, at his/her own expense, before commencement of said work/services or forfeit any right to compensation under this agreement.

13. RESPONSIBILITY AND LIABILITY FOR SUB-CONSULTANTS AND/OR SUBCONTRACTORS

Approval of or by CITY shall not constitute nor be deemed a release of responsibility and liability of FIRST PARTY or its sub-consultants and/or subcontractors for the accuracy and competency of the designs, working drawings, specifications or other documents and work, nor shall its approval be deemed to be an assumption of such responsibility by CITY for any defect in the designs, working drawings, specifications or other documents prepared by FIRST PARTY or its sub-consultants and/or subcontractors.

14. OWNERSHIP OF WORK PRODUCT

Work products of FIRST PARTY for this project, which are delivered under this agreement or which are developed, produced and paid for under this agreement, shall become the property of CITY. The reuse of FIRST PARTY's work products by City for purposes other than intended by this agreement shall be at no risk to FIRST PARTY.

15. REPRESENTATION OF WORK

Any and all representations of FIRST PARTY, in connection with the work performed or the information supplied, shall not apply to any other project or site, except the project described in Exhibit "A" or as otherwise specified in Exhibit "A."

16. TERMINATION OF AGREEMENT

- A. CITY may give thirty (30) days written notice to FIRST PARTY, terminating this agreement in whole or in part at any time, either for CITY's convenience or because of the failure of FIRST PARTY to fulfill its contractual obligations or because of FIRST PARTY's change of its assigned personnel on the project without prior CITY approval. Upon receipt of such notice, FIRST PARTY shall:
1. Immediately discontinue all services affected (unless the notice directs otherwise); and
 2. Deliver to the CITY all data, drawings, specifications, reports, estimates, summaries, and such other information and materials as may have been accumulated or produced by FIRST PARTY in performing work under this agreement, whether completed or in process.
- B. If termination is for the convenience of CITY, an equitable adjustment in the contract price shall be made, but no amount shall be allowed for anticipated profit on unperformed services.
- C. If the termination is due to the failure of FIRST PARTY to fulfill its agreement, CITY may take over the work and prosecute the same to completion by agreement or otherwise. In such case, FIRST PARTY shall be liable to CITY for any reasonable additional cost occasioned to the CITY thereby.
- D. If, after notice of termination for failure to fulfill agreement obligations, it is determined that FIRST PARTY had not so failed, the termination shall be deemed to have been effected for the convenience of the CITY. In such event, adjustment in the contract price shall be made as provided in Paragraph B of this Section.
- E. The rights and remedies of the CITY provided in this Section are in addition to any other rights and remedies provided by law or under this agreement.
- F. Subject to the foregoing provisions, the CITY shall pay FIRST PARTY for services performed and expenses incurred through the termination date.

17. INSPECTION OF WORK

It is FIRST PARTY's obligation to make the work product available for CITY's inspections and periodic reviews upon request by CITY.

18. COMPLIANCE WITH LAWS

It shall be the responsibility of FIRST PARTY to comply with all State and Federal Laws applicable to the work and services provided pursuant to this agreement, including but not limited to compliance with prevailing wage laws, if applicable.

19. BREACH OF AGREEMENT

- A. This agreement is governed by applicable federal and state statutes and regulations. Any material deviation by FIRST PARTY for any reason from the requirements thereof, or from any other provision of this agreement, shall constitute a breach of this agreement and may be cause for termination at the election of the CITY.
- B. The CITY reserves the right to waive any and all breaches of this agreement, and any such waiver shall not be deemed a waiver of any previous or subsequent breaches. In the event the CITY chooses to waive a particular breach of this agreement, it may condition same on payment by FIRST PARTY of actual damages occasioned by such breach of agreement.

20. SEVERABILITY

The provisions of this agreement are severable. If any portion of this agreement is held invalid by a court of competent jurisdiction, the remainder of the agreement shall remain in full force and effect unless amended or modified by the mutual consent of the parties.

21. CAPTIONS

The captions of this agreement are for convenience and reference only and shall not define, explain, modify, limit, exemplify, or aid in the interpretation, construction, or meaning of any provisions of this agreement.

22. LITIGATION OR ARBITRATION

In the event that suit or arbitration is brought to enforce the terms of this agreement, the prevailing party shall be entitled to litigation costs and reasonable attorneys' fees. The Dispute Resolution provisions are set forth on Exhibit "B," 'Dispute Resolution' attached hereto and by this reference incorporated herein.

23. RETENTION OF RECORDS

Contractor shall maintain all required records for three years after the City makes final payment and all other pending matters are closed, and shall be subject to the examination and /or audit of the City, a federal agency, and the state of California.

24. TERM OF AGREEMENT

This agreement shall remain in effect for the period of March 1, 2023 through August 31, 2023 unless extended, amended, or terminated in writing by CITY.

25. ENTIRE AGREEMENT

This document constitutes the sole agreement of the parties hereto relating to said project and states the rights, duties, and obligations of each party as of the document's date. Any prior agreement, promises, negotiations, or representations between parties not expressly stated in this document are not binding. All modifications, amendments, or waivers of the terms of this agreement must be in writing and signed by the appropriate representatives of the parties to this agreement.

26. STATEMENT OF ECONOMIC INTEREST

Consultants, as defined by Section 18701 of the Regulations of the Fair Political Practices Commission, Title 2, Division 6 of the California Code of Regulations, are required to file a Statement of Economic Interests with 30 days of approval of a contract services agreement with the City of its subdivisions, on an annual basis thereafter during the term of the contract, and within 30 days of completion of the contract.

Based upon review of the Consultant's Scope of Work and determination by the City Manager, it is determined that Consultant IS NOT required to file a Statement of Economic Interest. A statement of Economic Interest shall be filed with the City Clerk's office no later than 30 days after the execution of the agreement.

IN WITNESS WHEREOF, the parties hereto have executed this agreement on the day and year first above written.

FOR FIRST PARTY:

Signature

Date

Printed name

Title

Tax ID#

APPROVED AS TO FORM:

Nira F. Doherty, City Attorney

Date

FOR CITY OF MENLO PARK:

Justin I.C. Murphy, City Manager

Date

ATTEST:

Judi A. Herren, City Clerk

Date

EXHIBIT "A" – SCOPE OF SERVICES

A1. SCOPE OF WORK

FIRST PARTY agrees to provide consultant services for CITY's Community Development Department. In the event of any discrepancy between any of the terms of the FIRST PARTY's proposal and those of this agreement, the version most favorable to the CITY shall prevail. FIRST PARTY shall provide the following services:

Provide general consultant services for projects as determined by the CITY. The detailed scope of work for each task the CITY assigns the consultant shall be referred to as Exhibit A -1, which will become part of this agreement. A notice to proceed will be issued separately for each separate scope of work agreed to between the CITY and FIRST PARTY.

FIRST PARTY agrees to perform these services as directed by the CITY in accordance with the standards of its profession and CITY's satisfaction.

A2. COMPENSATION

CITY hereby agrees to pay FIRST PARTY at the rates to be negotiated between FIRST PARTY and CITY as detailed in Exhibit A-1. The actual charges shall be based upon (a) FIRST PARTY's standard hourly rate for various classifications of personnel; (b) all fees, salaries and expenses to be paid to engineers, consultants, independent contractors, or agents employed by FIRST PARTY; and shall (c) include reimbursement for mileage, courier and plan reproduction. The total fee for each separate Scope of Work agreed to between the CITY and FIRST PARTY shall not exceed the amount shown in Exhibit A-1.

FIRST PARTY shall be paid within thirty (30) days after approval of billing for work completed and approved by the CITY. Invoices shall be submitted containing all information contained in Section A5 below. In no event shall FIRST PARTY be entitled to compensation for extra work unless an approved change order, or other written authorization describing the extra work and payment terms, has been executed by CITY before the commencement of the work.

A3. SCHEDULE OF WORK

FIRST PARTY'S proposed schedule for the various services required will be set forth in Exhibit A-1.

A4. CHANGES IN WORK -- EXTRA WORK

In addition to services described in Section A1, the parties may from time to time agree in writing that FIRST PARTY, for additional compensation, shall perform additional services including but not limited to:

- Change in the services because of changes in scope of the work.
- Additional tasks not specified herein as required by the CITY.

The CITY and FIRST PARTY shall agree in writing to any changes in compensation and/or changes in FIRST PARTY's services before the commencement of any work. If FIRST PARTY deems work he/she has been directed to perform is beyond the scope of this agreement and constitutes extra work, FIRST PARTY shall immediately inform the CITY in writing of the fact. The CITY shall make a determination as to whether such work is in fact beyond the scope of this agreement and constitutes extra work. In the event that the CITY determines that such work does constitute extra work, it shall provide compensation to the FIRST PARTY in accordance with an agreed cost that is fair and equitable. This cost will be mutually agreed upon by the CITY and FIRST PARTY. A supplemental agreement providing for such compensation for extra work shall be negotiated between the CITY and the FIRST PARTY. Such supplemental agreement shall be executed by the FIRST PARTY and may be approved by the City Manager upon recommendation of the Assistant Community Development Director.

A5. BILLINGS

FIRST PARTY's bills shall include the following information: A brief description of services performed, project title and the agreement number; the date the services were performed; the number of hours spent and by whom; the current contract amount; the current invoice amount; Except as specifically authorized by CITY, FIRST PARTY shall not bill CITY for duplicate services performed by more than one person. In no event shall FIRST PARTY submit any billing for an amount in excess of the maximum amount of compensation provided in Section A2.

The expenses of any office, including furniture and equipment rental, supplies, salaries of employees, telephone calls, postage, advertising, and all other expenses incurred by FIRST PARTY in the performances of this agreement shall be incurred at the FIRST PARTY's discretion. Such expenses shall be FIRST PARTY's sole financial responsibility.

EXHIBIT “B” - DISPUTE RESOLUTION

- B1.0** All claims, disputes and other matters in question between the FIRST PARTY and CITY arising out of, or relating to, the contract documents or the breach thereof, shall be resolved as follows:
- B2.0 Mediation**
- B2.1** The parties shall attempt in good faith first to mediate such dispute and use their best efforts to reach agreement on the matters in dispute. After a written demand for non-binding mediation, which shall specify in detail the facts of the dispute, and within ten (10) days from the date of delivery of the demand, the matter shall be submitted to a mutually agreeable mediator. The Mediator shall hear the matter and provide an informal opinion and advice, none of which shall be binding upon the parties, but is expected by the parties to help resolve the dispute. Said informal opinion and advice shall be submitted to the parties within twenty (20) days following written demand for mediation. The Mediator’s fee shall be shared equally by the parties. If the dispute has not been resolved, the matter shall be submitted to arbitration in accordance with Paragraph B3.1.
- B3.0 Arbitration**
- B3.1** Any dispute between the parties that is to be resolved by arbitration as provided in Paragraph B2.1 shall be settled and decided by arbitration conducted by the American Arbitration Association in accordance with the Construction Industry Arbitration Rules of the American Arbitration Association, as then in effect, except as provided below. Any such arbitration shall be held before three arbitrators who shall be selected by mutual agreement of the parties; if agreement is not reached on the selection of the arbitrators within fifteen (15) days, then such arbitrator(s) shall be appointed by the presiding Judge of the court of jurisdiction of the agreement.
- B3.2** The provisions of the Construction Industry Arbitration Rules of the American Arbitration Association shall apply and govern such arbitration, subject, however to the following:
- B3.3** Any demand for arbitration shall be writing and must be made within a reasonable time after the claim, dispute or other matter in question as arisen. In no event shall the demand for arbitration be made after the date that institution of legal or equitable proceedings based on such claim, dispute or other matter would be barred by the applicable statute of limitations.
- B3.4** The arbitrator or arbitrators appointed must be former or retired judges, or attorneys at law with last ten (10) years’ experience in construction litigation.
- B3.5** All proceedings involving the parties shall be reported by a certified shorthand court reporter, and written transcripts of the proceedings shall be prepared and made available to the parties.
- B3.6** The arbitrator or arbitrators must be made within and provide to the parties factual findings and the reasons on which the decisions of the arbitrator or arbitrators is based.
- B3.7** Final decision by the arbitrator or arbitrators must be made within ninety (90) days from the date of the arbitration proceedings are initiated.
- B3.8** The prevailing party shall be awarded reasonable attorneys’ fees, expert and non-expert witness costs and expenses, and other costs and expenses incurred in connection with the arbitration, unless the arbitrator or arbitrators for good cause determine otherwise.
- B3.9** Costs and fees of the arbitrator or arbitrators shall be borne by the non-prevailing party, unless the arbitrator or arbitrators for good cause determine otherwise.
- B3.10** The award or decision of the arbitrator or arbitrators, which may include equitable relief, shall be final, and judgment may be entered on it in accordance with applicable law in any court having jurisdiction over the matter.

November 28, 2022

Fahteen Khan, Associate Planner
Corinna D. Sandmeier, Acting Principal Planner
City of Menlo Park
701 Laurel Street
Menlo Park, California 94025

Subject: Proposal to Prepare a CEQA Class 32 Categorical Exemption Report for the Proposed 1030 O'Brien Drive Project

Dear Ms. Khan and Ms. Sandmeier:

Rincon Consultants, Inc. is pleased to submit this proposal to prepare a Class 32 Categorical Exemption (CE) Report pursuant to California Environmental Quality Act (CEQA) Guidelines Section 15332 for the proposed 1030 O'Brien Drive Project (project). Our team has a proven track record of successfully completing CEQA exemption reports, including Class 32 CE eligibility documentation, as well as more comprehensive CEQA documentation, for many similar projects throughout the greater Bay Area and the state. We pride ourselves on our ability to prepare clear and thorough environmental documents and to develop and implement innovative approaches and solutions to planning and environmental issues. In fact, as we detail in the General Approach Section in our proposal, we believe that we are offering the City a streamlined, efficient, and legally defensible option for CEQA compliance.

At Rincon, we are proud of our team's ability to work effectively with City staff, decision-makers, and community groups to help ensure positive outcomes for the public agencies we support. We believe that our experience with CEQA statewide and our experience working on projects in Menlo Park and throughout San Mateo County are critical and give us a unique understanding of and sensitivity to the broad range of issues that will be important for the success of this project.

Leading the team as Principal-in-Charge will be **Abe Leider, AICP CEP**. He has 22 years of experience directing urban planning projects, including planning and policy document preparation and CEQA analyses. **Leo Mena** will serve as project manager in charge of day-to-day oversight and will serve as the City's primary contact. He has eight years of experience in CEQA analysis and project management, including on several projects in the City of Menlo Park, including the 1075 O'Brien and 1125 O'Brien EIR projects, and is valued by his clients as a clear communicator that collaborates closely with his team to resolve problems as they arise. **Hexagon Transportation Consultants, Inc. (Hexagon)** under the leadership of **Gary Black** will be a part of Rincon's team and will prepare the transportation impact analysis. Hexagon has worked on several projects with the City and are familiar with Menlo Park processes.

Thank you for considering Rincon Consultants for this assignment. Please do not hesitate to contact us if you have questions about this proposal or need additional information on our qualifications or proposed approach to the work program.

Sincerely,
Rincon Consultants, Inc.



Leo Mena | Project Manager
lmena@rinconconsultants.com



Abe Leider, AICP CEP | Principal
aleider@rinconconsultants.com

Proposal to Prepare a CEQA Class 32 Categorical Exemption Report for the Proposed 1030 O'Brien Drive Project



Richard Daulton | Principal/Vice President

Email: rdaulton@rinconconsultants.com

Authorized to contractually obligate and negotiate
on behalf of Rincon Consultants, Inc.

This proposal is valid for 90 days.

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Appendices

- Attachment A Key Personnel Resumes
- Attachment B Project Cost Estimate
- Attachment C Preliminary Project Schedule
- Attachment D Hexagon Scope of Work

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A. Project Team

A.1 About Rincon

Rincon Consultants, Inc. is a multi-disciplinary environmental science, planning, and engineering consulting firm that provides quality professional services to government and industry. Our principal service is to provide environmental support and scientific research to create and sustain innovative solutions to natural resources, sustainability, and environmental impacts. Rincon prides itself on the considerable depth of its staff, which includes certified urban and environmental planners, environmental scientists and engineers, climate planning and sustainability experts, accredited LEED professionals, noise and air quality experts, geologists, biologists, and cultural and historical resource specialists. Our approach to every project is centered upon the design and development of innovative solutions that respond to our clients' specific needs in a cost-effective manner.

Legal Name:

Rincon Consultants, Inc.

Legal Form:

California "S" Corporation

Founded: 1994

Professional Staff: 400+

California Offices: 12

Core Values: Trusted, Fair, Transparent, Accountable, Disciplined, Entrepreneurial

Rincon's corporate culture focuses on providing environmental consulting services in a manner that is beneficial to both the environment and our client's needs. When hired, we perceive ourselves as an extension of our client's team and function with the best interests of the client in mind. By managing each project with a focus on three primary objectives – economic efficiency, technical excellence, and sustainable approach – we can provide superior service that efficiently and effectively meets the needs of our clients.

Services

We have categorized our environmental consulting services into six core areas:

- Environmental Sciences and Land Use Planning
- Biological Resources
- Water Resources
- Cultural Resources
- Site Assessment and Remediation
- Sustainability Services



We also maintain a Geographic Information Systems (GIS) and Graphics Communications group to enhance our documents and support our data analyses for projects addressing issues in these service areas.

Subconsultant Team

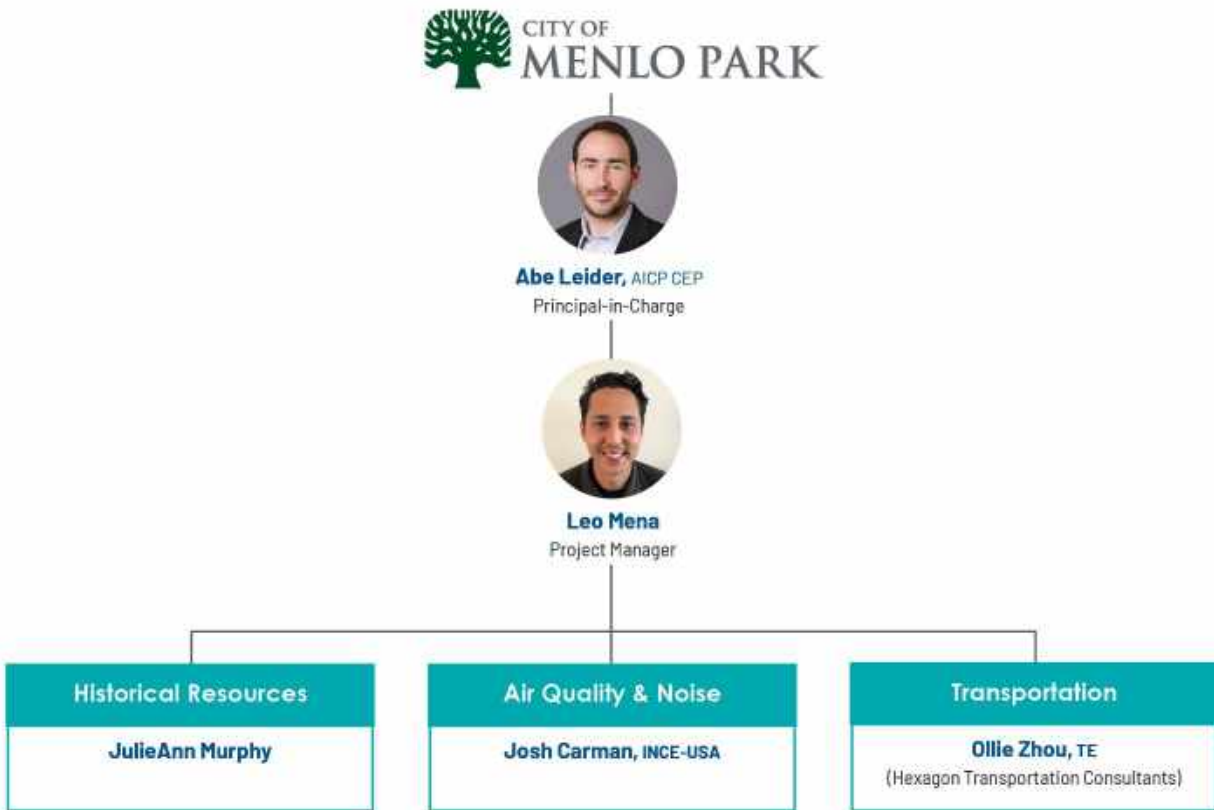


Hexagon Transportation Inc. (Hexagon) was founded in 1998 in San Jose, California with the goal of providing top-quality, professional transportation consulting services to private and public entities.

Hexagon provides services in all major aspects of transportation planning and traffic engineering. Hexagon's staff members have prepared thousands of studies, both large and small, over their professional careers. Hexagon's public clients include city, county and state agencies and regional planning organizations. Hexagon has a wide range of private clients including technology companies, developers, architects, civil engineers, and environmental firms.

A.2 Team Organization

Illustrated below is the composition of our team.



A.3 Resumes for Key Personnel

Rincon has assembled a team of highly skilled environmental science and planning professionals who combine extensive technical qualifications and knowledge of CEQA with many years of experience working in the region. Brief bios for key team members are included on the following pages, and full resumes for each team member can be found in Attachment A.

Rincon Team

Abe Leider, AICP CEP | Principal-in-Charge



Abe Leider has over 22 years of experience in long-range planning, development review, and performing environmental impact analyses for public and private infrastructure and development projects under CEQA. As manager of Rincon's Oakland office, he has overseen the preparation of Class 32 CEs, MNDs, and EIRs for institutional, residential, commercial, and industrial projects throughout California, with a focus on San Francisco Bay Area projects. Mr. Leider is experienced at providing professional planning support in all facets of the project review and permitting process to small, mid-sized, and large jurisdictions throughout California. His recent experience includes managing Rincon's on-call planning and CEQA work for numerous cities on the Peninsula and throughout the

greater Bay Area, including the cities of Palo Alto, Mountain View, Redwood City, San Jose, Millbrae, San Leandro, Hayward, and Berkeley, among others. Select experience is listed below.

- Principal-in-Charge, San Mateo County Parks - Flood County Park Landscape Plan EIR
- Principal-in-Charge, City of Palo Alto - On-Call Planning and CEQA Services - Multiple Projects
- Principal-in-Charge, City of South San Francisco - On-Call Planning and CEQA Services - Multiple Projects
- Principal-in-Charge, City of Redwood City - Mixed-Use: Live/Work and Emergency Shelter Zoning Amendment Project EIR Addendum
- Principal-in-Charge, City of Mountain View - On-Call Environmental Services - Multiple Projects

Leo Mena | Project Manager



Leo Mena is a Senior Environmental Planner and Project Manager with eight years of experience conducting CEQA and NEPA environmental evaluations. Leo is an adept analyst and writer that has prepared many Class 32 CEs, MNDs, and EIRs. Leo works on environmental documents for a diverse range of projects including, commuter rail; bike trails; roadway repairs; residential, commercial, and mixed-use development; specific plans; and energy projects. He has experience working with California state agencies and local agencies, including the San Joaquin Regional Rail Commission, California High Speed Rail Authority, the City of Menlo Park, the City of Burlingame, the City of Santa Cruz, City of Milpitas, and others.

As a Project Manager, Leo uses his skills of clear communication, organization, and attention to detail to keep projects moving forward and to identify and resolve potential issues before they become a problem on schedule or budget.

- Project Manager, City of Menlo Park - 1125 O'Brien Drive EIR
- Project Manager, City of Menlo Park - 1075 O'Brien/CSBio EIR
- Project Manager, City of Menlo Park - Commonwealth EIR
- Project Manager, City of Burlingame - 1868 Ogden Drive Project EIR
- Project Manager, City of Burlingame - Class 32 CEs for Adrian Court Mixed-Use Project, 128 Lorton Avenue Project, 601 California Drive Project, 30 Ingold Road Project, and 1766 El Camino Real

Josh Carman, INCE-USA | Noise/Air Quality



Josh Carman has over 20 years of experience in the field of acoustics and air quality and has participated in the environmental review and monitoring process for a wide variety of projects in California, Washington, Nevada, and New York. Josh prepares noise, air quality, greenhouse gas, and community health risk assessments for environmental impact studies (CEQA/NEPA) and technical studies using federal, state, and local guidelines and methodology. His experience includes complex project- and program-level analyses of general plan updates; specific plans; mixed-use development; traffic, transit and rail; vibration-sensitive; industrial; infrastructure, utilities, and telecommunications; long-term and remote construction noise and vibration monitoring; and underwater construction (e.g., pile driving) projects. He is certified in the use of the FHWA's Traffic Noise Model (TNM), SoundPLAN, and the US EPA AERMOD air dispersion model. Select experience is listed below.

- Senior Air Quality Specialist, City of Cupertino – Vallco Special Area Specific Plan EIR
- Noise Specialist, Sonoma County – Forestville Downtown Park Environmental Noise Assessment
- Senior Noise Specialist, City of Redwood City – Broadway Plaza EIR
- Senior Noise Specialist, University of California – UC Berkeley 2021 Long Range Development Plan EIR
- Senior Noise Specialist, City of Hayward – Hayward Downtown Specific Plan EIR

JulieAnn Murphy | Historic Preservation



Ms. Murphy is an Architectural Historian with over 10 years of experience. Her work includes archival research, historic resource evaluations, and review of projects for the conformance with the Secretary of the Interior's Standards for the Treatment of Historic Properties. Her professional experience includes the preparation of nominations for the National Register of Historic Places. Ms. Murphy has extensive experience preparing applications for the Federal Historic Tax Credit program as well as several state preservation incentive programs, including the Mills Act. Ms. Murphy has experience preparing documentation in support of Section 106 of the National Historic Preservation Act, NEPA, CEQA, and local ordinances. She meets requirements in the Secretary of the Interior's Professional Qualification Standards in Architectural History and History. Select experience is listed below.

- Architectural Historian, Tenderloin Neighborhood Development Corporation – 2550 Irving Street Environmental Assessment
- Architectural Historian, California Housing Finance Agency – Shiloh Terrace Apartments Project
- Architectural Historian, Town of Windsor – Estates at Ross Ranch Project
- Architectural Historian, City of Berkeley – Ashby and North BART Stations Historic Resource Evaluations
- Architectural Historian, City of Palo Alto – Private Residence

Subconsultant Team

Gary Black, AICP | Traffic/Transportation Studies | Hexagon Transportation Consultants



Since 1982, Mr. Black has directed a number of transportation planning, traffic engineering, parking, and transit studies. He has prepared transportation plans for the Cities of Menlo Park, San Jose, Palo Alto, San Mateo, and San Carlos, and areawide plans for reuse of the Bay Meadows racetrack site in San Mateo, the Cargill salt ponds site in Redwood City, and many parts of San Jose (North San Jose, Downtown, Edenvale, and Evergreen). He has prepared traffic studies for new development in most cities within the Bay Area. He also has prepared numerous parking studies, including downtown parking studies for San Carlos, San Mateo, Gilroy, and San Jose. Select experience is listed below.

- Transportation Lead, City of Menlo Park – Willow Village EIR
- Transportation Lead, Foothill College – Campus Traffic Studies
- Transportation Lead, For offices, hotels, restaurants, residential subdivisions, apartments, schools, warehouses, industrial complexes, and mixed-use developments in Menlo Park, San Jose, Santa Clara, Sunnyvale, Milpitas, Los Gatos, Fremont, Monterey, Palo Alto, Redwood City, San Carlos, San Mateo, Los Altos, Santa Rosa, Napa, Hayward, Bakersfield, Richmond, Concord, and Cupertino – Site Traffic Analyses

Ollie Zhou, TE | Traffic/Transportation Studies | Hexagon Transportation Consultants



Since January 2014, Mr. Zhou has managed a large variety of traffic engineering and transportation planning projects for both the public and private sectors throughout the greater San Francisco Bay Area. These projects mainly include travel demand model validation and application, VMT analysis, general plan updates and area plans, and traffic impact studies. Mr. Zhou is experienced in managing large-scale projects and areawide plans with prolonged schedules and complicated work scopes. Mr. Zhou mainly utilizes the CUBE software package for travel demand model applications, and manage a variety of projects conducted with Synchro, SimTraffic, Vistro, TRAFFIX software. Select

experience is listed below.

- Traffic Engineer, City of Menlo Park – Willow Village EIR
- Traffic Engineer, Menlo Park Citywide Model – Travel Demand Forecasting Model Development and Application
- Traffic Engineer, City of Menlo Park – Housing Element Update Vehicle-Miles Traveled Analysis

B. Project Understanding and Approach

B.1 Project Understanding

The 1030 O'Brien Project (project) site currently includes four parcels (980 O'Brien, 1010 O'Brien, 1020 O'Brien, and 1030 O'Brien) and the project site has a total area of 3.58 acres. As part of the project, the two buildings at 1020 O'Brien, and 1030 O'Brien would be demolished while the two buildings at 980 O'Brien and 1010 O'Brien would remain. It should be noted that all project requirements, including density, parking, and outdoor space, are calculated across the entire project site, which includes the four parcels.

The development proposal for the project contemplates a new three-story structure for research and development (R&D) uses, as permitted in the LS zoning district. The ground floor would include covered outdoor parking, including surface parking; building service and loading spaces; the main entry lobby; commercial space with public-facing uses such as food service or fitness; and landscaped outdoor space. Levels 2 and 3 would include tenant space dedicated to life science and related uses. Outdoor space in the form of elevated terraces would be located at Level 3. The roof would host mechanical equipment, screened from view, and would be unoccupied. The project would also include improving site areas around 980 O'Brien and 1010 O'Brien. In addition, the project would include streetscape improvements in front of all lots.



B.2 General Approach

Rincon's approach will be to identify the type of CEQA document that should be prepared early in the process by reviewing the proposed project in light of the CEQA Guidelines, successful City processes on prior projects, the requirements in the 2017 *City of East Palo Alto v. City of Menlo Park* settlement agreement, and the potential impacts of the project.

The settlement agreement identifies that Menlo Park will prepare an EIR for any project located in the Office (O), Life Science (LS) or Residential Mixed Use (R-MU) district that exceeds 250,000 net new square feet and would require a use permit, that proposes bonus level development, that proposes a master plan project, or that may have a significant environmental impact. The project is zoned as LS; however, the project would not exceed 250,000 square feet. Neither the RFP nor the project website identify that the project is proposing bonus level development and the project would not be a master plan project. As such, an EIR is not required pursuant to the settlement agreement and would only be required if the project were to result in a significant unavoidable impact.

There are multiple CEQA options available for the project, including the following:

- Class 32 Infill Exemption (CE)
- CEQA Document using CEQA Guidelines Section 15183, Projects Consistent with a Community Plan or Zoning
- Initial Study-Mitigated Negative Declaration
- Environmental Impact Report

Of these four options, it is Rincon's opinion that the Class 32 CE has the most advantages. This is a deviation from what was identified in the RFP; however, Rincon believes that this is the best option for both the Project Sponsor and the City. Class 32 CE documents are more efficient for eligible projects because there are less requirements. For example, public review is not necessary for Class 32 CEs, and neither is outreach through AB 52. Furthermore, Class 32 CEs provide greater legal defensibility than an IS-MND (i.e., Class 32 Infill Exemptions are not subject to the "fair argument" standard, like IS-MNDs).

Because the Class 32 CE appears to be the best CEQA option, Rincon's approach will be to first conduct studies to determine if the project qualifies for a Class 32 CE. The studies that will be conducted to determine whether a Class 32 CE are summarized under Task 1 of our Proposed Work Plan (see below). The advantage of this approach is that if it is determined that the project may not qualify for a Class 32 CE, then those same studies could still be used for a more involved CEQA document.

If it is determined that a Class 32 Infill Exemption cannot be used, this would be because the analysis indicates that there is a significant and unavoidable impact or a significant impact for which mitigation is needed for one of the topics requiring evaluation under Class 32. In addition, based on Rincon's Project Manager's experience working on nearby projects (1075 O'Brien and 1125 O'Brien) with the City, Rincon expects that an Initial Study would find that a Focused EIR or IS-MND would need to be prepared (due to new mitigation being needed that was not included in the ConnectMenlo EIR). Rincon expects that a Focused EIR or IS-MND would be needed if it is determined that a Class 32 Infill Exemption cannot be used. If this scenario happens, then Rincon can incorporate the work done in Task 1 to prepare the Focused EIR or an IS-MND.

C. Proposed Work Plan

The following proposed work plan provides a step-by-step breakdown of the tasks to be carried out. Estimates for personnel time allotted for each task are included in Attachment B.

C.1 Task 1: Identify CEQA Document

Task 1.1 Project Kick-Off Meeting and Site Visit

The project kickoff meeting will convene City staff, staff from the Project Sponsor if appropriate, and the Rincon team to introduce the project, clarify the scope of work and process, establish methods of communication, review preliminary data needs, and refine the project schedule. During the kickoff meeting, details of project schedule and deliverables will be determined. The project kickoff meeting will occur in-person or virtually, at the discretion of the City. If the project kickoff is held in-person, the kickoff meeting will be followed by a site visit. If the kickoff meeting is held virtually, Rincon will visit the site separately to observe and photo-document existing conditions and context.

Deliverables

- Kickoff Meeting Agenda, Updated Schedule, and Meeting Minutes. Please note that a preliminary schedule is shown in Attachment C of this proposal.

Assumptions

- Kickoff Meeting will be held in person, followed by a site visit. Depending on the situation related to Covid-19, the kickoff meeting can transition into a virtual meeting.

Task 1.2 Project Description and Data Needs

Rincon will prepare a Draft Project Description based on the latest Plans and documentation provided by the Project Sponsor and City and will provide an electronic copy (Microsoft Word) for review by staff prior to commencing analysis. As a basis for the environmental analysis to follow, a clear and thorough description of the project is a critical first step in the CEQA process. Textual, tabular, and graphic presentation (with city and/or planning team-prepared maps, plans, renderings, or diagrams) will be included as necessary to facilitate a thorough understanding of the proposed plan and its potential implementation. In addition, Rincon will review the existing information provided by the Project Sponsor, as well as the City Ordinances, General Plan, Zoning Code, and other CEQA documents prepared by the City. After review of those documents, Rincon will assemble a comprehensive list of data needs for the City to provide, in coordination with the Project Sponsor.

Deliverables

- Project Description (Microsoft Word)
- Data Needs (electronic format)

Assumptions

- The project schedule is based on the timely receipt of data required for the environmental analysis.

Task 1.3 Transportation Impact Analysis

Hexagon Transportation Consultants will prepare the TIA. The scope of work for the preparation of the TIA is included in Attachment D.

Deliverables

- Draft and Final TIA in electronic format.

Assumptions

- All deliverables will be provided and received electronically, no printed copies.

Task 1.4 Historical Resources Analysis

A Class 32 CE may not be used if the project would result in a substantial adverse change of a historical resource, which includes qualifying built environment and archaeological resources. Rincon therefore proposes to prepare a streamlined cultural resources study to confirm if the project would result in a substantial adverse change in the significance of a historical resource. Rincon will conduct a California Historical Resources Information System (CHRIS) search of the project site and a 0.5-mile radius surrounding it to identify previously recorded cultural resources and cultural resource studies that have previously conducted in the vicinity of the project site. Along with archival and background research, the CHRIS search will provide the basis for the cultural resources sensitivity analysis that will be presented in the study.

Upon completion of the CHRIS records search, a Rincon cultural resources specialist will survey the project site to determine if it contains qualifying historical resources. We understand the project site contains four buildings which were constructed prior to 1977 and which have not been previously evaluated for historical resources eligibility. Pursuant to guidance from the California Office of Historic Preservation, properties over 45 years may be considered for historical resources eligibility. Rincon understands however that there are no project actions proposed for the two buildings at 980 and 1010 O'Brien Drive; accordingly, historical resources evaluations will be limited to the two buildings proposed to be demolished at 1020 and 1030 O'Brien Drive. Rincon will record and evaluate both properties on Department of Parks and Recreation (DPR) 523 series forms. The evaluation will provide an architectural description of each property and present an assessment for national, state, and local historic significance. Rincon will provide preliminary findings to the City prior to finalizing the technical memorandum, which will present methods and findings and will include the DPR 523 series forms as an attachment.

Deliverables

- Draft and Final Historical Resources Technical Memorandum (electronic format).

Assumptions

- The CHRIS search will not exceed a direct expense of \$750.
- Record search requests are typically fulfilled within four weeks. If the results of the record search will have schedule or budget ramifications, it will be communicated to City staff in a timely manner to effectively maintain the project schedule and allow the City to manage project risk effectively.
- This scope does not include a Sacred Lands File Search, outreach to Native American tribal contacts or assistance with Assembly Bill 52 consultation.
- All deliverables will be provided and received electronically, no printed copies.

Task 1.5 Air Quality Report

Air Quality

The Air Quality analysis will consider both temporary construction and long-term operational emissions. The analysis will include the following tasks:

- Summarize the existing air quality conditions in the air basin using data for nearby air monitoring stations available from the California Air Resources Board and the Bay Area Air Quality Management District (BAAQMD).
- Calculate construction and operation emissions of criteria pollutants using the California Emissions Estimator Model (CalEEMod) and evaluate the significance of any air quality impacts.
- Evaluate the potential for the project to cause odor impacts to off-site uses.
- Evaluate localized air quality impacts to nearby sensitive receptors (see the *Health Risk Assessment* section below).
- Evaluate the project's compliance with applicable air quality plans.

The Air Quality Report will contain the following information: (1) an explanation of study methodology; (2) a discussion of the existing air quality conditions and regulatory framework; and (3) a summary of impacts.

Optional Task: Health Risk Assessment

According to the Office of Environmental Health Hazard Assessment, construction of individual projects lasting longer than two months or placed within 1,000 feet of sensitive receptors could potentially expose nearby sensitive receptors to substantial pollutant concentrations and therefore could result in potentially significant risk impacts. These future projects could exceed BAAQMD's thresholds of an increased cancer risk of greater than 10.0 in a million and an increased non-cancer risk of greater than 1.0 Hazard Index (Chronic or Acute). Rincon will work with the City to determine if a Health Risk Assessment (HRA) is needed, based on the project/construction information provided by the Project Sponsor. As an optional task, Rincon can prepare an HRA, if it is determined that one is needed.

If this optional task is authorized, the construction HRA will rely upon the CalEEMod run, as well as site-specific dispersion modeling, which will be prepared in compliance with the BAAQMD's Health Risk Assessment Modeling Protocol (2020) and other applicable guidance, as appropriate. The construction HRA will consider emission of diesel particulate matter and fugitive particulate matter (PM_{2.5}) associated with both on-site emissions from construction equipment and off-site hauling emissions, as appropriate. Rincon will determine average annual concentrations during the construction period at the nearest sensitive receptors.

Chronic and carcinogenic health risk calculations will be based on standardized equations from the State Office of Environmental Health Hazard Assessment Guidance (OEHHA) (2015). Rincon will use either in-house spreadsheets or the revised Hotspots Analysis and Reporting Program Version 2 (HARP 2) available from California Air Resources Board. If health risks are found to exceed BAAQMD significance criteria, Rincon will recommend project design features (such as Tier 4 construction equipment) to reduce the severity of health risk impacts to off-site receptors. Additionally, Rincon will use BAAQMD screening tools to evaluate cumulative health risk at the maximally exposed individual receptor, in accordance with BAAQMD health risk guidelines. The methodology and results of the construction HRA would be included in the Air Quality Report.

In addition, operation of the project could generate toxic air contaminant (TAC) emissions that could result in health impacts for local sensitive receptors. If needed, Rincon will conduct a screening level health risk assessment that will identify the individual screening risk associated with operation of the project facilities. The HRA will focus on chronic, carcinogenic, and non-carcinogenic risks in accordance with guidelines developed by the OEHHA and the BAAQMD. The provided site plan will be used to determine the location of stationary sources such as backup emergency generators and boilers. It is assumed that the project would

generate fewer than 100 diesel truck trips per day and fewer than 40 trucks with diesel transport refrigeration units (TRUs). No dispersion modeling is proposed to assess operational health risk impacts.

Deliverables

- Draft and Air Quality Report (electronic format).

Assumptions

- The City and/or Project Sponsor will provide site and grading plans.
- The City and/or Project Sponsor will provide construction equipment, phasing, and hauling volumes. A data request will be sent to the project applicant. Unknown inputs can be supplemented with CalEEMod defaults for a project of this type and size.
- All deliverables will be provided and received electronically, no printed copies.

Task 1.6 Noise and Vibration Analysis

The Noise and Vibration analysis will be conducted in accordance with City of Menlo Park guidelines, standards, and ordinances. The analysis will consider both temporary construction and long-term operational noise from the project. The noise and vibration analysis will include the following tasks:

- Rincon will conduct up to two short-term (15 minute) noise measurements and one long-term (24-hour+) noise measurements in the project area to characterize the existing ambient noise environment. It should be noted that the primary noise source in the project area is vehicular traffic on Willow Road.
- Rincon will quantify and address construction noise and vibration from implementation of the project, based on construction information (proposed equipment types and duration) to be provided by the Project Sponsor. If specific construction information is not available, assumptions can be developed by Rincon for a project of this type and size using the air quality model defaults.
- Rincon will address traffic noise impacts qualitatively based on the less than significant impact finding in the ConnectMenlo EIR.
- Rincon will prepare an analysis of the potential for on-site noise generators (e.g., mechanical equipment, truck loading) to exceed City standards at nearby sensitive receivers. Source noise levels for identified equipment will be obtained from equipment specification sheets provided by the Project Sponsor. If specific equipment has not been selected, Rincon can assist in the development of a list of typical equipment anticipated to be included in the project based on previous experience.
- Rincon will prepare the Noise and Vibration memorandum, which will contain the following information: (1) an explanation of study methodology; (2) a discussion of the existing noise levels and local noise criteria; and (3) a summary of noise impacts.

Deliverables

- Draft and Final Noise and Vibration Technical Memorandum (electronic format).

Assumptions

- The City and/or Project Sponsor will provide site will provide equipment specifications for proposed equipment and will assist in obtaining noise level data from manufacturers.
- The City and/or Project Sponsor will provide site access for noise monitoring.
- All deliverables will be provided and received electronically, no printed copies.

Task 1.7 CEQA Document Recommendation

When Tasks 1.1 through 1.6 are completed, Rincon's Project Manager will review the results of the analyses and provide the City with a recommendation of the type of CEQA document that should be prepared in the form of a brief memorandum. The memorandum will include a recommendation, as well as rationale for that recommendation. This task also includes time for one call with the City to discuss the recommendation being made.

Deliverables

- CEQA document recommendation (electronic)

Assumptions

- One phone call to discuss CEQA Document recommendation.

C.2 Task 2: Prepare a Class 32 Infill Exemption Document

Task 2.1 Project Description

Rincon will update the Project Description prepared under Task 1.2 if the applicant has modified the project in the interim, for example, based on information arising from the analysis in Tasks 1.3 through 1.6. Rincon will provide an electronic copy (Microsoft Word) for review by staff prior to beginning to write the documentation for the Class 32 Infill Exemption.

Deliverables

- Project Description (Microsoft Word)

Task 2.2 Draft Class 32 CE Report

Rincon will prepare the Draft Class 32 Infill Exemption Report (CE Report), which will begin with a discussion of how the project qualifies for a Class 32 Infill Exemption under CEQA and go on to address the criteria and environmental issue areas described in CEQA Guidelines Section 15332. The following information will be provided in the CE Report.

- Value as habitat for endangered, rare, or threatened species. To address criteria 'c' for the use of a categorical exemption, pursuant to CEQA Guidelines Section 15332, the analysis of biological resources will focus on the project site's potential to include habitat for endangered, rare, or threatened species. Because of the urban context of the site and its surroundings and the limited amount of vegetation on-site, it is expected that the discussion will focus on impacts related to on-site and adjacent mature trees and their potential habitat value for sensitive birds or bat species.
- Traffic, Noise, and Air Quality. To address criteria 'd' for the use of a categorical exemption, pursuant to CEQA Guidelines Section 15332, Rincon will summarize the analysis conducted under Tasks 1.3, 1.5, and 1.6.
- Water Quality. To address criteria 'd' for the use of a categorical exemption, pursuant to CEQA Guidelines Section 15332, the water quality analysis will consider both temporary hydrological changes during construction and long-term changes in hydrology due to project operation. Operational changes are expected to be minimal. The water quality analysis will focus on existing requirements that would apply to the project and would be expected to ensure that impacts remain less than significant. A quantitative drainage or water quality study is not included in this scope of work.
- Utilities and Public Services. To address criterion 'e' for the use of a categorical exemption, this analysis will confirm that the site can be adequately served by all required utilities and public services. Based on

the nature of the site in an urban developed area, it is assumed that the site can be adequately served. This analysis will be based on readily available information about public services and utilities from the City of Menlo Park and other service providers.

The report will also analyze whether the exceptions to categorical exemptions would apply (CEQA Guidelines Section 15300.2). The technical analysis will include the following:

- Cumulative impacts. A CE is inapplicable when the cumulative impact of successive projects of the same type in the same place, over time, is significant. Rincon will identify nearby projects, as well as the conclusions in the ConnectMenlo EIR and summarize their findings in the CE Report.
- Significant effects due to unusual circumstances. A CE may not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances. Rincon will consider whether there are any unusual circumstances for the project and summarize their conclusions.
- Scenic highways. A CE may not be used for a project which may result in damage to scenic resources, including but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within a highway officially designated as a state scenic highway. The project is not located near a scenic highway (the nearest designated scenic highway is Interstate-280) and no impact is expected. This information will be summarized in the CE Report.
- Hazardous Materials Sites (i.e., Cortese List). A CE may not be used for a project located on a site which is included on any list compiled pursuant to Section 65962.5 of the Government Code. Rincon will review online databases (i.e., GeoTracker, EnviroStor) to confirm that the project is not on a Cortese List site.
- Historical resources. A CE may not be used if the project would result in a substantial adverse change of a historical resource, which includes qualifying built environment and archaeological resources. To address this, Rincon will summarize the analysis conducted under Task 1.4.

Deliverables

- CE Report (MS Word Document)

Assumptions

- No significant impacts are identified throughout the analysis.

Task 2.3 Final Class 32 CE Report

Rincon will respond to one round of City comments on the Draft CE Report and prepare the Final CE Report. Rincon assumes two rounds of revisions to the report based on a consolidated set of comments from the City. This task includes completion of the State Office of Planning and Research's Notice of Exemption (NOE) form. We assume the Project Sponsor or City will be responsible for filing the Notice with the County Clerk, although Rincon can assist with this task if needed. The study will be transmitted digitally via e-mail. Rincon will provide up to 20 printed copies of the Final CE Report (with Appendices on a CD) to the City and will coordinate with the City to determine how many printed copies are needed. Additional rounds of review or printed copies may require additional budget.

Deliverables

- CE Report (MS Word Document and PDF)
- A maximum of 20 hard copies of the Final CE Report.

Assumptions

- One round of revisions.
- One consolidated set of comments from the City in MS Word, in track-changes.

Task 2.4 Public Hearing

Rincon Consultants will attend one public hearing to present the findings of the report and/or respond to questions on the environmental analysis or CEQA process.

C.3 Task 3: Project Management

Throughout the CEQA process, Rincon will proactively identify issues, immediately bring any such issues to the attention of City staff, identify potential solutions, and coordinate with City staff as to the best course of action. Rincon will use email communications and have telephone conversations with City staff, as needed, during the course of the project to ensure clear communication. Rincon will prepare monthly invoices with status reports documenting the progress made each month on completing the project, and review and update the schedule when necessary.

D. Project Cost and Schedule

Rincon Consultants will prepare the Class 32 CE Report in accordance with the above scope of work for a cost not to exceed **\$128,651**. The cost for the optional Health Risk Assessment task would be an additional \$8,808. A detailed cost estimate is provided in Attachment B. Additional work not included within our proposed work program will be completed only upon written City authorization consistent with our standard fee schedule. The project will be billed on a time and materials basis.

The Rincon team is prepared to begin the work described in this proposal immediately upon authorization to proceed. Rincon understands the importance of the project schedule for both the City, as well as the Project Sponsor. We understand that the project schedule is a tool for the whole project team to use to track project progress and identify any schedule bottlenecks. As such, Rincon has developed a project schedule using Microsoft Teams that can be automatically updated, depending on any changes that happen as the project moves forwards (i.e., delays on receiving data needs or additional review time needed). Rincon has provided estimates for the duration of certain tasks based on prior experience, but these are just estimates and will need to be updated based on the City's needs. For example, Rincon can update the review times for the City, depending on the City's needs. The schedule is preliminary, is subject to change, and should not be treated as final. Throughout the life of the project, our Project Manager will update the schedule to reflect any delays. Some of the main drivers of the schedule are as follows:

- Receipt of Data Needs from the City or Project Sponsor
- Timely receipt of a stable Project Description

As depicted in the schedule shown in Attachment C, assuming all data needs are provided in a timely manner; assuming a stable Project Description; and assuming timely review of documents prepared by Rincon staff, Rincon proposes to adhere to a schedule that allows the CEQA process for a Class 32 Infill Exemption Document to be completed between 5 and 6 months from the kickoff meeting. This schedule only applies for a Class 32 Infill Exemption. If it is determined that an EIR is needed, Rincon can prepare an updated schedule. If an EIR is needed, the work conducted under Task 1 of the Work Program can be used for the EIR. Nonetheless, EIR's typically have a timeline of 8-12 months; however, this can vary depending on multiple factors.

E. Rate Schedule

Standard Fee Schedule for Environmental Sciences and Planning Services

Professional, Technical and Support Personnel*	Hourly Rate
Principal II	\$295
Director II	\$295
Principal I	\$285
Director I	\$285
Senior Supervisor II	\$258
Supervisor I	\$240
Senior Professional II	\$224
Senior Professional I	\$208
Professional IV	\$184
Professional III	\$170
Professional II	\$153
Professional I	\$136
Associate III	\$117
Associate II	\$102
Associate I	\$95
Field Technician	\$82
Data Solutions Architect	\$175
Senior GIS Specialist	\$164
GIS/CADD Specialist II	\$146
GIS/CADD Specialist I	\$131
Technical Editor	\$130
Project Accountant	\$110
Billing Specialist	\$95
Publishing Specialist	\$105
Clerical	\$95

* Professional classifications include environmental scientists, urban planners, biologists, geologists, marine scientists, GHG verifiers, sustainability experts, cultural resources experts, and other professionals. Expert witness services consisting of depositions or in-court testimony are charged at the hourly rate of \$375.

Reimbursable Expenses

Direct Cost	Rates
Photocopies – Black and White	\$0.20 (single-sided), \$0.40 (double-sided)
Photocopies – Color	\$1.50 (single-sided), \$3.00 (double-sided)
Photocopies – 11 x 17	\$0.50 (B&W), \$3.30 (color)
Oversized Maps	\$8.00/square foot
Digital Production	\$15/CD, \$20/flash drive
Light-Duty and Passenger Vehicles*	\$88/day
4WD and Off-road Vehicles*	\$140/day

* \$0.65/mile for mileage over 50 and for all miles incurred in employee-owned vehicles.

Other direct costs associated with the execution of a project, that are not included in the hourly rates above, are billed at cost plus 15%. These may include, but are not limited to, laboratory and drilling services, subcontractor services, authorized travel expenses, permit charges and filing fees, mailings and postage, performance bonds, sample handling and shipment, rental equipment, and vehicles other than covered by the above charges.

Annual Escalation. Standard rates subject to 3.5% annual escalation.

Payment Terms. All fees will be billed to Client monthly and shall be due and payable upon receipt or as indicated in the contract provisions for the assignment. Invoices are delinquent if not paid within ten (10) days from receipt or per the contractually required payment terms.

Equipment	Day Rate
Environmental Site Assessment	
Soil Vapor Extraction Monitoring Equipment	\$155
Four Gas Monitor	\$133
Flame Ionization Detector	\$107
Photo Ionization Detector	\$80
Hand Auger Sampler	\$60
Water Level Indicator, DC Purge Pump	\$45
Natural Resources Field Equipment	
UAS Drone	\$268
Spotting or Fiberoptic Scope	\$165
Pettersson Bat Ultrasound Detector/Recording Equipment	\$165
Sound Level Metering Field Package (Anemometer, Tripod and Digital Camera)	\$110
GPS (Sub-meter Accuracy)	\$65
Infrared Sensor Digital Camera or Computer Field Equipment	\$55
Scent Station	\$22
Laser Rangefinder/Altitude	\$11
Pit-fall Traps, Spotlights, Anemometer, GPS Units, Sterilized Sample Jar	\$9
Mammal Trap, Large/Small	\$1.50/\$0.50
Water and Marine Resources Equipment	
Boat (26 ft. Radon or Similar)	\$600
Boat (20 ft. Boston Whaler or Similar)	\$335
Multi Parameter Sonde (Temp, Cond, Turbidity, DO, pH) with GPS	\$165
Water Quality Equipment (DO, pH, Turbidity, Refractometer, Temperature)	\$60
Refractometer (Salinity) or Turbidity Meter	\$37
Large Block Nets	\$110
Minnnow Trap	\$95
Net, Hand/Large Seine	\$55
Field Equipment Packages	
Standard Field Package (Digital Camera, GPS, Thermometer, Binoculars, Tablet, Safety Equipment, and Botanic Collecting Equipment)	\$110
Remote Field Package (Digital Camera, GPS, Thermometer, Binoculars, Tablet and Mifi, Delorme Satellite Beacon, 24-Hour Safety Phone)	\$140
Amphibian/Vernal Pool Field Package (Digital Camera, GPS, Thermometer, Decon Chlorine, Waders, Float Tube, Hand Net, Field Microscope)	\$165
Fisheries Equipment Package (Waders, Wetsuits, Dip Nets, Seine Nets, Bubblers, Buckets)	\$55
Underwater and Marine Sampling Gear (U/W Photo/Video Camera, Scuba Equipment (Tanks, BCD, Regulators, Wetsuits, etc.))	\$55/diver
Marine Field Package (PFDs – Personal Flotation Devices, 100-foot Reel Tapes with Stainless Carabiners, Pelican Floats, Underwater Slates, Thermometer, Refractometer, Anemometer, Various Field Guides)	\$55
Insurance, Hazard and Fees	
Historic Research Fees	\$50
L&H Dive Insurance	\$55/diver
Level C Health and Safety	\$65/person

F. Proposed Program Components

Rincon has a thorough understanding of the RFP and the full scope of this project and did not identify any critical components that were not included or stated in the RFP.

G. Other Pertinent Information

G.1 Approach to Quality Control

Since its founding in 1994, Rincon has developed a reputation for providing high-quality services and deliverables for our clients. Our quality control procedures ensure high-quality project execution, client communication, and production of deliverables within budget. The following section outlines our commitment to send all work products and deliverables to the City of Menlo Park after adhering to these procedures.

For every project Rincon undertakes we endeavor to provide our client with a high-quality product that meets or exceeds expectations, and for which all applicable professional standards and regulatory requirements are met. To meet this quality standard, Quality Assurance/Quality Control (QA/QC) procedures will be managed by Abe Leider.

We employ our published QA/QC guidelines and standard project management and quality control methods that include written project assignments, biweekly project progress meetings, project control using Microsoft Excel and Microsoft Project software, peer review of all technical sections, and principal review of all final products. The project manager and the team staff initiate the project by holding a briefing meeting whereby the work scope, expectations, costs, and timing (schedule) are determined. While each team member is responsible for the overall quality of their work effort and maintaining the deadline for their assignments, project managers are responsible for maintaining communication that the work is proceeding as envisioned on a regular basis depending on the needs of the project. Frequently this is done daily, but on no less than a weekly basis. Once the work product has been completed by the team members, it undergoes peer review by the project manager. After revisions as needed, a principal of the firm reviews the final product for quality control before it is provided to our administrative staff for production. Technical studies (e.g., biological, cultural, air quality, GHG, noise) also go through a separate technical review process before project manager and principal reviews. As part of the review, the document is checked against the prior established expectations and requirements of the work scope. Long, complex documents go through a rigorous technical edit to ensure correct grammar, style guide compliance, clear language, and consistent voice throughout. Finally, the document production team performs a comprehensive formatting check before delivery to ensure that the final product meets client expectations.

Our technical editors have extensive experience with a range of technical documents, including CEQA reports, permits, manuals, and whitepapers. With backgrounds as journalists, academics, and trainers, they also have skills in graphic design, web design, and visual arts. The Rincon graphics, technical editing, and production teams are skilled in the Microsoft Office suite of productivity software, Adobe Creative Suite, and web design software to produce visually attractive products. Additionally, in-house, high-speed copiers, printers, scanners, and binding machines allow us to produce high-quality reports and other presentation-ready products.

When a subcontractor provides information, it is initially checked by the staff person responsible for that task, and then checked by the supervisor/project manager for consistency with other aspects of the work and for compliance with the scope of work agreed to by the subcontractor. If the work is not acceptable, it is returned to the subcontractor, who is not paid for those services, until a work product that meets the scope of work is delivered. Our standard subcontracts contain language that provides us the necessary authority to reject unsuitable products.

G.2 Innovative Consulting

For each of our projects, we strive to achieve and maintain an outstanding and enduring consultant/client relationship. This starts with our goal of becoming the best consultant that each client has worked with. One of the keys to our high level of success in this area has been our development of client-specific management programs that outline the focused needs of the client and our tailored approach to meeting those needs. These programs include but are not limited to defining reporting structure and protocols, quality control procedures, systems development (billing, progress reporting, etc.) and other technical and operational elements that will be followed throughout the duration of the program.

We are also adept at developing internal databases for tracking contract status, project specifics, natural resources, geographic information system data and general performance and will apply that experience towards the implementation of tasks under this contract. As part of our internal project accounting, we were able to provide a large utility with a better accounting of their mitigation requirements and identify the need for additional endangered species coverage. Our field staff utilizes state-of-the-art PC tablets and applications for field data collection, mapping, and reporting. These tools streamline data collection and have significantly reduced cost and reliability for recording our field data. Additionally, we understand the importance of safety and accessibility. To ensure that staff and clients have access to principal-level support, we have established an after-hours call system that provides one number for important calls. The phone system relays calls to five Principals. The delivery ensures that the phone is answered or that a message is delivered to all five phones. The first principal to receive the call or message is equipped to respond or relay the needs to the appropriate personnel.

G.3 Digital Accessibility (Optional Task)

Digital accessibility requirements for public documents, including CEQA documents, apply to all California public entities pursuant to ADA Title II and California Government Code Section 7405. Rincon offers a variety of approaches and services to assist with compliance with these regulations. While our scope of work and fee estimate does not currently include digital accessibility services, we would be happy to discuss options and costs for converting our digital reports to comply with digital accessibility guidelines on request.

H. Diversity, Equity, and Inclusion

Rincon's commitment to diversity is an integral component of our culture. Our diversity efforts consist of formal and informal grassroots type programs and focuses on three areas: staff, vendors, and philanthropic efforts.

- Education: Increase the long-term diversity of our team by connecting with K-12 schools and higher education institutions that offer environmental/STEM programs within underrepresented communities.
- Subconsultants: Bring companywide awareness to increase participation in, and set targets for teaming with DBEs, including women-, minority-, disabled veteran-, and LGBT-owned business enterprises.
- Hiring: Connect with and create strong relationships with colleges and universities that foster increased equity and inclusion to recruit diverse, high-quality talent and continue to fortify Rincon as an industry leader.

H.1 Diversity within Rincon

Staff

- 60% of our team are women
- 67% of our team are under the age of 40

- 25% of our team are deemed "Generation Z"
- Our leadership team and ownership nearly match our gender profile
- We placed 1st in AEC Advisors (industry benchmarking firm) 2021 Gender Diversity Index for firms in the U.S. between \$25-\$100M
- We spend ~\$20 million annually supporting other businesses and their people (vendors/subs)

Workplace Perspectives

Rincon Consultants, Inc. believes that fostering a workplace where diversity of perspectives and opinions are encouraged and will lead to a more informed, creatively aligned, and socially aware team. It has been proven that a more diverse work environment enhances innovation, productivity, and success.

With the social and economic issues that arose in 2020, Rincon, as a firm, took action to hear from our employees on what they were feeling and what changes they wanted to see. This led to companywide discussions that allowed employees to share their ideas and take part in very real and transparent conversations. From these conversations, we developed goals and planned a path forward for each, which include the following:

- Environmental Education
- Improved Recruiting Processes
- Training and Development

Through these goals, Rincon acknowledges and commits to the fact that this is not something we are going to be able to check off our list after achieving a few milestones. This is something that is intertwined and is going to take time and diligent effort to achieve real, long-term change. To drive our internal initiatives forward, we formed staff-driven working groups to devise a path to implement the proposed actions.

As we grow as individuals and as a firm, Rincon sees diversity, equity, and inclusion as one of the strongest levers to create long-lasting and meaningful opportunities for change. We aim to continuously push one another's thinking and challenge problematic long-held beliefs and assumptions. We believe that our team will be the strongest and best positioned to realize our vision as we facilitate the following:

- **Representation** – with differences in race, religion, ethnicity, sexual identity, culture, age, gender expression, ability, economic status, education, credentials, and experience;
- **A Voice** – where all people are welcomed, heard, respected, understood, valued, connected, and able to do their best work; and
- **Fairness** – where people from all backgrounds are treated equitably and view our firm as a level playing field and platform from which to maximize their strengths and abilities.

As we seek to recruit and retain those who will learn with us, challenge us, and create with us, we realize this journey has a non-linear path that requires dedication, commitment, and perseverance. Holding each other accountable throughout our daily interactions to ensure alignment with our values and ideals will help us realize the full potential each person brings to our team and continue moving this important work forward.

H.2 How the Projects We Work on Make a Difference

The push for sustainable communities and cities is not simply a call for a greener world, but for a more equitable one that mitigates and hopefully reverses environmental injustices that have disproportionately affected people of color. As a group of engineers, scientists, and planners, we commemorate the history of June 19th and the movements that followed through our efforts for a more sustainable and equitable future.

In addition to the extraordinary impact the history of slavery has had on African Americans, the United States' more recent history of discriminatory practices also places the African American community at a significant disadvantage. Discrimination has also led to injustices for other communities of color and

indigenous communities. Rincon approaches our work with an acute awareness of these challenges and historic injustices, which is why we are involved in many projects which showcase our growing ability and understanding of how best to serve disadvantaged communities, elevate their voices during the project process, and adjust project outcomes to avoid disproportionate impacts to Black, Indigenous People of Color (BIPOC) communities.

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Attachment A

Key Personnel Resumes



Abe Leider, AICP CEP

Principle-In-Charge

Abe Leider has over 22 years of experience in long-range planning, development review, and performing environmental impact analyses for public and private infrastructure and development projects under CEQA. He has managed the preparation of MNDs and EIRs for institutional, residential, commercial, and industrial projects throughout California. Mr. Leider is experienced at providing professional contract planning support in all facets of the project review and permitting process to small, mid-sized, and large jurisdictions throughout California. His recent experience includes managing Rincon's on-call planning and CEQA work for numerous cities on the Peninsula and throughout the greater Bay Area, including the cities of Palo Alto, Mountain View, Redwood City, San Jose, Millbrae, San Leandro, Hayward and Berkeley, among others.

EDUCATION

BA, English and Environmental Studies, University of California at Santa Barbara

CERTIFICATIONS/REGISTRATIONS

Professional Certificate in Land Use and Environmental Planning, UCSB Extension

American Institute of Certified Planners, Certified Environmental Planner

AWARDS

Planning Excellence for Best Practice – Ashland Cherryland Specific Plan, County of Alameda, American Planning Association (APA) Nor Cal Chapter (shared), The Next Downtown, City of Los Angeles, APA, Los Angeles Chapter (shared)

SELECT PROJECT EXPERIENCE

Contract Planning

- Principal-In-Charge, City of Palo Alto – Multiple Projects
- Principal-In-Charge, City of Berkeley – Multiple Projects
- Principal-In-Charge, City of Los Angeles – Multiple Projects
- Principal-In-Charge, Major Projects, Cities of Ojai and Port Hueneme

On-Call Contract Management

- City of Millbrae – Environmental Services
- City of South San Francisco – On-Call Planning and CEQA Services
- City of Palo Alto – Environmental and Planning Services
- City of Mountain View – Environmental Services
- Alameda-Contra Costa Transit District - Environmental Services
- City of Berkeley – Environmental and Planning Services
- City of Oakland DOT – On-Call Environmental Consulting Services
- City of San Leandro – Environmental and Planning Services
- City of Hayward – Environmental Services
- City of Concord – Environmental and Planning Services
- Napa Valley Transportation Authority – Environmental Services

CEQA and NEPA Compliance

- Mixed-Use: Live/Work and Emergency Shelter Zoning Amendment Project EIR Addendum, City of Redwood City
- Housing Element Update EIR, City of Belmont
- CEQA Templates and CEQA Procedures Manual, City of Palo Alto
- 1700 Embarcadero Auto Dealership IS-MND, City of Palo Alto
- 2747 and 3045 Park Boulevard Office Project IS-MND, City of Palo Alto
- 3225 El Camino Real Mixed-Use Project IS-MND, City of Palo Alto
- BayFair BART TOD Specific Plan EIR, City of San Leandro



- Ashland-Cherryland Business District Specific Plan EIR, County of Alameda
- Adeline Corridor Specific Plan EIR, City of Berkeley
- 2211 Harold Way Mixed Use Project EIR, City of Berkeley
- 2190 Shattuck Avenue Mixed Use Project EIR, City of Berkeley
- Cal Berkeley Aquatics Center SEIR, University of California, Berkeley
- Upper Hearst Development SEIR, University of California, Berkeley
- Fairview Area Specific Plan MND, County of Alameda
- Housing Incentive Program Expansion and 788-796 San Antonio Road Mixed Use Project EIR, City of Palo Alto

Land Development

- Four Affordable Housing Project EA-FONSIs, City of San Francisco MOH
- 7701 Redwood Boulevard Hotel IS-MND, City of Novato
- 375 West El Pintado Road Residential Development EIR, Town of Danville
- Alvarado Commerce Center IS-MND, City of San Leandro
- ACI MRF and Transfer Facility Expansion Project IS-MND, City of San Leandro





Leo Mena

Senior Planner

Leo Mena is an Environmental Planner and Project Manager with eight years of experience conducting environmental evaluations consistent with the California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA). Leo is an adept analyst and writer that has prepared environmental impact reports (EIRs), initial studies (IS), Class 32 Infill Exemption Documents, and permits for the California Regional Water Quality Control Boards (RWQCB), U.S. Army Corps of Engineers (USACE), and California Department of Fish and Wildlife (CDFW). Leo works on environmental documents for a diverse range of projects including, commuter rail; bike trails; roadway repairs; residential, commercial, and mixed-use development; specific plans; and energy projects. He has experience working with California state agencies and local agencies, including the San Joaquin Regional Rail Commission, California High Speed Rail Authority, the City of Burlingame, the City of Santa Cruz, City of Milpitas, and others.

Leo is also well versed in the latest streamlining CEQA options available for residential, commercial, and mixed-use development projects. He works closely with his clients to ensure that all available streamlining options have been considered prior to any work beginning. Leo helped develop eCEQA, which is an online, user-friendly tool that developers and agencies can use to figure out which streamlining options may be available for their projects.

EDUCATION

BA, Ecology and Evolutionary Biology, Princeton University

SELECT PROJECT EXPERIENCE

Project Manager, Commonwealth Building 3 EIR—City of Menlo Park, Menlo Park

Leo Mena served as the Project Manager for the Commonwealth Building 3 Project, which proposes to construct a new office building in Menlo Park. Leo worked closely with technical authors and the City to prepare the Screen Check Draft EIR and the Public Draft EIR. Leo helped the City address several updates in CEQA, including updated thresholds from BAAQMD and new case law for construction noise impacts. Through Leo's leadership the City released the Draft EIR in July 2022.

Project Manager, 1075'O Brien Drive EIR and 1125 O'Brien Drive EIR—City of Menlo Park, Menlo Park

Leo Mena served as the Project Manager for two projects in the City of Menlo Park (1075'O Brien Drive and 1125 O'Brien Drive), both of which proposed to install new R&D uses. Leo worked closely with the City of Menlo Park to prepare the Administrative Draft EIR. In addition, Leo coordinated with City staff to develop approaches for the analysis of Greenhouse Gas Emissions and Noise, as a result of updated guidelines and case law.

Project Manager, City of Burlingame – 1868 Ogden Drive Project EIR, Burlingame

Leo served as the project manager for this residential project and successfully managed a team of technical specialists to complete a Draft EIR and Final EIR for this Project. As the Project Manager, Leo was able to successfully pivot from the preparation of an Infill Exemption to an EIR, when it was discovered that the building to be demolished was historic in nature. With Leo's leadership the CEQA process was successfully completed for this Project.



Project Manager, City of Milpitas – Milpitas Metro Specific Plan SEIR, Milpitas

Leo served as the Project Manager for the Milpitas Metro Specific Plan. Leo managed a team of technical experts and prepared the Draft Subsequent EIR for this Project, which entails an update to the Specific Plan to allow for the development of additional growth around the recently completed Milpitas BART Station. Leo worked closely with the City and the authors of the Specific Plan to identify policies and mitigation measures to streamline future projects that will be developed in this area. Leo was instrumental in getting the Draft EIR released to the public in April 2022.

Project Manager, City of Burlingame – Adrian Court Mixed-Use Project Infill Exemption, Burlingame

Leo served as the project manager for this mixed-use development project and successfully managed a team of technical specialists to complete a Class 32 Infill Exemption document. With Leo's management, this Project was approved by the City of Burlingame Planning Commission within four months of the kickoff.

Project Manager, City of Burlingame – 128 Lorton Avenue Project, 601 California Drive Project, 30 Ingold Road Project Infill Exemptions, 1766 El Camino Real, Burlingame

Leo served as the Project Manager for the 128 Lorton Avenue Project, 601 California Drive Project, 30 Ingold Road Project, and 1766 El Camino Real Project. Leo worked closely with the City to prepare Class 32 Infill Exemption documents for these four projects. Leo managed a team of technical experts and provided senior review for the documentation of the technical analysis. Through Leo's leadership, each of the four projects was approved by the City of Burlingame Planning Commission within a year of the kickoff meeting.

Deputy Project Manager, City of Burlingame – 1499 Bayshore Project IS/MND, Burlingame

Leo Mena served as the Deputy Project Manager for the Project, which proposed the construction of a 13-story hotel, restaurant, and parking garage in the City of Burlingame. Leo worked closely with the City to prepare an IS/MND for the Project. Leo prepared several sections for the IS/MND, including the Biological Resources, Hydrology and Water Quality, Population and Housing, Public Services and Recreation, Utilities and Service Systems Sections. Leo was instrumental in getting this Project through the CEQA process. This Project was approved by the City of Burlingame Planning Commission in September 2019.

Project Manager, ACE Extension Lathrop to Ceres/Merced EIR – San Joaquin Regional Rail Commission, Lathrop to Ceres/Merced

Leo was instrumental in the preparation of the Draft and Final EIR for the ACE Extension Lathrop to Ceres/Merced Project, which proposes to expand commuter train service to Ceres and Merced. Leo worked on both phases of the Project. Phase I included extension of service from Lathrop to Ceres and Phase II included extension of service from Ceres to Merced. Phase II included a station in Merced that would connect to the California High Speed Rail System. Leo oversaw the preparation of the EIR, including coordination with technical specialists and the San Joaquin Regional Rail Commission and was instrumental in the completion of the CEQA process for Phase I and II of this Project. Phase I of the Project was approved by the Board of the San Joaquin Regional Rail Commission in August 2018 and Phase II was approved in December 2021.

Project Manager, Valley Link EIR – Tri-Valley San Joaquin Valley Regional Rail Authority, Dublin/Pleasanton to Lathrop

Leo served as the Project Manager for the Valley Link Project, which proposes to add passenger train service between Dublin/Pleasanton and Lathrop and would connect the existing BART system at Dublin/Pleasanton with the existing ACE service in Lathrop. As the Project Manager, Leo managed a group of technical specialists and authors to prepare the EIR. Leo worked closely with the engineering firm (AECOM) and the Tri-Valley San Joaquin Valley Regional Rail Authority to identify the best solutions for problems that arose and kept the Project moving forward day-to-day. Through Leo's leadership, the EIR for Valley Link was certified in May 2021.

Environmental Planner, Gateway Park EIR – Bay Area Toll Agency, Oakland

The project involves the development of up to 45-acres of parkland along the waterfront near the eastern end of the east span of the San Francisco–Oakland Bay Bridge (Bay Bridge) in Oakland, California. Leo coordinated the preparation of the Draft EIR and reviewed, edited, and revised the EIR.





Josh Carman, INCE-USA

Director – Noise, Air Quality & GHG

Josh Carman has over 20 years of experience in the field of acoustics and air quality and has participated in the environmental review and monitoring process for a wide variety of projects in California, Washington, Nevada, and New York. Josh prepares noise, air quality, greenhouse gas, and community health risk assessments for environmental impact studies (CEQA/NEPA) and technical studies using federal, state, and local guidelines and methodology. His experience includes complex project- and program-level analyses of general plan updates; specific plans; mixed-use development; traffic, transit and rail; vibration-sensitive; industrial; infrastructure, utilities, and telecommunications; long-term and remote construction noise and vibration monitoring; and underwater construction (e.g., pile driving) projects. He is certified in the use of the FHWA's Traffic Noise Model (TNM), SoundPLAN, and the US EPA AERMOD air dispersion model.

Josh's project experience includes conducting the noise modeling, analysis and preparation of the noise assessment for the Prologis warehouse project in Los Angeles, the noise study report for the Mid-County Parkway EIR/EIS, a 16-mile transportation corridor in Riverside County; the air quality and GHG study for the Vallco Specific Plan in Cupertino; noise and vibration measurements for the VTA Silicon Valley Rapid Transit Extension; long-term construction noise and vibration monitoring for the EBMUD Summit Reservoir Replacement Project; and the Title 24 acoustical study for the Lincoln at Euclid residential development in Anaheim. Josh has provided third-party review on a variety of different projects over the years.

EDUCATION

BA, Environmental Studies,
University of California,
Santa Cruz

CERTIFICATIONS/ REGISTRATIONS

SoundPLAN (2018), Navcon
Engineering, Fullerton,
California

US EPA AERMOD (2015), Lakes
Environmental, Las Vegas,
Nevada

FHWA – Traffic Noise Model
2.5 (2011), Bowlby &
Associates, Inc., Brentwood,
Tennessee

Noise Control for Buildings,
Manufacturing Plants,
Equipment and Products
(2008), Hoover & Keith,
Houston, Texas

SELECT PROJECT EXPERIENCE

Senior Air Quality Specialist, City of Cupertino – Vallco Special Area Specific Plan EIR, Cupertino

While at Illingworth & Rodkin, Josh prepared the Air Quality and GHG technical report for the proposed specific plan. The Vallco Special Area Specific Plan comprises approximately 70 acres, approximately 58 acres of which is currently available for development. The developable area consists of multiple parcels and is located on both sides of North Wolfe Road – between Vallco Parkway and Interstate 280 (I-280) on the east side of North Wolfe Road and between Stevens Creek Boulevard and Vallco Parkway on the west side of North Wolfe Road. The Specific Plan would facilitate development of a minimum of 600,000 square feet of commercial uses, up to 2.0 million square feet of office uses, up to 339 hotel rooms, and up to 800 residential dwelling units on-site. In addition, up to 65,000 square feet of civic spaces in the form of governmental office space, meeting rooms and community rooms and a Science Technology Engineering and Mathematics (STEM) lab was proposed, as well as a 30-acre green roof.

Senior Noise Specialist, University of California – UC Berkeley 2021 Long Range Development Plan EIR, Berkeley

Josh conducted the noise and vibration assessment for the campus long range development plan (LRDP) while at PlaceWorks. The purpose of an LRDP is to provide adequate planning capacity for potential population growth and physical infrastructure that may be needed to support future population levels on each UC campus. In addition to the programmatic assessment, the EIR included the evaluation



of two project-level housing sites. Both programmatic and project-level mitigation measures were developed, including a screening procedure to evaluate construction vibration significance on future projects under the LRDP.

Senior Noise Specialist, City of Redwood City – Broadway Plaza EIR, Redwood City

The project consists of redeveloping the Redwood Plaza Shopping Center into a new mixed-use development that would include residential, commercial, and retail uses. The project site consists of 2 sites, which together comprise 15.3 acres. While at PlaceWorks, Josh conducted the noise and vibration assessment and recommended mitigation measures to reduce noise impacts.

Senior Noise Specialist, County of Los Angeles – Tapia Ranch/Project No. R2012-02667/Vesting Tentative Tract Map No. 072126, Los Angeles County

Josh conducted the environmental noise assessment while at PlaceWorks in support of an EIR for the development of 405-unit single-family residential lots with one public park (4.7 net acres), eight open space lots, one water pump lot, 63 HOA/Basin lots, one park trails lot, nine flood control district lots, 21 private and future street lots, and one private street lot in the Castaic area. Cumulative impacts considered the planned and approved Tesoro Del Valle residential development to the southeast.

Senior Noise Specialist, City of Santa Ana – Santa Ana General Plan Update PEIR, Santa Ana

Josh conducted the environmental noise assessment while at PlaceWorks in support of a PEIR for the City's General Plan Update. As part of this work, Josh worked with GIS staff to generate existing and future transportation noise contours, which included traffic and railroad noise in the City.

Senior Noise Specialist, City of Davis – Downtown Davis Specific Plan EIR, Davis

Josh conducted the environmental noise assessment while at PlaceWorks in support of an EIR for the City's Downtown Specific Plan where railroad noise was a community concern. As part of this work, Josh conducted short- and long-term noise measurements of freight train, commuter train, and switcher car pass-by events to document existing conditions. New specific plan policies were recommended to reduce community noise exposure to railroad noise.

Senior Air Quality Specialist, City of Modesto – Modesto General Plan Update EIR, Modesto

While at Illingworth & Rodkin, Josh prepared the Air Quality and GHG technical report for the City of Modesto General Plan Update. The report calculated air quality and GHG emissions in the Plan Area and region and analyzed potential air quality and GHG impacts associated with implementation of the Plan. In addition, a consistency analysis of Air District goals and policies was completed. Community health risk was evaluated including a detailed analysis of railroad emissions. Mitigation measures to reduce potentially significant air quality, community risk, and odor impacts were recommended.

Senior Noise Specialist, City of Hayward – Hayward Downtown Specific Plan EIR, Hayward

While at PlaceWorks, Josh prepared the noise and vibration assessment for the Hayward Downtown Specific Plan EIR. The proposed project would update the land use designations and zoning for the parcels in the 320-acre Specific Plan Area also referred to as the project site. Work included ambient noise measurements including BART rapid transit pass-bys, as well as evaluating program-level construction and operational noise and vibration impacts. Mitigation measures were identified to reduce potentially significant noise impacts.

Noise Analyst, California High-Speed Rail Authority – California High-Speed Rail Program EIR, Various Counties/Cities California

In August 2005, the California High Speed Rail Authority certified its Program FEIR/FEIS, which evaluated environmental impacts along a proposed 700 miles of rail corridor from Sacramento and San Francisco to San Diego. While at Wilson Ihrig, Josh assisted with a screening analysis to quantify the number of people potentially affected by wayside noise and vibration in the adjacent communities along the LOSSAN Corridor from Los Angeles to San Diego.





JulieAnn Murphy

Architectural Historian Project Manager

JulieAnn Murphy is an Architectural Historian with over 10 years of experience. Her work includes archival research, historic resource evaluations, and review of projects for the conformance with the Secretary of the Interior's Standards for the Treatment of Historic Properties. Her professional experience includes the preparation of nominations for the National Register of Historic Places. Ms. Murphy has extensive experience preparing applications for the Federal Historic Tax Credit program as well as several state preservation incentive programs, including the Mills Act. Ms. Murphy has experience preparing documentation in support of Section 106 of the National Historic Preservation Act, NEPA, CEQA, and local ordinances. She meets requirements in the Secretary of the Interior's Professional Qualification Standards in Architectural History and History.

EDUCATION

MS, Historic Preservation,
concentration in Preservation
Planning, University of
Pennsylvania

BA, History and Sociology,
University of California,
Riverside

CERTIFICATIONS/ REGISTRATIONS

Meets and exceeds Secretary
of the Interior's Professional
Qualification Standards in
Architectural History and
History

SELECT PROJECT EXPERIENCE

Architectural Historian, Town of Windsor – Estates at Ross Ranch Project, Windsor

As the Senior Architectural Historian, Ms. Murphy prepared an updated historical resource evaluation in support of California Environmental Quality Act (CEQA) compliance. The proposed project, the subdivision of a vacant parcel for development of single-family houses, was previously evaluated for historical resources. Following a peer review of the previous evaluation, Ms. Murphy reviewed the record search results from the California Historical Resources Information System (CHRIS), provided oversight for the site visit and field documentation of the built resources, performed supplemental archival and historical research, and evaluated the property for listing in the National Register of Historic Places (NRHP) and California Register of Historical Resources (CRHR). The assessment affirmed the findings of the previously performed evaluation that the property was not eligible for, listing in the NRHP or CRHR and was not a historical resource for the purposes of CEQA.

Architectural Historian, Tenderloin Neighborhood Development Corporation – 2550 Irving Street Environmental Assessment, San Francisco

Ms. Murphy served as the Senior Architectural Historian for the 2550 Irving Street Environmental Assessment and evaluated 16 historic-era commercial and residential structures for Section 106 of the National Historic Preservation Act compliance. The evaluations were completed in support of the preparation of the Environmental Assessment in accordance with NEPA and HUD requirements for the construction of a multi-family affordable housing development.

Architectural Historian, City of San Leandro – 2824 Halcyon Drive, San Leandro

As the Senior Architectural Historian, Ms. Murphy prepared a historic resource evaluation (HRE) of the proposed project property to identify potential historical resources in support of an Infill Checklist in compliance with the California Environmental Quality Act (CEQA). The proposed project included the demolition of four existing residential and accessory dwellings for the construction of 18 new single-family residences. The HRE included a site visit of the property to document its condition and any character-defining features, archival research, and evaluation of the property for the National Register of Historic Places (NRHP), the California

Register of Historical Resources (CRHR), and a City of San Leandro Landmark or Merit Resource. The property, found ineligible for listing, was also recorded on California DPR 523 Series forms.

Architectural Historian, RRM Design Group – Coast Rail Trail Segments 8 and 9, Santa Cruz

Rincon is currently supporting the preparation of environmental compliance documentation for the implementation of the Coastal Rail Trail Segments 8 and 9. Located along the Santa Cruz Branch Line Railroad, the former rail line is proposed to be rehabilitated for a recreation trail. Rincon is preparing Caltrans-format resource documentation in accordance with NEPA and Section 106. Ms. Murphy, as Senior Architectural Historian, is leading the preparation of the Historical Resources Evaluation Report (HRER) to determine if historic properties exist in the Area of Potential Effect (APE). As part of the effort, Ms. Murphy conducted a survey of the proposed project APE to identify and document potential historic properties impacted by the proposed project. Following the field survey, Ms. Murphy performed archival research to ascertain the age, alterations, and significance of each architectural resource. Potential historical significance was considered in accordance with the Caltrans Programmatic Agreement, which provides guidance on evaluating cultural resources. Properties were formally recorded on individual California DPR series 523 forms, and were evaluated for listing in the NRHP, CRHR, and for local listing. Results are being compiled into a short-format Caltrans Historic Property Survey Report (HPSR).

Architectural Historian, City of Berkeley – Ashby and North BART Stations Historic Resource Evaluations, Berkeley

Ms. Murphy served as the Project Manager and managed the day-to-day preparation of the Historical Resources Evaluations for the Ashby BART Station and the North Berkeley BART Station. The evaluations were completed in support of the California Environmental Quality Act (CEQA) compliance for the Ashby and North Berkeley BART Stations Transit Oriented Development (TOD) Zoning Project. The evaluations addressed the impacts to potential historical resources under CEQA. She performed field work, archival research, and outreach to gather important background information to inform the evaluations. She also researched trends and emerging best practices in the recordation and evaluation of cultural resources in the field to ensure that the provided analysis was as thorough as possible. Additionally, she provided oversight to the junior staff, including tracking the project budget and schedule to ensure that the final product was completed and delivered to the client on time and within budget.

Architectural Historian, City of Albany – 540 San Pablo Avenue, Albany

Ms. Murphy, as the Senior Architectural Historian, prepared a Cultural Resources Analysis technical report in support of a categorical exemption (CE) for the proposed project. The proposed project, located in Albany, included the demolition of several buildings for the construction of a five-story mixed use building with 207 residential units and four commercial spaces. Ms. Murphy performed a California Historical Resources Information Systems (CHRIS) search, conducted archival and background research, and provided oversight for a pedestrian survey to document the site. All of the site's age-eligible resources were evaluated for potential listing in the National Register of Historic Places (NRHP) and the California Register of Historical Resources (CRHR). Ms. Murphy also coordinated with the Senior Archaeologist to assess the project effects on a potentially sensitive archaeological resource on an adjacent property and developed a proposed monitoring program to implement during project construction.

Architectural Historian, City of Vallejo – Historic Preservation White Paper, Vallejo

Ms. Murphy prepared a Historic Preservation White Paper to provide recommendations to the City of Vallejo's Planning and Development Services Department. Funded by a grant from the California State Office of Historic Preservation, the Historic Preservation White Paper included an audit of the City of Vallejo's existing preservation policies, programs, and resources. The final paper included an evaluation the City's current policies and programs, an overview of the best historic preservation policies and practices, and provided a suite of options for the City's future preservation efforts.



Attachment B

Project Cost Estimate



RINCON CONSULTANTS, INC.
 22-13814 Menlo Park - 1030 O'Brien Drive
 Cost Estimate

	Rate	Hours	Labor Budget	Direct Expenses	Total Budget
Task 1: Identify CEQA Document		216.50	38,488.50	887.50	39,376.00
Task 1.1: Project Kickoff and Site Visit		8.00	2,036.00	0.00	2,036.00
Principal I	285.00	4.00	1,140.00		
Senior Planner II	224.00	4.00	896.00		
Task 1.2: Project Description and Data Needs		15.00	2,795.00	0.00	2,795.00
Principal I	285.00	2.00	570.00		
Senior Planner II	224.00	4.00	896.00		
Planner II	153.00	8.00	1,224.00		
Production Specialist I	105.00	1.00	105.00		
Task 1.3: Transportation Impact Analysis		10.00	2,362.00	0.00	2,362.00
Principal I	285.00	2.00	570.00		
Senior Planner II	224.00	8.00	1,792.00		
Task 1.4: Historical Resources Analysis		49.00	8,253.00	887.50	9,140.50
Principal II	295.00	1.00	295.00		
Supervisor Archaeologist I	240.00	2.00	480.00		
Senior Archaeologist I	208.00	10.00	2,080.00		
Archaeologist II	153.00	32.00	4,896.00		
GIS/CADD Specialist II	146.00	2.00	292.00		
Production Specialist I	105.00	2.00	210.00		
Record Search				862.50	
Travel - Mileage				25.00	
Task 1.5: Air Quality Analysis		54.00	9,424.00	0.00	9,424.00
Director I	285.00	2.00	570.00		
Senior Planner II	224.00	14.00	3,136.00		
Planner II	153.00	36.00	5,508.00		
Production Specialist I	105.00	2.00	210.00		
Task 1.6: Noise Analysis		67.50	11,694.50	0.00	11,694.50
Director I	285.00	0.50	142.50		
Senior Planner II	224.00	19.00	4,256.00		
Planner II	153.00	47.00	7,191.00		
Production Specialist I	105.00	1.00	105.00		
Task 1.7: CEQA Document Recommendation		3.00	614.00	0.00	614.00
Principal I	285.00	1.00	285.00		
Senior Planner II	224.00	1.00	224.00		
Production Specialist I	105.00	1.00	105.00		



RINCON CONSULTANTS, INC.
 22-13814 Menlo Park - 1030 O'Brien Drive
 Cost Estimate

	Rate	Hours	Labor Budget	Direct Expenses	Total Budget
Task 1.8: GIS		10.00	1,310.00	0.00	1,310.00
GIS/CADD Specialist I	131.00	10.00	1,310.00		
Task 2: Prepare Class 32 Infill Exemption Document		80.00	14,012.00	0.00	14,012.00
Task 2.1: Project Description		10.00	1,756.00	0.00	1,756.00
Principal I	285.00	1.00	285.00		
Senior Planner II	224.00	2.00	448.00		
Planner II	153.00	6.00	918.00		
Production Specialist I	105.00	1.00	105.00		
Task 2.2: Draft Class 32 CE Document		50.00	8,818.00	0.00	8,818.00
Principal I	285.00	6.00	1,710.00		
Senior Planner II	224.00	8.00	1,792.00		
Planner II	153.00	32.00	4,896.00		
Production Specialist I	105.00	4.00	420.00		
Task 2.3: Final Class 32 CE Report		10.00	1,756.00	0.00	1,756.00
Principal I	285.00	1.00	285.00		
Senior Planner II	224.00	2.00	448.00		
Planner II	153.00	6.00	918.00		
Production Specialist I	105.00	1.00	105.00		
Task 2.4: Public Hearing		4.00	896.00	0.00	896.00
Senior Planner II	224.00	4.00	896.00		
Task 2.5: GIS		6.00	786.00	0.00	786.00
GIS/CADD Specialist I	131.00	6.00	786.00		
Task 3: Project Management		15.00	2,453.00	0.00	2,453.00
Principal I	285.00	1.00	285.00		
Senior Planner II	224.00	4.00	896.00		
Planner II	153.00	4.00	612.00		
Project Accountant	110.00	6.00	660.00		



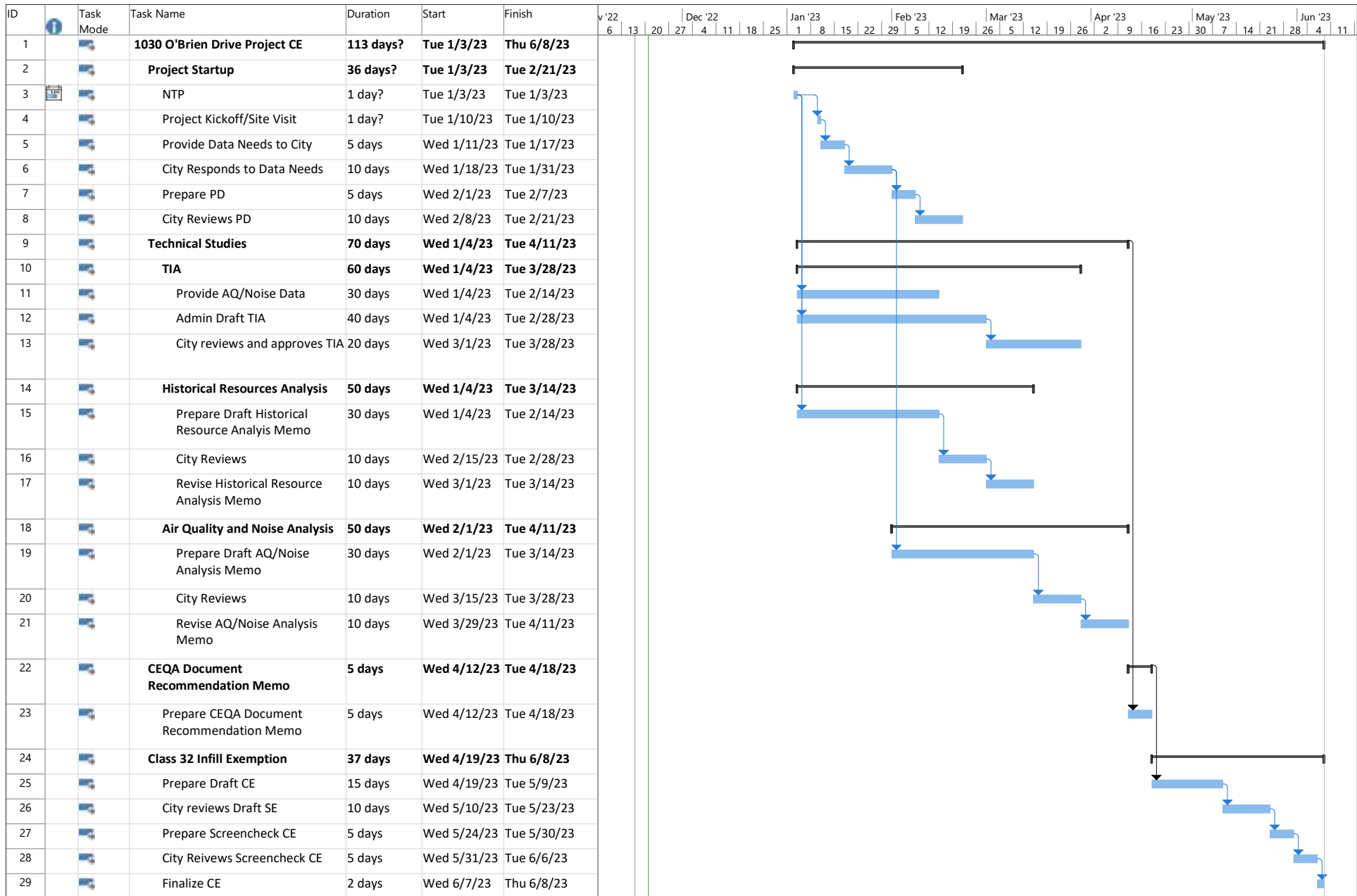
RINCON CONSULTANTS, INC.
 22-13814 Menlo Park - 1030 O'Brien Drive
 Cost Estimate

	Rate	Hours	Labor Budget	Direct Expenses	Total Budget
Expenses		0.00	0.00	72,810.00	72,810.00
		0.00	0.00	72,810.00	72,810.00
Postage, Shipping, Delivery				115.00	
Printing & Production				1,000.00	
Sound Level Field Monitoring Package				220.00	
Travel - Mileage				175.00	
Transportation Planning or Engineering				71,300.00	
Project Total		311.50	54,953.50	73,697.50	128,651.00

Direct Expenses Summary	Amount
Postage, Shipping, Delivery	115.00
Printing & Production	1,000.00
Record Search	862.50
Sound Level Field Monitoring Package	220.00
Travel - Mileage	200.00
Transportation Planning or Engineering	71,300.00
Direct Expenses Subtotal	73,697.50

Attachment C

Preliminary Project Schedule



Project: 1030 O'Brien Schedule Date: Mon 11/21/22	Task		Project Summary		Manual Task		Start-only		Deadline	
	Split		Inactive Task		Duration-only		Finish-only		Progress	
	Milestone		Inactive Milestone		Manual Summary Rollup		External Tasks		Manual Progress	
	Summary		Inactive Summary		Manual Summary		External Milestone			

Attachment D

Hexagon Scope of Work



November 21, 2022

Mr. Leo Mena
Rincon Consultants, Inc.
1530 Monterey Street, Suite D
San Luis Obispo, CA 93401

Re: *Proposal to Prepare a Transportation Impact Analysis for the Proposed R&D Development at 1030 O'Brien Drive in Menlo Park, California*

Dear Mr. Mena:

Hexagon Transportation Consultants, Inc. is pleased to submit this proposal to prepare a Transportation Impact Analysis for the proposed research and development (R&D) project at 1030 O'Brien Drive in Menlo Park, California. The project would combine the 1020 O'Brien Dr and 1030 O'Brien Drive parcels, demolish the existing buildings on site, and build 85,886 square feet of office/R&D space and 5,822 square feet of commercial space. It is unclear whether existing buildings or occupied. Vehicular access to the project site would be provided by driveways on O'Brien Drive.

Scope of Services

The purpose of the transportation study is to satisfy the California Environmental Quality Act (CEQA) requirements for an Initial Study, and to satisfy the non-CEQA operational analysis requirements for the City of Menlo Park. The CEQA analysis will include an analysis on the project's bicycle, pedestrian and transit impacts, VMT impact, emergency access impact, and potential hazards impacts. The non-CEQA operational analysis will include an analysis of weekday AM and PM peak-hour traffic conditions on key intersections in the project vicinity, daily roadway traffic analysis, and other transportation issues (queuing, site plan review, parking).

The City/County Association of Governments (C/CAG)'s Congestion Management Program (CMP) has additional non-CEQA operational analysis requirements for projects generating over 100 net peak hour trips. This scope assumes that the project's trip generation would receive trip credits for the existing buildings on-site to be demolished. This would lower the project's net peak hour trip generations below the C/CAG thresholds, and would not require the CMP-required freeway or ramp analysis.

The intersections we propose to study are identified below.

Study Intersections:

1. Willow Road and Bayfront Expressway
2. Willow Road and Hamilton Avenue
3. Willow Road and Ivy Drive
4. Willow Road (SR 114) and O'Brien Drive
5. Willow Road and Newbridge Street
6. Willow Road and US 101 NB Off-ramp
7. Willow Road and SB 101 Off-ramp
8. O'Brien Drive and Kavanaugh Drive (unsignalized)
9. University Avenue and Bayfront Expressway



In addition, the project's effect on neighborhood traffic conditions will be evaluated on the street segments identified below.

Street Segments:

1. O'Brien Drive between Willow Road and Kavanaugh Drive, and
2. O'Brien Drive between University Avenue and Kavanaugh Drive

CEQA Analysis

1. ***Bicycle, Pedestrian, and Transit Facilities.*** A qualitative analysis of the project's impact on transit service in the area and on bicycle and pedestrian circulation in the study area will be included in the traffic report. Any impacts of the project on the nearby facilities will be identified and improvements recommended to mitigate the impacts.
2. ***VMT Analysis.*** The vehicle miles travelled (VMT) associated with the proposed project will be evaluated using the citywide travel demand model. The project is likely to have a VMT impact. It is assumed that the project applicant will provide a Transportation Demand Management (TDM) Plan. Hexagon's TDM Plan peer review findings (Task 3) will inform whether or not additional VMT mitigation measures are necessary. If so, Hexagon will work with City staff to determine the additional mitigation measures necessary to fully mitigate the project VMT impact.
3. ***Peer Review of TDM Plan.*** It is assumed that the project applicant will provide a TDM plan. Hexagon will peer review the TDM Plan and determine a) whether the TDM Plan satisfies the City's TDM ordinance, b) whether the TDM Plan is sufficient to eliminate the project's VMT impact, and c) whether the TDM Plan satisfies the City/County Association of Governments' TDM Checklist requirements. Hexagon will summarize our findings in a separate memorandum.
4. ***Project Impact on Hazards and Emergency Vehicle Access.*** A qualitative analysis of the project's impact on hazards due to a geometric design feature or incompatible use, as well as the project's impact on emergency vehicle access will be conducted. Findings will also be informed by Task 15 below (site access and on-site circulation).
5. ***Provision of Data to Air Quality and Noise Consultants.*** It is anticipated that the Air Quality and Noise consultants will need traffic and VMT data for their analysis. This task includes Hexagon staff time to coordinate with the consultants and the data needs, formats, and to provide the requested data.

Non-CEQA Analysis

6. ***Site Reconnaissance.*** The physical characteristics of the site and the surrounding roadway network will be reviewed to identify existing roadway cross-sections, intersection lane configurations, traffic control devices, and surrounding land uses.
7. ***Observation of Existing Traffic Conditions in the Study Area.*** Field observations of existing traffic conditions will be limited to field-verifying signal timing at signalized intersections during peak hours. Due to COVID conditions, field observations of intersection-level operations issues are not included in this proposal.



8. **Data Collection.** Existing weekday AM (7:00 – 9:00 AM) and PM (4:00 – 6:00 PM) peak-hour traffic volumes at the study intersections and existing average daily traffic volumes for the study roadway segments will be obtained from the City of Menlo Park and previous studies with counts conducted in year 2019 or 2020. This task does not include collection of new turning movement counts.
9. **Evaluation of Existing Conditions.** Existing traffic conditions will be evaluated based on existing traffic volumes at the study intersections. The existing traffic conditions at the study intersections within the City of Menlo Park will be evaluated using the software VISTRO, which employs the *Highway Capacity Manual* (HCM) 6th Edition methodology for intersection analyses.
10. **Evaluation of Background Conditions.** Background traffic volumes represent the existing volumes plus the anticipated volumes from approved developments that have not yet been constructed and occupied. Hexagon proposes to reference the 1075 O'Brien Drive traffic study for background traffic volumes. Intersection levels of service under background conditions will be evaluated.
11. **Project Trip Generation, Distribution, and Assignment.** Estimates of trips to be added to the surrounding roadway network by the proposed development will be based on the trip generation rates recommended by the Institute of Traffic Engineers' *Trip Generation Manual, 11th Edition*. Hexagon will work with City staff on the methodology to estimate trip generation for the ground-floor commercial use, as well as for any potential credits for the existing buildings on site that are to be demolished. A 20% transportation demand management (TDM) reduction will be applied in accordance with Menlo Park requirements.

The directional distribution of site-generated traffic will reference the 1075 O'Brien Drive traffic study. The proposed trip generation estimation and project trip distribution patterns will be sent to the City to review. The site-generated net traffic will be assigned to the roadway network based on the City approved trip generation and distribution pattern.

12. **Evaluation of Background Plus Project Conditions.** Project-generated traffic will be added to the background condition traffic volumes. Intersection level of service calculations will be conducted to estimate project traffic conditions during the AM and PM peak hours after project completion. Intersection adverse effects associated with the development of the proposed project will be evaluated relative to background conditions.
13. **Cumulative Conditions.** The latest 2040 model run, conducted for the City's proposed Housing Element Update, included full buildout of the Menlo Park General Plan and relevant General Plan Amendments, and approved and pending projects within the City of East Palo Alto up to December 2020. This model run will be used to develop intersection volume forecasts at study intersections. Since the proposed project is within the development capacity allowed under the Menlo Park General Plan, the new model run will represent the cumulative plus project conditions for the project. Project traffic will be subtracted to represent the cumulative no project conditions. Intersection adverse effects associated with the development of the proposed project will be evaluated relative to cumulative no project conditions.
14. **Roadway AADT Analysis.** Hexagon will evaluate the project effects on roadway AADT under the cumulative with project scenario. Adverse effects will be identified in accordance with Menlo Park's guidelines.



- 15. Site Access and On-Site Circulation.** A review of the project site plan will be performed to determine the overall adequacy of the site access and on-site circulation in accordance with generally accepted traffic engineering standards and to identify any access or circulation issues that should be improved. Sight distance will be checked at the project driveways. Parking will be evaluated relative to the City of Menlo Park Parking Code.
- 16. Evaluation of Vehicle Queuing.** For selected locations where the project would add a significant number of left-turning vehicles, the adequacy of existing/planned storage at turn pockets will be assessed by means of comparison with expected maximum vehicle queues. Vehicle queues will be estimated using the Vistro software.
- 17. Description of Recommendations.** The results of the level of service calculations will be used to identify the locations and types of necessary improvements or modifications. Improvements could include street widenings, lane additions, changes in lane usage, or modifications to existing traffic signals, which will be consistent with the mitigation measures proposed in the City's General Plan Update - ConnectMenlo. Improvements could also include improvements to transit, bicycle and pedestrian infrastructure, as well as the development of measures that promote multi-modal travel and reduce the use of single-occupant automobile travel for the purpose of VMT reduction goals.

Reports and Meetings

- 18. Meetings.** The fee estimate includes Hexagon staff attendance at three staff meetings and one public hearing in connection with the project. Additional meeting attendance would be provided as additional services and will be billed based on staff time plus expenses.
- 19. Reports.** Our findings and recommendations will be summarized in a standalone TIA document. Hexagon will revise the report based on City comments.

Time of Performance

Barring any unforeseen delays, an administrative draft traffic analysis report will be submitted approximately eight weeks after 1) authorization to proceed, and (2) approval of project trip generation, distribution and assignment assumptions. The final traffic report will be delivered one week after receipt of all review comments. The major project milestones are described below.

Major Milestones

Week	Milestone
Week 1	Provide draft trip generation, distribution and assignment for City review and approval (assume 2-week City review time)
Week 2	Provide preliminary VMT results for City review (assume 2-week City review time)
Week 4	Provide preliminary results for non-CEQA analyses (assume 2-week City review time)
Week 6	Provide required data for AQ/Noise consultants
Week 8	Provide administrative draft TIA report



Cost of Services

The fee for the scope of services will be based on time and expenses up to a maximum budget of \$62,000 (see Table 1 for budget breakdown). Work not specifically specified in this scope are considered out-of-scope (such as analyzing additional intersections, revised project description after project initiation), and will require additional budget authorization.

We appreciate your consideration of Hexagon Transportation Consultants for this assignment. If you have any questions, please do not hesitate to call.

Sincerely,
HEXAGON TRANSPORTATION CONSULTANTS, INC.

Ollie Zhou, T.E.
 Vice President

**Table 1
 Budget Breakdown**

#	Task	Hours				Budget
		President	Principal Associate	Associate	Admin/ Graphics	
1	Bicycle, Pedestrian, and Transit Facilities		4	8		\$ 2,900
2	VMT Analysis		8			\$ 2,280
3	Peer Review of TDM Plan	1	20		2	\$ 6,270
4	Project Impact on Hazards and Emergency Vehicle Access		4			\$ 1,140
5	Provision of Data to AQ/Noise		4	4		\$ 2,020
6	Site Reconnaissance			4		\$ 880
7	Traffic Condition Observations			8		\$ 1,760
8	Data Collection			4		\$ 880
9	Evaluation of Existing Conditions		2	8		\$ 2,330
10	Background Conditions			4		\$ 880
11	Trip Generation, Distribution, Assignment	1	4	8	2	\$ 3,470
12	Background + Project Conditions			4		\$ 880
13	Cumulative Conditions		8	8		\$ 4,040
14	Roadway AADT Analysis			4		\$ 880
15	Site Access, On-Site Circulation, Parking		4	8		\$ 2,900
16	Vehicle Queuing			8		\$ 1,760
17	Description of Recommendations	4	8	8		\$ 5,360
18	Meetings	4	8			\$ 3,600
19	Reports	8	20	40	8	\$ 18,100
	Total	18	94	128	12	\$ 62,000

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△ Date revision description

4	2023-01-19	PLANNING REVISION 3
3	2022-11-04	PLANNING REVISION 2
2	2022-07-27	PLANNING REVISION 1
1	2022-02-11	PLANNING APPLICATION
#	Date	Issue description



1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

Project Number: 21-05
Date: 2023-01-19

TITLE SHEET

G0.00



NOTES

a. Any mitigation improvements which are damaged as a result of construction will be required to be replaced. All mitigation improvement work shall be in accordance with the latest version of the City Standard Details.

b. An encroachment permit from the Engineering Division is required prior to any construction activities, including utility materials, in the public right of way.

1030 O' BRIEN

PLANNING APPLICATION - REVISION 3

JANUARY 19, 2023

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1030 O'Brien Drive
Menlo Park, CA 94025
Tel: 650.325.1315
Fax: 650.325.1314

- GENERAL**
- G0.00 TITLE SHEET
 - G0.01 SHEET INDEX
 - G0.02 PROJECT INFORMATION
 - G1.01 CODE SUMMARY, OCCUPANCY SEPARATION SECTION, FIRE SEPARATION DIAGRAM
 - G1.02 CODE ANALYSIS PLANS
 - G1.03 CODE ANALYSIS PLANS
 - G2.01 BUILDING GROSS AREA - MENLO PARK MUNICIPAL CODE
 - G2.02 EXISTING BUILDING 980990 GROSS AREA - MENLO PARK MUNICIPAL CODE
 - G2.03 EXISTING BUILDING 1010 GROSS AREA - MENLO PARK MUNICIPAL CODE
 - G3.01 PARKING DATA AND DIAGRAMS
 - G4.01 EXISTING CONDITIONS PHOTOGRAPHS
 - G4.21 MATERIAL PALETTE
 - G4.42 TRANSPARENCY DIAGRAM
 - G4.43 BUILDING MODULATION DIAGRAM

GENERAL: 14

ARCHITECTURE

- A1.00 EXISTING SITE PLAN
- A1.01 AREA PLAN
- A1.02 PROPOSED SITE PLAN
- A1.03 ENLARGED SITE PLAN & SITE CIRCULATION PLAN
- A2.01 LEVEL 1 FLOOR PLAN
- A2.02 LEVEL 2 FLOOR PLAN
- A2.03 LEVEL 3 FLOOR PLAN
- A2.04 ROOF PLAN
- A3.01 BUILDING ELEVATIONS
- A3.02 BUILDING ELEVATIONS
- A3.03 3D VIEWS
- A3.04 3D VIEWS
- A3.11 BUILDING SECTIONS
- A8.01 EXTERIOR DETAILS - TYPICAL WALL ASSEMBLIES
- A8.02 EXTERIOR DETAILS - FLOOR AND ROOF ASSEMBLIES

ARCHITECTURE: 15

CIVIL

- C1.0 EXISTING CONDITIONS
- C2.1 PRELIMINARY CIVIL SITE PLAN
- C2.2 PRELIMINARY CIVIL SITE PLAN
- C3.1 PRELIMINARY GRADING & DRAINAGE PLAN
- C3.2 PRELIMINARY GRADING & DRAINAGE PLAN
- C4.1 PRELIMINARY UTILITY PLAN
- C4.2 PRELIMINARY UTILITY PLAN
- C5.0 STORMWATER QUALITY CONTROL PLAN

CIVIL: 8

LANDSCAPE

- L0.00 LANDSCAPE NOTES AND LEGENDS
- L0.01 LANDSCAPE NOTES AND LEGENDS
- L0.02 GROUND LEVEL LANDSCAPE COLOR PLAN
- L0.03 LANDSCAPE 3RD LEVEL TERRACE COLOR PLAN
- L0.05 OPEN SPACE CALCULATIONS
- L1.00 LANDSCAPE GROUND LEVEL MATERIALS PLAN
- L1.01 LANDSCAPE PLAZA ENLARGEMENT PLAN
- L1.02 LANDSCAPE 3RD LEVEL TERRACE MATERIALS PLAN
- L2.00 LANDSCAPE GROUND LEVEL PLANTING PLAN
- L2.01 LANDSCAPE 3RD LEVEL TERRACE PLANTING PLAN
- L3.00 LANDSCAPE PLANTING IMAGES
- L3.01 LANDSCAPE PLANTING IMAGES
- L3.02 LANDSCAPE PLANTING IMAGES
- L4.00 LANDSCAPE DETAILS
- L4.01 LANDSCAPE DETAILS
- L4.02 LANDSCAPE DETAILS
- L4.03 LANDSCAPE DETAILS

LANDSCAPE: 17

PHOTOMETRICS

- XE.301 980-1030 O'BRIEN STREET PHOTOMETRIC PLAN

PHOTOMETRICS: 1

SURVEY

- G0.10 BOUNDARY & TOPOGRAPHIC SURVEY - 990 1010 1020 1030 O'BRIEN

SURVEY: 1

TOTAL SHEETS IN SET: 56

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CONSULTANT

△ date revision description

#	date	revision description
4	2023.01.19	PLANNING REVISION 3
3	2023.01.04	PLANNING REVISION 2
2	2023.07.27	PLANNING REVISION 1
1	2022.02.11	PLANNING APPLICATION

date issue description



1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

project number: 21-005
date: 2023.01.19

SHEET INDEX

NOT FOR CONSTRUCTION



4-21-2021

LEGEND

- FOUND POINT W/ MONUMENT CASTING
- ◇ TITLE REPORT EXCEPTION NUMBER
- () RECORD DATA / REFERENCE
- WATER METER OF WATER VALVE BOX
- ⊕ FIRE HYDRANT
- TRUNK - TRUNK DIAMETER IN INCHES
- TREE - TREE SPECIES IDENTIFICATION; BEST EFFORT, WE ARE NOT ARBORISTS OR ENTOMOLOGISTS
- TREE - WITH MULTIPLE TRUNKS
- TREE - TREE SHIP LINE POINTS TOWARD TREE TRUNKS; TREE SHIP LINES ABOVE PROPERTY LOCATED AS SHOWN
- TSP OF CURB
- FENCE
- OVERHEAD WIRES
- POWER POLE
- SPOT ELEVATION
- SANITARY SEWER CLEAN OUT
- UTILITY BOX - TYPE AS NOTED SITE AS DRAWN
- IRRIGATION VALVE BOX
- BUSH
- DOWN SPOUT
- WATER VALVE
- HANDICAP RAUP OR PARKING
- EDGE OF AC PAVING
- STORM DRAIN CURB INLET
- GAS LINE (PER SURFACE PAINT)
- WATER LINE (PER SURFACE PAINT)
- ELECTRIC LINE (PER SURFACE PAINT)

ABBREVIATIONS

- AC ACRES
- BLK BLOCK
- CU CUBIC
- CSW OF CURB
- FL FLOW LINE
- STW STORM DRAIN WASTEW
- SWR SANITARY SEWER WASTEW
- PU PUBLIC UTILITY ALIGNMENT
- PLS POINT
- AS RECORD OF MONUMENT

I CERTIFY THAT THIS PROJECT'S SURVEYING WAS CONDUCTED BY ME OR UNDER MY SUPERVISION AND IS BASED ON A FIELD SURVEY IN CONFORMANCE WITH THE LAND SURVEYORS ACT. ALL MONUMENTS ARE OF THE SURVEYOR AND OCCUPY THE PROVISIONS INDICATED AND ARE SUFFICIENT TO EMPOWER THE SURVEY TO BE REPRODUCED.

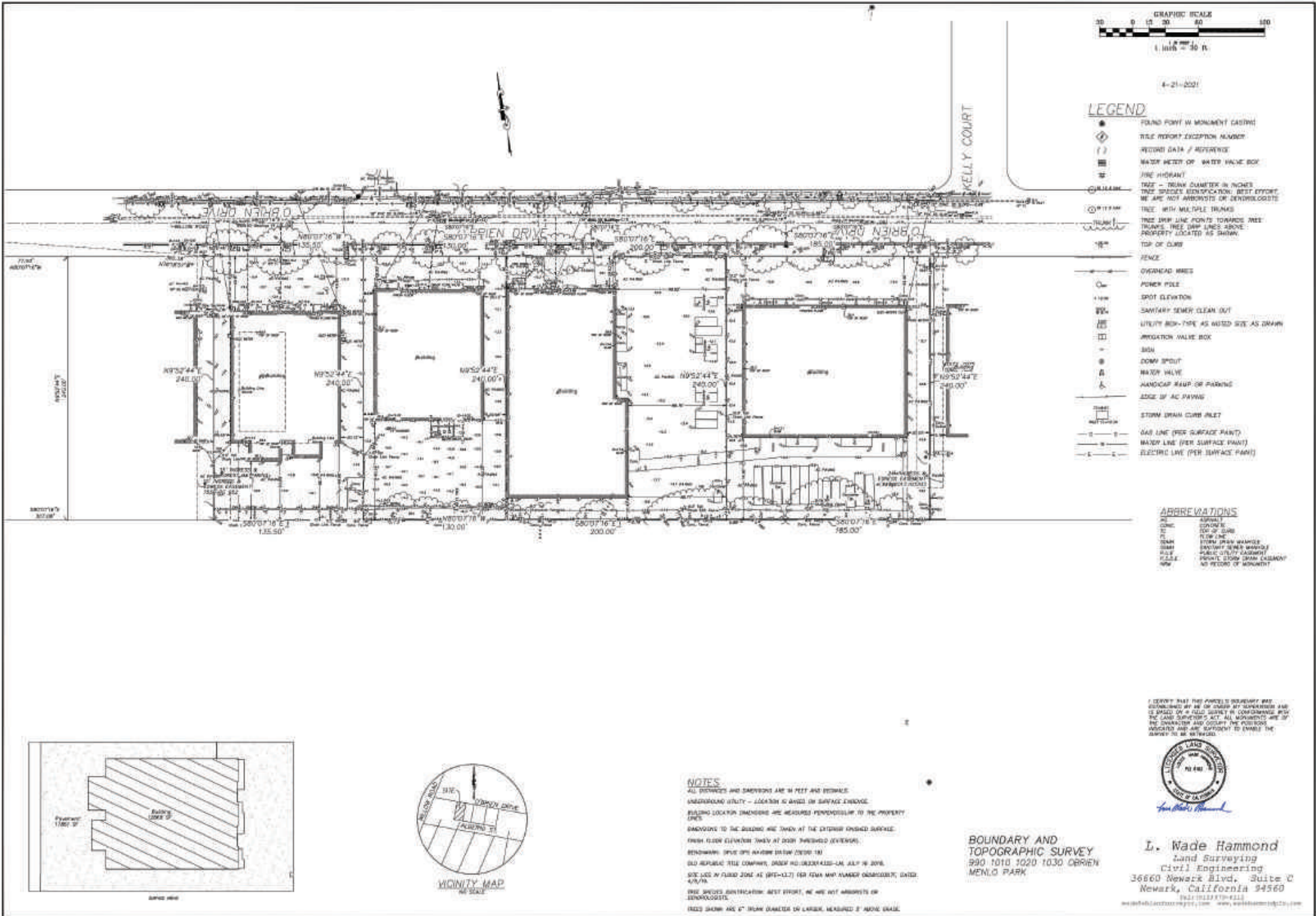
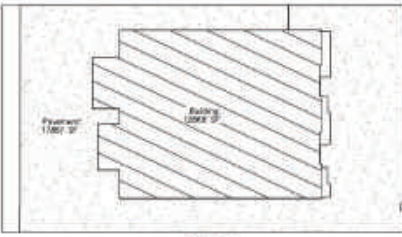


L. Wade Hammond
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 36660 Newark Blvd., Suite C
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BOUNDARY AND TOPOGRAPHIC SURVEY
 990 1010 1020 1030 O'BRIEN MENLO PARK

NOTES

- ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS.
- UNDERGROUND UTILITY - LOCATION IS BASED ON SURFACE ELEVATION.
- BUILDING LOCATION DIMENSIONS ARE MEASURED PERPENDICULAR TO THE PROPERTY LINES.
- DIMENSIONS TO THE BUILDING ARE TAKEN AT THE EXTERIOR FINISHED SURFACE.
- FRESH CLEAN ELEVATION TAKEN AT DOWN PAVED/ROAD (EXTENSIVE).
- BOUNDARY DRIVE GPS NAVIGATION SYSTEM (2020 18)
- OLD REPUBLIC TITLE COMPANY, ORDER NO. 0002445-144, JULY 19, 2016.
- SEE LOG IN FILED ZONE AS (815-112) FOR FEMA MAP ALREADY OBSOLETE, DATED 4/24/16.
- TREE SPECIES IDENTIFICATION; BEST EFFORT, WE ARE NOT ARBORISTS OR ENTOMOLOGISTS.
- TREES SHOWN ARE 1" TRUNK DIAMETER IN LARGEST MEASURED 3' ABOVE GRADE.





GREG BUNTON
ARCHITECTURE
1 3 3 7 0
5 8 Y L I N E
B O U L E V A R D
W O D S I D E
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FLOOR PLAN KEYNOTES:

- 1 FUTURE GENERATOR (D.L.C.)
- 2 (N) DFP 80 FURNISHING - SEE DETAIL 2
- 3 (N) NOT USED
- 4 (N) ROOF HATCH & ACCESS - SEE DETAIL 2
- 5 (N) SARCUS OPENING W/ STEEL REINFORCING & 2'x7' W/ DOOR & FRAME
- 6 (N) EPLOY SHK IN METAL BASE CAB W/ EPLOY TOP & SPLASH & CN = 3/4" SUPPLIES
- 7 (N) CONC. EXTERIOR WALL
- 8 (N) RECTANGULAR DUCTED LOW EXHAUST IN (N) PARTITIONS
- 9 (N) FULL HT 3-HOUR RATED OCCUPANCY SEPARATION WALL TO REMAIN (DETACHING WALL)
- 10 (N) METAL BASE CABINETS W/ SPOON TOP & SPLASH
- 11 (N) EMERGENCY EXHAUST / SHOWER STANDPIPE
- 12 (N) PLASTIC LAM REAGENT RACK
- 13 (N) FUME HOOD W/ TOP SHK & 2" SUPPLY, PLUMBED FOR CO2
- 14 (N) HOOD REAGENT LAB WASTE LINE - SEE PLUMBING DRAWINGS. SEE TRENCH REPAIR DETAIL 3
- 15 (N) 3 PIECE FIBERGLASS HC SHOWER, SEAT, GRAB BARS, CURTAIN
- 16 (N) ALUM. STONEFRONT IN (E) OPENING W/ 3/4" FRAMED TRIMMED GLASS
- 17 (N) ALUM. STONEFRONT DOOR W/ 3/4" FRAMED TRIMMED GLASS
- 18 (N) HOT WATER HEATER FOR RESTROOMS, LUNCH ROOM, SHOWER
- 19 (N) PLASTIC LAM UPPER CABINETS
- 20 (N) PLASTIC LAM BASE CABINETS
- 21 (N) BUILT-IN S.S. WOODRAME AT UPPER CABS
- 22 (N) S.S. DOWNSHOWER
- 23 (N) S.S. ACCESSIBLE SINK & FAUCET
- 24 (N) PLUMBING - N.L.C.
- 25 (N) FLOOR W/ HDG SHK W/ FACETS & 4" X 4" FIP ON 2" WALLS
- 26 (N) SINK RACK - N.L.C.
- 27 (N) FLOOR MOUNTED FLUSHMOUNT W.C. AT (E) PLUMBING
- 28 (N) LAM WOOD SHAPED POB IN LANDSCAPING - SEE PLUMBING DRAWINGS
- 29 (N) SANSARY SINKER DRAIN TO STREET - FIELD VERIFY LOCATION
- 30 (E) FIRE SPRINKLER RISER. VERIFY SUPPLY IS ADEQUATE FOR BOTH TOWERS. VERIFY IF BACKFLOW DEVICE IS REQ'D.
- 31 (E) DFP 80 FURNISHING TO REMAIN
- 32 (N) LAMING, LAMING & POWER ON WARMING HOLDING ROOM - PROVIDE SEPARATE PANEL FOR FUTURE BACKUP POWER.
- 33 (N) LOOKING PLUG, CEILING MOUNTED, HVAC EXHAUST TO FAH WITH TRIMBLE FITTING
- 34 (N) HOT WATER HEATER FOR LABS
- 35 (N) 1/4" HP W/ AMERICAN - SHOWER DRAINAGE
- 36 (N) MILK-OFF MAT - SEE DETAIL 3
- 37 (N) SEISMIC CLIP AT WALL TO SLAB - S.S.

990 O'BRIEN DRIVE
MENLO PARK, CA
FLOOR PLAN

scale

date 13 JAN 19

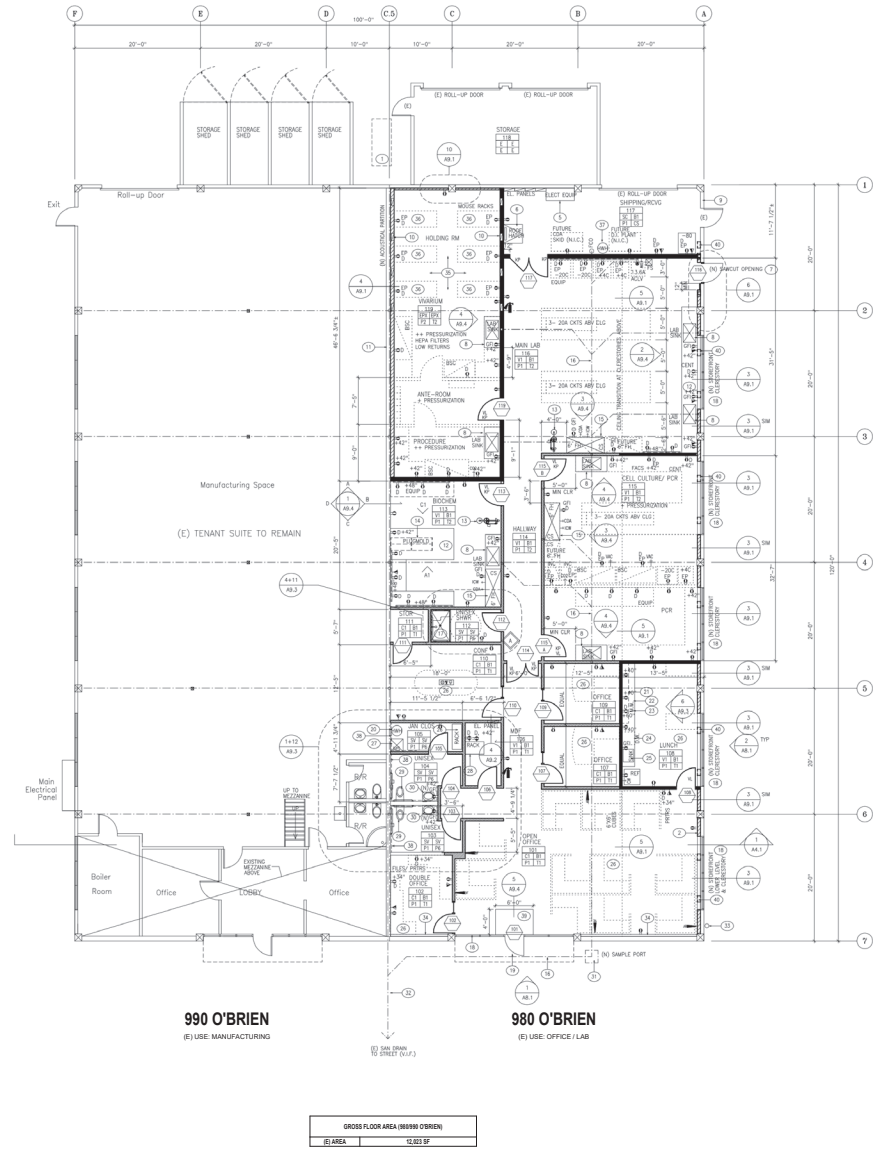
revisions

project 1915

drawing 1815

A2.2

1/8" = 1'-0"



990 O'BRIEN
(E) USE: MANUFACTURING

980 O'BRIEN
(E) USE: OFFICE / LAB

GROSS FLOOR AREA (980/990 O'BRIEN)
(E) AREA 14,023 SF

FOR REFERENCE ONLY
NO PROPOSED WORK

FLOOR PLAN LEGEND

- (E) DOOR & PARTITION TO REMAIN
- (N) FULL HT NON-RATED PARTITION W/ (N) DOOR
- (N) ACoustical PARTITION THRU CEILING
- (N) FURNISH THRU CEILING
- (N) FURNISH THRU CEILING AT WARHM
- POSSIBLE FUTURE IMPROVEMENTS (D.L.C.)
- (N) FIRE EXTINGUISHERS - SEE DETAIL IN SEMI-ACCESSIBLE CABINET AT DFP 80 PARTITIONS
- (N) S.S. RECIPIRATE
- (N) W/REUSE
- (N) NOT IN CONTRACT

- OFFICE - ROOM NAME
- ROOM NUMBER
- BASE
- CEILING
- NO FINISH
- EXISTING FINISH
- 4" CONED RUBBER BASE
- CARPET TILE
- DAY SHEET DOOR INSULATION
- EPHY (N) EPLOY W/FORM FLOORING & BASE
- FRIB (N) FIBERGLASS REINFORCED PLASTIC LAM LINER
- P1 (N) LATEX PAINT
- P2 (N) EPLOY PAINT
- (E) CONCRETE W/ (N) SEALER
- SV (N) SHEET VINYL
- T1 (N) 2x4 2ND LOOK CEILING TILE
- T2 (N) 2x4 W/SHIMBLE CEILING TILE
- V1 (N) VCT

PLUMBING LEGEND

- (N) FLOOR SINK
- (N) 1/4" DOMESTIC COLD WATER SUPPLY
- (N) EMERGENCY EXHAUST SHOWER STANDPIPE - SEE DETAIL
- (N) CLEAN DRY AIR
- (N) INDUSTRIAL COLD WATER
- (N) INDUSTRIAL HOT WATER

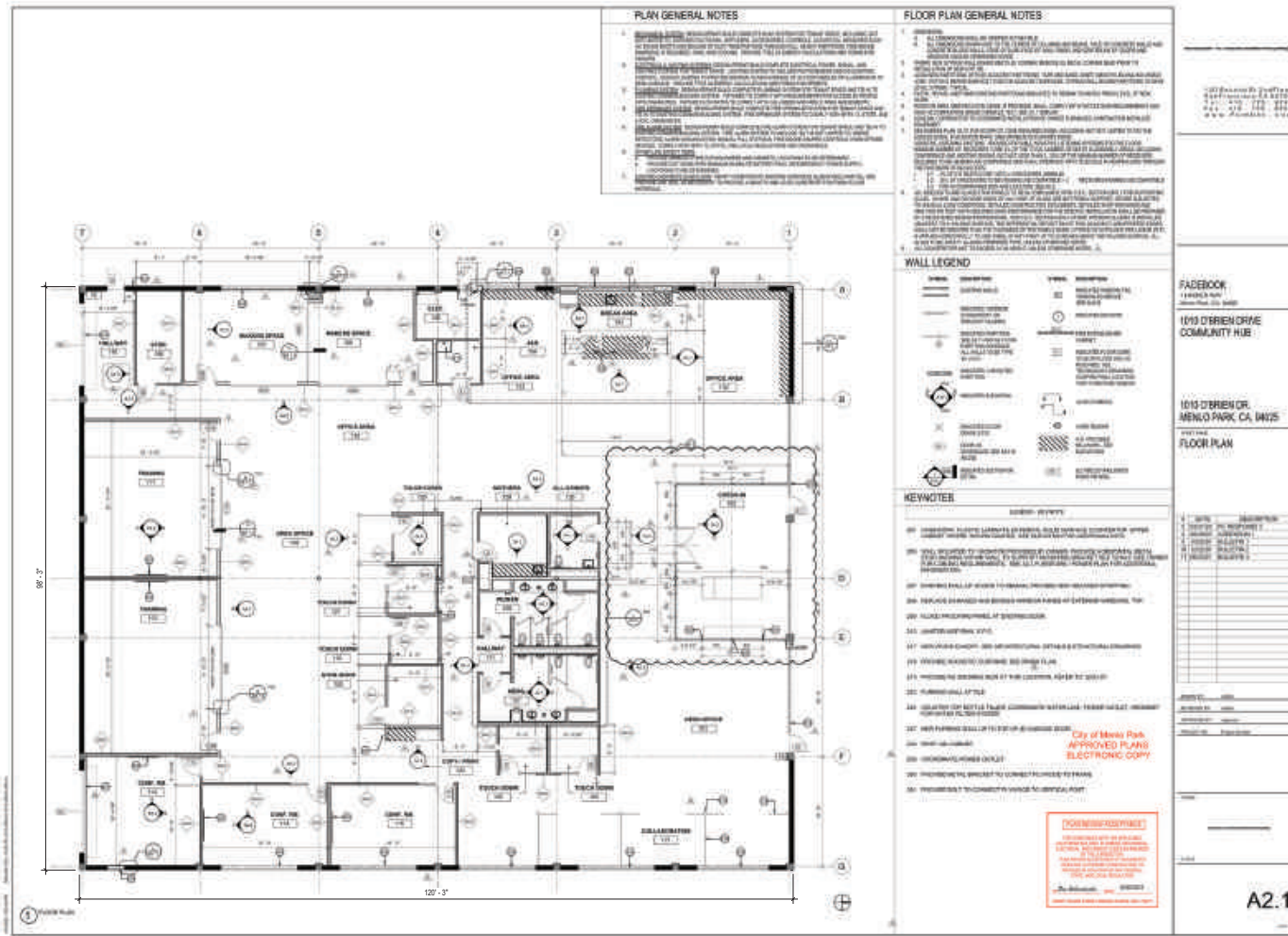
ELECTRICAL LEGEND

- (N) NEW 110V DUPLEX OUTLET (DEGATED CIRCUITS AT EQUIPMENT, THY)
- (N) NEW 110V DEGRADED CIRCUIT DUPLEX OUTLET
- (N) NEW 110V DEGRADED DUPLEX OUTLET ON EMERGENCY POWER
- (N) NEW 110V GROUND FAULT INTERRUPTED DUPLEX OUTLET
- (N) NEW 200V OUTLET (400V WHERE NOTED)
- (N) NEW PHONOGRAMA OUTLET
- (N) NEW 110V 4-REX OUTLET
- (N) NEW WIREMOLD 6.000 W/ SEPARATE POWER & DATA CHANNELS
- (N) NEW BASE FEED W/ 110V POWER & DATA PROVIDE W/AL 1 CIRCUIT FOR 3 CIRCULES
- (N) NEW POWER, DATA, & HDMI FLOOR BOX

FLOOR PLAN NOTES:

1. ALL DIMENSIONS ARE TO FACE UNLESS NOTED.
2. VERIFY ALL CONDITIONS PRIOR TO CONSTRUCTION. NOTIFY ARCHITECT IMMEDIATELY BEFORE COMMENCING WORK.
3. PROVIDE 24-100% FIRE EXTINGUISHERS & SEMI-ACCESSIBLE CABINETS AT DFP 80. PROVIDE 24-100% PRESSURIZED TIGHTEN SPACE.
4. PROVIDE FIRE ALARM SYSTEM.
5. FURNISHING IS N.L.C.
6. PROVIDE BACKING AT ALL WALL MOUNTED CABINETS PER DETAIL 3.
7. NOT USED.
8. DATA CABLEING IS N.L.C. - PROVIDE RINGS & STRIPS AT NEW PHONOGRAMA OUTLETS.
9. PROVIDE FACILE EGRESS SIGNAGE AS REQUIRED - SEE SHEET A12.
10. TEST SLAB FOR REINFORCING LEVEL PRIOR TO REINFORCING FORMWORKING. PROVIDE W/PHOS-PROOFING IF REQUIRED.
11. PROVIDE NEW MECHANICAL RESCORTP UNITS PER MECHANICAL PLANS.
12. PROVIDE NEW ROOF SENSORS - SEE ROOF PLAN.
13. PROVIDE LEVEL 4 WALL FINISH THROUGHOUT.
14. RE-FLOOR EXISTING CONC. SLAB ON GRADE AS NOTED FOR (N) FLOOR FINISHES.
15. PROVIDE (E) ACCENT WALL COLORS IN LOCATIONS TO BE DETERMINED.
16. FURNISHING HEIGHTS, SEE DETAIL.
17. FURNISHING DOOR CLEARANCES, SEE DETAIL.

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1010 O'BRIEN
(E) USE: OFFICE

GROSS FLOOR AREA (1010 O'BRIEN)	
NET AREA	11,811 SF

FOR REFERENCE ONLY
NO PROPOSED WORK

brick.

CLIENT
R+M Properties

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Oakland, CA 94612
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CONSULTANT

FADERBOOK

1010 O'BRIEN DRIVE
COMMUNITY HUB

1010 O'BRIEN DR.
MENLO PARK, CA, 94025
PROJECT #
FLOOR PLAN

DATE	DESCRIPTION
10/27/2021	PLANNING APPLICATION
11/10/2021	PLANNING REVISION 1
11/17/2021	PLANNING REVISION 2
11/23/2021	PLANNING REVISION 3
12/01/2021	PLANNING REVISION 4
12/08/2021	PLANNING REVISION 5
12/15/2021	PLANNING REVISION 6
12/22/2021	PLANNING REVISION 7
12/29/2021	PLANNING REVISION 8
1/05/2022	PLANNING REVISION 9
1/12/2022	PLANNING REVISION 10
1/19/2022	PLANNING REVISION 11
1/26/2022	PLANNING REVISION 12
2/02/2022	PLANNING REVISION 13
2/09/2022	PLANNING REVISION 14
2/16/2022	PLANNING REVISION 15
2/23/2022	PLANNING REVISION 16
3/01/2022	PLANNING REVISION 17
3/08/2022	PLANNING REVISION 18
3/15/2022	PLANNING REVISION 19
3/22/2022	PLANNING REVISION 20
3/29/2022	PLANNING REVISION 21
4/05/2022	PLANNING REVISION 22
4/12/2022	PLANNING REVISION 23
4/19/2022	PLANNING REVISION 24
4/26/2022	PLANNING REVISION 25
5/03/2022	PLANNING REVISION 26
5/10/2022	PLANNING REVISION 27
5/17/2022	PLANNING REVISION 28
5/24/2022	PLANNING REVISION 29
5/31/2022	PLANNING REVISION 30
6/07/2022	PLANNING REVISION 31
6/14/2022	PLANNING REVISION 32
6/21/2022	PLANNING REVISION 33
6/28/2022	PLANNING REVISION 34
7/05/2022	PLANNING REVISION 35
7/12/2022	PLANNING REVISION 36
7/19/2022	PLANNING REVISION 37
7/26/2022	PLANNING REVISION 38
8/02/2022	PLANNING REVISION 39
8/09/2022	PLANNING REVISION 40
8/16/2022	PLANNING REVISION 41
8/23/2022	PLANNING REVISION 42
8/30/2022	PLANNING REVISION 43
9/06/2022	PLANNING REVISION 44
9/13/2022	PLANNING REVISION 45
9/20/2022	PLANNING REVISION 46
9/27/2022	PLANNING REVISION 47
10/04/2022	PLANNING REVISION 48
10/11/2022	PLANNING REVISION 49
10/18/2022	PLANNING REVISION 50
10/25/2022	PLANNING REVISION 51
11/01/2022	PLANNING REVISION 52
11/08/2022	PLANNING REVISION 53
11/15/2022	PLANNING REVISION 54
11/22/2022	PLANNING REVISION 55
11/29/2022	PLANNING REVISION 56
12/06/2022	PLANNING REVISION 57
12/13/2022	PLANNING REVISION 58
12/20/2022	PLANNING REVISION 59
12/27/2022	PLANNING REVISION 60

City of Menlo Park
APPROVED PLANS
ELECTRONIC COPY

FOR REFERENCE ONLY
NO PROPOSED WORK

A2.1

△ date revision description

date issue description



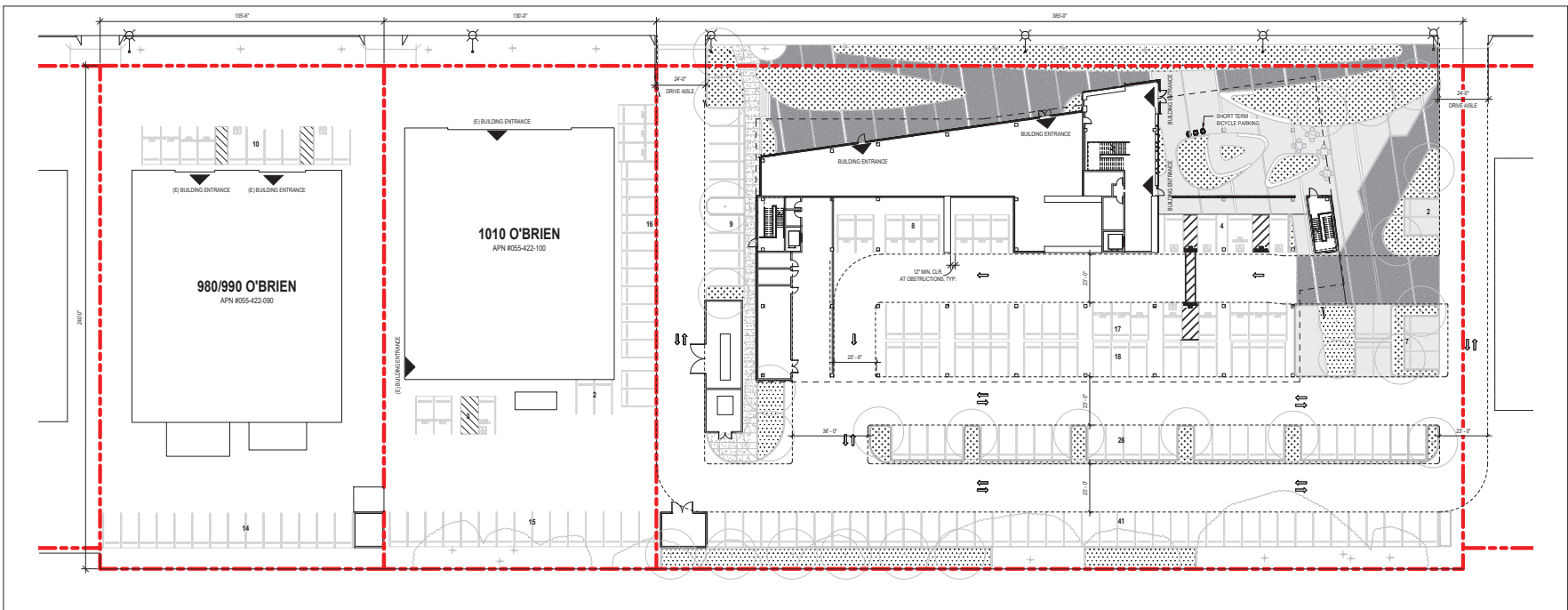
1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

PROJECT NUMBER: 21-05
DATE: 2023-01-19

EXISTING BUILDING
1010 GROSS AREA -
MENLO PARK
MUNICIPAL CODE

G2.03.

NOT FOR CONSTRUCTION



19 PARKING PLAN 3/84" = 1'-0"

VEHICLE PARKING CALCULATION

ISSUE	12,000 SF
EXISTING BUILDING 980/90	12,000 SF
NEW BUILDING AREA (INCL. +10% COMMERCIAL)	87,000 SF
AVAILABLE OFFICE AREA	99,000 SF
MIN. PARKING PROVISIONS	48 STALLS
OFFICE	21,000 SF MIN. 77 STALLS
LIGHT INDUSTRIAL, RFD	1.8, 1,000 SF MIN. 33 STALLS
	172 STALLS
AVAILABLE OFFICE AREA	118 STALLS
OFFICE	31,000 SF MAX. 98 STALLS
LIGHT INDUSTRIAL, RFD	2.5, 1,000 SF MAX. 274 STALLS
CLARENCE	10% OF MIN. PARKING (MARKED AS CAR/CARPOOL/EV) 28 STALLS
OF WHICH 51% = 14% OF TOTAL PARKING	14 STALLS
OF WHICH PER TABLE 118.02B.1.1	1 EV ADA (VAN)
	1 EV ADA
ADA PARKING	6 STALLS
ACCORDING TO TABLE 118.02B.2	
OF WHICH VAN ACCESSIBLE + ADA STALLS 1/6	1 STALL

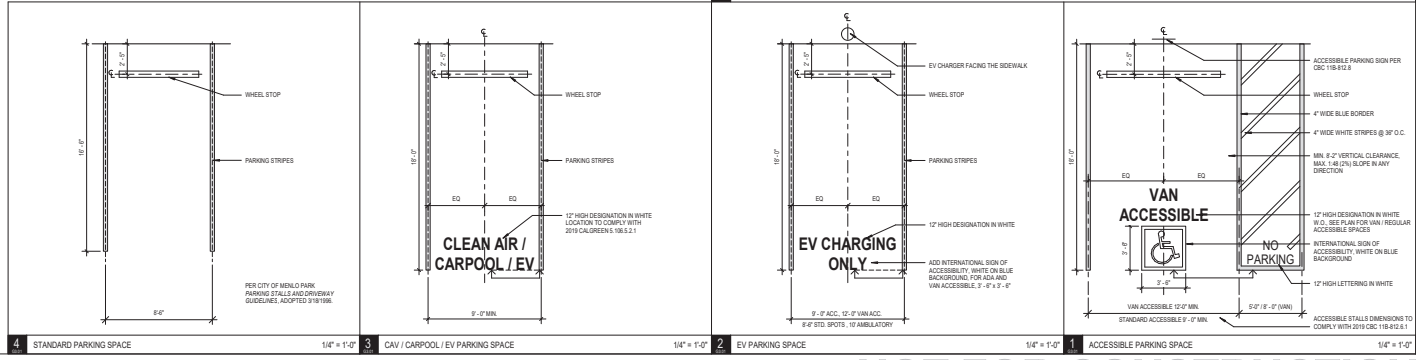
STALLS PROVIDED	150
STANDARD	130
EV VAN ACCESSIBLE	1
EV ADA	1
EV	19
CAR/CARPOOL, EV	19
EV VAN ACCESSIBLE	1
ADA	1
TOTAL STALLS PROVIDED	152
PARKING RATIO:	2.14 STALLS/1,000 GSF

BICYCLE PARKING CALCULATION

FOR OFFICE & RESEARCH DEVELOPMENT PER MUNICIPAL CODE SECTION 16.44.060)

MIN. PARKING CALCULATION	2,500 SF (MIN. 2 SPACES) 2 x (SF/888,500) + 28 SPACES
BREAKDOWN:	81% LONG TERM
	20% SHORT TERM
CLASS:	LONG-TERM BICYCLE PARKING REQ'D: 22 (81% % OF ALL BICYCLE SPACES)
	LONG-TERM BICYCLE PARKING PROVIDED: 22 (100% ON LEVEL 2, 16 ON LEVEL 1)
CLASS:	SHORT-TERM BICYCLE PARKING REQ'D: 4 (20% % OF ALL BICYCLE SPACES)
	SHORT-TERM BICYCLE PARKING PROVIDED: 4 (100% IN 2 SPACES EA.)

PARKING CALCULATIONS







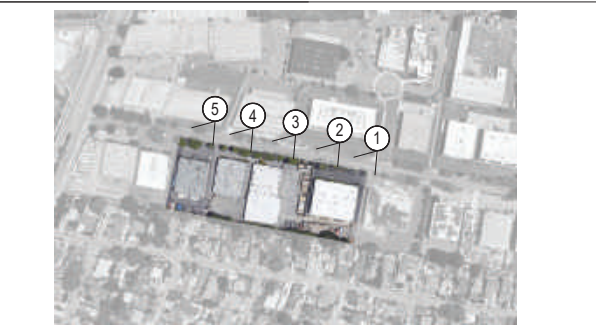
4	2023.01.19	PLANNING REVISION 3
3	2022.11.04	PLANNING REVISION 2
2	2022.07.27	PLANNING REVISION 1
1	2022.02.11	PLANNING APPLICATION
#	date	issue description



PROJECT NUMBER:	21-20
DATE:	2023.01.19
PARKING DATA AND DIAGRAMS	

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1030 O'Brien Drive, Menlo Park, CA 94025
Phone: 650.474.1774

		
		3 1020 O'BRIEN (TO BE DEMOLISHED)
		
		2 1020 O'BRIEN (TO BE DEMOLISHED)
		
5 980 - 990 O'BRIEN (TO REMAIN)		1 1030 - 1050 O'BRIEN (TO BE DEMOLISHED)
		
4 1010 O'BRIEN (TO REMAIN)		KEY PLAN - SITE PHOTOS

brick.

CLIENT
R+M Properties

ARCHITECT
brick.
402 First Street, Suite 300
Oakland, CA 94612
916.263.9197
www.brick-inc.com

CONSULTANT

△ date revision description

4	2023.01.19	PLANNING REVISION 3
3	2022.11.04	PLANNING REVISION 2
2	2023.07.27	PLANNING REVISION 1
1	2022.02.11	PLANNING APPLICATION

date issue description



1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

project number: 21-030
date: 2023.01.19

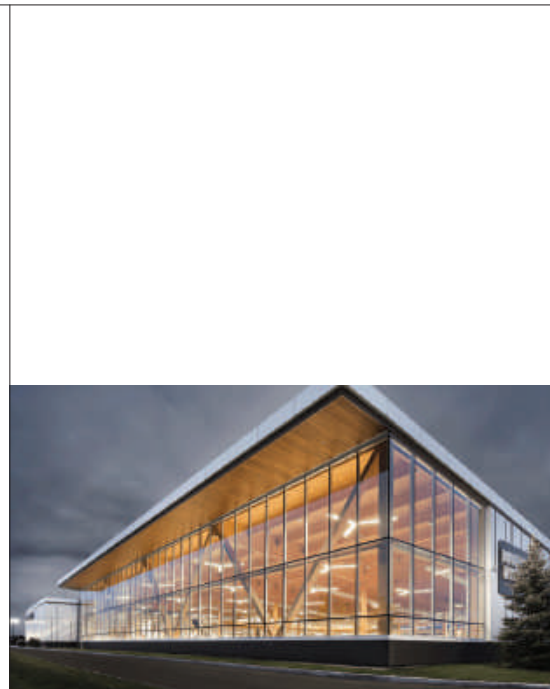
EXISTING
CONDITIONS
PHOTOGRAPHS

G4.01

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 PROJECT # 21074714



RENDERING VIEW LOOKING SOUTHWEST



3 WOOD SOFFIT



RENDERING VIEW LOOKING NORTHEAST



2 COMPOSITE PANEL



1 ALUMINUM & GLASS CURTAINWALL

BIRD FRIENDLY GLAZING: CERAMIC FRIT PATTERN @ GL-1.
 SEE A3.01 & A3.02

brick.

CLIENT
R+M Properties

ARCHITECT
brick.
 402 1st Street, Suite 300
 Oakland, CA 94612
 916.263.9197
 www.brick-inc.com

CONSULTANT

△ date revision description

4	2023.01.19	PLANNING REVISION 3
3	2022.11.04	PLANNING REVISION 2
2	2022.07.27	PLANNING REVISION 1
1	2022.02.11	PLANNING APPLICATION
#	date	issue description



1030 o'brien
 1030 O'Brien Drive
 Menlo Park, CA 94025

project number: 21-09
 date: 2023.01.19

MATERIAL PALETTE

G4.21

NOT FOR CONSTRUCTION

4	2023-01-19	PLANNING REVISION 3
3	2022-11-04	PLANNING REVISION 2
2	2022-07-27	PLANNING REVISION 1
1	2022-02-11	PLANNING APPLICATION

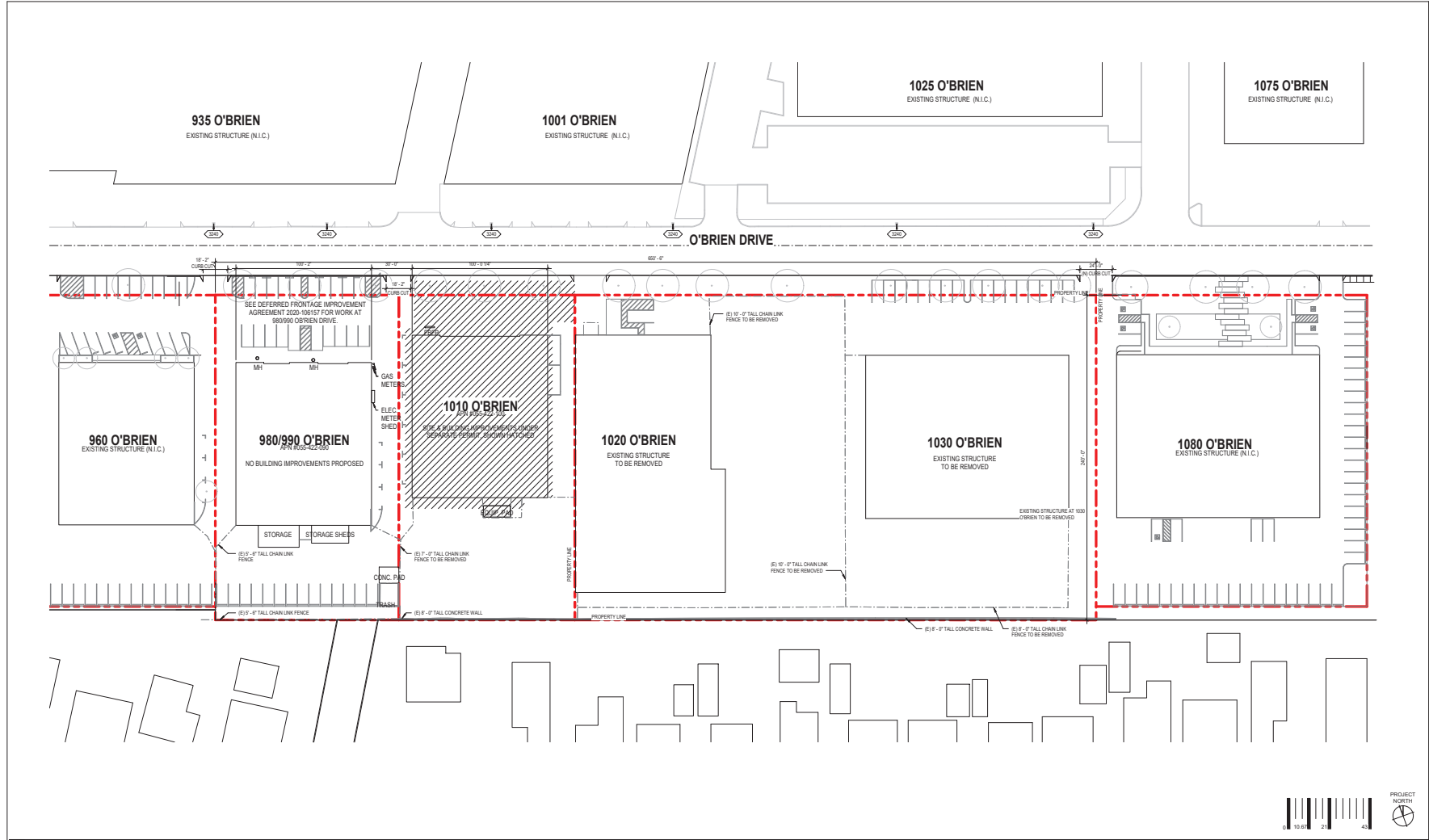
#	Date	Issue description
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1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

Project number: 21-20
Date: 2023-01-19

EXISTING SITE PLAN



1 SITE PLAN - EXISTING 1" = 30'-0"

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- SEE 00.00 FOR GENERAL PROJECT INFORMATION.
- SEE 01.00 SERIES DRAWINGS FOR CODE ANALYSIS.
- SEE 02.00 SERIES DRAWINGS FOR AREA.
- SEE 03.00 SERIES DRAWINGS PARKING DATA AND OPEN AREA.
- SEE 04.00 SERIES DRAWINGS FOR 3D VIEWS AND MATERIALITY.
- ALL OPEN JOINTS, PENETRATIONS AND OTHER OPENINGS IN THE BUILDING ENVELOPE SHALL BE SEALED, CALKED, GASKETED AND WEATHER-STRIPPED TO LIMIT AIR LEAKAGE AND MAINTAIN RATED ASSEMBLIES.
- ALL CONCEALED WOOD BLOCKING SHALL BE FIRE RETARDANT TREATED, TYPICAL.
- PROVIDE THE REQUIRED SACKING, BRACING, AND BLOCKING FOR ATTACHMENT OF CASEWORK, RESTROOM ACCESSORIES, EQUIPMENT, AND OTHER.
- ALL FURNITURE, APPLIANCES, AND TRASH CANS SHALL BE O.F.C.I., U.O.N.
- +0'-0" ELEVATION SHOWN ON THE FLOOR PLAN EQUALS DESIGN FLOOD ELEVATION -15.7 NAVD83.
- PROTECT ADJACENT PROPERTY AS REQUIRED BY CBC SEC. 336.
- ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPLACED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.
- AN ENCROACHMENT PERMIT FROM THE ENGINEERING DIVISION IS REQUIRED PRIOR TO ANY CONSTRUCTION ACTIVITIES, INCLUDING UTILITY LATERALS, IN THE PUBLIC FRONT OF WAY.
- AS PART OF THE ZERO WASTE MANAGEMENT PLAN REQUIRED BY THE CITY OF MENLO PARK, RESTROOMS SHALL INCLUDE ENERGY EFFICIENT HAND DRYERS AND SHALL NOT INCLUDE PAPER TOWEL DISPENSERS. DRINKING FOUNTAINS SHALL INCLUDE BOTTLE FILLING STATIONS AT ALL LOCATIONS.

- BUILDING MAIN ENTRY
- BUILDING OUTLINE
- EASEMENT LINE
- PROPERTY LINE
- SETBACKS
- EXISTING STRUCTURE OUTLINE
- CHAIN LINK FENCE. SEE PLANS FOR HEIGHT
- CONCRETE FENCE. SEE PLANS FOR HEIGHT
- TREE. SEE SHEET L2.00 FOR ADDITIONAL INFORMATION, INCLUDING HERITAGE TREE LOCATIONS.



GENERAL FLOOR PLAN NOTES 1/8" = 1'-0" FLOOR PLAN LEGEND KEYNOTE LEGEND

4	2023-01-19	PLANNING REVISION 3
3	2022-11-04	PLANNING REVISION 2
2	2022-07-27	PLANNING REVISION 1
1	2022-02-11	PLANNING APPLICATION

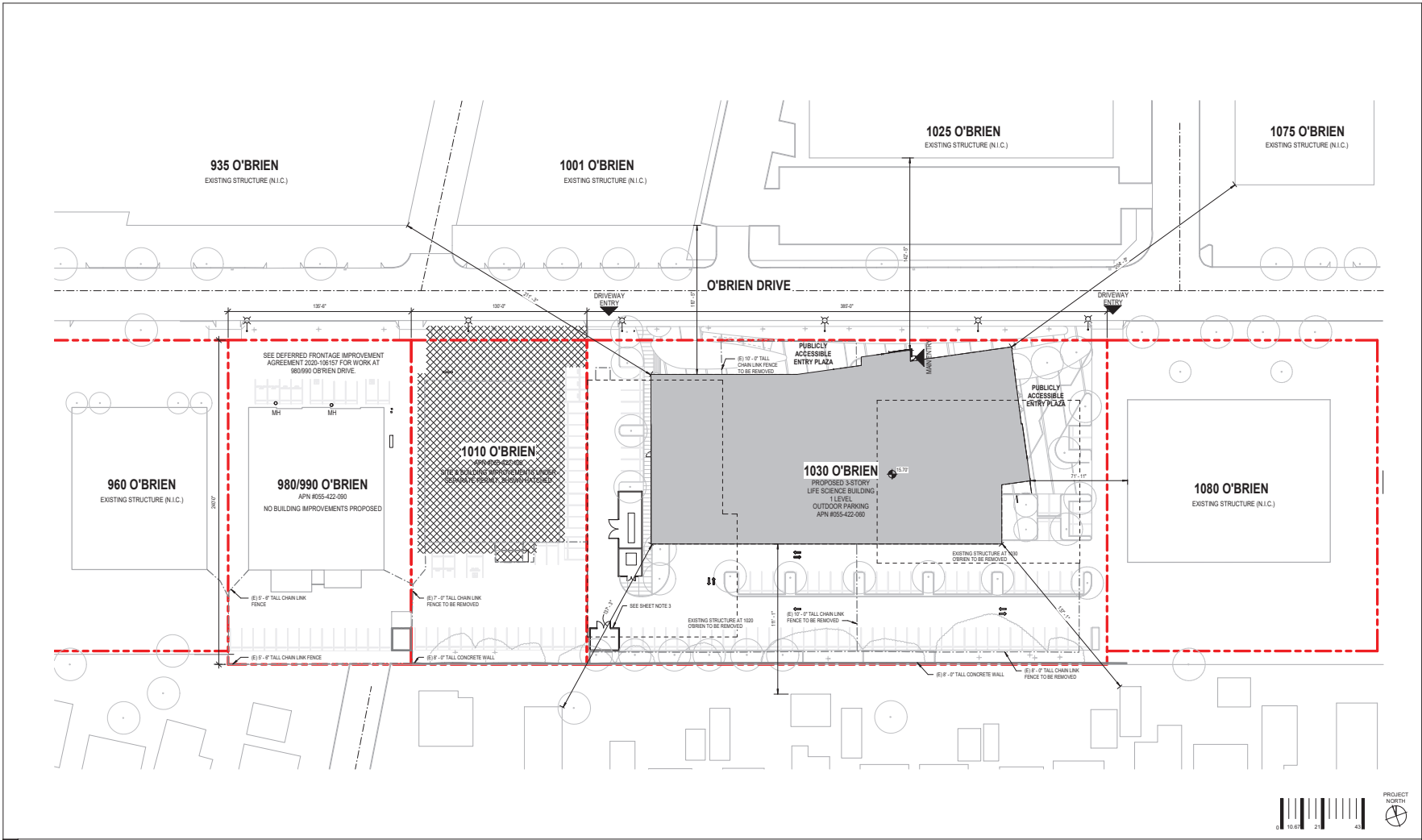
#	Date	Issue description
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1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

Project number: 21-20
Date: 2023-01-19

AREA PLAN



AREA SITE PLAN 1" = 30'-0"

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1. SEE 03.00 FOR GENERAL PROJECT INFORMATION.

2. SEE 01.00 SERIES DRAWINGS FOR CODE ANALYSIS.

3. SEE 02.00 SERIES DRAWINGS FOR AREA.

4. SEE 03.00 SERIES DRAWINGS PARKING DATA AND OPEN AREA.

5. SEE 04.00 SERIES DRAWINGS FOR 3D VIEWS AND MATERIALITY.

6. ALL OPEN JOINTS, PENETRATIONS AND OTHER OPENINGS IN THE BUILDING ENVELOPE SHALL BE SEALED, CALKED, GASKETED AND WEATHER-STRIPPED TO LIMIT AIR LEAKAGE AND MAINTAIN RATED ASSEMBLIES.

7. ALL CONCEALED WOOD BLOCKING SHALL BE FIRE RETARDANT TREATED, TYPICAL.

8. PROVIDE THE REQUIRED GASKING, BRACING, AND BLOCKING FOR ATTACHMENT OF CASEWORK, RESTROOM ACCESSORIES, EQUIPMENT, AND OTHER.

9. ALL FURNITURE, APPLIANCES, AND TRASH CANS SHALL BE OF C.I., U.G.N.

10. ±0'-0" ELEVATION SHOWN ON THE FLOOR PLAN EQUALS DESIGN FLOOR ELEVATION -15.7 NAVD88.

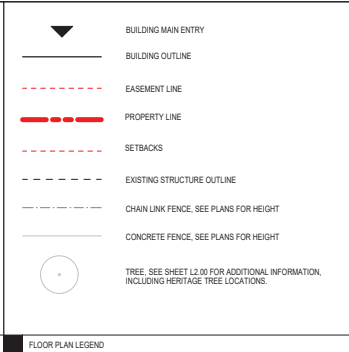
11. PROTECT ADJACENT PROPERTY AS REQUIRED BY CBC SEC. 3366.

12. ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPAIRED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.

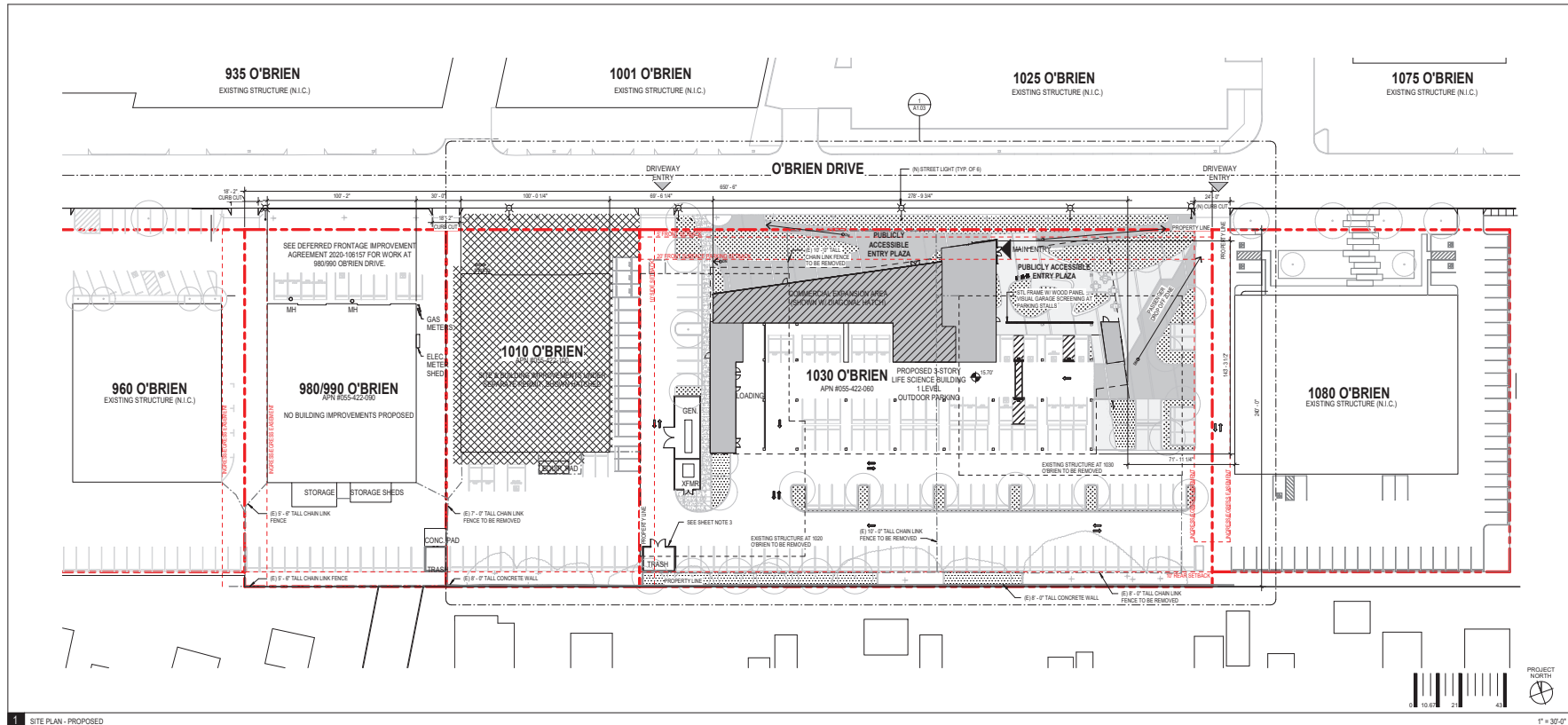
13. AN ENCROACHMENT PERMIT FROM THE ENGINEERING DIVISION IS REQUIRED PRIOR TO ANY CONSTRUCTION ACTIVITIES, INCLUDING UTILITY TRENCHES, IN THE PUBLIC FRONT OF WAY.

14. AS PART OF THE ZERO WASTE MANAGEMENT PLAN REQUIRED BY THE CITY OF MENLO PARK, RESTROOMS SHALL INCLUDE ENERGY EFFICIENT HAND DRYERS AND SHALL NOT INCLUDE PAPER TOWEL DISPENSERS. DRINKING FOUNTAINS SHALL INCLUDE BOTTLE FILLING STATIONS AT ALL LOCATIONS.

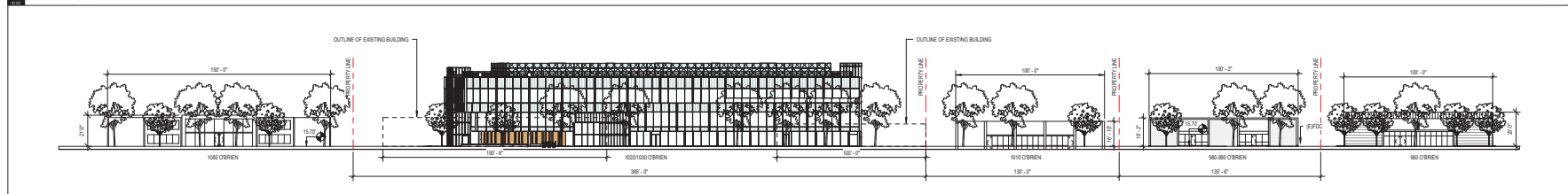
- GENERAL FLOOR PLAN NOTES 1/8" = 1'-0"
- SEE 03.00 FOR GENERAL PROJECT INFORMATION.
 - SEE 01.00 SERIES DRAWINGS FOR CODE ANALYSIS.
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 - ±0'-0" ELEVATION SHOWN ON THE FLOOR PLAN EQUALS DESIGN FLOOR ELEVATION -15.7 NAVD88.
 - PROTECT ADJACENT PROPERTY AS REQUIRED BY CBC SEC. 3366.
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NOT FOR CONSTRUCTION



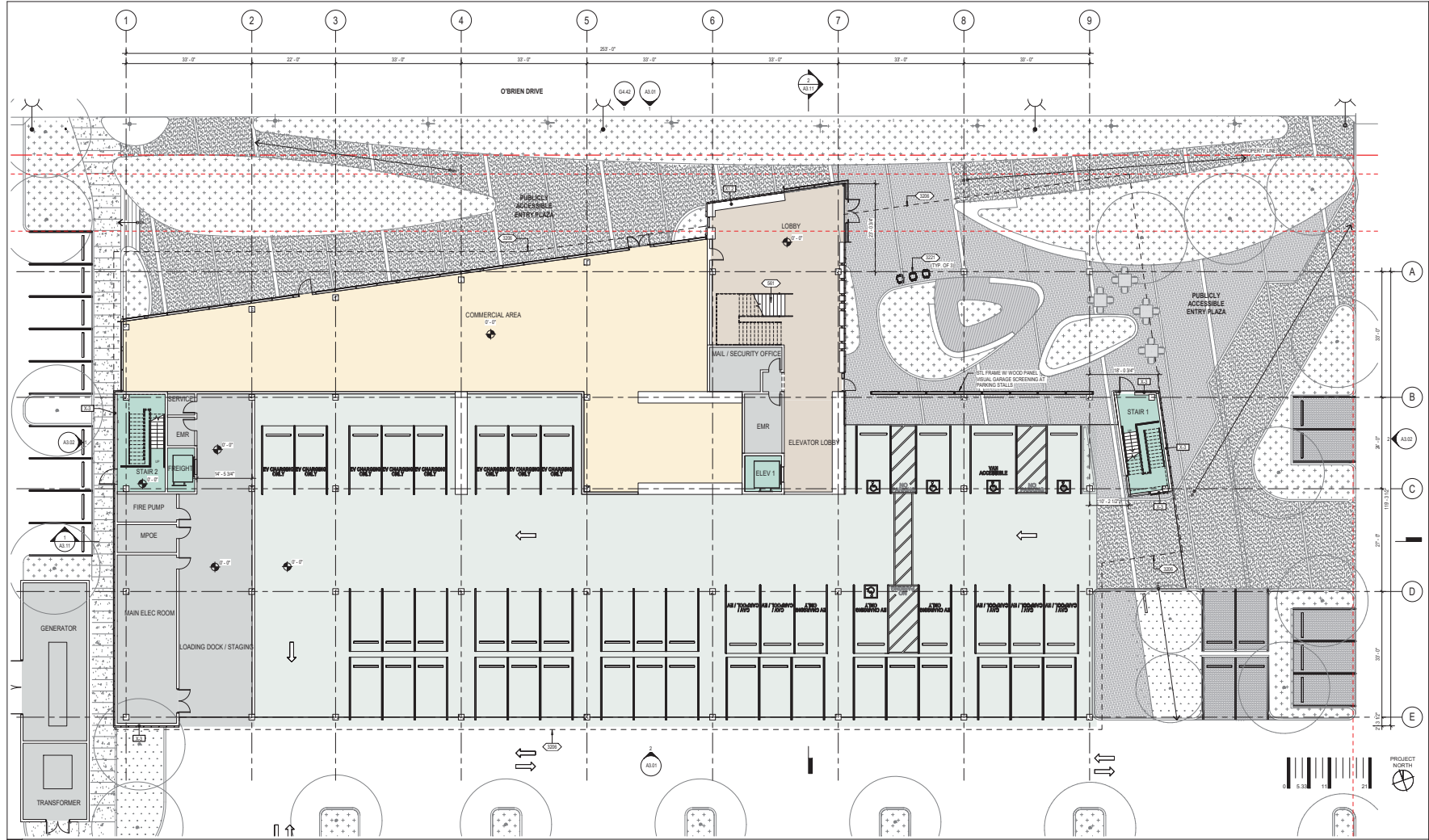
1 SITE PLAN - PROPOSED 1" = 30'-0"



2 STREET ELEVATION - NORTH 1" = 30'-0"

<p>SITE PLAN NOTES</p> <ol style="list-style-type: none"> WAYFINDING FEATURES WILL INCLUDE A FULLY LIT PATH OF TRAVEL, CHANGE IN MATERIAL AT CROSSWALKS, AND MONUMENT SIGNAGE AT THE PARKING ENTRANCE. PARKING ENTRY, EXIT AND CLEARANCE HEIGHT WILL BE INDICATED BY HANGING SIGNAGE AT THESE LOCATIONS. TRASH ENCLOSURE INCLUDES PERMANENT SORTING STATIONS FOR GARbage, RECYCLING, AND ORGANICS. ENCLOSURE SHALL INCLUDE PROPER SIGNAGE FOR EACH STATION. 	<p>GENERAL FLOOR PLAN NOTES</p> <ol style="list-style-type: none"> SEE 02.00 FOR GENERAL PROJECT INFORMATION. SEE 01.00 SERIES DRAWINGS FOR CODE ANALYSIS. SEE 02.00 SERIES DRAWINGS FOR AREA. SEE 03.00 SERIES DRAWINGS PARKING DATA AND OPEN AREA. SEE 04.00 SERIES DRAWINGS FOR 3D VIEWS AND MATERIALITY. ALL OPEN JOINTS, PENETRATIONS AND OTHER OPENINGS IN THE BUILDING ENVELOPE SHALL BE SEALED, CALKED, GASKETED AND WEATHER-STRIPPED TO LIMIT AIR LEAKAGE AND MAINTAIN RATED ASSEMBLIES. ALL CONCEALED WOOD BLOCKING SHALL BE FIRE RETARDANT TREATED, TYPICAL. PROVIDE THE REQUIRED GASKING, BRACING, AND BLOCKING FOR ATTACHMENT OF CASEWORK, RESTROOM ACCESSORIES, EQUIPMENT, AND OTHER. ALL FURNITURE, APPLIANCES, AND TRASH CANS SHALL BE O.F.C.I. U.G.N. +0'-0" ELEVATION SHOWN ON THE FLOOR PLAN EQUALS DESIGN FLOOR ELEVATION -15.7' NAVD83. PROTECT ADJACENT PROPERTY AS REQUIRED BY CBC SEC. 3366. ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPLACED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS. AN ENCROACHMENT PERMIT FROM THE ENGINEERING DIVISION IS REQUIRED PRIOR TO ANY CONSTRUCTION ACTIVITIES, INCLUDING UTILITY LATERALS IN THE PUBLIC RIGHT-OF-WAY. AS PART OF THE ZERO WASTE MANAGEMENT PLAN REQUIRED BY THE CITY OF MENLO PARK, RESTROOMS SHALL INCLUDE ENERGY EFFICIENT HAND DRYERS AND SHALL NOT INCLUDE PAPER TOWEL DISPENSERS. DRINKING FOUNTAINS SHALL INCLUDE BOTTLE FILLING STATIONS AT ALL LOCATIONS. 	<p>FLOOR PLAN LEGEND</p> <ul style="list-style-type: none"> BUILDING MAIN ENTRY BUILDING OUTLINE EASEMENT LINE PROPERTY LINE SETBACKS EXISTING STRUCTURE OUTLINE CHAIN LINK FENCE. SEE PLANS FOR HEIGHT CONCRETE FENCE. SEE PLANS FOR HEIGHT TREE. SEE SHEET L2.00 FOR ADDITIONAL INFORMATION, INCLUDING HERITAGE TREE LOCATIONS. 	<p>KEYNOTE LEGEND</p> <ul style="list-style-type: none"> KEYNOTE TAG
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LEVEL 01

GROSS BUILDING AREA		
LEVEL 01	5217 SF	COMMERCIAL AREA
LEVEL 01	4088 SF	GROSS FLOOR AREA
13305 SF		
LEVEL 02		
	11504 SF	GROSS FLOOR AREA
	11504 SF	
LEVEL 03		
	26229 SF	GROSS FLOOR AREA
	26229 SF	
TOTAL GROSS FLOOR AREA: 47688 SF		

FLOOR PLAN LEGEND

	COMMERCIAL
	LOBBY
	PARKING
	SERVICE
	VERTICAL CIRCULATION

- GENERAL FLOOR PLAN NOTES**
- SEE 01.00 FOR GENERAL PROJECT INFORMATION.
 - SEE 01.00 SERIES DRAWINGS FOR CODE ANALYSIS.
 - SEE 02.00 SERIES DRAWINGS FOR AREA.
 - SEE 03.00 SERIES DRAWINGS PARKING DATA AND OPEN AREA.
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 - PROVIDE THE REQUIRED BRACING, BRACING, AND BLOCKING FOR ATTACHMENT OF CASEWORK, RESTROOM ACCESSORIES, EQUIPMENT, AND OTHER.
 - ALL FURNITURE, APPLIANCES, AND TRASH CANS SHALL BE 0' C.I., U.G.N.
 - +0' ELEVATION SHOWN ON THE FLOOR PLAN EQUALS DESIGN FLOOR ELEVATION -15' NAVD 83.
 - PROTECT ADJACENT PROPERTY AS REQUIRED BY CBC SEC. 3306.
 - ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPAIRED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.
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FLOOR PLAN LEGEND

	KEYNOTE TAG, SEE A2.01 FOR ASSEMBLY DETAILS
	CURTAIN WALL TAG
	TRUNCATED DOMES
	FIRE EXTINGUISHING CABINET, SEE FEC TABLE
	FLOOR DRAIN
	ROOF DRAIN
FIRE EXTINGUISHING CABINET TYPES	
	FEC-A SURFACE MOUNTED
	FEC-B RECESSED MOUNTED
	FEC-C RECESS MOUNTED
	SLAB STEP
	COLUMN
	WALL
	MECHANICAL SHAFT, OPEN TO BELOW

KEYNOTE LEGEND

	KEYNOTE TAG
81	DESIGN BUILD STAIR, CONCRETE FILLED METAL PANT TREAD, HIGH PERFORMANCE PAINTED METAL RISER
108	BUILDING OUTLINE ABOVE, SHOWN DASHED
121	8" RC RACK, S.I.D.

NOT FOR CONSTRUCTION

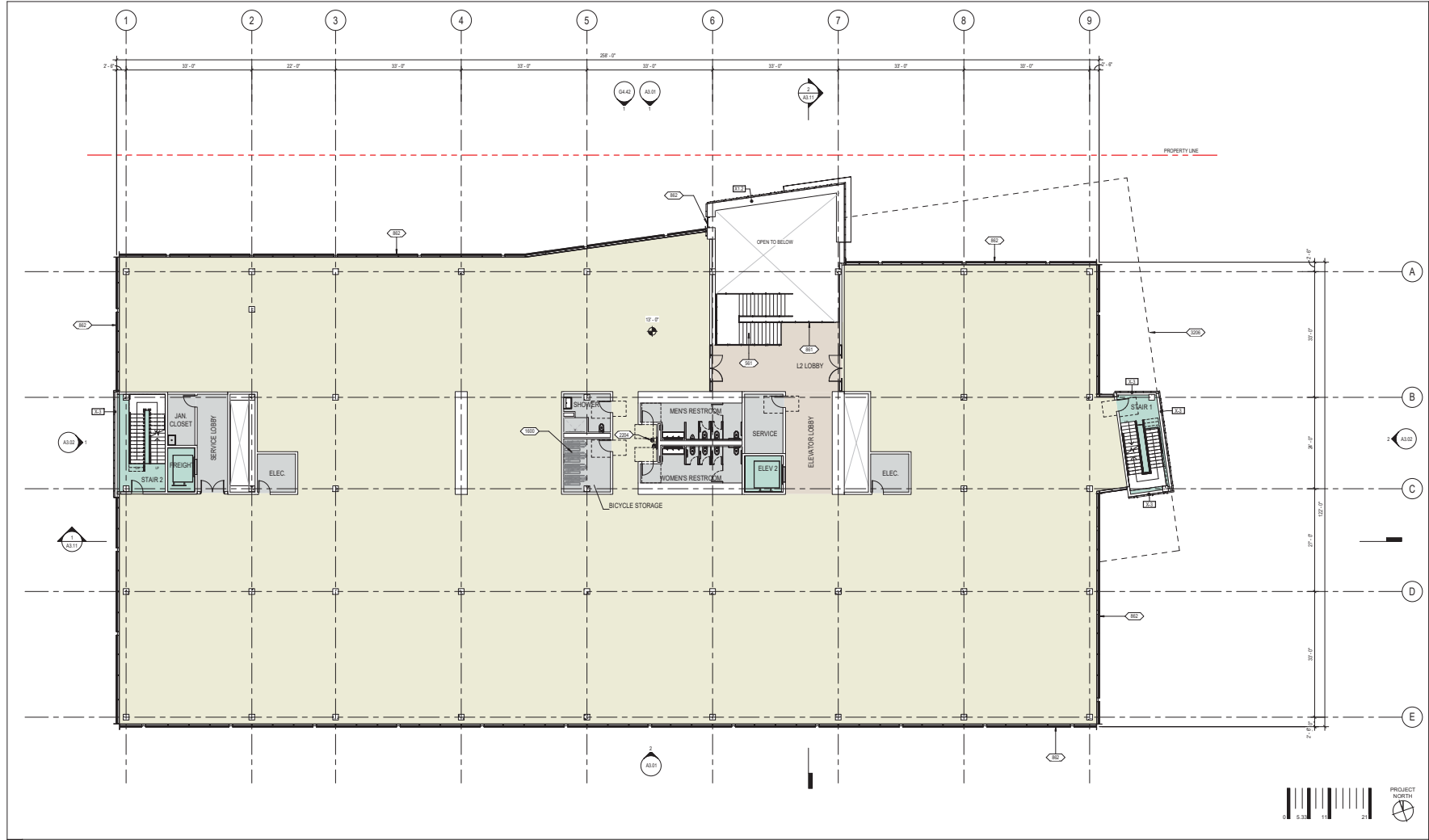
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4	2023.01.19	PLANNING REVISION 3
3	2022.11.04	PLANNING REVISION 2
2	2022.07.27	PLANNING REVISION 1
1	2022.02.11	PLANNING APPLICATION
0		ISSUE DESCRIPTION

1030 o'brien
 1030 O'Brien Drive
 Menlo Park, CA 94025

PROJECT NUMBER: 21-30
 DATE: 2023.01.19
LEVEL 2 FLOOR PLAN

A2.02



LEVEL 02

GROSS BUILDING AREA		
LEVEL 01	1317 SF	COMMERCIAL AREA
LEVEL 01	4328 SF	GROSS FLOOR AREA
LEVEL 02	11134 SF	GROSS FLOOR AREA
LEVEL 02	4194 SF	GROSS FLOOR AREA
LEVEL 03	26229 SF	GROSS FLOOR AREA
LEVEL 03	26229 SF	GROSS FLOOR AREA
TOTAL GROSS FLOOR AREA: 47988 SF		

FLOOR PLAN LEGEND	
	LOBBY
	OFFICE-LAB
	SERVICE
	VERTICAL CIRCULATION

FLOOR PLAN LEGEND	
	KEYNOTE TAG
	DESIGN BUILD STAIR, CONCRETE FILLED METAL PAN TREAD, HIGH PERFORMANCE PAINTED METAL RISER
	4\"/>
	CURTAIN WALL GLAZING SYSTEM AS SCHEDULED
	BIKE RACKS - DERIVED
	DRINKING FOUNTAIN W/ BOTTLE FILLER, S.P.D.
	BUILDING OUTLINE ABOVE, SHOWN DASHED

GENERAL FLOOR PLAN NOTES

- SEE 01.00 FOR GENERAL PROJECT INFORMATION.
- SEE 01.00 SERIES DRAWINGS FOR CODE ANALYSIS.
- SEE 02.00 SERIES DRAWINGS FOR AREA.
- SEE 03.00 SERIES DRAWINGS FOR PARKING DATA AND OPEN AREA.
- SEE 04.00 SERIES DRAWINGS FOR 3D VIEWS AND MATERIALITY.
- ALL OPEN JOISTS, PENETRATIONS AND OTHER OPENINGS IN THE BUILDING ENVELOPE SHALL BE SEALED, CAULKED, GASKETED AND WEATHER-STRIPPED TO LIMIT AIR LEAKAGE AND MAINTAIN RATED ASSEMBLIES.
- ALL CONCEALED WOOD BLOCKING SHALL BE FIRE RETARDANT TREATED, TYPICAL.
- PROVIDE THE REQUIRED SADDLING, BRACING, AND BLOCKING FOR ATTACHMENT OF CASEWORK, RESTROOM ACCESSORIES, EQUIPMENT, AND OTHER.
- ALL FURNITURE, APPLIANCES, AND TRASH CANS SHALL BE O.F.C.I., U.O.N.
- 4'-0" ELEVATION SHOWN ON THE FLOOR PLAN EQUALS DESIGN FLOOR ELEVATION -15' NAVD 83.
- PROTECT ADJACENT PROPERTY AS REQUIRED BY CBC SEC. 3366.
- ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPAIRED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.
- AN ENCROACHMENT PERMIT FROM THE ENGINEERING DIVISION IS REQUIRED PRIOR TO ANY CONSTRUCTION ACTIVITIES, INCLUDING UTILITY LATERALS, IN THE PUBLIC FRONT OF WAY.
- AS PART OF THE ZERO WASTE MANAGEMENT PLAN REQUIRED BY THE CITY OF MENLO PARK, RESTROOMS SHALL INCLUDE ENERGY EFFICIENT HAND DRYERS AND SHALL NOT INCLUDE PAPER TOWEL DISPENSERS. DRINKING FOUNTAINS SHALL INCLUDE BOTTLE FILLING STATIONS AT ALL LOCATIONS.

FLOOR PLAN LEGEND	
	WALL TAG, SEE A8.01 FOR ASSEMBLY DETAILS
	CURTAIN WALL TAG
	TRUNCATED DOMES
	FIRE EXTINGUISHING CABINET, SEE FCC TABLE
	FLOOR DRAIN
	ROOF DRAIN
FIRE EXTINGUISHER CABINET TYPES	
FEC-A	SURFACE MOUNTED
FEC-B	SEMI-RECESSED MOUNTED
FEC-C	RECESS MOUNTED
	SLAB STEP
	COLUMN
	WALL
	MECHANICAL SHAFT, OPEN TO BELOW

KEYNOTE LEGEND	
	KEYNOTE TAG
	DESIGN BUILD STAIR, CONCRETE FILLED METAL PAN TREAD, HIGH PERFORMANCE PAINTED METAL RISER
	4\"/>
	CURTAIN WALL GLAZING SYSTEM AS SCHEDULED
	BIKE RACKS - DERIVED
	DRINKING FOUNTAIN W/ BOTTLE FILLER, S.P.D.
	BUILDING OUTLINE ABOVE, SHOWN DASHED

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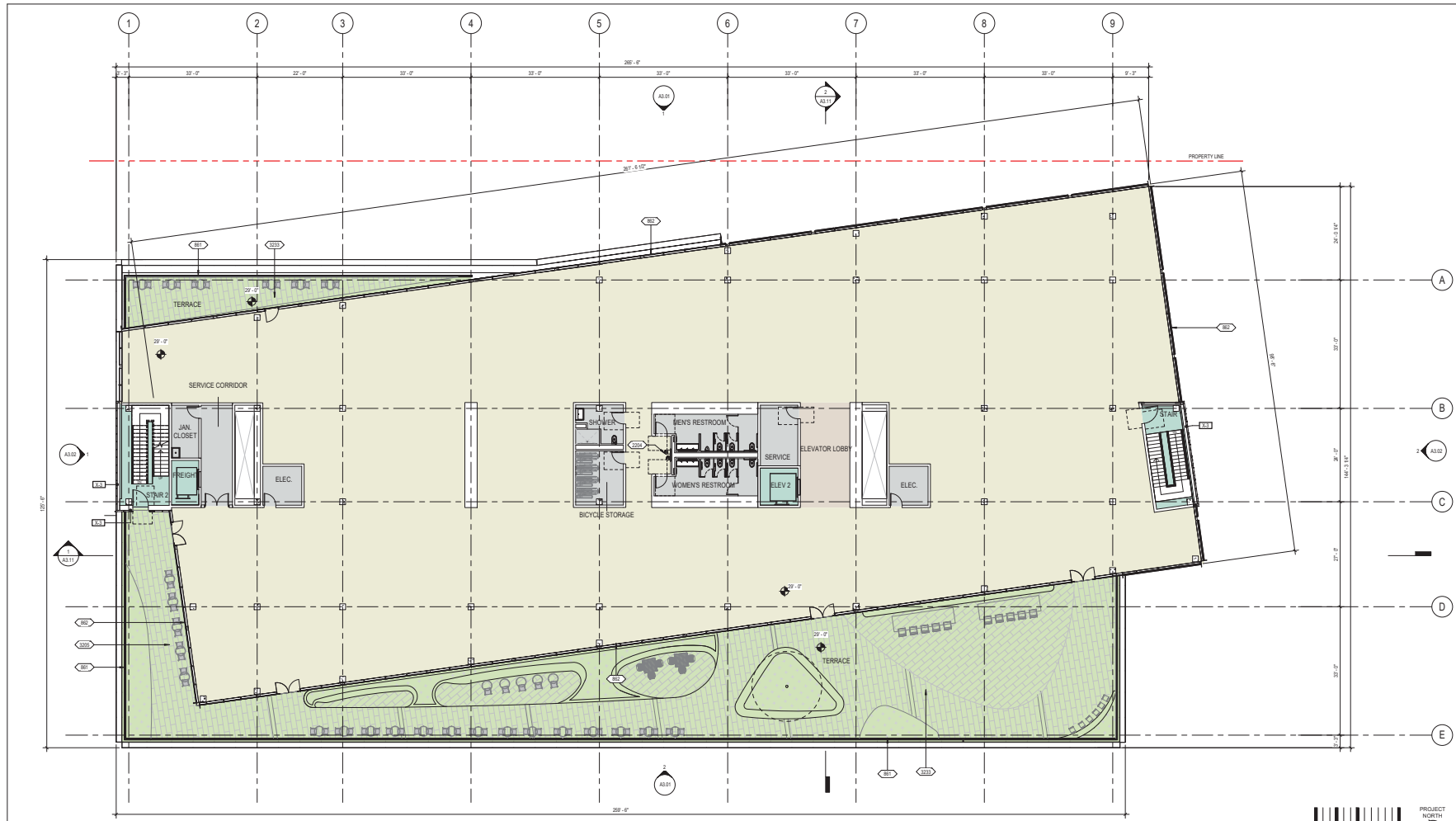
4	2023.01.19	PLANNING REVISION 3
3	2022.11.04	PLANNING REVISION 2
2	2022.07.27	PLANNING REVISION 1
1	2022.02.11	PLANNING APPLICATION
#	Date	Issue description

1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

PROJECT NUMBER: 21-05
DATE: 2023.01.19

LEVEL 3 FLOOR PLAN

A2.03



PROJECT NORTH

3/32" = 1'-0"

LEVEL 03

GROSS BUILDING AREA		
LEVEL 01	237 SF	COMMERCIAL AREA
LEVEL 01	458 SF	GROSS FLOOR AREA
LEVEL 02	1158 SF	GROSS FLOOR AREA
LEVEL 02	1194 SF	GROSS FLOOR AREA
LEVEL 03	2629 SF	GROSS FLOOR AREA
LEVEL 03	2629 SF	GROSS FLOOR AREA
TOTAL GROSS FLOOR AREA		6768 SF

FLOOR PLAN LEGEND
LOBBY
OFFICE/LAB
SERVICE
TERRACE
VERTICAL CIRCULATION

- GENERAL FLOOR PLAN NOTES
- SEE 01.00 FOR GENERAL PROJECT INFORMATION.
 - SEE 01.00 SERIES DRAWINGS FOR CODE ANALYSIS.
 - SEE 02.00 SERIES DRAWINGS FOR AREA.
 - SEE 03.00 SERIES DRAWINGS FOR DATA AND OPEN AREA.
 - SEE 04.00 SERIES DRAWINGS FOR 3D VIEWS AND MATERIALS.
 - ALL OPEN JOISTS, PENETRATIONS AND OTHER OPENINGS IN THE BUILDING ENVELOPE SHALL BE SEALED, CAULKED, GASKETED AND WEATHER-STRIPPED TO LIMIT AIR LEAKAGE AND MAINTAIN RATED ASSEMBLIES.
 - ALL CONCEALED WOOD BLOCKING SHALL BE FIRE RETARDANT TREATED, TYPICAL.
 - PROVIDE THE REQUIRED SCHEDING, BRACING, AND BLOCKING FOR ATTACHMENT OF CASEWORK, RESTROOM ACCESSORIES, EQUIPMENT, AND OTHER.
 - ALL FURNITURE, APPLIANCES, AND TRASH CANS SHALL BE O.F.C.I., U.O.N.
 - 4'-0" ELEVATION SHOWN ON THE FLOOR PLAN EQUALS DESIGN FLOOR ELEVATION -15.7 NAVD 88.
 - PROTECT ADJACENT PROPERTY AS REQUIRED BY CBC SEC. 3366.
 - ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPAIRED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.
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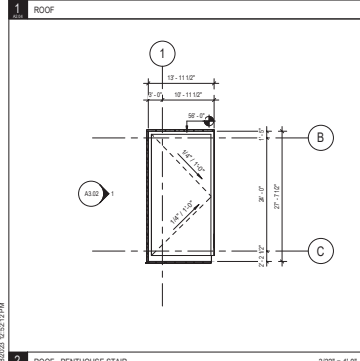
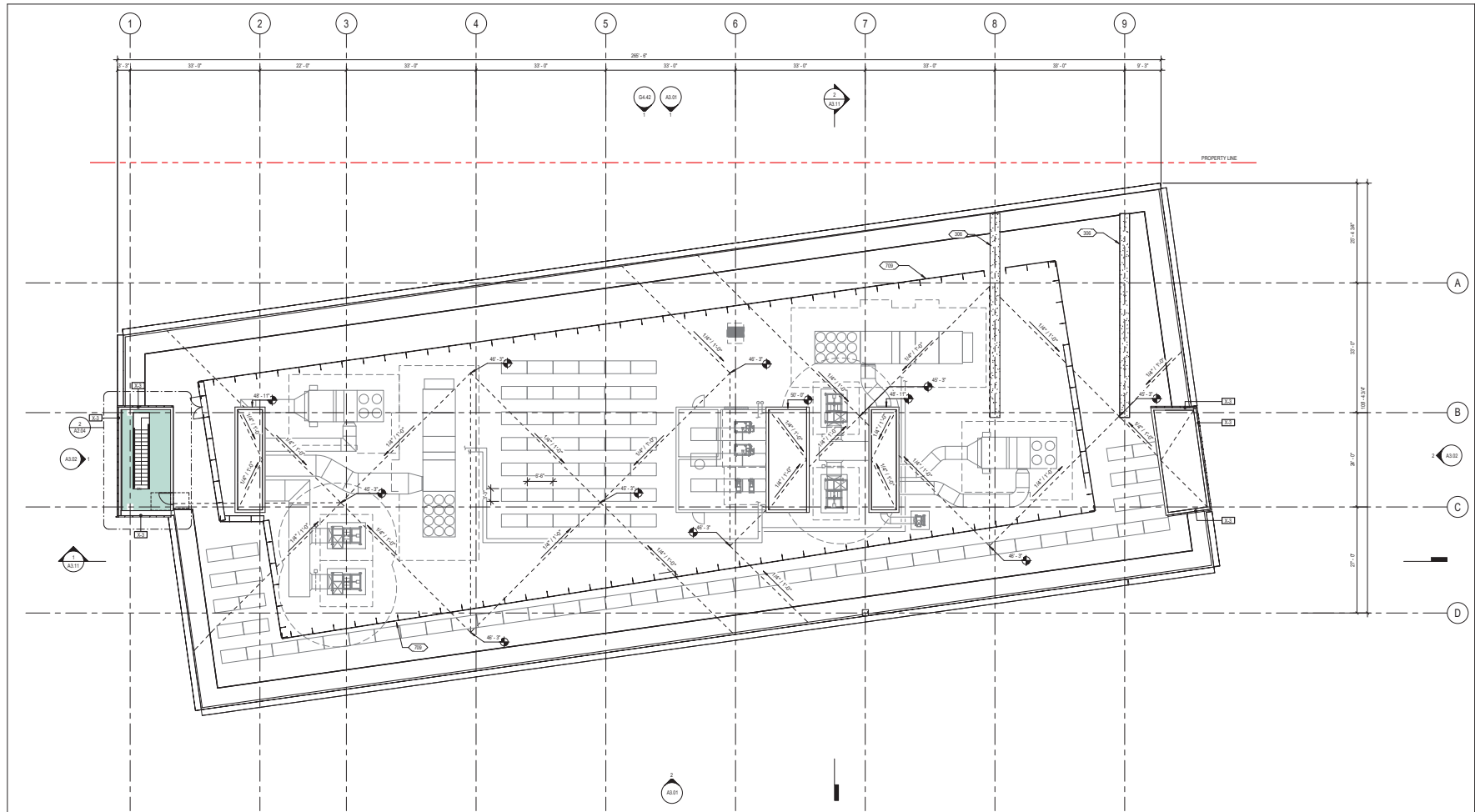
FLOOR PLAN LEGEND
WALL TAG, SEE A8.01 FOR ASSEMBLY DETAILS
CURTAIN WALL TAG
TRUNCATED DOMES
FIRE EXTINGUISHING CABINET, SEE FCC TABLE
FLOOR DRAIN
ROOF DRAIN
FIRE EXTINGUISHER CABINET TYPES
FCC-A SURFACE MOUNTED
FCC-B RECESSED MOUNTED
FCC-C RECESS MOUNTED
SLAB STEP
COLUMN
WALL
MECHANICAL SHAFT, OPEN TO BELOW

KEYNOTE LEGEND
KEYNOTE TAG
801 42" A.F.F. GLASS GUARDRAIL
802 CURTAIN WALL GLAZING SYSTEM AS SCHEDULED
2204 DRINKING FOUNTAIN W/ BOTTLE FILLER, S.P.D.
2205 ROOF, OUTLINE ABOVE
2201 PAVES, S.I.D.



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3/32" = 1'-0"

VERTICAL CIRCULATION

- GENERAL FLOOR PLAN NOTES
- SEE 04.00 FOR GENERAL PROJECT INFORMATION.
 - SEE 01.00 SERIES DRAWINGS FOR CODE ANALYSE.
 - SEE 02.00 SERIES DRAWINGS FOR AREA.
 - SEE 03.00 SERIES DRAWINGS PARKING DATA AND OPEN AREA.
 - SEE 04.00 SERIES DRAWINGS FOR 3D VIEWS AND MATERIALITY.
 - ALL OPEN JOINTS, PENETRATIONS AND OTHER OPENINGS IN THE BUILDING ENVELOPE SHALL BE SEALED, CAULKED, GASKETED AND WEATHER-STRIPPED TO LIMIT AIR LEAKAGE AND MAINTAIN RATED ASSEMBLIES.
 - ALL CONCEALED WOOD BLOCKING SHALL BE FIRE RETARDANT TREATED, TYPICAL.
 - PROVIDE THE REQUIRED SACKING, BRACING, AND BLOCKING FOR ATTACHMENT OF CASEWORK, RESTROOM ACCESSORIES, EQUIPMENT, AND OTHER.
 - ALL FURNITURE, APPLIANCES, AND TRASH CANS SHALL BE 0' F.C.I., U.O.N.
 - +0'-0" ELEVATION SHOWN ON THE FLOOR PLAN EQUALS DESIGN FLOOD ELEVATION -15.7 NAVD 88.
 - PROTECT ADJACENT PROPERTY AS REQUIRED BY CBC SEC. 3306.
 - ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPAIRED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.
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FLOOR PLAN LEGEND

	WALL TAG, SEE A8.01 FOR ASSEMBLY DETAILS
	CURTAIN WALL TAG
	TRUNCATED DOMES
	FIRE EXTINGUISHING CABINET, SEE FEC TABLE
	FLOOR DRAIN
	ROOF DRAIN
FIRE EXTINGUISHING CABINET TYPES	
	SURFACE MOUNTED
	SEMI-RECESSED MOUNTED
	RECESS MOUNTED
	SLAB STEP
	COLUMN
	WALL
	MECHANICAL SHAFT, OPEN TO BELOW

KEYNOTE LEGEND

	KEYNOTE TAG
	CONCRETE BEAM, S.S.D.
	PERF. METAL MECHANICAL SCREEN

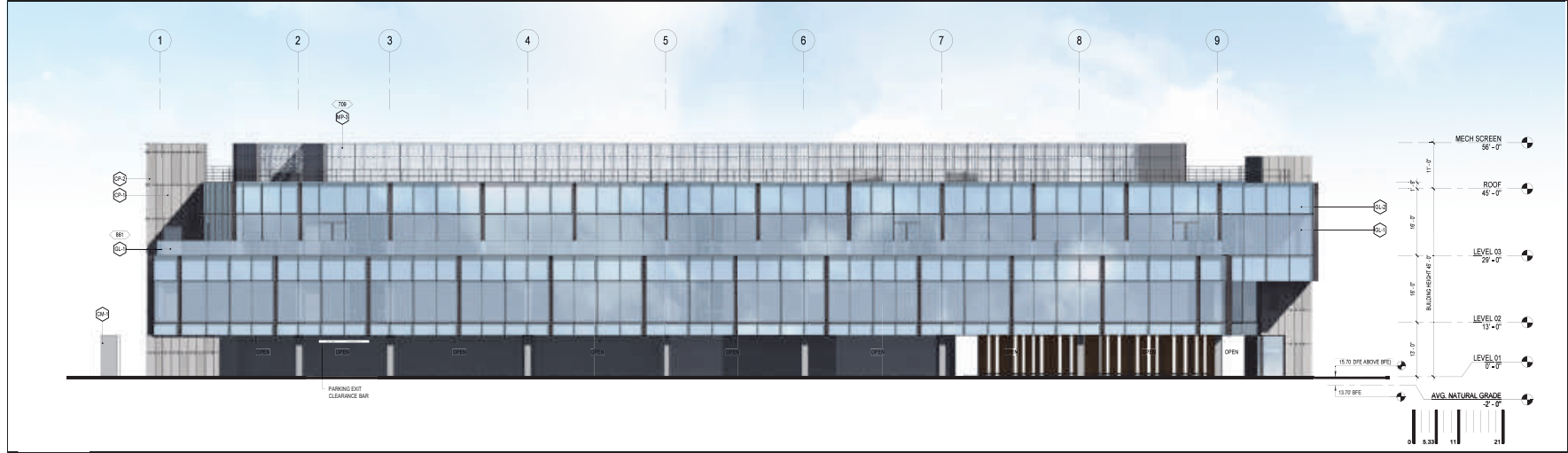
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NORTH ELEVATION

3/32" = 1'-0"

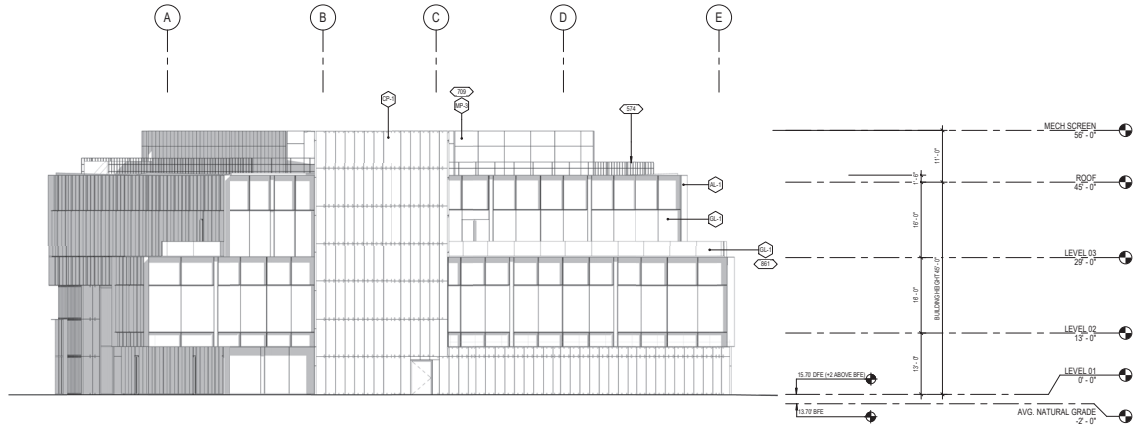


SOUTH ELEVATION

3/32" = 1'-0"

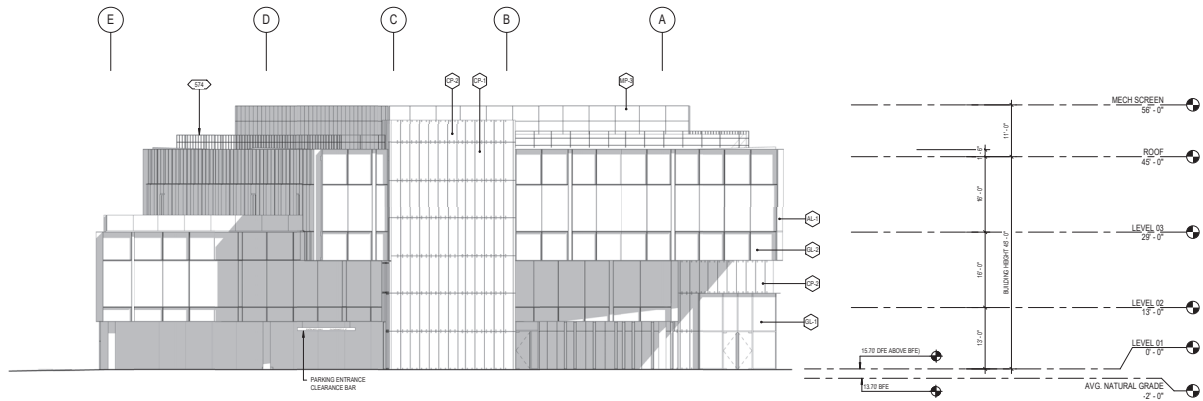
<p>1. TRANSPARENT GLAZING SHALL BE BIRD-FRIENDLY, FOLLOWING THE REQUIREMENTS OF HENLO PARK MUNICIPAL CODE SECTION 16.44.19 (B). TRANSPARENT GLAZING WILL INCLUDE COATINGS OR FRITS DESIGNED TO ALERT BIRDS TO AVOID COLLISIONS.</p>	<p>MATERIAL TAG</p> <ul style="list-style-type: none"> AC1 ARCHITECTURAL CONCRETE, TYPE 1 AC2 ARCHITECTURAL CONCRETE, TYPE 2 AL1 ALUMINUM - LIGHT COLOR FINISH CB1 CONCRETE MASONRY UNIT, TYPE 1 CP1 COMPOSITE PANEL, LIGHT COLOR FINISH CP2 COMPOSITE PANEL, DARK COLOR FINISH GL1 CLEAR GLAZING, TYPE 1 - BIRD-FRIENDLY FRIT GL2 OPAQUE SPRINGRILL GLASS - BIRD-FRIENDLY MM1 METAL WIRE MESH WP1 WOOD PANEL, CLEAR FINISH 	<p>KEYNOTE TAG</p> <ul style="list-style-type: none"> ET4 PFD STL OSHA COMPLIANT GUARDRAIL MS1 PFD METAL MECHANICAL SCREEN BS1 4" x 4" F. GLASS GUARDRAIL
<p>GENERAL NOTES</p>	<p>EXTERIOR MATERIAL LEGEND</p>	<p>KEYNOTE LEGEND</p>

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1. WEST ELEVATION

3/32" = 1'-0"



2. EAST ELEVATION

3/32" = 1'-0"

1. TRANSPARENT GLAZING SHALL BE BIRD-FRIENDLY, FOLLOWING THE REQUIREMENTS OF MENDO COUNTY MUNICIPAL CODE SECTION 18.44.19 (B). TRANSPARENT GLAZING WILL INCLUDE COATINGS OR FRITS DESIGNED TO ALERT BIRDS TO AVOID COLLISIONS.

MATERIAL TAG		KEYNOTE TAG	
AC1	ARCHITECTURAL CONCRETE, TYPE 1	84	PTD STL OSHA COMPLIANT GUARDRAIL
AC2	ARCHITECTURAL CONCRETE, TYPE 2	88	FSR METAL MECHANICAL SCREEN
AL1	ALUMINUM - LIGHT COLOR FINISH	81	4" x 4" F. GLASS GUARDRAIL
CB1	CONCRETE MASONRY UNIT TYPE 1		
CP1	COMPOSITE PANEL, LIGHT COLOR FINISH		
CP2	COMPOSITE PANEL, DARK COLOR FINISH		
GL1	CLEAR GLAZING, TYPE 1 - BIRD-FRIENDLY FRIT		
GL2	OPAQUE SPRINGED GLASS - BIRD-FRIENDLY		
MP1	METAL WIRE MESH		
WP1	WOOD PANEL, CLEAR FINISH		

STAMP HERE

1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

PROJECT NUMBER: 21-005
DATE: 2023.01.19

BUILDING ELEVATIONS

#	DATE	REVISION DESCRIPTION
4	2023.01.19	PLANNING REVISION 3
3	2022.11.04	PLANNING REVISION 2
2	2022.07.27	PLANNING REVISION 1
1	2022.02.11	PLANNING APPLICATION

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 02/23/2024



1 RENDERING - VIEW OF THE NORTHEAST CORNER



2 RENDERING - VIEW OF THE NORTHWEST CORNER



KEY PLAN - RENDERING VIEW LOCATIONS

brick.

CLIENT
 R+M Properties

ARCHITECT
 brick.
 402 1st Street, Suite 300
 Oakland, CA 94612
 916.263.9197
 www.brick-inc.com

CONSULTANT

△ Date revision description

4	2023-01-19	PLANNING REVISION 3
3	2022-11-04	PLANNING REVISION 2
2	2022-07-27	PLANNING REVISION 1
1	2022-02-11	PLANNING APPLICATION
#	Date	Issue description



1030 o'brien
 1030 O'Brien Drive
 Menlo Park, CA 94025

Project Number: 21-005
 Date: 2023-01-19

3D VIEWS

A3.03.

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 1030 o'brien 2/15/19



1 RENDERING - VIEW OF THE SOUTHEAST CORNER



2 RENDERING - VIEW OF THE SOUTHWEST CORNER



KEY PLAN - RENDERING VIEW LOCATIONS

brick.

CLIENT
R+M Properties

ARCHITECT
brick.
402 10th Street, Suite 300
Oakland, CA 94612
510.518.9197
www.brick-inc.com

CONSULTANT

△ Date revision description

- 4 2023.01.19 PLANNING REVISION 3
- 3 2022.11.04 PLANNING REVISION 2
- 2 2022.07.27 PLANNING REVISION 1
- 1 2022.02.11 PLANNING APPLICATION

Date Issue description



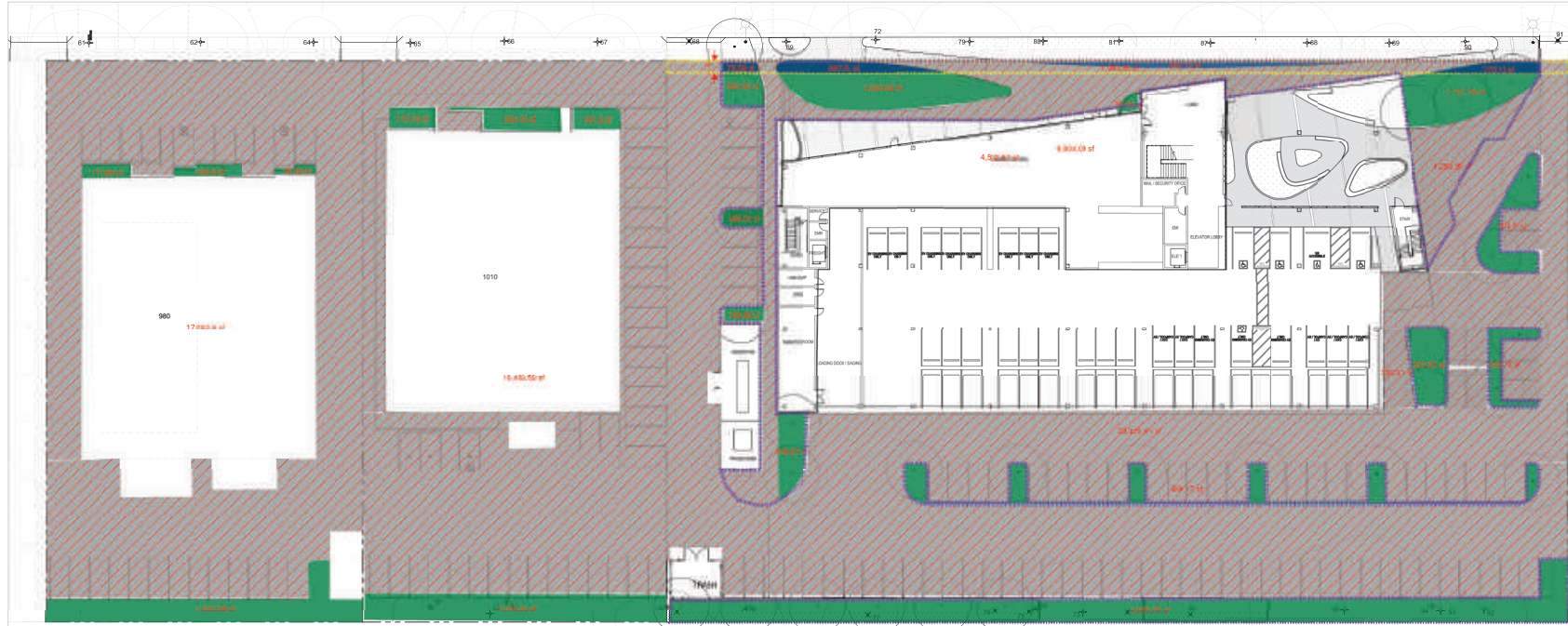
1030 o'brien
1030 O'Brien Drive
Menlo Park, CA 94025

Project Number: 21-20

Date: 2023.01.19

3D VIEWS

A3.04.



SCALE: 1"=40'-0"
0' 20' 40' 80'

990 O'BRIEN
TOTAL LOT AREA: 32,520 SF
TOTAL OPEN SPACE: 19,004 SF - 60%
PAVING - 17,262 SF - 93%
LANDSCAPE - 1,810 SF - 10%

1010 O'BRIEN
TOTAL LOT AREA: 31,200 SF
TOTAL OPEN SPACE: 19,237 SF - 62%
PAVING - 16,469 SF - 85%
LANDSCAPE - 209 SF - 11%

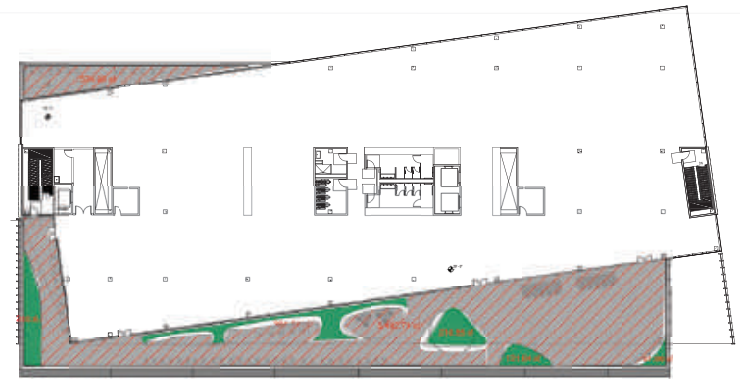
1020-30 O'BRIEN
TOTAL LOT AREA: 92,400 SF
TOTAL OPEN SPACE: 59,619 SF - 62%

PAVING - 48,163 SF - 81%
LANDSCAPE - 12,100 SF - 20%
FRONTAGE LANDSCAPE - 1,925 SF
FRONTAGE LANDSCAPE PLANTING - 816 SF - 42%

TOTAL OPEN SPACE REQUIRED

TOTAL LOT AREA FOR ALL 3 LOTS: 156,120 SF
TOTAL OPEN SPACE FOR ALL 3 LOTS: 31,244 SF
PUBLICLY ACCESSIBLE OPEN SPACE: 16,311 SF - 52% (EXCEEDS 50% REQUIREMENT)

1. L1-OPEN SPACE DIAGRAM



1020-30 O'BRIEN
TOTAL LOT AREA: 92,400 SF
TOTAL OPEN SPACE: 6,030 SF - 9%
PAVING - 6,504 SF - 80%
LANDSCAPE - 990 SF - 12%

2. L1-OPEN SPACE DIAGRAM

date revision description

- 4 2023.01.19 PLANNING REVISION
- 3 2023.01.04 PLANNING REVISION
- 2 2023.07.22 PLANNING REVISION
- 1 2023.02.11 PLANNING APPLICATION

date issue description



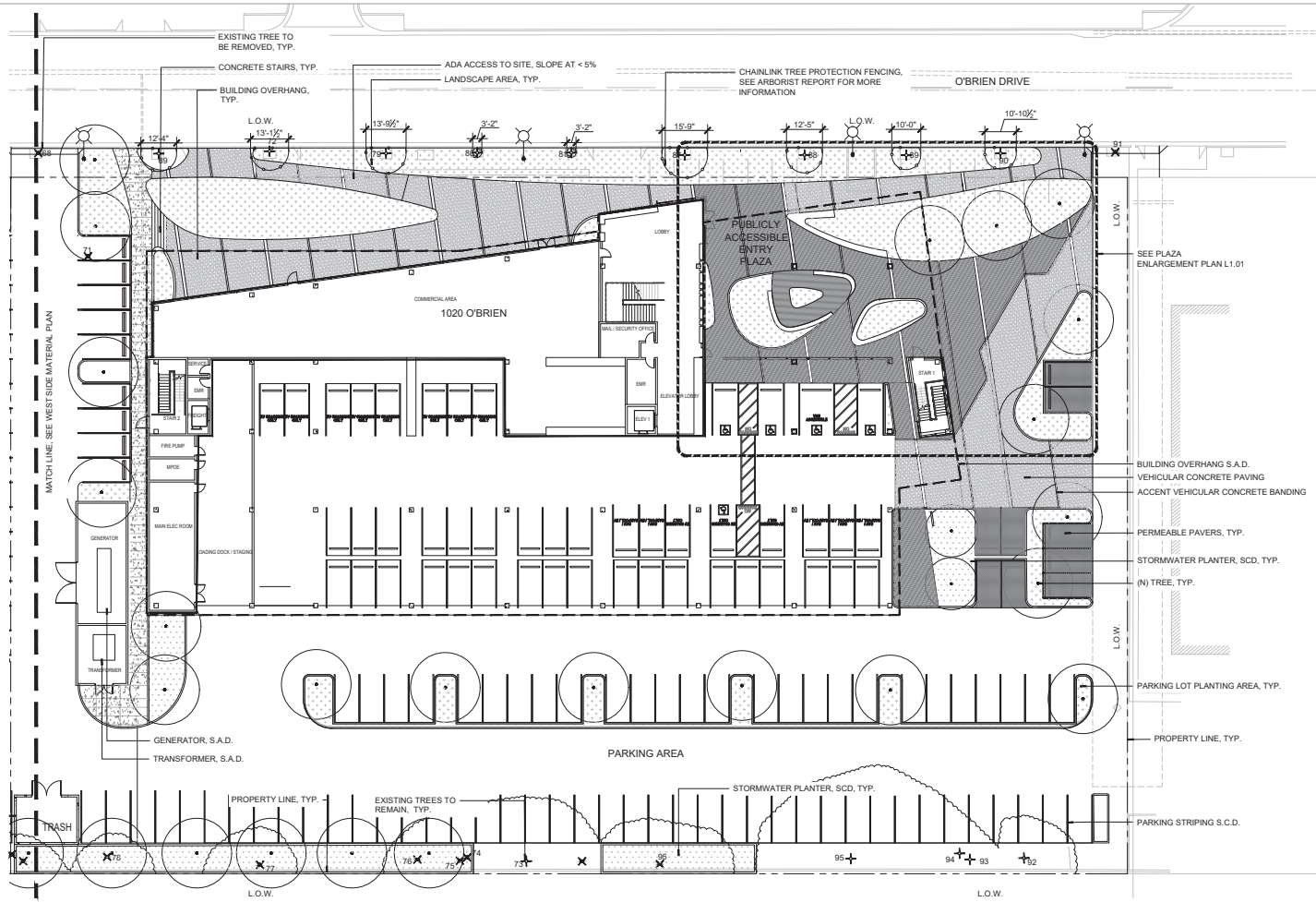
1030 O'Brien
1030 O'Brien Drive
West Park, CA 94593

PROJECT NUMBER: 24016
DATE: 2023.01.11

OPEN SPACE CALCULATIONS

City of Oakland, Department of Planning and Community Development, 1110 12th Street, Oakland, CA 94612
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 11/10/2023 11:07:32 AM

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1. EAST SIDE MATERIALS PLAN

SCALE: 1/16"=1'-0"

brick.

CLIENT
R+M Properties

ARCHITECT
brick.
400 West Street, Suite 900
Orange, CA 92667
949.636.9167
www.brick-inc.com

LANDSCAPE ARCHITECT
oreg
1030 O'Brien Drive
West Park, CA 92682

Δ	Date	revision description
1		

4	2023.01.19	PLANNING REVISION
3	2022.11.04	PLANNING REVISION
2	2022.07.12	PLANNING REVISION
1	2022.02.11	PLANNING APPLICATION



1030 O'Brien
1030 O'Brien Drive
West Park, CA 92682

project number: 21-015
date: 2022.02.11

L1.00 LANDSCAPE
GROUND LEVEL
MATERIALS PLAN

L1.00



CITY OF MENLO PARK

LOCATION MAP

980-1050 O'BRIEN DRIVE



CITY OF
MENLO PARK

Scale: 1:5,000

Drawn By: FNK

Checked By: DMC

Date: 2/28/2023





PROPOSAL

CEQA COMPLIANCE FOR LIFE SCIENCES DEVELOPMENT AT 980-10300'BRIEN AVENUE

CITY OF MENLO PARK

853 Lincoln Way Suite 208 / Auburn, CA 95603 / 530.887.8500

Cover Letter

November 28, 2022, revised January 6, 2023

Fahteen Khan
 Associate Planner, City of Menlo Park
 fnkhan@menlopark.org

Subject: CEQA Compliance for Life Sciences Development at 980-1030 O'Brien Drive

Dear Ms. Khan,

Dudek is excited for the opportunity to support the City of Menlo Park (City) by providing California Environmental Quality Act (CEQA) compliance services for the proposed Life Sciences Development at 980-1030 O'Brien Drive (project). Dudek's CEQA practitioners, planners, and technical experts are committed to helping California communities achieve their community planning and development goals while ensuring compliance with local, state, and federal planning and environmental regulations.

Responsive Project Management. Our Project Manager Katherine Waugh will lead the Dudek team and serve as the primary point of contact. Ms. Waugh has extensive experience preparing CEQA documents and managing environmental compliance efforts for development projects throughout the region, including two current projects in the City. She will be supported by Deputy Project Manager Kirsten Burrowes as well as technical leads with expertise in a wide range of disciplines to address the City's anticipated needs.

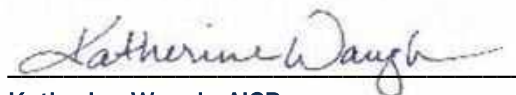
Local Experience and Expertise. Our team has provided similar environmental services for projects throughout the state. We provide the substantial evidence necessary to support a Lead Agency's CEQA compliance. When considering use of a Notice of Exemption, our in-house resource specialists prepare targeted technical studies to demonstrate that none of the exceptions to an exemption are triggered. Where an exemption is not possible, we will expand the range of technical studies to support an Initial Study that fully evaluates and mitigates environmental effects, where feasible.

We welcome any questions and look forward to the opportunity to further discuss our unique approach, capabilities, and proven track record with similar projects. If you have any questions, please feel free to contact Katherine Waugh at kwaugh@dudek.com or 530.863.4642.

Sincerely,



Joseph Monaco
 President and CEO
Joseph Monaco is authorized to sign on behalf of Dudek.



Katherine Waugh, AICP
 Project Manager



DUDEK AT A GLANCE

Multidisciplinary design, planning, engineering, and environmental services

700+ EMPLOYEES | **12** CALIFORNIA OFFICES

Employee-owned CALIFORNIA CORPORATION

TOP 120 U.S. ENVIRONMENTAL FIRMS (Engineering News-Record) | **Founded in 1980**

Table of Contents

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Project Team and Qualifications	3
Prior Projects and References.....	8
List of References	11
Approach, Work Plan, and Schedule	13
Additional Information	32
Pricing Proposal.....	34

TABLES

1	Key Staff Roles and Qualifications	4
2	References.....	11
3	Project Schedule	30

FIGURE

1	Dudek Team Organization	4
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APPENDIX

A	Resumes
---	---------

Project Team and Qualifications

We have assembled an in-house team that can provide all necessary services included in preparation of technical studies and the appropriate CEQA compliance document for the proposed Life Sciences Development at 980-1030 O'Brien Drive (project). Katherine Waugh will serve as the project manager and Kirsten Burrowes will serve as the deputy project manager; Ms. Waugh and Ms. Burrowes will be the City of Menlo Park's (City's) primary points of contact. Ms. Waugh is a senior planner with 23 years' experience in California Environmental Quality Act (CEQA) compliance, current planning methods, and environmental documentation. Ms. Burrowes is an environmental planner and project manager with 8 years' experience in CEQA, other types of environmental review, and resource permitting for a variety of projects throughout California and Nevada. Ms. Waugh and Ms. Burrowes will manage technical staff, reports, and preparation of environmental documentation. Further supporting the team as principal in charge will be Ann Sansevero, AICP. Ms. Sansevero has 36 years' experience in environmental assessment and land use planning for a broad range of public and private development projects, including preparation of CEQA/National Environmental Policy Act (NEPA) documents and other types of planning and environmental studies.

Figure 1 outlines the proposed organizational structure and lines of communication for this contract, followed by brief biographies for key personnel. Focused resumes for the key staff hand-picked for this project are provided in **Appendix A**.

Figure 1. Dudek Team Organization

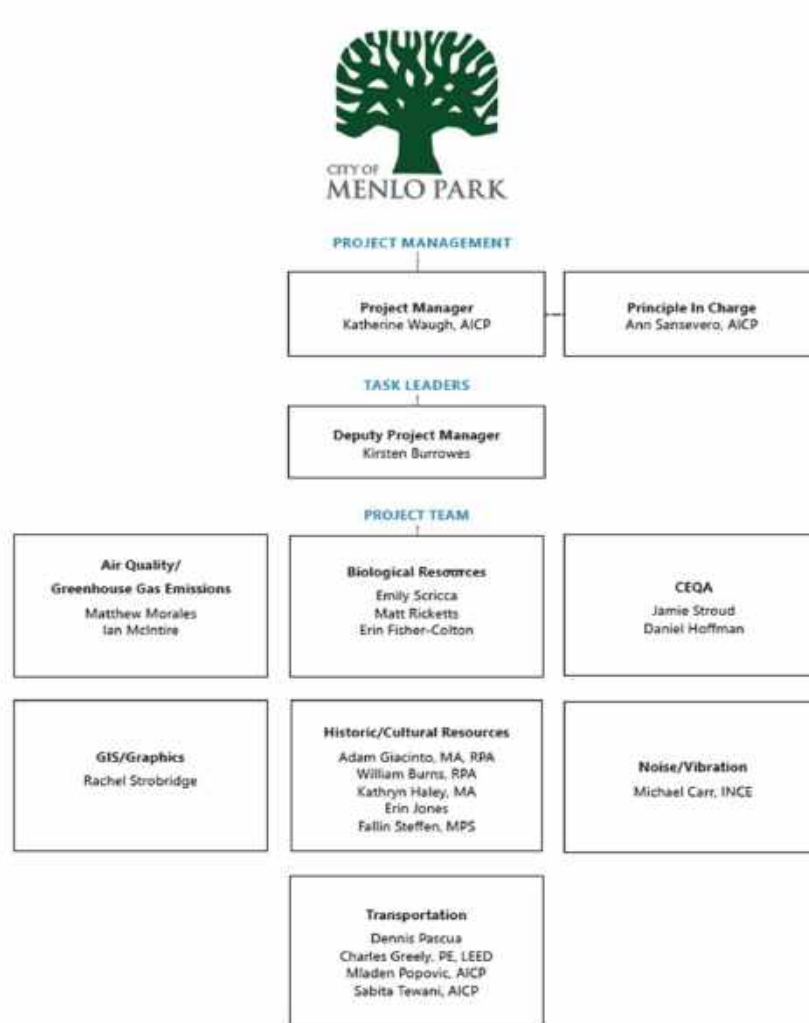


Table 1. Key Staff Roles and Qualifications

Key Staff Name and Role	Education and Experience
Katherine Waugh <i>Project Manager</i> Senior Specialist IV	University of California, Davis BS, Environmental Policy Analysis and Planning 23 years' experience in CEQA compliance and project management for public and private development and infrastructure throughout California, including industrial and office buildings and campuses, residential subdivisions, commercial uses, and public school modernization projects.
Ann Sansevero <i>Principal in Charge</i> Project Director/ Environmental Services	University of California, Santa Cruz BA, Biological Sciences and Environmental Studies American Institute of Certified Planners (AICP) Certified project manager and a certified planner with 36 years' experience in the field of environmental assessment and land use planning.
Kirsten Burrowes <i>Deputy Project Manager</i> Specialist III	Sonoma State University BA, Environmental Studies and Planning, German Language Minor 8 years CEQA compliance, environmental analysis, and mitigation monitoring

Table 1. Key Staff Roles and Qualifications

Key Staff Name and Role	Education and Experience
<p>Matthew Morales <i>Air Quality/Greenhouse Gas Emissions</i> Senior Specialist I</p>	<p>University of California, Davis BS, Environmental Toxicology 16 years' experience preparing air quality, greenhouse gas, and energy analysis for planning and environmental projects related to development, natural resource management, and facility expansion.</p>
<p>Ian McIntire <i>Air Quality/Greenhouse Gas Emissions</i> Specialist III</p>	<p>California State University, Sacramento BS, Environmental Studies 9 years' experience preparing air quality, greenhouse gas, and energy modeling and analysis, interpretation of state and federal legislation, environmental document preparation and review.</p>
<p>Matt Ricketts <i>Biological Resources</i> Senior Specialist II</p>	<p>Eastern Kentucky University MS, Biology/Applied Ecology University of Illinois at Urbana-Champaign BS, Natural Resources and Environmental Sciences U.S. Fish and Wildlife Service, ESA Section 10(a)(1)(A) Recovery Permit No. TE-61177B-0 21 years' experience as a wildlife biologist and conservation planner specializing in biological resource inventories and documentation, special-status species surveys, federal and California Endangered Species Act compliance, and environmental impact analysis.</p>
<p>Emily Scricca <i>Biological Resources</i> Specialist III</p>	<p>San Jose State University MS, Environmental Studies University of Vermont BS, Animal Sciences USFWS, Section 10(a)(1)(A) Native Endangered and Threatened Species Recovery Permit, No. TE45251C-0, California tiger salamander (<i>Ambystoma californiense</i>) CDFW, Memorandum of Understanding, Scientific Collecting Permit, No. SC-013755, California Tiger Salamander 12 years' experience with natural resource-related projects throughout California, including conducting protocol-level surveys, assessments, and biological monitoring for numerous species.</p>
<p>Erin Fisher-Colton, MS <i>Biological Resources</i> Analyst IV</p>	<p>San Jose State University MS, Environmental Studies California State Polytechnic University, Pomona BS, Environmental Biology Graduate Certificate in Environmental Planning, San Jose State University 3 years' experience providing biological surveying, monitoring, and mapping for nesting birds, special-status species, and rare plants and 5 years' related experience with California birds, mammals, reptiles, and amphibians.</p>
<p>Jamie Stroud <i>CEQA</i> Analyst I</p>	<p>University of Redlands BS, Environmental Science/Political Science Educational and professional experience preparing CEQA document sections for public and private development projects in the Bay Area and Tahoe region and in providing environmental education to elementary school students.</p>

Table 1. Key Staff Roles and Qualifications

Key Staff Name and Role	Education and Experience
<p>Daniel Hoffman <i>CEQA</i> Analyst V</p>	<p>Whitman College BA, Environmental Studies/Economics 5 years' experience in CEQA compliance, city and regional planning, permitting, and construction management.</p>
<p>Rachel Strobridge <i>GIS/Graphics</i> GIS Analyst II</p>	<p>California State University of Sacramento BA, Geography, Concentrations in Physical and Human Geography American River College, Certificate of Achievement, GIS 5 years' experience utilizing geospatial software to analyze data, organize and maintain spatial databases, create maps, and provide technical support to various environmental projects.</p>
<p>Adam Giacinto <i>Historic/Cultural Resources</i> Senior Specialist I</p>	<p>San Diego State University MA, Anthropology Sonoma State University BA, Anthropology/ Linguistics Santa Rosa Junior College AA, Anthropology Registered Professional Archaeologist (RPA) 15 years' experience preparing cultural resource reports and site records, and managing archaeological survey, evaluation, and data recovery-level investigations.</p>
<p>William Burns <i>Historic/Cultural Resources</i> Specialist I</p>	<p>University of York MS, Coastal and Marine Archaeology University of Massachusetts at Amherst BA, Anthropology (Mathematics minor) Register of Professional Archaeologists (RPA) 18 years' experience in cultural resource management, specializing in pre-contact archaeological sites, paleocoastline reconstruction, and artifact identification and analysis.</p>
<p>Kathryn Haley <i>Historic/Cultural Resources</i> Senior Specialist I</p>	<p>California State University, Sacramento MA, Public History BA, History 18 years' experience in architectural history assessments, historic/cultural resource management. historic research, field inventory, and site assessment.</p>
<p>Erin Jones <i>Historic/Cultural Resources</i> Analyst I</p>	<p>California State University, Sacramento MA, Public History with Honors University of Oregon BA, History 2 years of experience specializing in California, Oregon, and Washington history adept at context writing and the evaluation of historic properties.</p>
<p>Fallin E. Steffen <i>Historic/Cultural Resources</i> Analyst III</p>	<p>Tulane University, New Orleans MPS, Preservation Studies University of California, Santa Cruz BA, History of Art and Visual Culture 6 years' experience in historic preservation, architectural conservation, and cultural resource management in Northern California with experience conducting reconnaissance- and intensive-level surveys, preparation of resource-appropriate and</p>

Table 1. Key Staff Roles and Qualifications

Key Staff Name and Role	Education and Experience
	city-wide historic contexts, and historical significance evaluations for public and private development projects in both highly urbanized and rural areas. Previously served on the City of Santa Cruz Historic Preservation Commission.
<p>Michael Carr <i>Noise and Vibration</i> Specialist IV</p>	<p>Sierra College AS, Electronic Technology AS, Computer Technology Certificate in Mechatronic Systems AVIXA Certified Technology Specialist (CTS) 23 years' experience in acoustics and related industries, with an emphasis on environmental acoustics, noise, and vibration, for public and private development and transportation infrastructure projects.</p>
<p>Dennis Pascua <i>Transportation and Traffic</i> Project Director/ Environmental Services</p>	<p>University of California, Irvine BA, Social Ecology (Environmental Analysis and Design) Senior transportation planner with 28 years' experience in transportation planning/engineering, including traffic and circulation impact analyses and parking demand studies in both highly urbanized and rural areas, active transportation, context sensitive solutions, and complete streets.</p>
<p>Charles Greely <i>Transportation and Traffic</i> Principal Engineer II</p>	<p>University of Washington BS, Civil Engineering PE, CA No. 69056; WA No. 40823 South Coast Air Quality Management District Fugitive Dust Control Certification, No. 05-08-3112 LEED AP QSD, No. 69056 24 years' experience working with public and private entities on infrastructure improvement projects throughout California.</p>
<p>Mladen Popovic <i>Transportation and Traffic</i> Specialist I</p>	<p>University of California, Santa Barbara BA (with Honors), Geographic Information Systems BA (with Honors), Environmental Studies University of California, Irvine MS, Urban and Regional Planning AICP 7 years' experience evaluating transportation impacts for a wide variety of projects, including preparation of calculations and analysis for vehicle miles traveled, trip generation, level of service, signal warrants, construction traffic, internal circulation and access, and vehicle turning movements.</p>
<p>Sabita Tewani <i>Transportation and Traffic</i> Specialist IV</p>	<p>Newcastle University MSc, Transportation Engineering and Operations School of Planning and Architecture, New Delhi MA, Planning (Transportation specialization) BA, Architecture AICP 12 years' experience in transportation assessments for land use, transportation, and construction projects, specializing in vehicle miles traveled modeling and analysis.</p>

Prior Projects and References

The Dudek team offers experienced CEQA practitioners and technical experts with practical and directly applicable local, regional, and statewide experience, including for other office- and life-science-related development projects.

CITY OF MENLO PARK ENVIRONMENTAL REVIEW

123 Independence Drive Residential Development Environmental Impact Report: Dudek has prepared an Environmental Impact Report (EIR) evaluating a project that proposes to demolish five existing office/industrial buildings and construction of a 316-unit apartment building and 116 townhomes on an approximately 8-acre project site in the bayfront area of the City of Menlo Park.

1005 O'Brien Drive/1320 Willow Road Research and Development Project: Dudek is in the process of preparing a detailed IS and focused EIR evaluating the demolition of existing research and development buildings and the construction of two new research and development buildings, structured parking, and publicly accessible open space on an approximately 4-acre site in the eastern portion of the City of Menlo Park's bayfront area.

LOS ANGELES COUNTY DEPARTMENT OF HEALTH SERVICES

Willow Tree Permanent Supportive Housing NOE: The Willow Tree Permanent Supportive Housing project is located in the City of Compton within a building that was originally constructed as a motel in 1962 and was converted to an Interim Housing facility in 2020. The facility operator proposed to complete additional building and site modifications to repurpose the building for Permanent Supportive Housing for individuals who are experiencing homelessness or are at risk of becoming unhoused. Dudek prepared a Built Environment Inventory and Evaluation Report, conducted additional project site reviews and database searches, and prepared an NOE in support of the Los Angeles County Department of Health Services decision to allocate funding to this project. The NOE demonstrated that the project meets the State CEQA Guidelines for one Statutory Exemption as well as the State CEQA Guidelines and Los Angeles County CEQA Guidelines criteria for four Categorical Exemptions, and that none of the exceptions to the exemptions would occur. The project was approved on December 20, 2022.

ON-CALL CEQA CONSULTING SERVICES, PLEASANTON UNIFIED SCHOOL DISTRICT

Dudek prepared three NOEs for Pleasanton Unified School District's projects to add new classroom buildings to Amador Valley High School, Foothill High School, and Hart Middle School. The high school classroom buildings were each proposed as 2-story buildings with 10 to 12 new classrooms and support spaces, such as computer labs and a science preparation room. The middle school classroom is also a 2-story building that will provide 6 new science classrooms. The projects also included removal of portable classrooms at each campus. Dudek's work included the review of biological and hazardous materials databases, a cultural resource records search, and a field survey. Based on the results of these reviews, Dudek prepared an NOE for each project under CEQA Guidelines Class 1 Existing Facilities and Class 14 Minor Additions to Schools.

CITY OF PALO ALTO PLANNING AND ENVIRONMENTAL REVIEW SERVICES

Dudek has provided planning and environmental review services to the City of Palo Alto, including preparation of several EIRs and Mitigated Negative Declarations (MNDs). Dudek’s projects within the City of Palo Alto have included the following:



Castilleja School Project EIR: Dudek has prepared an EIR evaluating Castilleja School’s proposed program of facility modernization and requested amendment to the school’s Conditional Use Permit to increase the enrollment cap. The project proposes to demolish several existing structures, construct a below-grade parking garage, and construct a new academic building. Key issues include traffic, pedestrian and bicycle safety and access, noise, air pollution, tree removal, aesthetics, and other considerations of the compatibility of the project with the neighboring single-family residences.

3877 El Camino Real MND: Dudek prepared an Initial Study/Mitigated Negative Declaration (IS/MND) for the proposed demolition of a vacant commercial building and construction of a mixed-use development that would include retail, other commercial space, and 17 dwelling units. Key issues for the project included historic resources, traffic, aesthetics, and compatibility with the surrounding neighborhood.

1050 Page Mill Road EIR: Dudek prepared an EIR for the demolition of over 300,000 square feet of existing office/warehouse/research and development space and construction of the equivalent amount of dedicated office space. Key issues included defining the baseline condition as well as potential traffic and visual impacts to surrounding residential neighborhoods.

KAISER MEDICAL FOUNDATION

Roseville Kaiser Hospital Expansion Supplemental EIR: Dudek prepared a Supplemental EIR to analyze construction of a new hospital bed tower and parking garage for the existing Kaiser Hospital in the City of Roseville. The EIR tiered from the 2004 Hospital Expansion Project EIR and focused on the potential environmental effects resulting from any changed condition (i.e., changed circumstances, project changes, or new information of substantial importance). The Supplemental EIR evaluates aesthetics; air quality; greenhouse gases; land use; public utilities; and transportation and recommends feasible mitigation measures to reduce or avoid significant environmental impacts, including prior mitigation measures included in the 2004 Expansion Project EIR that remains applicable to the proposed project.

Roseville Kaiser Medical Office Building EIR: Dudek prepared an EIR to analyze the proposed Kaiser Riverside-Cirby Medical Office Building Project on a 14-acre site in the City of Roseville, adjacent to two major City roads and Interstate 80. The project entailed demolition of an existing Kaiser-Permanente medical office complex and parking facilities, which included six individual buildings constructed in the 1980s, and construction of two new medical office buildings – one that is five stories and one that is a single story – and a parking deck. The project also required construction of on-site and off-site public improvements, including improvements to City roads, new pedestrian facilities, and water, wastewater, and storm drainage infrastructure.

CALIFORNIA STATE UNIVERSITY CHICO SISKIYOU II SCIENCE REPLACEMENT BUILDING PROJECT

Dudek prepared an IS/MND for the Siskiyou II Science Replacement Building Project (project) in the southwest portion of the California State University (CSU) – Chico campus. The project included replacement of Siskiyou Hall, which was a single-story building constructed in 1957 as an industrial arts instructional facility with a four- to five-

story building (up to 85 feet in height and 135,000 square feet) to provide space for laboratories, support space, offices, classrooms, and a vivarium. The project also included various site improvements, such as landscaping, lighting, and utility connections. The project conformed to the CSU Chico 2005 Master Plan and associated EIR; therefore, the IS/MND for the project was tiered from the 2005 Master Plan EIR.

CITY OF CITRUS HEIGHTS CITY HALL AND MEDICAL OFFICE BUILDING EIR

Dudek prepared a Focused EIR to evaluate demolition of existing buildings and construction of a new Citrus Heights City Hall and a medical office building on two proximate sites. The City Hall had been located in repurposed buildings that suffered from deferred maintenance as well as general design limitations that impeded efficient and effective delivery of City services and functions. The City was approached by a major healthcare provider that had been searching for an appropriate location for a new medical office building. The Focused EIR evaluated construction of the medical office building on the original City Hall site and construction of the City Hall on a vacant site approximately 0.3 miles to the north, adjacent to an existing U.S. Post Office. Location of the medical office building along a major community corridor and adjacent to residential uses raised community concerns for aesthetic and noise impacts. Traffic, biological resources within the vacant site, and hazardous materials were also key project issues.



PLACER COUNTY GOVERNMENT CENTER MASTER PLAN EIR

Dudek provided environmental consulting services and EIR preparation for the Placer County Government Center Master Plan Update project. The project’s purpose was to develop a campus master plan update for the 200-acre Placer County Government Center. The adopted master plan update addresses future development needs at this government center, including demolition of buildings that are contributing features to a registered historic district, and construction of new public and private land uses in four major construction phases. The Master Plan Update anticipates that the site would support County offices and a mix of private office, commercial, and multifamily residential development. Dudek prepared several technical studies and an EIR that includes programmatic analysis of the overall Specific Plan as well as project-level analysis of the first two projects anticipated to be constructed (a 79-unit affordable housing project and construction of a new Health and Human Services building). The Board of Supervisors adopted the Specific Plan and certified the Final EIR in April 2019. Dudek also prepared a NEPA Environmental Assessment for construction of the affordable housing project and is continuing to work with the County in implementing mitigation measures required for individual construction projects throughout the campus.

NORTHSTAR CALIFORNIA BIOMASS ENERGY PLAN IS/MND

Northstar Community Services District: Dudek is working with the Northstar Community Services District to prepare an IS/MND evaluating their proposed development of a biomass energy plant, which would be connected to approximately 13 buildings and facilities within and adjacent to Northstar Village to provide heating and reduce annual natural gas consumption. Northstar Community Services District already conducts vegetation clearing for fuel management and defensible space and, until recently, has relied on pile burning, chipping, and spreading to dispose of the biomass collected through these programs. Under the proposed project, the biomass materials would be transported to the Tahoe Truckee Sierra Disposal Eastern Regional Landfill to be sorted. The materials that meet the biomass plant criteria would be returned to Northstar Community Services District for use in the plant. Critical project issues include air quality and aesthetics.

List of References

Table 2 includes our list of references for which the Dudek team has provided similar services.

Table 2. References

Client	Reference
City of Palo Alto Planning and Development Services Department	Amy French, Chief Planning Official 650.329.2336 amy.french@cityofpaloalto.org
County of Placer Department of Facilities Management	Paul Breckenridge, Capital Improvements Manager 530.889.6892 pbrecken@placer.ca.gov
City of Citrus Heights Community Development Department	Casey Kempenaar, Community Development Director 916.727.4740 ckempenaar@citrusheights.net
Northstar Community Services District	Eric Martin, District Engineer 530.550.6133 ericm@northstarcsd.org

Rate Schedule

Dudek's standard 2023 Schedule of Charges is presented below, followed by a detailed project-specific cost estimate that documents the individuals who would be involved in each task, their billing rates, and their allocated hours of work.

DUDEK 2023 Standard Schedule of Charges

Engineering Services		Hydrogeology/HazWaste Services	
Project Director	\$325.00/hr	Project Director	\$325.00/hr
Principal Engineer III	\$290.00/hr	Principal Hydrogeologist/Engineer II	\$295.00/hr
Principal Engineer II	\$280.00/hr	Principal Hydrogeologist/Engineer I	\$275.00/hr
Principal Engineer I	\$270.00/hr	Senior Hydrogeologist V/Engineer V	\$260.00/hr
Program Manager	\$260.00/hr	Senior Hydrogeologist IV/Engineer IV	\$250.00/hr
Senior Project Manager	\$260.00/hr	Senior Hydrogeologist III/Engineer III	\$240.00/hr
Project Manager	\$250.00/hr	Senior Hydrogeologist II/Engineer II	\$230.00/hr
Senior Engineer III	\$245.00/hr	Senior Hydrogeologist I/Engineer I	\$220.00/hr
Senior Engineer II	\$235.00/hr	Project Hydrogeologist V/Engineer V	\$205.00/hr
Senior Engineer I	\$225.00/hr	Project Hydrogeologist IV/Engineer IV	\$195.00/hr
Project Engineer IV/Technician IV	\$215.00/hr	Project Hydrogeologist III/Engineer III	\$185.00/hr
Project Engineer III/Technician III	\$205.00/hr	Project Hydrogeologist II/Engineer II	\$175.00/hr
Project Engineer II/Technician II	\$195.00/hr	Project Hydrogeologist I/Engineer I	\$165.00/hr
Project Engineer I/Technician I	\$175.00/hr	Hydrogeologist/Engineering Assistant	\$130.00/hr
Senior Designer II	\$195.00/hr		
Senior Designer I	\$190.00/hr	District Management & Operations	
Designer	\$180.00/hr	District General Manager	\$225.00/hr
Assistant Designer	\$175.00/hr	District Engineer	\$215.00/hr
CADD Operator III	\$170.00/hr	Operations Manager	\$165.00/hr
CADD Operator II	\$160.00/hr	District Secretary/Accountant	\$140.00/hr
CADD Operator I	\$145.00/hr	Collections System Manager	\$140.00/hr
CADD Drafter	\$130.00/hr	Grade V Operator	\$130.00/hr
CADD Technician	\$120.00/hr	Grade IV Operator	\$115.00/hr
Project Coordinator	\$150.00/hr	Grade III Operator	\$105.00/hr
Engineering Assistant	\$125.00/hr	Grade II Operator	\$85.00/hr
		Grade I Operator	\$80.00/hr
		Operator in Training	\$75.00/hr
		Collection Maintenance Worker	\$75.00/hr
Environmental Services		Creative Services	
Senior Project Director	\$300.00/hr	Creative Services IV	\$165.00/hr
Project Director	\$265.00/hr	Creative Services III	\$150.00/hr
Senior Specialist V	\$250.00/hr	Creative Services II	\$135.00/hr
Senior Specialist IV	\$235.00/hr	Creative Services I	\$120.00/hr
Senior Specialist III	\$225.00/hr		
Senior Specialist II	\$210.00/hr	Publications Services	
Senior Specialist I	\$200.00/hr	Technical Editor IV	\$165.00/hr
Specialist V	\$185.00/hr	Technical Editor III	\$160.00/hr
Specialist IV	\$175.00/hr	Technical Editor II	\$135.00/hr
Specialist III	\$165.00/hr	Technical Editor I	\$120.00/hr
Specialist II	\$155.00/hr	Publications Specialist IV	\$125.00/hr
Specialist I	\$145.00/hr	Publications Specialist III	\$115.00/hr
Analyst V	\$135.00/hr	Publications Specialist II	\$105.00/hr
Analyst IV	\$125.00/hr	Publications Specialist I	\$95.00/hr
Analyst III	\$115.00/hr	Clerical Administration	\$90.00/hr
Analyst II	\$105.00/hr		
Analyst I	\$95.00/hr		
Technician III	\$85.00/hr		
Technician II	\$75.00/hr		
Technician I	\$65.00/hr		
Mapping and Surveying Services		Expert Witness – Court appearances, depositions, and interrogatories as expert witness will be billed at 2.00 times normal rates.	
Application Developer II	\$195.00/hr	Emergency and Holidays – Minimum charge of two hours will be billed at 1.75 times the normal rate.	
Application Developer I	\$185.00/hr	Material and Outside Services – Subcontractors, rental of special equipment, special reproductions and blueprinting, outside data processing and computer services, etc., are charged at 1.15 times the direct cost.	
GIS Analyst V	\$205.00/hr	Travel Expenses – Mileage at current IRS allowable rates. For diem where overnight stay is required, as charged at cost.	
GIS Analyst IV	\$165.00/hr	Invoices, Late Charges – All fees will be billed to Client monthly and shall be due and payable upon receipt. Invoices are delinquent if not paid within 30 days from the date of the invoice. Client agrees to pay a monthly late charge equal to 1% per month of the outstanding balance until paid in full.	
GIS Analyst III	\$145.00/hr	Annual Increases – Unless identified otherwise, these standard rates will increase in line with the CPI-U for the nearest urban area per the Department of Labor Statistics to where the work is being completed or by 3% annually, whichever is higher.	
GIS Analyst II	\$130.00/hr	The rates listed above assume prevailing wage rates does not apply. If this assumption is incorrect, Dudek reserves the right to adjust its rates accordingly.	
GIS Analyst I	\$115.00/hr		
UAS Pilot	\$115.00/hr		
Survey Lead	\$185.00/hr		
Survey Manager	\$145.00/hr		
Survey Crew Chief	\$120.00/hr		
Survey Rod Person	\$95.00/hr		
Survey Mapping Technician	\$95.00/hr		
Construction Management Services			
Principal/Manager	\$195.00/hr		
Senior Construction Manager	\$185.00/hr		
Senior Project Manager	\$175.00/hr		
Construction Manager	\$170.00/hr		
Project Manager	\$165.00/hr		
Resident Engineer	\$160.00/hr		
Construction Engineer	\$155.00/hr		
On-site Owner's Representative	\$145.00/hr		
Prevailing Wage Inspector	\$145.00/hr		
Construction Inspector	\$140.00/hr		
Administrator/Labor Compliance	\$100.00/hr		

Approach, Work Plan, and Schedule

The following Work Plan identifies three phases of work – the initial phase includes preparation of a Project Description and those technical studies necessary to determine whether the project meets the criteria for one or more classes of Categorical Exemptions. Based on the results of the technical studies, Dudek will consult with the City to determine the CEQA compliance approach for the project. If a Notice of Exemption (NOE) can be supported, Dudek will complete the second phase of the Work Plan, resulting in preparation and filing of an NOE. If an NOE cannot be supported, Dudek will complete the third phase of the Work Plan, resulting in preparation, public review, responses to comments, and filing of an IS/MND. If it is determined that a Focused EIR is necessary, Dudek will submit a supplemental Work Plan and budget.

Phase A – Project Description and Technical Studies

Task A1: Project Initiation and Project Description

Project Initiation

Upon execution of the contract, the Dudek project manager and deputy project manager will attend a virtual project initiation meeting with City staff and the project applicant representative, if invited by the City. This meeting will be critical to the ultimate success of the project, as it provides the project team an opportunity to discuss and review the scope of the project, formalize key project assumptions, and define key milestones and other success factors for the project. This meeting will also offer an opportunity to confirm document format requirements, points of contact, status report details, and any other logistical, technical, or procedural concerns. We approach every project with the understanding that attention on the front end of a project can save substantial time and costs in the long run.



Dudek will also conduct a site visit to observe existing conditions in the project vicinity and review applicable background and technical data for the project area, such as the ConnectMenlo General Plan and EIR. From this review, Dudek will identify applicable policies and standards that will be cited in the EIR as portions of the regulatory framework governing impact analysis for this project. A data request memorandum will be prepared to identify any remaining information/data needs required to prepare the project description as well as to inform technical studies and sections of the IS.

Project Description and Data Request

Dudek will prepare a project description for use in the IS. It will include the planning and environmental context for the project and project site, including documenting the existing land uses and condition of the site, providing a detailed description of the project components, and identifying general construction logistics and schedule. The draft project description will be submitted to the City and the project applicant for review and comment, and Dudek will revise the project description as necessary.

The approved project description will be used as the basis for all project analyses. Minor revisions to the project description are anticipated as part of the IS process; however, major changes could substantially affect impact analyses. Any changes to the project description that require revisions to completed or in-progress tasks could represent additional costs not included in the proposed budget.

Task A1 Deliverables

- Data Request Memo (submitted electronically [MS Word])
- Draft Project Description (submitted electronically [MS Word])

Task A2: Technical Studies

Dudek will prepare technical studies and peer reviews of applicant-provided technical studies to develop the substantial evidence which will inform the determination of whether a Notice of Exemption can be supported or if completion of an IS is necessary. Under all exemptions, it must be demonstrated that there is no potential for significant cumulative impacts, there are no unusual circumstances that could result in a project having a significant environmental effect, the project would not result in damage to scenic resources within a state scenic highway, the project is not located on a hazardous waste site, and the project would not cause a substantial adverse change to a historical resource. Further, under the Class 32 Exemption, it must be demonstrated that the project site has no value as habitat for endangered, rare, or threatened species and that the project would not result in any significant impacts related to transportation, noise, air quality, and water quality.

Task A2.1: Air Quality, Greenhouse Gas, and Energy Assessment

Air Quality

Dudek will prepare an assessment of the air quality impacts of the project utilizing the significance thresholds in Appendix G of the CEQA Guidelines and the Bay Area Air Quality Management District (BAAQMD) emissions-based thresholds as the basis. After reviewing all available project materials, Dudek will prepare a request for any outstanding data needed to conduct the analysis. If precise information on a particular factor is not available from the applicant or its representatives, Dudek will make every effort to quantify these items using the best available information for comparable data sources, but in all cases, will consult first with the applicant regarding the information needed.

Dudek will estimate criteria air pollutant emissions associated with the construction phase of the project using the California Emissions Estimator Model (CalEEMod). The analysis of short-term construction emissions will be based on scheduling information (e.g., overall construction duration, phasing, and phase timing) and probable construction activities (e.g., construction equipment type and quantity, workers, and haul trucks) developed by the applicant, its representatives, and/or standardized approaches. Dudek will then evaluate the significance of the construction emissions based on the BAAQMD significance criteria.



The project would result in a short-term and long-term increase in toxic air contaminant (TAC) emissions related to construction and operation. Based on the project’s proximity (i.e., less than 1,000 feet) to existing residences and the Cesar Chavez Ravenswood Middle School, an analysis of construction TAC emissions and associated health risk is recommended, as described under the Optional Tasks AQ-1, below. If a dispersion model construction and/or operational health risk assessment (HRA) is not required to evaluate the potential health risk the project

may have on nearby sensitive receptors, Dudek will qualitatively address the potential for the project to expose sensitive receptors to substantial TACs in the IS/MND.

Additionally, the project would result in long-term sources of TACs, particularly from the diesel emergency generator; therefore, the HRA described under Optional Task AQ-2 is strongly recommended. If a dispersion model HRA is not chosen by the City to evaluate the potential health risk the project may have on nearby sensitive receptors, Dudek will qualitatively address the potential for the project to expose sensitive receptors to substantial TACs analysis.

CalEEMod will also be used to estimate criteria air pollutant emissions generated by mobile, energy, and area sources for operation of the new research and development/commercial building, as well as anticipated stationary source equipment (emergency generator, etc.). Dudek will estimate mobile source emissions using the trip generation rates and additional necessary trip characteristics provided in the Transportation Impact Analysis (TIA) report to be prepared for the project. Energy and area source emissions will be estimated using project-specific information if available, or default values in CalEEMod for the proposed land use. If applicable, Dudek will estimate the emissions of the existing uses and compare the project's net emissions to the BAAQMD significance thresholds.

Dudek will evaluate whether traffic associated with the project could lead to potential exposure of sensitive receptors to substantial concentrations of air pollutant emissions, specifically carbon monoxide hotspots, based on the project's traffic impact analysis and the criteria recommended by the BAAQMD. Dudek anticipates that the project would not exceed the BAAQMD carbon monoxide hotspots criteria and that a qualitative analysis will be adequate.

Additional Appendix G thresholds will also be evaluated, including the potential for the project to expose sensitive receptors to substantial pollutant concentrations, to result in other emissions such as odors, or to impede attainment of the current BAAQMD air quality management plan. Details of the analysis (e.g., daily criteria air pollutant emission calculations) will be included in an appendix to the IS/MND.

Optional Task AQ-1: Construction HRA. The main contaminant of concern associated with construction activities is diesel particulate matter (DPM), which has been listed as a TAC by the California Air Resources Board (CARB). Dudek will evaluate the project's potential health risks associated with construction activities using an appropriate exposure period to evaluate short-term emissions increases. The dispersion of DPM will be modeled using the American Meteorological Society/Environmental Protection Agency Regulatory Model (AERMOD) dispersion model and the CARB Hot Spots Analysis and Reporting Program Version 2 (HARP2), along with meteorological data provided by BAAQMD for the project area. HARP2 performs health impact calculations based on the Office of Environmental Health Hazards Assessment's Air Toxics Hot Spots Program Guidance Manual for Preparation of Health Risk Assessments (Health Risk Assessment Guidance Manual). Additionally, fine particulate matter (PM_{2.5}), which can pose a localized health threat to sensitive receptors at relatively low concentrations, will be estimated. The results will be compared to BAAQMD thresholds, and a summary of the methodology and results will be provided in the air quality section of the IS/MND. Costs for this optional task are included in the Contingency Tasks portion of our proposed budget.

Optional Task AQ-2: Operation HRA. Sources of TACs during operations that would be assessed in the operational HRA would include stationary sources, such as the diesel-fueled emergency generator. The dispersion of TACs and their health risk impacts for sensitive receptors proximate to the project will be modeled using a combination of AERMOD and the CARB HARP2 programs, along with meteorological data provided by BAAQMD for the project area, the site plan to determine the location of the sources, and the estimated TAC emissions. As with the construction HRA, operational health impact calculations using HARP2 will be based on the Health Risk Assessment Guidance Manual. The maximum cancer risks, hazard indices, and PM_{2.5} concentrations (if emitted)

at the appropriate receptors will be tabulated. If the health impacts exceed the thresholds of significance, we will suggest appropriate mitigation measures to reduce the health impacts. The modeling files will be included as an appendix and a summary of the methodology and results will be provided in the IS/MND. Notably, our budget includes health risk analysis for one stationary source during project operations. Costs for this optional task are included in the Contingency Tasks portion of our proposed budget.

Greenhouse Gas Emissions

The greenhouse gas (GHG) emissions assessment will include a brief description of global climate change and a summary of key, applicable regulatory measures. Dudek will estimate the GHG emissions associated with construction of the project using CalEEMod based on the same construction scenario utilized in the air quality analysis. Project-generated operational GHG emissions that will be estimated will include those associated with area sources, electrical generation, water supply, wastewater, solid waste disposal, and stationary sources. When project details are not available, CalEEMod default values will be used to calculate direct and indirect source GHG emissions. Details of the analysis (e.g., annual GHG emission calculations) will be included in an appendix to the IS/MND.

The City has an adopted their 2030 Climate Action Plan (CAP), which was approved in April 2021. Dudek will discuss how the project complies with the City’s CAP, state regulations (Assembly Bill [AB] 32); the Plan Bay Area; and applicable laws and regulations that would increase energy efficiency, such as the California Building Code. In addition, since the City does not have a quantitative threshold for post-2020 development, Dudek will work with the City to calculate a scaled project-specific threshold for GHGs based on the anticipated buildout year of the project, the latest City inventories, and the City and/or state reduction goals. Dudek will also analyze potential on-site and/or off-site renewable energy options available for the project to reduce GHG emissions associated with natural gas usage from the project. This calculated threshold will be used to determine whether the project GHG emissions are significant.

Energy Assessment

Dudek will prepare an energy assessment for the project per Appendix G of the CEQA Guidelines. Based on Appendix G of the CEQA Guidelines, the impact analysis will assess if the project would (1) result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation, and (2) conflict with or obstruct a state or local plan for renewable energy or energy efficiency. The project will be assessed in regard to construction and operational energy consumption, which will be quantified to the extent estimation methods and project-specifics are available.

Project electricity (kilowatt-hours, kWh) and/or natural gas (British thermal units, BTU) usage will be estimated based on project specifics; CalEEMod default values will be used, as appropriate, when project specifics are not available. Petroleum consumption will be estimated using CalEEMod and based on the same equipment and vehicle assumptions assumed in the air quality and GHG emissions analysis. The net increase in energy (i.e., project minus existing consumption) will be presented in the IS/MND and details of the analysis will be included in an appendix. Project elements that would reduce the project’s energy demand during construction and operations will be identified in the analysis and quantified as available. Dudek assumes that the applicant or its representatives will provide a list of the project’s energy conservation measures prior to initiating air quality and GHG emissions modeling, as the energy analysis will be prepared consistent with the emissions modeling assumptions.



Task A2.2: Biological Resources Technical Report

Dudek biologists will conduct a biological resources evaluation of the study area and immediate vicinity to support CEQA compliance. The biological technical report (BTR) will provide a discussion of land covers and habitat types on the site, as well as potentially occurring special-status species and other sensitive resources on or in the vicinity of the project site. The BTR will identify and evaluate biological resource issues and potential constraints associated with development of the proposed project. The study area contains approximately 3.58-acres of existing commercial buildings, parking lots, associated paved sidewalks, walkways, and ornamental landscaping, including several trees. This work will involve the following subtasks:

Literature Review. Dudek will conduct a literature review to identify sensitive biological resources known to occur in the study area vicinity. The literature review will include a search of the California Department of Fish and Wildlife's (CDFW) California Natural Diversity Database, the U.S. Fish and Wildlife Service's (USFWS) Information for Planning and Consultation online planning tool, and the California Native Plant Society's Inventory of Rare and Endangered Plants of California. Other public biological data sources (e.g., iNaturalist, eBird) and available biological reports or CEQA documents from other projects in the vicinity will also be reviewed, as appropriate.

Field Reconnaissance. A Dudek wildlife biologist will conduct a one-day, reconnaissance-level site visit to document existing biological resources (e.g., vegetation or land cover types, wildlife habitat) and assess the potential for special-status species to occur. No sensitive vegetation communities or jurisdictional aquatic resources (e.g., wetlands) are expected to occur because of the site's location within an area historically developed for industrial, research and development, warehousing, and office space land uses. Based on Dudek's experience with similar projects in the area and a review of Google Earth aerial imagery, potential biological resources include trees and shrubs that provide habitat for nesting birds and tree-roosting bats, buildings that may provide habitat for roosting bats, and trees that may be protected under the City's heritage tree ordinance (Municipal Code Chapter 13.24, July 2020).

Report (BTR). Based on the results of the literature review and site visit, Dudek biologists will prepare a stand-alone, BTR. The BTR will provide information on existing vegetation or land cover types, wildlife habitat, special-status species occurrences, and suitability of the site for special-status plant and animal species. Maps and figures showing the location/extent of any sensitive special-status species occurrences or suitable habitat identified on the property will be prepared for incorporation into the BTR. The BTR is intended to provide a foundation of information regarding biological resources on the property that can be used for future CEQA analysis and documentation. It will review the implications of any sensitive biological resources documented, including recommendations for avoidance, other measures to reduce impacts, and the need for regulatory permitting. It will also include analysis of the proposed project's compliance with the City's Heritage Tree Ordinance and Chapter 16.44.130(6) of the Municipal Code concerning bird-friendly designs for buildings.

Task A2.3: Cultural and Historic Resources

Archaeological Resources Study

Dudek will conduct a Phase I Cultural Resources Study of the proposed study area to assess the presence or absence of potentially significant prehistoric and historic-era sites in accordance with CEQA. This study will consist of a review of literature and site records on file with the Northwest Information Center (NWIC) for the project area and a 0.5-mile radius surrounding it, followed by an intensive pedestrian survey of the project site.



It is assumed that the direct cost for the NWIC records search will not exceed \$1,200. In the event that direct costs for the NWIC records search exceeds \$1,200, the City will be immediately notified. Archival information, including historic maps for the area, will also be reviewed. The archival study involves a review of previous cultural resources investigations performed within the records search limits.

Dudek will initiate correspondence with the Native American Heritage Commission (NAHC) to request a search of the Sacred Lands File for any known Native American resources identified within the project site. As part of the results of this search, the NAHC will provide a Contact List of tribal individuals and organizations that may have additional information concerning resources in the vicinity. No follow up communications will be initiated by Dudek with NAHC-listed tribes. It is assumed that formal consultation with Native American tribes pursuant to AB 52 will be completed by City staff.

Pedestrian survey boundaries will be confirmed while in the field by using GPS receiver with sub-meter accuracy, loaded with project files. An archaeologist qualified in prehistoric and historic-era resources will participate in the survey to document sites and features. The survey archaeologist will meet or exceed the applicable Secretary of Interior Professional Qualifications Standard for archaeological survey. The archaeologist will survey the entire project site walking transects spaced no more than 15 meters apart and oriented along cardinal directions. Evidence for buried cultural deposits will opportunistically be sought through inspection of natural or artificial erosion/excavation exposures and the spoils from rodent burrows. Field recording and photo documentation of resources, as appropriate, will be completed.

The present scope anticipates one archaeologist will be able to completely survey the project site in no more than one day of survey, and that no Native American monitor is required during the survey. Dudek assumes that no archaeological resources will be identified within the survey area. Should such resources be identified, they shall be subject to field inventory-level documentation sufficient to prepare a Department of Parks and Recreation (DPR) 523 Site Record Form. Preparation of a DPR form would be considered outside of the present scope of work. Should additional field documentation of identified resources be required to evaluate these resources for California Register of Historical Resources (CRHR) listing, Dudek will work with the City to document the cost and scope to support these efforts, which may be accommodated under the general contingency budget for this project.

Upon completion of the survey, an archaeological resources assessment letter report will be prepared for submittal and review by the City. This report will consist of a description of the project's natural and cultural setting, study methods, results, potential impacts, and mitigation recommendations. Following review and comment, Dudek will prepare the final technical report for incorporation into the CEQA documentation.

Assembly Bill 52 Support

The proposed project is subject to compliance with AB 52, which requires lead agencies to provide tribes (who have requested notification) with early notification of the proposed project and, if requested, consultation to inform the CEQA process with respect to tribal cultural resources. While AB 52 is a government-to-government process between the CEQA lead agency and California Native American Tribes, Dudek will assist the City with the notification process, including preparation of AB 52 template letters, as well as guidance for coordinating with tribes that may respond to letters or request consultation. AB 52 consultation will be summarized in the Tribal Cultural Resources section of the IS.

No in-person meetings with Native American groups are included in the base scope of work; however, Optional Task CULT-1 includes budget to accommodate Dudek participation at in-person Native American meetings as needed.

Optional Task CULT-1: Additional AB 52 Support. This task provides for up to 20 hours of additional archaeologist and project management time to provide additional support to the City in the AB 52 consultation process. This could include Dudek participation in meetings with Native American groups, input in the development of mitigation measures to address Native American concerns, and preparation of letter reports of the findings and outcomes reached through AB 52 consultation. Costs for this optional task are included in the Contingency Tasks portion of our proposed budget

Built Environment Inventory and Evaluation Report

Dudek’s professionally qualified built environment cultural resources staff will support the project by preparing a Built Environment Inventory and Evaluation Report (BEIER). Dudek cultural resources staff understands that the project is situated within the Kavanaugh Industrial Park and proposes to demolish two buildings located at 1020 O’Brien Drive (APN 055-422-060, built c.1960), and 1030 O’Brien Drive (APN 055-422-010, built c.1965) to construct a three-story research and development building adjacent to two existing buildings located at 980/990 O’Brien Drive (APN 055-422-090, built 1962) and 1010 O’Brien Drive (APN 055-422-100, built 1959). Dudek recommends evaluating the four properties proposed for modification and/or demolition that exceed 45 years in age for historical significance in consideration of National Register of Historic Places (NRHP) and CRHR designation criteria, and local requirements to ensure that the proposed demolition and new construction will not result in material impairment of an historical resource under CEQA. Tasks involved in the preparation of this technical work are as follows:

Background Research

Under this task, Dudek will conduct a search of the Built Environment Resource Database (BERD) available through the California Office of Historic Preservation (OHP) to determine if any of the properties in the project study area have been previously evaluated. Dudek will also conduct building development research through the City and/or San Mateo County to understand the construction history of the properties, determine the nature and extent of any alterations to the property, and retrieve information on any previous owners/occupants. In addition, Dudek will conduct archival research to develop the historic context under which the properties will be evaluated. Research in support of the historic context may include visiting local libraries, archives, and contacting relevant historical societies. To the extent that is possible, the development of the historic context will utilize existing context that was produced as part of the 1005 O’Brien Drive and 1320 Willow Road Research and Development Project, including the comprehensive regional and Kavanaugh Industrial Park contexts.

Create Study Area Map and Conduct Field Survey

Upon completion of the background research, Dudek architectural historians will prepare a draft Study Area Map for built environment resources in consultation with the City and Project design staff. The Study Area Map will include all properties within the proposed project footprint, as well as some parcels immediately adjacent to the proposed project, dependent on the potential for direct and indirect impacts to built environment resources. The extent of the Study Area will be based on the project description and project design drawings. Dudek assumes that the project applicant will provide all of the required project details in GIS or CAD along with parcel data. This scope of work assumes that Dudek will produce no more than two versions of the Study Area Map: draft and final. Dudek assumes that the Study Area for this project will be limited to the proposed project boundary and only included four historic era buildings.

After approval of the final Study Area Maps, Dudek architectural historians will survey the study area. The built environment survey will entail taking detailed notes and photographs of all buildings located in the Study Area. This includes documentation of exterior features, spatial relationships, landscaping, alterations, and the overall existing conditions of the buildings.

Record and Evaluate Resources

Dudek assumes recordation and evaluation of no more than four buildings. Because the O’Brien Drive business park appears to have been developed together, Dudek may group individual properties into complexes to simplify recordation. The City does not have local historical resource registration criteria and appears to defer to NRHP and CRHR guidelines. Consequently, the properties will be evaluated under NRHP and CRHR criteria and integrity requirements. Dudek assumes that preparation of no more than four DPR 523 form sets will be required.

Built Environment Inventory and Evaluation Report

Dudek will prepare a BEIER that will summarize the results of the background research, field survey, and property significance evaluation for each property. The report will also discuss the proposed project description, regulatory framework, all sources consulted, research and field methodology, setting, and findings. Dudek assumes that the proposed project will not result in significant impacts to any properties that qualify as historical resources under CEQA, and development of mitigation will not be necessary.

All DPR forms for built environment resources will be included as an appendix to the report. Dudek assumes no more than one (1) draft and one (1) final version of the report will be required and that comments on the draft report will be editorial in nature and will not require additional research or field survey.

Task A2.4: Noise and Vibration Study

Dudek will prepare an assessment of the noise and vibration impacts potentially associated with the proposed project based on information provided by the applicant, a sound level measurement survey, relevant reference material, ConnectMenlo and the City General Plan and Code.

Dudek will conduct a noise monitoring program at the proposed project site and in the project vicinity to characterize baseline ambient acoustical conditions in the area and catalog existing noise levels. The noise monitoring program is anticipated to include noise monitoring at up to three (3) locations in the proposed project area. Short-term (approximately 10 to 30 minutes in duration) attended noise measurements will be conducted at up to two (2) locations to provide adequate representation and relative exposure of noise-sensitive receptors to existing noise levels and to characterize sound generated by existing traffic. Additionally, a continuous long-term unattended noise measurements will be conducted at one (1) location in the proposed project study area (approximately 24 hours in duration).



Monitoring locations will be selected to adequately represent noise exposure at areas of key interest in the project vicinity; such as property lines and nearby noise-sensitive receptors. Noise level data will be used for establishing existing baseline noise level in the project vicinity and will serve as a basis of evaluation for future noise levels at

receivers within the project area. Dudek will coordinate with designated project team member as directed to coordinate access where required.

Noise Modeling and Analysis

Dudek will model and analyze existing ambient and project-generated noise levels throughout the project study area. The analysis will be based on proposed project information provided by the project applicant, City, along with observations and noise measurement data from the field survey, the proposed project’s traffic study and available reference data. We assume that the project applicant or team will provide all necessary information for stationary sound sources (e.g., heating, ventilation and air-conditioning equipment, cooling towers, exhaust and intake fans, pumps, compressors, generators, etc.) and other on-site noise generating activities (e.g., deliveries, loading dock activities, etc.).

Dudek will analyze potential short-term, construction-related noise impacts associated with the proposed project (e.g., on-site heavy-duty equipment, generators, pumps, etc.). Construction-related noise impacts will be assessed with respect to nearby noise-sensitive receptors and their relative exposure, based on the City’s Noise Control Ordinance and ConnectMenlo. The analysis will utilize the Federal Highway Administration (FHWA) Roadway Construction Noise Model and Federal Transit Administration (FTA) reference noise level data and industry-standard propagation methodologies. Vibration impacts (construction and operational) will be evaluated through the application of FTA and California Department of Transportation (Caltrans) reference data and methodologies. Although the proposed project will not be able to tier from the ConnectMenlo EIR, Dudek will analyze impacts and propose mitigation measures consistent with the ConnectMenlo Mitigation Monitoring and Reporting Plan (MMRP), as noise and vibration issues are expected to be the similar.

Existing and future traffic noise exposure at nearby existing noise-sensitive receptors will be analyzed based on the traffic study, available regional traffic data and application of the FHWA traffic noise propagation modeling algorithms. Where traffic noise levels are calculated to exceed applicable thresholds, mitigation measures will be evaluated to determine the requirements to achieve compliance with the thresholds.

Reporting

Dudek will prepare a technical noise study report that will discuss the existing environment, noise monitoring results, analysis methodology and findings. The report will provide a summary of the relevant regulatory framework against which noise and vibration impacts are assessed based on the relevant City, state, and federal standards. If significant impacts are identified, mitigation measures to reduce impacts to a less-than-significant level (where feasible) will be recommended. Proposed mitigation measures consistent with the ConnectMenlo MMRP will be recommended whenever feasible. Dudek will also use the report to the necessary information for preparation of the proposed project’s CEQA documentation.

Task A2.5: Transportation Impact Analysis

Dudek’s in-house transportation planners and engineers will prepare the TIA for the proposed project. The project site includes four contiguous properties addressed 980-1030 O’Brien Drive. The proposed project would demolish two existing, one-story commercial buildings on two parcels and construct one new three-story building for Life Sciences commercial/office uses on the newly merged parcel. Two existing commercial buildings would remain. Unused development potential from the remaining buildings would be incorporated into the new building. The proposed total gross floor area of the project site would be approximately 85,886 square feet of research and design/office space, and 5,822 square feet of commercial space.

The TIA will be prepared consistent with the requirements of the City’s TIA guidelines, the San Mateo County Congestion Management Program (CMP), Caltrans Transportation Impact Study Guide (TISG) (where applicable),

and Senate Bill 743 (SB 743) regarding vehicle miles traveled (VMT). The following scope of work has been prepared based on our recent experience in the City, on a similar project; and, prior to the initiation of the TIA, Dudek staff will seek approval of the following work scope by the City. Should additional items be requested and/or refined (or items removed), Dudek will amend the work scope and seek contract modification (if needed).

Transportation Demand Management Plan

In accordance with City Municipal Code Section 16.45.090, projects with a net new increase (or change in land use) of 10,000 square feet of gross floor area will be required to develop a transportation demand management (TDM) plan to reduce at least 20% of net new vehicular trips. Dudek will review the applicant provided TDM plan to determine whether the 20% reduction is achievable. If it's determined to be achievable, the VMT and level of service (LOS) analyses, described below, will include 20% project trip generation reduction to reflect the proposed TDM plan. If, through the VMT analysis (described below), it is determined that a TDM reduction of more than 20% is required, Dudek will work with the City and applicant to determine appropriate measures to meet the required reduction to mitigate VMT impacts.

Vehicle Miles Traveled Analysis

The City has recently updated their TIA guidelines to comply with SB 743, which requires that transportation impacts in CEQA be determined based on the VMT metric, instead of the LOS metric.

The City's Transportation Impact Analysis Guidelines (2020) are largely based on the State Office and Planning Research (OPR) Technical Advisory (2018). Projects that have certain characteristics are exempted from further VMT analysis based on VMT exemption criteria. It is presumed that the proposed project cannot be screened-out from further VMT analysis as it is not within a low VMT area and within ½ mile of an existing "major transit stop" or within ½ mile of a "high-quality transit corridor." However, per the Metropolitan Transportation Commission Final Plan Bay Area 2050 (2021), the project is within a Transit Priority Area with a planned major transit stop at the intersection of Willow Road and Bayfront Expressway.

For purposes of this scope of work, it is assumed that the VMT analysis will be needed and will be confirmed with the City based on the potential location of the project within a low VMT area and within a future Transit Priority Area. The VMT analysis will be prepared using a travel demand model. Dudek will subcontract with a City-approved traffic modeling consultant to modify and run the Menlo Park City Travel Demand Model. Our budget includes \$15,000 for the subcontracted VMT modeling, which is consistent with current VMT modeling estimates in the Bay Area. Dudek will coordinate with the City to modify the Traffic Analysis Zone within the project site with the land uses of the proposed project and perform a Select Zone model run to determine the project's trip assignment. As part of the Select Zone run, the VMT estimate of the project will be determined for the per capita, per employee, and per service population variables. Then, the regional baseline VMT estimate for the study area (extent to be determined by the City) for those same variables will also be provided from the travel demand model. Dudek will analyze that data using the City's VMT thresholds.

If a significant VMT impact is found, Dudek will identify feasible mitigation measures that could avoid or reduce the impact. TDM strategies to mitigate VMT will be utilized from the document Quantifying Greenhouse Gas Mitigation Measures (August 2010), prepared by California Air Pollution Control Officers Association. Dudek will utilize the reduction factors provided in the document to quantify, wherever possible, the effect of applicable TDM strategies on VMT reduction of single occupant vehicle trips. It should be noted that the reduction of VMT for some of the measures is qualitative; therefore, the mitigation measures will include both quantitative and qualitative significance after mitigation analysis. It should be noted that within

the project area, strategies to reduce VMT are limited due to the lack of other existing transportation modes and reliance on private vehicles.

Level of Service Analysis

Dudek will also conduct an LOS analysis of the surrounding street network per the City’s TIA guidelines and the San Mateo County CMP. For purposes of this scope, up to two roadway segments and 12 intersections will be analyzed. Dudek will confirm the study area and specific analysis locations with the City prior to initiation of the TIA. Dudek will work with the City to obtain recent traffic counts for the study area roadway segments and intersections. Traffic counts may come from, and be consistent with, the traffic studies being prepared for on-going projects in the City.

Intersection and roadway segment LOS analyses will be prepared for the weekday daily, a.m. and p.m. peak hours at the study area locations listed above for the following analysis scenarios:

- Existing condition
- Near-Term base condition
- Near-Term plus project condition
- Cumulative (including all future potential development by year 2040)
- Cumulative plus project traffic condition

The LOS analyses will be prepared consistent with the required analysis methodology of the City, which is the Highway Capacity Manual (HCM) methodology using VISTRO traffic analysis software, which is currently being used on other projects under review by the City’s Transportation Division. Project trip generation estimates will be based on trip rates in the Institute of Transportation Engineers Trip Generation, 11th Edition (2021). The project’s trip generation, distribution, and assignment will be approved by the City prior to completion of the traffic analysis.

For the near-term conditions, cumulative projects’ traffic volumes will be based on the City’s volumes in their VISTRO files. Dudek will also request approved and pending project lists (and traffic volumes and/or studies) from the City of East Palo Alto. This scope and budget include the manual trip assignment of up to 10 approved and pending projects. Dudek will revise the near-term and cumulative VISTRO files as needed. For the cumulative conditions, Dudek will obtain the post-processed traffic volumes from the City’s travel demand modeler. Our budget includes \$7,000 for the subcontracted travel demand modeling for forecast cumulative traffic volumes.

Optional Task TRAN-1: Peak Hour Traffic Counts. As an optional task, at the direction of the City, new weekday daily roadway segment, and a.m. (7:00–10:00 a.m.) and p.m. (4:00–7:00 p.m.) peak hour intersection counts will be collected at the study area locations. Traffic counts will be collected during a typical weekday of a non-holiday week. The peak hour traffic counts will include bicycle and pedestrian volumes at the study intersections. Costs for this optional task are included in the Contingency Tasks portion of our proposed budget.

Optional Task TRAN-2: CMP Review. As an optional task, at the direction of the City, if it’s determined that the project triggers CMP review, a CMP-level analysis of CMP roadway segments will be prepared. For purposes of this scope of work, four (4) CMP roadway segments are assumed to be analyzed. The CMP study area will be confirmed with the City. Costs for this optional task are included in the Contingency Tasks portion of our proposed budget.

Transit, Pedestrian, and Bicycle Facilities, and Project Access

Dudek will also qualitatively analyze the transit, pedestrian, and bicycle facilities that serve the project site. Project access and on-site circulation will be based on the City’s Standard Plans/Drawings for access and on-site circulation design requirements. Vehicular queuing at the project’s driveway will be analyzed for adequacy based on the 95th percentile (design) queues.

For any significant project traffic impacts found, Dudek will determine appropriate and feasible mitigation measures to offset significant project impacts.

TIA Document

The methodologies, assumptions, analyses, findings, and mitigation measures (if any) will be summarized in a TIA report. All necessary tables, figures, and appendices will be provided in the TIA. A Draft TIA will be submitted to the City for review. This scope assumes one round of consolidated review by the City. Once comments are received from the City, Dudek will prepare a Final TIA for use in the project’s CEQA document.

Task A2 Deliverables:

- Draft and Final Air Quality/GHG Assessment (submitted electronically [MS Word and PDF] as well as 15-20 printed copies)
- Draft and Final Construction HRA (submitted electronically [MS Word and PDF] as well as 15-20 printed copies)
- Draft and Final BTR (submitted electronically [MS Word and PDF] as well as 15-20 printed copies)
- Draft and Final Noise Assessment (submitted electronically [MS Word and PDF] as well as 15-20 printed copies)
- Draft and Final TIA (submitted electronically [MS Word and PDF] as well as 15-20 printed copies)

Phase B – Notice of Exemption

Task B1: Administrative Draft Notice of Exemption

Upon completion of the Project Description under Task 1, receipt of all project-supporting information and documentation (i.e., reports, studies), and completion of the technical studies identified under Task 2, Dudek will prepare an administrative draft Notice of Exemption (NOE) for the proposed project. The NOE will include the project description, a brief statement regarding the environmental setting of the project site including an overview of other development projects in the vicinity, a discussion of the specific exemption classes that are being applied to the project and an explanation of how the project is consistent with each class, and a discussion demonstrating that none of the exceptions to use of an NOE are triggered.

Task B1 Deliverables

- Administrative Draft NOE (submitted electronically [MS Word]).

Task B2: Screencheck Draft Notice of Exemption

Based on comments and suggested edits to the Administrative Draft NOE, Dudek will prepare a Screencheck NOE document for City review to ensure the document incorporates all requested revisions. We assume no revisions to the technical studies would be required.

Task B2 Deliverables

- Screencheck Draft NOE (submitted electronically [MS Word]).

Task B3: Final Notice of Exemption

Based on comments on the Screencheck Draft NOE, Dudek will prepare a Final NOE document for City review. Upon City approval of the NOE, Dudek will coordinate with City staff to ensure the NOE is filed with the County Clerk and the State Clearinghouse. Dudek assumes the City will be responsible for the County Clerk filing and Dudek will be responsible for the State Clearinghouse filing.

Task B3 Deliverables

- Final NOE (submitted electronically [pdf]).
- Filing of the NOE with the State Clearinghouse

Phase C – Initial Study/Mitigated Negative Declaration

Task C1: Administrative Draft Initial Study

Upon completion of the Project Description under Task A1, receipt of all project-supporting information and documentation (i.e., reports, studies), and completion of the technical studies identified under Task A2, Dudek will prepare an administrative draft IS for the proposed project tiering from the ConnectMenlo EIR. The IS will be prepared using the City-approved checklist format that is consistent with the procedural and substantive provisions of Sections 15063 and 15082 and Appendices C, F, G, and I of the CEQA Guidelines. In completing the CEQA Environmental Checklist (Appendix G), Dudek will include explanations and analysis for each response, including “No Impact” responses. The level of analysis and degree of impact will vary depending upon the environmental topic but will be sufficient to provide the substantial evidence to support the responses, consistent with CEQA requirements and legal direction from established case law. The assessment of project impacts will identify potentially significant environmental impacts, and if required, feasible mitigation measures will be recommended to reduce potentially significant impacts to a less-than-significant level. Environmental setting, impact analyses, and substantiating documentation will be provided to support all impact conclusions, including tables and full-color figures, if applicable.

We are anticipating all impacts would be less than significant or able to be reduced to less than significant with mitigation. However, if it is determined that development of the site would result in any impacts that cannot be mitigated to a less-than-significant level an EIR may be required.

- **Aesthetics** – The project would redevelop the existing project site with a new three-story building for life sciences research and design. Dudek will evaluate the proposed building’s consistency with the city’s Design Guidelines under Section 16.68.020 of the Zoning Ordinance, consistency of the proposed lighting plan with applicable General Plan and municipal code standards, and compatibility with surrounding properties. This will include comparing building scale, massing, and height with the existing building, describing building design elements, materials, and colors, describing proposed landscaping, and characterizing potential changes in light and glare. The change in visual character is subjective; therefore, the analysis will focus on

the degree to which the proposed project will change the existing visual character of the site and evaluate if it would be substantially different from the current visual character. The IS will also summarize the findings of the ConnectMenlo EIR regarding scenic vistas, scenic corridors, and scenic highways.

- **Agricultural and Forestry Resources** – The project site is developed with four existing commercial buildings, surface parking, and ornamental landscaping. There are no agricultural or forestry resources within the project site or in the surrounding area. The project would have no effect on agricultural or forestry resources.
- **Biological Resources** – The project site is developed and is not expected to support sensitive natural communities or aquatic resources. The ConnectMenlo EIR determined that redevelopment within the Bayfront area would not result in significant impacts to special-status species, sensitive natural communities, wetlands, and wildlife movement. Dudek understands that the project applicant will submit an arborist report by a City-approved arborist and City staff will review that report and the project plans to ensure compliance with any heritage/street tree removal replacement and other landscaping requirements. Dudek will incorporate the findings of (1) the BTR prepared for the project site under Task 2.2, (2) the arborist report and City staff findings regarding tree removal replacement, and (3) other landscaping requirements in the IS. The IS will also discuss whether the project would meet the City’s Water-Efficient Landscaping as defined in Municipal Code Chapter 12.44.
- **Cultural Resources** – The project site is developed and is unlikely to support any surface area cultural resources, consistent with the findings of the ConnectMenlo EIR. The IS will summarize the findings of the Archaeological Resources Study prepared under Task 2.3 as well as the ConnectMenlo EIR regarding archeological sensitivity and potential to encounter human remains and will incorporate Mitigation Measures CULT-2a and CULT-4 from the ConnectMenlo EIR. The IS will also summarize the findings of the BEIER prepared under Task 2.3.
- **Energy** – The IS will summarize the findings of the ConnectMenlo EIR regarding energy efficiency and sustainable project design, including requirements for buildings to comply with the CALGreen Building Code, the California Public Utility Commission’s Long Term Energy Efficiency Strategic Plan, Chapter 12.18 of the Menlo Park Municipal Code which contains the Green Building Ordinance, and green building requirements for LEED certification, including providing outlets for Electric Vehicle charging, providing on-site renewable energy generation, and enrolling in the U.S. Environmental Protection Agency’s Energy Star Building Portfolio Manager. The IS will also document the project’s energy consumption during construction and operation based on Dudek’s modeling of the project using the CalEEMod, as described in Task 2.1.
- **Geology and Soils** – The project site is developed and thus does not contain significant topsoil resources and has been shown to be capable of supporting urban-level development. Dudek assumes the project applicant will submit a preliminary geotechnical investigation report that includes specific engineering and design recommendations to ensure that any geologic and seismic conditions are appropriately addressed to avoid any significant impacts related to geology and soils. Based on the preliminary geotechnical investigation, the IS will discuss whether the project could exacerbate any existing geologic and seismic hazards and evaluate the potential project effects related to geologic and soil stability, including expansive soils. This section of the IS will also summarize the findings of the ConnectMenlo EIR regarding paleontological resources and incorporate ConnectMenlo EIR Mitigation Measure CULT-3 to ensure such impacts remain less than significant.
- **Hazards and Hazardous Materials** – Dudek assumes the project applicant will submit a Phase I Environmental Site Assessment (ESA), and a Phase II ESA or other supplemental investigations if recommended in the Phase I ESA. The IS will summarize the findings of the ConnectMenlo EIR regarding routine use and transport of hazardous materials, potential release of hazardous materials, airport-related hazards, and emergency response and evacuation plans. If appropriate, based on the findings of

the Phase I ESA, the IS will incorporate relevant mitigation measures from the ConnectMenlo EIR and the Phase I ESA to ensure that impacts associated with any existing hazardous materials or hazardous conditions within the site are reduced to less-than-significant levels.

- **Hydrology and Water Quality** – The existing buildings and parking lots within the project site represent impervious surfaces. The IS will document the degree to which the proposed project would increase or decrease the amount of impervious surface within the project site and discuss the potential temporary impacts to water quality as a result of building demolition, removal of existing pavement, grading and excavation. The IS will also document how water quality impacts would be minimized through compliance with the provisions and requirements under the San Mateo Countywide Water Pollution Prevention Program, which includes the C.3 provisions set by the Regional Water Quality Control Board, the City’s Municipal Code Chapter 7.42, Stormwater Management Program the State Water Resources Control Board–Construction General Permit, and a construction-related Stormwater Pollution Prevention Plan, including best management practices. The IS will demonstrate that the proposed project would not be expected to cause significant impacts to hydrology and water quality with through compliance with adopted standards and conditions that would reduce construction and operation phase impacts on water quality to a less-than-significant level and that the project would not result in any new or more severe impacts beyond those examined in the ConnectMenlo EIR. The IS will also summarize the findings of the ConnectMenlo EIR related to erosion, groundwater, and flooding.
- **Land Use and Planning** – The proposed project is consistent with the LS zone district and with the buildout projections included in ConnectMenlo. The IS will summarize the findings of the ConnectMenlo EIR regarding disruption to existing communities and will demonstrate that the proposed project would be consistent with that analysis because it would not create barriers to connectivity or substantially alter any existing roadways. The IS will also summarize the City’s applicable land use plans, policies, and regulations adopted for the purpose of avoiding or mitigating an environmental effect and evaluate the project’s consistency with these.
- **Mineral Resources** – The project site is not known to support any mineral resources or mineral resource extraction activities. Consistent with the findings of the ConnectMenlo EIR, redevelopment of the project site would have no impact on mineral resources.
- **Public Services** – The IS will summarize the findings of the ConnectMenlo EIR regarding provision of public services, identify the public service facilities nearest the project site, characterize the project’s contribution to demand for public services and the ability of the existing and planned public services to meet that demand, identify project design elements and regulatory requirements that would serve to minimize the increase in demand, and incorporate applicable mitigation measures to ensure impacts remain less than significant.
- **Recreation** – The project site does not contain any recreation resources. The proposed replacement of existing buildings would not directly increase the City’s residential population but would contribute indirectly to population growth as a result of new employment opportunities, consistent with the buildout projections of the ConnectMenlo EIR. The IS will summarize the findings of the ConnectMenlo EIR regarding the City’s goals and policies for provision of recreation facilities and evaluate the project’s contribution to city-wide recreation demand.
- **Tribal Cultural Resources** – As described in Task 2.3, Dudek will support City staff in completing Native American consultation as required under AB 52. Given the developed condition of the project site, the base proposed budget assumes that the consultation process will not identify any known or potential tribal cultural resources that could be affected by the proposed project; however should tribal cultural resource concerns be identified, a portion of the contingency budget can be used to provide analysis of the potential impacts in the IS.

- **Utilities and Service Systems** – The IS will summarize the findings of the ConnectMenlo EIR regarding provision of utilities and service systems, identify the utility facilities that would serve the project site, characterize the project’s contribution to demand for utilities and the ability of the existing and planned utilities and service systems to meet that demand, identify project design elements and regulatory requirements that would serve to minimize the increase in demand, and incorporate applicable mitigation measures to ensure impacts remain less than significant.
- **Wildfire** – As stated in the ConnectMenlo EIR, the Bayfront Area, which includes the project site, does not contain areas of moderate, high, or very high Fire Hazard Severity for the Local Responsibility area, nor does it contain any areas of moderate, high, or very high Fire Hazard Severity for the State Responsibility Area. The IS will demonstrate that the project would not exacerbate wildfire risks or interfere with emergency response and evacuation plans.

Task C1 Deliverables

- Administrative Draft IS (submitted electronically [MS Word]).

Task C2: Screencheck Draft Initial Study/Mitigated Negative Declaration

Based on comments and suggested edits to the Administrative Draft IS, Dudek will prepare a Screencheck IS/MND document for City review to ensure the document incorporates all requested revisions. Typically, comments on a Screencheck document are minor and do not require significant updates or revisions to the analysis.

Task C2 Deliverables

- Screencheck Draft IS/MND (submitted electronically [MS Word]).

Task C3: Public Draft Initial Study/Mitigated Negative Declaration

Upon receipt of comments on the Screencheck Draft IS/Proposed MND, Dudek will prepare the Draft IS/Proposed MND for public review. This will include preparation of a Notice of Intent (NOI) to adopt the MND and a Notice of Completion (NOC). Dudek will work with City staff to assemble, notice, and distribute the Draft IS/Proposed MND for public review. Dudek assumes City staff will deliver the NOI to the San Mateo County Clerk for posting and will undertake local agency distribution. Dudek submit 15 to 20 hard copies of the Draft IS/Proposed MND to the City for distribution; technical appendices will be provided on a CD or flash drive.

Dudek will coordinate with City to determine if state agency review is needed and will assist City staff in identifying responsible and trustee agencies and will undertake electronic submission of the Draft IS/Proposed MND to the Office of Planning and Research (OPR) website to initiate start of the public comment period. It is assumed that City staff will prepare public notices for newspaper notice/publishing and mailing as required and will post a copy of the document on the City’s website.

Task C3 Deliverables

- Public Draft IS/Proposed MND (submitted electronically [MS Word] as well as 15 to 20 printed copies).

Task C4: Responses to Comments/Mitigation Monitoring and Reporting Plan

Upon completion of the public review period, Dudek will review comments received and prepare responses to comments in a memorandum format. Should comments on the Draft IS/Proposed MND raise new issues or require surveys or technical studies be conducted to complete an adequate response, Dudek will initiate discussions immediately with the City and the project applicant to evaluate the options and next steps.

Consistent with CEQA requirements, an MMRP will also be prepared for any required mitigation measures. An electronic copy of the responses to comments memorandum and MMRP will be provided to the project applicant and the City for review and comment. It is anticipated that the City will incorporate any comments from the applicant, if necessary. Upon receipt of City comments on the responses to comments memorandum and MMRP, Dudek will finalize both documents for submittal to the City. If requested, Dudek can prepare the Notice of Determination (NOD). It is assumed that City staff will post the NOD with the County Clerk's office if the project is approved and the MND is adopted within five working days after project approval.

Task C4 Deliverables

- Response to Comments Memorandum and MMRP (submitted electronically [MS Word and PDF]).

Task C5: Meetings and Hearings

Dudek Project Manager Katherine Waugh will attend the following meetings. At this time, it is assumed that all meetings will be virtual. However, our proposed contingency budget could be used to allow for in-person attendance at meetings and/or for Dudek's participation in additional meetings:

- Project kickoff meeting (included in Task 1)
- Four project status/document review meetings with City staff to review project status, technical study assumptions and content, document progress, comments on administrative drafts of documents, and other project issues
- One Planning Commission meeting

At public meetings and hearings, Dudek will be available to present a summary of the documents being reviewed or considered, respond to questions, and provide any necessary information. During each public meeting, Dudek staff will summarize and explain the results of the IS/MND to public officials and take notes to document comments received on the IS/MND.

Task C6: Project Management

We prioritize project management and believe that a focused, well-managed effort on the part of the Dudek team will be key to achieving the City's processing goals for the proposed project. This task includes preparation of regular progress reports to be submitted with our monthly invoice to the City. A key element of Dudek's progress report procedures is identifying upcoming issues and information needs, as well as a summary of tasks completed during the previous month. This helps maintain project momentum by identifying issues as early in the process as possible and building a record of project progress.

Throughout the project, Ms. Waugh and Ms. Burrowes will be available to consult with City staff by telephone and email, with a goal of responding to emails within 24 hours. They will also actively engage with all Dudek team members and the City to ensure all parties have consistent project information, are meeting project milestones, and are working within the agreed-upon scope of work and budget.

Project Schedule

Dudek’s estimated schedule to complete the scope of work described in this proposal is presented in **Table 3**. As shown, Dudek anticipates that an NOE could be complete approximately 23 weeks after project initiation; and if an IS/MND is required, the administrative draft IS would be submitted to the City for review approximately 17.5 weeks after project initiation and circulated for public review approximately 26 weeks after project initiation.

Table 3. Project Schedule

Task Name	Weeks Elapsed	Total Weeks Elapsed
Task A1 Project Initiation and Project Description		
Initiation meeting and site visit	1 week	1 week
Preliminary Project Description and Request for Information	2 weeks	3 weeks
City Review	2 weeks	5 weeks
Final Project Description	1 week	6 weeks
Task A2 Technical Studies		
A2.1 Air Quality, Greenhouse Gas, and Energy Assessment	8 weeks	14 weeks
A2.2 Biological Technical Report	4 weeks	10 weeks
A2.3 Cultural and Historic Resources	12 weeks (starting at week 3)	15 weeks
A2.4 Noise and Vibration Study	6 weeks	12 weeks
A2.5 Transportation Impact Analysis	10 weeks	16 weeks
Task B1 Administrative Draft NOE		
Administrative Draft NOE	2 weeks after completion of task A2	18 weeks
City Review	2 weeks	20 weeks
Task B2 Screencheck Draft NOE		
Screencheck Draft NOE	1 week	21 weeks
City Review	1 week	22 weeks
Task B3 Final NOE		
Final NOE	0.5 week	22.5 weeks
Filing	0.5 week	23 weeks
Task C1 Administrative Draft Initial Study		
Administrative Draft IS/Proposed MND, technical appendices, and source document files	1.5 weeks after completion of task A2	17.5 weeks
City Review	3 weeks	20.5 weeks
Task C2 Screencheck Draft Initial Study/Mitigated Negative Declaration		
Screencheck Draft IS/Proposed MND	2 weeks after completion of task C1	22.5 weeks
City Review	2 weeks	24.5 weeks

Table 3. Project Schedule

Task Name	Weeks Elapsed	Total Weeks Elapsed
Task C3 Public Draft Initial Study/Mitigated Negative Declaration		
Public Review Draft IS/MND	1.5 weeks after completion of task C2	26 weeks
Notification and Distribution	0.5 weeks	26.5 weeks
Public Review Period	4.5 weeks	31 weeks
Task C4 Responses to Comments/Mitigation Monitoring and Reporting Plan		
Draft Response to Comments and MMRP	2.5 weeks	33.5 weeks
City Review	1.5 weeks	35 weeks
Final Response to Comments and MMRP	1 week	36 weeks
Task C5 Meetings and Hearings		
Meetings and Hearings	Ongoing throughout	
Task C6 Project Management		
Project Management	Ongoing throughout	
Optional Tasks		
AQ-1: Construction HRA	Concurrent with Task A2.1	
AQ-2: Operation HRA		
CULT-1: Additional AB 52 Support	Concurrent with Task A2.3	
TRAN-1: Peak Hour Traffic Counts	Concurrent with Task A2.5	
TRAN-2: CMP Review		

Additional Information

Dudek's Diversity, Equity, and Inclusion Plan

Dudek recognizes that the City values diversity and has made important decisions in recent years to ensure that City practices and policies support community efforts to improve equity and inclusion within the City. Dudek shares in the commitment to supporting diversity, equity, and inclusion (DEI). Dudek launched a focused DEI efforts in 2020 as a direct response to a pivotal moment in time. Today, we are dedicated to continuing these and other diversity and inclusion efforts over time. Our ongoing mission and goals are as follows:

At Dudek, we need inclusivity to succeed and diversity to lead.

GOALS:

- Launching the aforementioned statement defining our mission and strategy to reflect diversity, inclusion, and equity across our organization. We leverage this statement to spark and encourage internal dialogues and make specific commitments to actions that follow our mission and strategy.
- Supporting our communities through strategic partnerships and outreach. We have partnered with the National Urban League as well as local affiliates to develop and deliver education programs to encourage and foster diversity in our industry and support underserved communities.
- Expanding recruiting to reach a more diverse pool of candidates. We have partnered with Circa to expand our reach and visibility within community-based organizations, websites catering to veteran and disabled candidates, as well as other niche sites focusing on diverse talent. We have also expanded our college recruitment efforts focusing on colleges and universities with more diverse student and faculty populations.
- Financially investing in underserved and minority communities. In 2020 we donated to the National Urban League and have since partnered on various educational initiatives. We are eager to support similarly vetted organizations and have set aside an annual budget to meet this goal. Additionally, we are planning to provide scholarships to offer greater opportunity to a more diverse pool of students pursuing planning, engineering, and the sciences.
- Supporting our employee's diversity and inclusion efforts in their communities. Social responsibility is an integral piece of our culture. Dudek offers volunteer time for each employee to pursue causes of their own choosing.

We understand that the best problem-solving happens when diverse viewpoints and experiences are applied. We recognize that different perspectives, inclusivity, and trust build a stronger culture and add value to our firm. We celebrate our differences and strive to make meaningful progress toward being a more diverse company. We are committed to continuous improvement within our company and our communities through incremental steps and bold decisions. We are more successful together. In recognition of the critical importance of our DEI goals and efforts, Dudek has retained Patti Perez, CEO of PersuasionPoint, a modern-day consulting firm dedicated to teaching leaders and teams how to create and sustain healthy, inclusive, and profitable workplace cultures. With her support, Dudek has created Employee Resource Groups to further our commitment to and help drive our DEI strategy and resulting initiatives.

We have implemented two strategies within our employee recruitment program to foster our development of a diverse, equitable, and inclusive workforce. These include emphasizing meaningful DEI recruitment strategies, outreach, sourcing, and interviewing; and expanding college recruitment efforts with a focus on reaching and attracting diverse candidates. Dudek has participated in various career fairs and is forging relationships with numerous schools in an effort to reach underrepresented minority groups. We are building relationships with the California Department of Development Workforce Development for Veteran Affairs and Hire, the National Society of Black Engineers, San Diego Chapter, and Black Women in Science and Engineering.

In addition, Dudek continues to partner with community organizations to engage in outreach and education so that we are an active force in statewide and regional efforts to inspire diverse populations and communities to pursue environmental consulting, engineering, and related fields.

Pricing Proposal

Dudek's cost estimate to complete the scope of work described in this proposal is presented in Table 4. It includes a time and materials cost for the base scope of work described above, additional time and materials costs for the optional tasks described above, and an additional contingency budget that can be applied to unanticipated tasks and services as needed. As shown, Dudek estimates the cost to complete Phase A of the Work Plan will be \$115,413.25; the cost to complete Phase B of the Work Plan will be \$7,600.00; and the cost to complete Phase C of the Work Plan will be \$62,134.00. If an NOE can be supported, Dudek will only complete Phases A and B, resulting in a total cost of \$123,013.25. If an IS/MND is warranted, Dudek will complete Phases A and C, resulting in a total cost of \$177,547.25. We have also identified five optional tasks within Phase A with a total additional cost of \$26,190.00 and a general contingency budget within Phase C of \$15,125.00.

		Dudek Labor Hours and Rates																														
Project Team Role:		Project Director	Senior Specialist IV	Specialist III	Analyst V	Analyst I	Senior Specialist I	Specialist III	Senior Specialist I	Specialist I	Senior Specialist I	Analyst I	Senior Specialist II	Specialist III	Analyst IV	Specialist IV	Specialist III	Analyst V	Analyst III	Principal Engineer II	Project Director	Specialist IV	Specialist I	GIS Analyst II								
Team Member:		Ann Sansonero	Katherine Waugh	Kirsten Burrows	Daniel Hoffman	Jamie Stroud	Matthew Morales	Ian McIntire	Adam Giacinto	William Burns	Katie Haley	Erin Jones	Matt Ricketts	Emily Scricca	Erin Fisher-Colton	Michael Carr	Cole Martin	Daniel Hoffman	Ashley Vir	Charles Greely	Dennis Pascua	Sabita Tewani	Mladen Popovic	Rachel Strobridge	Technical Editor II	Publications Specialist II	TOTAL DUDEK HOURS	DUDEK LABOR COSTS	OTHER DIRECT COSTS	TOTAL FEE		
Billable Rate:		\$265.00	\$235.00	\$165.00	\$135.00	\$95.00	\$200.00	\$165.00	\$200.00	\$145.00	\$200.00	\$35.00	\$210.00	\$165.00	\$125.00	\$175.00	\$165.00	\$135.00	\$115.00	\$280.00	\$265.00	\$175.00	\$145.00	\$130.00	\$135.00	\$105.00						
Task A1	Project Initiation and Project Description																															
A1.1	Project Initiation		10	12																							22	\$4,330.00		\$4,330.00		
A1.2	Project Description		3	5		10																			6	2	2	28	\$3,740.00		\$3,740.00	
Subtotal Task A1			13	17		10																				50	\$8,070.00		\$8,070.00			
Task A2	Technical Studies																															
A2.1	Air Quality/Greenhouse Gas/Energy		2	1			6	60																			69	\$11,735.00		\$11,735.00		
A2.2	Biological Technical Report			1									6	4	33										4	4	6	58	\$7,900.00	\$166.75	\$8,066.75	
A2.3	Cultural and Historic Resources																															
A2.3.1	Archaeological Resources Study			1					11	18																	5	4	39	\$6,070.00	\$1,546.75	\$7,616.75
A2.3.2	AB 52			1					2	2																	5	\$855.00		\$855.00		
A2.3.3	Built Environment Inventory		2								12	88														4	16	6	128	\$14,540.00	\$345.00	\$14,885.00
A2.4	Noise and Vibration			2	8											30	8			4							2		54	\$8,700.00	\$115.00	\$8,815.00
A2.5	Transportation Impact Analysis																															
A2.5.1	TDM Plan			1																	4	12				1	2	20	\$3,670.00		\$3,670.00	
A2.5.2	VMT Analysis		2																		8	24	2	4	5	5	50	\$8,800.00	\$17,324.75	\$26,124.75		
A2.5.3	LOS Analysis			1																4	12	80	2	5	5	5	109	\$17,525.00	\$8,050.00	\$25,575.00		
Subtotal Task A2			6	8	8		6	60	13	20	12	88	6	4	33	30	8		4	4	24	36	82	16	36	28	532	\$79,795.00	\$27,548.25	\$107,343.25		
Phase A Total Hours			19	25	8	10	6	60	13	20	12	88	6	4	33	30	8	0	4	4	24	36	82	22	38	30	582					
Phase A Total Cost																													\$87,865.00	\$27,548.25	\$115,413.25	
Task B1	Administrative Draft NOE		5	8		10																					26	\$3,850.00		\$3,850.00		
Task B2	Screencheck Draft NOE		3	4		5																					12	\$1,840.00		\$1,840.00		
Task B3	Final NOE and Filing		2	2		3																					3	4	14	\$1,910.00		\$1,910.00
Phase B Total Hours			10	14	0	18	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	4	52				
Phase B Total Cost																													\$7,600.00	\$0.00	\$7,600.00	
Task C1	Administrative Draft Initial Study	3	20	24	24	40																				8	15	8	142	\$20,400.00		\$20,400.00
Task C2	Screencheck Draft IS/MND		12	15		18																				4	4	4	57	\$8,485.00		\$8,485.00
Task C3	Public Draft IS/MND		5	8		12																										
Task C4	Response to Comments/MMRP	2	14	20	18	26																					5	3	88	\$13,610.00	\$149.50	\$13,759.50
Task C5	Meeting and Hearings		18	8		3																						29	\$5,835.00		\$5,835.00	
Task C6	Project Management	2	28	12																												
Phase C Total Hours		7	37	87	42	99	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	28	23	335				
Phase C Total Cost																														\$61,835.00	\$239.00	\$62,134.00
Optional Services																																
TRAN-1	Peak Hour Intersection Counts			1																								5	\$745.00	\$6,440.00	\$7,185.00	
TRAN-2	CMP Analysis			1																		2						27	\$4,175.00		\$4,175.00	
AQ-1	Construction HRA		2				6	25																								
AQ-2	Operational HRA		1				3	21																								
CIJLT-1	Additional AB 52 Consultation Support		5						12	8																						
CONT-1	General Contingency	4	20	24	14	20																				8						
Total Optional Hours and Fee		4	28	26	14	20	9	46	12	8	0	0	0	0	0	0	0	0	0	0	2	0	28	8	0	0	205	\$34,300.00	\$7,015.00	\$41,315.00		

Appendix A

Resumes

Katherine Waugh, AICP

SENIOR PLANNER

Katherine Waugh is a senior planner with 22 years' experience with California Environmental Quality Act (CEQA) statutory requirements, current planning methods, and environmental documentation procedures. She prepares CEQA documents for a wide range of public and private projects, managing projects effectively and maintaining momentum to meet schedule and budget requirements. Ms. Waugh applies planning and environmental laws and regulations practically and with an attention to detail, allowing her to quickly identify and resolve critical planning and environmental issues. She engages in efficient consultation with local and state agencies to ensure the resources and infrastructure these agencies manage are thoroughly evaluated and that outside agency requirements can be integrated with the Lead Agency's mitigation measures and development review procedures.

Education

University of California,
Davis
BS, Environmental Policy
Analysis and Planning,
1997

Certifications

American Institute of
Certified Planners (AICP)

Professional Affiliations

American Planning
Association

Association of
Environmental
Professionals

Project Experience

123 Independence Drive Residential Development, City of Menlo Park, California. Project manager for the preparation of an environmental impact report (EIR) evaluating a project that proposes to demolish five existing office/industrial buildings and construct a 316-unit apartment building and 116 townhomes on an approximately 8-acre project site in the bayfront area of the City of Menlo Park.

1005 O'Brien Drive/1320 Willow Road Research and Development Project, City of Menlo Park, California. Project manager for a detailed initial study (IS) and focused EIR evaluating the demolition of existing research and development buildings and the construction of two new research and development buildings, structured parking, and publicly accessible open space on an approximately 4-acre site in the eastern portion of the City of Menlo Park's bayfront area.

City Hall and Medical Office Building, City of Citrus Heights, California. Project manager for the preparation of a detailed IS and focused EIR to evaluate the proposed demolition of the existing city hall, the construction of a medical office building on the current city hall site, and the construction of a new city hall at a new site. Key issues included traffic, hazards and hazardous materials, noise, aesthetics, and land use compatibility.

1050 Page Mill Road, City of Palo Alto, California. Project manager for an EIR for the demolition of 285,000 square feet of existing office/warehouse/research and development space and construction of the equivalent amount of office space. Determining the appropriate baseline condition from which to evaluate impacts to provide an appropriate representation of the historic and recent use of the site was a critical issue.

Castilleja School Project, City of Palo Alto, California. Project manager for the preparation of an EIR evaluating the private school's request for a Conditional Use Permit amendment to increase the campus's enrollment cap and approval of a modernization program that includes the demolition of several existing buildings and the construction of an underground parking garage and a new academic building and the replacement of the existing pool. Critical project issues relate to the project's compatibility with the surrounding single-family residential neighborhood, including transportation, noise, air quality, and aesthetics. Despite the high level of public interest and controversy, no legal challenge to the City's EIR certification or project approvals was filed.

3877 El Camino Mixed-Use Project, City of Palo Alto, California. Project manager for an IS/MND evaluating the demolition of an existing one-story commercial building; the construction of a three-story, mixed-use building and the construction of 15 additional townhomes in three buildings, with one level of underground parking.

PG&E Sacramento Street Renovation Project, City of Auburn, California. Project manager for Dudek's peer review of technical studies, an IS/MND, and a mitigation monitoring and reporting program drafted by the project applicant's consultants for PG&E's proposed renovations to their storage and operations yard located in the City of Auburn. The project includes interior renovations to one building, the demolition of several storage buildings, the construction of a single warehouse, and associated hardscape and landscape updates.

Northstar California Biomass Energy Plant, Northstar Community Services District, Northstar California Resort, California. Project manager for Dudek's preparation of an IS/MND evaluating Northstar Community Services District's proposed development of a biomass energy plant, which would be connected to approximately 13 buildings and facilities within and adjacent to Northstar Village to provide heating and reduce annual natural gas consumption. Key project issues include air quality and aesthetics.

Dorsey Marketplace Mixed-Use Lifestyle Center, City of Grass Valley, California. Project manager for Dudek's preparation of an EIR for the Dorsey Marketplace project in the City of Grass Valley, which includes 104,350 square feet of commercial space; 8,500 square feet of office space; and 172 multifamily dwelling units. Key issues included traffic, aesthetics, and remediation of hazardous soil conditions due to the site's prior mining use.

Placer County Government Center Master Plan Update, County of Placer, California. Project manager for Dudek's role in the County's recent effort to update the master plan for the County's primary Government Center, located north of the City of Auburn. Dudek participated in public workshops and a preliminary site evaluation and design led by the County's architectural consultant. Dudek also prepared an EIR for the proposed master plan update. Critical project issues included provision of public services and utilities, effects to the designated historic district on site, and aesthetics. Ms. Waugh continues to manage Dudek consulting services in support of County staff as they work to implement the master plan. Between 2003 and 2005, served as project manager for an EIR, EIR addendum, and two MNDs for a series of projects at the government center campus. Some of these projects involved the demolition of World War II-era buildings and the construction of new office buildings, justice center facilities, and an emergency residential shelter at the campus. These projects required a California State Historic Preservation Officer consultation; California Department of Transportation (Caltrans) Division of Aeronautics and Placer County Airport Land Use Commission approval of the height of a communications tower; special-status species surveys; and aquatic resource permits.

Augustin Bernal Mountain Bike Trail, City of Pleasanton, California. Project manager for the preparation of an IS/MND evaluating the City's proposal to construct a single-track downhill mountain bike trail within an existing community park. The location of the proposed trail already contains segments of user-created trails that exhibit erosion and rutting and contain substandard jumps and other trail features. The City's goal is to create a properly engineered and constructed trail that can be used by riders with a wide range of abilities and decommission user-created trails to reduce adverse environmental effects caused by those trails.

Ann Sansevero, AICP

PRINCIPAL AND SENIOR PROJECT MANAGER

Ann Sansevero is a certified project manager and a certified planner through the American Institute of Certified Planners (AICP). Ms. Sansevero has 35 years' experience in the field of environmental assessment and land use planning with a broad range of experience in the management and preparation of California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA) documents, other types of planning and environmental studies, and regulatory and land use permitting applications for public projects. Her areas of expertise include environmental planning, project management, CEQA/NEPA compliance, regulatory and coastal permitting, land use planning and design, sustainability, differential site assessments, and mitigation monitoring and reporting.

Education

University of California,
Santa Cruz
BA, with double major in
Biological Sciences and
Environmental Studies

Certifications

American Institute of
Certified Planners (AICP)

Project Experience

CSU Chancellor's Office Master Enabling Agreement for Systemwide CEQA Services. Ms. Sansevero is the Northern California lead for Dudek under the Master Enabling Agreement (MEA) with the CSU Chancellor's Office. Dudek is one of only six firms selected to perform CEQA services system wide. Under this contract, Ms. Sansevero has overseen or is overseeing CEQA processes at San Francisco State University, CSU East Bay, CSU Stanislaus, CSU Monterey Bay, CSU Maritime Academy, CSU Chico, CSU Fresno, and Cal Poly Pomona. Most recently, the CSU Monterey Bay Master Plan EIR was completed and certified in May 2022 by the CSU Board of Trustees without any legal challenges. Ms. Sansevero participated in planning and presenting the 2012, 2017 and 2019 CSU System-wide CEQA Workshop sponsored by the CSU Chancellor's Office and she regularly provides CEQA advice to the Chancellor's Office and to individual campuses.

UC Berkeley Clark Kerr Campus Beach Volleyball Project EIR and Cal Softball Field Renovation Project EIR. Ms. Sansevero is overseeing two EIRs for women's athletic projects at UC Berkeley. The beach volleyball project involves the conversion of the existing recreational softball field to recreational and Intercollegiate Athletic (IA) beach volleyball courts to improve the training and competition facilities provided to female student athletes, support ongoing gender equity, and comply with Title IX. In addition, the project would demolish a portion of Building 21, the former Wilkinson Lodge, which is a contributor to the National Register of Historic Places District No. 82000962 State Asylum for the Deaf, Dumb, and Blind. The softball project involves renovation of the existing softball field to meet modern safety and competition standards for the IA Women's Softball Program and Recreational Sports (Rec Sports) Intramural softball players, as well as support campus compliance with Title IX. Both projects are of interest to a range of UC Berkeley stakeholders, including surrounding neighbors.

Santa Cruz Water Rights Project EIR. Served as project manager for the Santa Cruz Water Rights Project EIR that provided program- and project-level CEQA coverage for numerous project components spanning the City's entire service area. The components of the project include modifications to existing water rights and related actions required to implement the proposed modifications. The underlying purpose of the project is to improve City water system flexibility while enhancing stream flows for local anadromous fisheries. To account for further constraints on the City's limited surface water supply from implementation of bypass flows for Central California Coast coho salmon, a federally listed endangered species, and the Central California Coastal steelhead, a federally listed

threatened species, the City needs to improve water system flexibility within existing allocations to allow better integration and use of this limited resource. Specifically, the EIR evaluated water rights modifications, expanded places of use, diversion methods and points that involve the Tait Street Diversion and the Felton Diversion, extension of time to put water to full beneficial use, and physical infrastructure improvements associated with the water rights modifications including aquifer storage and recovery (ASR) facilities, Tait Street Diversion/Coast Pump Station improvements, Felton Diversion improvements, and interties and water transfers with neighboring agencies. Ms. Sansevero worked effectively as part of a large project team that included City staff, water supply and habitat modeling consultants, water rights experts, and legal counsels. Primary responsibilities included providing project description support, managing all aspects of EIR preparation, contract management, and CEQA public meeting support. The EIR was certified in December 2021.

SCWD² Regional Seawater Desalination Project EIR and Up-Front Project Planning Support Services, Santa Cruz, California. Served as project manager for the scwd² Regional Seawater Desalination Project EIR and related services conducted for the City of Santa Cruz (City) and the Soquel Creek Water District (District). These agencies were pursuing a joint desalination facility project to address water supply shortages during drought and concerns about groundwater over-pumping and seawater intrusion. Primary responsibilities included providing upfront planning and project development support; managing the EIR and Seawater Intake Conceptual Design Report preparation process, in-house staff, and all aspects of contract management, scope definition, document content, administrative reviews, and team coordination; interfacing with the City, District, and consultant team involved with the proposed project; and playing the lead role in all CEQA-related public meetings. The draft EIR was issued in May 2013, but the project was put on hold in 2013 while the City’s Water Supply Advisory Committee (WASC) completed a water supply planning process. Desalination was identified as a backup source of water supply in the final WASC report that was recently completed. Ms. Sansevero also oversaw an updated 2018 review of the feasibility, cost, timeliness, and approach for pursuing a seawater desalination facility for use by the City of Santa Cruz, in collaboration with Kennedy/Jenks Consultants. This review is supporting the City’s selection of a supplemental or replacement water supply.

City of Santa Cruz On-Call Environmental Services for Water Department Capital Investment Program. Serving as program manager on a 5-year master services agreement with the City of Santa Cruz to provide environmental, permitting, and technical services for the Santa Cruz Water Department’s Capital Investment Program (2020–2024). Projects completed include Laguna Creek Dam Retrofit Project Environmental Impact Report (EIR) and permitting and the Newell Creek Pipeline Replacement Project and permitting. Projects underway include the Graham Hill Water Treatment Plant Facility Improvement Project EIR, the Santa Cruz/Scotts Valley Intertie EIR Addendum, and providing support documentation for federal funding for a number of different projects. Under prior on-call agreements, oversaw environmental review and permitting projects related to the North Coast Pipeline System, the U5 tank, the Tait Well System, the Graham Hill Water Treatment Plant, and a Water Transfer Pilot Project with Soquel Creek Water District. Additionally, prior to her tenure with Dudek, Ms. Sansevero served as extension of staff to Santa Cruz Water Department (SCWD), and oversaw the environmental consultant preparing the CEQA document, technical studies, and regulatory permitting for the North Coast Pipeline Rehabilitation Phase 3 Project adjacent to Highway 1 in Northern Santa Cruz County.

Kirsten Burrowes

ENVIRONMENTAL PLANNER/PROJECT MANAGER

Kirsten Burrowes (*KEER-stin BURR-ohs; she/her*) is an environmental planner and project manager with 8 years' professional experience in environmental consulting and California Environmental Quality Act (CEQA)/National Environmental Policy Act (NEPA) compliance. She brings experience in management, environmental review, and resource permitting for a variety of projects throughout California and Nevada. She has successfully held management roles on a range of small-scale to complex and controversial projects involving community planning, public and private development, stormwater and water resources, utility and infrastructure, and transportation. Ms. Burrowes has comprehensive experience with projects that involve unique cultural and tribal cultural resource issues, and she has a broad understanding of required federal and state regulations. She is also versed in environmental monitoring management with experience assisting clients in implementation of mitigation monitoring programs during construction.

Ms. Burrowes offers a diversity of environmental compliance expertise, which enables her to perform difficult tasks under deadlines, execute strong project deliverables, and effectively collaborate with clients and team members.

Education

Sonoma State University
BA, Environmental Studies
and Planning, German
Language Minor, 2015

Professional Affiliations

Superior Chapter of
Association of
Environmental
Professionals

Former Communications
Chair, Young Planners
Group, Sacramento Valley
Section, American Planning
Association

Relevant Experience

1005 O'Brien and 1320 Willow Road Life Science Project, City of Menlo Park, San Mateo County, California. Tarlton Properties has submitted an application to the City of Menlo Park requesting approval of various discretionary entitlements in support of the proposed 1005 O'Brien Drive and 1320 Willow Road Research and Development project. The project involves demolition of existing site structures, construction of two research and development buildings that support light industrial and bioscience/biomedical product development, construction of a 6.5-story parking garage, and public space/onsite amenities for users. Currently serves as deputy project manager for preparation of the Initial Study and focused EIR. (2022)

123 Independence Drive, City of Menlo Park, San Mateo County, California. The project involves demolition of five existing office and industrial buildings and construction of 316 rental apartments and 116 for-sale townhomes, onsite parking, and residential amenities. The project site is in the Bayfront Area of Menlo Park, which has been identified in the City of Menlo Park General Plan Land Use and Circulation Update for buildout potential and future development over a 24-year planning horizon. Key issues included air quality, biological resources, cultural resources, geology/soils, and hazardous materials. Served as environmental planner for the EIR and authored the population/housing, hazards and hazardous materials, and project alternatives sections. (2022)

Capitol Annex Project EIR, California DGS, Real Estate Services Division, Sacramento County, California. An EIR was prepared for the Capitol Annex Project in downtown Sacramento. The project would demolish and reconstruct the existing 325,000-square-foot existing Capitol Annex building with a new approximately 525,000-square-foot building. The project would address numerous deficiencies in the existing building, including life safety/building code deficiencies, noncompliance with Americans with Disabilities Act (ADA) standards, overcrowding, aging and failing infrastructure, and insufficient public and working space. A new underground visitors/welcome center would be located between 10th Street and the west steps of the Capitol. Existing basement parking under the Annex would be

abandoned and replaced with new underground parking on the south side of the Capitol. Key issues in the EIR include historic architecture, tribal cultural resources, impacts to Capitol Park, transportation and circulation, air quality and GHG emissions, and energy efficiency. Served as an assistant project manager. Responsibilities involved regular and extensive coordination with the client, project stakeholders, and subconsultants, as well as project strategy and planning and coordinating financial aspects of the project. (2019–2022)

10th and O Street Building EIR, California DGS, Sacramento County, California. An EIR was prepared for the demolition of a surface parking lot and development of a new office building in downtown Sacramento on the block bound by 10th/11th Streets and O Street/Neighbors Alley and construction of a new office building at the same location. The EIR also evaluated demolition of a state-owned surface parking on the southwest corner of the block bounded by P, Q, 11th, and 12th Streets and construction of a new childcare facility at the site to replace an existing childcare facility on the southeast corner of the same block. The EIR has been certified, and the office building is under construction. The CEQA team is managing the provision of environmental monitoring and assisting with implementation of EIR mitigation measures during construction. The new structure, completed via the design-build method of project delivery, will be a state-of-the-art, Zero Net Energy building that includes numerous sustainability and energy efficiency measures. The project was authorized and funded by the State of California through the State Projects Infrastructure Fund. The EIR was completed on an expedited schedule, extending only 6 months from project initiation to approval. Key issues addressed in the EIR included traffic and transportation, utility conveyance, air quality and GHG emissions, and energy efficiency. Served as an assistant project manager and authored various EIR sections, attended client and community meetings, and regularly coordinated with client and subconsultants. (2018)

Resources Building Renovation Project EIR, California DGS Sacramento County, California. The existing Resources Building at 1416 9th Street was judged to be among the poorest in terms of physical condition in a 2015 study conducted by the State. The building houses departments of California’s Natural Resources Agency, including Fish and Wildlife, Parks and Recreation, Water Resources, and others. The CEQA team prepared an EIR for the DGS, Real Estate Services Division for renovation of the building at its current location in downtown Sacramento. Served as the assistant project manager and coordinated with construction, design, and subconsultant teams, attended client meetings, and oversaw project management tasks, such as contracting and agreements with the client and subconsultants. Also served as an author to several sections of the EIR. (2019–2020)

Jesse M. Unruh Building Renovation Project EIR, California DGS, Sacramento County, California. An EIR was prepared to evaluate the environmental effects of renovating the Jesse M. Unruh Building, located at 915 Capitol Mall in downtown Sacramento. The building was constructed on land donated to the State by the City of Sacramento in 1913 and was first occupied in 1929. The building has received limited updates since its original construction. An infrastructure study completed in 2008 and updated by DGS in 2013 identified a variety of fire and life safety, building code, and other infrastructure deficiencies, as well as the presence of hazardous materials needing remediation. The project would involve a comprehensive renovation to extend the useful life span of the building by approximately 50 years and improve tenant safety and comfort. The project would restore historic elements as feasible, provide a new office layout for existing tenants, and remove the Capitol fountain. The project’s goal is to achieve Zero Net Energy and LEED v4 Silver certification. As the assistant project manager, assisted in preparation of the CEQA document for the project and authored the following environmental analyses: utilities and service systems and hazards and hazardous materials. (2019)

Matthew Morales

AIR QUALITY SPECIALIST

Matthew Morales is an air quality specialist with 18 years' experience preparing technical analyses for numerous planning and environmental projects related to development, natural resource management, and facility expansion. Mr. Morales is trained in air quality, including toxic air contaminants (TACs) and greenhouse gas (GHG), and he is adept at applying air quality models, such as the California Emissions Estimator Model, Caline4, AERSCREEN, AERMOD, and HARP 2, to perform quantitative analyses for National Environmental Policy Act and California Environmental Quality Act (CEQA) environmental documents, such as environmental impact reports (EIRs), initial studies (ISs), and mitigated negative declarations (MNDs).

Education

University of California,
Davis
BS, Environmental
Toxicology

Professional Affiliations

Association of
Environmental
Professionals
Air and Waste
Management Association

Project Experience

Station Avenue Project – Central Rohnert Park Priority Development Area Plan EIR Consistency Review, City of Rohnert Park, California. The Station Avenue Project is within the Central Rohnert Park Priority Development Area Plan area. This analysis was prepared to evaluate the consistency of the project with the Priority Development Area EIR. The project would remove the two existing buildings (former State Farm Insurance building and City's Corporation Yard), surface parking lots, trees, and grass areas and would result in the construction of a central business district, urban neighborhood, and new downtown area for the city. As part of the consistency review, an HRA was performed that assessed potential cancer and chronic health risk at existing residences proximate to the site, as well as operational health risk for the new residents associated with exposure to TACs from major roadways and the adjacent Sonoma-Marin Area Rail Transit operations.

Belden Barns Farmstead and Winery EIR, Sonoma County, California. As the air quality analyst, assessed the criteria air pollutant and GHG emissions associated with construction and operation of the project, which includes development of a winemaking, hospitality, and farmstead food production facility.

Roberts' Ranch Specific Plan EIR, City of Vacaville, California. As the air quality analyst, assessed the criteria air pollutant emissions associated with construction and operation of the Roberts' Ranch Specific Plan land uses in the City of Vacaville.

Grapevine Project Air Quality and GHG Technical Report, Tejon Ranch Corporation, Kern County, California. Prepared the air quality and GHG emissions technical report for the project. The Grapevine Specific Plan project, which is located in the west-central portion of 270,000-acre Tejon Ranch, would be developed as a residential community and employment center within 4,780 acres of the 8,010-acre property. The project, which includes up to 12,000 residential units and 5.1 million square feet of commercial and light industrial land uses (including a community college and medical campus), is designed as a series of conveniently located village centers, each composed of a mix of housing, neighborhood-serving retail and office uses, schools, parks, and community services. Specific tasks include construction and operational criteria air pollutant and GHG emissions estimates, industrial source emissions calculations, odor assessment, Valley Fever assessment, and other air quality topics.

Avram Apartments Air Quality and GHG Technical Memorandum, City of Rohnert Park, California. Served as air quality analyst. Assessed the criteria air pollutant, GHG, and TAC emissions associated with the construction and operation of the Avram Apartments project. A construction health risk assessment was prepared to estimate potential risk of proximate sensitive receptors from exposure to diesel exhaust from construction equipment and trucks. An operational health risk assessment was also prepared to estimate potential risk of on-site residents to diesel particulate matter from truck traffic on Highway 101.

Oakmont Senior Assisted Living Facility IS/MND, City of Novato, California. As the air quality analyst, assessed the criteria air pollutant and GHG emissions associated with construction and operation of the proposed assisted living community within the City of Novato.

Clearwater at Sonoma Hills Assisted Living and Memory Care Facility IS/MND, City of Rohnert Park, California. As the air quality analyst, assessed the criteria air pollutant and GHG emissions associated with construction and operation of the project, which includes development of an assisted living and memory care facility within the City of Rohnert Park.

Residences at Five Creek Project IS/MND, City of Rohnert Park, California. As the air quality analyst, assessed the criteria air pollutant, GHG, and TAC emissions associated with the construction and operation of the Residences at Five Creek mixed-use and City public safety and public works facility. A construction health risk assessment was prepared to estimate potential risk of proximate sensitive receptors from exposure to project-related diesel exhaust from construction equipment and trucks. A cumulative operational health risk assessment was also prepared to estimate potential risk of on-site residents to TACs from permitted stationary sources within 1,000 feet of the project site.

Bellevue Ranch 7 Project IS/MND, City of Santa Rosa, California. As the air quality analyst, assessed the criteria air pollutant and GHG emissions associated with construction and operation of the project, which includes development of 30 single-family homes within the City of Santa Rosa.

Creative Arts and Holloway Mixed-Use Project EIR, San Francisco State University, San Francisco, California. The proposed project includes construction of new housing, neighborhood-serving retail, and student support services on the south side of Holloway Avenue, and construction of the Creative Arts replacement building and concert hall on the north side of the Holloway Avenue/Font Boulevard intersection. The project would also include preparation and implementation of design guidelines, transportation and parking improvements, utility connections, storm drainage improvements, landscaping, and lighting. Prepared the air quality and GHG chapters of the EIR for the project.

Performing Arts and Culinary Services Facility Project IS/MND, Woodland Community College, Woodland, California. The proposed project includes construction of a new 29,118-square-foot Performing Arts and Culinary Services Facility, which will provide for a new facility to consolidate and expand space for the Woodland Community College's Performing, Fine Arts and Speech programs while creating space for a new Culinary Arts program. Prepared the air quality and GHG analyses for the project.

San Pablo Municipal Broadband Project IS/MND, San Pablo, California. The proposed San Pablo Municipal Broadband Project includes the installation of a fiber-optic ring, spur lines (or running lines), and aggregators that connect to the fiber-optic ring infrastructure. From these aggregators (either in prefabricated fiber huts or existing equipment rooms in existing commercial buildings), the fiber-optic cables would travel along existing streets (below ground) into vaults or utility cabinets and to and from the handholes/cabinets directly to customers. Prepared the IS/MND sections to address air quality and GHG emissions impacts of the project.

Ian McIntire

AIR QUALITY SPECIALIST

Ian McIntire is an air quality specialist with 9 years' experience specializing in the preparation of technical documents and analysis through interpretation of state and federal legislation, environmental document preparation and review, criteria pollutant and greenhouse gas emissions (GHG) modeling. Mr. McIntire is trained in air quality, including GHG and climate change. He is adept at applying air quality models, such as the California Emissions Estimator Model, Caline-4, AERMOD, and HARP2 to perform quantitative analyses for National Environmental Policy Act (NEPA) and California Environmental Quality Act (CEQA) environmental documents, such as environmental impact reports (EIRs), initial studies (ISs), and mitigated negative declarations (MNDs).

Education

California State University,
Sacramento
BS, Environmental
Studies

Professional Affiliations

Association of
Environmental
Professionals

Project Experience

123 Independence Drive Residential Development EIR, City of Menlo Park, California. Lead the air quality, GHG, and energy assessments for the EIR for a project that proposes to demolish five existing office/industrial buildings and construct a 316-unit apartment building and 116 townhomes on an approximately 8-acre project site in the bayfront area of the City of Menlo Park. As air quality, GHG, and climate change analyst, developed the CEQA analysis of air quality and GHG impacts from construction and operation of the project including preparation of construction and roadway HRAs. Air quality emissions and impacts were based on calculations using the CalEEMod models and project specific information.

24th and Waverly Air Quality and Greenhouse Gases Technical Assessments, City of Oakland, California. The project includes construction of a mixed-use residential building of approximately 411,000 GSF. The building would be up to 160 feet in height and composed of an 11 to 12-story L-shaped mid-rise residential tower with 328 dwelling units, of approximately 294,000 GSF, with 13,640 GSF of double height ground floor retail. As air quality, GHG, and climate change analyst, developed the CEQA analysis of air quality and GHG impacts from construction and operation of the project, as well as a health risk assessment to analyze TAC (such as diesel particulate matter) exposure at off-site sensitive receptors from construction and operation of the project.

5531 State Farm Drive Air Quality and Greenhouse Gases Technical Report, City of Rohnert Park, California. The project includes the construction of a 9,000 square foot warehouse building on a 1.70-acre site, which is currently occupied by two buildings totaling 14,156 square feet. As air quality, GHG, and climate change analyst, developed the CEQA analysis of air quality and GHG impacts from construction and operation of the project facilities. Air quality emissions and impacts were based on calculations using the CalEEMod model.

655 4th Street Project Air Quality Assessment, City of San Francisco, California. The project included demolition of existing uses on the project site and construct two new buildings with approximately 960 dwelling units in a mix of 257 studios, 323 one-bedroom units, 343 two-bedroom units, and 37 three-bedroom condominiums. In addition the proposed project would include 10,439 square feet of retail, 11,459 square feet of restaurant space, Building 2 would include 38 hotel rooms, which would be located on the sixth and seventh floors and 21,840 square feet of office space. As air quality analyst, prepared the CEQA air quality technical assessment and emission calculations using the CalEEMod model.

Egbert Switching Station Project EIR, California Public Utilities Commission, California. The project includes the construction, operation, and maintenance of a new 230-kilovolt (kV) switching station in the City and County of San Francisco. The switching station would be connected to the local 230 kV system by reconfiguring two existing, underground, single-circuit, 230 kV transmission lines located in the City and County of San Francisco, City of Daly City, and City of Brisbane. As air quality, GHG, and climate change analyst, developed the CEQA analysis of air quality and GHG impacts from construction and operation of the project. Air quality emissions and impacts were based on calculations using the CalEEMod models.

Estero Trail EIR, Sonoma County, California. The project would implement two new 50-foot-wide, pedestrian-only trail corridors, two parking and trailhead amenity areas, and an improved access road to the trailheads. The EIR studied 31.8 acres of project area, with approximately 4.8 acres of actual potential disturbance associated with construction of two 5-foot-wide trails within the designated trail corridors. As air quality, GHG, and climate change analyst, developed the CEQA analysis of air quality and GHG impacts from construction and operation of the project. Air quality emissions and impacts were based on calculations using the CalEEMod models.

Fairfield Inn and Suites Project IS/MND, City of Rohnert Park, California. The project included development of a 100-room hotel on a vacant 1.83-acre parcel. The hotel building would have five floors would have a combined total building area of approximately 57,670 square feet. As air quality analyst, prepared CEQA air quality modeling and prepared IS/MND air quality and GHG sections.

Home2 Suites by Hilton Project IS/MND, City of Rohnert Park, California. The project included development of a 96-room Home2 Suites by Hilton hotel at 6490 Redwood Drive in Rohnert Park. As air quality analyst, prepared the CEQA Air Quality and GHG Emissions Assessment Technical Memorandum. Air quality and GHG emissions impacts were based on calculations using the CalEEMod model.

Hunter Subdivision Project EIR, City of St. Helena, California. Lead the air quality, GHG, and energy assessments for the EIR for a proposed residential project on an approximately 17-acre parcel located near downtown St. Helena. The project includes 51 single-family homes, 25-multi-family units, and up to 25 accessory dwelling units. The analysis included a detailed weekday, Saturday intersection LOS analysis, vehicle miles traveled analysis, and horizon year analysis. As air quality, GHG, and climate change analyst, developed the CEQA analysis of air quality and GHG impacts from construction and operation of the project. Air quality emissions and impacts were based on calculations using the CalEEMod models and project specific information.

70-74 Liberty Ship Way Project IS/MND, City of Sausalito. The project included redevelop a 3.9-acre site and construct three two-story buildings within the Marinship Specific Plan area. As air quality analyst, prepared the CEQA Air Quality and GHG Emissions Assessment Technical Memorandum. Air quality and GHG emissions impacts were based on calculations using the CalEEMod model.

Shadelands Gateway Specific Plan and the Orchards at Walnut Creek Project EIR, City of Walnut Creek, California. The project included development of a new Safeway grocery store, retail, restaurant, and a senior housing/continuing care retirement community (CCRC) facility. As air quality analyst, prepared air quality and GHG analyses and developed mitigation measures for both the project and its less intense alternatives.

Emily Scricca

BIOLOGIST

Emily Scricca is a wildlife biologist with more than 12 years' experience with natural resource-related projects throughout California, including experience conducting protocol-level surveys, assessments, and biological monitoring for numerous species, including burrowing owl (*Athene cunicularia*), Swainson's hawk (*Buteo swainsonii*), tricolored blackbird (*Agelaius tricolor*), western snowy plover (*Charadrius alexandrinus*), California tiger salamander (*Ambystoma californiense*), California red-legged frog (*Rana draytonii*), foothill yellow-legged frog (*Rana boylei*), California giant salamander (*Dicamptodon ensatus*), Santa Cruz black salamander (*Aneides flavipunctatus niger*), western pond turtle (*Actinemys marmorata*), American badger (*Taxidea taxus*), San Francisco dusky-footed woodrat (*Neotoma fuscipes annectens*), and San Joaquin kit fox (*Vulpes macrotis*). Ms. Scricca possesses a federal 10(a)1(A) recovery permit for California tiger salamander. From her various positions in California, Ms. Scricca has strong knowledge of California flora and fauna and is well versed in environmental survey and sampling techniques. Ms. Scricca has a strong background in endangered species permitting, biological impact assessment, and biological compliance implementation. Ms. Scricca has prepared numerous technical reports in support of the environmental review and permitting processes for dozens of projects.

Past Employment

- 2016 – 2019 Live Oak Associates, Inc., Assistant Project Manager and Ecologist
- 2015 – 2017 Santa Clara Valley Water District (Valley Water), Water Utility Biologist Graduate Student Intern
- 2013 – 2015 San Francisco Bay Bird Observatory, Snowy Plover Biologist

Project Experience

UC Berkeley Levine-Fricke Stadium Project, Alameda County, California (2022- present). Lead biologist for the proposed Levine-Fricke Stadium Project within the Strawberry Canyon Recreational Area in Berkeley, CA. Conducted a field biological resources assessment and prepared the biological resources section of the Draft ADEIR CEQA document for the project.

123 Independence Drive Project, City of Menlo Park, California (2021-2022). Lead biologist for the proposed re-development project. Conducted reconnaissance-level biological field survey assessments and prepared the biological resources chapter of the EIR. Responded to comments and drafted edits of the chapter in response to comments.

Education

San Jose State University
MS, Environmental
Studies, 2017

University of Vermont
BS, Animal Sciences,
2010

Certifications

USFWS, Section
10(a)(1)(A) Native
Endangered and
Threatened Species
Recovery Permit,
No. TE45251C-0

- California tiger
salamander

CDFW, Memorandum of
Understanding, Scientific
Collecting Permit,
No. SC-013755

- California Tiger
Salamander

CDFW, Scientific
Collecting Permit, Special
Use Permit, No. S-
220390005-22040-002

Professional Affiliations

National Wildlife
Federation

San Francisco Bay Bird
Observatory

The Wildlife Society

Western Bird Banding
Association

State Route 17 Emergency Shaded Fuel Break, CAL Fire & Santa Clara County FireSafe Council, Santa Clara County, California (2019- 2021). Lead Biologist for the 6.5 mile, 494-acre shaded fuel break project along both sides of Highway 17 from the Santa Clara/Santa Cruz County boundary to the Town of Los Gatos. Work included conducting biological surveys, reporting, pre-construction trainings, implementation of project-specific best management practices, agency coordination, mapping locations of sensitive resources, permit package preparation and implementation of avoidance measures.

As-Needed Environmental Services, San Jose Water Company, Santa Clara County, California (2019- present). Lead Biologist/CDFW approved biologist under CDFW LSAA, for multiple projects associated with the Los Gatos Creek Watershed Maintenance Program. Work includes conducting biological surveys and reporting, surveys for special status animals such as roosting bats, foothill yellow-legged frog, California red-legged frog, western pond turtle, steelhead, and San Francisco dusky-footed woodrat, compliance monitoring, and implementation of best management practices associated with RWQCB, CDFW, and USFWS authorizations.

County of Santa Clara Solar Panel Installation Project, County of Santa Clara, San Jose to Morgan Hill, California (2021-2022). The project consisted of the installation of photovoltaic solar panels at 14 County-owned sites between San Jose and Morgan Hill to further expand on the County’s renewable energy portfolio and reduce County emissions from operations. Assisted in the preparation of a biological resource assessment to support CEQA documentation and preparing Santa Clara Valley Habitat Plan Reporting Form Application for Public Projects on behalf of the County.

Lexington Reservoir Eucalyptus Management Project, Valley Water, Santa Clara County, California (2021- present). The project consists of the herbicidal treatment of large stands of Eucalyptus along Lexington Reservoir in Los Gatos, California. Conducted biological resources assessments and special-status species habitat assessments within the project area and prepared a biological resource assessment to support CEQA documentation and CDFW LSAA permit application package on behalf of Valley Water.

Newby Island Landfill Bank Stabilization Project, San Jose, California. Assisted in the preparation of a biological evaluation report for the Newby Island Bank Stabilization project site. Conducted field evaluations in which the biotic resources of the site were determined, and evaluated the potential for green sturgeon (*Acipenser medirostris*), central California coast steelhead (*Oncorhynchus mykiss*), California Ridgeway’s rail (*Rallus obsoletus obsoletus*), and salt marsh harvest mouse (*Reithrodontomys raviventris*) to be impacted by planned project activities. The report also included an Essential Fish Habitat Assessment. (2018)

Specialized Training

- California Red-Legged Frog Level II Workshop, The Wildlife Project, Feb – Oct 2020.
- Foothill Yellow-Legged Frog Workshop, The Wildlife Project, October 2019.
- Fifty Plant Families in the Field: San Francisco Bay Area, Jepson Herbarium, March 2018.
- Rare Pond Species Survey Techniques Workshop, Laguna de Santa Rosa Foundation, March 2017.
- Amphibians of the San Francisco Bay Area Workshop, Laguna de Santa Rosa Foundation, November 2016.
- CEQA Essentials Workshop, Association of Environmental Professionals, November 2016.
- California Tiger Salamander Terrestrial Ecology Workshop, Laguna de Santa Rosa Foundation, March 2016.

Matt Ricketts

FORESTRY/BIOTICS/BIOLOGICAL RESOURCES

Matt Ricketts (*MAT RICK-ets; he/him*) is a senior biologist with 21 years' experience as a wildlife biologist and conservation planner specializing in biological resource inventories and documentation, special-status species surveys, federal Endangered Species Act (ESA)/California ESA compliance, and environmental impact analysis under the California Environmental Quality Act (CEQA). He is also a skilled field biologist with extensive experience in the San Francisco Bay Area and Sacramento–San Joaquin Delta conducting biological resource site assessments, special-status wildlife surveys (e.g., burrowing owl [*Athene cunicularia*] and Swainson's hawk [*Buteo swainsoni*]), and preconstruction nesting bird surveys.

Mr. Ricketts enjoys the challenge of synthesizing complex scientific and regulatory information into reader-friendly documents and communicating this information to clients, regulatory agencies, and project stakeholders. He has worked on a wide range of project types and sizes under many roles, from construction monitor to meeting facilitator. He therefore understands the importance of balancing technical rigor with practical feasibility in environmental documents and strives to bring this balance to every project he works on.

Education

Eastern Kentucky University
MS, Biology/Applied Ecology, 1999

University of Illinois at Urbana-Champaign
BS, Natural Resources and Environmental Sciences, 1997

Certifications

U.S. Fish and Wildlife Service, ESA Section 10(a)(1)(A) Recovery Permit No. TE-61177B-0

Professional Affiliations

National Habitat Conservation Plan Coalition

The Wildlife Society

Project Experience

123 Independence Drive Project, City of Menlo Park, California. Served as senior peer reviewer for biological resources chapter of EIR. Used previous experience and familiarity with ConnectMenlo EIR to guide technical staff in their analysis. (2021)

1350 Adams Court Project, City of Menlo Park, California. While working at ICF, served as lead biologist. The proposed project involved the construction of a new life science building within the existing Menlo Park Labs Campus. Conducted site assessment to verify findings of previous biological resource report and wrote biological resources section of Initial Study. Summarized previous biological resource analysis conducted for ConnectMenlo EIR and analyzed whether the project would result in any changes from the previous analysis. (2018)

Sustainability Policy and Regulatory Update of the County of Santa Cruz General Plan/Local Coastal Program and Santa Cruz County Code, County of Santa Cruz, California. Serving as senior biologist. The proposed project is an update to the County's General Plan/Local Coastal Program and associated revisions to the Santa Cruz County Code to implement policies from the Sustainable Santa Cruz County Plan that was accepted by the Board of Supervisors in October 2014. Prepared the biological resources chapter of the draft Environmental Impact Report (EIR) that required synthesis of previous County-level policy EIRs. Analyzed potential impacts of over 30 policy updates on biological resources at a program level. The Administrative Draft EIR is currently under County review. (2021–Ongoing).

Newell Creek Pipeline Improvement Project, City of Santa Cruz, California. Serving as senior biologist. The project is being proposed to address structural deficiencies in and improve maintenance access to the existing 9.25-mile Newell Creek Pipeline between Loch Lomond Reservoir and the Graham Hill Water Treatment Plant. Co-authored the biological resource assessment report and biological resources section of the draft EIR (released for public review in November 2021). Upcoming work includes assisting the City with the federal ESA permitting strategy, including determining if the project qualifies for coverage under the City's Operations and Maintenance Habitat Conservation Plan (HCP) (2021–Ongoing).

Santa Cruz Water Rights Project, City of Santa Cruz, California. Served as biologist. The proposed project would modify water rights to expand authorized place of use, improve existing diversions, extend the City's time to put water to full beneficial use, and provide for underground storage to expand the City's water supply. Conducted field reconnaissance of project-level impact sites and co-authored the biological resources chapter of the EIR. Compiled and synthesized a large amount of available information on Santa Cruz County biological resources and analyzed potential impacts at both project and program levels for a complex project with many components. (2020–2021)

Erin Fisher-Colton, MS

BIOLOGIST I

Erin Fisher-Colton (*she/her*) is a biologist with 3 years' experience providing biological surveying, monitoring, and mapping for nesting birds, special-status species, and rare plants and 5 years' related experience with California birds, mammals, reptiles, and amphibians. Ms. Fisher-Colton has experience with California red-legged frog (*Rana draytonii*), western pond turtle (*Emys marmorata*), Ridgway's rail (*Rallus obsoletus*), burrowing owl (*Athene cunicularia*), San Francisco dusky-footed woodrat (*Neotoma fuscipes annectens*), and others. She is proficient with technical report writing and has familiarity with state and federal environmental laws, the California Environmental Quality Act, and the Santa Clara Valley Habitat Plan.

Project Experience

Development

East Bay Applied Sciences Center Project; California State University, East Bay; Hayward; California. Conducted a nesting bird and roosting bat habitat assessment to evaluate the potential for future nesting bird and roosting bat occupancy on the proposed project site. (2022)

Vaca Valley Hotel Construction, J and P Hospitality, Vacaville, California. Conducted biological monitoring and daily sweeps for western pond turtle and burrowing owl during culvert installation. (2022)

2410 and 2384 Stony Point Road Temporary Fence Installation, Affordable Housing Development Corporation/ Santa Rosa Ridge Point LLC, Santa Rosa, California. Conducted biological monitoring and daily sweeps for California tiger salamander (*Ambystoma californiense*) during ground disturbance activities on a 3.96-acre site. (2022)

Eastern Hillside Stabilization, California State University Maritime Academy, Vallejo, California. Conducted surveys for burrowing owl, roosting bats, and nesting birds on a steep 3.2-acre site. (2022)

Energy

Gas Transmission Line R-985 In-Line Inspection and Upgrade Project, Pacific Gas & Electric, Millbrae, California. Provided biological construction monitoring for California red-legged frog, San Francisco garter snake (*Thamnophis sirtalis tetrataenia*), and nesting birds under the direct supervision of a permitted biologist during earthmoving work and excavation. Provided biological training to construction personnel and vehicle escort between active work areas. (2021)

Alto Substation Defensible Space Project, Pacific Gas & Electric, Mill Valley, California. Provided biological construction monitoring for Ridgway's rail, salt marsh harvest mouse (*Reithrodontomys raviventris*), and nesting birds during earthmoving and trenching activities. Provided biological training to construction personnel. (2021)

Education

San Jose State University
MS, Environmental
Studies, 2020

California State
Polytechnic University,
Pomona
BS, Environmental
Biology, 2012

Professional Affiliations

The Wildlife Society –
Western Section &
SF Bay Area Chapter

Raptor Research
Foundation

Golden Gate Raptor
Observatory

Transportation

Interstate 280 Hazardous Fuel Reduction Project, Los Altos Hills County Fire District, Los Altos Hills, California.

Prepared a biological resources analysis report for a 150-acre, 5.5-mile-long proposed fuel management project. Conducted land cover mapping and field evaluations in which the biotic resources of the site were determined. Evaluated the potential for Santa Cruz black salamander (*Aneides niger*), California red-legged frog, western pond turtle, white-tailed kite (*Elanus leucurus*), Townsend’s big-eared bat (*Corynorhinus townsendii*), and San Francisco dusky-footed woodrat to be impacted by planned project activities. (2022)

Highway 84 Roadside Fuel Management Project, Fire Safe San Mateo County, San Mateo County, California.

Co-authored a biological resources analysis report for a 90-acre, 8.2-mile-long proposed fuel management project. Conducted land cover mapping and field evaluations in which the biotic resources of the site were determined. Evaluated the potential for Santa Cruz black salamander, California giant salamander (*Dicamptodon ensatus*), California red-legged frog, burrowing owl, peregrine falcon (*Falco peregrinus anatum*), bald eagle (*Haliaeetus leucocephalus*), pallid bat (*Antrozous pallidus*), Townsend’s big-eared bat, San Francisco dusky-footed woodrat, and other special status species to be impacted by planned project activities. (2022)

Highway 35 Roadside Fuel Management Project, Fire Safe San Mateo County, San Mateo County, California.

Co-authored a biological resources analysis report for a 182-acre, 25-mile-long proposed fuel management project. Conducted land cover mapping and field evaluations in which the biotic resources of the site were determined. Evaluated the potential for Santa Cruz black salamander, California giant salamander, California red-legged frog, burrowing owl, white-tailed kite, peregrine falcon, bald eagle, pallid bat, Townsend’s big-eared bat, San Francisco dusky-footed woodrat, and other special status species to be impacted by planned project activities. (2022)

Water Resources

Cheltenham Way Culvert Replacement Project, Valley Water, San Jose, California. Prepared a biological site assessment in accordance with Valley Water Avoidance and Mitigation Measures, Santa Clara Valley Habitat Plan applications, the California Environmental Quality Act, and other permits. Conducted reconnaissance-level biological surveys at the project site and geospatial analyses of wildlife records. (2020)

San Francisquito Creek Flood Protection Project, Valley Water, Palo Alto, California. Assisted with protocol-level passive detection surveys for Ridgway’s rail as part of the mitigation and monitoring plan for the San Francisquito Flood Protection Project. (2020)

In-House Water Utility Enterprise Projects, Valley Water, Santa Clara County, California. Projects included, but were not limited to, routine maintenance on pipelines, pump stations, water treatment plants, canals, and groundwater recharge facilities; vegetation management; invasive plant control and removal; and dam maintenance and inspection. Conducted pre-activity surveys and biomonitoring for nesting birds, burrowing owl, California red-legged frog, California tiger salamander, western pond turtle, San Francisco dusky-footed woodrat, rare plants, and other special status species. Produced internal field reports and biological clearances for distribution to facility managers and project leads. Conducted database searches in the California Natural Diversity Database and managed database of survey activities. (2019–2021)

Specialized Training

- **Graduate Certificate in Environmental Planning, San Jose State University, June 2020.** Certificate program of 12 semester units, including courses on environmental planning, land use planning and law, and environmental impact analysis, intended to provide a working knowledge of environmental planning regulations and procedures.

Jamie Stroud

ENVIRONMENTAL PLANNER

Dudek Project Experience Development

123 Independence Drive Residential Development, City of Menlo Park, California. Project manager for the preparation of an environmental impact report (EIR) evaluating a project that proposes to demolish five existing office/industrial buildings and construct a 316-unit apartment building and 116 townhomes on an approximately 8-acre project site in the bayfront area of the City of Menlo Park.

1005 O'Brien Drive/1320 Willow Road Research and Development Project, City of Menlo Park, California. Project manager for a detailed initial study (IS) and focused EIR evaluating the demolition of existing research and development buildings and the construction of two new research and development buildings, structured parking, and publicly accessible open space on an approximately 4-acre site in the eastern portion of the City of Menlo Park's bayfront area.

Northstar California Biomass Energy Plant, Northstar Community Services District, Northstar California Resort, California. Project manager for Dudek's preparation of an initial study/mitigated negative declaration (IS/MND) evaluating Northstar Community Services District's proposed development of a biomass energy plant, which would be connected to approximately 13 buildings and facilities within and adjacent to Northstar Village to provide heating and reduce annual natural gas consumption. Northstar Community Services District already conducts vegetation clearing for fuel management and defensible space and, until recently, has relied on pile burning, chipping, and spreading to dispose of the biomass collected through these programs. Under the proposed project, the biomass materials would be transported to the Tahoe Truckee Sierra Disposal Eastern Regional Landfill to be sorted. The materials that meet the biomass plant criteria would be returned to Northstar Community Services District for use in the plant. Critical project issues include air quality and aesthetics.

Education

*University of Redlands, Redlands, CA
Bachelor of Science - Environmental Science with a Concentration in Geology
Bachelor of Science - Political Science with a Concentration in Constitutional Law
Minor - Spatial Studies, April 2022*

Professional Experience

Server and Greeter, Wings Grill, Auburn, California. Warmly greeted new and returning customers to develop and maintain customer loyalty and rapport. Swiftly and successfully resolved any conflicts to ensure customer satisfaction and a healthy work environment. Maintained and enforced up-to-date policies regarding the pandemic to ensure employee and customer safety. Successfully managed high stress environments to maintain a high-quality experience for every guest and allow for effective teamwork amongst my fellow staff members. (May 2021–Present)

Resident Assistant (RA) University of Redlands, Redlands, California. Developed a positive and inclusive living environment for residents, often resolving conflicts and problem solving to ensure each student felt safe and heard within their living community. Implemented and enforced all College and Campus policies, promoted academic success, addressed individual needs, and encouraged student involvement in an engaging and creative manner. Created, marketed, and lead student centered programs to educate students on relevant topics including the environment, diversity, emotional wellbeing, and safety while on a college campus. Collaborated with the University Garden staff to create a dorm composting drop-off box allowing students to easily participate in environmentally friendly practices while living in the dorms. (December 2019–May 2020)

Leadership and Volunteering

- **Roots and Shoots, Redlands, California.** (September 2018–May 2020)
 - Volunteered at three elementary schools in under-privileged communities to teach the youth about the environment through hands-on gardening and weekly lesson plans
 - Curated age specific and engaging teaching modules to expose children to environmental topics including climate change, plant and animal species, biodiversity, air pollution, and California history
- **Student Advisor, Redlands, California.** (May 2020–January 2021)
 - One of 21 students selected from my university to serve as an advisor for first year Redland's students
 - Maintained a 3.8 GPA taking 18 units while having weekly individual meetings with students and professors
 - Communicated with professors, the tutoring center, and counseling center acting as an advocate for my students
- **Sorority Rush and Formal Chair, Redlands, California** (September 2021–May 2022)
 - Managed a large budget for all formal and rush events utilizing Microsoft Excel and careful preplanning
 - Prioritized accessibility for individuals with disabilities by selecting locations that were wheelchair friendly
 - Included and enforced pronouns on all nametags to provide an inclusive environment for all attendees

Professional Skills

Experience Using:

- ArcGIS, ArcGIS Online, and ArcGIS StoryMaps
- CEQA, NEPA, and EIR Writing
- Microsoft Word, Excel, and PowerPoint
- Photography and Adobe Suite
- California laws, policies, and regulations

Daniel Hoffman

CEQA/NEPA PLANNER

Daniel Hoffman is a California Environmental Quality Act/National Environmental Policy Act (CEQA/NEPA) planner with 5 years' professional experience specializing in CEQA/NEPA compliance, planning and permitting, and construction management.

Mr. Hoffman has worked as an environmental planner and contract city planner for several municipalities throughout Northern California such as the Cities of Martinez, San Pablo, Vallejo, and Watsonville and Counties of Santa Clara and Sonoma. Mr. Hoffman has produced defensible CEQA documents, implemented and navigated regulatory permitting processes, and prepared and presented staff reports in public hearings.

Education

*Whitman College
BA, Environmental
Studies/Economics*

Professional Affiliations

*Association of
Environmental
Professionals*

Project Experience

University of California (UC) Berkeley Levine-Fricke Softball Field EIR, Berkeley, California. Served as deputy project manager and environmental planner for an intercollegiate athletic softball field project that includes the renovation of a softball field, fan concourse, team facilities, lighting, and scoreboard. Primary tasks involved project management and environmental analysis.

UC Berkeley Clark Kerr Campus Beach Volleyball EIR, Berkeley, California. Served as environmental planner for an Intercollegiate Athletic beach volleyball facility project involving the construction of new beach volleyball courts, lighting, scoreboard, spectator area, and supporting team and locker rooms. Primary task involved drafting multiple sections of the project CEQA EIR.

Santa Clara County Rural Zoning Ordinance Amendments EIR, Santa Clara County, California. Served as environmental planner for zoning ordinance amendments project intended to simplify and streamline permit processes, preserve rural areas, and further the County's General Plan goals for continued agricultural viability. Primary task involved drafting multiple sections of the project CEQA EIR.

Santa Cruz Water Rights Project EIR, City of Santa Cruz, California. Served as environmental planner for a water rights modification project that would modify water rights to expand authorized place of uses, better utilize existing diversions, and extend the City's timeline to put water to full beneficial use and provide for underground storage to address further constraints on the City's limited surface water supply caused by instream flow requirements. Primary task involved the preparation and maintenance of the project's administrative record.

123 Independence Drive EIR, City of Menlo Park, California. Served as environmental planner for a residential development project that involved construction of 116 for-sale townhomes and 316 rental apartments, along with associated parking and landscaping, on an 8.15-acre parcel. Primary tasks involved environmental analysis by drafting multiple sections of the CEQA EIR and performing on-site ambient noise measurements.

San Francisco Fell Street DMV Field Office Initial Study, San Francisco, California. Served as project manager and environmental planner for the California Department of General Services (DGS) DMV field office reconstruction project that involves the replacement of a current San Francisco DMV field office with a larger building. Primary tasks involved project management and environmental analysis.

City of Concord Housing Element Update, Concord, California. Served as lead environmental planner for the City of Concord’s General Plan housing element update project that is intended to address the comprehensive housing needs of the city. Primary tasks included environmental analysis by drafting multiple sections for the project CEQA Initial Study and mapping research to conduct the project’s required Affirmatively Furthering Fair Housing analysis.

Environmental Review for City of Santa Cruz, Santa Cruz, California. Served as environmental planner for various development projects within the City of Santa Cruz. Primary task involved environmental analysis by drafting project-level review checklists for compliance with the city’s General Plan EIR. Projects included residential, commercial, and assisted living facility uses.

Relevant Previous Experience

Santa Clara Valley Medical Center Behavioral Health Services Center Initial Study/Mitigated Negative Declaration (IS/MND), MIG Inc., Santa Clara, California. Served as environmental planner for a medical facility project consisting of a three-story Behavioral Health Services Center building and associated four-story parking structure. The project was to replace, consolidate, and expand existing mental health services on the medical campus. Tasks included environmental analysis through the preparation of a project CEQA IS/MND and preparation of public noticing documents.

Tru Hilton Hotel IS/MND and Conditional Use Permitting (CUP), MIG Inc., Santa Rosa, California. Served as environmental planner and contract project planner for a hospitality project consisting of a four-story hotel. Tasks included environmental analysis through the preparation of a project CEQA IS/MND, planning review and staff report for the CUP and Design Review, preparation of public noticing documents, and correspondence with interested public.

Carlton Senior Living Facility IS/MND and CUP, MIG Inc., Vallejo, California. Served as environmental planner and contract project planner for a senior living facility project involving the construction and operation of a 156-unit senior living facility. Tasks included environmental analysis through the preparation of a CEQA IS/MND, correspondence with interested public, and staff report presentation to City’s Planning Commission.

Sonoma County Cannabis Program CEQA Analysis and Local Permitting, MIG Inc., Sonoma County, California. Served as environmental planner and contract county planner for Permit Sonoma. The program was established in 2015 to locally permit cannabis cultivation, manufacturing, and dispensing operations throughout Sonoma County. Tasks included generating a template IS/MND for the client’s internal use as well as working with applicants and operators to zoning compliance and Use Permit issuance.

City of San Pablo Contract Planning, MIG Inc., San Pablo, California. Served as contract planner for the City of San Pablo’s Planning Department. Provided staff support to the City of San Pablo by providing project management, staff report preparation, general plan and zoning analysis, and customer service. Presented staff reports to City’s Planning Commission and helped author the City’s revised Accessory Dwelling Unit ordinance.

City of Martinez Contract Planning, MIG Inc., Martinez, California. Served as contract planner for the City of Martinez’ Planning Department. Provided staff support to the City of Martinez by providing project management, staff report preparation, general plan and zoning analysis, and customer service.

Rachel Strobridge

GEOGRAPHIC INFORMATION SYSTEMS ANALYST

Rachel Strobridge (RAY-chull STROH-bridge; she/her) is a geographic information systems (GIS) analyst with 5 years' experience utilizing geospatial software to analyze data, organize and maintain spatial databases, create maps, and provide technical support to various environmental projects. In addition to having expertise in programs such as ArcGIS, Ms. Strobridge is also experienced in AutoCAD and graphics software, including the Adobe Creative Cloud suite of programs.

Project Experience

Placer County Government Center Master Plan Update, Placer County, California. Provided analytical and creative support for various environmental reports, including the biological technical report, notice of preparation, and jurisdictional delineation report.

Del Mar Resort Project, City of Del Mar, California. Served as the primary GIS contact and analyst for the Del Mar Resort project, including the creation of biological figures as well as support for visual and acoustical analyses. Primarily assisted in the creation and maintenance of the specific plan figures. Performed extensive work modifying maps and maintaining graphics as well as spatial data.

The Farm at Alamo Creek, City of Vacaville, California. Worked extensively to provide GIS services in the form of map creation, data analysis, and database organization. Moved through multiple iterations and provided analysis for land use and zoning needs.

City of Rohnert Park On-Call Contract, California. Served as the GIS lead for several projects under this contract, including the following:

- The Press Democrat Industrial Project
- The Copeland Creek Detention Basin
- The Fairfield Inn and Suites Construction Project
- The Residences at Five Creek
- The Learning Experience Child Development Center

Completed various report figures and provided analytical support such as for biological technical reports, cultural memos, and acoustical analyses.

Mitchell Farms Development Project, City of Citrus Heights, California. Served as GIS co-lead for the Mitchell Farms Development project, a 55-acre housing development project which includes detached single-family homes, townhomes, and parks. Worked to provide figures for the environmental impact report as well as technical analyses such as acousitcal analysis and biological impacts.

Education

*California State University of Sacramento
BA, Geography,
Concentrations
in Physical and Human
Geography, 2014*

*American River College,
Certificate of
Achievement,
GIS, 2015*

Professional Affiliations

*URISA, Northern
California Chapter*

Santa Cruz Wharf Master Plan, California. Provided creative and analytical services for creating and modifying various figures for the Wharf Master Plan. These included visual analyses as well as photo figure creation, photo stitching and modification, and geographic map creation.

University of California, Davis, On-Call Contract, California. Served as the GIS lead for several projects under this contract, including the following:

- The Core 2, Phase 1 Greenhouses
- The Emerson Hall Replacement Project
- The Orchard Park Demolition Project
- The Tercero Dining Commons 2 Project
- The Webster Hall Project

California State University, Monterey Bay, Campus Master Plan Project, California. Provided graphical as well as GIS expertise to create new and manipulate existing figures for an environmental impact report as well as a notice of preparation.

Dodge Flat Solar Energy Project, Washoe County, Nevada. Provided GIS support for cultural studies and acted as the GIS lead for data collection, data organization, analysis, and map creation for U.S. Army Corps of Engineers-verified jurisdictional delineation report. Worked extensively on the jurisdictional delineation, including post-processing of GPS data and working with biologists to perfect the delineation and complete preparatons for U.S. Army Corps of Engineers approval.

Lakeside Anaerobic Digester Project, City of Lakeside, California. Provided GIS support for acoustical analysis and created figures for technical reports. Worked closely with acoustical analysts to develop a system of data transfer for use between disciplines that enabled advanced GIS analysis, allowing for more detailed and data-rich figures for acoustical technical report.

San Pablo Municipal Broadband Project, City of San Pablo, California. Served as the GIS lead for the San Pablo municipal broadband project. Worked extensively to provide report figures for the initial study, technical reports, constraints analysis, and environmental narrative document. Also provided data analysis and summary tables.

Santa Clara County Parks, Mt. Madonna Forest Health Plan, California. Assisted forestry management and served as the GIS lead for the Mt. Madonna Forestr Health Plan. Provided extensive analysis for the Forest Health Plan document and assisted in creating figures for all sections, including biology, geology, planning, and hydrology.

Relevant Previous Experience

GIS Intern, Teichert Aggregates, Sacramento, California. Worked as an entry-level intern for Teichert Aggregates in the mine planning and geology department. Became familiarized with the various aspects of environmental planning and mitigation measures needed for mining projects and obtained extensive hands-on experience utilizing ArcGIS and AutoCAD. Worked closely with biologists, mining engineers, and geologists to cultivate a thorough and interdisciplinary view of mine planning, construction, and mitigation. Was also introduced to Trimble Pathfinder software and was instructed in using handheld GPS receivers and total stations for increased data accuracy as well as post-processing actions.

Adam Giacinto

HISTORIC/CULTURAL RESOURCES

Adam Giacinto is an archaeologist with 15 years' experience preparing cultural resource reports, site records, and managing archaeological survey, evaluation, and data recovery-level investigations. His research interests include prehistoric hunter-gatherer cultures and contemporary conceptions of heritage. His current research focuses on the social, historical, archaeological, and political mechanisms surrounding heritage values. He has gained practical experience in archaeological and ethnographic field methods while conducting research in the Southwest, Mexico, and Eastern Europe.

Mr. Giacinto brings specialized experience in cultural resources information processing gained while working at the South Coastal Information Center. He has worked as part of a nonprofit collaboration in designing and managing a large-scale, preservation-oriented, standardized database and conducting site and impact predictive Geographic Information Systems (GIS) analysis of the cultural resources landscape surrounding ancient Lake Cahuilla. He provides experience in ethnographic and applied anthropological methods gained in urban and rural settings, both in the United States and internationally.

Project Experience

Park Boulevard Environmental Impact Report (EIR), City of Palo Alto, California.

As principal archaeological investigator, coordinated a Northwest Indian College (NWIC) records search, Native American Heritage Commission (NAHC) and Native American consultation, archaeological survey, and preparation of a technical report and EIR section. An appropriate mitigation strategy was developed and provided to the City of Palo Alto for this negative cultural inventory.

Vacaville Center Campus Project, Solano Community College District, City of Vacaville, California. As principal archaeological investigator, coordinated a NWIC records search, NAHC and Native American communication, archaeological survey, and preparation of a technical report. Recommendations were framed in compliance with California Environmental Quality Act (CEQA) regulations and submitted to the lead agency.

Makani Power Wind Turbine Pilot Program, Google Inc., Alameda, California. As principal investigator, coordinated a NWIC records search, NAHC and Native American consultation, archaeological survey, and preparation of a negative technical memo for this potential wind farm. The mitigation strategy did not require additional archaeological monitoring or other work based on the lack of archaeological sites, and the low potential for encountering unrecorded subsurface cultural resources. Recommendations were submitted as a categorical exemption to the reviewing agency.

Yokohl Ranch Cultural Resources, The Yokohl Ranch Company LLC, Tulare, California. As co-principal investigator and field director, managed 15 archaeologists in conducting 1,900 acres of survey throughout the Yokohl Valley.

Education

San Diego State University
MA, Anthropology

Sonoma State University
BA, Anthropology/
Linguistics

Santa Rosa Junior College
AA, Anthropology

Professional Affiliations

Register of Professional Archaeologists

Society for California Archaeology

American Anthropological Association

Institute of Archaeomythology

American Anthropological Association

Hamilton Hospital Project, City of Novato, California. As principal investigator, managed tribal and archaeological fieldwork and methodological reporting relating to the extended Phase I inventory geoprobe drilling and shovel test pit excavation. Considerations included compliance under CEQA and local regulations.

Maidu Bike Path and Park Projects, City of Auburn, California. As principal investigator, managed the survey, archival searches, tribal correspondence, and reported management recommendations for a cultural resources inventory. Considerations included compliance under CEQA and Section 106 of the NHPA.

Stepphollow Creek and Bear River Restoration, Nevada County, California. As principal investigator, assisted with management of field efforts and preparation of a technical report for a cultural inventory. Resources were evaluated for significance under CEQA, and Section 106 of the NHPA.

As Needed Planning and Environmental Contract, Recycled Wastewater Treatment Plant Secondary Process Upgrade Improvement Project, City of Auburn, California. As principal investigator, managed the survey, archival searches, tribal correspondence, and reported management recommendations for a cultural resources inventory. Considerations included compliance under CEQA and Section 106 of the NHPA.

Recycled Water Pipeline Project, City of Woodland, California. As principal investigator, managed the survey, archival searches, tribal correspondence, and reported management recommendations for a cultural resources inventory. Considerations included compliance under CEQA and Section 106 of the NHPA.

Hanson El Monte Pond Restoration, Lakeside's River Park Conservancy, San Diego, California. As principal investigator, managed the field efforts, reporting, and agency interface for a cultural inventory. Resources were evaluated for significance under county guidelines, CEQA, and Section 106 of the NHPA. Worked with the ACOE for submittal of documents to State Historic Preservation Office (SHPO).

Lake Cahuilla Management Plan, ASM PARC, Riverside County, California. As project archaeologist and lead analyst, led in the formation of a standardized database associated with ancient Lake Cahuilla. Performed GIS data integration and predictive analysis, data entry of site record information, and completed multi-day, multi-person record search for Riverside County.

South Palm Canyon West Fork Flood Emergency Work, Agua Caliente Band of Cahuilla Indians, Palm Springs, California. As principal investigator, worked with the Agua Caliente Band of Cahuilla Indians Tribal Historic Preservation Office to conduct archaeological monitoring on tribal lands of emergency repairs within Andreas Canyon National Register of Historic Places listed district. A monitoring report with a summary of findings and implemented mitigation activities, daily monitoring logs and photos, and confidential figures was provided to the tribe.

South Palm Canyon Improvements, Agua Caliente Band of Cahuilla Indians, Palm Springs, California. As principal investigator, worked with the Agua Caliente Band of Cahuilla Indians Tribal Historic Preservation Office to conduct archaeological monitoring on tribal lands of facility improvements within Andreas Canyon National Register of Historic Places listed district. A monitoring report with a summary of findings and implemented mitigation activities, daily monitoring logs and photos, and confidential figures was provided to the tribe.

Shu'luuk Wind Project Cultural Resource Study Survey, Campo Environmental Protection Agency and Invenergy LLC, Campo Indian Reservation, California. As field director, managed two teams of archaeologists, consisting of seven total practitioners, in conducting a survey of the 2,400-acre study area in a general inventory of potentially impacted cultural resources. Worked with Campo Environmental Protection Agency, of the Campo Kumeyaay Nation, in forming management objectives and integrating six Native American Monitors into daily survey activities.

William Burns, RPA

PROJECT ARCHAEOLOGIST

William Burns (*WILL-ee-em BURNS; he/him*) is an archaeologist with 18 years' experience in cultural resource management. Mr. Burns is highly knowledgeable about the California Environmental Quality Act, the National Environmental Policy Act, the Native American Graves Protection and Repatriation Act, and the National Historic Preservation Act, particularly the Section 106 process. He evaluates buildings and districts for archaeological sensitivity and possible inclusion on the National Register of Historic Places. Mr. Burns assesses project and building plans for archaeological sensitivity and reviews archaeological reports on the state government regulatory end of the process.

Mr. Burns possesses expertise about pre-contact archaeological sites, paleocoastline reconstruction, and artifact identification and analysis. He applies this expertise to archaeological report writing and editing for Section 106 projects. He also serves on field crews and as a supervisor on archaeological projects, overseeing surveys, site examinations, data recoveries, and artifact database creation and maintenance. For precise site mapping, Mr. Burns uses GPS devices, primarily Trimble GEO XH, ArcGIS, and Maptitude.

Dudek Project Experience

123 Independence Drive Project, Menlo Park, San Mateo County, California. Prepared cultural resources chapter, tribal cultural resources chapter, and cultural resource mitigation measures for Environmental Impact Report for a mixed-use residential and commercial development.

CitizenM Hotel Project, Menlo Park, San Mateo County, California. Performed Cultural Resources Awareness Training for construction personnel for new hotel project. Provided and managed cultural monitors for the project, reviewed monitoring results and prepared the Cultural Resources Monitoring Report.

Alameda County Advanced Meter Infrastructure Project, Alameda County, California. The project proposed to upgrade existing traditional water meter reading systems within its service area with an Advanced Metering Infrastructure system across 84,200 existing meters. As project archaeologist, performed the records search and reviewed and summarized the data of the approximately 41,000-acre project area. Reviewed geomorphological data for the project site and developed a sensitivity model and monitoring plan for the project.

Solar Panel Installation Project, Santa Clara County, California. The project consisted of the installation of solar facilities at 14 sites across the County of Santa Clara. Conducted surveys and site visits to all 14 work sites, documenting existing conditions on several historic structures. Prepared the cultural resources report.

Education
University of York
MS, Coastal and Marine
Archaeology, 2010,
University of
Massachusetts at
Amherst
BA, Anthropology
(Mathematics minor),
2004

Certifications
Register of Professional
Archaeologists (RPA)
Divemaster (National
Association of Underwater
Instructors)
Occupational Safety and
Health Administration
(OSHA) HAZWOPER
(40-hour)
Basic First Aid/BBP
(American Heart
Association)
Adult CPR/AED (American
Heart Association)

Advanced Meter Infrastructure Project, Santa Clara County, California. Served as project archaeologist on this countywide project to upgrade metering systems for the San Jose Water Company at 16 locations. Tasks included conducting the records search, performing a survey at each of the 16 sites, and preparing the cultural resources report. Two historic structures were identified during the study.

Delta Dams Burrow Remediation Project, California Department of Water Resources, Alameda and Contra Costa Counties, California. Project consisted of maintenance conducted at three separate reservoirs to repair damage cause by rodent burrowing. As project archaeologist, performed the cultural survey at each location, conducted records searches, and prepared the cultural resources report.

B.F. Sisk Dam Safety of Dams Project, California Department of Water Resources, Merced County, California. The B.F. Sisk Dam Safety of Dams Project consisted of repairs and maintenance of the historic dam. The vicinity around the dam contained many historic features of the dam’s construction, as well as prehistoric sites including reported burials. Performed the cultural survey documenting these resources, conducted records search, and prepared cultural resources report.

Rohnert Park Water Tank Project, City of Rohnert Park, California. As project archaeologist and field director, led crew for an extended phase I field survey, including subsurface testing around a newly discovered prehistoric resource. Prepared the cultural resources report for the extended phase I.

Las Gallinas Valley Sanitary District Secondary Treatment Upgrade Project, Las Gallinas Valley Sanitary District, Marin County, California. Conducted field survey and prepared the cultural resources report for water treatment plant improvements.

Mission College Plaza Completion Project, Santa Clara, California. Conducted the records search, performed pedestrian survey, and prepared cultural resources report for university redevelopment project.

San Carlos Fuel Management Project, California Department of Forestry and Fire Protection, San Carlos, California. Project consisted of the California Department of Forestry and Fire Protection’s controlled burn of vegetation to control potential wildfires in the San Carlos area. As project archaeologist, performed the cultural survey, conducted records search, and prepared cultural resources report.

Selected Publications

Burns, W., K. Haley, and A. Giacinto. 2021. *Cultural Resources Assessment Report, County of Santa Clara Solar Panel Installation Project*. Dudek No. 12853, County of Santa Clara, California.

Burns, W., R. Owen, and A. Giacinto. 2020. *Cultural Resources Inventory for the Advanced Meter Infrastructure Project, Santa Clara County, California*. Dudek No. 11567, Santa Clara County, California.

Giacinto, A., W. Burns, N. Hanten, F. Steffen, and K. Haley. 2019. *Cultural Resources Inventory and Monitoring Plan for the Alameda County Water District Advanced Metering Infrastructure Project*. Dudek No. 11567, Alameda County, California.

Kathryn Haley, MA

HISTORICAL/CULTURAL RESOURCES

Kathryn Haley is a historic built environment resource specialist/architectural historian with 18 years' professional experience in historic/cultural resource management. Ms. Haley has worked on a wide variety of projects involving historic research, field inventory, and site assessment conducted for compliance with Section 106 of the National Historic Preservation Act (NHPA), National Environmental Policy Act (NEPA), and California Environmental Quality Act (CEQA). Ms. Haley specializes in the California Register of Historical Resources (CRHR); the National Register of Historic Places (NRHP); and evaluations of built environment resources, including water management structures (levees, canals, dams, and ditches), buildings (residential, industrial, and commercial), and linear resources (railroad alignments, roads, and bridges). She specializes in managing large-scale surveys of built environment resources, including historic district evaluations.

She meets the Secretary of the Interior's Professional Qualification Standards for historian and architectural historian. Ms. Haley has also assisted in preparation of Historic Properties Inspection Reports (condition assessments) under the direction of the Naval Facilities Engineering Command (NAVFAC) in accordance with Section 106 and Section 110 of the NHPA. Moreover, Ms. Haley has served as project manager, coordinator, historian, and researcher for a wide variety of projects. She is also experienced in the preparation of Historic American Building Survey (HABS), Historic American Engineering Record (HAER), and Historic American Landscape Survey (HALS) documents, as well as the preparation for National Register nominations.

Project Experience

Bidwell and El Rancho Verde Parks Master Plan, Cities of Hayward and Union City, Alameda County, California.

Dudek was retained to prepare a cultural resources technical report for the Bidwell and El Rancho Verde Parks Master Plan project located in Alameda County. Co-authored the cultural resources technical report and provided QA/QC. Preparation of the technical report entailed archival building development research in local repositories and the composition of an appropriate historic context focused on the history of Hayward and the development of Post-war residential communities in the Bay Area, exterior survey fieldwork of the resources, and historical significance evaluations for the resources in consideration of NRHP, CRHP, and local designation requirements.

The Keiser Avenue Reconstruction Project, City of Rohnert Park, Sonoma County, California. The City of Rohnert Park retained Dudek to prepare an HRER for the proposed Keiser Avenue Reconstruction project, which proposed the demolition of a residential property within the project area limits, located at 5040 Snyder Lane. Served as lead architectural historian for the project and co-authored the HRER. Preparation of the report entailed extensive archival research; the composition of an appropriate historic context focused on the history of Rohnert Park; evaluation of the built features of the property within the framework of NRHP, CRHP, and local designation requirements; and the preparation of accompanying Department of Parks and Recreation (DPR) 523 forms.

Education

*California State University,
Sacramento
MA, Public History
BA, History*

Professional Affiliations

*California Council for the
Promotion of History
(former Treasurer)
California Preservation
Foundation*

Mitigation Implementation for 3093 Broadway (HABS and Interpretative Signage), The Martin Group, City of Oakland, Alameda County, California. Dudek was retained by The Martin Group to prepare documentation required under CEQA for the 3093 Broadway Project, which resulted in a significant impact of a CEQA historical resource. Preparation of local-level HABS documentation of the Connell Motor Company Building in Oakland, California and interpretative signage were included as mitigation requirements outlined in the EIR for this project. In coordination with the City of Oakland, Dudek assisted The Martin Group in implementation of the required mitigation. Dudek prepared the HABS documentation and created the interpretive signage for the building that documented the history of the Connell Motor Company building following its demolition in 2016 and integration of the showroom façade into the new complex located at the site completed in 2019. Tasks involved in completing this work included intensive research related to the Connell Motor Company building, writing the HABS report, and the compilation of all known existing physical evidence related to the building. Additionally, the information compiled as part of the HABS documentation was used to produce the text and graphics for the interpretative signage in the form of an informational plaque documenting the historic significance of the Connell Building that is now featured at the new complex. Provided senior-level technical support and QA/QC.

University of California Berkeley, Clark Kerr Campus Beach Volleyball Complex EIR, University of California Berkeley, Alameda County, California. Dudek is in the process of assisting the University of California Berkeley (UC Berkeley) with an EIR for the Clark Kerr Campus (CKC) Beach Volleyball Complex Project, which is located within NRHP District No. 82000962 *State Asylum for the Deaf, Dumb, and Blind* (also known as *California Schools for the Deaf and Blind*) (historic district). The historic district is also considered a City of Berkeley Landmark. UC Berkeley is proposing to partially demolish CKC Building 21/NRHP California Schools for the Deaf and Blind Building B-11, which is a contributing building to the historic district. The partial demolition of CKC Building 21 is being pursued due to the seismic safety risk posed by the building pursuant to the University of California's Seismic Safety Policy, as well as certain obligations under the CKC Covenants, which limit the ability of the campus to construct new buildings on the CKC. This subject building has been vacant since at least the late 1970s. To comply with Public Resources Code (PRC) Section 5024(b), regarding state-owned historical resources, UC Berkeley must consult with the State Historic Preservation Officer (SHPO) on the project. Is serving as lead architectural historian on the project, assisting UC Berkeley with SHPO correspondence and preparing the technical finding of effect/Secretary of the Interior's Standards (SOIS) analysis for the project. Will work with UC Berkeley on mitigation development if necessary.

1624 Mission Street, SOIS Compliance Analysis, City and County of Santa Cruz, California. Dudek was retained to review the proposed project to rehabilitate the locally listed historic resource located at 1624 Mission Street in the City of Santa Cruz, California. The City of Santa Cruz required that a qualified architectural historian assess the proposed project, which involved the replacement of the wooden storefront window wall and full-light metal door, the concrete entry slab, wall and stairs, and the addition of a new front railing for conformance with SOIS for the Treatment of Historic Properties, specifically, the Standards for Rehabilitation. Project work entailed consultation to design a suitable replacement storefront after structural failure caused by a storm. Dudek also drafted the memorandum providing an analysis of how the project conforms to SOIS. Provided senior level technical support and QA/QC.

On-Call Services for Department of Water Resources, Operations and Maintenance Division, State of California. Dudek was retained by the State of California's Department of Water Resources (DWR), Operations and Maintenance Division (O&M), to assist in a wide range of on-going environmental compliance efforts. Under this on-call contract, serves as project manager for a task order focused on assisting DWR in efforts to streamline cultural resources environmental documentation for O&M projects. Dudek is also preparing built environment technical reports for several projects under this contract. Led the creation of a standardized historic context statement for State Water Project (SWP), as well as Historical Resources Technical Reports for various SWP maintenance projects. Technical reports have been prepared for O&M projects at the following sites: Dyer Dam and Reservoir, Patterson Dam and Reservoir, Clifton Court Forebay, California Aqueduct Milepost 230.

Erin Jones, MA

ARCHITECTURAL HISTORIAN

Erin Jones (*AIR-in JO-nēs; she/her*) is a highly committed, well-qualified cultural resource manager with over 2 years of experience specializing in Washington, Oregon, and California history. Ms. Jones is an expert researcher that is adept at context writing and the evaluation of historic properties. She has experience authoring California Environmental Quality Act (CEQA) compliance documents, National Historic Preservation Act (NHPA) Section 106 compliance reports, Historic Resource Evaluation Reports (HRER), Cultural Resources Inventory and Evaluation Report (CRIER), Historical Resource Inventories (HRI), Cultural Resource Technical Reports (CRTR), Historical Resources Inventory and Evaluation Report (HRIER) and, Historic American Building Survey (HABS) level documentation. Ms. Jones meets the Secretary of the Interior's Professional Qualification Standards for architectural history.

Education

*California State University,
Sacramento
Master of Arts Public
History with Distinguished
Honors, Spring 2021.*

*University of Oregon
Bachelor of Arts in History
and Political Science,
Fall 2017*

Dudek Project Experience

Built Environment Inventory and Evaluation Report: 1005 O'Brien Drive and 1320 Willow Road Research and Development Project. Menlo Park, San Mateo County, California. Ms. Jones was the architectural historian and author of the HRIER for the 1535-1575 Industrial Avenue Warehouse Project. Dudek was retained by the City of Menlo Park to prepare a Built Environment Inventory and Evaluation Report for the proposed 1005 O'Brien Drive and 1320 Willow Road Research and Development Project. The proposed project includes the construction of new research and development (R&D) buildings and a parking structure, replacing the three existing R&D buildings on the site. The proposed project is located in the Bayfront Area of the City of Menlo Park north of U.S. Highway 101 (US 101), south of Bayfront Expressway (State Route [SR] 84), east of Willow Road (SR 114), and west of University Avenue (SR 109). Ms. Jones conducted an in-person field survey; building development and archival research; created an original historic context; developed the built environment study area; and assessed potential impacts to historic era buildings and structures. This report was prepared in conformance with California Environmental Quality Act (CEQA) Guidelines Section 15064.5 for historical resources. None of the properties located in the study area contain properties were considered historical resources under CEQA and the report concluded that there is No Impact.

650 North King Road, San Jose, Santa Clara County, California. Served as the architectural historian and author of the Standalone Department of Parks and Recreation (DPR) 523 form for the 650 North King Road Project. Ms. Jones conducted archival, map, aerial, property, and building development research; conducted an evaluation of property for the NRHP, CRHR, CHL, and local eligibility criteria and integrity requirements; an assessment of impacts on historical resources in compliance with CEQA (2021).

Historical Resources Inventory and Evaluation Report for the 1535-1575 Industrial Avenue Warehouse Project, San Jose, California. Ms. Jones was the architectural historian and author of the HRIER for the 1535-1575 Industrial Avenue Warehouse Project. The project proposed the demolition of four existing buildings on the project site and the construction of a single-story 71,550 square-foot concrete tilt-up building with a loading dock and adjacent parking lot. Ms. Jones conducted the CHRS records search that covered the project area and a 0.50-mile

buffer; an intensive pedestrian survey of the project area; building development, archival, and development research; recordation and evaluation of the buildings over 45 years of age for the National Register of Historic Places (NRHP), California Register of Historical Resources (CRHR), and local historic resource (local) eligibility criteria and integrity requirements in compliance with the CEQA; and the completion of the preparation of the report and the accompanying DPR 523 forms (2021).

1586 Industrial Avenue Warehouse Project, San Jose, California. Ms. Jones was the architectural historian and author of the 1586 Industrial Avenue Warehouse Project. The project proposed the demolition of two existing buildings on the project site and the construction of a single-story square-foot concrete tilt-up building with a loading dock and adjacent parking lot. The 2022 Kings Row project is associated with the 2021 1535-1575 Industrial Avenue Warehouse Project, which is adjacent to this area. Ms. Jones conducted the CHRS records search that covered the project area and a 0.50-mile buffer; an intensive pedestrian survey of the project area; building development, archival, and development research; recordation and evaluation of the buildings over 45 years of age for the NRHP, CRHR, and local historic resource (local) eligibility criteria and integrity requirements in compliance with CEQA; and the completion of the preparation of the report and the accompanying DPR 523 forms (2022).

Built Environment Inventory and Evaluation Report for Golden State Natural Resources Gould Site, Lassen County, California. Ms. Jones was the architectural historian and co-author of the BEIER for the Golden State Natural Resources Gould Site Project. Dudek was retained by the Golden State Finance Authority (Client) to complete a BEIER for a proposed project that would redevelop a site in Nubieber, Lassen County, to facilitate the transport of forest material (such as trees or underbrush that have no lumber value). The project parcels comprised approximately 65 acres, a light-industrial site associated with regional logging, and an abandoned 1930s Great Northern and Western Pacific Railroad station. Ms. Jones conducted the CHRS records search; the pedestrian surface reconnaissance survey of the built environment Area of Potential Effects (APE); extensive archival and building development research; the development of an appropriate historic context for the Project area; recordation and evaluation of the buildings over 45 years of age for the NRHP, CRHR, and local historic resource eligibility criteria and integrity requirements in compliance with the CEQA; and report and accompanying DPR 523 form set preparation (2022).

1400 Thompson Avenue Project, Santa Cruz County, California. Ms. Jones was the architectural historian, main researcher, and co-author of the HRER for the 1400 Thompson Avenue Project which proposed to demolish the existing buildings and subdivide the site into four new parcels slated for individual development. Dudek was retained by a private homeowner to produce a report in support of the proposed project by conducting a CHRS records search that covered the project location and a 0.25-mile buffer; archival and building development research; evaluation of property for the NRHP, California Register of Historical Resources (CRHR), CHL, and local eligibility criteria and integrity requirements; and an assessment of impacts to historical resources in compliance with CEQA (2021).

Fallin E. Steffen, MPS

ARCHITECTURAL HISTORIAN

Fallin Steffen (*FAL-in STEF-in; she/her*) is an Architectural Historian with 6 years' experience in historic preservation, architectural conservation, and cultural resource management in the Monterey Bay Area and Northern California. Ms. Steffen's professional experience encompasses a variety of projects for local agencies, private developers, and homeowners in both highly urbanized and rural areas, including reconnaissance- and intensive-level surveys, preparation of resource-appropriate and city-wide historic contexts, and historical significance evaluations in consideration of the National Register of Historic Places (NRHP), California Register of Historic Resources (CRHR), and local designation criteria. Additionally, Ms. Steffen was appointed as a Commissioner to the Santa Cruz City Historic Preservation Commission assisting Santa Cruz City Staff with design review and conformance with the Secretary of the Interior Standards for proposed residential, commercial, and municipal projects involving historic properties. Ms. Steffen meets the Secretary of the Interior's Professional Qualification Standards for Architectural History. She is experienced with interdisciplinary projects spanning private and public development, transportation, and water infrastructure, and maintains experience forming educational sessions about the identification of and best practices for the preservation of historic resources.

Education

*Tulane University,
New Orleans, LA
Masters of Preservation
Studies, 2015*

*University of California,
Santa Cruz, CA
B.A. History of Art & Visual
Culture, 2010*

Relevant Dudek Project Experience

Vista Woods Apartment Project, CEQA Compliance and HUD Permitting, Pinole, California. Served as architectural historian and co-authored the Built Environment Resources Inventory and Evaluation Report for the Vista Woods Apartment Project. The purpose of the project was to replace existing buildings on a development site comprising three parcels located in the City of Pinole, California with a new 4-story, 179-unit apartment complex providing affordable housing to seniors. As the project includes funding from the Department of Housing and Urban Development (HUD), the City of Pinole requested the document comply with Section 106 of the NHPA of 1966 and CEQA. Ms. Steffen authored the historical significance evaluation for 1106 San Pablo Avenue and its associated components. The significance evaluation determined that the property does not appear eligible for designation under NRHP, CRHR and local designation criteria. (2021)

The Delivery Station Building Project, Cultural Resources Inventory and Evaluation Report, San José, California. Served as architectural historian for the project, conducted fieldwork and co-authored the Cultural Resources Inventory and Evaluation Report. Dudek was retained by Kimley-Horn to complete a Historic Resources Evaluation for an industrial complex located in the City of San José, California. The purpose of the project is the replacement of the existing industrial complex with an approximate 94,325 square feet new warehouse building and site related improvements. The report entailed archival building development research in local repositories and the composition of an appropriate historic context focused on the history of San José, exterior survey fieldwork of the resources, and historical significance evaluations for the resources in consideration of NRHP, CRHP, and local designation requirements. As a result of the significance evaluation, the subject property does not appear eligible for listing in the NRHP, CRHR, or local inventory, due to a lack of significant architectural merit. (2021)

County of Santa Clara Solar Panel Installation Project, Cultural Resources Assessment, Santa Clara County, California. Served as architectural historian and co-author of the historic resource evaluations. The purpose of the project is to install Solar Panels at 14 County-owned sites to further expand on the County's renewable energy portfolio and reduce the County's emissions from operations. As a result of Dudek's extensive archival research, field survey, assessment of potential project impacts, and property significance evaluations, the cultural resources assessment identified no historical resources were identified within the Project sites. Nor were any adjacent resources identified that could be indirectly impacted by proposed project activities. Therefore, the Projects were found to result in no impact to historical resources under CEQA. (2021)

123 Independence Drive Mixed-Use Project, Department of Community Development, City of Menlo Park, California. Served as architectural historian and co-author of the Historical Resources Evaluation Report. The Sobrato Organization retained Dudek to prepare a cultural resources study in support of the 123 Independence Drive Mixed-Use Project located in the City of Menlo Park. The study included a pedestrian survey of the subject properties for buildings and structures over 45 years of age; building development and archival research for the identified properties located within the project site; recordation and evaluation of cultural resources identified within the study area for the NRHP, CRHR, and local eligibility criteria and integrity requirements; and an assessment of potential impacts to historical resources in conformance with the California Environmental Quality Act (CEQA) and all applicable local municipal code and planning documents. Ms. Steffen's efforts included exterior survey fieldwork of the resources and archival building development research in local repositories. (2020)

Bidwell and El Rancho Verde Parks Master Plan, Cities of Hayward and Union City, California. Served as architectural historian and co-author of the Cultural Resources Study. Dudek was retained by Carducci Associates to prepare a cultural resources study in support of the Bidwell and El Rancho Verde Parks Master Plan project proposed by the Hayward Area Recreation Park Department and located in Alameda County. The study included a California Historical Resources Information System (CHRIS) records search of the project sites and a 0.5-mile radius buffer; a pedestrian survey of the subject properties for cultural resources; building development and archival research; recordation and evaluation of cultural resources identified within one property in the study area; and an assessment of potential impacts to historical resources in conformance with CEQA and all applicable local municipal code and planning documents. The former Bidwell School property and all associated buildings and structures were found not eligible under all NRHP, CRHR, and local designation criteria. The cultural resources study and efforts included exterior survey fieldwork of resources, archival building development research in local repositories, composition of appropriate historic context focused on the development of Post-war residential communities in the Bay Area, and historical significance evaluations for the resources in consideration of NRHP, CRHP, and local designation requirements. (2020)

Mitigation Implementation for 3093 Broadway (Historic American Building Survey [HABS] and Interpretative Signage), The Martin Group, City of Oakland, Alameda County, California. Dudek was retained by The Martin Group to prepare documentation required under CEQA for the 3093 Broadway Project, which resulted a significant impact of a CEQA historical resource. Preparation of local level HABS documentation of the Connell Motor Company Building in Oakland, California and interpretative signage were included as mitigation requirements outlined in the Environmental Impact Report (EIR) for this project. In coordination with the City of Oakland, Dudek assisted The Martin Group in implementation of the required mitigation. Dudek prepared the HABS documentation and created the interpretive signage for the building that documented the history of the Connell Motor Company building following its demolition in 2016 and integration of the showroom façade into the new complex located at the site completed in 2019. As architectural historian, Ms. Steffen's contributions to the project included intensive research related to the Connell Motor Company building, the written HABS report, and the compilation of all known existing physical evidence related to the building. (2020)

Michael Carr, INCE

SENIOR ACOUSTICIAN

Michael Carr is an acoustician with more than 20 years' experience in acoustics and related industries, with an emphasis on environmental acoustics, noise and vibration. Mr. Carr is a member of the Institute of Noise Control Engineering (INCE) and an expert in acoustics, noise and vibration control, sound insulation and electro-acoustics. His broad range of experience and technical depth encompass a number of markets including structural and building acoustics, residential, commercial, recreational, transportation, environmental noise and vibration control. In the area of transportation noise and vibration, Mr. Carr has expertise in measurement, prediction and assessment of noise and vibration associated with aviation, vehicular and rail/transit-based transportation modes.

Mr. Carr has managed, supervised and performed acoustic, noise and vibration analyses for both private and public sectors including federal, state, regional and local agencies; preparing technical studies, environmental assessments, and documentation in support of CEQA and NEPA. He has authored, and become expertly skilled with proprietary modeling programs, SoundPLAN, Cadna|A, Insul, and the Environmental Noise Model; along with many agency developed noise models such as the Federal Aviation Administration's Integrated Noise Model (INM), Federal Highway Administration based software such as Sound 32, the Roadway Construction Noise Model (RCNM), and the Traffic Noise Model (TNM), along with many others.

Relevant Previous Experience

123 Independence Drive Multi-Family, Menlo Park, California. Coordinated ambient noise monitoring and traffic noise calibration measurements. Analyzed potential noise sources associated with the proposed project and their potential effect on nearby noise-sensitive receptors. Prepared noise and vibration section for the proposed project's CEQA documentation.

University Avenue Shell Station Car Wash, East Palo Alto, California. Researched alternative mechanical equipment and mitigation options for a proposed car wash project, and the potential noise effects at nearby noise-sensitive uses.

Santa Cruz Water Department, Laguna Creek Dam Coanda Screen Retrofit, Santa Cruz, California. Develop and implement a sound level monitoring program to characterize existing noise sources associated with project operations and quantify the existing noise environment. Perform an analysis of construction and operational noise levels. The dam was also of historical significance which required substantial research and evaluation of the potential for construction operations to jeopardize the historical significance.

Education

Sierra College
AS, Electronic Technology,
2006
AS, Computer Technology,
2006
Certificate in Mechatronic
Systems, 2005

Certifications

AVIXA Certified
Technology Specialist
(CTS)

Professional Affiliations

Acoustical Society of
America
Association of
Environmental
Professionals
AVIXA
INCE

San Jose Water Advanced Metering Infrastructure Project. The SJW AMI project will update the metering capabilities of water distribution within the SJC service area. The project included installation of metering end-points that connect to sixteen receiving stations throughout the service area. Existing conditions and potential noise impacts associated with the project were analyzed and compiled into the Proponent's Environmental Assessment.

Southport Levee Improvement Program, West Sacramento, California. Develop and implement a construction vibration-monitoring program for the levee improvement project. Conduct pre- and post-construction structural inspections at more than 50 structures in close proximity to vibration generating construction activities. Install, monitor and maintain 24-hour remote vibration monitoring stations throughout the project area.

Morris Dill Pickle Ball Courts, City of Pacific Grove, California. Perform in-depth monitoring of the ambient noise environment in the community Morris Dill Courts and characterization of source-noise levels generated by the pickle ball activities at the courts. Develop a 3D computer model to evaluate the effectiveness of a proposed barrier surrounding the pickle ball courts.

Amakara Dublin, Dublin, California. Developed conceptual and program level acoustical, audio and visual design improvements based on an evaluation of the existing building shell and input from key stakeholders. Characterize traffic noise level exposure at the proposed outdoor dining area. Prepare an environmental noise memo in regards to the potential noise associated with the outdoor dining area and the potential impact on residential units above.

Walnut Grove Storage Tank and Booster Station Improvements, Isleton, California. Developed and performed an environmental noise monitoring and prediction program for the proposed booster station improvement project. Characterized and evaluated transportation noise levels due to haul truck and pump truck activities at the proposed project site.

State Route 85 Noise Reduction Feasibility Study, Santa Clara County, California. Developed an assessment methodology to determine if feasible and reasonable measures exist within today's highway noise mitigation technology, to reduce the impact of SR 85 traffic noise at nearby receptors. Collaborated with local and regional stakeholder agencies as well as Caltrans and the FHWA. Modeled noise level reductions at pilot locations along nearly 20-miles of SR 85.

Faith Lutheran, Pleasant Hill, California. Developed conceptual and programmatic acoustical, audio and visual design improvements based on an evaluation of the existing sanctuary and input from key stakeholders.

Riviera Family Apartments and Townhomes, Walnut Creek, California. Evaluated land use compatibility for a multi-site apartment home community, located adjacent to an elevated portion of the I-680 and nearby intermodal transit facility. Developed structural and sound insulation measures to address interior noise exposure within the community. Predicted and characterized construction noise level impacts at nearby sensitive uses.

Day Care Code Compliance, City of Livermore, California. Develop and implemented a noise monitoring program to characterize noise levels generated by an existing day care use. Evaluate day care noise levels for compliance with applicable criteria and provide recommendations for treatment and mitigation.

CDCR Infill Facility EIR, State-wide, California. Evaluation of proposed infill facilities at five locations throughout the state; including R.J. Donovan, CSP Solano, CSP Sacramento, Mule Creek State Prison and California Institute for Men, Chino. Transportation and stationary source noise modeling, future noise-source impact assessment and report authoring.

Warm Springs/South Fremont Community Plan, Fremont, California. Analyzed community noise impacts associated with the implementation of the Warm Springs Community Plan. Authored the noise section for inclusion in the EIR.

Dennis Pascua

SENIOR TRANSPORTATION PLANNER

Dennis Pascua (DEN-iss PAS-kwa; he/him) is a senior transportation planner and Dudek's transportation services manager with 29 years' experience in transportation planning/engineering throughout California. Mr. Pascua has successfully managed a variety of projects for local agencies and private developers, including traffic and circulation impact analyses and parking demand studies in both highly urbanized and rural areas. He is highly experienced with California Environmental Quality Act/National Environmental Policy Act and transportation topics and policies surrounding active transportation, context sensitive solutions, and Complete Streets throughout California. Mr. Pascua also offers an international perspective, having managed transportation planning projects in the Philippines, Japan, and the United Arab Emirates.

Education

University of California,
Irvine
BA, Social Ecology
(Environmental Analysis
and Design)

Professional Affiliations

Institute of Transportation
Engineers
American Planning
Association
Association of
Environmental
Professionals

Project Experience

Martin Substation/Egbert Switching Station Project, On-Call CEQA/NEPA Services, California Public Utilities Commission (CPUC), San Francisco, California. Serving as senior transportation specialist for review of the Pacific Gas & Electric's proposed switching station and transmission line project. Responsibilities included review of the traffic section of Proponent's Environmental Assessment, preparation of the traffic section of an Initial Study and Environmental Impact Report, and responses to public comments related to traffic issues.

Hunter Residential Subdivision, St. Helena, California. Managed the team that prepared the traffic impact analysis (TIA) for a 101 residential dwelling-unit (DU) development, located on a 16.9-acre vacant site within the City of St. Helena, along the eastern terminus of Adams Street and along the northern terminus of Starr Avenue. The proposed project includes a residential subdivision tentative map consisting of 51 single-family residences, 25 work force income-restricted multifamily units, and 25 accessory (granny) dwelling units (ADUs) for a total of 101 dwelling units. Currently, the project site is located without access to roadways; however, as part of the project Adams Street and Starr Avenue would be extended to serve the site. Primary access to the project site would be provided via the eastern extension of Adams Street, as well as the northern extension of Starr Avenue. The TIA contained both, a vehicles miles traveled (VMT) and level of service (LOS) analysis prepared consistent with CEQA and City requirements.

Bellevue Ranch 7 Residential Development, Santa Rosa, California. Managed the team that prepared the focused traffic analysis for a new residential development in the City of Santa Rosa. The proposed project would include the demolition of an existing single-family residence and development of 30 single-family homes with up to seven accessory dwelling units on the 5.75-acre project site. Access to the project would be via Dutton Meadow. The proposed project would also include construction of new roadways including Vintana Drive, Common Way, Countryside Lane, and Crosswinds Way. The focused traffic analysis assessed vehicular sight distance related to new roadways and intersections proposed by the project, as well as, alternative emergency access designs proposed on project streets (e.g., turnarounds, hammerheads, etc.).

EID Wastewater Collections Operations Facility Relocation, El Dorado County, California. Managed the team that prepared the transportation impact study (TIS) for a wastewater facility relocation project. The El Dorado Irrigation District (EID) is proposing to relocate their existing Bass Lake wastewater collection operation and maintenance facility to EID's existing El Dorado Hills Wastewater Treatment Plant. The proposed project includes expansion of the existing facilities including new office buildings, maintenance and materials storage buildings, resurfacing areas for vehicle circulation and parking, operations support materials and equipment staging areas. The project would be located within the existing 82.5-acre property at 4625 Latrobe Road in the community of El Dorado Hills in unincorporated El Dorado County. The TIS focused on the cumulative traffic impacts forecast along Latrobe Road and White Rock Road.

Former Dixon High School Modernization Project, Dixon, California. Managed the team that prepared the traffic assessment for a new middle school, on the former Dixon High School campus, in the Dixon Unified School District. The proposed project will modernize the existing old Dixon High School campus and repurpose the site as a 750-student middle school, serving grades 6 through 8. No significant growth in new students are anticipated to occur under the proposed project. Instead, all 219 6th grade students from existing elementary schools in the District (Anderson, Gretchen Higgins, and Tremont Elementary Schools), and all 535 7th and 8th grade students from the existing C.A. Jacobs Middle School, would be re-distributed to the new Dixon Middle School, for a total of 754 students. The proposed project is located within residential zoning, and is bordered by Anderson Elementary School to the north, single-family homes to the west and south, and farmland to the east. The traffic assessment analyzed the traffic impacts from the diversion of students from the existing schools within Dixon, to the proposed new middle school; as well as, proposed traffic demand management measures proposed by the District (i.e., staggered school start times, and specified student loading areas).

Montclair Place District Specific Plan, Montclair, California. Managed the in-house Transportation team that prepared the Traffic Impact Analysis (TIA) that identified potential project-related traffic impacts associated with the buildout of the Montclair Place District Specific Plan (MPDSP), on an approximately 104.35-acre project site in downtown Montclair. The TIA was prepared per the requirements of the City, San Bernardino County Transportation Authority (SBCTA), and Caltrans requirements; and, included a vehicle miles traveled analysis per Senate Bill 743 (SB 743). The TIA comprised a study area of 60 intersections spread over four cities and two counties. A key feature of the MPDSP would provide for the construction of a pedestrian-oriented, mixed-use downtown district, with structured parking facilities through a series of planned phases. At buildout of the MPDSP, the following uses would be operating on the project site: 5,366 mid-rise residential units; 955 high-rise residential units; 331,056 square feet (SF) of general office; 201,452 SF of medical offices; a 250 room hotel; 74,030 SF of civic uses; 1,170,853 SF shopping center uses; 72,682 SF of retail uses; and, a 109,836 SF movie theater.

Ball Road Basin EIR, Anaheim, California. Managed and prepared the traffic impact analysis (TIA) for the rezoning of 19 acres of Open Space (an existing empty/unused groundwater basin) to General Commercial uses on Orange County Water District property in the City of Anaheim. Transportation issues for this project include coordination with Caltrans and the adjacent City of Orange. The study area consists of over 40 intersections and 30 roadway segments, including the SR 57 freeway from I-5 to SR 91. The Anaheim Transportation Analysis Model (ATAM) was used for this analysis. In addition, Caltrans operations analyses were conducted for freeway mainline lanes, freeway on-ramp metering, and merge-diverge segments.

Charles Greely, PE, LEED AP, QSD

PRINCIPAL ENGINEER

Charles Greely (CHAR-uhls GREE-lee; he/him) is a civil engineer with 24 years' experience working with public and private entities on infrastructure improvement projects throughout California. Mr. Greely provides a diverse skill set, having provided project management and design services on water resource, wastewater treatment, environmental mitigation and restoration, stormwater management and quality control, transportation, and site development projects. As a Leadership in Energy and Environmental Design Accredited Professional (LEED AP), he specializes in the application of low-impact development techniques for infrastructure improvement projects. Mr. Greely is experienced in federal, state, and regional permits and requirements for construction work within sensitive environmental settings and can therefore successfully bridge the gap between environmental concerns and cost-efficient, field-tested construction solutions.

Project Experience

West Mission Bay Bridge Traffic Control Plan, Flatiron Construction, City of San Diego, California. Lead engineer for the preparation of alternative staging and traffic control plans for the West Mission Bay Drive Bridge crossing the San Diego River. The alternative staging plan allowed for half of the existing bridge to accommodate two-way traffic during demolition and construction of the west bound lanes of the new bridge. The staging allowed for an accelerated construction schedule, reduced traffic impacts to residents and businesses, and saved the City over \$1,000,000 in cost.

Industrial Way Street Improvement and Pavement Rehabilitation, Catholic Diocese of San Bernardino, City of Coachella, California. As part of the as-needed services agreement with the City of Coachella, evaluated the existing condition of Industrial Way between Enterprise Way and Polk Street. The existing pavement was found to be in moderate to poor shape, and a significant portion of the street existed at less-than-ultimate width and without curb and gutter. Based upon these findings, prepared street improvement and pavement rehabilitation plans for the City of Palm Desert that will improve the aesthetic quality of Industrial Way, as well as driver safety and comfort for this stretch.

Calle Tampico Street Improvement and Pavement Rehabilitation, City of La Quinta, California. Project manager and lead engineer for design of improvements to reduce nuisance water in Calle Tampico. The design includes a new curb and gutter, spandrels, and cross gutters; new access ramps; and a Maxwell Plus© dry well. The design also included a new sidewalk and access ramp on Eisenhower Drive from Avenida Martinez to Calle Tampico, and design of a new dry well for nuisance water at Avenue 50 and Park Street.

Essential Transportation Infrastructure Phase II, City of Desert Hot Springs, California. Provided civil engineering services for the repair and rehabilitation of several roadways. This fast-track design job depended on partial federal funding by Safe, Accountable, Flexible, Efficient, Transportation Equity Act: A Legacy for Users funds,

Education

University of Washington
BS, Civil Engineering,
1998

Certifications

Professional Civil
Engineer (PE)
CA No. 69056;
WA No. 40823

South Coast Air Quality
Management District
Fugitive Dust Control
Certification,
No. 05-08-3112

LEED AP

Qualified SWPPP
Developer (QSD),
No. 69056

Professional Affiliations

California Water
Environment Association,
State Board of Directors
2013–2019

requiring the completion and submittal to Caltrans of 95% complete construction plans within three months of notice to proceed. Key design issues included careful incorporation of existing driveways, mailboxes, water meters, sewer clean-outs, and power poles, as well as compatibility of existing curb and gutters into the new design improvements. Provided the control survey for the photogrammetry work, coordinated the associated geotechnical work for basis of design and repair, and conducted the detailed existing utilities research. The city was provided 30% design concepts for initial review, and 90%, 100%, and final construction drawings according to the city's scheduling. Assisted the city during the bidding process.

Dinah Shore and Portola Improvements, City of Palm Desert, California. Provided project management and design services for improvements along Dinah Shore and Portola in Palm Desert, California. The improvements include median island, curb and gutter, storm drains, water and sewer improvements, as well as utility relocations. The improvements also included a traffic signal, signing and striping improvements, and super elevated street cross sections. The project also included coordination with Riverside County Transportation Commission staff and consultants as the design will ultimately be integrated into a future freeway interchange. Dudek staff has also assisted the City with property acquisition and prepared legal plats and descriptions for acquisitions and easements required for the final improvements.

Pavement Management Plan Rehabilitation, City of La Quinta, California. Project Manager and lead technical engineer for the rehabilitation of various street segments throughout the City. The project included review of existing City Pavement Management Plan data and condition assessments, field review of paving conditions; coordination and management of a geotechnical evaluation to review existing structural sections and subgrade conditions; and review of ADA improvements (sidewalk and access ramps) in the project corridors. Following the condition assessment, led the design team in the preparation of improvement plans utilizing cold-in-place recycling (CIR) techniques for the majority of the rehabilitation. In addition to the "green" benefits of a recycled product and process, the CIR process saved the City significant time and budget versus traditional grind and overlay methodology.

Avenue 58 and Eisenhower Drive Pavement Rehabilitation, City of La Quinta, California. In an effort to rehabilitate and convert older Riverside County streets to conform to current city standards, the City of La Quinta contracted with Dudek to perform field investigation of current street conditions (including sub-grade and structural section analysis). The project areas were found to have an insufficient structural section that required full rehabilitation of the roadways. Prepared full street rehabilitation improvement plans, including design of curb and gutter, sidewalk, and ADA ramps in locations where existing improvements either did not exist or did not meet current city standards.

Jefferson Street Wall and Sidewalk Project, City of La Quinta, California. Project Manager for the design of civil engineering design for sidewalk and grading improvements to the Jefferson Street corridor from Westward Ho Drive to the bridge over the Whitewater Wash. The project required close coordination with the Imperial Irrigation District for the resolution of utility conflicts throughout the proposed alignment. Dudek prepared meandering sidewalk, garden wall, and wrought iron fence improvement plans for the project area. Also included was earthwork analysis to confirm that the site grading balanced (no net import or export) and preparation of project specifications and an engineer's estimate of probably construction costs.

Mladen Popovic, AICP

TRANSPORTATION PLANNER

Mladen Popovic (*MLAH-den PAH-puh-vitch; he/him*) is a transportation planner with 7 years' experience focusing on transportation impacts for a wide variety of projects. Mr. Popovic has an educational background in planning and experience working within traffic impact analysis procedures, including vehicle miles traveled analysis, data collection, cumulative project development, trip generation calculations, level of service (LOS) analysis for intersections and roadway segments, signal warrant analysis, construction traffic, internal circulation and access evaluation, and vehicle turning analysis. He has utilized various types of transportation and design software including Synchro, Traffix, AutoTurn, as well as other technical programs, such as ArcGIS and AutoCAD.

Project Experience

Santa Cruz Water Rights Project EIR, City of Santa Cruz, California. Worked as part of the transportation team on the Santa Cruz Water Rights Project EIR, including preparation of the EIR Transportation section. Components of the project include modifications to existing water rights and related actions required to implement the proposed modifications, including expansion of place of use, clarifications on method and points of diversion and redirection, adding an underground storage supplement, extension of time to put water to full beneficial use, and incorporation of bypass requirements for each water right. The underlying purpose of the project is to improve City of Santa Cruz water system flexibility while enhancing stream flows for local anadromous fisheries. Physical infrastructure improvements include aquifer storage and recovery facilities in the Beltz system and potentially elsewhere, intertie facilities to allow for water transfers with neighboring agencies, and improvements to the Tait Diversion/Coast Pump Station and the Felton Diversion.

Santa Cruz County Sustainability Update, Santa Cruz County, California. Providing transportation assessment and analysis for the EIR of a comprehensive update to Santa Cruz County's General Plan/Local Coastal Program and modernization of the County Code. The current General Plan/Local Coastal Program will reflect new state laws, new regional and local plans, and a community vision that is focused on sustainable growth. Key changes include establishing a multi-modal transportation network, preserving agricultural land, and modernizing commercial and industrial properties. Tasks include transportation analysis, preparation of technical analysis, and preparation of EIR sections.

Hunter Subdivision Project EIR, City of St. Helena, California. Lead the Traffic Impact Analysis and Transportation Section for the Environmental Impact Report (EIR) for a proposed residential project on an approximately 17-acre parcel located near downtown St. Helena. The project includes 51 single-family homes, 25-multi-family units, and up to 25 accessory dwelling units. The analysis included a detailed weekday, Saturday intersection LOS analysis, vehicle miles traveled analysis, and horizon year analysis. The project is very controversial, and the City of St. Helena anticipates a legal challenge to the EIR. It is anticipated the EIR will be released for public review in 2021.

Education

University of California,
Santa Barbara
BA (with Honors),
Geographic Information
Systems
BA (with Honors),
Environmental Studies
University of California,
Irvine
Masters, Urban and
Regional Planning

Certifications

AICP No. 31419

Professional Affiliations

American Planning
Association

Institute of Transportation
Engineers

Focused Traffic Analysis for the Bellevue Ranch 7 Project, City of Santa Rosa, California. Worked as part of a team to analyze the impacts associated with 30 single-family homes and up to 7 accessory dwelling units in the City of Santa Rosa. Specialized analyses included sight distance analysis, pedestrian, bicycle, and transit accessibility, site circulation, and emergency vehicle analysis. American Association of State Highway and Transportation Officials guidelines were utilized to create AutoTurn turning analysis for the maneuverability of emergency vehicles on site, as well as the stopping distance for the site that fronts a rural two-lane roadway.

Stony Point Flats Project Technical Studies and CEQA Document, Santa Rosa, California. Prepared the transportation technical memorandum and transportation analysis in support of the preparation of an addendum to the 2016 Roseland Area/Sebastopol Road Specific Plan EIR for the Stony Point Flats project. The 50-dwelling unit affordable apartment community consists of three buildings on a mostly vacant lot in the Roseland area of Santa Rosa. Major transportation issues addressed included: vehicular circulation, emergency access, site access, and vehicular queuing. An appeal to approval of the addendum was upheld by the City Council in November 2021.

Former Dixon High School Modernization, City of Dixon, California. Lead analysis which dictated the re-opening of the former site of Dixon High School into a 750-student middle school. Since the former high school was abandoned, all incoming middle school students in the Dixon Unified School District, as well as the existing middle school, were re-allocated to the project site. This required a detailed analysis of students' trip origin points, key arterial roadways that facilitate vehicular trips within the City of Dixon, as well as an analysis of the railroad tracks that separate the City into two halves. Additionally, vehicular queuing at the 95th percentile, project site access, and pedestrian and bicycle safety were evaluated as part of the traffic impact analysis.

Warner-Ivy Street Vacation California State University – Chico, City of Chico, California. Technical analysis that evaluated a proposed street closure of Warner Street and Ivy Street, which bisects the campus of California State University, Chico. A detailed origin-destination analysis was conducted to determine the vehicular users affected by the proposed project. Finally, a LOS analysis was conducted and compared to the existing conditions, to determine the impact of the street vacation.

Home2Suites Hotel, City of Rohnert Park, California. Lead analysis which included a proposed Home2Suites Hotel location within a shared commercial corridor. The report focused on the internal site circulation of the hotel, sight distance analysis, and LOS analysis. In-person field visits were vital in cataloging the existing roadway network and intersection conditions and providing an accurate analysis.

Huntington Park General Plan Update, City of Huntington Park, California. Contributed on the traffic impact analysis for this project that served to analyze the proposed update to the City of Huntington Park's General Plan. The project involved re-analyzing existing conditions and utilizing model data from the Southern California Association of Governments travel demand model for buildout year conditions for seven planning areas that comprised 12 critical intersections. Once model results were post-processed and refined, intersections and roadway segments were distributed accordingly based upon the changes of density of uses proposed within the General Plan Update. Regional transit and pedestrian circulation were also analyzed.

Los Angeles Department of Water and Power Projects, Los Angeles, California. Worked as a part of a team multiple Los Angeles Department of Water and Power projects that analyzed various construction impacts associated with trunk line, pipeline, and facility construction. The analyses focused on the temporary construction impacts upon both residential and urban corridors, impacts to pedestrian and multi-modal accessibility, and LOS. As part of the Los Angeles Department of Transportation guidelines, critical movement analysis was performed for a wide range of areas across Los Angeles.

Sabita Tewani, AICP, PTP

TRANSPORTATION SPECIALIST

Sabita Tewani (*SAB-ih-tah TE-wah-ni; she/her*) is a transportation planner with 12 years' experience in transportation assessments for due diligence planning. Ms. Tewani is experienced in preparing transportation-related environmental documentation for land use, transportation, and construction projects and vehicle miles traveled (VMT) estimation requirements per Senate Bill (SB) 743 compliance for the updated California Environmental Quality Act (CEQA) Guidelines for transportation impact analyses. Ms. Tewani has prepared several VMT assessments using web and/or Geographic Information System (GIS)-based mapping tools, VMT Calculators and Estimators, Location Based Service data from StreetLight big data company, greenhouse gas (GHG) emission thresholds, sketch-planning tools, as well results from custom travel demand model runs conducted for projects located in several jurisdictions in California. Ms. Tewani is familiar with using GHG reductions in transportation sector using the California Air Pollution Control Officers Association guidance. Ms. Tewani is experienced in all phases of traffic study analysis, including data collection, trip generation calculations, level of service (LOS) analysis for freeway and roadway segments and intersections, signal warrant analysis, all-way stop-control warrant analysis, and evaluation of internal circulation and access. She has used Synchro, Traffix, and Highway Capacity Software for traffic data and operations analysis. Ms. Tewani is proficient in preparing traffic study reports and technical memorandums per General Plan Consistency and screening requirements of numerous jurisdictions in California.

Relevant Project Experience

Creative Offices Specific Plan Project, City of Beverly Hills, California. Prepared the transportation section of the Beverly Hills Creative Office Specific Plan Project EIR, which identifies associated regulatory requirements, evaluates potential impacts, and identifies mitigation measures related to implementation of up to 11 office buildings arranged in a linear fashion within the site as part of the Specific Plan. The environmental document included analysis of project-level VMT prepared by a subconsultant using the current 2016 SCAG Regional Transportation Plan/Sustainable Communities Strategy trip-based model to assess the project's VMT and comparing it with the city's baseline VMT metrics and VMT impact threshold. The analysis also included construction traffic and parking impacts, as well as impacts to the existing and proposed roadway, bicycle, transit, and pedestrian facilities. (2021–Present)

Encompass Health, City of Chula Vista, California. Prepared the Local Transportation Analysis (LTA) and Vehicle Miles Traveled (VMT) Analysis for an 80-bed inpatient rehabilitation facility with supporting amenities located on a 9.56-acre site in the City of Chula Vista. Based on review of published trip rates and operational characteristics of similar facilities, recommended a composite trip generation rate for the proposed rehabilitation center, which was approved by the city staff to be used in the project's LTA. The LTA documented existing traffic conditions, including roadway segment and intersection LOS in the study area; estimated trip generation, distribution, and assignment

Education

Newcastle University
MSc, Transportation
Engineering and
Operations, 2004

School of Planning and
Architecture, New Delhi
MA, Planning
(Transportation
specialization), 2002
BA, Architecture, 2000

Certifications

American Institute of
Certified Planners (AICP)

Professional
Transportation Planner
(PTP)

Professional Affiliations

American Planning
Association

Institute of Transportation
Engineers (ITE)

ITE San Diego Section
Transportation Capacity
and Mobility Task Force
and ITE SB 743 Modeling
Sub-Committee

characteristics for the proposed project; and analyzed the traffic impacts that would occur as a result of project traffic under the existing and buildout year (2035) conditions. The project's VMT analysis was prepared consistent with the OPR 2018 Technical Advisory and the ITE 2019 Draft Guidelines for Transportation Impact Studies in the San Diego Region. (2019–2020)

Byron Airport Development Program EIR, Contra Costa County, California. Prepared the transportation section for the EIR, which identifies regulatory requirements, evaluates potential impacts, and identifies mitigation measures related to implementation of the warehousing, industrial, general commercial, and office land uses proposed as part of the Byron Airport Development Project. This section was based on the TIA prepared by Dudek that used VMT for CEQA and the LOS metric for General Plan consistency requirements. To mitigate project's significant VMT impact, prepared a detailed discussion of strategies from Contra Costa County's Transportation Demand Management Ordinance Guide and the California Air Pollution Control Officers Association that would be most effective in areas like the community of Byron and are appropriate for the project to avoid or reduce the significant impact. VMT reductions for each strategy (using the applicable range of effectiveness) were applied to the project per California Air Pollution Control Officers Association's calculations and selected measures were included as mitigation measures to reduce project's VMT impact. (2020–Present)

Morse Recreation Center, City of Elk Grove, California. Prepared the transportation section and analysis that addressed the potential of VMT impacts of the proposed recreation center, tournament, and league play operation at the Morse Recreation Center new, 54,222 square foot recreation center on 3 acres of undeveloped land within an approximately 14-acre parcel that is part of the existing 30-acre Morse Community Park. (2021-Present)

Palm Villas Saratoga Project EIR, City of Santa Clara, California. Managed the transportation subconsultant (Fehr and Peers) for the proposed residential care facility for the elderly. The project consists of two buildings on two adjacent lots that have been designed to function as a single complex. One building would be for individuals with mild stage Alzheimer's/dementia and the second for individuals with advanced stage Alzheimer's/dementia. The proposed project would include a combined total of 78 beds, related support functions, 47 parking spaces, landscaping, utility connections, and an extension of Saratoga Creek Drive to the south by approximately 150 feet (included a bifurcated portion at the end for fire truck turnaround). (2019)

Focused Traffic Analysis for the Bellevue Ranch 7, City of Santa Rosa, California. Prepared a Focused Traffic Analysis for development of 30 single-family homes on a 5.75-acre project site in the City of Santa Rosa. The project is consistent with the City of Santa Rosa General Plan 2035 and the City of Santa Rosa Municipal Code. The analysis included the project's trip generation and distribution, site circulation analysis, sight distance analysis, and traffic control at the project access and a pedestrian, bicycle, and transit accessibility analysis. (2017–2018)

Trip Generation Analysis for 2600 Telegraph Avenue, Holland Acquisition Co., LLC City of Oakland, California. Prepared a Trip Generation Analysis of the mixed-use project proposed at 2600 Telegraph Avenue, located at the southeast corner of Telegraph Avenue and 27th Street in the City of Oakland. The proposed project would demolish the existing 19,600 square-foot single-story commercial building on the site and would construct an eight-story mixed use development comprising of 225 residential units and approximately 6,039 square feet of retail on an approximately 0.91-acre property in the city. The analysis was conducted to determine the trip generation estimates of the proposed project and determine which study components under the City of Oakland's Traffic Impact Review Guidelines are required for the travel and transportation analysis for the CEQA document. (2019–2020)



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REVISED PROPOSAL FOR CEQA DOCUMENT PREPARATION FOR 980-1030 O'BRIEN DRIVE



SUBMITTED TO

Fahteen Khan, Associate Planner
City of Menlo Park
701 Laurel Street
Menlo Park, California 94025

January 6, 2023

SUBMITTED BY

SWCA Environmental Consultants
60 Stone Pine Road, Suite 100
Half Moon Bay, California 94019

A. COVER LETTER

January 6, 2023

Fahteen Khan, Associate Planner
City of Menlo Park
701 Laurel Street
Menlo Park, California 94025
Submitted via email: fnkhan@menlopark.gov

Re: Revised Proposal for CEQA Document Preparation for 980-1030 O'Brien Drive Project

Dear Ms. Khan:

SWCA, Incorporated, dba **SWCA Environmental Consultants (SWCA)**, appreciates the opportunity to submit our revised proposal to prepare a Notice of Exemption (NOE) or Initial Study/Mitigated Negative Declaration (IS/MND) and associated technical backgrounds studies for the proposed Life Sciences Research and Development property (project) at 980–1030 O'Brien Drive in the city of Menlo Park (City), California. SWCA offers the size and stability of a larger company and the focus and responsiveness of a small local firm. With over two decades of experience conducting California Environmental Quality Act (CEQA) environmental review for projects throughout northern California and having worked on over 100 projects within San Mateo County (County), our team is intimately familiar with the nuances of preparing an IS in the Bay Area. Our scope of work reflects our familiarity with applicable regional and City plans and policies and a clear understanding of the key environmental and procedural issues likely to be associated with the project.

We are well versed in working with cities that are balancing ongoing rapid growth and development. Our experience demonstrates in-depth knowledge of environmental review and analysis for high-profile projects with intense public comment and scrutiny throughout the greater Bay Area. Examples of such projects include the Canyon Lane Roadway Improvements (Environmental Impact Report) in San Mateo County, which received hundreds of individual comments; the 201 Haskins Way EIR in San Francisco, which involved construction of significant new office/research and development space; and Raley Boulevard IS/MND in Sacramento, which balanced industrial development with traffic concerns and on-site biological resources.

Drawing on over two decades of experience providing environmental services in northern California, SWCA offers the ideal combination of local focus, bench strength, and technical expertise.

Our core management team will be led by **Project Manager Kara Laurenson-Wright**, who will serve as the day-to-day primary point of contact with the City. Ms. Laurenson-Wright has experience working on the 123 Independence Drive Project, another high-profile project in the Bayfront area of the City. Her proven ability to manage teams and prepare technically sound and defensible documents that meet fast-track deadlines will be a definite and immediate asset to the IS for the proposed project. Ms. Laurenson-Wright will be supported by SWCA's Bay Area Planning Team Lead **Julie Barlow, AICP**, who will provide project

oversight, and [Senior Project Planner Peter Mye, MURP](#). SWCA's long-time teaming partner and subconsultant [Kittelson & Associates](#) will provide traffic impact analysis support.

If you have any questions regarding our project approach or experience, please feel free to contact Ms. Laurenson-Wright at (415) 599-6464 or Kara.LaurensonWright@swca.com. We are always willing to negotiate our approach, scope of work, and team to suit your needs and expectations. Thank you for the opportunity to submit a proposal for this project, and we look forward to the possibility of working with you.

Sincerely,

A handwritten signature in blue ink, appearing to read 'LM', is positioned above the typed name.

Laura Moran
Vice President, Northern and Central California



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B. PROJECT TEAM

FIRM PROFILE

A 100% employee-owned S-corporation established in 1981, **SWCA Environmental Consultants (SWCA)** specializes in providing comprehensive environmental planning, regulatory compliance, and natural and cultural resources management services to businesses and government clients across the United States. SWCA has grown into a national business with **over 1,300 employees in 39 offices nationwide, including seven offices in California—Half Moon Bay, San Francisco, Oakland, Sacramento, San Luis Obispo, Pasadena, and San Diego.** We are a firm of scientists, planners, and technical specialists who combine scientific expertise with in-depth knowledge of environmental compliance to achieve technically sound, cost-effective solutions for our clients.

In the past two decades, SWCA has completed hundreds of planning and environmental documents for state, local, and private projects for local agencies throughout northern California, including the County of San Mateo (County). We offer local expertise, but we are also able to rapidly pool resources from all our California offices to serve the City of Menlo Park (City) more efficiently.

Our team assists local jurisdictions in the preparation of a wide range of California Environmental Quality Act (CEQA) documentation, including preparation of initial studies (ISs), mitigated negative declarations (MNDs), environmental impact reports (EIRs) and supplemental EIRs, addenda to EIRs, and categorical exemption determinations. Our team of scientists and technical specialists also prepares the full range of background technical analyses needed to support CEQA determinations, such as air quality, greenhouse gas (GHG) emissions, energy, and noise studies and biological and cultural resources reports and determinations.

In addition to managing and preparing CEQA documents, SWCA has the qualifications and experience to:

- prepare key technical sections of CEQA documents;
- conduct strategic third-party review of documents prepared by others;
- comprehensively administer the environmental review process for agencies needing additional staff resources, including noticing, staff reports, presentations, and scoping;
- prepare mitigation monitoring and reporting program (MMRPs) and CEQA findings; and
- provide consulting and support to planning, design, and engineering teams.

SWCA has demonstrated a high level of competency in preparing all levels of CEQA documentation and has consistently produced quality deliverables in a timely manner for public agencies. We have completed EIRs under highly expedited time frames to accommodate outside factors affecting the project schedule, such as public interest

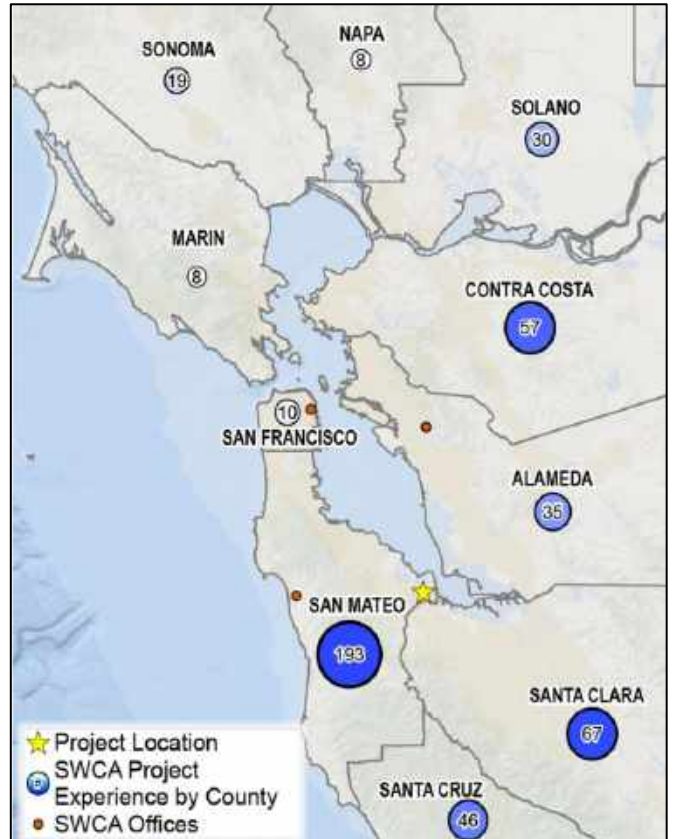



Figure 1. SWCA has extensive project experience in the San Mateo County and surrounding counties. This project will be managed out of our Half Moon Bay office.

and controversy, funding triggers, political demands, critical construction windows, and applicant-driven scheduling needs. The professionals on our growing team understand the full life cycle of a project and are familiar with the procedural steps and coordination necessary to successfully take a project from inception through the environmental review and entitlement process.

SUBCONSULTANTS

KITTELSON & ASSOCIATES, INC.

 Kittelson & Associates, Inc. (Kittelson) has provided transportation engineering, planning, and research services to public agencies and private organizations since 1985. The firm was established in Portland, Oregon, where it remains headquartered. In 2012, Kittelson merged with Dowling Associates, a California-based transportation planning and travel demand modeling consulting firm with 25 years of experience serving public agencies across Northern California. Today, Kittelson has a staff of over 300 and 24 offices in the U.S., including four offices in California (Oakland, Sacramento, San Diego, and Orange).

Kittelson offers support for all stages of transportation planning and environmental analysis, from site analysis to approval and construction. Staff are thoroughly knowledgeable about compliance with the CEQA and the National Environmental Policy Act (NEPA), including recent changes to CEQA-required transportation analyses such as SB 743. Kittelson brings a demonstrated understanding of transportation and circulation requirements, travel demand modeling, and regional knowledge developed over the course of more than 30 years of project experience in California. The firm approaches each effort by building an understanding of client and project needs and jurisdictional requirements and tailoring the work to assist clients in successfully fulfilling environmental requirements in an efficient and cost-effective manner.

Kittelson has provided environmental analysis for many development projects in California. These include general plans for the Cities of Colma, San Mateo, San Leandro, Millbrae, and Hayward; transit-oriented developments, including BART station area plans; specific plans for the Isabel BART Station, Concord Naval Weapons Station redevelopment, San Leandro Shoreline, and Downtown Hayward; public infrastructure, such as roadway extensions and commercial and retail centers; mixed-use developments involving housing; and K-12 schools. Kittelson has worked with many clients recently (including the City of San Mateo and the City of Dublin) to assist with developing the methodology, thresholds, and procedures for CEQA analysis of land use and transportation projects and plans in compliance with SB 743. Kittelson also advised the City of Menlo Park on the modeling for their vehicle miles traveled (VMT) guidelines.

ORGANIZATIONAL CHART

The SWCA project team, as illustrated in Figure 1, consists of permanent staff of project managers, technical specialists, and support staff in geographic information systems (GIS), technical editing, and document production. SWCA values sharing resources across offices as projects dictate. Resource flexibility, combined with a deep bench of expertise, allows for field crews to be supplemented by regional crews with California experience when necessary to meet project schedules. SWCA will be adaptable and responsive to City requests to provide specific personnel for the project.

LOCAL EXPERTISE & CONSISTENT QUALITY

SWCA has developed strategic programs to retain experienced staff and minimize employee turnover. The City will benefit from SWCA's commitment to our employees, knowing they will consistently receive high-quality work from a stable pool of local workers who are leaders in their fields.

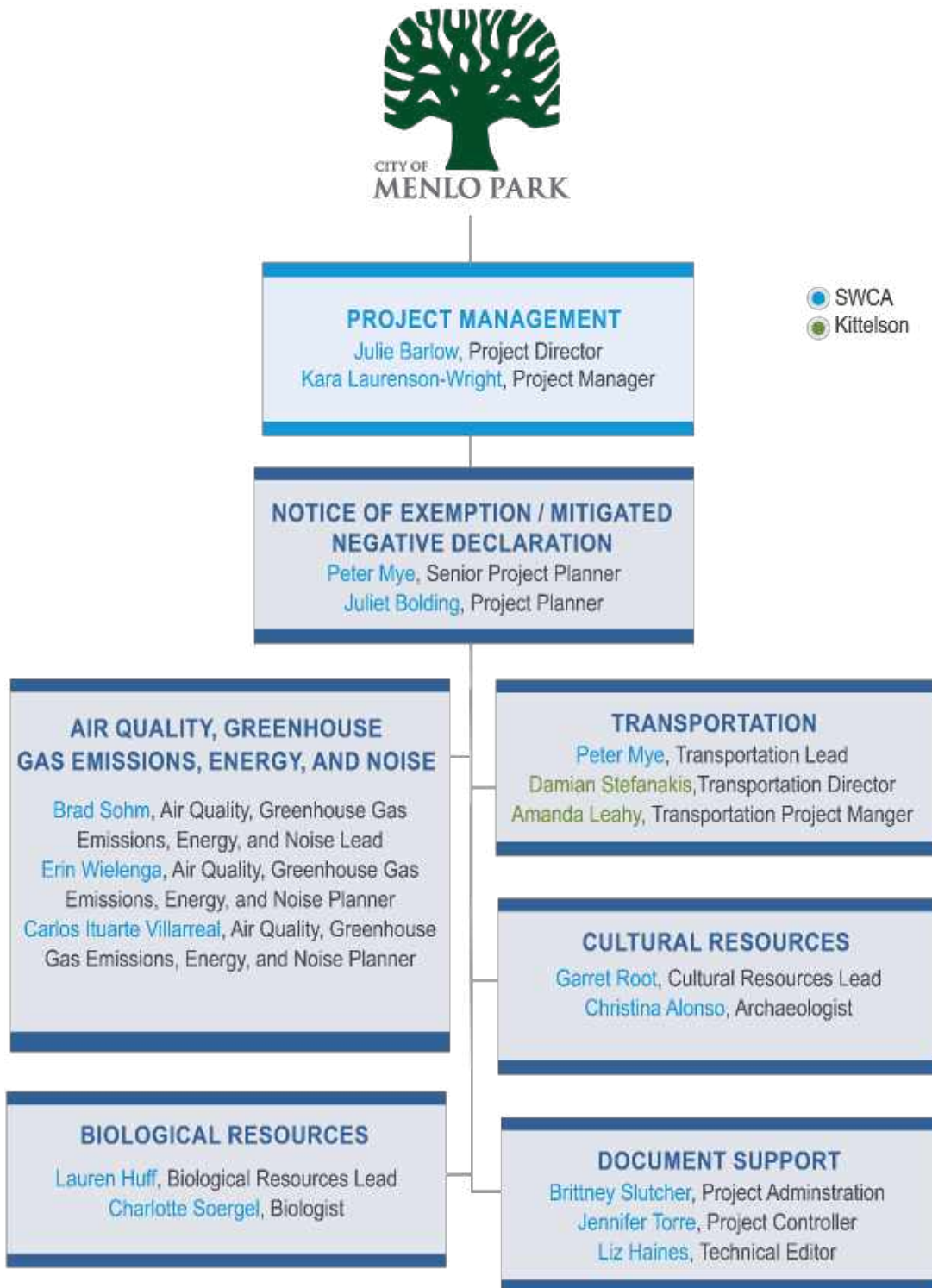


Figure 1. SWCA Organizational Chart. The SWCA team consists of a project management team experienced in environmental compliance, supported by specialists who have many years of experience in their fields. The SWCA team is configured to manage and successfully complete all aspects of the project efficiently and effectively. SWCA has the resources to substitute or add support staff if needed.

TEAM QUALIFICATIONS

JULIE BARLOW, AICP | PROJECT DIRECTOR

[Julie Barlow](#), a highly regarded senior-level project manager with more than 20 years of experience in community planning and environmental analysis in the public and private sectors in the Bay Area, will serve as project director. As a project manager and Planning Team Lead, she manages environmental planning documents, including EIRs, ISs, MNDs, supplemental EIRs, and responses to comments, for mixed-use development projects, business park developments, new medical centers, aviation projects, and educational/institutional projects, oversees staff work, and manages subconsultants for projects throughout northern California. Before joining SWCA, she served as the senior planner for the Town of Woodside.

KARA LAURENSEN-WRIGHT, B.S. | PROJECT MANAGER

[Kara Laurenson-Wright](#) will serve as the project manager. She will also be a primary contact for the City and will support Ms. Barlow. Ms. Laurenson-Wright is an environmental planner with 6 years of experience specializing in the management, research, and analysis of projects subject to CEQA and NEPA compliance. Ms. Laurenson-Wright has served in project manager and project planner roles on a variety of projects within the cities of Menlo Park, Saratoga, San Francisco and Santa Clara, San Mateo, and San Francisco Counties. Relevant projects on the Peninsula and in the South Bay Area include the 123 Independence Drive Project, Palm Villas Senior Living Facility Project, and Advanced Metering Infrastructure Project. She has experience in mixed-use development, large-scale residential projects, and development involving historic resources.

PETER MYE, MURP | SENIOR PROJECT PLANNER/TRANSPORTATION LEAD

[Peter Mye](#) will serve as the senior project planner and transportation lead. He has more than 20 years of environmental impact analysis, long-range planning, historic preservation, and public outreach experience in the public and private sectors. Mr. Mye has worked on projects that include large park components, including the Yerba Buena Island Redevelopment Project EIR and Pier 70 Mixed Use District Project EIR. He has managed CEQA projects, such as the Potrero Yard Modernization and Sixth Street Pedestrian Safety projects for the San Francisco Municipal Transportation Agency, and a complex mixed-use project at 3333 California Street in San Francisco. He is thoroughly familiar with the CEQA requirements and excels at managing complex data sets for technical projects and directing teams of staff and subconsultants, making sure each delivers on schedule and on budget. He oversees the transportation, noise, and air quality sections for many of our environmental review documents and has worked closely with technical experts on supporting background studies.

JULIET BOLDING, B.A. | PROJECT PLANNER

[Juliet Bolding](#) will serve as a supporting project planner. She is an environmental planner with over 20 years of experience specializing in the management, research, and analysis of projects subject to compliance with CEQA. She has served in deputy project manager and project planner roles on a variety of projects within Santa Mateo County, Santa Cruz County, Half Moon Bay, and San Luis Obispo County. Ms. Bolding was a project planner for the Canyon Lane Roadway Improvements Project, has completed five ISs for the City of Half Moon Bay in the last 2 years, and recently worked on the Pajaro Valley Water Management Agency F Pipeline Expansion Project and University of California, Santa Cruz, stormwater pollution prevention plan implementation. She has experience in land use and planning, hydrology, and water resources. Ms. Bolding will be one of the supporting staff planners.

BRAD SOHM, B.S., PE | AIR QUALITY, GHG EMISSIONS, ENERGY AND NOISE LEAD

[Brad Sohm](#) will serve as the air quality, GHG emissions, energy, and noise lead. He is a chemical engineer who specializes in air quality compliance and permitting, noise impact analysis, health and safety, environmental site

investigations, environmental remediation, and CEQA and NEPA impact assessments. He is thoroughly familiar with California Air Resources Board and Bay Area Air Quality Management District (BAAQMD) plans, policies, rules, and regulations. He also has extensive experience with state and county air quality permitting programs and compliance, including preparing non-Title V, Title V, and prevention of significant deterioration permits; completing technical review and data assessment of permitting air pollution control technologies to identify current best available control technology and lowest achievable emissions rate requirements for fuel-fired emission units; assisting with facility air permit audits to identify potential permit revisions/modifications; and other non-compliance issues. He has prepared and managed air quality permitting projects, including for ethanol plants, refineries, various manufacturing facilities, slag recovery facilities, oil and gas projects, recycling facilities, and electric utilities for industrial, military, and utility clients throughout the U.S.

GARRET ROOT, M.A. | CULTURAL RESOURCES LEAD

[Garret Root](#) will serve as the cultural resources lead. He has 13 years of experience in environmental compliance and historic research and resource assessments, survey, and mitigation and meets the Secretary of the Interior's Professional Qualification Standards for history and architectural history. He has extensive experience in the Bay Area developing regional and local historic contexts on residents, neighborhoods, and industries. These efforts have included documenting a wide variety of resource types, such as public buildings and schools, sewer and water lines, electrical and wastewater infrastructure, parks, waterfront properties, and residential and commercial buildings. This work was completed largely within local regulatory nexuses but also included CEQA- and National Historic Preservation Act Section 106-compliant documentation.

LAUREN HUFF, M.S. | BIOLOGICAL RESOURCES LEAD

[Lauren Huff](#) will serve as the biological resources lead. She is a senior wildlife biologist, project manager, and permitting specialist who has been working in the scientific field for over 16 years. She has been involved in all aspects of projects, including project management, field surveys, endangered and threatened species surveys, siting/constraints analyses, construction monitoring and compliance, document preparation, permit preparation, agency coordination and negotiations, impact analysis, and mitigation planning. Ms. Huff has extensive knowledge of federal and state regulations, including the federal Endangered Species Act, NEPA, Magnuson-Stevens Fisheries Act, Clean Water Act, Bald and Golden Eagle Protection Act, Migratory Bird Treaty Act, CEQA, Porter-Cologne Water Quality Control Act, McAteer-Petris Act, California Endangered Species Act, and California Fish and Game Code. Ms. Huff is certified in wetland delineation and holds a California Department of Fish and Wildlife (CDFW) Scientific Collecting Permit (SC-11361). She has experience performing and managing reconnaissance-level site surveys, habitat assessments, wetland delineations, nesting bird surveys and monitoring, construction monitoring and permit compliance, mitigation monitoring and compliance, and special-status species surveys.

DAMIAN STEFANAKIS, GDE | TRANSPORTATION PROJECT DIRECTOR

[Damian Stefanakis](#) will serve as the transportation project director. Mr. Stefanakis has 30 years of experience in transportation planning and travel demand forecast modeling. He specializes in the development and application of travel demand models for highway and transit projects using many types of software. He also has experience with application of regional MPO and local agency models, including CCAG-VTA and Alameda CMA countywide models. Mr. Stefanakis has also advised jurisdictions on preparation of VMT guidelines per SB-743, including San Mateo, Menlo Park and Dublin for regional transit and highway studies. Damian was the project principal working on four key development projects in Menlo Park, including Commonwealth, 111 Independence, Menlo Uptown, and Menlo Portal.

AMANDA LEAHY, AICP | TRANSPORTATION PROJECT MANAGER

Amanda Leahy will serve as the Transportation Project Manager. As a transportation planner with more than 13 years of experience working on and leading transportation studies in the Bay Area, Ms. Leahy understands the City of Menlo Park’s environmental review processes and requirements. Her areas of expertise include project management, environmental review, multimodal traffic operations and safety analysis, bicycle and pedestrian planning and design, and complete streets policy and implementation. She has worked on many complex projects from planning through to implementation that have required objective and transparent technical analysis and multi-agency coordination. Ms. Leahy is committed to creating a safe and comfortable public realm for users of all ages and abilities. She is passionate about strengthening connections between people and places through sound planning and analysis; creative, context-sensitive design and engineering; and effective stakeholder and community engagement.

PROJECT QUALIFICATIONS

201 HASKINS WAY PROJECT, SAN FRANCISCO, CALIFORNIA



SWCA prepared the EIR for the 201 Haskins Way Project in South San Francisco, an 18.2-acre site in the East of 101 area. The project includes two phases that will construct approximately 677,600 gross square feet of new office/research and development space and 158,000 gross square feet of light industrial space. The approach to impact analysis was complex in that a specific development plan was proposed for the first development, while a conceptual development program was proposed for second phase.

The project required amendments of the City of South San Francisco’s zoning code, as well as entitlements to enable development of the project area, including approval of a subdivision map, design review, transportation demand management plan, and conditional use permit. Key environmental issues included project-level and cumulative transportation and circulation impacts, land use impacts, and noise and vibration impacts. SWCA prepared the MMRP and public hearing presentation materials and presented the EIR conclusions for entitlement hearings. The City Council approved the project on March 27, 2019.

PIER 70 MIXED-USE DISTRICT PROJECT, SAN FRANCISCO, CALIFORNIA



SWCA directed the EIR on the Pier 70 Mixed-Use District Project, a major land development proposal for 35 acres of historic shipyard property along the City and County of San Francisco’s Central Waterfront. The project site is generally under the jurisdiction of the Port of San Francisco and encompasses roughly half of the 69-acre Pier 70 area.

Under the Pier 70 Mixed-Use District Project, a special use district (SUD) was adopted, which initiated rezoning and development of design standards and controls for a multi-phased, mixed-use development program. The project site will be redeveloped with residential, commercial, retail, arts, and light industrial uses. Because of the National Register of Historic Places (NRHP) designation, the project included rehabilitation and adaptive reuse

of three contributing historic architectural resources of the Historic District consistent with the Secretary of Interior Standards for Treatment of Historic Properties. The project envisions a flexible land use program under provisions of the SUD, which will allow between 4,179,300 and 4,212,230 gross square feet of new and rehabilitated development area. The project also includes 9 acres of new public open space, such as a waterfront park; transportation and circulation improvements, including new streets and district parking facilities; new and upgraded utilities and infrastructure, including a new regional pump station; and geotechnical and shoreline improvements to address flooding and rising sea levels.

“When I asked Julie to prepare a forward schedule, I know that it will be as aggressive as possible, but also achievable – a hard balance to find.”

– Kelly Pretzer, Development Director
 Brookfield Power US Asset Management LLC
 Pier 70 Mixed-Use District Project EIR

To provide flexibility over the 30-year construction duration, the EIR assumed a full range of potential land uses that could be developed under the proposed SUD; a maximum residential-use scenario and a maximum commercial-use scenario for the project site were analyzed at equal level of detail through all topics in the EIR. In addition, SWCA analyzed four separate infrastructure variants, each of which focused on sustainability and modified one limited feature or aspect of the project, including an option for use of on-site wastewater treatment collection and reuse. Other key considerations included in the EIR were sea-level rise impacts to historical resources and an expanded network of utilities, including construction of a new wastewater pump station designed in coordination with the San Francisco Public Utilities Commission.

For the responses to comments document, the EIR authors crafted responses to over 300 comments and evaluated revisions to the proposed project identified after the draft EIR was published. Revisions addressed a change to the proposed grading and stabilization plan to include controlled rock fragmentation; changes to the mix of proposed bedroom units; updates to the *Pier 70 SUD Design for Development*; and, to address concerns raised by commenters, a new variant that would create an east-west-running view corridor to the Irish Hill remnant, a contributing landscape feature of the Union Iron Works Historic District.

SAN MATEO COUNTY CANYON LANE ROADWAY IMPROVEMENTS PROJECT, COUNTY OF SAN MATEO, CALIFORNIA



SWCA prepared the EIR for the Canyon Lane Roadway Improvements Project, which proposed upgrades to Canyon Lane, one single-family residence, and 11 developable parcels that the roadway improvements project would serve, as well as a comprehensive alternatives analysis. The County of San Mateo, as the lead agency and primary project decision-maker, as well as the San Mateo Local Agency Formation Commission and the City of Redwood City, will rely on these environmental documents for approval of the City’s water line extension outside of the city boundaries. SWCA prepared a site-specific wildfire assessment to evaluate the project site’s exposure to wildland fire and a range of other technical supporting documents and analyses, such as air quality emissions modeling, cultural resources study, and a biological resources technical report.

SWCA worked with the County of San Mateo to hold a public scoping meeting and answered questions related to the CEQA process. SWCA and our subconsultants also provided peer review of the applicant-prepared technical background studies for the project, including a traffic study and geotechnical study. The public was also concerned with the loss of recreational opportunities, as Canyon Lane and the surrounding area were used as a trail by the

surrounding community. The County of San Mateo received hundreds of individual comments on the draft EIR, and SWCA prepared the responses to comments.

RALEY BOULEVARD AND DIESEL DRIVE WAREHOUSE PROJECT, CITY OF SACRAMENTO, CALIFORNIA



SWCA prepared the IS/MND and background technical studies for an approximately 4.95-acre parcel at Raley Boulevard and Diesel Drive in Sacramento, California. The project will construct two industrial warehouse buildings with site improvements and include a lot line adjustment. The background technical studies included an updated biological resources assessment and wetland delineation, a cultural resources technical report, a California Emissions Estimator Model (CalEEMod) (air quality) tabulation, and associated Section 404 permitting and Section 401 certification tasks. The City of Sacramento approved the project on November 4, 2022.

MENLO PARK TRANSPORTATION MASTER PLAN & IMPACT FEE PROGRAM UPDATE, CITY OF MENLO PARK, CALIFORNIA

Kittelson was part of the consultant team responsible for travel forecasts for the City of Menlo Park toward preparation of a transportation master plan. Kittelson applied the model developed for the ConnectMenlo General Plan to the CSA trip distribution assumptions update that the City uses for its traffic studies (previously developed by Kittelson staff). Kittelson conducted modeling to update the trip distribution percentages for Menlo Park. Kittelson summarized all AM and PM peak hour trips by Traffic Analysis Zone (TAZ) and city jurisdictions within Menlo Park, by city jurisdictions within San Mateo County, and by county jurisdictions outside of San Mateo. As part of a second task order, Kittelson prepared travel forecasts for the Bayfront grade separation and Dumbarton Rail scenarios using assumptions from the Dumbarton Transportation Corridor Study.

MENLO PARK VMT MODEL UPDATE, CITY OF MENLO PARK, CALIFORNIA

Kittelson provided VMT services on a citywide travel model update for the City of Menlo Park. The travel model is based on the 2015 C/CAG countywide model. Per SB 743, the City had recently contracted to CHS Consulting Group (CHS) to update the City guidelines for VMT. The City asked Kittelson to update and recalibrate the model to improve the prediction of VMTs and other metrics for development projects. Kittelson provided results to City and CHS staff that helped to realign the City's VMT metrics for their transportation impact analysis (TIA) and VMT guidelines.

COMMONWEALTH BUILDING 3 PROJECT, CITY OF MENLO PARK, CALIFORNIA

Kittelson prepared a traffic and transportation analysis for the CEQA environmental review of a proposed expansion of the City of Menlo Park's Commonwealth Corporate Center. The project will add a third office building encompassing 320,000 sq. ft. The project site is in the in the Bayfront area east of U.S. Route (US) 101, between Commonwealth Drive and Jefferson Drive. Kittelson has prepared a TIA that will serve as the transportation section of the environmental document. Kittelson used the ConnectMenlo model to evaluate up to 31 intersections for existing, near-term, and cumulative conditions. Outputs of the analysis also included VMT for SB 743 and GHG analysis.

MENLO PARK 141 JEFFERSON UPTOWN PROJECT, CITY OF MENLO PARK, CALIFORNIA

Kittelsohn prepared a traffic and transportation analysis for an EIR analyzing the proposed redevelopment of three parcels with 483 multi-family dwelling units comprised of 42 for-sale condominium units and 441 rental units on a 4.83-acre site near the Facebook campus in the Bayfront area. The three project parcels are at 141 Jefferson Drive and 180-186 Constitution Drive in the R-MU-B (Residential Mixed Use-Bonus) zoning district and are named Menlo Uptown. Kittelsohn used the ConnectMenlo model to evaluate up to 25 intersections for existing, near-term, and cumulative conditions. Outputs of the analysis also included VMT for SB 743 and GHG analysis.

C. DETAILED STATEMENTS

PROJECT APPROACH

PROJECT UNDERSTANDING

The City has received an application from Mark Moragne of R&M Properties for a development comprising 85,886 square feet of research and development (R&D)/life sciences office space and 5,822 square feet of commercial (retail) space. The project site includes four contiguous properties addressed as 980-990, 1010, 1020 and 1030 O'Brien Drive. There are four existing buildings across the four parcels. The 980-1030 O'Brien Project (project) would demolish two existing, one-story commercial buildings located at 1020 and 1030 O'Brien Drive, merge these two lots into a new parcel, and construct one new three-story building for R&D and commercial uses on the newly merged parcel at 1030 O'Brien Drive. The two other existing commercial buildings at 980 and 1010 O'Brien Drive would remain.

The project size is within the maximum amount of new R&D development potential identified in ConnectMenlo. The Land Use Element identifies the potential for 2,100,000 square feet of net new R&D, 1,285,000 square feet of office, and 275,000 square feet of commercial space in the Bayfront Area in an area historically developed with industrial, warehousing, and office uses north of US 101 and south of State Route (SR) 84 (Bayfront Expressway), adjacent to the San Francisco Bay. This project, in combination with all other previously submitted life sciences projects since ConnectMenlo was approved, equals 538,568 square feet of the total net new R&D space, 534,622 square feet of the total net new office, and 236,369 square feet of the total net new commercial development studied under the ConnectMenlo EIR for the Bayfront Area.

SUGGESTED MODIFICATIONS TO THE PROJECT TASKS

SWCA does not have any suggested modifications to the project tasks. However, please note the tasks presented below have been consolidated to avoid repetition and to reflect the CEQA process.

PROJECT WORK PLAN

SWCA developed the following approach and scope of work for the project based on our understanding of the project, review of the City's request for proposal (RFP), conversations with City staff, and experience with projects of a similar type and scale within the Bay Area. We understand the proposed project will require environmental review pursuant to CEQA, and the City will serve as the CEQA Lead Agency. The analysis will be tiered off the ConnectMenlo EIR, certified by the City Council in 2016.

Based on our email communication received from City Planner Fahteen N. Khan on December 12, 2022, and subsequent conversation, our revised proposal includes preparation of either a notice of exemption (NOE) (CEQA Guidelines Section 15332) or an IS/MND (CEQA Guidelines Section 15070). The potential impact significance

concluded in the Traffic Impact Analysis will help guide the City's determination of which CEQA document to prepare. The Class 32 exemption is not available for any project that requires mitigation measures to reduce potential environmental impacts to less than significant.

The project work plan (described by task below) provides an overview of the preparation of either the NOE or IS/MND. In addition, SWCA will provide evidence and technical background studies to support the determination that a categorical exemption applies, or the analysis included in the IS/MND. Personnel time allotted for each task is included in Table 5 and Table 6.

TASKS

TASK A. PROJECT KICK-OFF MEETING AND SITE VISIT

Upon receipt of notice to proceed, SWCA Project Manager Kara Laurenson-Wright will initiate the environmental review process and mobilize technical staff. As requested in the RFP, Kara Laurenson-Wright, Project Director Julie Barlow, and Kittelson Project Manager Amanda Leahy will attend an in-person project kick-off meeting (not to exceed 1 hour in duration) with representatives from the City and the project applicant (if appropriate). Before the kick-off meeting, SWCA will prepare and circulate a meeting agenda and project schedule.

The purposes of the kick-off meeting are:

- 1) To review assumptions in the scope of work;
- 2) To discuss the project schedule, timing of the technical studies, and important assumptions for achieving the schedule;
- 3) To identify all anticipated discretionary actions;
- 4) To establish early communication among various project team members and the protocols for ongoing communication;
- 5) To identify the preferred document format and compilation process; and
- 6) To familiarize the SWCA team with the issues and concerns that the City determines to be important issues for analysis to be included in the environmental document.

The kick-off meeting will be directly followed by a site visit. SWCA will use the site visit to verify existing conditions and photo-document the project site. After the kick-off meeting and site visit, SWCA will distribute written notes, a list of action items, and a finalized project schedule incorporating any changes or adjustments discussed at the meeting.

Assumptions

- SWCA's attendance at an in-person kick-off meeting and site visit will include Project Director Julie Barlow, Project Manager Kara Laurenson-Wright, and Kittelson Project Manager Amanda Leahy.
- All in-person agency and public meetings are proposed as requested in the RFP. However, because of the current worldwide COVID-19 pandemic, please note SWCA has a COVID safety plan (available upon request) and best practices intended to protect SWCA employees, clients, agencies, and the public. We will work with the City to identify appropriate safety considerations if in-person meetings are required soon.
- If there is a need to move away from in-person meetings to virtual meetings (either agency/internal or public/external), we will revisit our costs.

Deliverables

- Kick-off meeting agenda, project schedule, and meeting notes.

TASK B. REVIEW OF CITY DOCUMENTS AND DATA COLLECTION

Task B.1 Data Request

After reviewing the project site plans, ConnectMenlo General Plan and EIR, and other relevant supporting materials provided by the City and/or project applicant, SWCA and Kittelson will prepare a comprehensive data request. This data request will support preparation of the project description ([Task B.2](#)) and completion of the technical studies ([Task E](#)).

Deliverables

- One data request (submitted electronically in Microsoft Word format).

Task B.2 Project Description

Upon receipt of responses to the data request, SWCA will prepare a preliminary project description for review and confirmation by the City and project applicant. The project description will document the existing land uses and condition of the project site, provide a description of the project components, and identify general construction logistics and schedule. The final project description will establish the basis of project understanding for the technical studies ([Task E](#)) and preparation of the NOE ([Task Option C.1](#)) or IS/MND ([Task Option C.2](#)).

Assumptions

- The City will submit consolidated, non-conflicting comments on the preliminary project description and incorporate the final project description into the CEQA document.
- Once SWCA initiates preparation of the CEQA document, no substantive changes to the project description will occur.

Deliverables

- Preliminary project description for City and project applicant review (submitted electronically in Microsoft Word format).

TASK C. CEQA DOCUMENT

At the City's direction, SWCA will proceed with preparation of either an NOE ([Task Option C.1](#)) or an IS/MND ([Task Option C.2](#)). Both Task Option C.1 and [Task Option C.2](#) include the preparation of technical studies ([Task E](#)).

Task Option C.1 Notice of Exemption

Based on the conclusions and potential impact significance determined in the TIA, the City may determine the project is categorically exempt from CEQA, pursuant to State CEQA Guidelines Section 15061. If so, SWCA will prepare an NOE pursuant to State CEQA Guidelines Section 15062. The NOE will be based on Appendix E of the CEQA guidelines in a format approved by the City. The project description ([Task B.2](#)) will establish the basis of project understanding for the technical studies ([Task E](#)). The NOE will include the following information developed under Tasks A and B:

- A description of the project;
- The location of the project;
- A finding that the project is exempt from CEQA, including a citation to the Class 32 "Infill" categorical exemption (CEQA Guideline Section 15332) or other State guidelines section or statute under which it is found to be exempt, and;
- A statement of reasons to support the finding, and;

- The administrative record with documentation supporting the statement of reasons, which is assumed to include the TIA prepared by Kittelson and air quality and GHG emissions technical report, cultural resources technical report, and noise report prepared by SWCA.

The NOE will be supported by the technical studies outlined in [Task E](#), including the TIA, Air Quality and Greenhouse Gas Emissions Technical Report, Cultural Resources Technical Report, and Noise Report. These technical studies will support the Class 32 statement "...approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality." However, SWCA can revisit the inclusion of these technical studies appended to the NOE if requested by the City.

SWCA will respond to one set of consolidated comments and finalize the NOE. SWCA will coordinate with the City to register with the State Office of Planning and Research (OPR) as the City's authorized representative and will upload the NOE electronically. We assume the City will file the NOE with the County of San Mateo Clerk-Recorder's Office and pay any associated environmental filing fees upon project approval.

Assumptions

- *The City will submit consolidated, non-conflicting comments on the NOE.*
- *Once SWCA initiates preparation of the NOE, no substantive changes to the project description will occur.*

Deliverables

- *Administrative draft and final NOE (submitted electronically in Microsoft Word and Adobe PDF formats).*

Task Option C.2 Preparation of Initial Study Documents

Based on the conclusions and potential impact significance determined in the TIA, the City may determine the project is not categorically exempt from CEQA. In that case, SWCA will prepare an IS/MND pursuant to State CEQA Guidelines Section 15063 and 15070.

Task Option C.2.1 Administrative Draft Initial Study/Mitigated Negative Declaration

Upon finalization of the project description in ([Task B.2](#)), SWCA will prepare an administrative draft IS/MND that meets all requirements set forth in CEQA (Public Resources Code [PRC] Section 21000 et seq.) and the State CEQA Guidelines (California Code of Regulations 15000 et seq.) Title 14, including an evaluation of all issues identified in the CEQA Guidelines Appendix G environmental checklist. The analysis will consider all information provided by the City and project applicant team, including the permit application materials, site plans, technical reports, and other supporting documents and information provided by the City. Project-specific and cumulative analyses will be based on standard methodologies and techniques and focus on the net changes anticipated at the project site.

SWCA will summarize the proposed technical studies ([Task E](#)) and incorporate the information into the appropriate environmental resource topic in the IS/MND. Kittelson will prepare the transportation section of the IS under the direction of SWCA. SWCA assumes City staff will provide the list of cumulative development projects.

Aesthetics

SWCA will evaluate the change in land uses, visual character, and views of the project site associated with the proposed redevelopment. Information referenced to evaluate visual effects of the project will include the site visit ([Task A](#)) and photo documentation of existing conditions; proposed site plans and design elements; and information from ConnectMenlo, the Menlo Park Municipal Code, and development standards applicable to the project. Lighting and glare effects will also be addressed. The significance of visual changes will be based, to the extent feasible, on conformance with the City's policies and regulations that pertain to visual character, light, and design.

Agricultural and Forestry Resources

SWCA will cite the California Department of Conservation Farmland Mapping and Monitoring Program, ConnectMenlo, and the General Plan Land Use and Zoning Map to support a determination that no agricultural land or forest resources would be affected by the project.

Air Quality

SWCA will prepare the air quality section based on the results of the air quality and GHG technical report, as described in [Task E.1](#). Local and regional climate, meteorology, and topography, as they affect the accumulation or dispersal of air pollutants will be presented, and current air quality conditions and recent trends in the San Francisco Bay Area Air Basin and project area, will be described based on the California Air Resources Board and the U.S. Environmental Protection Agency annual air quality monitoring data summaries. Federal, state, and local regulatory agencies responsible for air quality management will be identified, and applicable federal, state, and local air quality policies, regulations, and standards will be summarized. The impact analysis will be based on the significance thresholds in Appendix G of the CEQA Guidelines and the BAAQMD emissions-based thresholds. The net increase in operational emissions (i.e., project minus existing) will be compared with the significance thresholds established by BAAQMD.

Biological Resources

SWCA will conduct a literature review to identify known records of special-status plant and animal species and other sensitive biological resources in the project vicinity. The literature review will include a search of the CDFW California Natural Diversity Database, U.S. Fish and Wildlife Service (USFWS) Information for Planning and Consultation (IPaC) database, and the California Native Plant Society's On-line Inventory of Rare and Endangered Plants. SWCA will also review the U.S. Geological Survey (USGS) topographic quadrangle maps, aerial imagery, vegetation, and land use mapping, USFWS National Wetland Inventory (NWI) data, surface water data, and Natural Resources Conservation Service Soil Surveys.

The project site is paved, developed with existing buildings, and surrounded by urban development that provides limited habitat for special-status species. While there is limited vegetation on-site, there are street trees adjacent to the project site. Certain street trees are considered protected and require special permits for removal. These trees could support habitat for migratory nesting birds protected under the California Fish and Game Code or the Migratory Bird Treaty Act. SWCA will describe these biological issues, identify standard requirements and mitigation measures, and evaluate the project's compliance with the Tree Protection Specifications, Street Tree Ordinance (Municipal Code Chapter 13.20), and the Heritage Tree Ordinance (Municipal Code Chapter 13.24).

A staff biologist will conduct a half-day pedestrian survey to verify information identified in the literature review. Therefore, this section will be based on the pedestrian survey, the literature review, and the ConnectMenlo EIR, as well as landscaping, tree protection, and other information provided by the project applicant team.

Cultural Resources

SWCA will prepare the cultural resources section and the tribal cultural resources section of the IS/MND based on the cultural resources technical report, as described in [Task E.2](#), which will document the results of the records searches, Native American outreach, and historic resources evaluation.

Energy

SWCA will prepare the energy section through an analysis of the project's consumption of energy resources, particularly electricity, natural gas, and transportation fuels, during both the project's construction and operational phases. Energy consumption associated with project construction will result primarily from transportation fuels (e.g., diesel and gasoline) associated with construction vehicles and equipment and worker commuting. Energy use associated with short-term construction activities will be discussed qualitatively based on the CalEEMod computer program and on-road mobile-source fuel usage rates derived from the EMFAC computer program. SWCA will extract

the following data from CalEEMod output for the air quality analysis and confirm all assumptions: electricity usage, natural gas usage, and mobile fuel usage (both gasoline and diesel).

The energy use associated with long-term operational activities will also be discussed qualitatively. Any potential electricity generated by the project will be identified and discussed. SWCA's analysis will account for project features that would avoid or reduce the inefficient, wasteful, and unnecessary consumption of energy, such as building design and energy efficiency features. SWCA will also evaluate the project's consistency with state and local energy efficiency goals, applicable regulatory framework pertaining to energy conservation, and energy suppliers for the project areas. Energy use and conservation associated with short-term construction and long-term operational activities will be evaluated.

Geology/Soils

SWCA will prepare this section based on information from the ConnectMenlo Final EIR; the Open Space/Conservation, Noise and Safety Element; published geologic maps and reports from the California Geological Survey and USGS; and the preliminary geotechnical investigation provided by the project applicant. The potential impacts related to geologic and seismic conditions will be identified, including ground-shaking and liquefaction potential from earthquakes, potential seismic-induced ground failure, slope stability, expansive soils, subsidence, and erosion, with respect to implementation of the project.

Greenhouse Gas Emissions

SWCA will prepare the GHG emissions section based on the results of GHG emissions modeling as described in [Task E.1](#). The GHG emissions assessment will include a brief description of global climate change and a summary of key applicable regulatory measures. The net increase in GHG emissions (i.e., project minus existing emissions) will be presented in the GHG section, and details of the analysis (e.g., annual GHG emission calculations) will be included in an appendix.

Hazards and Hazardous Materials

SWCA will evaluate potential impacts from current and past hazardous materials/waste storage and/or use and will identify potential environmental concerns related to construction and operation of the project. Potential impacts will be assessed through review of federal, state, and local regulatory agency records per Government Code Section 65962.5, the 2021 County of San Mateo Multijurisdictional Local Hazard Mitigation Plan, the ConnectMenlo final EIR, and the Phase I environmental site assessment (ESA) provided by the applicant.

Hydrology/Water Quality

SWCA will prepare this section based on information from the ConnectMenlo final EIR, published maps and reports by the California Department of Water Resources, USGS, and Federal Emergency Management Agency. SWCA notes the project is with the flood hazard Zone AE on Flood Insurance Rate Map 06081C0307F. SWCA will examine the potential impacts of the project on hydrology and water quality, including analysis of the stormwater system and potential flooding impacts.

Land Use/Planning

SWCA will evaluate consistency with applicable City General Plan policies, life sciences district zoning requirements, and R&D potential identified in ConnectMenlo. The project will demolish two buildings with commercial uses and build a new building with 85,886 square feet of R&D/office space and 5,822 square feet of commercial space. This section will also consider the project's compatibility with adjacent existing development, roadways, and public utilities. This section will analyze whether the proposed changes in land use and zoning designations will adversely affect the land use element development potential and the City's long-range land use planning goals.

Mineral Resources

SWCA will cite the California Department of Conservation Farmland Mapping and Monitoring Program and ConnectMenlo EIR to support a determination that no known mineral resources will be affected by the project.

Noise

SWCA will summarize the analysis in the noise report ([Task E.3](#)) for the noise and vibration section. This section will describe existing noise and vibration levels and sources in the project vicinity, identify nearby sensitive receptors, and discuss the noise and vibration impacts to these sensitive receptors from the project's construction and operation.

Population/Housing

The project will not substantially change the population and housing conditions in the city, as it will not displace existing housing or induce substantial population growth. The project will demolish existing commercial uses and construct a building with R&D and commercial uses. However, SWCA will analyze the potential for population growth related to employment opportunities.

Public Services

SWCA will describe the existing public services and facilities (e.g., police protection, fire protection, emergency medical services, libraries, and schools) that serve the project area. Contact will be made with each of the service providers to determine system capacity and to estimate the potential impact of the project.

Recreation

SWCA will identify recreational facilities in the vicinity of the project and the potential impact of the project.

Transportation

SWCA's transportation subconsultant, Kittelson, will prepare the transportation section based on the TIA prepared in Task E.4, with support from SWCA. This section will identify existing traffic conditions and traffic generated by the project and will provide an analysis of estimated impacts to local area circulation and transportation resulting from the project based on consideration of VMT and non-passenger-vehicle modes of transportation.

Tribal Cultural Resources

SWCA will assist the City with Tribal consultation, as outlined in [Task E.2](#). The section will summarize the Assembly Bill (AB) 52 outreach process in the IS/MND.

Utilities Service Systems

SWCA will identify the existing utilities and service systems (e.g., water supply, wastewater treatment, storm drainage, solid waste collection) that serve the project site. SWCA will describe consideration of the project's possible sources of water supply over the life of the project and the environmental impacts of supplying water and wastewater services to the project. Service providers will be contacted to identify capacity and confirm the providers' ability to serve the project.

Wildfire

SWCA will cite the fire hazard severity zones developed by the California Department of Forestry and Fire Protection and ConnectMenlo EIR to analyze any potential wildfire risk.

Mandatory Findings of Significance

SWCA will discuss the project's potential to degrade the quality of the environment; substantially reduce the habitat of a fish or wildlife species; cause a fish or wildlife population to drop below self-sustaining levels; threaten to eliminate a plant or animal community; reduce the number or restrict the range of a rare or endangered plant or animal; eliminate important examples of the major periods of California history or prehistory; have impacts that would be individually limited but cumulatively considerable; or have environmental effects that would cause substantial adverse effects on human beings, either directly or indirectly.

Assumptions

- City staff will provide a list of cumulative development projects.

- The project applicant will provide copies of the preliminary geotechnical investigation, stormwater and hydrology reports, arborist report, and Phase I ESA.
- It is assumed that no further technical background studies will be necessary, including but not limited to, visual simulations, a quantitative hydrology analysis, and water supply assessment. If further studies are found to be required, they would be performed under a separate scope and budget.

Deliverables

- Administrative Draft IS/MND (submitted electronically in Microsoft Word and PDF formats).

Task Option C.2.2 Public Review Draft IS/MND and Mitigation and Monitoring Reporting Program

SWCA will respond to consolidated comments on the Administrative Draft IS/MND from the City and project applicant (if authorized by the City) and prepare the public review draft IS/MND, notice of intent (NOI), notice of completion (NOC), and summary form for the State Clearinghouse for public distribution. SWCA will provide the City with one electronic copy (in PDF format) of the draft IS/MND and up to 15 paper copies.

SWCA will coordinate with the City to become registered to submit CEQA documents to the State Clearinghouse portal on behalf of the City of Menlo Park, in accordance with the OPR guidelines (https://opr.ca.gov/docs/20210405-CEQA_Submit_Registration_Form.pdf). SWCA will electronically submit the public review Ddraft IS/MND, NOI, NOC, and summary form to the State Clearinghouse portal. SWCA assumes the City will be responsible for necessary noticing and public circulation tasks, such as the newspaper advertisement listing, mailing public hearing notices, and posting of the appropriate notice at the County of San Mateo Clerk-Recorder's Office.

SWCA will prepare an MMRP pursuant to CEQA Guidelines Section 15097. The MMRP will be checked against the City's ConnectMenlo MMRP, and any additional mitigation measures identified in the IS/MND. The MMRP will identify responsibility for implementing and monitoring each mitigation measure, along with monitoring triggers and reporting frequency, which will be linked with existing processes of project design, development, and review.

Assumptions

- *The City will submit consolidated, non-conflicting comments on the Administrative Draft IS/MND.*
- *The City will be responsible for the public distribution list and necessary noticing, public circulation tasks, and posting of the IS/MND at the County of San Mateo Clerk-Recorder's Office.*
- *SWCA will upload the IS/MND, NOI, NOC, and summary form to the State Clearinghouse portal.*
- *No public meeting will occur during the public comment period.*
- *State and federal regulatory agency coordination will not be necessary for completion of this scope of work.*

Deliverables

- *Public review draft IS/MND, MMRP, NOI, summary form for State Clearinghouse, and the NOC (submitted electronically in Microsoft Word and PDF formats).*
- *Fifteen paper copies of the public review draft IS/MND.*

Task Option C.2.3 Administrative Draft Final IS/MND and Responses to Comments

After the close of the 30-day public comment period on the public review draft IS/MND, SWCA will prepare responses to any substantive comment letters (up to 10 comment letters) received during public circulation. Responses will be in the form of a responses-to-comments memorandum. We assume major revisions to the public review draft IS/MND will not be required. Changes important to the analysis of content of the administrative final IS/MND will be shown in track changes. SWCA will also prepare a draft Notice of Determination (NOD) for the City's review.

Assumptions

- *The City will submit consolidated, non-conflicting comments on the administrative draft IS/MND.*
- *Public comments received on the administrative draft IS/MND will be minor in nature and not require significant additional analysis or major revisions to the public review draft IS/MND. The maximum number of substantive comment letters received will not be more than 10, resulting in 50 hours of staff time to develop responses.*

Deliverables

- *Administrative draft IS/MND and responses to comments, draft MMRP, and draft NOD (submitted electronically in Microsoft Word and PDF formats).*

Task Option C.2.4 Final IS/MND and Responses to Comments

As with the public review draft IS/MND, SWCA will incorporate all City comments and edits on the administrative draft final IS/MND and responses to comments, MMRP, and NOD. SWCA will provide the City with one electronic copy (in PDF format) of the final IS/MND, MMRP, and NOD and up to 10 paper copies. SWCA assumes that the City will be responsible for distribution of the final IS/MND document, although SWCA is available to assist with these efforts upon request of the City.

SWCA will upload the NOD to the State Clearinghouse portal. SWCA assumes the City will file the NOD at the County of San Mateo Clerk-Recorder's Office upon final project determination and the project applicant will pay the associated CDFW MND filing fee (\$2,764.00) and County handling fee (\$50.00).

Alternatively, if the IS/MND determines there are no impacts to biological resources, SWCA can work with the City to prepare a No Effect Determination (NED). An NED is submitted to CDFW for the purpose of determining environmental document filing fees and is not part of the assessment a lead agency makes under CEQA (<https://wildlife.ca.gov/Conservation/Environmental-Review/CEQA/NED#56216942-ned-eligibility>).

Assumptions

- *The City will file the NOD with the County of San Mateo Clerk-Recorder's Office.*
- *SWCA will upload the IS/MND, NOD, and summary form to the Clearinghouse Portal.*
- *The City or project applicant will be responsible for payment of the CDFW filing fees (\$2,764.00).*

Deliverables

- *Final IS/MND and responses to comments, MMRP, and NOD (submitted electronically in Microsoft Word and PDF formats).*
- *Ten paper copies of the final draft IS/MND, MMRP, and NOD.*

TASK D. PROJECT MANAGEMENT

SWCA prioritizes project management and believes a focused, well-managed effort on the part of the SWCA team will be key to achieving the City's processing goals for the project. SWCA Project Manager Kara Laurenson-Wright, supported by Ms. Julie Barlow, will be the day-to-day contact for the City and will be responsible for and oversee all aspects of environmental analysis, including providing quality assurance/quality control (QA/QC) for work products and communicating and coordinating with City staff by telephone and email, with a goal of responding to emails within 24 hours. Ms. Laurenson-Wright will also actively engage with all SWCA and Kittelson team members to ensure all parties have consistent project information, are meeting project milestones, and are working within the agreed-upon scope of work and budget. This coordination will ensure the project remains on schedule, concerns that require attention are promptly communicated, and efficient and practical solutions can be developed for avoiding and/or mitigating impacts to environmental resources.

Task D includes participation for up to two SWCA team members in up to six regular project-status conference calls throughout the duration of the environmental review period. Kittelson team members will be available to attend up to three of these conference calls. These meetings would be no more than 1 hour in length to discuss technical strategy, approach, analysis results, and mitigation. SWCA will prepare meeting agendas and notes, as necessary. Attendance at the City Planning Commission hearing is included separately under Task G.

As outlined in [Task M](#), our scope of work also includes invoice and contract oversight by our project controls team to ensure accurate and timely invoice and contract coordination.

TASK E. TECHNICAL STUDIES

Task E involves the preparation of technical background studies and supporting documentation. Initiation of the technical background studies will not commence until after SWCA receives the responses to the data request and the City confirms the project description ([Task C.1](#)). The technical background studies and supporting documentation needed to prepare the NOE or IS/MND are described in [Tasks E.1](#), [Task E.2](#) and [Task E.3](#).

Task E.1 Air Quality and Greenhouse Gas Emissions Technical Report

SWCA will prepare an air quality and GHG technical report to characterize the existing ambient air quality conditions and identify applicable regulations that would apply to construction and long-term operation of the proposed project. All development within the San Francisco Bay Air Basin, which is under the jurisdiction of the BAAQMD, must comply with the standards and regulatory requirements of the BAAQMD.

The air quality and GHG technical report will evaluate direct and indirect impacts from the regulated pollutants (e.g., criteria air pollutants and GHGs) that are expected to occur during both project construction and operations and will summarize the results of the analysis. SWCA's in-house experts will use the latest version of CalEEMod (version 2022.1) to quantify mass emissions of criteria pollutants from the project. SWCA will use the data provided by the client regarding the construction schedule and equipment and operational details, including the project's net increase in vehicle trips or vehicle miles traveled. Based on current project details regarding operations and the potential land uses, CalEEMod defaults will be used to quantify mass emissions of criteria pollutants from the operations phase of the project and will be compared against the BAAQMD criteria pollutant operational thresholds. Operational GHG emissions will also be quantified; however, significance determination will be based on the April 2022 BAAQMD updated thresholds of significance for climate impacts.

The project criteria pollutant emission rates will be compared with the BAAQMD Thresholds of Significance for Air Quality Impact Analysis for construction and operation. SWCA assumes that this methodology will be sufficient to demonstrate that impacts to ambient air quality standards are less than significant. Based on the proposed development, SWCA assumes that the proposed project will be below the BAAQMD significance thresholds. It is assumed that the project will not require a health risk assessment to address the sensitive receptors. However, if BAAQMD determines a health risk assessment is necessary, a scope of work can be provided at that time.

The air quality and GHG emissions technical report will have one round of consolidated comments by the City.

Assumptions

- *All equipment specifications (e.g., vehicle, construction, electrical, etc.) will be provided by the project applicant.*
- *Construction vehicle and equipment rosters, traffic volumes, and work schedules will be provided by the project applicant.*
- *Size, rating, number, and manufacturer specifications (e.g., air pollutant emissions data) of the regulated air pollutant emitting equipment at the project site will be provided to SWCA.*

- *A total of two CalEEMod modeling runs has been assumed.*
- *Dispersion modeling and/or hotspots analysis will not be required.*
- *A health risk assessment will not be required.*
- *The project applicant will provide information sufficient to make a determination that the project will be consistent with California's long-term climate goal of achieving carbon neutrality by 2045. If the project identifies the use of natural gas within any new building, the BAAQMD GHG impacts will be significant and unavoidable. In this case, the client will provide details to analyze the estimated natural gas GHGs, on-site renewable energy to offset emissions of the estimated natural gas GHGs, and off-site renewable energy.*

Deliverables

- *Administrative draft and final air quality and GHG technical report (submitted electronically in Microsoft Word and Adobe PDF formats).*

Task E.2 Cultural Resources Technical Report/Assembly Bill 52 Tribal Consultation

SWCA archaeologists and architectural historians will conduct the necessary studies to identify and assess impacts to historical resources. The project site has four historic age buildings built before 1972. As such, we have outlined the following tasks as part of the CEQA compliance.

Task E.2.1 Cultural Resources Records Search and Archival Research

SWCA will prepare a records search map for the project, which will include the project area and a buffer encompassing a 0.25-mile search radius. SWCA will conduct a confidential California Historical Resources Information Center (CHRIS) records search at the Northwest Information Center (NWIC) for the project area and a 0.25-mile search radius. In addition to the cultural resources inventory records and reports, an examination will be made of historic maps, determinations of eligibility listings, the Office of Historic Preservation–prepared historic property data file (Data File), Built Environment Resources Database (BERD), and any previously conducted cultural studies within the project site. The Data File contains listings for the NRHP, California Register of Historical Resources (CRHR), State Historical Landmarks, local eligibility listings, and California Points of Historical Interest.

An SWCA architectural historian will conduct focused archival research into the development and operation of the four industrial buildings within the project site. Research will include building permits and local and online repositories.

Task E.2.2 Native American Coordination

SWCA will contact the Native American Heritage Commission (NAHC) to request a sacred lands file (SLF) search. The SLF search will identify the presence of Native American cultural sites, traditional places, or landscapes. Additionally, the NAHC will provide a list of Native American contacts who may have unique knowledge of such resources. SWCA will prepare and mail letters to each of the Tribal contacts, requesting information in writing concerning Native American religious or cultural resources within or immediately adjacent to the project area. Up to two calls will be made to each of the Native American groups on the NAHC list to document good-faith efforts to follow up. This is not considered AB 52 consultation; it is assumed that all official AB 52 consultation will be handled by the lead agency.

Task E.2.3 Assembly Bill 52 Coordination

PRC Section 21080.3.1, as amended by AB 52, requires the lead CEQA agency to conduct outreach to Native American groups and to consider potential impacts to tribal cultural resources. SWCA will assist the City with Tribal consultation to satisfy AB 52 requirements. SWCA will then provide the City with draft letters, checklists, and detailed instructions to ensure the City can complete meaningful consultation with interested Native American groups pursuant to AB 52. SWCA will respond to one round of comments from the City to finalize the letters. It is assumed that the City will send the letters to all tribes and conduct all follow-up communication. SWCA assumes no in-person meetings or

other consultation activities are required. Should additional meetings or consultation be requested, SWCA can provide support by coordinating and attending meetings and recommending an outreach and consultation strategy; however, this scope of work does not assume that level of coordination will be necessary.

Task E.2.4. DPR 523 Forms

An SWCA architectural historian, who meets the qualifications as Architectural Historian and Historian under the Secretary of the Interior's Professional Qualifications Standards (36 Code of Federal Regulations 61) will lead the built environment inventory of the property. The built environment inventory will include written field documentation and photographs of primary exterior physical components and contextual relationships. SWCA will prepare four Department of Parks and Recreation (DPR) 523 forms, one for each building. Each DPR 523 form will include written description and photographic documentation of the properties, development of specific historic contexts, and evaluation using the CRHR.

Task E.2.5. Cultural Resources Report

Upon completion of the field survey, SWCA will prepare a survey report memorandum, which will document the results of the records searches, a buried site sensitivity analysis, Native American outreach, field survey, and the results of the built resource evaluation. Locations of sensitive archaeological sites or tribal cultural resources may be depicted or described in the report and will be considered confidential, in which case the report may not be distributed to the public. Following response to one round of comments, SWCA will finalize the memorandum for digital submission.

Assumptions

- *The cultural resources report will have two rounds of consolidated comments by the City before digital submission to the NWIC as required.*
- *The field survey will take one staff member no longer than one 8-hour day to complete.*
- *Up to four built resources will be recorded during the survey effort.*
- *No archaeological resources will be present in the project site in either the records search results or during the archaeological survey.*
- *Any previously unrecorded archaeological resources identified during the survey will be recorded on DPR 523 forms. No testing or excavation will be conducted, nor will any artifacts, samples, or specimens be collected during the survey.*
- *The NWIC will complete the records search requests on a first-come/first-served basis. Fees are based on a standard, non-expedited rate and will not exceed \$1,000.00. Charges above this limit may require a change order. Records search results typically take between 4 and 6 weeks.*
- *No in-person meetings or other consultation activities will be required for AB 52 coordination.*
- *This scope of work does not include an outreach and consultation strategy under AB 52.*
- *SWCA proposes to provide up to 10 hours of support for AB 52 work.*
- *No federal nexus will be required.*

Deliverables

- *Administrative draft and final cultural resources technical report (submitted electronically in Microsoft Word and Adobe PDF formats)*

Task E.3 Noise Report

SWCA will evaluate the potential for the project to impact adjacent sensitive receptor uses as a result of construction, increased traffic, and operational noise. SWCA will describe background noise conditions and regulatory requirements

and evaluate the effect of project-related increases on ambient noise levels for the construction and operational phases in the context of applicable noise standards.

Key noise issues to be addressed will include:

- Exposure of existing noise-sensitive land uses to noise and vibration associated with construction activity
- Exposure of existing noise-sensitive land uses to project-related changes in traffic noise
- Exposure of existing noise-sensitive land uses to operational noise from the project site

Background noise level conditions at the project site will be taken from the existing General Plan and/or evaluated based on land use. The American National Standards Institute (ANSI) has published a standard (ANSI/ASA S12.9-1993/Part 3) with estimates of general ambient noise levels (average noise level and day-night average noise level) based on detailed descriptions of land use categories. It is assumed that no on-site noise monitoring will be required to complete the analysis.

The significance of noise impacts will be assessed based on predicted noise exposures and the defined CEQA significance thresholds.

For construction and operational noise, SWCA will use information provided by the client, to the extent available. Predicted construction-generated noise levels at nearby receptors will be calculated using the Federal Highway Administration (FHWA) Roadway Construction Noise Model (RCNM). The RCNM is FHWA's national model for the prediction of construction noise. This software is based on actual sound level measurements from various equipment types taken during the Central Artery/Tunnel project conducted in Boston, Massachusetts, during the early 1990s. The maximum noise levels presented at a specified distance from the source are based on a roster of likely construction equipment operating. Although the project is not a road construction project, the RCNM includes the same types of equipment that will be used in the construction of the project. It is assumed that the required information regarding the construction roster and schedule, as well as detailed information about traffic volumes at nearby roadways and intersections, will be provided.

Operational traffic noise will be evaluated for roadway segments near the project site using the FHWA Traffic Noise Model (TNM) and traffic data to be provided by the transportation subconsultant, Kittelson. It is assumed that traffic data will include average annual daily traffic volumes, posted speeds, and heavy truck percentages for each roadway segment to be analyzed. Traffic noise will be evaluated in terms of how project-related traffic noise increases may affect existing noise-sensitive land uses. Non-traffic noise generated by building operations will be evaluated using standard acoustical modeling methods and operational data provided by the project sponsor (including, but not limited to, locations, models, and makes of noise-generating mechanical equipment). Where significant noise impacts are identified, mitigation to reduce impacts to a less-than-significant level (where feasible and reasonable) will be identified. Noise mitigation will be described at a level of detail appropriate for environmental review and not at a design level of detail.

Ground-borne vibration may be induced by the operation of construction equipment. The effects of ground-borne vibration may include perceptible movement of building floors, interference with vibration-sensitive instruments, rattling of windows, shaking of items on shelves or hanging on walls, and rumbling sounds. Determining vibration effects requires a comparison of predicted vibration levels with established criteria at a sensitive location, or at a distance from the vibration source at which a predicted level would just exceed the criteria.

SWCA will use the vibration criteria and methods presented in the Federal Transit Administration (FTA) guidelines published in the Transit Noise and Vibration Impact Assessment manual to assess potential impacts. The methodology presented in the FTA guidance employs reference vibration values for the construction or operation equipment and the distance to the vibration-sensitive receiver to determine projected peak particle velocities for

comparison with the impact criteria for architectural damage, or to determine projected vibration velocities for comparison with the human annoyance vibration thresholds.

SWCA will determine whether temporary noise or vibration associated with construction and operation will have a significant impact, as well as compare the future noise and vibration levels with applicable federal and regional (Caltrans) standards, in addition to City General Plan and municipal code standards.

The noise technical report will include the methodology and results of the analysis of project noise impacts.

Assumptions

- *This proposal assumes no baseline noise surveys will be required. Any request for a baseline noise survey can be provided under a separate scope and cost.*
- *A construction equipment roster that identifies the specific noise sources, including equipment type, rating, quantity, maximum hours of use per day, and total number of days of use, will be provided to SWCA. If SWCA is required to prepare equipment roster estimates from equipment associated with construction operations, data can be prepared as a separate scope and cost.*
- *An equipment roster for the operational phase and a plot plan of the project site that identifies the specific noise sources, including equipment type, sound-level data, rating, quantity, maximum hours of use per day, and total number of days of use, will be provided to SWCA. If SWCA is required to obtain noise data from equipment associated with the operation of the facility, the data collection can be provided under a separate scope and cost.*
- *The refined modeling noise impact assessment will be performed using TNM software.*
- *This scope assumes a maximum of two TNM modeling runs for the operational phase. No noise mitigation is assumed for this evaluation.*
- *The noise technical report will have one round of consolidated comments by the City before the findings will be incorporated in the IS/MND noise sections by SWCA planners.*

Deliverables

- *Administrative draft and final noise technical report (submitted electronically in Microsoft Word and Adobe PDF formats)*

Task E.4 Traffic Impact Analysis

Task E.4.1 Vehicle Miles Traveled Assessment

In accordance with SB-743, Kittelson will prepare a VMT assessment in accordance with City of Menlo Park VMT Guidelines. Kittelson will determine whether this project screens out for VMT or has a significant VMT impact. Kittelson will review the land uses in the model TAZ to ensure it adequately represents the project for VMT. If not, then Kittelson proposes to add the project into the 2020 model and rerun to extract more project applicable VMT results. These will be compared to the City/County average to determine if the project is impacted for VMT. Kittelson will document the VMT assessment in a technical memorandum to SWCA and the City for inclusion in the IS/MND transportation section.

Task E.4.2 Traffic Demand Model Mitigation

If the project is impacted for VMT, then Kittelson will determine the appropriate trip reduction needed and prepare Traffic Demand Model (TDM) measures that can mitigate the project to less than significant.

Task E.4.3 Local Traffic Impact Analysis

Project Description

Kittelson will use the project description prepared in [Task C.1](#). This section will include a brief description of the existing use on the project site, the current land use, and a summary of the project. A graphic representation of the project area and the planned location for the project will be provided.

Kittelson will obtain the following information from the City and/or SWCA during the data collection process as outlined in [Task B](#):

- Final project description with project site plan
- Additional information relevant to the project
- Recent traffic counts from the Adams Court and/or Willow Village EIR (from City) (pre-COVID, if available)
- Travel demand model files from the General Plan or more recent work conducted for Adams Court and/or Willow Village EIR
- Most current Menlo Park traffic analysis guidelines
- VISTRO model containing the study intersections and the existing AM and PM signal timings for the signalized study intersections from the Adams Court or Willow Village EIR.
- A list of projects (under construction, approved but not yet constructed, proposed) to be included in the near-term and cumulative scenarios. The information provided by the City should include trip generation, trip distribution and trip assignment information for these approved projects.
- A list of roadway system improvements associated with the developments to be included in each of the near-term and cumulative scenarios.
- The City's parking requirement for the various land use types.

Data Collection

Given COVID-19 conditions, Kittelson will rely on recent traffic counts available from other studies. Kittelson proposes to analyze up to 12 intersections within the city of Menlo Park (*Note: these may be adjusted or paired down during scoping meetings with the City*). The locations will be determined in consultation with City engineering staff. Most of the locations would likely have available pre-COVID counts that City will provide. Additional counts would require a cost amendment to collect.

Existing Conditions

Kittelson will document the existing traffic, transit, bicycle, and pedestrian components of the transportation system within the study area. Kittelson staff will conduct a field visit during the AM and PM peak periods on a typical weekday in the immediate study area to observe:

- Traffic patterns and circulation in the project vicinity
- Study intersection lane geometrics
- Traffic control
- Pedestrian circulation and facilities/amenities
- Bicycle circulation and facilities/amenities
- Proximity of public transit service
- Sight distance issues at study intersections
- Potential access issues

Roadway, Transit, Bicycle, Pedestrian

Kittelson will describe the existing roadway network, transit services, bicycle facilities, and pedestrian facilities in the study area. Kittelson will also prepare the following figures:

- Map of all study intersections illustrating existing counts, existing lane configurations and signal control.
- Map of transit services within the study area;
- Map of bicycle facilities in the study area; and

Intersections

Kittelson will determine and report the existing intersection level-of-service (LOS) conditions for up to twelve study intersections during the weekday AM and PM peak hours.

Study intersections will be analyzed using the VISTRO software package and the Highway Capacity Manual (HCM 2010 or HCM 6) Operations Methodology. It is assumed the City can provide the most updated Existing Year VISTRO model file as developed for the recent General Plan or from the Adams Court or Willow Village EIR's that includes the existing AM and PM signal timing information for all signalized study intersections.

The existing traffic volumes for all study intersections will be illustrated in a figure. The resultant LOS will be summarized in a table format, and to the extent relevant, they will be compared against the Existing LOS as reported in the ConnectMenlo General Plan (or more recent EIRs). For unsignalized intersections, the LOS will be reported for the worst approach movement. Signal warrant analysis will be performed for any unsignalized study intersections.

Routes of Regional Significance – CMP Segments

If the project generates more than 100 PM peak hour trips, the project will be subject to review by the County of San Mateo Congestion Management Program (CMP) and its requirements. As such, Kittelson will evaluate the routes of regional significance identified below.

Kittelson will perform segment-based LOS analysis during the weekday AM and PM peak hours for the following CMP locations. However, the analysis associated with the CMP facilities will not be required if the project does not generate more than 100 PM peak hour trips (to be determined):

Arterials:

1. SR)84 Bayfront Expressway
2. SR 109 University Avenue
3. SR 114 Willow Avenue

Freeways:

1. US 101, North of Marsh Road
2. US 101, north of Willow Road
3. US 101, north of University Avenue
4. US 101, south of University Avenue

Freeway Ramps:

1. US 101 ramps at Willow Road

Development of Near-Term Conditions

The near-term conditions (existing plus approved developments) will include traffic projections of all the approved but not yet constructed developments in the study area that the City will identify. Near-term conditions will also include selected roadway system improvements associated with the approved developments. The project site is assumed to remain as current conditions under the near-term conditions.

Kittelson will review whether the City's VISTRO model already includes development projects representing the near-term condition. If not, then Kittelson will need to update the City VISTRO model with a list of relevant near-term projects to be obtained from City staff.

Traffic projections for US 101 will be developed by adding traffic from the approved but not yet constructed developments to the existing traffic counts.

Intersections

Kittelson will determine the intersection LOS analysis for the 12 study intersections during weekday a.m. and p.m. peak hours for the near-term conditions using the same methodology as presented under the existing conditions. Kittelson will perform signal warrant analysis for any unsignalized study intersections.

CMP Segments

For the near-term conditions, Kittelson will perform the following analyses during the weekday AM and PM peak hours. However, the analysis associated with the CMP facilities will not be required if the project does not generate more than 100 PM peak hour trips (to be determined):

- Freeway mainline LOS analysis for the four study segments
- CMP arterial analysis for the three roadways and related segments
- Freeway ramps in the study area

Development of Cumulative No Project Conditions

The cumulative no project conditions will be represented by Year 2040 conditions which include traffic projections from all cumulative future development projects in the study area. The cumulative no project conditions will also include roadway system improvements as identified in the ConnectMenlo General Plan and/or the recent studies for Adams Court or Willow Village. The project site is assumed to remain as current conditions under the cumulative no project conditions. This scope assumes most information on cumulative development is already included in the ConnectMenlo VISTRO model to be provided by the City.

Traffic projections for US 101 through San Mateo and Menlo Park will be developed from freeway forecasts using the City model.

Intersections

Kittelson will determine the intersection LOS analysis for the 12 study intersections during weekday AM and PM peak hours for the cumulative no project conditions using the same methodology as presented under the existing conditions. Kittelson will perform signal warrant analysis for any unsignalized study intersections.

CMP Segments

For the near-term conditions, Kittelson will perform the following analyses during the weekday AM and PM peak hours. However, the analysis associated with the CMP facilities will not be required if the project does not generate more than 100 PM peak hour trips (to be determined):

- Freeway mainline LOS analysis for the four study segments
- CMP arterial analysis for the three roadways and related segments
- Freeway ramps in the study area

Trip Generation

Kittelson will compute project trip generation using the latest Institute of Transportation Engineers (ITE) Trip Generation Manual 11th Edition. Kittelson will use the most appropriate land use category for the project, most likely an R&D category.

ITE Trip Generation Manual

Kittelson will use published trip generation rates in the ITE Trip Generation Manual 11th Edition to determine the total trip generation for the project. This will be determined for the weekday daily, AM, and PM peak hours. Kittelson will review both the average rates and fitted regression curve rates and use the most conservative for the analysis.

Trip Generation Recommendations

Kittelson will prepare a trip generation memorandum, which summarizes the rates and total trips generated by the project for daily, AM, and PM peak hours. Kittelson will provide a recommended trip generation, including any TDM reductions for review by the City. Since the project does include multiple uses, Kittelson will compute appropriate internal trip and pass-by trips per ITE.

Trip Distribution, And Assignment

Kittelson will use the City model to obtain appropriate trip distribution percentages. The project trips will then be distributed and assigned through the study intersections based on the approved trip distribution percentages using the VISTRO model.

Project Operational Analysis

Traffic impacts or operational traffic deficiencies for the project will be identified using City of Menlo Park General Plan and traffic impact guidelines.

Intersections

Kittelson will document the significance criteria representing project operational traffic deficiencies for intersections. Kittelson will then identify the transportation impacts associated with the project. This assessment will document the proposed changes and potential impacts to intersection LOS for the study intersections. The LOS will be calculated and presented for the following scenarios:

- Existing
- Near term
- Near term plus project conditions
- Cumulative
- Cumulative plus project conditions

Impacts will only be identified for the plus project conditions. Kittelson will also prepare a signal warrant analysis for unsignalized study intersections.

All study intersections will be evaluated during the AM and PM peak hours using VISTRO software and HCM 2010 methodology (or more current HCM versions that the City may request). This traffic analysis will include estimates of average vehicle delays on all approaches. For any impact found to be significant, Kittelson will determine the traffic contribution from the proposed project. Any proposed operational improvements from other studies will be considered in the cumulative assessment.

CMP Segments

If the proposed project generates more than 100 PM peak hour trips, then the project will be subject to review by the County of San Mateo Congestion Management Program (CMP) and its requirements. As such, Kittelson will evaluate the routes of regional significance identified above. Evaluation of the CMP routes will be based on the most recently approved CMP TIA guidelines in the land use section of the CMP.

Kittelson will determine and report the project's impacts to the CMP segments for the near term plus project conditions and cumulative plus project conditions.

Kittelson will identify project generated impacts to the transportation network under the existing plus project conditions, near-term plus project conditions, and 2040 cumulative plus project conditions. Kittelson, in consultation with the City, will determine whether significant project-generated impacts could be mitigated using measures approved in the Menlo Park City General Plan EIR, require additional mitigation, or cannot be mitigated and would thus be considered significant and unavoidable.

Other Topics

Congestion Management Program

Facilities under the City and County of San Mateo Association of Governments (C/CAG) Congestion Management Program (CMP) are required to be analyzed per C/CAG guidelines. Kittelson will analyze the intersections and/or freeway facilities that are part of the County of San Mateo CMP network in the study area for all scenarios.

Pedestrian and Bicycle Facilities

Kittelson will qualitatively discuss the project's impacts to the pedestrian and bicycle network for the existing plus project, near-term plus project conditions, and 2040 cumulative plus project conditions. A figure illustrating any proposed improvements to the pedestrian and bicycle facilities will be prepared.

Transit Facilities

Kittelson will qualitatively discuss the project's impacts to the transit network for the existing plus project, near-term plus project conditions, and 2040 cumulative plus project conditions. A figure illustrating any proposed improvements to the transit facilities will be prepared.

Parking Assessment

Kittelson will identify the City's parking requirement for the project based on its land use type. Kittelson will also estimate the parking demand based on the Parking Generation (4th edition) reference published by the ITE. A parking analysis will be performed by assessing the proposed number of parking spaces and comparing it to the City's parking requirement and the parking demand calculated using the ITE Parking Generation rates.

Site Circulation

Kittelson will review the project site circulation and identify any potential issues within the project site, assuming the project applicant would provide the project site plan. Truck turning templates will also be obtained from the applicant's architect and reviewed for adequate circulation.

Emergency Access

Kittelson will review the project site plan and the roadways surrounding the project to identify any potential issues for emergency vehicle access.

Air Traffic

If necessary, Kittelson will assess the potential project impact to air traffic because of the increased number of trips generation by the project. In addition, Kittelson will review project site plans to determine if the height of any proposed building will interfere with flight operations at local airports.

Construction

Kittelson will qualitatively discuss how the project's construction might impact off-site circulation because of increased truck traffic to and from the project site. In addition, Kittelson will also qualitatively discuss the impact on transit, pedestrian, and bicycle facilities during construction.

C/CAG Transportation Demand Management Requirement

As part of the land use element of the CMP, all projects that generate 100 or more new trips during the AM or PM peak hour are required to implement TDM programs that have the capacity to reduce the demand for new peak-hour trips.

The City has a requirement that the proposed development implement a TDM plan that reduces peak hour trips by 20%. Kittelson will peer review this proposed TDM plan and determine if it adequately meets the 20% goal.

Kittelson will also make recommendations of how the City could monitor the effectiveness of TDM measures.

Project Alternatives

Kittelson will determine the trip generation for one additional project alternative. Kittelson will perform a qualitative analysis for a reduced development project alternative to identify whether it would further add or reduce any project identified impacts.

Documentation

Kittelson will document all work assumptions, analysis procedures, findings, graphics, impacts and recommendations in the administrative draft IS/MND transportation section and technical appendix for review by SWCA and City staff.

The section will also include:

- Description of new or planned changes to the street system serving the project site, including changes in driveway location and traffic control, if any
- Future project condition volumes (ADTs, a.m. peak hour, p.m. peak hour)
- Project trip generation rates
- Project trip distribution
- Discussion of impact of project trips on study intersections
- Levels of service discussion and table for each study scenario
- Comparison table of project condition and existing LOS along with average delay and percent increases at intersections
- Impacts of additional traffic volumes on city streets
- CMP analysis
- Intersection level of service calculation sheets (electronic format)

To support the IS/MND transportation section, Kittelson will provide a technical appendix. The appendix may include more detailed transportation analysis such as level of service calculations, technical memoranda that were developed as part of this proposal, and other supporting materials. To expedite the review process, and if requested, Kittelson will provide a separate copy of the IS/MND transportation section with its appendix to City staff for their review.

Kittelson will provide SWCA with all intersection and roadway segment traffic data for use in noise, air quality, and GHG analyses.

Assumptions

- All study scenarios will be evaluated based on existing intersection geometrics. Should significant impacts be determined with the proposed project development, mitigation measures which may include changes to the intersection geometrics will be recommended.
- Any material modifications to the project site plan, driveway locations or project description once Kittelson has begun the traffic analysis may constitute a change in work scope and/or budget.

- Should analysis of additional phases, scenarios, intersections, or roadway segments or more than one administrative draft report or additional meetings be required, a modification to this scope and budget will be requested.
- Should additional time be necessary to prepare the final IS/MND beyond the budgeted hours (to be determined by the number of public review draft IS/MND comments received and the level of effort required to respond), we will request additional budget at that time and proceed only after receiving written authorization for additional services.
- Any services not explicitly identified above are excluded.

Deliverables

- VMT assessment memorandum (submitted electronically in Microsoft Word and PDF formats)
- Administrative draft and final IS/MND transportation section and technical appendix (submitted electronically in Microsoft Word and PDF formats)

TASK F. TRAFFIC IMPACT ANALYSIS AND TRIBAL CONSULTATION

This task is included under [Task E, Technical Studies](#).

TASK OPTION G. ATTENDANCE AND PRESENTATION AT PLANNING COMMISSION MEETING

If SWCA prepares an IS/MND ([Task C.2](#)), the City Planning Commission will be the final decision-making body on the requested land use entitlements and certification of the IS/MND. [SWCA Project Manager Kara Laurenson-Wright](#) will attend one Planning Commission meeting (2 hours) virtually and support City staff in the process, including being available to answer questions regarding the technical studies and IS/MND.

Assumptions and Deliverables

- *City staff will prepare the PowerPoint for and present the project at the Planning Commission hearing. SWCA will support the City with any questions related to the technical studies and IS/MND.*

TASK H. ADMINISTRATIVE DRAFT INITIAL STUDY/MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING PROGRAM

This task is included under [Task C.2, Preparation of Initial Study Documents](#).

TASK I. MITIGATION MONITORING AND REPORTING PROGRAM

This task is included under [Task C.2, Preparation of Initial Study Documents](#).

TASK J. INITIAL STUDY/MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING PROGRAM

This task is included under [Task C.2, Preparation of Initial Study Documents](#).

TASK K. POTENTIAL ENVIRONMENTAL IMPACT REPORT

If SWCA discovers a potential for significant unavoidable impacts while preparing CEQA topic analyses and conducting background technical studies, we will notify City staff immediately and discuss the potential for preparation of an EIR. If an EIR is required, this scope can be revised accordingly.

TASK L. PUBLIC AND AGENCY REVIEW

This task is included under [Task C.2, Preparation of Initial Study Documents](#).

TASK M. INVOICES

Task M includes preparing regular invoicing progress reports to be submitted with our monthly invoice to the City. A key element of SWCA's progress report procedures is identifying and outlining upcoming issues, information needs, and deliverables. Progress reports will contain beginning and ending dates of the billing period, serial identifications of progress reports, a summary for each work task, the name of the person performing the work, the hours spent by each person, and a brief description of the work. These reports help maintain project momentum by identifying issues as early in the process as possible and building a record of project progress.

PROJECT SCHEDULE

SWCA expects that this environmental review process, including completion of the NOE and technical studies, can be completed within 5 months of a Notice to Proceed and receipt of relevant project data. SWCA is prepared to initiate this scope of work immediately after receiving a signed contract. [Table 1](#) identifies anticipated general timeframes for completion of the deliverables described above.

Table 1. Proposed Project Schedule: Notice of Exemption. We commit to timely deliverables, consistent with the schedule agreed to by SWCA and the City.

MILESTONE	ESTIMATED TIMEFRAME
Task A. Project Kick-Off Meeting and Site Visit	2 weeks after notice to proceed
Task B. Review of City Documents and Data Collection	
Task B.1 Data Request	1 week after project kick off
Task B.2 Project Description	2 weeks after receipt of data request
Task E. Technical Studies	
Task E.1. Air Quality and Greenhouse Gas Emissions Technical Report	8 weeks after finalization of project description
Task E.2. Cultural Resources Assessment	8 weeks after finalization of project description
Task E.3. Noise Report	8 weeks after finalization of project description
Task E.4. Traffic Impact Assessment	10 weeks after finalization of project description
Task C. Preparation of Notice of Exemption	
Task C.2. Administrative Draft NOE	2 weeks after finalization of technical studies
Task C.3. Final Draft NOE	2 weeks after City comments on Task C.2
TOTAL TIME FRAME FOR PREPARATION OF NOE	Approximately 4.5 months

*Note: Although out of alphabetical order, the tasks in the schedule are listed in the order of consecutive preparation.

*Note: See Task A regarding scoping and timing of the technical background studies.

SWCA expects that this environmental review process, including completion of the IS/MND and technical studies, can be completed within 7 months of a Notice to Proceed and receipt of relevant project data. SWCA is prepared to initiate this scope of work immediately after receiving a signed contract. [Table 2](#) identifies anticipated general timeframes for completion of the deliverables described above.

Table 2. Proposed Project Schedule: Initial Study/Mitigated Negative Declaration. We commit to timely deliverables, consistent with the schedule agreed to by SWCA and the City.

MILESTONE	ESTIMATED TIMEFRAME
Task A. Project Kick-off Meeting and Site Visit	2 weeks after notice to proceed
Task B. Review of City Documents and Data Collection	1 week after project kick-off
Task B.1 Data Request	1 week after project kick-off
Task B.2 Project Description	2 weeks after receipt of data request
Task E. Technical Studies	
Task E.1. Air Quality and Greenhouse Gas Emissions Technical Report	8 weeks after finalization of project description
Task E.2. Cultural Resources Assessment	8 weeks after finalization of project description
Task E.3. Noise Report	8 weeks after finalization of project description
Task E.4. Traffic Impact Assessment	10 weeks after finalization of project description
Task C.2 Preparation of Initial Study/Mitigated Negative Declaration	
Task C.2.1 Administrative Draft IS/MND	2 weeks after finalization of technical studies
Task C.2.2 Public Review Draft IS/MND	2 weeks after City comments on Task C.2.1
Public Circulation	30 days
Task C.2.3. Administrative Draft Final IS/MND	3 weeks after end of public review period
Task C.2.4 Final Draft Initial Study/Mitigated Negative Declaration	2 weeks after City comments on Task C.2.3
Task G. Planning Commission Hearing	At City's discretion after Task C.2.4
TOTAL TIME FRAME FOR PREPARATION OF IS/MND	Approximately 7 months

*Note: Although out of alphabetical order, the tasks in the schedule are listed in the order of consecutive preparation.

PROPOSED PROGRAM COMPONENTS

SWCA does not have any additional proposed program components.

OTHER PERTINENT INFORMATION

At SWCA, we believe our employees are our most valuable resource. As a 100% employee-owned firm, we have created several programs, described below, so we can invest in our people, find ways to enhance our team environment, diversify our workforce, and give back to our communities.

EMPLOYEE LEARNING AND DEVELOPMENT

SWCA's Learning and Development program empowers employees to take control of their own learning and, subsequently, craft a more intentional direction for their careers. As part of our North Star, a huge contributor to the success of becoming a great workplace is the degree of opportunities employees feel are available to them for their development. Among the programs we offer to foster our employees' personal and professional development are the Science Leadership Program (SLP), the Educational Assistance Program, the Employee Referral Program, the Always Learning Lab, and the new Career Development Program.

Science Leadership Program

SWCA recognizes and rewards our scientists and technical experts for providing high-quality expertise to our projects, clients, and strategic goals. The SLP—and its members—preserves the technically focused culture of the company,

provides input for corporate strategies, generates creative solutions for our clients, provides career growth and mentorship opportunities within the company, and makes significant contributions to scientific advancement. In 2020, 90 employees maintained active membership in the program, an increase from previous years.

The SLP encourages our scientists' professional growth and scientific knowledge through supporting presentations at professional meetings and offering bonuses to employees who publish articles and other works relevant to their technical disciplines. All employees are eligible for publication bonuses, with amounts varying by the type of publication and increasing with the level of participation in the SLP. SWCA staff publish many peer-reviewed publications annually.

The Brownbag Lecture series provides an internal opportunity for SLP members to share knowledge and business tips, forge connections among employees, and encourage cross-fertilization among disciplines and offices throughout the company. This series is open to all SWCA employees, regardless of participation in the SLP. There were 14 Brownbag Lectures in 2020.

Great work deserves great rewards. Each quarter, one employee is selected by the Science Leadership Committee to receive the Lawrence S. Semo Scientific Achievement Award. Of these four recipients, the Science Leadership Committee and the Senior Leadership Team choose one employee to receive the Steven W. Carothers Scientific Merit Award, which is accompanied by a \$1,000 bonus. The 2020 Lawrence S. Semo Scientific Achievement Award recipients include Vicky Amato, Ken Lawrence, Rick Howard, and Nate Wojcik. The 2020 Steven W. Carothers Scientific Merit Award was awarded to Nate Wojcik, senior ecologist and project manager in the Denver, Colorado, office.

Publication Bonus Program

SWCA offers bonuses to employees who publish articles and other works relevant to their technical disciplines. All employees are eligible for publication bonuses, with rates varying by the type of publication and increasing with the level of participation in the SLP. SWCA proudly touts this benefit to our employees and is pleased to see many team members receive these awards each year.

Educational Assistance Program

SWCA's Educational Assistance Program provides tuition assistance to eligible employees pursuing personal and professional development through continuing education in work-related courses. In recent years the company has provided an average of over \$80,000 in tuition reimbursement to eligible employees. The growth of this assistance program highlights one of SWCA's core values: Always Learning, which we continually emphasize as a critical element of what makes SWCA special and unique.

Employee Referral Program

Good people know good people. SWCA looks to our employees to refer potential colleagues for available positions. We offer bonuses to employees who refer a new hire who is brought on to the SWCA team. After averages just over \$20,000 annually in employee referral bonuses total 2022 payout will increase threefold to more than \$65,000.

Career Development Program

In 2020, SWCA rolled out an award-winning Career Development program in response to Engagement Survey feedback in which employees conveyed the desire for greater clarity around career paths available at SWCA.

The program was developed with four main goals:

1. Prepare employee-owners for the next career step.
2. Build a well-rounded team of consultants.

3. Build consistency in job titles and roles.
4. Show what career options exist and what it takes to get there.

SWCA facilitated over 5,030 training sessions introducing over 1,000 employees to this new program, which offers a rich library of developmental resources, information, and road maps to show employees how their roles fit into the broader SWCA organization and what is possible for their careers at SWCA. This career pathing framework enables employees to take ownership of their own growth and development.

Using the provided materials, employees work with their supervisors each year to complete a multi-level Professional Development Plans (PDP), outlining individual developmental goals and growth plans (PDP). The PDP outlines attainable, realistic ways for employees to leverage their unique competencies of strength and improve areas of weakness to achieve their professional goals most effectively.

Following program rollout, SWCA performed another engagement survey, which showed the significant positive impact of the initiative. Scores related to career development increased drastically since the prior survey, including an astounding 24% increase in response to the statement, "I am aware of career paths and future opportunities available to me at SWCA."

Since that time SWCA has achieved greater consistency in job titling, which enables employees to measure developmental goals and identify potential career pathways with the company. This effort has resulted in the publishing of Career Mobility Guides, adding further clarity around achieving objectives for career growth.

Leadership Development

SWCA rolled out a new 2.5-day leadership development workshop titled Level Up. This intensive workshop incorporates the best in active learning methodologies, providing participants hands-on experience with using new tools and techniques which they can begin to apply immediately upon returning to the office. Nearly 100 supervisors have completed this training in 2022 with 130 more program graduates expected for 2023.

With the success of Level Up the team plans to not only accelerate the rollout of this program but to provide other such developmental opportunities focused on time management, productivity, project management, change management, etc.

Always Learning Lab

SWCA's learning and development team carefully curates valuable developmental content in the Always Learning Lab, a repository for asynchronous training courses, available to all employees. Courses include safety training, competency-related developmental content, technical training sessions, and more. Employees can access these courses from any web-based computer or connected mobile device, further supporting them in the execution of their professional development plans.

DIVERSITY AND INCLUSION

We believe that fostering a diverse and inclusive work environment is fundamental to the strength of our organization and a key part of our core company values.

SWCA's Inclusion and Diversity Strategy

SWCA took the courageous and vulnerable step to address social unrest directly, clearly expressing the organization's intolerance of racism and dedication to do more to influence needed changes. As the nation continues to grapple with the topic of racial justice and inclusion, SWCA continues to discuss ways in which, as an organization, we can make a long-lasting and meaningful difference for inclusion and diversity. While in no way complete, the following list is the SWCA Inclusion and Diversity (I&D) strategy to follow up our words with action.

1. **Inclusion and Diversity Strategy:** SWCA's vision of our I&D strategy is built upon three pillars: 1) an Inclusive Culture, 2) Talent Diversity, and 3) SWCA Gives Back and Supplier Diversity. In 2020, the new I&D strategy and action plan was fully unveiled with a commitment statement and defined quarterly action items.
2. **Dedicated Talent Acquisition Support:** Our Talent Acquisition Specialist now has 25% of their time dedicated to support I&D recruiting activities. Additionally, we completed and rolled out our Career Development program built on a competency mode to further transparency and establish upward career mobility.
3. **Employee Training:** Understanding our own implicit/unconscious biases is the key to opening doors to equity. SWCA has mandated Unconscious Bias Training for all new hires and leadership.
4. **Educational Content:** The SWCA I&D internal webpage provides a curated list of educational resources on various inclusion and diversity topics. This page provides presentations and activities for leaders to use during facilitator-led group discussions. Lastly, we launched the I&D External Speaker Series for Directors/VPs with Dr. Johnetta Cole and Howard Ross, who spoke at the Leadership Conference.
5. **Measurement:** We measure the results of our I&D initiatives to track our success and to identify areas for improvement by utilizing scorecards and employee engagement surveys.
6. **Employee Resource Groups (ERGs):** ERGs are employee-led groups composed of employees who have common interests, issues, and/or bond or background. Their primary focus is to develop and improve SWCA culture by increasing the presence of diverse voices. In addition, they help increase morale, provide insights into diverse areas, build bridges to the community, and empower members.

SWCA's Supplier Diversity Program

SWCA is also committed to partnering with small and diverse subconsultants. We formalized that commitment by developing a Supplier Diversity program, which establishes and maintains mutually beneficial contracting relationships with small and diverse businesses. As part of our commitment to increase the amount of work we contract with other types of diverse businesses, we set goals to increase our subcontractor revenue with Native-owned and minority-owned businesses. In 2019, SWCA was able to increase the amount of Native-owned subcontracted businesses from \$290k to \$450k. In 2020, we increased our minority-owned subcontracted business from \$3.7m to \$6.3m. By implementing our plan and focusing on these goals, we doubled the amount of subcontracted work with Native-owned and minority-owned companies over a 1-year time span.

While we are proud of our inclusion and diversity efforts, we recognize there is still a lot of work to be accomplished. Here at SWCA, we are passionate about giving a voice to the historically marginalized and creating a work environment where all SWCA employees can bring their whole, authentic selves.

Kittelson's Approach to Diversity, Inclusion, And Equity

Kittelson's commitment (including Corporate-level leadership) to diversity, equity, and inclusion (DE&I) is demonstrated daily through how Kittelson builds relationships with teaming partners, hiring practices, employee-related functions, and community involvement. Kittelson's policies and programs ensure all staff members have safe, collaborative, and vibrant working environment, with meaningful opportunities.

Training resources focus on DE&I topics and includes the following:

- An online resource library with materials about race, gender, and unconscious bias
- Internal forums where staff post recommendations for books, podcasts, articles, and movies for self-education on DE&I
- All-staff meetings and huddles, where the DE&I topics are given central spotlights
- Open, firmwide, staff-driven dialogues

- Participation in Employee Resources Groups to have an intentional source of community and access to leadership to address concerns

In 2021, Kittelson continued their commitment to DE&I with several outreach and engagement activities. Kittelson had a third party conduct a Staff Experience Survey, which resulted in a 77% participation rate (224 of 290 staff). The survey highlighted that Kittelson built an uplifting culture that benefits from inclusion and diversity. Moving forward, Kittelson is working on enhancing DE&I practices by focusing on providing a safe and inclusive workplace; creating clear paths for professional development; and improving upon recruitment, retention, and leadership diversity.

Kittelson's Approach to Supplier Diversity

Kittelson's policy of promoting DE&I doesn't stop within their organization. Kittelson continuously looks for opportunities to partner with small businesses and support them through mentorships. Kittelson does this often by including small businesses on projects, providing marketing support for preparing proposals, sharing software with smaller companies, educating small businesses about accounting and business practices, and providing opportunities to prime contracts. Through these and other efforts Kittelson has seen small business partners grow and prosper. Their partnership with small businesses is about far more than meeting a requirement from a particular agency or contract. Kittelson's very culture is founded on providing opportunities for learning and growth. Their partnerships with small businesses firms mirror the way Kittelson treats their own staff—they are intended to maximize small business firms' access to new opportunities, mentorship, and growth.

SWCA GIVES BACK PROGRAM

At SWCA, Giving Back is one of our strong core values. We support the places where we live and work by donating to and participating in an array of community service projects and organizations. Providing staff with paid volunteer hours, matching grants for office volunteer events and donations, and organizing annual company-wide fundraising events all foster a company-wide culture of giving. Additionally, we support our business communities by diversifying our supplier base to foster the growth and development of small and disadvantaged businesses. Our SWCA Gives Back and Supplier Diversity programs have helped us unify our efforts to strengthen the positive impact we can make in our communities.

D. RATE SCHEDULE AND PRICING PROPOSAL

For each CEQA document option, Option C.1 Notice of Exemption or Option C.2 Initial Study/Mitigated Negative Declaration, SWCA has provided two pricing proposals outlined below: 1) Time and Materials and 2) Alternative pricing structure – Fixed Fee. The budget was prepared based on our knowledge of the project, consultation with our technical experts, and experience. Our updated pricing proposals are outlined below. These pricing proposals reflect SWCA’s current 2023 rates and may contain a 0-2% rate escalation from the budget submitted in November 2022.

We have attempted to be conservative in the preparation of the budget regarding the level of effort required so that the overall cost estimates are reasonable for your planning purposes. As outlined in Task M, SWCA will submit a progress report with each invoice to provide visibility and track monthly spending. SWCA will not proceed with any out-of-scope tasks without written consent from the City. Any additional work beyond the scope and assumptions outlined in this proposal will be addressed via change order.

The outlined proposed costs are valid for 90 days from the date of the proposal receipt, at which time SWCA reserves the right to submit a revised cost estimate, project schedule, and staffing plan. Any delay in contracting may require a revised cost estimate, project staffing, and project schedule.

HOURLY RATE SCHEDULE

SWCA staff will bill at our current hourly rate at the time of service in accordance with Table 3, SWCA 2023 Labor Rate Categories and Hourly Rates. If necessary, similar-level staff members may be substituted to complete the work at the time of contracting. These rates have been updated to reflect SWCA’s current 2023 rates, and are inclusive of all overhead rates. SWCA reserves the right to escalate rates by up to 3% starting January 1, 2024. Our subconsultant, Kittleson & Associates, will bill at the hourly rates outlined in Table 4, Kittleson Labor Rate Categories and Hourly Rates.

Table 3. SWCA 2023 Standard Labor Rate Categories and Hourly Rates

LABOR CATEGORY	2023 RATE	LABOR CATEGORY	2023 RATE
CONSULTING SERVICES			
Cultural Resources, Environmental Resources, Paleontology, Scientific Resources, Planning Resources, Air Quality, Landscape Architecture, Ecological Restoration, Disaster Recovery, GIS/CADD Resources, Technical Writing/Editing, Training/Facilitating, Graphics/Media Production, Administrative			
Subject Matter Expert IV	\$279.00	Specialist VIII	\$150.00
Subject Matter Expert III	\$255.00	Specialist VII	\$140.00
Subject Matter Expert II	\$230.00	Specialist VI	\$130.00
Subject Matter Expert I	\$219.00	Specialist V	\$117.00
Specialist XIV	\$230.00	Specialist IV	\$107.00
Specialist XIII	\$219.00	Specialist III	\$97.00
Specialist XII	\$215.00	Specialist II	\$85.00
Specialist XI	\$197.00	Specialist I	\$72.00
Specialist X	\$180.00	Technician II	\$60.00
Specialist IX	\$161.00		

LABOR CATEGORY	2023 RATE	LABOR CATEGORY	2023 RATE
ENGINEERING SERVICES			
Subject Matter Expert IV	\$265.00	Specialist X	\$197.00
Subject Matter Expert III	\$265.00	Specialist IX	\$180.00
Subject Matter Expert II	\$240.00	Specialist VIII	\$161.00
Subject Matter Expert I	\$230.00	Specialist VII	\$150.00
Specialist XIV	\$255.00	Specialist VI	\$140.00
Specialist XIII	\$240.00	Specialist V	\$130.00
Specialist XII	\$230.00	Specialist IV	\$117.00
Specialist XI	\$215.00		

Table 4. Kittelson Labor Rate Categories and Hourly Rates

LABOR CATEGORY	2023 RATE	LABOR CATEGORY	2023 RATE
Principal/Senior Principal	\$255 - \$345	Data Analyst/Software Developer	\$130 - \$165
Associate Engineer/Planner	\$220 - \$250	Software Technician	\$100 - \$125
Senior Engineer/Planner	\$190 - \$210	Associate Technician	\$180 - \$200
Engineer/Planner	\$170 - \$190	Senior Technician	\$160 - \$180
Transportation Analyst	\$150 - \$165	Technician II	\$140 - \$155
Principal Data Scientist/Developer	\$245 - \$330	Technician I	\$120 - \$135
Senior Data Scientist/Developer	\$200 - \$240	Office Support	\$90 - \$110
Data Scientist/Developer	\$170 - \$200		

1. TIME AND MATERIALS

For this pricing proposal, we understand that the contract will be on a time-and-materials, not-to-exceed basis using the hourly bill rates outlined in Table 3 above.

Direct expenses and subcontractor expenses are subject to a 10% administrative markup. Per diem is billed at the GSA rate in place at the time of billing. Mileage is billed at the IRS mileage rate in place at the time of billing. To accommodate project changes and scheduling, it is assumed that SWCA will be able to utilize the overall project funding and will not be held to task limits so long as the overall budget is not exceeded without client approval. The following table provides a cost summary, including labor hours and expense summary, for the proposed scope of work.

Table 5 outlines the time and materials project costs for the Notice of Exemption (Task Option C.1) while Table 6 outlines the costs for the Initial Study/Mitigated Negative Declaration (Task Option C.2).

Table 5. Time & Materials - Total and Per-Task Estimated Project Costs: Notice of Exemption

CITY OF MENLO PARK

City of Menlo Park Initial Study Preparation for 980-1030 O'Brien Drive ISMND Time and Materials Budget	TASK TOTAL	SWCA Labor Hours	SWCA Environmental Consultants (Hourly Rates)																SWCA Labor Total	Direct Expenses			Total SWCA Expenses	Sub-consultants						
			Kara Laursen-Wright Project Manager	Julie Barlow Project Director	Peter Mye Senior Project Planner/Transportation Lead	Brenda Sohm Air Quality, Greenhouse Gas Emissions Lead	Carlos Ituarte Villarreal Air Quality, Greenhouse Gas Emissions Planner	Erin Wielenga Air Quality, Greenhouse Gas Emissions, Energy, and Noise	Joshua Peabody Cultural Director	Garret Root Cultural Resources Lead	Christina Alonso Archaeology and Tribal Cultural Resources Lead	Josh Bevan Project Architectural/Historian	Kerry Boutte Project Archaeologist	Reid Donovan Project Archaeologist	Sayre Borden Project Archaeologist	Erin Butts GIS Analyst	Liz Haines Technical Editor	Jennifer Torre Project Controller		General Biller	Mileage	Records Search		Direct Expense Markup	Kiteelson & Associates, Inc. Transportation Engineering / Planning	Subconsultant Total (includes 10% markup)				
			\$181	\$218	\$215	\$279	\$181	\$140	\$230	\$218	\$180	\$181	\$130	\$117	\$97	\$117	\$140	\$197		\$97	\$0.625	\$1,000		10%		10%				
TASK A. PROJECT KICK-OFF MEETING AND SITE VISIT																														
Kickoff Meeting and Site Visit	\$ 5,402.25	28	15	10													3							\$ 5,196.00	\$ 187.50		\$ 18.75	\$ 206.25		
Subtotal	\$ 5,402.25	28	15	10	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	\$ 5,196.00	\$ 187.50	\$ -	\$ 18.75	\$ 206.25	\$ -	\$ -
TASK B. REVIEW OF CITY DOCUMENTS AND DATA COLLECTION																														
B.1 Data Request	\$ 644.00	4	4																					\$ 644.00				\$ -		
B.2 Project Description	\$ 1,316.00	8	4	2													2							\$ 1,316.00				\$ -		
Subtotal	\$ 1,960.00	12	8	2	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	\$ 1,960.00	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
TASK C: CEQA DOCUMENT																														
<i>Task Option C.1 - Notice of Exception</i>																														
C.1 Notice of Exception	\$ 6,650.00	39	22	6	4												2	5						\$ 6,650.00				\$ -		
Subtotal	\$ 6,650.00	39	22	6	4	0	0	0	0	0	0	0	0	0	0	0	2	5	0	0	0	0	\$ 6,650.00	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
TASK D: PROJECT MANAGEMENT																														
Project Management	\$ 6,492.00	36	24	12																				\$ 6,492.00				\$ -		
Subtotal	\$ 6,492.00	36	24	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$ 6,492.00	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
TASK E: TECHNICAL STUDIES																														
E.1 Air Quality and Greenhouse Gas Emissions Technical Report	\$ 14,272.00	94				8		80																\$ 14,272.00				\$ -		
E.2 Cultural Resources Technical Report / ABS2 Tribal Consultation	\$ 30,031.50	202							6	4	32	56	32	12	48	6	6							\$ 28,794.00	\$ 125.00	\$ 1,000.00	\$ 112.50	\$ 1,237.50		
E.3 Noise Report	\$ 12,966.00	76				8	60									2	6							\$ 12,966.00				\$ -		
E.4 Traffic Impact Analysis	\$ 82,276.70	0																						\$ -				\$ -	\$ 74,797.00	\$ 82,276.70
Subtotal	\$ 139,546.20	372	0	0	0	16	60	80	6	4	32	56	32	12	48	8	18	0	0	0	0	0	\$ 56,032.00	\$ 125.00	\$ 1,000.00	\$ 112.50	\$ 1,237.50	\$ 74,797.00	\$ 82,276.70	
TASK M. INVOICES																														
Invoices	\$ 1,268.00	8	3																					\$ 1,268.00				\$ -		
Subtotal	\$ 1,268.00	8	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$ 1,268.00	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
TOTAL BUDGET	\$ 161,318.45	495	72	30	4	16	60	80	6	4	32	56	32	12	48	12	23	6	2	0	0	\$ 77,598.00	\$ 312.50	\$ 1,000.00	\$ 131.25	\$ 1,443.75	\$ 74,797.00	\$ 82,276.70		

Total Labor	\$77,598.00
Total Expenses	\$1,443.75
Total Subconsultants	\$82,276.70
TOTAL BUDGET	\$161,318.45

Note: Time & Materials for subconsultant, Kittelson, are included as Table 7.

Table 7. Time & Materials – Kittelson Budget Summary

SUBCONSULTANT TASK E	TASK TOTAL	TOTAL HOURS	KITTELSON & ASSOCIATES, INC. (HOURLY RATES)						LABOR TOTAL	DIRECT EXPENSES	
			DAMIAN STEFANAKIS TRANSPORTATION DIRECTOR	AMANDA LEAHY TRANSPORTATION PROJECT MANAGER	AARON ELIAS TECHNICAL ADVICE	MICHAEL SAHIMI PROJECT ANALYSIS	JOEL AMARILLAS GASTELUM PROJECT ANALYSIS	JON SOMMERVILLE GIS		MILEAGE	TOTAL EXPENSES
			\$310 / hr.	\$235 / hr.	\$235 / hr.	\$205 / hr.	\$175 / hr.	\$170 / hr.			
TASK E: TECHNICAL STUDIES											
E.4 Traffic Impact Analysis	\$ 74,797.00	375	18	46	5	122	158	26	\$ 74,645.00	\$ 152.00	\$ 152.00
<i>Subtotal</i>	<i>\$ 74,797.00</i>	<i>375</i>	<i>18</i>	<i>46</i>	<i>5</i>	<i>122</i>	<i>158</i>	<i>26</i>	<i>\$ 74,645.00</i>	<i>\$ 152.00</i>	<i>\$ 152.00</i>
TOTAL KITTELSON BUDGET	\$ 74,797.00	375	18	46	5	122	158	26	\$ 74,645.00	\$ 152.00	\$ 152.00

2. CONTINGENCY FEE

SWCA declines to submit a pricing proposal under the Contingency Fee pricing structure.

3. ALTERNATIVE PRICING STRUCTURES (OPTIONAL): FIXED FEE BY TASK

For this pricing proposal, we understand the contract will be on a fixed fee basis. SWCA will bill the City monthly on a percent-complete basis by task, up to the authorized limit on the contract. Should additional work be required beyond the scope of work outlined in this proposal a change order request will be initiated. SWCA reserves the right to propose a revised fee should the project extend beyond the expected project schedule outlined in this proposal.

[Table 8](#) outlines the fixed fee project costs for the Notice of Exemption ([Task Option C.1](#)) while [Table 9](#) outlines the costs for the Initial Study/Mitigated Negative Declaration ([Task Option C.2](#)).

Table 8. Fixed Fee – Total and Per-Task Estimated Project Costs: Notice of Exemption

TASK	TASK TOTAL
TASK A. PROJECT KICK-OFF MEETING AND SITE VISIT	
Kickoff Meeting and Site Visit	\$ 5,402.25
<i>Subtotal</i>	\$ 5,402.25
TASK B. REVIEW OF CITY DOCUMENTS AND DATA COLLECTION	
B.1 Data Request	\$ 4,448.00
B.2 Project Description	\$ 7,168.00
<i>Subtotal</i>	\$ 11,616.00
TASK C: CEQA DOCUMENT: NOTICE OF EXEMPTION	
C.1 Notice of Exemption	\$ 6,650.00
<i>Subtotal</i>	\$ 6,650.00
TASK D: PROJECT MANAGEMENT	
Project Management	\$ 6,492.00
<i>Subtotal</i>	\$ 6,492.00
TASK E: TECHNICAL STUDIES	
E.1 Air Quality and Greenhouse Gas Emissions Technical Report	\$ 14,272.00
E.2 Cultural Resources Technical Report / Assembly Bill 52 Tribal Consultation	\$ 30,031.50
E.3 Noise Report	\$ 12,966.00
E.4 Traffic Impact Analysis	\$ 82,276.70
<i>Subtotal</i>	\$139,546.20
TASK G: ATTENDANCE AND PRESENTATION AT PLANNING COMMISSION MEETING	
Planning Commission Hearing	\$ 966.00
<i>Subtotal</i>	\$ 966.00
TASK M. INVOICES	
Invoices	\$ 1,268.00
<i>Subtotal</i>	\$ 1,268.00
TOTAL BUDGET	\$161,318.45

Table 9. Fixed Fee – Total and Per-Task Estimated Project Costs: Initial Study/Mitigated Negative Declaration

TASK	TASK TOTAL
TASK A. PROJECT KICK-OFF MEETING AND SITE VISIT	
Kickoff Meeting and Site Visit	\$ 5,402.25
Subtotal	\$ 5,402.25
TASK B. REVIEW OF CITY DOCUMENTS AND DATA COLLECTION	
B.1 Data Request	\$ 4,448.00
B.2 Project Description	\$ 7,168.00
Subtotal	\$ 11,616.00
TASK C: CEQA DOCUMENT: PREPARATION OF INITIAL STUDY DOCUMENTS	
C.2.1 Administrative Draft IS/MND	\$ 24,095.94
C.2.2 Public Review Draft IS/MND and MMRP	\$ 12,164.00
C.2.3 Administrative Draft Final IS/MND and Responses to Comments	\$ 9,591.00
C.2.4 Final IS/MND and Responses to Comments	\$ 5,854.50
Subtotal	\$ 51,705.44
TASK D: PROJECT MANAGEMENT	
Project Management	\$ 9,081.00
Subtotal	\$ 9,081.00
TASK E: TECHNICAL STUDIES	
E.1 Air Quality and Greenhouse Gas Emissions Technical Report	\$ 14,272.00
E.2 Cultural Resources Technical Report / Assembly Bill 52 Tribal Consultation	\$ 30,031.50
E.3 Noise Report	\$ 12,966.00
E.4 Traffic Impact Analysis	\$ 82,276.70
Subtotal	\$139,546.20
TASK G: ATTENDANCE AND PRESENTATION AT PLANNING COMMISSION MEETING	
Planning Commission Hearing	\$ 966.00
Subtotal	\$ 966.00
TASK M. INVOICES	
Invoices	\$ 3,163.00
Subtotal	\$ 3,163.00
TOTAL BUDGET	\$221,479.89

SWCA

APPENDIX A:
Resumes

JULIE BARLOW, B.S., AICP, PROJECT DIRECTOR

Ms. Barlow has over 20 years of experience in community planning and environmental analysis in the public and private sectors. She prepares all levels of environmental planning documents and is skilled in managing high-visibility projects and area plans. She is recognized for working efficiently with staff, clients, and consultants, and has a demonstrated ability to meet fast-track deadlines. Ms. Barlow has an in-depth understanding of the importance of clear communication with clients to tailor a project's work program to meet its particular objectives.

Previously, Ms. Barlow was the Senior Planner for the Town of Woodside. She was responsible for current planning applications and authored and managed Negative Declarations for historic resources and streambed alteration plans. She was the head of the Town's stormwater pollution prevention program, and staff liaison to the Town's Historic Preservation Committee. Ms. Barlow received a B.S. in City and Regional Planning from California Polytechnic State University, San Luis Obispo. She has studied design and arts programs at the University of London and the Czech University of Technology. Ms. Barlow was a Peace Corps Volunteer in the Czech Republic, serving as a municipal and environmental consultant. She is a member of the American Institute of Certified Planners, the American Planning Association, and the San Francisco Planning and Urban Research Association.

YEARS OF EXPERIENCE

23

EXPERTISE

Project management
CEQA compliance
City and regional planning

EDUCATION

B.S., City and Regional Planning;
California Polytechnic State University,
San Luis Obispo

REGISTRATIONS / CERTIFICATIONS

Certified Planner No. 015654; American
Institute of Certified Planners

TRAINING

Architecture and Urban Planning
Summer Design Program, Czech
University of Technology (CVT)
Study Abroad Program, University of
London
Project Management Bootcamp, PSMJ
Resources, Inc.; 2018

MEMBERSHIPS

Member, SPUR
Member, American Institute of Certified
Planners
Member, American Planning
Association

SELECTED PROJECT EXPERIENCE

Concord Reuse Project; City of Concord; Concord, Contra Costa County, California. SWCA is currently assisting the City of Concord and Lennar/FivePoint in preparation of a Draft EIR for the Concord Reuse Project (CRP) Specific Plan. The Draft EIR includes analysis of the 2,300-acre CRP Project Specific Plan Project, which encompasses roughly 45 percent of the former 5,028-acre Concord Naval Weapons Station site. The CRP Specific Plan will include phased mixed-use development, including residential, commercial, institutional, and greenway and park uses. *Role: Project Manager. Responsible for preparation, review, scheduling, and agency coordination.*

Pier 70 Mixed-Use District Project EIR; FC Pier 70, LLC; San Francisco, California. SWCA directed the EIR on the Pier 70 Mixed-Use District Project, a major land development proposal for 35 acres of historic shipyard property along the City and County of San Francisco's Central Waterfront. The project site is generally under the jurisdiction of the Port of San Francisco and encompasses roughly half of the 69-acre Pier 70 area. Most of Pier 70 is listed on the NRHP as the Union Iron Works Historic District in recognition of its industrial architecture and decades-long role in the development of steel shipbuilding in the United States. *Role: Project Manager. Responsible for review, coordination, and preparation of EIR and technical background analyses.*

Parkmerced Project EIR; Parkmerced Owner, LLC; San Francisco, California. SWCA prepared the EIR for a 152-acre, long-term mixed-use development program to comprehensively re-plan and re-design the site, increase residential density, and provide new services and transit facilities. Some existing apartments will be retained, some demolished, and about 5,700 units will be constructed. The project includes approximately 68 acres of public open space and would reroute the existing Muni Metro M-Ocean View light rail into the project site. *Role: Planning Specialist. Responsible for review, coordination, and preparation of EIR and technical background analyses.*

201 Haskins Development Project; Alexandria Real Estate Equities, Inc.; South San Francisco, San Mateo County, California. SWCA prepared an EIR for redevelopment of an existing light industrial site to a research and development office campus. The project involves comprehensive land use compatibility, recreation, water resources, air quality and noise, and transportation and circulation analysis. *Project Manager. Responsible for review, coordination, and preparation of EIR and technical background analyses.*

19th Avenue Corridor Study; San Francisco Planning Department; San Francisco, California. Cumulative transportation and public services/utilities study along San Francisco's 19th Avenue Corridor that analyzed the effects of projected growth in the southwest area of San Francisco. Key issues included traffic, transit, bicycle, and pedestrian conditions, and cumulative analyses for public services addressed police, fire and emergency services, schools, recreation and open space, water supply, and wastewater. *Role: Project Manager. Responsible for preparation, review, scheduling, and agency coordination.*

706 Mission Street/The Mexican Museum and Residential Tower Project EIR; Millennium Partners/706 Mission St. Co., LLC; San Francisco, San Francisco City and County, California. EIR on a controversial proposal to construct an institutional mixed-use project, including a 550-foot-tall mixed-use tower with museum uses on the first four levels that would be adjacent to, and physically connected to, the historic Aronson Building. Key issues included wind hazards, pedestrian access and circulation, historic resources, and shadow impacts on public open space. *Role: Project Manager. Responsible for review, coordination, and preparation of EIR and technical background analyses.*

Tivoli Specific Plan Project EIR; City of Modesto; Modesto, California. EIR for a Specific Plan to guide future development of a 454-acre mixed-use community, including 30 acres of public open space. Key issues included traffic and circulation, air quality, noise, loss of prime agricultural land, biology, water supply, and police and fire services impacts. *Role: Project Manager. Responsible for review, coordination, and preparation of EIR and technical background analyses.*

75 Howard Street; Paramount Group, Inc.; San Francisco City and County, California. EIR on a proposal to construct a new 350-foot-tall mixed-use tower along The Embarcadero, within the Transit Center District Plan Area. The project required legislation to increase the height limit from 200 to 350 feet. Key issues included visual, traffic, and shadow impacts on public open space. *Role: Project Manager. Responsible for review, coordination, and preparation of EIR and technical background analyses.*

Rehabilitation and Detention Facility – Hall of Justice Jail Replacement; San Francisco Department of Public Works; San Francisco, San Francisco City and County, California. Mitigated Negative Declaration to construct a new multi-story, 640-bed jail facility adjacent to the existing Hall of Justice building at 850 Bryant Street. The proposed new jail facility would replace existing deficient facilities located on the sixth and seventh floors of the Hall of Justice building. The project would include an underground, secure, basement-level tunnel connecting the new building with the existing Hall of Justice building. *Role: Project Manager. Responsible for preparation, review, scheduling, and agency coordination.*

The Infinity / 300 Spear Street Mixed Use Project; San Francisco, California. EIR for a mixed-use residential, retail and office project in San Francisco's Rincon Hill area. Key issues included urban design/visual effects of a proposed increase in height limit, transportation, and wind effects. *Role: Planner. Responsible for technical EIR analysis.*

The Lumina / 201 Folsom Street Mixed Use Project; San Francisco, California. EIR for a mixed-use residential, retail and office project in San Francisco's Rincon Hill area. Key issues included urban design/visual effects of a proposed increase in height limit, transportation, and wind effects. *Role: Planner. Responsible for technical EIR analysis.*

Kaiser Modesto Medical Center – Cornerstone Business Park EIR; City of Modesto; Modesto, California. EIR for a 1,427,000-square-foot Medical Center Campus and 400,000-square foot. business park development. Key issues included loss of prime agricultural land, water supply, traffic, and air quality impacts, and use of an emergency helicopter landing pad. *Role: Project Manager. Responsible for preparation, review, scheduling, and agency coordination.*

Kaiser Permanente Roseville Medical Center Expansion EIR; City of Roseville Planning Department; Roseville, California. EIR for the expansion of the facilities on the Kaiser Roseville Medical Center campus, the addition of several new buildings, and the expansion of existing buildings on the site. *Role: Project Manager. Responsible for preparation, review, scheduling, and agency coordination.*

KARA LAURENSEN-WRIGHT, B.S., PROJECT MANAGER

Kara Laurenson-Wright is an environmental planner and project manager with 7 years' experience specializing in the management, research, and analysis of projects subject to compliance the California Environmental Quality Act (CEQA) and the National Environmental Policy Act (NEPA). She has worked with a diverse client base consisting of both public and private entities within the greater Bay Area. Her experience ranges from mixed-use development, large-scale residential projects, industrial projects, linear utility projects, and water and wastewater infrastructure. She served as deputy project manager and authored environmental impact analysis sections for several projects in the Peninsula and South Bay Area, including 123 Independence Drive Project, Palm Villas Senior Living Facility Project, and Advanced Metering Infrastructure Project. In addition to her CEQA experience, Ms. Laurenson-Wright brings knowledge of commercial real estate, as well as competency in geographic information systems.

YEARS OF EXPERIENCE

7

EXPERTISE

Environmental planning

CEQA compliance

NEPA compliance

Project management

Environmental impact reports

Environmental impact statements

Mitigated negative declarations

Categorical exemptions

Proponent's environmental assessments

EDUCATION

B.S., Environmental Analysis and Policy; Boston University, Boston, MA; 2015

TRAINING

Gas Transmission Land Planner Training, Pacific Gas and Electric; 2021

MEMBERSHIPS

Member, American Planning Association

SELECTED PROJECT EXPERIENCE (* denotes project experience prior to SWCA)

*** 123 Independence Drive Project; City of Menlo Park; Menlo Park, California.**

Prepared an environmental impact report (EIR) for the redevelopment of five parcels with approximately 432 dwelling units and 586 parking spaces on an 8.15-acre site. Assisted with initiation of the project description, technical studies, and Notice of Preparation.

Role: Deputy Project Manager.

*** Palm Villas Senior Living Facility Project; City of Saratoga; Saratoga, California.**

Prepared an EIR that analyzed the construction of two senior living facility buildings on a vacant lot, containing a combined total of 78 patient beds. The project included related support functions, 48 parking spaces, landscaping, utility connections, and an extension of Saratoga Creek Drive. The project included a robust public outreach process. The Planning Commission certified the EIR on September 9, 2020, and the decision was subsequently appealed to the City Council. The City Council upheld the certification on November 4, 2020. *Role: Deputy Project Manager and CEQA Specialist. Primary author of the EIR.*

*** 1431 El Camino Real Project; City of Burlingame; City of Burlingame, California.**

Prepared a mitigated negative declaration (MND) for the demolition and reconstruction of a three-story residential apartment building in Burlingame. The project required evaluation for historical significance and a Caltrans encroachment permit. The Planning Commission approved the project on February 12, 2018. *Role: Deputy Project Manager and CEQA Specialist. Assisted in the preparation of a Caltrans-compliant historical resources compliance report and had to address impacts to a National Register of Historic Places-listed tree row within the project area.*

*** Advanced Metering Infrastructure Project; San Jose Water Company; County of Santa Clara.**

Preparation of a Proponent's Environmental Assessment for San Jose Water Company to submit to the California Public Utilities Commission (CPUC). The project would replace 230,000 existing water meters with advanced meter-reading devices and an infrastructure system of 16 base stations throughout San Jose Water

Company's service area. The project included coordination with the CPUC, Santa Clara County, cities of San Jose and Saratoga, and town of Los Gatos. *Role: Deputy Project Manager.*

Potrero Yard Modernization Project Environmental Impact Report; San Francisco Municipal Transportation Agency (SFMTA); City and County of San Francisco, California. SWCA prepared a Notice of Preparation, Initial Study, EIR, and associated and technical background studies for the modernization of Potrero Yard and its potential redevelopment as a unique mixed-use site with transit, residential, and commercial uses. SWCA is currently managing the Responses to Comments phase of the EIR. SWCA also administered the required noticing and the public outreach efforts of the Potrero Yard component of the SFMTA's larger Building Progress Program.

Redevelopment of the 4.4-acre Potrero Yard infill site requires the demolition of a historic resource and a large amount of earthwork for a basement and foundation for the proposed 1,300,000-gross-square-foot structure, which would have a total height of up to 150 feet at the tallest point. The structure will include a new three-level, 75-foot-tall transit facility that will serve an expanded and all-electric bus fleet. Up to 575 residential units would be developed along the perimeter of the new transit facility and on residential floors above the transit facility. *Role: CEQA/ NEPA Specialist. Responsible for authoring EIR sections in response to comments, coordinating DEIR publication process, and keeping the project on schedule.*

* **70-74 Liberty Ship Way Project; City of Sausalito; Sausalito, California.** Prepared an MND for the redevelopment of a 3.9-acre waterfront site on the shore of Richardson Bay. The project would construct three two-story buildings within the Marinship Specific Plan area, 108 parking spaces, and include marine, industrial, storage, and other related uses. The project would add pedestrian access and connectivity improvements to the San Francisco Bay Trail. The project had an extensive public outreach process and included in-house preparation of cultural and biological resource assessments as well as a transportation impact assessment under the State's VMT guidelines to support the CEQA document. *Role: Deputy Project Manager. Responsible for authoring sections of the Addendum, reviewing and coordinating staff and technical background analyses, and representing the City at public meetings.*

* **Egbert Switching Station Project; CPUC; City and County of San Francisco, City of Daly City, and City of Brisbane, California.** Prepared an EIR analyzing the construction of a new 230-kilovolt (kV) switching station and the rerouting of two existing underground 230-kV transmission lines currently connected to the existing Martin Substation to connect to the proposed Egbert Switching Station. The project was located in the City and County of San Francisco, the City of Daly City, and the City of Brisbane and required coordination with all three cities. Key issues included land use and planning, hydrology, geology and soils, and determination of project alternatives. *Role: Environmental Planner. Author of Land Use and Planning, Noise, and Hazards and Hazardous Materials EIR sections; coordinated the publication and public outreach efforts alongside the CPUC.*

1125 Market Street Mitigated Negative Declaration; Confidential Client; City and County of San Francisco, California. SWCA prepared an Initial Study/Mitigated Negative Declaration for a new 12-story mixed-use building with about 180 hotel rooms, hotel meeting and event space, about 10,500 gross square feet of office space, and 5,500 gross square feet of retail/restaurant space. SWCA directed the analyses of impacts on adjacent and nearby historic resources, transportation (key issues were commercial and pedestrian loading), construction noise, air quality, wind, and shadow on public open spaces. Prior to project approval, the proposal was revised by the project sponsor to remove the office space and add hotel rooms; SWCA is assisting the project sponsor and Planning Department staff in amending the MND to account for these modifications to the proposed project. *Role: CEQA/ NEPA Specialist. Responsible for authoring and coordinating changes to the MND.*

* **655 4th Street Project; City of San Francisco Planning Department; San Francisco, California.** Prepared a Community Plan Exemption (CPE) for a project on a 71,300-square-foot site located within San Francisco's Central SoMa Neighborhood Plan. The project proposed to demolish three existing buildings, merge the seven existing lots, and construct two new buildings containing approximately 1,083,000 square feet of residential, hotel, office, and retail area. Key issues included noise, air quality, traffic, and wind impacts. The Planning Commission approved the project on June 20, 2019. *Role: Deputy Project Manager. Responsible for authoring sections of the CPE and reviewing and coordinating staff and technical background analyses.*

* **West-of-Bayshore Recovery Action Plan CEQA Addendum; San Francisco International Airport; San Francisco, California.** Prepared an Addendum under the direction of the San Francisco Planning Department to support operation and management actions on the 180-acre West-of-Bayshore property owned by the San Francisco International Airport. The project included the conservation of habitats for the San Francisco garter snake and California red-legged frog. Actions covered included canal widenings, an alternative canal vegetation maintenance pilot program that entails herbicide use, upland habitat enhancement, and fuel abatement/firebreaks. The Addendum was approved August 17, 2020. *Role: Deputy Project Manager. Responsible for authoring sections of the Addendum, reviewing and coordinating staff and technical background analyses, and keeping the project on schedule.*

* **Levine-Fricke Field Improvements; University of California, Berkeley; Berkeley, California.** Prepared an Addendum tiered from the University's 2020 Long Range Development Plan analyzing the redesign and development of a new Levine-Fricke Softball Field on the UC Berkeley campus. The project would demolish all existing buildings, rotate the orientation of the field, increase spectator seating, and construct an approximately one-story structure with locker rooms, press box, etc. *Role: Deputy Project Manager. Responsible for authoring sections of the Addendum, reviewing and coordinating staff and technical background analyses, and keeping the project on schedule.*

PETER MYE, B.A., M.U.R.P., SENIOR PROJECT PLANNER / TRANSPORTATION LEAD

Peter Mye has over 20 years of land use planning, historic preservation, public outreach, and environmental impact analysis experience in the public and private sectors in the Bay Area and throughout California. He is a seasoned regulatory practitioner who is thoroughly familiar with the review and compliance processes under the California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA). He is currently the Project Manager for the Potrero Yard Modernization Project at 2500 Mariposa Street, San Francisco, and is preparing Mitigated Negative Declarations (MNDs) for residential, retail/commercial, office, and warehouse projects for the Cities of Sacramento, San Luis Obispo, and Healdsburg. Mr. Mye recently completed work on the 1125 Market Street Hotel Project MND (City of San Francisco), the Raley & Diesel Warehouses Project MND (City of Sacramento), and the 3333 California Street Mixed-Use Project EIR (City of San Francisco). He has also managed multiple projects in San Francisco, including Environmental Impact Reports (EIRs) for the 1028 Market Street, Chinese Hospital, 1527-1545 Pine Street, and 1285 Sutter Street Projects and MNDs for the Sixth Street Pedestrian Safety and the Rehabilitation and Detention Facility Replacement Projects.

YEARS OF EXPERIENCE

23

EXPERTISE

- CEQA/NEPA compliance
- City and regional planning
- Local government assistance
- Project management
- Transportation and land use policy

EDUCATION

- B.A., History; Pomona College, Claremont, California
- M.U.R.P., Land Use and Transportation; Portland State University, Portland, Oregon
- Peace Corps Volunteer in Municipal Service Development, San Juan Bautista, Paraguay

MEMBERSHIPS

- San Francisco Planning and Urban Research Association
- American Planning Association

SELECTED PROJECT EXPERIENCE (* project experience prior to SWCA)

Potrero Yard Modernization Project EIR; San Francisco Municipal Transportation Agency (SFMTA); City and County of San Francisco, California. SWCA prepared a Notice of Preparation (NOP), Initial Study (IS), EIR, and associated and technical background studies for the modernization of Potrero Yard and its potential redevelopment as a unique mixed-use site with transit, residential, and commercial uses. SWCA is currently managing the Responses to Comments phase of the EIR. SWCA also administered the required noticing and the public outreach efforts of the Potrero Yard component of the SFMTA’s larger Building Progress Program. Redevelopment of the 4.4-acre Potrero Yard infill site requires the demolition of a historic resource and a large amount of earthwork for a basement and foundation for the proposed 1,300,000-gross-square-foot structure, which would have a total height of up to 150 feet at the tallest point. The structure will include a new three-level, 75-foot-tall transit facility that will serve an expanded and all-electric bus fleet. Up to 575 residential units would be developed along the perimeter of the new transit facility and on residential floors above the transit facility. *Role: Project Manager. Reviewed, coordinated, and prepared EIR and technical background analyses.*

1125 Market Street MND; Confidential Client; City and County of San Francisco, California. SWCA prepared an Initial Study/Mitigated Negative Declaration for a new 12-story mixed-use building with about 180 hotel rooms, hotel meeting and event space, about 10,500 gross square feet of office space, and 5,500 gross square feet of retail/restaurant space. SWCA directed the analyses of impacts on adjacent and nearby historic resources, transportation (key issues were commercial and pedestrian loading), construction noise, air quality, wind, and shadow on public open spaces. Prior to project approval, the proposal was revised by the project sponsor to remove the office space and add hotel rooms; SWCA is assisting the project sponsor and Planning Department staff in amending the MND to account for these modifications to the proposed project. *Role: Project Manager. Responsible for coordination, technical review, and management of MND and technical background analyses for transportation, noise, air quality, wind, and shadow.*

3333 California Mixed Use Project Environmental Services; Laurel Heights Partners, LLC; City and County of San Francisco, California. SWCA prepared an IS and Focused EIR for redevelopment of an approximately 10.25-acre parcel currently occupied by the University of California–San Francisco Laurel Heights Campus. The site would be redeveloped to include a mix of residential, retail, office, childcare, and associated parking uses. These proposed uses would be in 13 new buildings and in two adaptively reused buildings (originally a single office building). Proposed parking would be provided in four below-grade parking garages and six individual, two-car parking garages. *Role: Project Manager. Responsible for review, coordination, and preparation of EIR and technical background analyses.*

Sixth Street Pedestrian Safety Project MND; SFMTA; City and County of San Francisco, California. SWCA prepared an IS/MND for the Sixth Street Pedestrian Safety Project, one of the initial sets of innovative Vision Zero San Francisco projects dedicated to making City streets safer for pedestrians, bicyclists, drivers, and transit. This road diet project addressed the high concentration of pedestrian and bicyclist safety issues along a major arterial in downtown San Francisco with connections to the state freeway system. Its goals are to calm motor vehicle traffic and reduce speed; reduce pedestrian collisions; improve pedestrian crossings at intersections; improve safety and comfort for bicyclists; and create a safe and inviting public space. The project site encompasses an 82.5-foot-wide public right-of-way, including the street and sidewalks, along the entire length of Sixth Street between Market and Brannan streets in the South of Market area of San Francisco. *Role: Project Manager. Responsible for preparation of the IS/MND and coordination with technical specialists for transportation, noise, and air quality background analyses.*

Pier 70 Mixed Use District Project EIR; FC Pier 70, LLC; City and County of San Francisco, California. SWCA prepared an NOP and EIR for a long-range project located on 35 acres of historic shipyard property along San Francisco's Central Waterfront. The proposed project envisions a flexible land use program under provisions of a new Special Use District, which would allow up to nearly 4.2 million gross square feet of residential, commercial, retail, and arts and light industrial uses, along with parking, 9 acres of new public parks, and shoreline trail access. *Role: Project Planner/Task Lead. Responsible for QA/QC of land use, population and housing, recreation, public services, utilities and service systems, geology and soils, hazards, and hydrology and water quality analysis for the EIR and preparation of the administrative record.*

Rehabilitation and Detention Facility – Hall of Justice Jail Replacement; San Francisco Department of Public Works; City and County of San Francisco, California. SWCA prepared an IS/MND to construct a new multi-story, 640-bed jail facility adjacent to the existing Hall of Justice building at 850 Bryant Street. The proposed new jail facility would replace existing deficient facilities located on the sixth and seventh floors of the Hall of Justice building. The project would include an underground, secure, basement-level tunnel connecting the new building with the existing Hall of Justice building. *Role: Deputy Project Manager/Project Planner. Responsible for coordination and review of transportation, noise, air quality, wind, and shadow technical background analyses.*

Chinese Hospital Replacement Project EIR; Chinese Hospital; City and County of San Francisco, California. SWCA prepared an IS and Focused EIR on the construction of an approximately 103,710-gross-square-foot replacement hospital, interior renovation of the existing Chinese Hospital, and leasing/interior renovation of buildings at 1140 Powell Street (off-street parking and storage) and 827 Pacific Avenue (new Radiology Center). SWCA managed this project on an accelerated schedule to meet hospital bond financing deadlines. In coordination with Environmental Planning staff and the project sponsor team, SWCA developed a flexible range of feasible alternatives to the proposed project and analyzed different construction phasing alternatives that would allow the existing hospital to continue operation while the new replacement hospital was built. *Role: Project Manager. Responsible for review, coordination, and preparation of EIR and technical background analyses.*

* **Transit Effectiveness Project Focused EIR; SFMTA; City and County of San Francisco, California.** Prepared IS and Focused EIR on citywide transit system improvements for the San Francisco Municipal Transportation Agency. Key issues included analyzing portions of the project at a program level and portions at a project level of detail; transportation impacts on both the transit system and on traffic; air quality impacts of an increased number of diesel bus vehicle-miles traveled; and noise impacts of buses traveling on streets not now served by transit. *Role: Deputy Project Manager. Responsible for preparation of the IS, coordination with technical specialists for EIR sections, revisions to project, and responses to public comments.*

* **Treasure Island / Yerba Buena Island Redevelopment Project EIR; Treasure Island Development Company, LLC; City and County of San Francisco, California.** This project included an EIR on a long-range development program for approximately 500 acres on the two islands in San Francisco Bay to replace Navy facilities and housing. *Role: Deputy Project Manager / Planning Specialist. Prepared analyses of access to recreational resources and public health concerns related to use of synthetic turf fields.*

* **Parkmerced Project EIR; Parkmerced Owner, LLC; City and County of San Francisco, California.** This project included an EIR for a 152-acre, long-term mixed-use development program to comprehensively re-plan and re-design the site, increase residential density, and provide new services and transit facilities. Some existing apartments will be retained, some demolished, and about 5,700 units will be constructed. The project includes approximately 68 acres of public open space and would reroute the existing Muni Metro M-Ocean View light rail into the project site. *Role: Project Planner. Conducted research for and prepared the land use, population and housing, traffic, recreation, public services, utilities and service systems, geology and soils, hazards, and hydrology and water quality sections of the EIR and prepared the administrative record.*

JULIET BOLDING, B.A., PROJECT PLANNER

Ms. Bolding is an environmental planner with experience in environmental assessments, environmental constraints reviews, alternatives studies, and federal and state permit acquisition. Her expertise includes all phases of the environmental review process under the California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA). She has authored and contributed to numerous sections of environmental impact reports (EIRs), as well as entire categorical exemption (CE) analyses, Initial Studies and Mitigated Negative Declarations (IS/MNDs), and other planning documents for agency, utility, renewable energy, and land development clients throughout California.

YEARS OF EXPERIENCE

29

EXPERTISE

Environmental planning

CEQA/NEPA compliance

Water resources management

Water agencies

General Order 131-D compliance

EDUCATION

B.A., Zoology; University of California at Davis; 1984

Post-Baccalaureate work in Water Quality; San Jose State University (1986–1987)

TRAINING

CEQA Guidelines and Energy Workshop, AEP; 2018

CEQA Essentials Workshop, AEP; 2019

Implementing SB 743 – VMT Analysis for Transportation

Resiliency 101. An introduction to the Principles of Resilience Planning

SELECTED PROJECT EXPERIENCE

County of San Mateo EIR for Canyon Lane Roadway Improvements; County of San Mateo; Redwood City, San Mateo County, California. SWCA prepared an EIR and technical background studies for the improvement of Canyon Lane, an undeveloped paper street in unincorporated San Mateo County; the development of a single-family residence on one parcel; and the construction of new utilities, including a water line that would loop in with an existing water line within the city of Redwood City and an underground electrical distribution line and stormwater infrastructure. *Role: Project Planner. Authored EIR impact sections for Energy, Hydrology and Water Quality, Noise, Utilities, and Mandatory Findings.*

Grand View IS/MND; City of Half Moon Bay; Half Moon Bay, San Mateo County, California. SWCA prepared an IS/MND to achieve CEQA compliance in support of the merging of two existing substandard lots into one substandard lot, the construction a residence and an attached accessory dwelling unit, and the extension of Grand View Boulevard 50 feet from the end of the existing pavement to cover the frontage of the parcel in the city of Half Moon Bay. *Role: Project Planner. Authored IS/MND and responded to City comments.*

Magnolia Street IS/MND; City of Half Moon Bay; Half Moon Bay, San Mateo County, California. SWCA prepared an MND and supporting technical studies, including a biological resources evaluation, addendum to the biological resources evaluation, and a wetland and waters delineation and assessment in support of the proposed Magnolia Street Project in the City of Half Moon Bay. The proposed project includes the construction of two residences and the construction of a fire truck turnaround on a dead-end street. In order to achieve CEQA compliance, SWCA prepared the MND and requisite technical studies for submittal to the Half Moon Bay Planning Department. SWCA prepared deliverables on an accelerated schedule and worked closely with the project planner to deliver defensible documents. *Role: Project Planner. Authored IS/MND and responded to City comments.*

Marquette Residential Development; Pizzulli Associates, Inc.; Los Angeles, Los Angeles County, California. SWCA prepared an MND and supporting technical studies, including a tribal cultural resources study, historical resources assessment, biological assessment, and air quality analysis, in support of the proposed Marquette Residential Development Project in the Pacific Palisades neighborhood of Los Angeles. The proposed project includes the demolition of two residences and the construction of eight new single-family homes. In order to achieve CEQA compliance, SWCA prepared the MND and requisite technical studies for submittal to the Los Angeles Department of City Planning. SWCA prepared deliverables on an accelerated schedule and worked closely with the project owner to deliver defensible documents. *Role: Project Planner. Authored Cultural Resources,*

Hydrology and Water Quality, Land Use and Planning, Public Services, and Utilities and Service Systems, Assisted in response to public comments. Assisted in converting the IS/MND to a Class 32 CE.

Kelt Reservoir Environmental Review; Golden State Water Company; Orcutt, Santa Barbara County, California. SWCA prepared technical studies and an IS/MND for the installation of a new approximately 1.3-mile water pipeline and two new 1 million-gallon reservoir water tanks along Orcutt Hill Road near the southern boundary of the community of Orcutt. *Role: Project Planner. Authored IS/MND.*

PV Water Coastal Distribution System F-Line Expansion Planning Support; Pajaro Valley Water Management Agency; Watsonville, Santa Cruz County, California. SWCA provided planning, permitting, and compliance support for the PV water coastal distribution system pipeline expansion, including preparation of a CEQA Addendum, technical background studies, and California Department of Fish and Wildlife Notification of Lake and Streambed Alteration; assistance with securing the Coastal Development Permit; review of the stormwater pollution prevention plan; and construction compliance surveys, monitoring, and reporting. *Role: Project Planner. Authored EIR addendum for the F Pipeline Extension Project.*

Seymour Street IS/MND; City of Half Moon Bay; Half Moon Bay, San Mateo County, California. SWCA prepared an MND and supporting technical studies, including a biological resources evaluation, in support of the proposed Seymour Street Parcel Map Project in the City of Half Moon Bay. The proposed project includes the construction of two residences. In order to achieve CEQA compliance, SWCA prepared the MND and requisite technical studies for submittal to the Half Moon Bay Planning Department. SWCA prepared deliverables on an accelerated schedule and worked closely with the project planner to deliver defensible documents. *Role: Project Planner. Authored IS/MND and responded to City comments.*

Poplar Street Pedestrian Pathway IS/MND; City of Half Moon Bay; Half Moon Bay, San Mateo County, California. SWCA prepared an MND and supporting technical studies, including a biological resources evaluation, sensitive plant survey, and cultural records search, in support of the proposed Poplar Street Pedestrian Pathway Project in the City of Half Moon Bay. The proposed Public Works Project includes the removal, replacement, and widening of an existing pedestrian pathway, approximately 1,010 feet long. In order to achieve CEQA compliance, SWCA prepared the MND and requisite technical studies for submittal to the Half Moon Bay Planning Department. SWCA prepared deliverables on an accelerated schedule and worked closely with the project planner to deliver defensible documents. *Role: Project Planner. Authored IS/MND and responded to City comments.*

City of Half Moon Bay Corporation Yard Constraints Map and Memorandum; City of Half Moon Bay; Half Moon Bay, San Mateo County, California. SWCA provided environmental consulting services for the City of Half Moon Bay Corporation Yard Constraints Review Project, including preparation of a constraints analysis for the Corporation Yard Parcel after the Planning Commission suggested using the remainder of the parcel for low-income housing. *Role: Project Planner. Authored constraints analysis sections for Hazards and Land Use & Zoning.*

City of Half Moon Bay Corporation Yard Upgrade Environmental Services; City of Half Moon Bay; Half Moon Bay, San Mateo County, California. SWCA prepared an MND and supporting technical studies, including a biological resources evaluation and cultural records search, in support of the proposed Half Moon Bay Corporation Yard Upgrade Project in the city of Half Moon Bay and within the Coastal Zone. The proposed Public Works Project included demolition and removal of some existing structures, construction of paved access roads and parking areas, a new tension fabric warehouse structure, a new trash and materials enclosure, solar yard, and wildlife corridor, including a wildlife tunnel under the road. In order to achieve CEQA compliance, SWCA prepared the MND and requisite technical studies for submittal to the Half Moon Bay Planning Department. SWCA worked closely with the Public Works Department, Planning Department, and the project planner to deliver defensible documents. *Role: Project Planner. Authored the IS/MND and responded to City and public comments.*

BRAD SOHM, P.E., AIR QUALITY, GHG EMISSIONS, ENERGY AND NOISE LEAD

Mr. Sohm is a Chemical Engineer and specializes in air quality compliance and permitting, noise impact analysis, health and safety, environmental site investigations, environmental remediation, and National Environmental Policy Act (NEPA) impact assessments. He also has extensive experience with state and county air quality permitting programs and compliance, including California Environmental Quality Act (CEQA) regulations; preparing non-Title V, Title V, and Prevention of Significant Deterioration (PSD) permits; fugitive dust control plans; completing technical review and data assessment of permitting air pollution control technologies to identify current best available control technology and lowest achievable emissions rate requirements for fuel-fired emission units; assisting with facility air permit audits to identify potential permit revisions/modifications; and other non-compliance issues. He has prepared and managed a wide range of air quality permitting projects for ethanol plants, refineries, various manufacturing facilities, slag recovery facilities, oil and gas projects, recycling facilities, and electric utilities, for a wide range of industrial, military, and utility clients throughout the country.

YEARS OF EXPERIENCE

19

EXPERTISE

Air quality permitting and compliance

Noise impact analysis

Health and safety

Environmental site investigations

Environmental remediation

Soil, groundwater, and asbestos sampling

Subcontractor oversight

Phase I Environmental Site Assessment

Spill Prevention Control and Countermeasures site inspection

EDUCATION

B.S., Chemical Engineering, option Environmental Engineering; University of Arizona, Tucson, Arizona; 2002

REGISTRATIONS / CERTIFICATIONS

Professional Engineer, Arizona No. 58554; 2014; Texas No. 119997; 2015; Professional Engineer, New Mexico No. 23408; 2016

TRAINING

EPA Method 9 Visible Emissions Training "Smoke School", Arizona Department of Environmental Quality/Arizona State University; 2013

SELECTED PROJECT EXPERIENCE

Pier 70 Mixed Use District Project Environmental Impact Report (EIR); FC Pier 70, LLC; San Francisco, San Francisco City and County, California. SWCA completed an EIR for a long-range project located on 69 acres of historic shipyard property along San Francisco's Central Waterfront, south of Mission Bay. The proposed project would introduce a new mixed-use innovation district intended for commercial and office uses, residential development, and a new retail and arts component, along with parking and new and expanded public parks and shoreline access. *Role: Air Quality Specialist. Responsible for technical support and review of the Air Quality section of the EIR.*

Cal Poly Student Housing South EIR; California Polytechnic State University; San Luis Obispo, San Luis Obispo County, California. SWCA prepared an EIR for the campus development project, certified in May 2014, which includes the construction of approximately 1,475 beds of freshman housing and a 300- to 500-space parking structure on the Cal Poly campus. *Role: Air Quality Specialist. Responsible for technical support and review of the Air Quality section of the EIR.*

Estrella Substation Project and Paso Robles Area Reinforcement Project; Pacific Gas and Electric Company; San Luis Obispo County, California. SWCA is providing planning and permitting support for a new 230-kilovolt (kV) substation and greenfield 70-kV power line in the Paso Robles area. Services include siting and alternatives analysis support for the substation; cultural, biological, and paleontological surveys for both project components; preparation of a Proponent's Environmental Assessment (PEA); and discretionary environmental permitting support. *Role: Air Quality and Noise Specialist. Responsible for the technical review of the Air Quality, Climate Change, and Noise sections of the PEA.*

Suncrest Dynamic Reactive Power Support Project; Confidential Client; San Diego County, California. SWCA is providing planning and permitting support for a dynamic reactive power support facility and associated 230-kV transmission line near Alpine, California. Services include routing and siting support; alternatives analysis; cultural, biological, and paleontological surveys; preparation of a PEA; and discretionary environmental permitting support. The application for Certificate of Public Convenience and Necessary was filed in summer 2015, and the PEA was deemed complete in December 2015 (Application No. A.15-08-027). *Role: Air Quality and Noise Specialist. Responsible for technical support and review of the Air Quality and Noise sections of the PEA.*

TRAINING, CONTINUED

AHERA Building Inspector Refresher, Environmental Protection Agency/The Asbestos Institute; 2008

40-hour Hazardous Materials Worker Training (HAZWOPER), OSHA; 2003; 8-hour refresher; 2010

Los Osos Wastewater Project CAMP; County of San Luis Obispo – Department of Public Works; Los Osos, San Luis Obispo County, California. Provided technical support and review pertaining to a Construction Activities Management Plan (CAMP) for a proposed wastewater project. *Role: Air Resource Specialist. Responsible for technical support and review of the CAMP.*

Air Quality Planning, Riverside Transmission Reliability Project; Riverside, California. Responsible for the preparation and review of a CEQA Analysis pertaining to air quality impacts for a sub-transmission line expansion project in southern California. *Role: Air Resource Specialist. Responsible for preparation and review of the Air Quality Technical Report in support of a CEQA Analysis.*

Cold Canyon Landfill Expansion Conditional Use Permit EIR; San Luis Obispo County; San Luis Obispo County, California. SWCA prepared an EIR evaluating the environmental impacts of the expansion of an existing 209-acre landfill in Edna Valley, 5 miles south of the city of San Luis Obispo. *Role: Air Quality Specialist. Responsible for technical support and review of the Air Quality section of the EIR.*

SCE Fort Irwin Reliability Project Environmental Assessment (EA); Southern California Edison Company; San Bernardino County, California. SWCA is providing support for this transmission line improvement project located on lands administered by the Bureau of Land Management (BLM) and Department of Defense, as well as private landowners. Services include the development of the BLM Plan of Development (POD), preparation of the EA, and biological, jurisdictional waters, cultural, and paleontological technical studies and reports to support the EA, POD, and environmental permits. *Role: Air Quality Specialist. Responsible for technical support and review of the Air Quality section of the EA.*

Bog Creek Road Data Collection, Endangered Species Consulting, and Environmental Impact Statement (EIS); U.S. Customs and Border Protection (CBP); Boundary County, Idaho. SWCA is currently assisting the Spokane Sector of CBP with NEPA compliance for the Bog Creek Road repair and maintenance project in the Idaho Panhandle National Forests to improve border security. SWCA advised and assisted CBP through the scoping process, prepared all scoping materials, facilitated the scoping meetings, and prepared the scoping summary report. SWCA collected field data and prepared reports for cultural, terrestrial, and aquatic resources; is completing the preliminary draft EIS; and is assisting with Endangered Species Act Section 7 analysis and reporting. The road is located within the Selkirk Grizzly Bear Recovery Zone; thus, the CBP, U.S. Forest Service, and U.S. Fish and Wildlife Service are coordinating to avoid potential impacts to the grizzly bear, a threatened species. *Role: Air Quality Specialist. Responsible for the preparation and review of the Air Quality and Climate Change and Soundscape sections.*

Northern Arizona Proposed Withdrawal EIS; BLM; Coconino and Mohave Counties, Arizona. SWCA was the primary contractor to the BLM and four federal cooperating agencies (U.S. Forest Service, National Park Service, U.S. Fish and Wildlife Service, and U.S. Geological Survey) to determine the potential impact of a 20-year withdrawal of approximately 1 million acres of federal lands from new mineral exploration and mining near Grand Canyon National Park. As a land withdrawal, a key component of the project is the potential land use in the absence of the withdrawal. The EIS process attracted widespread national and international interest, with nearly 400,000 individual comment submittals received during the scoping and public comment periods. *Role: Air Quality Specialist. Responsible for the preparation of the Air Quality, Climate Change, and Noise sections of the EIS.*

Ray Mine Supplemental EIS (SEIS); ASARCO LLC; Kearny, Multiple Counties, Arizona. SWCA is working to complete the Ray Land Exchange SEIS under the guidance of the Arizona State Office and Tucson Field Office of the BLM. The copper mining firm ASARCO LLC hopes to acquire up to 10,976 acres of BLM surface and mineral estate adjacent to its long-established Ray open-pit mine in east-central Arizona in exchange to BLM of approximately 7,304 acres of riparian and other sensitive native habitat, primarily in northwestern Arizona. *Role: Air Quality Specialist. Responsible for the preparation of the Air Quality and Climate Change section of the SEIS.*

GARRET ROOT, M.A., CULTURAL RESOURCES LEAD

Garret Root has 13 years of experience in cultural resources management, research, and regulatory compliance relating to the built environment. He has served as an architectural historian on a multitude of documentation projects, including those under Sections 106 and 110 of the National Historic Preservation Act, National Environmental Policy Act (NEPA), California Environmental Quality Act (CEQA), California Public Resources Code (PRC) 5024 and 5024.5, and several local preservation ordinances. Mr. Root has extensive knowledge of all facets of project development, including methodology development and area of potential effects (APE) development, research, intensive survey, field recordation and management, report documentation, and agency review. Mr. Root has extensive field knowledge in a variety of areas, including urban, rural, military, agricultural, and infrastructural settings. From this broad-based background, Mr. Root adds technical skill, efficiency, and strong regional knowledge to all his projects and documentation efforts.

YEARS OF EXPERIENCE

13

EXPERTISE

Historic resources surveys

NRHP

Historic preservation

EDUCATION

B.A., History; California State University, Chico; 2009

M.A., Public History; California State University, Sacramento; 2011

REGISTRATIONS / CERTIFICATIONS

Hydro Field Safety Certified, PG&E; Bay Area, California; 2017

TRAINING

PG&E Substation Safety Field Orientation for Contractors; California; 2018

SRI Foundation Section 106 and Bringing Projects to Completion, day 2; Sacramento, California, 2019

MEMBERSHIPS

Member, California Preservation Foundation

SELECTED PROJECT EXPERIENCE (* denotes project experience prior to SWCA)

1315 I Street Historic Resource Evaluation (HRE); HRGA; Sacramento, Sacramento County, California. SWCA prepared an HRE for a single commercial property in Sacramento. The project was prepared in accordance with CEQA and the City of Sacramento's design guidelines. The project involved documentation of the building, preparation of a California Department of Parks and Recreation (DPR) 523 form and memo, and evaluation using the significance criteria for the California Register of Historical Resources (CRHR). *Role: Senior Architectural Historian.*

Bellarmine College Preparatory HRE and CEQA Analysis; Bellarmine College Preparatory; Santa Clara County, California. SWCA prepared an HRE for three buildings on the Bellarmine High School Campus as part of the redevelopment of that segment of the campus. The project was prepared in accordance with CEQA and the City of San Jose. It included documentation, historic research and context development, preparation of three DPR 523 forms, and evaluation using the significance criteria for the CRHR and San Jose Historic Register. It included an impacts analysis for a historic property slated to be moved. *Role: Project Manager/Senior Architectural Historian. Prepared report and evaluations.*

Task Order 18 – Mills Drive and Park Drive – ASR, HPSR, HRER; Kern County Public Works Department; Kern County, California. SWCA is preparing various technical studies for NEPA compliance for the proposed project is to reconstruct and restripe the existing roadway, provide ADA improvements to the extent possible, and make improvements to drainage and sidewalks along Mills Drive. *Role: Architectural Historian. Preparing the Historic Resources Evaluation Report, which included documentation of nine properties. Leading a team of three architectural historians who are preparing the site records and writing the report.*

***Midway Village Redevelopment Project, Section 106 Inventory and Evaluation Report; MidPen Housing; Daly City, California.** Development of the APE, documentation of resources, development of historic context, oversight of junior staff,

development of Section 106 compliant report, and senior review. *Role: Senior Architectural Historian. Interacted with San Mateo County and the California State Historic Preservation Office on behalf of the client.*

***Mendocino Avenue Section 106 Report; Burbank Housing, Santa Rosa, California.** APE development, recordation of historic properties, development of historic context, National Register of Historic Places (NRHP) evaluation of resources, and development of

Section 106 compliant reports *Role: Senior Architectural Historian. Assisted in the development of the CEQA cultural resources section for this effort.*

***1322 El Camino Real Peer Review; City of Redwood City, Redwood City, California.** *Role: Senior Architectural Historian/Project Manager. Responsible for senior review of the project deliverables and with presentation of the project to the Redwood City Historic Preservation Commission.*

***Historic Properties Identification Report in Support of the Treasure Island Wastewater Treatment Plant Project; Woodard and Curran and San Francisco Public Utilities Commission; City and County of San Francisco, California.** Conducted fieldwork and oversaw preparation of a DPR 523 form to evaluate the Treasure Island Wastewater Treatment Plant that was originally constructed in 1961 for potential eligibility for listing in the NRHP and the CRHR. *Role: Senior Architectural Historian.*

***Cultural Resources Assessment Report Sobrante Water Treatment Plant Reliability Improvements Project; East Bay Municipal Utility District; El Sobrante, Contra Costa County, California.** Mr. Root conducted fieldwork, research, and preparation of a DPR 523 form to evaluate two sections of the East Bay Municipal Utility District Sobrante Water Treatment Plant, originally constructed in 1964, for potential eligibility for listing in the CRHR. The entire property was documented on DPR 523 forms for the purposes of compliance with CEQA. *Role: Senior Architectural Historian.*

***Historic and Archaeological Resources Report in Support of the South Lake Tahoe Master Plan; Ascent Environmental, Inc.; South Lake Tahoe, California.** Preparation of a joint report that recorded and evaluated 10 historic-age built environment resources/properties for eligibility for listing in the CRHR. The resources/properties were documented on California DPR 523 forms for the purposes of compliance with CEQA. The Osgood Toll House at the Lake Tahoe Historical Museum at 3058 Lake Tahoe Boulevard was recommended as a historic resource for the purpose of CEQA. *Role: Senior Architectural Historian. Conducted fieldwork, research, and preparation of joint report.*

***City of Sacramento McKinley Water Vault Project, EIR; City of Sacramento; Sacramento, California.** The highly publicized and controversial CEQA project required in-depth analysis of the proposed project and the effects on the cultural landscape associated with the historic McKinley Park. *Role: Senior Architectural Historian. Coordinated with City of Sacramento staff to craft a response to legal challenges to the project based on the merits of potential impacts to cultural resources. The approach and cultural resources response were upheld in court, and the project was allowed to proceed on schedule.*

***Cultural Resources Service and Standard Literature Search for Columbus Park Redevelopment Project; David J. Powers and Associates, San Jose, California.** Oversaw preparation of a letter report to evaluate the 9.9-acre Columbus Park for potential eligibility for listing in the CRHR. The proposed project would result in the demolition of the current buildings and baseball fields, the oldest of which were constructed between 1950 and 1956. The entire property was documented on California DPR 523 forms for the purposes of compliance with CEQA. *Role: Senior Architectural Historian/Principal in Charge.*

***Cultural Resources Assessment and Finding of Effect for the Burroughs Property Project; WestGate Ventures; Oakley, Contra Costa County, California.** Served as the senior technical expert, reviewed all project deliverables, consulted with the lead federal and state agencies, participated in client meetings, developed the Finding of Effect, and will prepare the mitigation measures. *Role: Senior Architectural Historian/Principal in Charge.*

***Destination Sacramento Waterfront Revitalization Project; City of Sacramento, Sacramento, California.** Development of scope and cost for architectural history tasks. The project involves historic resources redecoration, historic context development and assessment of impacts for CEQA. *Role: Senior Architectural Historian.*

***Aspire ERES Oakland Charter School Project; Aspire Charter Schools, Oakland, California.** Development of the APE, documentation of resources, development of historic context, oversight of junior staff, development of Section 106 compliant report, and senior review. *Role: Senior Architectural Historian. Mr. Root liaised with City of Oakland staff.*

***Historic Resource Inventory and Evaluation Report: Harris Yacht Harbor; Pacific Gas and Electric Company, Bay Point, California.** Survey of the Harris Yacht Harbor building, historic context development, and evaluation for the NRHP. *Role: Architectural Historian.*

LAUREN HUFF, M.S., BIOLOGICAL RESOURCES LEAD

Ms. Huff is a project manager and an ecological restoration and natural resources planning director in Northern California. She has been involved in all aspects of projects, including project management, field surveys, endangered and threatened species surveys, siting/constraints analyses, document preparation, agency coordination and negotiations, impact analysis, and mitigation planning. She has experience performing and managing reconnaissance-level site surveys, habitat assessments, wetland delineations, nesting bird surveys and monitoring, construction monitoring and permit compliance, vegetation management planning and implementation, and mitigation monitoring and compliance. She also has extensive experience in National Environmental Policy Act (NEPA), California Environmental Quality Act (CEQA), aquatic resources, Federal Endangered Species Act (FESA) Sections 7 and 10, and other permitting. Ms. Huff has extensive knowledge of federal and state regulations, including the FESA, Magnuson-Stevens Fisheries Act, Clean Water Act, Migratory Bird Treaty Act, Porter-Cologne Water Quality Control Act, California Endangered Species Act, and California Fish and Game Code.

YEARS OF EXPERIENCE

16

EXPERTISE

CEQA/NEPA planning and compliance

Environmental permitting

Vegetation management planning

FESA Sections 7 and 10

EDUCATION

M.S., Conservation Biology; University of Minnesota; 2009

B.S., Animal Physiology and Neuroscience; University of California–San Diego; 2001

REGISTRATIONS / CERTIFICATIONS

Adult First Aid/CPR/AED; 2019

Certified Wetland Delineator; 2011

PERMITS

CDFW Scientific Collection permit, Number S-213470005-21347-001.

TRAINING

Project Management; 2020

Advanced Hydric Soils; 2014

Endangered Species Act Section 7 Workshop; 2013

SELECTED PROJECT EXPERIENCE (* project experience prior to SWCA)

Los Gatos Vegetation Management Plan; Town of Los Gatos Department of Parks and Public Works; Los Gatos, Santa Clara County, California. SWCA is currently assisting the Town of Los Gatos with the development of a comprehensive Vegetation Management Plan (VMP) to promote wildfire safety along the town’s roadways and open space areas. *Role: Deputy Project Manager / Senior Biologist. Assisted with and managed preparation of Roadway VMP to address wildfire issues; assisted with and managed CEQA Statutory Exemption associated with roadway work; assisting with management and oversight of Open Space VMP, including ensuring that all management activities are aligned with California Vegetation Treatment Program (Cal VTP). Overseeing CEQA analysis through Cal VTP Project Specific Analysis process associated with Open Space VMP; and assisting with public outreach associated with project.*

* **Atherton Channel Maintenance, City of Menlo Park, San Mateo County.** Biological monitoring and permit compliance for the removal of debris and trash, in-stream vegetation, and roots on the side banks of the Atherton Channel in Menlo Park, California. These maintenance activities were required to maintain channel capacity and prevent flooding of adjacent areas in the vicinity of Atherton Creek. *Role: Project Manager/Senior Biologist. Scheduled pre-construction surveys, environmental trainings, and biological monitoring. Provided oversight of biological monitors and permit compliance. QA/QC of post-construction reporting documenting compliance with the Lake and Streambed Alteration Agreement. Provided guidance to the City on future permit strategies and regulations.*

Oceano Dunes State Vehicular Area Habitat Conservation Plan (HCP); MIG; San Luis Obispo County, California. SWCA is working as a subcontractor for team assisting the California Department of Parks and Recreation (CDPR) formulate a strategy to resolve conflicts between the presence of endangered species and recreation within an approximately 5,000-acre area in the Oceano Dunes District in San Luis Obispo County. *Role: Project Manager / Senior Biologist. Prepared HCP for CDPR for impacts to western snowy plover, California least tern, California red-legged frog, tidewater goby, and numerous listed plants. Prepared wildlife and vegetation sections of U.S. Fish and Wildlife Service (USFWS) Environmental Assessment (EA) for NEPA and biological portions of the CDPR Environmental Impact Report (EIR) for CEQA. Coordinated with USFWS on impacts and mitigation. Reviewed and provided guidance on previous informal consultation with National Oceanic and Atmospheric Administration’s National Marine*

Fisheries Service regarding Central California Coast steelhead in HCP area. Responding to public comments on the HCP, EA, and EIR. Assisting with discussions and negotiations with California Department of Fish and Wildlife (CDFW) regarding preparation of Natural Community Conservation Plan.

West Bay Sanitary District Menlo Park Equalization Basin Project; Freyer Laureta, Inc.; San Mateo County, California. SWCA is providing environmental consulting and restoration design services to the West Bay Sanitary District (WBSD) to facilitate flood protection and expansion of an existing wastewater facility along the San Francisco Bay shoreline. These improvements include the development of an ecotone slope that will enhance flood protection and provide resilient tidal marsh habitat as sea levels rise over the century. SWCA deliverables include complete plans, specifications, and costs estimates for the ecotone levee components of the project, as well as all required permit applications and environmental documentation for the project. SWCA helped WBSD secure a \$3.9 million National Fish and Wildlife Foundation (NFWF) Grant for implementation of the project. *Role: Project Manager and Principal Ecologist. Reviewed the biological resources evaluation for the project. Conducted a third-party review of the draft CEQA EIR for the project. Assisted with NFWF grant application, which was successfully secured for project implementation. Preparing permit applications and coordinating with the regulatory agencies for the project, including the Regional Water Quality Control Board (RWQCB) 401 Water Quality Certification; the U.S. Army Corps of Engineers (USACE) Standard Permit, including the alternatives analysis and adaptive management plan; and the San Francisco Bay Conservation and Development Commission Major Permit.*

Big Wave North Parcel Alternative Environmental Services; Big Wave LLC; Half Moon Bay, San Mateo County, California. SWCA is preparing the Environmental Report and other documentation for a NEPA Categorical Exclusion (CE) for the construction of an approximately 50,000-square-foot building with approximately 25,000 square feet of commercial space. The Environmental Report is being prepared for the U.S. Department of Agriculture (USDA) Rural Development-Business and Industry Loan Program in support of a loan for Big Wave, LLC, to construct the project. *Role: Senior Biologist / NEPA Lead. Conducted Section 7 FESA consultation with USFWS as USDA's non-federal representative and prepared environmental report in support of USDA's CE 1970.54: CEs Involving Small-Scale Development with Environmental Report.*

Butano Creek Backfield Floodplain Project Environmental Services; San Mateo Resource Conservation District; Half Moon Bay, San Mateo County, California. SWCA conducted a CEQA Exemption Analysis and prepared a cultural resources inventory for the restoration of floodplain habitat, improvement of instream complexity, and enhancement of dry-season instream flows along an 1,800-foot stretch of Butano Creek. *Role: Project Manager / Principal Restoration Ecologist. Managed biological and cultural resources evaluations; prepared Statutory Exemption for Restoration Projects under Cutting the Green Tape Effort for project, including coordinating with CDFW and obtaining CDFW concurrence; and assisting with project permitting, including obtaining USACE Nationwide Permit, RWQCB 401 Water Quality Certification under new Statewide General Order for Restoration, and CDFW Lake and Streambed Alteration Agreement.*

Purisima-to-the-Sea Parking Area and Trails Feasibility Study; Midpeninsula Regional Open Space District; San Mateo County, California. SWCA is providing a full-service design team to provide professional site planning, technical analysis, and design services to conduct a feasibility study for a new parking area, trailhead, trail connections and pedestrian crossings for the Purisima-to-the-Sea Project. The trail will be an approximately 15-mile regional trail that will provide east-west connections from Midpen's 5,038-acre Purisima Creek Redwoods Open Space Preserve to the San Mateo County coast, linking the Bay Area Ridge Trail along the Skyline corridor with the California Coastal Trail along the San Mateo coast. *Role: Permitting Specialist. Assisting and managing biological resources evaluation to identify sensitive biological resources along proposed trail alignments; preparing Biological Resources Evaluation to support project permitting and CEQA analysis; and leading effort to identify permits and engage resource agencies, including CDFW, California Coastal Commission, San Mateo County, USACE, and RWQCB.*

Carmel River Floodplain Restoration Project; California Coastal Conservancy; Carmel Valley, Monterey County, California. SWCA is providing environmental services to model, design, conduct environmental review, and permit restoration of an approximately 1-mile reach of the Carmel River in Palo Corona Regional Park. *Role: Project Manager / Permitting Specialist. Manages project; collaborates with the team and technical advisory committee; conducts public meetings; and oversees the biological resources study, aquatic resources delineation, permitting, and environmental review.*

* **Hercules New Town Center Project; City of Hercules; California.** CEQA documentation for the construction of a Safeway shopping center in the City of Hercules. *Role: Senior Biologist/Project Manager. Prepared the CEQA addendum to the February 2009 Final EIR for the project.*

CHARLOTTE SOERGEL, B.A., BIOLOGIST

Ms. Soergel is a biologist in SWCA's Half Moon Bay office and has experience biological monitoring for special-status species, including amphibians, fish, and bat species. She also conducts monitoring in sensitive communities, including riparian corridors, vernal pools, controlled burn areas, wetlands, and coastal bluffs. Her areas of expertise include habitat restoration, biological surveys (including nesting bird surveys), special-status flora and fauna surveys, wetland delineations, vegetation surveys and invasive species removal, compliance and construction monitoring, erosion and sediment control, and water quality monitoring. She has experience conducting surveys for, monitoring, and handling California red-legged frog, western toad, foothill yellow-legged frog, California tiger salamander, dusky footed wood rat, and several Bay Area bat species. Ms. Soergel has prepared and contributed to a variety of environmental documents, including Restoration Plans, CEQA Initial Studies/Mitigated Negative Declarations (IS/MNDs), jurisdictional wetland delineations, environmental permit applications, biological assessments, and natural resource management plans. She has experience with special-status species, including California red-legged frog, foothill yellow-legged frog, western pond turtle, California tiger salamander, burrowing owl, and San Francisco garter snake. She has 5 years of experience working and conducting surveys across California, including in Sacramento, Mendocino, Butte, Napa, Sonoma, Marin, Livermore, San Mateo, San Jose, and Kern County.

YEARS OF EXPERIENCE

5

EXPERTISE

Habitat Restoration
 Special-Status Species Surveys
 Natural Resource Surveys
 Permitting

EDUCATION

B.A. cum laude, Environmental Studies;
 University of California, Santa Barbara,
 California; 2017

REGISTRATIONS / CERTIFICATIONS

Certified Ecological Restoration
 Practitioner – In Training (CERPIT),
 2021

Certified Wetland Delineator; Jepson
 Herbarium; 2018

TRAINING

Bullfrog Control in California (CRLF ID
 and Handling, Bullfrog Eradication), The
 Wildlife Project; 2018

Intro to Botany, Jepson Herbarium,
 2019

QSP/QSD Training, Keish
 Environmental; 2020

SELECTED PROJECT EXPERIENCE (* denotes project experience prior to SWCA)

***Lower American River Natural Resource Management Plan; Sacramento County Regional Parks; Sacramento County, California.** The 23-mile-long American River Parkway is located in the city of Sacramento. The project boundary lies within the jurisdiction of the USACE and most of the parkway lies within the flood zone. The plan balances recreation uses with natural resource and cultural resource protection and considers numerous natural resource issues including fire in the parkway. The plan assists in identifying potential locations where restoration activities would be appropriate and evaluates where bank protection enhancements may take place. The plan also considers the role of gravel augmentation in providing habitat for fish species in the Wild and Scenic River. *Role: Conducted research and reporting for the 2019 Natural Resource Management Plan update.*

***Puente Hills Recreational Survey; Puente Hills Habitat Authority; Whittier, California.** *Role: Biologist. Conducted recreational use surveys and data collection for the Habitat Authority (2016-2018). Performed data analysis and assisted with reporting for the Trail Use Management Plan.*

Los Gatos Vegetation Management Plan; Town of Los Gatos - Dept of Parks and Public Works; Los Gatos, Santa Clara County, California. SWCA is currently assisting the Town of Los Gatos with the development of a comprehensive Vegetation Management Plan to promote wildfire safety along the town's roadways and open space areas. SWCA's restoration ecologists, arborists, and biologists are currently mapping existing native and non-native vegetation communities and sensitive resources to establish the baseline conditions within the town, which will then be utilized by our fire planning experts to assess treatment techniques that can balance safety of the built environment with the enhancement and restoration of the natural environment. *Role: Biologist. Conducted field assessments, assisted with the Vegetation Management Plan, and assisting with the preparation of the Project Specific Assessment (PSA).*

Vineyard RV Flood Capacity Enhancement and Restoration Project; Vineyard RV; Vallejo, California. *Role: Biologist. Performed and assisted with nesting bird surveys, restoration plan implementation and monitoring, and Stormwater Pollution Prevention Plan monitoring. Assist with annual reporting to resource agencies and will assist with future beaver management planning.*

GTL Linear Projects; PG&E; Bay Area, California. SWCA provided on-call services for PG&E throughout the Bay Area. Projects included conducting biological surveys, nesting bird surveys, wetland delineations, monitoring vegetation removal for fire management, and conducting surveys for sensitive species, including California tiger salamander. Project work was conducted in Mendocino, Sonoma, and Marin Counties.

***Sonoma County On-Call; County of Sonoma; Sonoma, California.** Conducted biological surveys for sensitive flora and fauna species, environmental compliance, and CEQA reporting. *Role: Biologist. Conducted monitoring for special-status flora and fauna species including California tiger salamander and California red-legged frog, habitat assessments including assessments for several bat species, environmental and permit compliance surveys, and wrote material for CEQA IS/MNDs. Surveys were conducted in pre-construction and often post-burn conditions as well as for cannabis operations.*

Purisima to the Sea Parking Area and Trails Feasibility Study; Midpeninsula Regional Open Space District; San Mateo County, California. SWCA is providing a full-service design team to provide professional site planning, technical analysis, and design services to conduct a feasibility study for a new parking area, trailhead, trail connections and pedestrian crossings for the Purisima-to-the-Sea Project. The Purisima-to-the-Sea Trail will be an approximately 15-mile regional trail that will provide east-west connections from Midpen's 5,038-acre Purisima Creek Redwoods Open Space Preserve to the San Mateo County coast, linking the Bay Area Ridge Trail along the Skyline corridor with the California Coastal Trail along the San Mateo coast. *Role: Biologist. Conducted biological assessments and reporting for a trail feasibility study.*

Tecopa Hot Springs Wetland Restoration; UC Davis; Inyo County, California. SWCA is providing grant writing and permitting support, environmental review, and ecological restoration to enhance habitat for the endangered Amargosa vole. *Role: Biologist. Conducted biological surveys and wetland delineation, and assisting with restoration design and planning.*

Carmel River Floodplain Restoration; McBain Associates; Monterey County, California. SWCA is providing environmental services to model, design, and permit restoration of an approximately 1-mile reach of the Carmel River in the Rancho Cañada Unit, Palo Corona Regional Park. The project will restore the floodplain of the river to foster spawning and rearing of steelhead. *Role: Biologist. Conducted visual biological surveys for special-status species including California red-legged frog and steelhead in the Carmel River for a floodplain restoration project.*

Big Wave Wellness Center Wetland Restoration Project; Big Wave LLC; San Mateo County, California. SWCA is preparing the Environmental Report and other documentation for a NEPA Categorical Exclusion for the construction of an approximately 50,000-square-foot building with approximately 25,000 square feet of commercial space. The Environmental Report is being prepared for the U.S. Department of Agriculture Rural Development-Business and Industry Loan Program in support of a loan for Big Wave, LLC to construct the project. *Role: Biologist. Prepared the project freshwater wetland restoration plan.*

North Shoreview Levee Improvement Project; 4Leaf; San Mateo County, California. SWCA is providing pre-construction biological surveys, biological monitoring, and reporting services for the project, which involves replacing two pump stations with new structures, pumps, electrical equipment, and discharge piping to San Francisco Bay and raising approximately 1,300 feet of levee by approximately 3 to 5 feet in San Mateo that is deemed insufficient by the Federal Emergency Management Agency to protect against 100-year storm events. SWCA conducted preconstruction surveys for Ridgway's rail, special-status plants, nesting birds, and bats. *Role: Lead Environmental Inspector. Conducted pre-construction surveys for nesting birds and bats, conduct daily inspections during Ridgway's rail season.*

Chico Pond Restoration; Northern California Law Group; Chico, Butte County, California. Water quality, biological assessment, and restoration feasibility plan for a post-burn area at a private residence in Chico, California. The property contained a pond that had experienced high-quantity inputs of ash and debris as a result of the Camp Fire. EPA 525.3 protocol-level water quality monitoring was conducted by SWCA, and samples were processed by a local laboratory. A feasibility analysis was conducted to assess the potential for restoration of the impacted vegetation and pond. *Role: Biologist. Conducted comprehensive water quality monitoring, including turbidity, pH, temperature, dissolved oxygen, total dissolved solids, and oxidation-reduction potential, for a pond impacted by the Camp Fire in Chico, California. Prepared restoration feasibility study.*

CHRISTINA ALONSO, M.A., RPA, ARCHAEOLOGIST

Ms. Alonso, a Senior Project Manager out of SWCA's Half Moon Bay office, has 13 years of experience in cultural resource management with investigations in support of CEQA, NEPA, and Section 106 of the National Historic Preservation Act (NHPA). Her professional experience includes all phases of survey, excavation, laboratory analysis, research design, report preparation, construction monitoring, Native American consultation, and project management. She has prepared numerous technical reports and environmental documents for compliance with CEQA, NEPA, and NHPA Sections 106 and 110. Ms. Alonso has conducted projects involving gas and electric, utilities, and private developers in cooperation with agencies such as the Bureau of Land Management, California Energy Commission, USACE, and U.S. Forest Service.

YEARS OF EXPERIENCE

13

EXPERTISE

CEQA / NEPA compliance

Archaeological excavation, testing, and data recovery

National Historic Preservation Act compliance

Archaeological monitoring

EDUCATION

M.A., Anthropology, Bioarchaeology; San Francisco State University; 2013

B.A., Anthropology; California State University, Chico; 2010

REGISTRATIONS / CERTIFICATIONS

Registered Professional Archaeologist; 2015

MEMBERSHIPS

Member, Society for California Archaeology

SELECTED PROJECT EXPERIENCE (* project experience prior to SWCA)

Carmel River Floodplain Restoration Project Environmental Services; McBain Associates; Monterey County, California. SWCA is currently providing environmental services to model, design, and permit restoration of an approximately 1-mile reach of the Carmel River in the Rancho Cañada Unit, Palo Corona Regional Park. SWCA is collaborating with McBain Associates and a 20-person technical advisory committee for holistic habitat restoration of 190 acres of former golf course to provide vital linkage from the park to Big Sur. *Role: Archaeologist. Senior Archaeologist responsible for reporting and senior review.*

SCVOSA Coyote Valley Conservation Areas Master Plan Environmental Services; Santa Clara Valley Open Space Authority; Santa Clara County, California. SWCA is currently assisting the Santa Clara Valley Open Space Authority with the development of a comprehensive conservation area master plan to provide an innovative, integrated, science-based, community-informed plan for recently conserved lands in Coyote Valley. SWCA will be providing comprehensive natural and cultural resources, ecological restoration, open space and recreation planning, environmental permitting and review, community engagement, and management services throughout the Master Plan process to facilitate communications between the team, Authority staff, and project stakeholders. *Role: Archaeologist. Senior archaeologist responsible for assisting with cultural resources analysis and Tribal engagement.*

*** Oakland Unified School District Cultural Resources Study; Lamphier-Gregory; Oakland, Alameda County, California.** *Role: Project Manager. Served as project manager and field supervisor for field survey and technical report preparation. Responsibilities changed with needs of project; everyday duties included crew management, field direction, data management, documentation, and report preparation.*

*** Gateway Crossings Building 1 and 2 Archaeological Services; Holland Partner Group; Santa Clara, Santa Clara County, California.** *Role: Project Manager. Drafted*

archaeological treatment plan and archaeological testing plan, conducted site sensitivity assessments and historic trenching, supervised field efforts, and prepared report. Responsibilities changed with needs of project; everyday duties included crew management, field direction, data management, and report preparation.

*** Oakley Creekside Park Archaeological Services; Restoration Design Group, Inc.; Oakley, Contra Costa County, California.** *Role: Project Manager. Served as project manager and field supervisor for field survey and Section 106 technical report preparation for the U.S. Army Corps of Engineers. Responsibilities changed with the needs of the project and everyday duties included crew management, field direction, data management, documentation, and report preparation.*

CARLOS ITUARTE VILLARREAL, PH.D., AIR QUALITY, GHG EMISSIONS, ENERGY, AND NOISE PLANNER

Mr. Ituarte-Villarreal is an environmental specialist with experience providing noise permitting, modeling, engineering, and compliance services. Mr. Ituarte-Villarreal is an engineer with research interests in the areas of energy systems optimization, reliability and maintainability, and systems optimization. Mr. Ituarte-Villarreal has conducted numerous acoustical surveys to determine baseline and modeled noise levels for projects. He has worked with Ms. Gladding on developing action area sizes for indirect impacts to wildlife species based on modeled noise-levels from construction equipment for NEPA-required analyses. These analyses have been used for Section 7 consultations, including documentation used between the U.S. Forest Service (for a confidential mining project) and the USFWS, and between the BLM and the USFWS (on a confidential natural gas pipeline repair project).

YEARS OF EXPERIENCE

8

EXPERTISE

Emissions Inventory

Environmental Permitting, Engineering and Modeling

Carbon Footprinting

Noise Impact Assessment and Monitoring

Environmental Impact Assessment

Conventional and Renewable Energy

Title V Permitting

CAA Compliance and Reporting

Sustainable Energy

Wind Turbines

EDUCATION

Ph.D., Environmental Science & Engineering, Energy Science; The University of Texas at El Paso; El Paso, Texas; 2015

M.S., Industrial Engineering; The University of Texas at El Paso; El Paso, Texas; 2010

B.S., Industrial Engineering; Instituto Tecnológico de Parral; Mexico; 2008

TRAINING

Lean Manufacturing, TMAC; 2012

AERMOD Air Dispersion Modeling, Lakes Environmental; 2013

SELECTED PROJECT EXPERIENCE (* denotes project experience prior to SWCA)

County of San Mateo EIR for Canyon Lane Roadway Improvements; County of San Mateo; Redwood City, San Mateo County, California. SWCA is preparing an EIR and technical background studies for the project for the improvement of Canyon Lane. Project includes development of a single-family residence on one parcel, and future development of residences on eleven parcels. The project also involves the construction of new utilities, including a waterline and an underground distribution line. *Role: Environmental Specialist.*

Middlefield Road Parking Lot Project, County of San Mateo; Department of Public Works. SWCA is assisting the County with preparation of an environmental document to support the construction of a parking lot on lands in the North Fair Oaks community previously occupied by commercial uses. The project will support the Middlefield Road Redesign project by off-setting street parking eliminated by the redesign project and will also create a flexible space to support community events such as farmers markets. *Role: Environmental Specialist. Participated in the Noise and Vibration impact analysis for the construction and operation phases of the project.*

SCE Large Capital On-Call Environmental Consulting Services; Multiple Counties in California - SWCA is under a three-year on-call contract, to provide environmental consulting services (biological, cultural and paleontological, water quality/wetland permitting, licensing), for moderate to large scale utility system upgrades and improvements (~\$300,000-\$3M) for SCE's Transmission and Distribution Organizational Unit. Projects include but are not limited to: support for CPUC licensed projects, PEA development, National Environmental Policy Act (NEPA) projects, renewable interconnections, execution of environmental mitigation, and environmental oversight during construction. *Role: Air Quality and Noise Specialist. Has worked on CWA L007a GO 131D, CWA L007c GO 131D, and CWA L007d GO 131.*

Antelope Expansion 3 Project Environmental Services; sPower Development Company, LLC; Los Angeles County, California. SWCA is providing biological technical reports for the utility project located in Los Angeles County. *Role: Environmental Specialist.*

Estrella Substation and Paso Robles Area Reinforcement Project; Confidential Client; Paso Robles, San Luis Obispo County, California. Confidential

Transmission Project; Confidential Clients; California. SWCA is providing permitting and licensing support, including a preparation of a PEA, for a new 230/70 kV substation, 7 miles of new aboveground 70 kV power line, 3 miles of reconducted 70 kV line, and a 230 kV interconnection in Paso Robles. Services include cultural, biological, and paleontological surveys; PEA preparation; PTC application filing support and noticing; and post-filing CEQA and permitting support. *Role: Environmental Specialist.*

Marquette Residential Development; Pizzulli Associates, Inc.; Los Angeles, Los Angeles County, California. SWCA prepared an MND and supporting technical studies, including a tribal cultural resources study, historical resources assessment, biological assessment, and air quality analysis, in support of the proposed Marquette Residential Development Project in the Pacific Palisades neighborhood of the City Los Angeles. The proposed project includes the demolition of two residences and the construction of eight new single-family homes. In order to achieve CEQA compliance, SWCA prepared the MND and requisite technical studies for submittal to the Los Angeles Department of City Planning. SWCA prepared deliverables on an accelerated schedule and worked closely with the project owner to deliver defensible documents. *Role: Environmental Specialist. Led the preparation of an air quality assessment and associated emissions inventory.*

High Speed Rail CP4 NEPA/CEQA Re-Examination; Confidential Client; Multiple Counties, California. – Construction Package 4 (CP 4) is the third design-build construction contract for the high-speed rail program. The California High-Speed Rail Authority proposes to construct, operate, and maintain an electric-powered high-speed rail system in California. When completed, the nearly 800-mile high-speed train system will provide new passenger rail service to California's major metropolitan areas and through the counties that are home to more than 90% of the state's population. The CP4 construction area is a 22-mile stretch within the counties of Tulare and Kern and the cities of Wasco and Shafter. CP 4 work will include construction of at-grade, retained fill and aerial sections of the high-speed rail alignment, relocation of four miles of existing Burlington Northern Santa Fe (BNSF) tracks, construction of waterway and wildlife crossings and roadway reconstructions, relocations and closures. This phase of construction received state and federal environmental clearances in 2014 and is estimated to cost \$400-500 million. *Role: Environmental Specialist.*

PG&E Estrella Substation and Paso Robles Area Reinforcement Project Proponent's Environmental Assessment; Confidential; Paso Robles, San Luis Obispo County, California. SWCA provided planning and permitting support for a new 230-/70-kV substation, 8 miles of new above-ground 70-kV power line, 6 miles of reconductoring a 70-kV line, and a 230-kV interconnection. Services included cultural, biological, and paleontological resource surveys; PEA preparation; certificate of public convenience and necessity application filing and noticing, and post-filing California Environmental Quality Act (CEQA); and permitting support. *Role: Environmental Specialist.*

Pajaro Valley Water Management Agency CEQA Support and Permitting; Pajaro Valley Water Management Agency; Monterey And Santa Cruz Counties, California. SWCA provided environmental permitting and document sufficiency assessments for three water management projects located in the Watsonville area. The projects entailed construction of additional water storage tanks and upgrades to the agency's existing water treatment facility and the addition of two new distribution pipelines to better serve Santa Cruz and Monterey County customers. SWCA also prepared a CEQA plus environmental document addendum, Section 106 and CEQA archaeological surveys and reports, consultation assistance with the SHPO, a wetland delineation, biological technical reporting, permitting, and preparation assistance for funding applications. *Role: Environmental Specialist.*

South Lake Solar Project; County of Fresno Planning Department; Fresno, Fresno County, California. SWCA is preparing an Environmental Impact Report (EIR) for an up to 80 megawatt (MW) photovoltaic solar energy facility, up to 80 MW battery storage system, and 70 kilovolt (kV) overhead generation tie line on an approximately 585 acres located in western Fresno County, California. Key issues addressed in the EIR included air quality impacts resulting from construction emissions, biological impacts from the loss of Swainson's hawk foraging habitat, and the potential for cumulative impacts caused by the project. *Role: Environmental Specialist.*

Suncrest Project Environmental Planning and Compliance Services; NEET West, LLC; Multiple Counties, California. SWCA provided planning and permitting support for a dynamic reactive power support facility and associated 230 kV underground electric transmission line near Alpine, San Diego County, CA. Services included routing and siting support; alternatives analysis; resource surveys; preparation of a PEA; post-filing CEQA support; and discretionary environmental permitting. SWCA is currently providing environmental compliance management and inspection during construction. *Role: Environmental Specialist.*

ERIN WIELENGA, B.S., AIR QUALITY, GHG EMISSIONS, ENERGY, AND NOISE PLANNER

Erin Wielenga is an air quality specialist with 10 years of experience preparing air quality assessments and providing air quality permit services, including preparing regulatory and non-regulatory air pollutant emission inventories and conducting air quality dispersion modeling. As an Air Quality Specialist, Ms. Wielenga is experienced in conducting the air quality permitting, compliance, and reporting driven by state, federal, and local air quality rules and regulations. Ms. Wielenga has a demonstrated ability to evaluate project impacts with respect to Federal Energy Regulatory Commission Resource Reports, the National Environmental Policy Act (NEPA), and the California Environmental Quality Act (CEQA) and to write air quality and GHG resource sections for these programs. She possesses good technical and project management skills and has the ability to successfully manage multiple projects and daily responsibilities. She is self-motivated and is skilled in developing and fostering creative and innovative solutions.

YEARS OF EXPERIENCE

10

EXPERTISE

Air modeling and analysis of results

Project acquisition and management

Preparation of air quality permit applications

U.S. Environmental Protection Agency, federal, state, and local regulations

Preparation of CEQA and NEPA air quality assessments

Preparation and adherence to scope/budget/schedule

Proficiency in AERMOD computer modeling software

Emission estimates (AP-42, CARB, emission controls)

EDUCATION

B.S., Botany; California State University, Chico, California; 2008

TRAINING

CalEEMod Training, South Coast Air Quality Management District; 2011

Fundamentals of Air Dispersion Modeling, Trinity Consultants; 2015

Practical Air Dispersion Modeling Workshop, Trinity Consultants; 2015

SELECTED PROJECT EXPERIENCE

Vikings Solar Technical Studies; Apex Energy Solutions, LLC; Imperial County, California. SWCA conducted studies to support an environmental impact report (EIR). *Role: Air Quality Specialist. Conducted air quality and GHG technical studies using CalEEMod.*

Zuzu Solar Project Permitting; Heliogen SR1, LLC; Kern County, California. SWCA conducted studies to support an EIR for a 5-MW concentrated solar and photovoltaic project located in unincorporated Kern County. *Role: Air Quality Specialist. Conducted air quality and GHG technical studies using CalEEMod.*

880 Doolittle Drive Initial Study; City of San Leandro; San Leandro, Alameda County, California. SWCA assisted the City of San Leandro in the peer review of applicant-prepared technical background studies and supporting documentation. The SWCA team reviewed the Air Quality, Greenhouse Gas, and Energy technical background studies and prepared a memorandum summarizing the results of their peer reviews. *Role: Air Quality Specialist. Peer review of prepared assessments.*

841 Patricia Initial Study; City of San Luis Obispo; San Luis Obispo County, California. The SWCA team conducted CalEEMod modeling for the air quality and GHG portions of the Initial Study/Mitigated Negative Declaration (IS/MND) to make significance determinations for the project. *Role: Air Quality Specialist. Air quality and GHG CalEEMod modeling and IS/MND Air Quality and GHG sections.*

Norumbega Drive Residence Initial Study; City of Monrovia, Los Angeles County, California. SWCA assisted in the preparation of an Initial Study to determine potential environmental impacts of the proposed project. *Role: Air Quality Specialist. Prepared the Air Quality, Greenhouse Gas, and Energy studies.*

Estrella Substation and Paso Robles Area Reinforcement Project; Confidential Client; Paso Robles, San Luis Obispo County, California. Confidential Transmission Project; Confidential Clients; California. SWCA is providing permitting and licensing support, including a preparation of a Proponent's Environmental Assessment (PEA), for a new 230/70-kV substation, 7 miles of new above-ground 70-kV power line, 3 miles of reconducted 70 kV line, and a 230-kV interconnection in Paso

Robles. Services include cultural, biological, and paleontological surveys; PEA preparation; PTC application filing support and noticing; and post-filing CEQA and permitting support. *Role: Air Quality Specialist.*

Morehouse Community Medical Centers Environmental Assessment; CommuniHealth Services; Bastrop, Morehouse Parish, Louisiana. This project involves construction of a state-of-the-art clinic that would consolidate three stand-alone sites (a pediatric clinic, a family practice clinic, and an administrative site) and allow for the following: the expansion of medical, dental, and behavioral health services; on-site access to pharmacy, radiology, and laboratory services; and community space that can be converted to a 25-bed shelter to house people who need low-level medical attention during an emergency. The Federal Emergency Management Agency issued a disaster declaration for Louisiana Flooding (DR-4462-LA) making grant money available through the Health Resources and Services Administration FY20 Capital Assistance for Disaster Response and Recovery Efforts Program for the project. *Role: Air Quality Specialist.*

Skeleton Creek Environmental Impact statement (EIS); Confidential Client; Garfield County, Oklahoma. SWCA conducted environmental and social impact assessment, cumulative effects assessment, public involvement, and interagency coordination, and compiled the results of these efforts in an EIS. *Role: Air Quality Specialist. Prepared the air quality and GHG assessment for the EIS.*

Capella Solar Project Critical Issues Analysis; Heliogen SR1, LLC; Kern County, California. SWCA prepared a Critical Issues Analysis and technical studies for a conditional use permit application for an approximately 120-acre, 5 MW concentrated solar and photovoltaic project located in unincorporated Kern County. *Role: Air Quality Specialist. Conducted air quality and GHG technical studies using CalEEMod.*

Oak Creek Delivery Meter Station; Confidential Client; Kern County, California. SWCA completed an environmental report as required for applications involving a new meter station outside the right-of-way of a regulated natural gas pipeline, which included preparing 13 resources reports, as part of a Prior Notice Filing with the Federal Energy Regulatory Commission (FERC). In addition, SWCA completed environmental permitting tasks under CEQA. *Role: Air Quality Specialist. Completed FERC air and noise resource report.*

BLM New Mexico 2021 and 2022 Lease Sale NEPA Support; U.S. Bureau of Land Management; County, New Mexico. SWCA continued work with the BLM New Mexico State Office to further develop and support issues-based EAs for oil and gas lease sales for the BLM in New Mexico. *Role: Air Quality Specialist. Presented, discussed, and summarized technical information on air quality, air quality-related values, GHG emissions, and climate change relative to air resources with BLM New Mexico.*

BLM Arizona Lease Sale NEPA Support; U.S. Bureau of Land Management; County, Arizona. SWCA continued work with BLM to further develop and support issues-based EAs for oil and gas lease sales for the BLM in Arizona. *Role: Air Quality Specialist. Presented, discussed, and summarized technical information on air quality, air quality-related values, GHG emissions, and climate change relative to air resources with BLM Arizona.*

BRITTNEY SLUCHER, B.S., PROJECT ADMINISTRATION

Ms. Slucher is the senior administrative specialist for SWCA’s Half Moon Bay office and has over 3 years of experience, including organizing and implementing field team staffing, support in recruitment and hiring, timesheet and attendance, expenses/allowances, and payroll questions. She also supports project set-up, health and safety compliance, file organization, and documentation management. She has experience reviewing and compiling data, establishing schedules, and arranging meetings. Additionally, Ms. Slucher has recent experience as an Associate Planner with the City of San Leandro. She supports the intake and review of building permits and business license applications and assists the public with any questions related to the zoning code at the City’s Permit Counter. Ms. Slucher also has experience in geological oceanographic research pertaining to sediment transport in coastal environments in the Puget Sound.

YEARS OF EXPERIENCE

3

EXPERTISE

Coordination

Scheduling

Document Control

Job Hazard Analysis Documents

Project Set-up

EDUCATION

B.S., Oceanography; University of Washington, Seattle, Washington; 2018

REGISTRATIONS / CERTIFICATIONS

Adult First Aid/CPR/AED Certified, California No. 00JRU1; American Red Cross; 2021

SELECTED PROJECT EXPERIENCE

City of San Leandro On-Call Planning Assistance; City of San Leandro; San Leandro, Alameda County, California. SWCA is providing on-call planning assistance to the City of San Leandro Community and Development Department. *Role: Associate Planner. Receives, routes and reviews applications; logs calls and responds to phone messages and emails; reviews electronic business licenses; and performs basic plan checks.*

880 Doolittle Drive CEQA Support; City of San Leandro; San Leandro, Alameda County, California. SWCA is preparing an MND for a proposed redevelopment project in the city of San Leandro. The project would demolish four existing buildings, construct a single 98,015 square-foot warehouse building with a total building area of 106,747 square feet, and include surface parking spaces, landscaping, and stormwater improvements. *Role: Administrative Support. Conducting file management and tracking.*

Butano Creek Backfield Floodplain Project; San Mateo Resource Conservation District (RCD); San Mateo County, California. SWCA conducted a CEQA Exemption Analysis and prepared a cultural resources inventory for the restoration of floodplain habitat, improvement of instream complexity, and enhancement of dry-season instream flows along an 1,800-foot stretch of Butano Creek. *Role: Administrative Support. Conducting file management and tracking.*

Coyote Creek Flooding Expert Testimony; Needham, Kepner and Fish LLP; San Jose, Santa Clara County, California. SWCA provided expertise in hydrology, hydraulics, geomorphology, GIS, and flood risk management to assist in the successful out-of-court settlement of a lawsuit filed by flood victims of the city of San Jose against the regional water management agency, Valley Water. SWCA partnered with G. Mathias Kondolf, PhD, professor of geomorphology at UC Berkeley on the effort, which included field reconnaissance, field data collection, GIS, thorough review of depositions and supporting documentation, hydrologic analysis, and hydraulic modeling. *Role: Administrative Support. Provided safety coordination and logistics support.*

Estrella Substation and Paso Robles Area Reinforcement Project; Confidential Client; Paso Robles, San Luis Obispo County, California. Confidential Transmission Project; Confidential Clients; California. SWCA is providing permitting and licensing support, including a preparation of a PEA, for a new 230/70 kV substation, 7 miles of new above ground 70 kV power line, 3 miles of reconducted 70 kV line, and a 230 kV interconnection in Paso Robles. Services include cultural, biological, and paleontological surveys; PEA preparation; PTC application filing support and noticing; and post-filing CEQA and permitting support. *Role: Administrative Support. Providing safety coordination, logistics support, and recordkeeping.*

Kinder Morgan Product Pipelines Seam Weld Metal Loss Integrity Dig Environmental Services; Kinder Morgan Product Pipelines; Harris County, Texas. SWCA is assisting with environmental permit clearances for roughly 1,800 pipeline anomalies across Kinder Morgan’s U.S. liquids assets. SWCA is providing natural and cultural resources, CEQA, wetlands, and permitting services. Several projects are located along the San Francisco Bay shoreline and require Clean Water Act, CDFW Section 1602, and San Francisco Bay Conservation and Development Commission (BCDC) permitting. *Role: Administrative Support. Providing safety coordination, logistic support, recordkeeping, and project coordination.*

JENNIFER PEREZ TORRE, B.A., PROJECT CONTROLLER

Ms. Perez Torre is a dual-function employee with a diverse professional background who performs the role of a Senior Project Controls Analyst and Assistant Project Manager. She has 26 years of administrative experience, including supporting and assisting attorneys in case management, logistics, accounting, record keeping, and policy analysis. As a Senior Project Controls Analyst, she tracks project financial metrics; presents financial analysis to key stakeholders and clients; reviews and identifies opportunities to improve project financials; prepares budgets and forecasts, ensuring all commitments are met in accordance with the contract; provides invoice oversight; prepares scope changes; and executes various administrative project management tasks.

As an Assistant Project Manager, she organizes and implements field team staffing. She also manages project set-up and review, resources allocation and multi-project scheduling, health and safety compliance, file organization and documentation management, and project closeout. She is experienced in reviewing and compiling technical data, establishing project schedules, and preparing status reports.

YEARS OF EXPERIENCE

27

EXPERTISE

Coordination

Scheduling

Budget tracking and forecasting

Large document control

Policies and procedures

Preparing agendas

Local, state, and federal court rules and procedures

EDUCATION

B.A., Psychology; Texas Tech University, Lubbock, Texas; 2003

TRAINING

Project Management Bootcamp, PSMJ Resources, Inc.; 2020

SELECTED PROJECT EXPERIENCE

Magnolia Street IS/MND; City of Half Moon Bay; Half Moon Bay, San Mateo County, California. SWCA prepared an MND and supporting technical studies, including a biological resources evaluation, addendum to the biological resources evaluation, and wetland and waters delineation and assessment, in support of the construction of two residences and a fire truck turnaround on a dead-end street in the city of Half Moon Bay. In order to achieve CEQA compliance, SWCA prepared the MND and requisite technical studies for submittal to the City of Half Moon Bay Planning Department. SWCA prepared deliverables on an accelerated schedule and worked closely with the project planner to deliver defensible documents. *Role: Project Controls Analyst/Assistant Project Manager. Provided staff coordination to meet deliverables. Prepared progress reports. Managed budget tracking, invoicing, and forecasting.*

Seymour Street IS/MND; City of Half Moon Bay; Half Moon Bay, San Mateo County, California. SWCA prepared an MND and supporting technical studies, including a biological resources evaluation, in support of the proposed Seymour Street Parcel Map Project in the City of Half Moon Bay. The proposed project includes the construction of two residences. In order to achieve CEQA compliance, SWCA prepared the MND and requisite technical studies for submittal to the Half Moon Bay Planning Department. SWCA prepared deliverables on an accelerated schedule and worked closely with the project planner to deliver defensible documents. *Role: Project Controls Analyst/Assistant Project Manager. Provided staff coordination to meet deliverables. Prepared progress reports. Managed budget tracking, invoicing, and forecasting.*

Big Wave North Parcel Alternative Environmental Services; Big Wave LLC; Half Moon Bay, San Mateo County, California. SWCA is preparing the Environmental Report and other documentation for a NEPA Categorical Exclusion for the construction of an approximately 50,000-square-foot building with approximately 25,000 square feet of commercial space. The Environmental Report is being prepared for the U.S. Department of Agriculture (USDA) Rural Development-Business and Industry Loan Program in support of a loan for Big Wave, LLC to construct the project. Services being provided by SWCA include preparation of the Environmental Report, U.S. Fish and Wildlife Service Section 7 Endangered Species Act consultation as the designated representative for USDA, preparation of the USDA Natural Resources Conservation Service Form AD-1006 Farmland Conversion Impact Rating to evaluate impacts to prime farmland, ongoing coordination with USDA, and construction monitoring. *Role: Project Controls Analyst/Assistant Project Manager. Manage budget, invoicing and forecasting. Provide staff coordination to meet deliverables.*

LIZ HAINES, B.A., TECHNICAL EDITOR

Ms. Haines has over 30 years of experience as an editor and publications manager for environmental reports, technical documents, and business and marketing communications. She has extensive experience producing well-written environmental documents that meet the San Francisco Planning Department’s standards for document quality pertaining to content, completeness, consistency, clarity, readability, and organization. Ms. Haines understands that the quality of documents rests on thoughtful, precise analysis carried forward by clear and concise writing, consistent fact and detail, and excellent organization and presentation. She is experienced in all phases of review and production of environmental impact reports (EIRs) and other types of environmental reports.

YEARS OF EXPERIENCE

30

EXPERTISE

EIR content and presentation

Editing and writing

Production coordination

EDUCATION

B.A.; Hunter College, City University of New York; 1975

SELECTED PROJECT EXPERIENCE (* denotes project experience prior to SWCA)

Pier 70 Mixed Use District Project EIR; FC Pier 70, LLC; San Francisco, San Francisco City and County, California. SWCA completed an EIR on a long-range project located on 35 acres of historic shipyard property along San Francisco’s Central Waterfront. The proposed project envisions a flexible land use program under provisions of a new Special Use District, which would allow up to nearly 4.2 million gross square feet of residential, commercial, retail, and arts and light industrial uses, along with parking, 9 acres of new public parks and shoreline trail access. For flexibility, the EIR analyzed two development scenarios at an equal level, along with four variants of individual project features. *Role: Editor. Edited EIR and coordinated production.*

3333 California Mixed Use Project EIR; Laurel Heights Partners, LLC; San Francisco, San Francisco City and County, California. SWCA prepared an Initial Study and Focused EIR on redevelopment of an approximately 10.25-acre parcel

currently occupied by the University of California San Francisco Laurel Heights Campus. The site would be redeveloped with a mix of residential, retail, office, childcare, and associated parking uses, which would be located in 13 new buildings and two adaptively reused buildings (originally a single office building). Proposed parking would be provided in four below-grade parking garages and six two-car parking garages. *Role: Editor.*

SFMTA Potrero Yard EIR & Outreach Amendment; San Francisco Municipal Transportation Agency; San Francisco, San Francisco City and County, California. SWCA, working under the direction of the San Francisco Planning Department as the CEQA lead agency, published an Initial Study and Focused EIR on the modernization of the San Francisco Municipal Transportation Agency’s Potrero Yard at 2500 Mariposa Street, in the northeast portion of the Mission neighborhood. Potrero Yard consists of a bus storage yard and maintenance facility that services the agency’s electric trolley coaches. *Role: Editor.*

1028 Market Street EIR; San Francisco City and County; San Francisco, California. SWCA completed a Focused EIR on demolition of a contributing building to the Market Street Theater and Loft National Register Historic District, and construction of an approximately 178,313-gross-square-foot residential building with ground-floor retail space and one level of subterranean parking. *Role: Editor.*

Rehabilitation and Detention Facility – Hall of Justice Jail Replacement Mitigated Negative Declaration (MND); San Francisco Department of Public Works; San Francisco, San Francisco City and County, California. Mitigated Negative Declaration on a project to construct a new multi-story, 640-bed jail facility adjacent to the existing Hall of Justice building at 850 Bryant Street. *Role: Editor.*

Parkmerced Project EIR; Parkmerced Owner, LLC; San Francisco, San Francisco City and County, California. EIR on a 152-acre, long-term mixed-use development program that would comprehensively re-plan and re-design the Parkmerced site, increase residential density, and provide new services and transit facilities. *Role: Editor. Edited EIR and coordinated production; tracked comments for the Responses to Comments document.*

DAMIAN STEFANAKIS | SENIOR PRINCIPAL PLANNER



Damian Stefanakis has 32 years of experience in transportation planning and travel demand forecast modeling. He specializes in the development and application of travel demand models for highway and transit projects using many types of software, including EMME, TRANPLAN, UTPS, MINUTP, and CUBE/TP+/Voyager. He also has experience with application of regional MPO models in California, Houston, Cleveland, and Florida. Damian has significant experience developing the CCAG-VTA and Alameda CMA Countywide Models for regional transit and highway studies. He also provides on-call support and model training to clients in the use of EMME/2 and CUBE/TP+/ VOYAGER.

PROJECT EXPERIENCE

EDUCATION

- Graduate Degree Engineering (GDE), University of Witwatersrand Johannesburg, South Africa, 1988
- BS Civil Engineering, University of Witwatersrand Johannesburg, South Africa, 1985

YEARS OF EXPERIENCE

32

AFFILIATIONS

- Institute of Transportation Engineers, Member

Menlo Park Transportation Master Plan and Transportation Impact Fee Program Update; Menlo Park, CA. Damian was the project manager and the key task leader for preparation of travel forecasts for Kittelson’s work (as a subconsultant to W-Trans) for the City of Menlo Park toward preparation of a transportation master plan. Kittelson applied the model developed for the ConnectMenlo General Plan to the CSA trip distribution assumptions update that the City uses for its traffic studies (previously developed by Kittelson staff). Kittelson conducted modeling to update the trip distribution percentages for Menlo Park. Kittelson summarized all AM and PM peak hour trips by TAZ and city jurisdictions within Menlo Park by city jurisdictions within San Mateo County and by county jurisdictions outside of San Mateo. As part of a second task order, Kittelson is preparing travel forecasts for the Bayfront grade separation and Dumbarton Rail scenarios using assumptions from the Dumbarton Transportation Corridor Study (DTCS).

El Camino Real Corridor Study; Menlo Park, CA. Damian was project manager for Kittelson’s work (as a subconsultant to W-Trans) related to travel demand forecasts to improve circulation and safety along El Camino Real in Menlo Park. Using the San Mateo County C/CAG Travel Demand Model, Kittelson developed 2040 travel demand forecasts using the Citywide ConnectMenlo model for a number of multimodal alternatives, including the addition of a third through-travel lane on El Camino Real and improved bicycle/pedestrian connections. The modeling results included both link and turning movement volumes. Kittelson also provided additional computational results to support the environmental review report.

Commonwealth Building 3 Project CEQA Review; Menlo Park, CA. Damian was the project principal for Kittelson’s work (as a subconsultant to ICF International) related to traffic and transportation analysis as part of the California Environmental Quality Act (CEQA) clearance for a proposed expansion of the City of Menlo Park’s Commonwealth Corporate Center to include a third office building encompassing 320,000 sq. ft. Kittelson prepared a transportation impact analysis (TIA) that served as the transportation section of the environmental document. Kittelson used the ConnectMenlo model to evaluate up to 31 intersections for existing, near-term, and cumulative conditions. Outputs included VMT for SB 743 and greenhouse gas analysis.

SAMTRANS Express Bus Implementation Feasibility Study; San Mateo County, CA. Damian served as project manager for Kittelson’s work (as a subconsultant) to prepare travel demand and ridership forecasting for an initial Proof of Concept study to test express buses on the future Express/managed lanes on US-101 in San Mateo County. The study developed in parallel with the US 101 Managed Lanes PA&ED that Kittelson was also doing for the Transportation Authority to take advantage of the travel demand modeling and related managed lanes alternatives conducted for that study. Damian used the C/CAG-VTA Bi-County model with ABAG Projections Plan Bay Area to test express bus service traveling on managed lanes along US-101. Travel forecasts and ridership estimates were developed for the 2020 horizon year primarily for a lane-convert managed lane alternative. Origin-Destination travel patterns were extracted from the model and potential routes were developed based on the highest OD trip rankings and screened down to 9 potential routes using the GIS based REMIX software. Routing focused on connecting residential neighborhoods to commercial and high-tech areas with park and ride lots and access to Caltrain and

BART stations on the Peninsula. Total ridership for all nine routes with peak period service was estimated at about 9,000 daily riders.

US 101 Managed Lanes PA & ED; San Mateo, CA. Damian is the Kittelson project manager currently working with the San Mateo Transportation Authority and Caltrans on the US 101 Managed Lanes PA&ED as a subconsultant providing travel forecasting for the entire 101 corridor from Santa Clara County to the San Francisco county line. The project requires use of the C/CAG-VTA Countywide model in CUBE to prepare forecasts for 2013, 2020 opening year, and 2040 design year in support of the traffic operations; VMT and CEQA analysis; HOV 2, HOV 3+, Express Lane 2, and Express Lane 3+ conditions scenarios; tolling; and revenue generation tasks.

On-Call Modeling and Training Programs. Damian has supported many agencies with on-call modeling services, including San Mateo C/CAG, BART, Marin TAM, Alameda CTC, Alameda County, CCTA, Kern County, Livermore, and Hayward. He has developed and led training programs in travel forecasting, forecasting software, traffic impact analysis and computer applications. Clients have included California Department of Transportation, Alameda County, Kern County, City of Livermore and Oregon Department of Transportation. Kittelson is one of the authorized on-call consultants to advise and support CCTA, Alameda CTC, C/CAG and Marin TAM.

Travel Demand Forecasting. Damian has led and worked on many travel demand modeling studies, including development and adaptation of the MTC Regional Model (MTCFCAST and BAYCAST), Alameda Countywide model, San Mateo C/CAG model, Marin TAM model, City of Livermore model, City of Hayward model, City of Alameda model, KERN COG model, Fresno COG model, Boise model, PSRC model, Orlando FDOT model, Washington DC model, and HGAC Houston model.

Corridor Studies. Damian has led and worked on numerous freeway corridor studies providing travel demand modeling and traffic operations, including the 2016 US 101 HOV/Express Lane PAED and HOV Hybrid studies in San Mateo, I-880 HOV and Interchange studies in Alameda, I-580 HOV and HOT lanes studies in Alameda, SR 238 Corridor Study in Hayward, and Menlo Park El Camino Real Corridor Study.

Transportation Impact Studies/Environmental Impact Reports. Damian has managed a number of transportation impact studies and EIRs that involved the analysis of traffic operations, on-site access and circulation, parking, and impacts on alternative modes. Such projects include transit-oriented developments, large scale mixed use projects, general plans, station area plans and traffic impact studies. Key projects include:

- **General Plans:** Pacifica, Belmont, East Palo Alto, Hayward, San Leandro, Livermore, San Carlos
- **Housing Elements:** Marin County Housing Element
- **K-12 Schools:** Hayward High School Access Study, Cherryland Elementary School Relocation and Expansion, Harder Elementary School Expansion
- **Large-scale Mixed-Use and Specific Plans:** San Leandro Shoreline EIR, Alameda Point EIR, Kaiser San Leandro Mixed Use EIR, El Charro Specific Plan EIR (Livermore), South Hayward BART Specific Plan EIR, Mission Boulevard Specific Plan EIR, West Oakland Specific Plan, San Leandro Shoreline Specific Plan, Fairview Specific Plan
- **Transit-Oriented Developments:** Lake Merritt Station Area Plan, San Leandro TOD, Livermore Isabel Station Area Plan

Transit Studies. Damian has led or worked on numerous transit studies, including AC Transit BRT, BART Fremont South-Bay, BART to Livermore, SMART Rail, NOACA Rail, SAMTRANS Express Bus Study and WCCTAC High Capacity Transit Study.

AMANDA LEAHY, AICP | ASSOCIATE PLANNER



Amanda Leahy's areas of expertise include environmental review for land use development, multimodal traffic operations and safety analysis, vehicle miles traveled and transportation demand management analysis, bicycle and pedestrian planning and design, and complete streets policy and implementation. Amanda has worked on many complex projects from planning through implementation, which have required objective and transparent technical analysis, multi-agency coordination, and environmental clearance. Amanda is committed to creating a safe and comfortable public realm for users of all ages and abilities. She is passionate about strengthening connections between people and places through sound planning and analysis; creative, context-sensitive design and engineering; and effective stakeholder and community engagement.

EDUCATION

- Bachelor of Arts, Geography and Urban Studies, University of California, Berkeley

YEARS OF EXPERIENCE

13

CERTIFICATIONS

- Certified Planner, American Institute of Certified Planners (#026690)

AFFILIATIONS

- American Institute of Certified Planners (AICP), Member
- American Planning Association (APA), Member
- Association of Pedestrian and Bicycle Professionals (APBP), Board President
- Bike East Bay, Board Member
- Women in Transportation Seminar (WTS), Member

PUBLICATIONS/PRESENTATIONS

- Association of Pedestrian & Bicycle Professionals Conference, August 2019 – *Leveraging Vision Zero and Black Lives Matter to Achieve Transportation Safety and Equity Goals* (Presentation/Publication)
- Rice Urbanists, November 2018 – *Multimodal Transportation* (Webinar)
- California ITE, October 2018 – *Complete Streets* (Presentation)
- Walk Bike Places Conference, September 2018 – *Designing and Implementing Effective Project Evaluations* (Poster)

PROJECT EXPERIENCE

San Francisco Planning Department Pre-Qualification List; San Francisco, CA.

Kittelson is prequalified for general transportation planning and environmental review for new development projects. Amanda has managed and successfully led multiple projects through the transportation impact analysis approval, environmental impact report certification, and entitlement process. As part of the environmental review, Kittelson typically completes the following tasks:

- Collect intersection turning movement counts and conduct field observations to document existing transportation conditions
- Prepare travel demand estimates, including person trip generation by mode and freight and passenger loading demand
- Evaluate site access and circulation for all modes
- Conduct vehicle miles traveled analysis
- Conduct transit delay analysis
- Assess and document project-related effects to access and circulation for people walking and biking
- Prepare truck turning templates to confirm truck and emergency vehicle access to the site and proposed loading docks
- Prepare driveway loading operations and management plans
- Assess transportation-related construction impacts
- Prepare transportation demand management (TDM) plans and evaluate the effectiveness of the TDM measures in reducing vehicle trips generated by the project
- Develop mitigation measures and/or conditions of approval to minimize potential project impacts
- Review consistency with Planning Code requirements
- Prepare a transportation impact study and/or environmental impact report summarizing and documenting findings
- Communicate and coordinate with the applicant, Planning Department, and San Francisco Municipal Transportation Agency (SFMTA) staff
- Participate in public hearings and respond to public comments on the draft environmental impact report, if applicable

Throughout the process, Amanda works closely with applicant team, Planning Department staff, and SFMTA staff to develop appropriate analysis methodology and assumptions, including the project description, land use program and streetscape modifications as well as the baseline and cumulative transportation network. Recent projects that Amanda has led/is leading include:

- Stonestown Galleria EIR (Ongoing)

- Freedom West Homes EIR (Ongoing)
- 3333 California Street Mixed-Use Development EIR
- Balboa Reservoir EIR
- 725 Harrison Street TIS/CPE
- Parcel F (542-550 Howard Street) TIS
- 542-550 Howard Street TIS
- 524 Howard Street TIS
- 555 Howard Street TIS

San Francisco Municipal Transportation Agency (SFMTA), Transportation Planning On-Call Services; San Francisco, CA. Task orders under this contract include transportation planning and engineering services, data analytics, and environmental documentation support. Amanda has led multiple task orders, including:

- **Slow Streets Support.** Amanda is leading consultant support for the Slow Streets Program efforts to convert the temporary treatments into longer term designs for the 24 corridors identified as slow streets. Kittelson's role on the project includes data collection and evaluation, concept design development, survey development, survey distribution and analysis, as well as support with the communications strategy and community engagement.
- **Frida Kahlo / Ocean / Geneva Intersection Project.** The complex intersection serves a significant amount of vehicle, bus, and light rail traffic and is also an important walking and bicycling connection due to its proximity to BART, City College of San Francisco, and a large planned residential development at Balboa Reservoir. Amanda led the Kittelson team's work developing near- and long-term concept designs and evaluating them against multimodal safety and operational performance metrics. The result of the effort was the selection of a near-term concept to carry forward to implementation with quick build funding.
- **Safe Streets Evaluation Handbook.** Amanda served as a task lead for the development of a project evaluation handbook and data collection standard operating practices with supporting templates and guidance for SFMTA staff. After completion, the evaluation framework developed by the team continues to be applied to projects throughout the city.
- **Valencia Street Bikeway Implementation Plan.** Amanda was project manager for a plan to establish baseline conditions and identify options to enhance the existing bicycle facilities along Valencia Street to make the corridor safer for all users. Kittelson's role encompassed data collection, multimodal analysis of existing transportation conditions (including parking supply/demand and travel behavior and interactions), intercept surveys, and support of public engagement activities.

San Francisco Transportation Element; San Francisco, CA. Amanda is project manager for a task order (under Kittelson's on-call contract with the City of San Francisco Planning Department) to assist the City with the development of a new Transportation Element for San Francisco's General Plan. Kittelson is supporting community engagement and policy development efforts and will be developing a project description for the Transportation Element EIR. Ultimate outputs of the work will be an adopted EIR and a City policy framework and projects that seek to align transportation and land use, achieve equitable outcomes, and reduce greenhouse gas emissions.

RELEVANT EXPERIENCE WITH RACIAL AND SOCIAL EQUITY

Board President, Co-Chair Equity and Inclusion Committee, Co-Chair Policy Committee, Association of Pedestrian and Bicycle Professionals; United States and Canada. Amanda is the Association of Pedestrian and Bicycle Professionals (APBP) Board President and leads the organization's Equity and Inclusion Committee and Policy Committee. The E&I Committee is working to highlight issues and foster discussions within the association, identify ways to engage historically underrepresented people within leadership, membership, and the profession at large, identify ways to measure and report on progress, and develop or recommend capacity-building resources to APBP members. The work is aimed at advancing equity and inclusion within the industry and demonstrates APBP's commitment to mobility justice and community equity in the transportation field.

Board Member, Chair Equity Task Force, Bike East Bay; Bay Area, CA. Amanda is a Bike East Bay Board Member and is leading the Board's Equity Task Force to educate Board members and support the Bike East Bay's mission to build a more just and equitable transportation system and prevent and redress racial and class bias in transportation planning, policies, and enforcement. The task force seeks to build capacity of less well-resourced partner organizations to engage in the planning process, and create an environment where people of all races, ethnicities,

incomes, ages, religions, sexual orientations, gender identities, abilities, and countries of origin feel welcomed, seen, celebrated, and treated equitably.

Diversity, Equity, and Inclusion Steering Committee, Kittelson; United States, CA. Amanda is a member of Kittelson's Diversity, Equity, and Inclusion Steering Committee. The group seeks to amplify Kittelson's values by continuing to raise the bar for ourselves in the areas of equity and inclusion. Over the past year we have launched an Equity Challenge to foster practical ideas for advancing equity in projects, held a series of firmwide "safe spaces" open discussions, curated a collection of education resources, and initiated Employee Resource Groups. We continue to investigate ways to create a more inclusive recruiting and hiring process and reduce biases within our screening and interviewing processes, among other initiatives.

Commonwealth Building 3 Project CEQA Review; Menlo Park, CA. Amanda was the project manager for Kittelson's work (as a subconsultant to ICF International) related to traffic and transportation analysis as part of the California Environmental Quality Act (CEQA) clearance for a proposed expansion of the City of Menlo Park's Commonwealth Corporate Center to include a third office building encompassing 320,000 sq. ft. Kittelson prepared a transportation impact analysis (TIA) that served as the transportation section of the environmental document. Kittelson used the ConnectMenlo model to evaluate up to 31 intersections for existing, near-term, and cumulative conditions. Outputs included VMT for SB 743 and greenhouse gas analysis.



1030 O'Brien Drive Project

Response to RFP

Class 32 Infill Exemption Document



Submitted to:
City of Menlo Park 701
Laurel Street Menlo
Park, CA 94025

Fahteen Khan
Associate Planner
fnkhan@menlopark.gov

Submitted by:
ICF Jones & Stokes, Inc.
201 Mission Street, Suite 1500
San Francisco, CA 94105

Jessica Viramontes
Sr. Project Manager
415.677.7108
jessica.viramontes@icf.com

January 6, 2023

This proposal contains ICF's confidential information and shall not be disclosed to any third party or used for any purpose other than to evaluate this proposal.



City of Menlo Park

1030 O'Brien Drive Project

Response to RFP

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Submitted to:

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Submitted by:

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Fahteen Khan
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January 6, 2023

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January 6, 2023

Fahteen Khan, Associate Planner
City of Menlo Park
701 Laurel Street
Menlo Park, CA 94025

Subject: RESPONSE TO RFP FOR CLASS 32 INFILL EXEMPTION DOCUMENT – 1030 O'BRIEN DRIVE PROJECT

Dear Mr. Khan:

Thank you for inviting ICF Jones & Stokes (ICF) to provide consulting services for the proposed 1030 O'Brien Drive Project (hereafter referred to as the Project). This scope of work reflects the proposed Project information provided to ICF in the Request for Proposals (RFP), subsequent discussions with the City regarding the applicability of preparing a Class 32 Infill Exemption document, knowledge of the area, and prior experience with similar projects within Menlo Park. We offer a team of highly skilled environmental professions who are familiar with the City and will produce legally defensible and comprehensive California Environmental Quality Act (CEQA) documentation allowing the Project to be developed as expeditiously as possible. Our considerable experience on projects in Menlo Park for almost a decade allows our staff to respond quickly to your needs. This submittal includes our overall approach and scope of work, proposed staffing, cost, and schedule.

ICF is a leader in CEQA compliance with over 40 years of experience working on a wide array of projects throughout the San Francisco Bay Area. We are recognized as industry leaders in CEQA streamlining, having prepared hundreds of streamlined documents throughout California. ICF has written manuals and provided CEQA streamlining training to agency staff. Our project management staff consists of a team who is familiar with the intricacies of development in Menlo Park, and is backed by ICF's depth and breadth of technical expertise across the subject areas required to be addressed under CEQA. Our team includes Hexagon who will contribute their expertise to perform transportation consulting services. ICF's team has worked together extensively on prominent CEQA projects throughout the Bay Area (including most recently the Willow Village Master Plan Project), and we know how to deliver environmental documents and manage environmental processes for innovative and complex projects.

Our team is existing about the opportunity to provide CEQA services for this important project. We are eager to discuss our approach with you and hope you will decide that our team is the most qualified for your needs. If selected as the successful bidder, ICF looks forward to negotiating mutually acceptable contract terms and conditions. Please feel free to contact Jessica Viramontes at 415.677.7108 or jessica.viramontes@icf.com should you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to read "R Krusen".

Robert P. Krusen
Associate Contracts Manager

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- Appendix D Cost Estimate



Firm and Project Team Qualifications

ICF Jones & Stokes, Inc. (an ICF company hereafter referred to as ICF), is a global consulting and technology services provider focused on making big things possible for our clients, is the bidding entity for this proposal and a subsidiary of parent company ICF International, Inc. (NASDAQ:ICFI). ICF is a recognized leader in California Environmental Quality Act (CEQA) compliance, having prepared thousands of environmental impact studies and related documents since 1970. ICF is proposing a team of dedicated professionals who are familiar with Menlo Park, who are knowledgeable about local issues, and who have the capacity to provide timely and exceptional environmental services for the 1030 O'Brien Drive Project (Project). In addition, we are deliberately putting forth a similar team that has ushered several recent City of Menlo Park (City) development projects through the CEQA clearance process, including subconsultant Hexagon (transportation). The proposed specialists on the team have a key area of expertise to contribute and will work closely with other specialists on all aspects of the Project to keep the process cohesive.

ICF

Since 1969, ICF has been serving government at all levels as well as major corporations and multilateral institutions on infrastructure improvement projects, restoration and planning projects, and compliance with mandated government programs. More than 8,000 employees (inclusive of corporate affiliates) in 70 offices serve these clients worldwide. More information is available at www.icf.com.

ICF provides consulting and implementation services to address today's most complex management, technology, and policy challenges. Our work is focused primarily in four key markets: environment and infrastructure, energy and climate change, health and social programs, and homeland security. The firm provides end-to-end solutions to support clients through the entire program life cycle, from analysis and design through implementation and evaluation. The ICF team brings the following resources to deliver the full scope of services that may be required to meet the City's needs on the Project:

- A professional staff with full-time and on-call environmental analysts, hazardous materials experts, land use and natural resource planners, wildlife and fisheries biologists, plant and wetland biologists, arborists, watershed planners, restoration and mitigation experts, hydrology and water quality specialists, noise and vibration specialists, environmental permitting and conservation planning experts, archaeologists, and architectural historians.
- A wide network of environmental regulatory expertise. ICF is a recognized leader in CEQA compliance. ICF professionals have authored guidelines and taught University of California Extension courses on complying with environmental law and regulations. Many of our specialists are also authors who have collaborated on definitive CEQA publications that have become industry standards for assisting planning professionals with compliance documentation. Our own environmental regulatory staff includes former agency regulators. We stay up to date on the latest policy developments at the federal, state, and local levels.

Office Where Work Will Be Managed
ICF's San Francisco Office
201 Mission Street, Suite 1500
San Francisco, CA 94105

ICF has direct and ongoing experience throughout the Bay Area. With more than 250 technical staff members in Northern California, ICF is one of the largest CEQA consulting firms in the region. Our deep bench ensures our ability to meet client needs quickly and effectively. ICF has staff members operating out of multiple offices on the

West Coast, and we have the capacity to provide the City with timely and high-quality services. Our staff is experienced in balancing an existing backlog of projects and has the capacity to initiate new

ICF Profile Snapshot
Founded in: 1969
ICF Employees: 8,000+
Northern California Office Locations:
 San Francisco and Sacramento



projects weekly. If selected, although we are able to draw expertise from all West Coast offices, we will service the Project primarily from our San Francisco office.

Subconsultant

Our team includes Hexagon who will contribute their expertise to perform transportation consulting services.

Hexagon

Hexagon has a long history of working on projects within Menlo Park as well as the entire Bay Area. ICF and Hexagon have collaborated extensively and will work together to provide a transportation study that will satisfy the requirements of the City, CEQA, and the City/County Association of Governments (C/CAG) Congestion Management Program (CMP). Hexagon has prepared transportation studies and CEQA-specific traffic analyses throughout the Bay Area. These include analyses of vehicle miles traveled (VMT), operational peak-hour traffic, freeway segments and ramps, queuing, bicycle and pedestrian facilities, transit facilities, and site access and circulation. Hexagon also provides thorough peer reviews of Transportation Demand Management (TDM) plans for its clients. Hexagon has provided services for projects in Menlo Park that included the 1350 Adams Court Project, 1125 O'Brien Drive, CSBio, 3723 Haven Avenue Hotel, Menlo Park Housing Element Update, Willow Village Master Plan, and the Menlo Park Citywide Model.

Project Team Approach and Qualifications

ICF offers unique advantages with our local knowledge and experience with issues important to the City. This local knowledge and familiarity with City practices directly relates to enabling us to deliver high-quality environmental support by understanding the nuances of the City's needs. Understanding that the expediency of the environmental clearance process is essential to moving a project forward, we have developed an approach that allows us to ensure the production of high-quality and legally sound products in a timely fashion. Our success with meeting aggressive schedules while adeptly handling complex environmental issues on similar projects in Menlo Park, including projects in the immediate Project vicinity, is a testament to our ability to usher the Project efficiently through the CEQA process. Aside from our commitment to a careful and efficient execution of the detailed scope of work (SOW), we bring the key characteristics described below to support implementation of this project.

Relevant Class 32 Infill Exemption Experience

ICF is an industry leader in CEQA streamlining, having prepared hundreds of streamlined documents throughout California. Our team has written manuals and provided CEQA streamlining training to agency staff. In 2020, we launched eCEQA.com, an interactive online tool that helps project sponsors, lead agencies, and CEQA practitioners determine if a project may qualify for an exemption or streamlining. The following projects highlight ICF's recent relevant experience with CEQA Categorical Exemptions (CEs).

Acacia Building Replacement Project (September-October 2022), Stockton, CA

A Class 14 (Minor Addition to Schools) Exemption and Class 32 (Infill Development Projects) Exemption was prepared for this project, which would include the construction of an approximately 55,200-gross-square-foot two-story educational building on an approximately 1.4-acre site at University Park in Stockton, on a satellite campus for California State University Stanislaus. ICF prepared the document under an expedited schedule.

1766 El Camino Real Project (February-June 2022), Burlingame, CA

A CE was prepared for this project that entailed the demolition of a two-story mixed use building, and the construction of a new eight-story, multi-unit residential building with 311 residential units, amenity space, and public and private open spaces, as well as bicycle and vehicle parking. An IS/MND was previously prepared by ICF for a different project at the same location. To the extent possible, the



analysis within the CE document relied on and/or updated the analysis that was previously prepared for the IS/MND where relevant to the currently proposed project.

1669/1699 Old Bayshore Highway and 810/821 Malcom Road Project (August-October 2022), Burlingame, CA

A CE was prepared for this project which would demolish four existing, vacant buildings, and construct a new office and R&D campus that would include a north and south parcel that would be bisected by Malcolm Road, and a nine-level parking structure. Upon Project implementation, the Project would include approximately 150,374 gsf of office uses, 225,560 gsf of R&D uses, 6,390 gsf of restaurant and amenity uses, and 19,519 sf of open spaces. ICF prepared the document under an expedited schedule.

Adrian Court Project (May-September 2019), Burlingame, CA

A CE was prepared for this project that entailed the demolition of two commercial buildings, surface parking, and landscaping on two parcels. The project involved merging the two parcels to create a 2.83-acre development with mixed-use residential, commercial/office, park space, and parking. The project was approved in September 2019.

Relevant Menlo Park Project Experience

ICF has almost a decade of experience working on CEQA documents in Menlo Park. Previously completed and ongoing projects in the city include the following:

- Parkline Master Plan Project EIR
- Willow Village Project EIR
- 1350 Adams Court Initial Study and EIR
- Commonwealth Corporate Center Building 3 Initial Study and EIR
- 1125 O'Brien Drive Initial Study and EIR
- CSBio/1075 O'Brien Drive Initial Study and EIR
- Facebook Campus Expansion Project EIR and EIR Addendums 1 & 2
- Commonwealth Corporate Center EIR
- Middle Plaza Project at 500 El Camino Real EIR
- 1300 El Camino Real Project
- Menlo Park Facebook Campus Project EIR Addendum

In addition to the projects listed above, ICF has extensive experience from preparing CEQA documents for similar jurisdictions throughout the San Francisco Peninsula and in the Bay Area. For example, ICF recently prepared several CE documents for the City of Burlingame, including, but not limited to, the 1766 El Camino Real Project and the 1669/1699 Old Bayshore Highway and 810/821 Malcom Road Project.

Additional project information is available upon request.

Seasoned Team with Relevant Experience, Commitment, and Availability

The ICF team's expertise from working on CEQA documents on the San Francisco Peninsula as well as in the Silicon Valley/South Bay, including our recent experience in Menlo Park, means we understand the planning context of the area, including constituents in a variety of jurisdictions, environmentally challenging topics, and interrelationships among cities in the area. In addition to this wide-ranging knowledge, we have built productive and respectful relationships with Menlo Park staff members. We strive to perform to the satisfaction of the City. Our goal is to devote the same level of

commitment and dedication to the 1030 O'Brien Drive Project that we devoted to our past successful CEQA compliance efforts in Menlo Park.

Our management team has extensive experience working on previous projects in the City. Jessica Viramontes, our project director, has managed projects in the City, including the Facebook Campus Expansion Project EIR and EIR Addendum 1 and Middle Plaza Project at 500 El Camino Real EIR. Victoria Chung, our deputy project manager, is managing the 1125 O'Brien Drive EIR. Jessica and Victoria both have extensive experience successfully coordinating and writing CEQA compliance documents, including Categorical Exemptions documents, for projects in Menlo Park and throughout the Bay Area. Kirsten Chapman, our senior advisor, has been part of the project management team on projects in the City for over 11 years. Kirsten will contribute valuable local knowledge and strategic thinking to move the Project through the CEQA process. Jessica, Victoria, and Kirsten, are known to the City for their excellence in environmental compliance work, the lead agency process experience, and management, all of which are necessary attributes for successfully achieving CEQA clearance for high-profile priority development projects. This management team has also successfully worked directly with Hexagon for years.

Our diverse team, including the talented and experienced technical specialists included in the organization chart on the following page, are available and excited to get to work on the Project. Although ICF is currently working on EIRs in Menlo Park, some of these projects were successfully completed in 2022 (including 1350 Adams Court and Willow Village Master Plan), or are nearing completion, freeing up team members who are familiar with the City to work on new projects.

Strong Team Management

As a multidisciplinary environmental consulting firm, ICF provides a full range of in-house services related to environmental planning. Using our own staff expertise, in combination with the skills and services provided by our subconsultants, we have the capability to complete every element of our proposed scope successfully. The key to successful teaming is a clearly defined scope of work with well-established roles for each consultant on a project. In approaching the tasks included in our scope of services, our management team works with all team members to develop and outline their roles, thereby ensuring a quality project.

With collaboration being a vital focus of our team, ICF's role will be to ensure that the team understands Project goals, objectives, and individual responsibilities. We will bring together all perspectives provided by the team throughout all phases of the Project and set incremental delivery schedules to meet Project milestones. As a prime consultant, ICF will be responsible for reviewing subconsultant work products (e.g., technical studies) to ensure that they meet applicable CEQA requirements, are clearly written, and provide the necessary level of technical analysis to support the CEQA document.

Proposed Project Team

ICF is proposing a team of dedicated professionals who are familiar with the City and the department's expectations, who are knowledgeable about local issues, and who have the capacity to provide the City with timely and exceptional environmental services. The organizational chart on the following page and the resumes in Appendix A highlight the experience and qualifications of the relevant ICF team members, demonstrating their ability to meet the minimum qualifications outlined in the request for proposals (RFP). The resumes provided in Appendix A include the team leads at ICF and Hexagon. Resumes for additional staff members can be provided upon request.



City of Menlo Park

Jessica Viramontes, Project Director
 Victoria Chung, Project Manager
 Kirsten Chapman, Senior Advisor

<p><u>Noise</u> Cory Matsui Noah Schumaker</p> <p><u>Air Quality</u> Laura Yoon Cory Matsui</p> <p><u>Water Quality</u> Katrina Sukola</p> <p><u>Hazardous Materials</u> Mario Barrera</p>	<p><u>Cultural (Archaeology)</u> Lora Holland Megan Watson</p> <p><u>Cultural (Historic)</u> Susan Lassell Nicole Felicetti</p> <p><u>Transportation</u> Gary Black & Ollie Zhou at Hexagon (subconsultant)</p>	<p><u>Public Services/Utilities</u> Victoria Chung</p> <p><u>Biological Resources</u> Lisa Webber</p> <p><u>Land Use Consistency</u> Victoria Chung</p> <p><u>Document Production</u> John Mathias John Conley</p>
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Diversity, Inclusion, and Equity at ICF

ICF was founded more than 50 years ago by former Tuskegee Airman C.D. Lester. We were then called the Inner City Fund, with a mission to finance minority-owned businesses in Washington, DC. Since then, ICF has evolved to grow our consulting business, partnering with clients on complex energy, environment, health, and socioeconomic development issues. Our purpose as a company is to build a more prosperous and resilient world for all, including for each of our employees. Currently, ICF is a global company with offices in more than 60 countries. As a reflection of its commitment to diversity and inclusion, 53 percent of its leaders across the company are female and 39 percent of its executives are women or minorities. ICF constantly seeks inclusive opportunities to collaborate with experts, suppliers, contractors, and technology partners with diverse perspectives who are just as driven as we are to make a difference.

ICF's company values are driven equally by company leaders and by local teams of colleagues, who are committed to using our professional positions to ensure our respective fields have intentional, positive impacts on our communities. In 2020, ICF prioritized making diversity, equity, and inclusion foundational company values. The company began to match employees' personal donations at 100 percent to organizations supporting racial and social injustices, with no limits. ICF engaged in company-wide town halls, listening tours, and employee surveys. Informed by employee feedback, ICF developed a clear diversity and inclusion roadmap that we are committed to continually measuring and monitoring.

Key initiatives that grew from these efforts included the hiring of our vice president of diversity and inclusion, who leads companywide efforts to advance equity, starting with recruiting and hiring but extending to the current staff through required diversity and inclusion training (e.g., increasing awareness of microaggressions and unconscious bias) and employee community networks. These voluntary, employee-led networks are centered around Asian, Black, First Nations/Indigenous Peoples, Diverse Abilities, Hispanic/Latinx, LGBTQIA+, and Women's community issues; they reflect ICF's mission and values by supporting a diverse, inclusive workplace through mentoring, professional development, and community outreach opportunities for all employees. ICF also implemented a corporate training course to promote an inclusive workplace and supported that training with a peer-to-peer Diversity and Inclusion (D&I) Champion program. The D&I Champions participate in peer coaching and team meetings, bringing course content to life in ongoing discussions to further explore our inclusive culture framework: objectivity, belonging, voice, and growth.

Within the Environment and Planning Division, ICF conducts focused diversity, equity, and inclusion learning courses, group discussions, and workshops. Our cultural resources group recently conducted a focus group on "Cultural Resource Management in the Time of Black Lives Matter." Through the use of a mural board, the group identified vision statements and action items identifying how clients and projects can benefit from a proactive, innovative, and intentional approach to bringing new voices and under-represented groups to the table during the identification of cultural resources and assessment of impacts.

We are committed to improving our hiring practices to encourage minority and under-represented groups to pursue a consulting career at ICF. We conduct focused recruiting at the University of Hawaii, where 80 percent of the student population identifies as black, indigenous, and people of color, and San José State University, an Asian American and Native American Pacific Islander-Serving Institution grantee. We recently launched a guest speaker pilot program at community colleges and high schools with prominent minority populations. We regularly post jobs to LinkedIn groups targeting women, such as Women in Transportation, and distribute them through targeted professional organizations such as Latinos in Heritage Conservation.

For more information about ICF's diversity, equity, and inclusion values and initiatives, see <https://www.icf.com/company/about/diversity-inclusion>.



Scope of Work

Project Understanding

The 3.6-acre Project site encompasses four contiguous parcels located at 980-990, 1010, 1020 and 1030 O'Brien Drive. Currently, there are four existing buildings within the Project site.

The Project would include the demolition of two existing buildings, retention of two existing buildings, and construction of one new building at the Project site. The two existing buildings at 1020 and 1030 O'Brien Drive that would be demolished are on two parcels; the parcels would be merged and the new building would be constructed on the newly-merged parcel. The two existing buildings at 980-990 and 1010 O'Brien Drive that would remain are currently used for commercial space; these parcels are included as part of the Project site to capture unused FAR and apply it to the proposed new building. The new three-story, 45-foot tall building would include approximately 85,886 square feet of R&D/office space and 5,822 square feet of commercial space.¹ The ground floor would include covered outdoor parking, building service and loading spaces, the main entry lobby, and expansion commercial space that could provide uses such as food service or fitness. Ground level improvements would include new surface parking (including 193 parking spaces) and landscaped outdoor space, with additional outdoor space in the form of elevated terraces at the third floor. Levels 2 and 3 would be tenant space dedicated to R&D/office uses.

According to the site plans dated July 27, 2022, tenant improvements at 1010 O'Brien Drive would occur under a separate permit from the Project. Therefore, this scope of work does not assume the tenant improvements would be part of the Project.

General Approach

Based on our preliminary review and discussions, we understand that a Categorical Exemption (CE) may provide the required level of environmental review for the Project under the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15300 to 15333. This submittal includes a scope of work, cost estimate, and tentative schedule for ICF to prepare environmental documentation for the CE (referred to as the Infill Exemption Document). If it becomes evident that the Project would have the potential to result in significant impacts on the environment (e.g., if the Transportation Impact Assessment findings indicate the Project would have significant transportation impacts), even impacts that can be mitigated to a less-than-significant level, the Project would not be eligible for a CE and another CEQA pathway (e.g., an Initial Study) would need to be identified under an amended scope of work and budget. If it is determined that an Initial Study is required, please refer to ICF's previously submitted proposal dated November 28, 2022; that proposal includes a scope of work, cost estimate, and tentative schedule for ICF to prepare an Initial Study for the Project. If significant and unavoidable impacts are identified, an EIR would be required. Overall, ICF's approach is to rely, where appropriate, on applicable policies in ConnectMenlo and the conclusions made in the ConnectMenlo EIR.

As discussed above, ICF and Hexagon are currently working on, or have worked on, the following projects: 1350 Adams Court, Commonwealth Corporate Center Building 3, 1125 O'Brien Drive, and CSBio (1075 O'Brien Drive). All these projects are within the Bayfront Area and tier from the ConnectMenlo EIR. ICF has also worked with Hexagon on many of the Class 32 CEs we prepared for the City of Burlingame. This scope draws from our experience with projects in the City of Menlo Park and other Class 32 CEs, as appropriate for the Infill Exemption Document. This will allow for

¹ These Project details are consistent with the RFP. Some of the Project details therein differ from the Project's website and the published site plans. For the sake of consistency, the Project details from the RFP are included in this proposal and form the basis of ICF's proposed scope of work.

schedule and budget efficiencies, as well as consistency between the CEQA documents being prepared for all office/life science projects in the Bayfront Area.

Scope of Work

This scope of work assumes that all impacts would be less than significant with implementation of Project commitments, applicable ConnectMenlo policies and/or uniformly applied development policies or standards (e.g., standard conditions of approval). If the analysis in the Infill Exemption Document concludes that the Project would result in significant impacts for certain topic(s) and that the Project would still result in a significant impact after implementation of Project commitments, applicable ConnectMenlo policies, and/or uniformly applied development policies or standards, the City and ICF would discuss the next steps. Should the preparation of an Initial Study, MND, EIR, Addendum, Subsequent EIR, or other type of CEQA document be required, this would be performed under an amended scope of work, and is not included in this proposal.

Task 1: Project Initiation

The CEQA documentation effort will be initiated by discussing key issues, reviewing completed environmental documents, planning data collection efforts including a site visit, and refining the schedule for completion of individual tasks. At the outset of the CEQA process, ICF will meet with City of Menlo Park staff, the Project Sponsor team, and Hexagon. At the kick-off meeting, the team will:

- Confirm procedures for contacting the Project Sponsor team, City staff, and public agencies.
- Discuss City preferences regarding Infill Exemption Document format and organization.
- Discuss in more detail how to apply the ConnectMenlo policies.
- Discuss data needs to complete the Infill Exemption Document.
- Discuss CEQA baseline and cumulative projects.
- Review and agree on schedules and deadlines.
- Summarize the next steps.

Deliverables: refined schedule, revised scope of work (if needed), kick-off meeting agenda, data needs request for the City and Project Sponsor.

Task 2. Project Description

ICF will prepare the Project Description in accordance with City of Menlo Park Planning Division requirements. The Project Description will be based on input provided by the Project Sponsor, including Project plans/entitlements and background studies. A complete and stable Project Description will form the basis for the environmental review. The Project Description will include, at a minimum, the following general topics:

- Project Overview and Background
- Project Site Location
- Project Characteristics, including:
 - Site plan
 - Employment levels
 - Site access, circulation, and parking
 - TDM program

- Project design, architectural themes, massing, building design, potential sustainable design features, and materials
- Amenities such as landscaping, lighting, signage, courtyards, and gathering spaces
- Utilities
- Recycling and waste
- Phasing and Construction Scenario
- Project Approvals and Entitlements
- Figures

This scope of work assumes ICF will prepare up to six figures for the Project Description. ICF will prepare two drafts (i.e., one draft and one final) of the Project Description in response to the City's comments.

Deliverables: One electronic copy of the draft and final version of the Project Description (in MS Word and PDF formats)

Task 3. Administrative Draft Infill Exemption Document and Technical Studies

CEQA Guidelines Section 15332 identifies the conditions that must be met in order to qualify for a CE for an infill project. These conditions are summarized below and, pursuant to Section 15332 of the CEQA Guidelines, will be considered in the Infill Exemption Document that ICF prepares:

- (a) Consistency with applicable general designation and zoning;
- (b) Within city limits, no more than five acres, and substantially surrounded by urban uses;
- (c) The project site has no value for endangered, rare, or threatened species;
- (d) The project would not result in any significant impacts related to traffic, noise, air quality, or water quality;
- (e) The project site can be adequately served by utilities and public services.

With the exception of biological resources, traffic, noise, and air quality, a detailed analysis is not needed for these topics. The Project is consistent with existing land use regulations, located on a site smaller than five acres surrounded by urban uses, served by existing utilities and infrastructure, and developed with urban uses. It is not anticipated that the Project would result in any significant impacts on water quality because it is assumed that the Project would comply with regulations that require specific measures for reducing potential impacts on hydrology and water quality, including the Construction General Permit, Municipal Regional Permit for stormwater discharges (including how the project relates to C.3 requirements), the City Code, and the California Building Code. These requirements will be summarized in the Infill Exemption Document. In addition, the Infill Exemption Document will include an overview of the Project's potential impact on water supply, water infrastructure capacity, wastewater infrastructure and treatment capacity, stormwater capacity, solid waste capacity, fire protection services, police protection services, and schools. The water supply analysis will summarize the results of the ConnectMenlo EIR; this scope of work assumes no water supply assessment will be required for the Project. The Infill Exemption Document will identify the additional estimated demand on these public services and utilities and compare the additional demand to the existing capacity, supply, or service ratio goal.

The following topics will be evaluated in more detail in the Infill Exemption Document.



Biological Resources

Peer Review of the Arborist Report. This scope of work assumes the Project Sponsor will provide an Arborist Report prepared for the Project. ICF will peer review the Arborist Report. ICF will document comments generated through the peer review in a memorandum; ICF will not make any revisions to the Arborist Report.

This scope of work assumes:

- The Arborist Report will provide the number of trees on the Project site, how many trees will be removed, how many trees will be transplanted (if any), how many trees planned for removal are protected by local tree ordinances, and how many trees will be planted in response to planned tree removals (as determined by local tree ordinances).
- The Project Sponsor's consultant will adequately revise the Arborist Report and/or provide requested supporting information in response to ICF's peer review comments.
- In response to ICF's peer review comments, the Project Sponsor's consultant will provide a clean PDF version of the revised Arborist Report and a version showing all revisions in track changes.
- ICF will conduct two rounds of peer review for the Arborist Report, including an initial review of the Arborist Report with ICF's comments provided in a memorandum and a subsequent review to confirm ICF's peer review comments provided were adequately addressed.

Biological Resources Assessment. The City requires all projects subject to discretionary review to prepare a Biological Resources Assessment. An ICF biologist will conduct background research to determine the biological resources that could be affected by the Project, such as special-status species. This research will include the use of the California Department of Fish and Wildlife's Natural Diversity Database (CNDDB), the U.S. Fish and Wildlife Service's Special-Status Species Online Database, and the California Native Plant Society's (CNPS) online inventory, and review of the City's protected tree ordinance. The database results will be appended to the Biological Resources Assessment. In addition, an ICF biologist will conduct a site visit to identify existing site conditions, biological resources (e.g., vegetation and wildlife habitat), and habitat suitability for special-status plant and wildlife species in and around the Project area. A list of plant and wildlife species observed during the survey will be collected and presented in the biological Resources Assessment. An ICF biologist will evaluate the Project's effects on the identified biological resources. Based on prior experience in the region and the urban nature of the Project site, it is anticipated that the prominent biological resources issues for the Project will be limited to nesting migratory birds and raptors, roosting bats, and protected trees, per the City of Menlo Park Municipal Code. If mature trees or other biological resources are identified in the Biological Resources Assessment, ICF will recommend avoidance measures that will be incorporated into the project.

This scope of work assumes:

- Focused studies or surveys (e.g., formal wetland delineation, bird surveys) will not be conducted during the site visit.
- The entire Project site, not including inside buildings, will be accessible during the site visit. If areas are not accessible due to blockades such as fences, ICF assumes the Project Sponsor will provide access to these areas.
- The Project would result in no impact to fish species or sensitive biological communities.
- ICF will prepare up to five figures using GIS for the Biological Resources Assessment.
- ICF will prepare two drafts (i.e., one draft and one final) of the Biological Resources Assessment in response to the City's comments.

- Comments from multiple reviewers will be consolidated, conflicting comments will be resolved, and that the comments will not result in substantial revisions or additional analyses.

Infill Exemption Document. ICF will summarize the conclusions of the Arborist Report and the Biological Resources Assessment in the Infill Exemption Document. The Arborist Report and the Biological Resources Assessment will be appended to the Infill Exemption Document. This scope of work assumes that all impacts would be less than significant with implementation of Project commitments, applicable ConnectMenlo policies, and/or uniformly applied development policies or standards.

Transportation

Appendix B includes Hexagon's scope of work for the preparation of the standalone Transportation Impact Analysis (TIA) Report and Transportation section of the Infill Exemption Document. ICF will oversee Hexagon's preparation of the TIA Report and Transportation section to ensure consistency with the Project Description and the analysis approach for the other topics in the Infill Exemption Document. The TIA Report will be appended to the Infill Exemption Document.

Noise

ICF will evaluate potential Project-related impacts on noise and vibration. No standalone noise and vibration technical report will be prepared. Model outputs will be provided as an appendix to the Infill Exemption Document. The noise and vibration analysis will address:

- Exposure of existing noise sensitive land uses to noise and vibration associated with construction activity at the Project site.
- Exposure of existing noise-sensitive land uses to Project-related changes in traffic noise.
- Exposure of existing noise-sensitive land uses to operational noise from the Project site. ICF assumes that this will be limited to an analysis of noise from emergency generators, HVAC equipment, and operations at truck loading and unloading areas.

In the regulatory setting, the discussion will focus on local noise standards and guidance, and applicable noise and vibration standards from ConnectMenlo will be summarized.

In the environmental setting section, existing sources of noise in the Project area will be identified at a high level, along with existing noise-sensitive land uses in the Project area. Land uses in the area include single- and multi-family residences, an elementary, middle, and high school, and commercial and light-industrial uses. Other sources of noise may include aircraft noise, noise from landscaping equipment and HVAC equipment on surrounding parcels, and other typical noise sources in commercial areas.

Because ICF has conducted noise monitoring in the Project area for previous projects, ICF proposes to use the noise measurement data from those previous projects to inform the environmental setting for the Infill Exemption Document. In July 2021, ICF conducted noise measurements at noise-sensitive land uses in the general vicinity of the Project site along the O'Brien Drive corridor. Thus, the noise measurements are anticipated to be reasonably representative of the Project area, assuming that construction and operation of the Project does not encounter substantial delays. If the City prefers that the Infill Exemption Document include new measurements for the Project, ICF can conduct noise monitoring as part of Optional Task 1 (described below):

Optional Task 1: Noise Monitoring

To determine the baseline noise levels, ICF will conduct noise monitoring at selected locations, as follows:

- Short-term (15 minutes or less) noise monitoring will be conducted at up to four (4) locations in the Project area.

- Continuous long-term monitoring (24 hours or more) will be conducted at up to three (3) locations in the Project area.

Construction noise and vibration will be evaluated using construction noise and vibration modeling methods recommended by the U.S. Department of Transportation and construction equipment data to be provided by the Project Sponsor.

Operational traffic noise will be evaluated at up to a maximum of 20 roadway segments under the following conditions using the Federal Highway Administration Traffic Noise Model (TNM) and average annual daily traffic data to be provided by Hexagon:

- Existing
- Existing plus Project
- Future
- Future plus Project

Traffic noise will be evaluated in terms of how Project-related traffic noise increases may affect existing noise sensitive land uses. This scope of work assumes that traffic data provided by Hexagon will include average annual daily traffic volumes, posted speeds, and heavy truck percentages for each roadway segment analyzed. Non-traffic noise generated by facility operations (such as HVAC equipment, emergency generators, parking lots and/or operational loading docks) will be evaluated using standard acoustical modeling methods and operational data provided by the Project Sponsor.

The analysis of vibration will be consistent with the requirements of the City, including ConnectMenlo, the municipal code and/or other applicable noise and/or vibration standards, including Caltrans guidance.

Thresholds by which to assess potential noise and vibration impacts will be developed based on CEQA Appendix G, ConnectMenlo, and relevant local, state, and federal noise and vibration regulations. The significance of noise impacts will be assessed based on predicted noise exposures and the defined CEQA significance thresholds.

This scope of work assumes:

- Required construction and operational data and/or input assumptions will be provided by the Project Sponsor.
- Traffic data (including average annual daily traffic volumes, posted speeds, and heavy truck percentages for each roadway segment) will be provided by Hexagon.
- Non-traffic operational noise sources would be limited to HVAC and electrical equipment, an emergency generator, and loading/unloading areas.

This scope of work assumes that all impacts would be less than significant with implementation of Project commitments, applicable ConnectMenlo policies, and/or uniformly applied development policies or standards.

Air Quality

ICF will analyze and disclose criteria pollutants and potential health risks associated with construction and operation of the Project. No standalone air quality technical report will be prepared. Model outputs will be provided as an appendix to the Infill Exemption Document. The air quality analysis will address the following.

Criteria Pollutants. ICF will quantify construction and operations-generated criteria pollutant emissions using data collected from the Project Sponsor and the City (e.g., construction schedule, offroad equipment list, operational utility consumption, vehicle miles traveled) and a methodology

consistent with the California Emission Estimator Model (CalEEMod). Estimated criteria pollutant emissions will be compared to the Bay Area Air Quality Management District's (BAAQMD) thresholds to determine Project significance for construction and operational activities.

In response to the Supreme Court decision in *Sierra Club v. County of Fresno*, ICF will discuss potential regional and localized health effects from increased criteria pollutant emissions. This scope of work assumes the analysis will qualitatively and generally describe health risks associated with exposure to the types of criteria pollutant emissions expected under the Project. ICF will review air quality attainment plans for the Project area and existing community health data published by the California Department of Public Health to support the analysis. No Project-specific photochemical modeling is currently proposed.

Toxic Air Contaminants. The primary toxic air containments (TAC) of concern are asbestos and diesel particulate matter (DPM). ICF will qualitatively assess the potential for asbestos from demolition will be qualitatively assessed and ICF will discuss compliance with BAAQMD asbestos rules. Residential and educational receptors are within 1,000 feet of the Project site. Accordingly, ICF proposes to quantitatively evaluate potential health risks from Project construction. ICF will prepare a construction health risk assessment (HRA) using the United States Environmental Protection Agency's (USEPA) AERMOD dispersion model. The HRA will be consistent with methodologies and procedures recommended by the Office of Environmental Health Hazard Assessment (OEHHA), California Air Resources Board (CARB), and BAAQMD. The HRA will evaluate potential cancer and non-cancer health hazards to existing offsite receptors within 1,000 feet of exposure to construction generated DPM and fine particulate matter (PM_{2.5}). Modeled health risks (Project and cumulative) will be compared to thresholds recommended by the BAAQMD. If needed, ICF will identify measures to reduce the level of health risk exposure at impacted receptor locations.

With regard to long-term Project operation, ICF assumes Project-related sources of TAC would be limited to diesel trucks, emergency generators, and potentially organic gases, depending on the type of laboratory and research operations. The Project does not include any residential uses. As such, the Project would not place new sensitive land uses near major sources of air pollution.

The City requires the preparation of project-level HRAs for non-residential projects that: 1) have the potential to generate 100 or more diesel truck trips per day or have 40 or more trucks with operating diesel powered transport refrigeration units, and 2) are within 1,000 feet of a sensitive land use. This scope of work assumes the Project would not generate 100 daily diesel truck trips, and that operational health risks can be analyzed qualitatively. However, given that residential and educational receptors are located within 1,000 feet of the Project site, ICF can conduct a quantitative operational HRA as part of Optional Task 2 (described below), if requested to do so by the City.

Optional Task 2: Operational HRA

ICF will prepare a quantitative operational HRA using the USEPA's AERMOD dispersion model. The HRA will be consistent with methodologies and procedures recommended by the OEHHA, CARB, and BAAQMD. The HRA will evaluate potential cancer and non-cancer health hazards to offsite (within 1,000 feet) receptors from exposure to operations generated TAC. This scope of work assumes operational sources of TAC are limited to diesel trucks, emergency generators, and organic gases. Modeled health risks at receptor locations will be compared to thresholds recommended by the BAAQMD. If needed, ICF will identify measures to reduce the level of health risk exposure at impacted receptor locations.

Carbon Monoxide Hot-Spots. The ConnectMenlo EIR concluded the air quality impacts from localized carbon monoxide (CO) concentrations would be less than significant. ICF will review traffic data from the transportation analysis for affected intersections against the BAAQMD's CO screening criteria. This scope of work assumes the Project will meet BAAQMD's screening criteria and Project-specific dispersion modeling would not be needed. The air quality analysis will qualitatively document the consistency finding.



Air Quality Plan Consistency. The ConnectMenlo EIR concluded that ConnectMenlo would be consistent with the goals and applicable control measures of BAAQMD's Clean Air Plan. The proposed total gross floor area of the Project is within the development enveloped envisioned by ConnectMenlo. Accordingly, ICF will qualitatively document consistency of the Project with ConnectMenlo and with BAAQMD's 2017 Clean Air Plan.

Odors. The ConnectMenlo EIR concluded the odor impacts would be less than significant. New land uses developed under the Project are not expected to result in substantial odor emissions. Likewise, any odors generated during construction from diesel fuel combustion are expected to be minor. ICF will therefore qualitatively assess odor emissions.

For the impact categories discussed above, this scope of work assumes that all impacts would be less than significant with implementation of Project commitments, applicable ConnectMenlo policies, and/or uniformly applied development policies or standards. Therefore, it is expected that the Project would have similar impacts related to air quality emissions as previously analyzed in the ConnectMenlo EIR.

Exceptions

In addition to investigating the conditions that must be met in order to qualify for a Class 32 CE, the Infill Exemption Document will also assess whether any of the exceptions to qualify for the Class 32 CE are present. The analysis would compare the criteria of the CEQA Guidelines Section 15300.2 (Exceptions) to the Project. Topics include location, cumulative impacts, significant effects, scenic highways, hazardous waste sites, and historical resources. Two of these topics require some further evaluation, as described below.

Hazards and Hazardous Materials

This scope of work assumes the Project Sponsor will provide a Phase I Environmental Site Assessment prepared for the Project site. ICF will use the Phase I Environmental Site Assessment to analyze the hazardous materials present on the Project site. This scope assumes that ICF would incorporate any recommendations made in the Phase I Environmental Site Assessment into the Project Description.

Historical Resources

ICF cultural resources staff will conduct the analyses described below to identify cultural resources that may meet the CEQA definition of a historical resource (California Public Resources Code [PRC] Section 21084 or unique archaeological resource (PRC Section 21083.2) that may be impacted by the Project.

Records Search and Background Literature Review. ICF will conduct a records search at the Northwest Information Center (NWIC) at Sonoma State University, the official state repository for cultural resource reports and records for San Mateo County, to identify recorded cultural resources and studies within and adjacent to the Project site. The records search will include a 0.25-mile radius of the Project site. ICF will review archaeological, ethnographic, historical, geoarchaeological, and environmental documents and maps relevant to the Project site and vicinity. The literature review will include a review of published archaeological, geological, and soil data to characterize the potential for buried surfaces (paleosols) within the Project site, as some paleosols have the potential to be associated with archaeological deposits and human remains.

Built Environment Resources. Based on ICF's review of historic aerial photographs, the two existing buildings at 1020 and 1030 O'Brien Drive that would be demolished as part of the Project were both constructed prior to 1968. The properties are therefore over 45 years of age, the age above which built resources typically may be considered eligible for listing in the California Register of Historical Resources (California Register) and thus could qualify as historical resources under CEQA. Neither of the properties appears to have been previously assessed for CEQA historical resource status. To support the evaluation of built environment resources, ICF's professionally

qualified architectural history staff will conduct a site visit to take photographs and field notes of the historic-aged buildings.

ICF will prepare two Department of Parks and Recreation (DPR) 523-series form sets to document the historic-aged built environment resources proposed for demolition (i.e., one DPR series form set for 1020 O'Brien Drive and one DPR form set for 1030 O'Brien Drive); to assess whether they qualify as CEQA historical resources. Each DPR form set will record an architectural description, photographs from field survey, construction history of the resource, local historic development context, and ICF's application of the California Register of Historical Resources evaluative criteria to the resource.

ICF will prepare a brief Built Resources Technical Memorandum that summarizes the methodology of the investigation, results of the records search and background literature review, and findings of the built resources evaluations (documented on the DPR form sets, which will be appended to the memorandum). The Built Resources Technical Memorandum will be submitted to the NWIC in fulfillment of a requirement to access their archives. ICF will summarize the Built Resources Technical Memorandum in the Historical Resources section of the Infill Exemption Document and the memorandum will be appended to the Infill Exemption Document.

This scope of work assumes:

- The resources located on the Project site proposed for demolition will not ultimately qualify as CEQA historical resources. If an eligible resource is identified, a contract amendment may be required due to the additional complexity of the required analysis.
- Evaluation of historic-aged built resources adjacent to the parcels that would contain the new building will not be required.
- The tenant improvements at 1010 O'Brien would occur under a separate permit. Therefore, this scope of work does not include the tenant improvements as part of the Project. The two existing buildings at 980-990 and 1010 O'Brien Drive would remain as part of the Project. Therefore, this scope of work does not include the evaluation of any the properties at 980-990 and 1010 O'Brien Drive.
- ICF will prepare two drafts (i.e., one draft and one final) of the DPR form sets in response to the City's comments.
- ICF will prepare two drafts (i.e., one draft and one final) of the Built Resources Technical Memorandum in response to the City's comments.

This scope of work assumes the Project has limited potential to cause visual or auditory changes in the environment that would materially impair the significance of adjacent built environment resources.

Archaeological Resources. An ICF qualified archaeologist will conduct a pedestrian surface survey of the Project site to identify archaeological resources in the Project site and document current site conditions. ICF will prepare a brief Archaeological Resources Technical Memorandum that summarizes the findings of the records search and background literature review, findings of the pedestrian surface survey, and the methodology and findings of the archaeological investigation. The Archaeological Resources Technical Memorandum will be submitted to the NWIC in fulfillment of a requirement to access their archives. ICF will summarize the Archaeological Resources Technical Memorandum in the Historical Resources section of the Infill Exemption Document and the memorandum will be appended to the Infill Exemption Document.

This scope of work assumes:

- No archaeological resources will be identified during the records search or pedestrian survey. If archaeological resources are identified, a contract amendment may be required.



- ICF will prepare two drafts (i.e., one draft and one final) of the Archaeological Resources Technical Memorandum in response to the City's comments.

Deliverables: One electronic copy of each draft of the following standalone deliverables in MS Word and Adobe PDF format: peer review comments on the Arborist Report, Biological Resources Assessment Report, Built Resources Technical Memorandum, Archaeological Resources Technical Memorandum, TIA Report; and one electronic copy of the Administrative Draft Infill Exemption Document in MS Word and Adobe PDF format.

Task 4. Screencheck Draft Infill Exemption Document

The purpose of this task is to prepare and submit the Screencheck Draft Infill Exemption Document to the City for review. ICF will prepare a Screencheck Draft Infill Exemption Document to respond to the City's comments on the Administrative Draft Infill Exemption Document. This scope of work assumes that comments from multiple reviewers will be consolidated, conflicting comments will be resolved, and that the comments will not result in substantial revisions (e.g., remodeling) or additional analyses.

Deliverables: One electronic copy of the Screencheck Draft Infill Exemption Document in MS Word and Adobe PDF format.

Task 5. Final Infill Exemption Document

The purpose of this task is to prepare and submit the Final Infill Exemption Document to the City. ICF will prepare the Final Infill Exemption Document to respond to the City's comments on the Screencheck Draft Infill Exemption Document. This scope of work assumes that comments from multiple reviewers will be consolidated, conflicting comments will be resolved, and that the comments will not result in substantial revisions (e.g., remodeling) or additional analyses. This scope of work assumes that no substantive comments on the Screencheck Draft Infill Exemption Document will be provided and the revisions for the Final Draft Infill Exemption Document will be minimal.

This scope of work assumes that ICF will prepare the Notice of Exemption (NOE). ICF will prepare two drafts (i.e., one draft and one final) of the NOE in response to the City's comments. Consistent with recent standard practices in the City, this scope of work assumes that the City will file the NOE with the State Clearinghouse and with the County Clerk; the City will also post the NOE on the City's website.

As previously discussed, this scope of work assumes that all impacts would be less than significant with implementation of Project commitments, applicable ConnectMenlo policies, and/or uniformly applied development policies or standards. If the analysis in the Infill Exemption Document concludes that the Project would result in significant impacts for certain topic(s) and that the Project would still result in a significant impact after implementation of Project commitments, applicable ConnectMenlo policies, and/or uniformly applied development policies or standards, the City and ICF would discuss the next steps. Should the preparation of an Initial Study, MND, EIR, Addendum, Subsequent EIR, or other type of CEQA document be required, this would be performed under an amended scope of work, and is not included in this proposal.

ICF will also submit a full administrative record, documenting and providing backup for all information cited in the Infill Exemption Document. The administrative record will be submitted electronically via email or on a flash drive, with an accompanying matrix indexing each entry.

Deliverables: Up to 20 hard copies of the Final Infill Exemption Document, with the appendices on a flash-drive affixed to the back cover; print-ready and web-ready electronic copies of the Final Infill Exemption Document in MS Word and Adobe PDF format; one electronic copy of the draft and final version of the NOE (in MS Word and PDF formats); and electronic submittal of the administrative record.

Task 6. Project Management, Meetings, and Public Hearing

The purpose of this task is to effectively manage the above tasks and maintain communication with City staff members. ICF's project management team will be responsible for coordination activities, maintaining quality assurance/quality control (QA/QC) requirements for document preparation, and monitoring the schedule and budget for all work tasks. ICF's project management team will also maintain internal communications among ICF staff members, Hexagon, and City staff members through emails and frequent phone contact, along with preparation of all correspondence. ICF's project manager will coordinate the internal staff, Project-specific guidance, and analysis criteria.

This task also includes attending meetings to accomplish the above tasks. Team members will attend and participate in meetings on an as-needed basis. This scope of work assumes up to two members of ICF's project management team will attend up to eight meetings with City staff and one public hearing. This scope of work assumes ICF will attend all meetings and hearings virtually. Additional meetings may be appropriate during the course of this effort and will be invoiced on a time-and-materials basis. If necessary (e.g., if ICF needs to attend more than eight meetings and/or more than one public hearing), ICF will revisit the scope and budget associated with this effort to determine if additional budget is needed.

In terms of progress reporting, ICF will prepare a brief progress report every month documenting the key accomplishments regarding the CEQA process, schedule progress, and identification of any key issues that have arisen that may affect the Infill Exemption Document, budget, or schedule. ICF will also report key deliverable progress to examine task burn rates and determine if budget progress is matching deliverable progress, as necessary. Invoices will include the beginning and ending dates of the billing period, serial identification of progress bills, and a task summary.

Schedule

Appendix C includes ICF's preliminary schedule for the preparation of an Infill Exemption Document.

Cost Estimate

Appendix D includes ICF's cost estimate for the services described in our scope of work. ICF's cost estimate includes a contingency fee of approximately 10 percent, which could be applied to tasks that require a greater than anticipated level of effort (e.g., if the City requests to review more drafts of a technical report than assumed under this scope of work, if changes to the Project result in remodeling for the air quality or noise analyses) or the contingency could be applied to optional tasks. ICF will request approval from the City prior to applying any portion of the contingency fee. In addition, a rate schedule is provided on the following page for all ICF personnel expected to provide services for the Project.

Our scope of work and cost estimate are provided for discussion purposes, based on our preliminary understanding of the project and your needs. They can be adjusted based on your feedback and project developments. We are confident that our experience and qualifications make ICF the best fit for the Project, and we are happy to discuss opportunities to refine our assumptions and identify potential cost efficiencies. In addition, it is important to note that the technical reports and analyses prepared for the Infill Exemption Document (e.g., the Biological Resources Assessment, Built Resources Technical Memorandum, Archaeological Resources Technical Memorandum, TIA Report, air quality analysis in the Infill Exemption Document, and noise analysis in the Infill Exemption document) will also be useful to support an MND or EIR for the Project (if it is determined that either CEQA document is required).

ICF will invoice monthly, on a time and materials basis. Invoices are due net thirty (30) days from receipt. As outlined in Task 6 of our scope of work, ICF will submit monthly invoices for the cost for services performed prior to the invoice date. Included will be a summary of each task, the name of the person doing the work, the hours spent by each person, and a brief description of the work.



To complete the scope of work in a cost-effective manner within the overall proposed budget ceiling, ICF reserves the right to reallocate hours between tasks and labor categories as it deems necessary.



ICF's Rate Schedule

Rates Effective through December 31, 2023

Labor Classification	Per Hour
Senior Project Director	\$300
Project Director	\$255
Technical Director	\$230
Senior Technical Analyst	\$225
Managing Consultant	\$205
Senior Consultant III	\$185
Senior Consultant II	\$170
Senior Consultant I	\$150
Associate Consultant III	\$135
Associate Consultant II	\$125
Associate Consultant I	\$105
Assistant Consultant	\$105
Administrative Technician	\$80
Technician	\$75
Intern	\$70
Other Direct Expenses	
Copy Center Services:	
- Color printing (8.5" x 11"—11" x 17")	\$0.16 to \$0.32/page
- Black & White printing (8.5" x 11"—11" x 17")	\$0.08 to \$0.16/page
Automobile mileage at current IRS rate	\$0.625/mile
Electronic Field Equipment	\$10.00/day
A general and administrative charge of 10% will be applied to all other direct costs, inclusive of subcontractor charges.	
Per diem is charged at \$175.00/day. A lodging surcharge will apply in high rate areas.	
Billing rates are subject to revision effective January 1, 2024.	

*ICF Jones & Stokes, Inc.



Other Relevant Information/Disclosures

To the best of our knowledge and understanding, ICF Jones & Stokes, Inc., is not aware of findings against the firm and does not have judgments against it.

ICF maintains a robust insurance program, providing appropriate coverage limits for the professional services we provide and a company of our size. Some of the main coverages are outlined below:

- (i) Commercial general liability and property (CHUBB), covering bodily injury and property damage, with minimum limits of one million dollars (\$1,000,000) per occurrence and two million dollars (\$2,000,000) general aggregate, including premises liability, products/completed operations, and contractual liability coverage;
- (ii) Business automobile liability (CHUBB), covering owned, hired, and non-owned vehicles, with limits of at least one million dollars (\$1,000,000) combined single-limit (bodily injury and property damage);
- (iii) Workers compensation (CHUBB), as required by all applicable statutory laws and employer's liability coverage, with a limit of not less than one million dollars (\$1,000,000);
- (iv) Umbrella liability (CHUBB) coverage, with minimum limits of five million dollars (\$5,000,000) each occurrence/aggregate, sitting excess of the general liability, business auto liability, and employer's liability programs;
- (v) Professional liability/cyber (CNA) coverage, with minimum limits of five million dollars (\$5,000,000) per claim with respect to negligent acts, errors, or omissions in connection with services to be provided;
- (vi) Cyber liability coverage with minimum limits of five million dollars (\$5,000,000) per claim and sufficiently broad to respond to claims, including, but not limited to, infringement of copyright, trademark, trade dress, invasion of privacy violations, information theft, damage to or destruction of electronic information, release of private information, alteration of electronic information, extortion, and network security;
- (vii) Crime coverage (ZURICH), with limits not less than five million dollars (\$5,000,000) per claim; and
- (viii) Contractors pollution liability (IRONSHORE), with limits not less than one million dollars (\$1,000,000) per occurrence.



Appendix A Resumes



Jessica Viramontes

Senior Environmental Planner

Ms. Viramontes is a project manager with ICF's environment and planning division. She has 16 years of experience in environmental planning and project management. She is skilled in the California Environmental Quality Act (CEQA) process and has worked with both municipal and private sector clients throughout the San Francisco Bay Area, including the San Francisco Planning Department, Oakland Planning and Building Department, and Contra Costa County Public Works Department. She has managed infill development projects and transportation projects of varying sizes. As a project manager, she serves as the day-to-day point of contact and ensures deliverables are on time, within budget, and meet or exceed scope requirements.

Years of Experience

Professional start date: 12/2006
ICF start date: 10/2014

Education

MS, Environmental Management, University of San Francisco, 2011

BS, Environmental Management and Protection, California Polytechnic University San Luis Obispo, 2006

BA, English Literature, California Polytechnic University San Luis Obispo, 2006

Professional Memberships

Association of Environmental Professionals (AEP)

Project Experience

Middle Plaza at 500 El Camino Real Infill EIR—City of Menlo Park, California, 06/2016 – 12/2017

Project Manager. Ms. Viramontes oversaw the preparation of the Infill Checklist and EIR as well as coordinating with the team and the City. Stanford University proposed to redevelop six parcels of land along the east side of El Camino Real into a mixed-use development. The six new buildings at the project site would include approximately 305,000 square feet of residential space (215 housing units); 144,000 square feet of non-medical office space; and 10,000 square feet of ground floor retail/restaurant space. Major environmental issues include transportation, noise, and air quality.

Facebook Menlo Park Campus Expansion EIR and EIR Addendum—City of Menlo Park, California, 05/2015 – 10/2017

Deputy Project Manager for the EIR and Project Manager for the EIR Addendum. Ms. Viramontes was on the management team for both the EIR and the EIR Addendum for the Facebook Campus Expansion Project, which includes the demolition of the existing buildings at the site and the construction of two new office buildings (Buildings 21 and 22), encompassing approximately 985,720 sf (a net increase of approximately 149,880 sf at the Project site). The project would be organized around a 5-acre publicly accessible green space and a bicycle/pedestrian corridor that would run through the middle of the site. The project would also include construction of a new bicycle/pedestrian bridge over Bayfront Expressway to allow for access to the Bay Trail and Bedwell Bayfront Park from the project site and the Belle Haven neighborhood. Major environmental issues include transportation and greenhouse gas emissions.

CEQA Class 14 (Minor Addition to Schools) Exemption and Class 32 (Infill Development Projects) Exemption for the Acacia Building Replacement Project – 09/22 – 10/22

Project Manager. Ms. Viramontes oversaw the preparation of the Class 14 Exemption and Class 32 Exemption. The Project would include the construction of an approximately 55,200-gross-square-foot two-story educational building on an approximately 1.4-acre site at University Park in Stockton, on a satellite campus for California State



University Stanislaus. The primary environmental issue was historic resources.

555 W. Middlefield Road & 777 W. Middlefield Road EIRs—City of Mountain View, California, 06/2017 – 05/2022

Project Manager. Ms. Viramontes served as the day-to-day contact for the City, overseeing the internal section authors, and coordinating with the subconsultants. The 555 W. Middlefield Project would retain the existing, 402 market rate apartment units and construct 341 new apartment units in site locations currently in use as landscape open area and surface parking lots for the existing complex. The project would be phased to maintain onsite parking as development occurs. The 777 W. Middlefield Project would demolish the entire, existing 208 market-rate apartment units and construct 711 new apartment units (including 144 affordable units). The environmental review for the 555 W. Middlefield Project was completed in 2022 and the environmental review for the 777 W. Middlefield Project was completed in 2019. Major environmental issues include transportation, noise, and air quality.

The Village at San Antonio – Phase II Project, Addendum to the San Antonio Precise Plan EIR for Precise Plan Amendments—City of Mountain View, California, 12/2020 – Present

Project Manager. Ms. Viramontes served as the day-to-day contact for the City, overseeing the internal section authors, and coordinating with the subconsultants. The Village at San Antonio – Phase II Project proposes the demolition of two single-story commercial buildings and associated surface parking, and the construction of a commercial building with public open spaces and below-grade parking garage. The project would construct a seven-story building with ground-floor retail and six levels of office uses above, along with three levels of subterranean parking that would connect to the adjacent underground parking. Major environmental issues include transportation, noise, and air quality.

San Francisco Housing Element 2022 Update Environmental Impact Report (EIR)—San Francisco Planning Department, San Francisco, CA, 07/2020 – Present

Project Manager. Ms. Viramontes currently serves as project manager for the San Francisco Housing Element Update 2022 EIR. The EIR will provide a programmatic analysis of the indirect impacts that could result from adoption and implementation of the San Francisco Housing Element 2022 Update, which revises the adopted 2014 Housing Element of the General Plan. The proposed action is evaluating the objective of producing an average of approximately 5,000 housing units per year between 2020 and 2050. Major environmental issues include transportation, cultural resources, air quality, noise, aesthetics, and population and housing.

Station East Mixed Use Project EIR – Integral, Union City, California, 06/2019 – 01/2022

Project Manager. Ms. Viramontes served as the day-to-day contact for the City and oversaw the preparation of the EIR. The project proposes the demolition of the buildings and surface parking lots and development of up to approximately 1.8 million sf, including up to 974 new residential units (apartments, condominiums, and townhome-style condominiums, referred to in this report as townhomes) and approximately 30,800 sf of commercial space. Three community parks, one tot lot, and one outdoor amphitheater would be located throughout the project site. The project site is part of the *Decoto Industrial Park Study Area (DIPSA) Specific Plan* area. Major environmental issues include transportation and hazardous materials.

751 Gateway EIR—City of South San Francisco, California, 11/2019 – 01/2021

Project Manager. Ms. Viramontes served as the day-to-day contact for the City, overseeing the internal section authors, and coordinating with the subconsultants. The proposed building would be constructed on the site of an existing surface parking lot. The proposed project involves the construction of a 148-foot-tall, seven-story building with approximately 208,800 sf of usable space (60 percent R&D uses, and 40 percent office uses). The existing building at 701 Gateway Boulevard would remain. Major environmental issues include transportation (vehicle miles traveled) and air quality.



Victoria R. Chung, MURP

Senior Environmental Planner

Ms. Chung is a Senior Environmental Planner and a San Francisco resident. She has a Master's in Urban and Regional Planning (MURP) from UC Irvine. She has over four years of experience preparing environmental documents in accordance with CEQA for various types of land use projects throughout California. She has worked on General Plan updates, transportation infrastructure improvements, specific plans, multi-family and single-family residential, urban in-fill, bridge shelters, logistics warehouses, and school projects. Her most recent project experience includes preparing documents in the Central Valley region of California, including the City of Kerman General Plan Update EIR, a Specific Plan EIR for Madera County as well as several Initial Studies and EIR Addendums. Prior to becoming an Environmental Planner, she worked in State and local government in southern California specializing in public policy, community engagement, special events, public relations, and social justice advocacy.

Project Experience

Transportation

Regional Transportation Plan and Sustainable Communities Environmental Impact Report—San Joaquin County Association of Governments, San Joaquin County, 10/2021 – 04/2022*

Assistant Project Manager/Environmental Planner. Prepared a Program Environmental Impact Report for the SJCOG Envision 2050 Regional Transportation Plan & Sustainable Communities Strategy (RTP/SCS). Envision 2050 would result in the 2022 RTP/SCS that has policies and strategies to meet future challenges of the San Joaquin region such as growth of the e-economy resulting in increased telecommuting and internet shopping, and the widespread use of autonomous and connected electric vehicles. Ms. Chung is responsible for preparing multiple sections of the EIR including Aesthetics, Geology and Soils, and Wildfire.

2040 Regional Transportation Plan and Sustainable Communities Environmental Impact Report—Santa Barbara County Association of Governments, Santa Barbara County, 05/2021 – 03/2022*

Assistant Project Manager/Environmental Planner. Prepared a Program Supplemental Environmental Impact Report for the SBCAG Fast Forward 2040 Regional Transportation Plan and Sustainable Communities Strategy. The Regional Transportation Plan component includes a list of all transportation improvement projects planned in Santa Barbara County. The Sustainable Communities Strategy identified a countywide land use scenario for the region that would reduce greenhouse gas emissions, primarily through mixed-use development, transit-oriented development, and active transportation modes. Ms. Chung served as a planner for the SBCAG Fast Forward 2040 RTS/SCS Environmental Impact Report effort. Ms. Chung prepared sections of the EIR including the Air Quality, Energy and Aesthetics sections.

Years of Experience

Professional start date: 05/2016
ICF start date: 05/2022

Education

Master's Degree, Urban and Regional Planning, UC Irvine, 2018

Bachelor's Degree, Political Science, CSU Long Beach, 2013

Professional Affiliations

APACA Central Section, Special Projects Coordinator, 2019-Present

Association of Environmental Professionals (AEP), Member, 2018-Present

APACA Orange Section Board, Member 2016-2018

Urban Planning Student Association, UC Irvine, APA Student Representative, 2016–2017



Construction Package No. 1, Ballast Track to Slab Track Re-Examination – California High Speed Rail Authority – Merced to Fresno, Merced, and Fresno Counties*

Environmental Planner. Rincon is currently providing environmental compliance oversight as a member of the Project Construction Management team for the Merced to Fresno Construction Package No. 1 portion of the California High-Speed Rail project. The re-examination documents minor modifications to the project since approval of the original environmental document (Final EIR/EIS). Ms. Chung assisted in preparing the re-examination of the Final EIR/EIS for the proposed Ballast track to Slab track. This includes all aspects of environmental compliance such as biological resources, cultural resources, storm water pollution prevention, paleontological resources, air quality, noise, hazardous materials, and aesthetics.

San Ysidro Road Roundabout Addendum–County of Santa Barbara, Santa Barbara County*

Environmental Planner. Ms. Chung was the lead author of the Addendum to the Santa Barbara County South Coast 101 HOV Lane Project. The Addendum evaluated the proposed project's potential environmental issues as it pertains to the Caltrans 101 HOV lane project. The analysis included incorporation of several technical documents including a Visual Impact Assessment, a Natural Environment Study-Minimal Impact, Tree Root Analysis and a Tree Replacement Plan.

Land Use/Contract Planning

1125 O'Brien EIR–City of Menlo Park, Menlo Park, 05/2022-Present

Project Manager/Environmental Planner. Ms. Chung manages the EIR for the project for this contract which includes the redevelopment of an existing office park and would construct an approximately 131,284-square-foot (sf) building for research and development (R&D) uses and surface parking. The project includes the preparation of an initial study tiered from the ConnectMenlo Program EIR and subsequent Focused EIR. Ms. Chung prepared sections of the EIR including the executive summary and alternatives.

General Plan Update Program Environmental Impact Report–City of Kerman, Kerman, 06/2019–01/2021*

Assistant Project Manager/Environmental Planner. Ms. Chung was responsible for drafting several sections of the Program Environmental Impact Report for the Kerman General Plan Update. The Program Environmental Impact Report and General Plan were constructed in coordination to be self-mitigating. Potential environmental issues identified were addressed through the development of General Plan policies to address the impact. This included assessing transportation impacts based on a Vehicle Miles Travelled threshold.

Circuit Planning and Engineering–Fresno Council of Governments, Fresno County, 06/2019 – 05/2022 *

Assistant Project Manager/Environmental Planner. Ms. Chung provided support to the project manager for this contract which includes the 13 smaller cities in Fresno County, implementing Blueprint Smart Growth principles in conducting specific planning and engineering projects tailored to each City. These projects included: City of Selma Downtown Overlay Zone/Zoning Code Update Project, City of San Joaquin Zoning Code Update Assessment, and community outreach.

Recognition and Commendations

Awards

- APA California, Comprehensive Plan Award (Small), Kerman General Plan Update and Program EIR, 2021
- APA Central Section, Merit in Planning Award-Comprehensive Planning; Small Jurisdiction Category, City of Kerman General Plan Update and Program EIR, 2021

Employment History

Rincon Consultants, Inc. Assistant Project Manager/Environmental Planner. Fresno, CA. 05/2019 – 05/2022.

FirstCarbon Solutions. Environmental Services Analyst. Irvine, CA. 04/2018 – 05/2019.

City of Long Beach. First District, Communications Director. Long Beach, CA. 10/2014 – 08/2016.

State of California. 70th Assembly District, Field Representative. Long Beach, CA. 05/2013 – 10/2014.

*These projects were completed under Rincon Consultants, Inc.



Kirsten Chapman, BA

Ms. Chapman has over 15 years of experience in project management/coordination and environmental planning. She is skilled in the California Environmental Quality Act (CEQA)/National Environmental Policy Act (NEPA) process and has worked with municipal and federal clients, particularly in the San Francisco Bay Area. Ms. Chapman has focused on large, mixed-use infill development projects, tech campuses, office developments, and housing projects. She is responsible for writing sections for CEQA documents such as initial studies (ISs), mitigated negative declarations (MNDs), draft environmental impact reports (EIRs), final EIRs, and statements of overriding considerations. Ms. Chapman leads multidisciplinary teams through project management and conducts environmental analysis, prepares technical reports, performs quality control (QC)/quality assurance (QA), and edits documents for production. Project management responsibilities include client and internal team coordination, development of the scope of work and budget, project initiation tasks, subconsultant communication, preparation of progress reports, and contracting/billing. She has helped manage key projects such as the Willow Village Master Plan EIR, the Facebook Campus Expansion EIR, the City Place Santa Clara EIR, the Seawall Lot 337/Pier 48 (Mission Rock) Mixed-Use EIR, and the Burlingame Point EIR.

Years of Experience

Professional start date: 02/2007
ICF start date: 01/2013

Education

BA, Environmental Studies/Economics, University of California, Santa Cruz, 2007

BA, Politics, with honors, University of California, Santa Cruz, 2007

Project Experience

Willow Village Master Plan EIR—City of Menlo Park, CA, 01/2018 – Present

Senior Advisor/Former Project Manager. Ms. Chapman conducts project management tasks such as coordinating with the client and subconsultants, tracking billing and invoices, overseeing staff, reviewing and producing documents, and leading meetings and conference calls. The project sponsor, on behalf of Meta/Facebook, Inc., is proposing to redevelop an existing, approximately 59-acre industrial site as a multiphase, mixed-use development. The Willow Village Master Plan project would demolish existing onsite buildings and construct new buildings within a residential/shopping district, town square district, and campus district. The project would increase the area for nonresidential uses (i.e., office space and nonoffice commercial/retail) by approximately 1 million square feet (sq. ft.), for a total of approximately 2 million sq. ft. of nonresidential uses. The proposed project would also include multifamily housing units, a hotel, indoor space dedicated for community facilities/uses, park buildings/improvements, open space, and a bicycle/pedestrian tunnel.

1350 Adams Court-Lot 3 Project EIR—City of Menlo Park

Senior Advisor/Former Project Manager. Overseeing the preparation of the Initial Study and EIR, which tiers off of the Menlo Park General Plan EIR (ConnectMenlo). Writing section, conducting senior review, and coordinating subconsultants. The Project Sponsor is proposing to construct an approximately 260,400 gross-square-foot (gsf) building for life science (research and development) uses. Parking for the new building would be provided in both podium-level and above-grade garages that would be integrated into the building.



The Initial Study and EIR discloses relevant impacts and mitigation measures covered in the ConnectMenlo EIR and discusses whether the Project is within the parameters of the ConnectMenlo EIR.

Commonwealth Building 3 Project EIR—City of Menlo Park

Senior Advisor/Former Project Manager. Overseeing the preparation of the Initial Study and EIR, which tiers off of the Menlo Park General Plan EIR (ConnectMenlo). Writing section, conducting senior review, and coordinating subconsultants. The Sobrato Organization (Project Sponsor) is proposing to construct an approximately 249,500-gross-square-foot (gsf) office building and an approximately 349,100 gsf parking structure as part of the Commonwealth: Building 3 Project (Project). The Project site is the existing Commonwealth Corporate Center property, which includes the Commonwealth Site at 162 and 164 Jefferson Drive and the Jefferson Site (also at 164 Jefferson Drive). Two buildings (Buildings 1 and 2), currently occupied by Facebook, were constructed at the Project site as part of the Commonwealth Corporate Center Project. The Project would add a four-story office building (Building 3) and a five-story parking structure with 1,061 parking spaces to the Project site.

Facebook Campus Expansion Project EIR—City of Menlo Park

Served as Project Manager. Conducted project management tasks such as coordination with the client and subconsultants, tracked billing and invoices, oversaw staff, reviewed and produced the documents, and lead meetings and conference calls. The proposed Facebook Campus Expansion Project included the demolition of the existing buildings at the site and the construction of two new office buildings (Buildings 21 and 22), encompassing approximately 985,720 sf (a net increase of approximately 149,880 sf at the Project site). The Project would be organized around a 5-acre publicly accessible green space and a bicycle/pedestrian corridor that would run through the middle of the site. The Project would also include construction of a new bicycle/pedestrian bridge over Bayfront Expressway to allow for access to the Bay Trail and Bedwell Bayfront Park from the Project site and the Belle Haven neighborhood.

1300 El Camino Real Project Infill Checklist and Infill EIR—City of Menlo Park

Served as Deputy Project Manager. The Project's development parameters are consistent with the development anticipated by the El Camino Real/Downtown Specific Plan. Therefore, CEQA analysis for this Project demonstrates consistency with SB 226, CEQA Streamlining for Infill Projects. Kirsten led the ICF team in preparing an Infill Environmental Checklist, followed by a focused Infill EIR. The Project would demolish the existing structures and construct approximately 420,000 sf of mixed uses. In total, the Project would include three mixed-use buildings up to four stories in height, a surface parking lot, an underground parking garage, onsite linkages, landscaping, and a privately-owned, publicly accessible park. The uses at the Project site would include a range of approximately: 200,000 sf of non-medical office space in two buildings; 200,000 sf of residential space (up to 202 housing units) in one building; and 30,000 sf of community-serving space.

SRI International Campus Modernization Project EIR—City of Menlo Park

Served as Deputy Project Manager. Conducted project management tasks such as progress reports, scope/budget development, contract preparation and tracking, client and internal coordination, and working with subconsultants. SRI International is proposing to modernize its Campus with phased development over the next 25 years, amend the existing employee cap, and modify other land use regulations governing the site.

Commonwealth Corporate Center Project EIR—City of Menlo Park

Served as Deputy Project Manager. Conducted project management tasks such as progress reports, scope/budget development, contract preparation and tracking, client and internal coordination, and working with subconsultants. The project, which requires an EIR, involves two four-story office buildings totaling 237,000 sf of office, biotech, and R&D uses.



Laura Yoon, MS

Managing Director

Ms. Yoon is an air quality and climate change managing director with experience in preparing criteria pollutant and greenhouse gas (GHG) inventories for both public and private sector projects. She focuses on technical modeling and report preparation in support of California Environmental Quality Act (CEQA), National Environmental Policy Act (NEPA), and recent GHG legislation. Laura's expertise includes air quality impact studies; conformity analyses; dispersion modeling and health risk assessments; GHG inventories; and climate action plan (CAP) development. She has served as the technical lead and project manager for numerous air quality and climate change analyses throughout California. Prior project work provides a solid background for understanding and evaluating air quality, climate change, and energy impacts from projects.

Years of Experience

Professional start date: 06/2009
ICF start date: 06/2009

Education

MS, Environmental Management,
University of San Francisco, 2013

BA (summa cum laude),
Environmental Studies (minor in
Resource Management),
University of Washington, 2009

Certifications/Registrations

None.

Professional Affiliations

Association of Environmental
Professionals (2010 – Present)

Association of Women in Water,
Energy, and Environment (2020 –
2021)

Project Experience

Bayhill Specific Plan EIR—City of San Bruno, San Bruno, CA, 05/2018 – 12/2021

Air Quality and Climate Change Specialist. Laura oversaw the air quality and climate change chapters for the EIR. The Bayhill Specific Plan will outline a cohesive, long-term, community driven vision for this key district, that is home to the largest cluster of offices in San Bruno, including headquarters of YouTube, as well as several other uses. Construction and operational emissions were quantified using CalEEMod, EMFAC, the EPA's AP 4.2, and other accepted tools. A detailed health risk assessment was also prepared to analyze potential receptor exposure to diesel emissions generated during construction of the new YouTube headquarters.

Belmont General Plan and Specific Plan EIR—City of Belmont, CA, 01/2017 – 04/2019

Air Quality and Climate Change Specialist. Laura prepared the air quality and climate change changes for the City of Belmont's 2035 General Plan Update and Belmont Village Specific Plan. Construction emissions from buildout of both the General Plan and Specific Plan were estimated using CalEEMod. Air quality and GHG impacts from motor vehicles operating within the General Plan and Specific Plan areas were evaluated using the CT-EMFAC2014 model whereas area and energy emissions were estimated using CalEEMod. Carbon monoxide hot-spots from increased traffic were modeled using the CALINE4 dispersion model. The analysis also evaluated health risks from receptor exposure to asbestos containing material and particulate matter. GHG emissions from buildout of the General Plan were evaluated by examining consistency of the plan, which includes the CAP, with the recommendations of the California Air Resources Board for municipalities to support Assembly Bill 32, Senate Bill 32, and Executive Order S-3-05 reduction targets.

City Place Santa Clara EIR—RELATESC, Santa Clara, CA, 03/2013 – 04/2017

Air quality and climate change specialist. The project is a 240 acre multiphased, mixed-use City neighborhood including up to 9.16 million gross square feet of office buildings, retail and entertainment



facilities, residential units, and hotel rooms, and would also include surface and structured parking facilities. Laura helped prepare the health risk assessment for the project and conducted emissions modeling. She evaluated air quality impacts using CalEEMod, EMFAC, marine emission factors from the California Air Resources Board, the EPA's AP 4.2, and other resources. She prepared a detailed health risk assessment using AERMOD and HARP.

San Francisco Giants Mission Rock Seawall Lot 337 Pier 48 EIR—Seawall Lot 37 Associates LLC, San Francisco, CA, 6/2013 – 2/2016

Air quality and climate change specialist. Laura served as technical analyst for CEQA review, conducting the air quality analysis, HRA, and climate change analysis. Seawall Lot 337, LLC proposes a mixed-use, multiphase waterfront development of Seawall Lot 337, rehabilitation/reuse of Pier 48, and construction of associated open spaces, public access areas, assembly areas, and an internal grid of new streets and utilities. In total, the project would include approximately 3.6 million gross square feet of flexible development including residential, commercial, and retail uses. ICF led a multidisciplinary team to evaluate the changes to the project site. Air quality impacts were evaluated using CalEEMod, EMFAC, marine emission factors from the California Air Resources Board, the EPA's AP 4.2, and other resources. A detailed health risk assessment was prepared using AERMOD and HARP.

Folsom Center for Health Master Plan—UC Davis Health, Sacramento, California, 5/2021 – ongoing

Air Quality and Climate Change Specialist. Through the Master Services Agreement with UC Davis Health, Laura is leading the air quality and GHG analyst for this program and project-level EIR. The project includes include a 110,000-sf medical office building, a 114,000-sf ambulatory surgery center, an 80,000-sf hotel, an 86,000-sf micro-hospital, a Central Utility Plant (CUP), and approximately 1,357 parking stalls. Laura is quantifying construction and operational emissions using CalEEMod and other accepted models. Heath risks to existing and planned future development are being analyzed using AERMOD. The EIR will evaluate project consistency with the City of Folsom's CAP, UC Davis' Sustainability Policy and Carbon Neutrality Initiative, and State climate change goals.

Greenhouse Gas Mitigation Measures and California Emissions Estimator Model—Sacramento Metropolitan Air Quality Management District, California, 2020 – 2022

Project manager. Laura lead a team of ICF subject matter experts and three subconsultants to incorporate climate adaptation and environmental justice into the California Air Pollution Control Officers Association (CAPCOA) *Quantifying Greenhouse Gas Mitigation Measures* handbook and to expand the GHG reduction measures to reflect the latest methodologies and best practices. Concurrently, ICF transformed CalEEMod into a web-based platform that integrated the updated Handbook to help mainstream climate adaptation and public health planning into project-level analysis.

Awards for Managed Projects

Climate Change Business Journal Achievement Award (2021). Advancing Best Practices Award for *Handbook for Analyzing Greenhouse Gas Emission Reductions, Assessing Climate Vulnerabilities, and Advancing Health and Equity: Designed for Local Governments, Communities, and Project Developers* ("Handbook"). Laura severed as the project manager and lead analyst for the project.

American Planning Association, Sacramento Valley Section (2022). Award of Merit – Best Practices for the "Handbook".

American Planning Association (2015). Award of Merit – Comprehensive Plan, Large Jurisdiction, for the *Los Angeles County General Plan Update*. Laura severed as the project manager and lead analyst for the Climate Action Plan, which was part of the General Plan Update.



Years of Experience

Professional start date: 01/2011
ICF start date: 01/2011

Education

BA, Atmospheric Science,
University of California Berkeley,
2009

Cory Matsui, BA

Manager—Air Quality and Climate Change

Mr. Matsui is a manager and senior air quality, climate change, and noise specialist, with experience in environmental impact analysis in accordance with the California Environmental Quality Act. In his 11 years of experience in the field of environmental science, Mr. Matsui has analyzed a diverse suite of projects, including rail and roadway projects, mixed-use development projects, and infrastructure projects. He is a talented writer and excels at drafting compelling narratives. His skill set also includes emissions modeling, quantitative Excel-based assessments, report preparation, and noise monitoring surveys. Mr. Matsui's expertise includes point-, area-, and mobile-source air quality impact studies; greenhouse gas (GHG) emissions inventory and reduction plan development; air quality conformity analyses; and air quality dispersion modeling. He has experience with standard air quality modeling software including EMFAC, AERMOD, CALRoads, and CALEEMOD, and with the Federal Highway Administration's Roadway Construction Noise Model and Traffic Noise Model, and standard noise protocols.

Project Experience

City of San Francisco Housing Element Update—San Francisco, CA, 03/2020 – Present

Lead Analyst. Mr. Matsui serves as lead analyst for the analysis of environmental impacts in the air quality and GHG resource areas resulting from the update of the city's Housing Element. He has used complex air quality modeling results and distilled them into a streamlined narrative, presenting the impacts of the Housing Element update in a reader-friendly and conclusive manner.

Updates to the Quantifying GHG Mitigation Measures—Sacramento Metropolitan Air Quality Management District, Sacramento, CA, 08/2020 – Present

Emissions Reduction Analyst. Mr. Matsui serves as lead analyst in the effort to update the California Air Pollution Control Officers Association's (CAPCOA's) *Quantifying Greenhouse Gas Mitigation Measures* handbook and expand the GHG reduction measures to reflect latest methodologies and best practices. He leads and guides other analysts to perform a technically sound update of one of the most respected GHG reduction measure guidance documents.

Facebook Constitution Campus Expansion EIR and Addendum, Menlo Park, California

Mr. Matsui served as a technical expert for the air quality and noise analyses for the Facebook Constitution Campus Expansion Project Environmental Impact Report and EIR addendum. He conducted long-term and short-term noise measurements at the existing campus facility to evaluate the ambient noise levels, and quantified construction- and operational-related criteria pollutant and greenhouse gas emissions and evaluated the project's impact with respect to the Bay Area Air Quality Management District's guidelines.

One Vassar Project Community Plan Exemption, San Francisco, CA, 02/2019 – 02/2020

Lead Analyst and Senior Reviewer. Mr. Matsui served as lead analyst and author of the noise technical report and as senior reviewer of the air quality technical report for a mixed-used office building. He led a noise monitoring survey and conducted a comprehensive noise analysis of the project. He also performed quality assurance (QA) and thoroughly reviewed the air quality technical report. Because of Mr. Matsui's contributions, the project was able to successfully document consistency with the Central SoMa Plan.

3700 California Street—TMG Partners, San Francisco, CA, 07/2018 – 02/2020

Lead Analyst. Mr. Matsui served as lead author and technical specialist of the noise analysis for an environmental impact report (EIR) for a large, multiblock housing development project in San Francisco. He conducted noise measurements at the project site to determine existing noise levels and assessed construction and operational noise and vibration impacts of the project in accordance with the City of San Francisco's noise guidelines.

Santa Clara Building V5 Data Center Project & Santa Clara Building V6 Data Center Project – Santa Clara, California

Conducted air quality and greenhouse gas analyses for two data center projects in the City of Santa Clara. Evaluated the impacts of construction and operational criteria pollutant emissions, including the impacts from a large number of back-up diesel generators at each project site. Additionally, Cory successfully assessed the significance of the projects' impacts on greenhouse gases, taking into consideration the substantial energy consumption typically associated with data centers.

Sonoma County Climate Action 2020—Sonoma County Regional Climate Protection Agency, County of Sonoma, CA, 04/2013 – 07/2016

Lead Technical Analyst. Mr. Matsui served as lead analyst in the development of a GHG emissions inventory—backcast and forecast—prepared for the Sonoma County Regional Climate Protection Agency, for nine jurisdictions in the county. He was one of the primary analysts to quantify GHG emissions for all standard sectors (i.e., building energy, water, waste, etc.) in accordance with the Local Governments for Sustainability's (ICLEI's) Community Greenhouse Gas guidelines. Mr. Matsui developed and quantified GHG reduction measures for GHG reduction potential to help the county achieve its GHG reduction goal. He constructed a GHG reduction planning tool for municipal jurisdictions to assess GHG reductions associated with each reduction measure, based on user-inputted commitment levels. Results of the GHG inventory and reduction analyses were incorporated into a comprehensive climate action plan document, for which Mr. Matsui served as a primary author.

Station East Residential/Mixed-Use Project – City of Union City, CA

Mr. Matsui was the senior air quality and greenhouse gas reviewer and lead noise analyst for a large mixed-used development project in Union City. Cory provided expert air quality oversight on the air quality analysis and comprehensively addressed comments from the public. He also led a noise monitoring survey and thoroughly evaluated noise impacts from project construction and operations. The project is a residential and commercial development in Union City, in close proximity to the BART station, and would provide much-needed housing units in a transit rich area.

220 Park Road Development Initial Study – City of Burlingame, California

Cory was the lead air quality analyst for an office and retail development in the former post-office building in downtown Burlingame. Cory efficiently modelled the project's construction and operational emissions in accordance with Bay Area Air Quality Management District guidelines, and quantified the health risks for people living near the project site.



Lisa Webber, MS

Botanist/Wetland Ecologist

Lisa Webber specializes in coordinating and conducting botanical field surveys and wetland delineations of study areas in vegetation communities throughout northern California. She prepares California Environmental Quality Act (CEQA) and National Environmental Policy Act (NEPA) documentation, including environmental impacts reports (EIRs), wetland delineations, and various forms of environmental documentation for a wide variety of projects, including specific plans and master plans for development and redevelopment. Lisa also prepares application packages and coordinates with agency staff for Clean Water Act (CWA) Section 404 permits, California Department of Fish and Wildlife (CDFW) streambed alteration agreements, and Endangered Species Act (ESA) Section 10a requirements.

Years of Experience

Professional start date: 10/1990
ICF start date: 05/1998

Education

MS, Botany, University of Massachusetts, Amherst, 1987

BA, Biology, University of California, Santa Cruz, 1980

Professional Affiliations

California Native Plant Society

Project Experience

San Francisco Housing Element 2022 Update EIR—San Francisco Planning Department, San Francisco, CA, 01/2022 – Present

Biologist. Lisa assisted with preparation of the biological resources section of the administrative draft EIR for the proposed update of the adopted 2014 housing element of the San Francisco General Plan. She provided peer review and guidance for the special-status plants and aquatic resources impact analyses.

San Rafael Transit Center Replacement Project—Golden Gate Bridge, Highway and Transportation District, San Rafael, CA, 07/2020 – Present

Biologist. Lisa performed field studies, including botanical surveys, evaluation of vegetation communities, and an assessment of aquatic resources in an urban area (downtown San Rafael) proposed for relocation of a transit center. She prepared the botanical and aquatic resources sections of the project EIR.

Geary Road Bridge Replacement Project IS/MND—San Francisco Public Utilities Commission (SFPUC), Alameda County, CA, 01/2010 – 07/2012

Biologist. Lisa conducted botanical surveys and the delineation of waters of the US. She prepared the botanical survey technical report, wetland delineation report, and IS/MND for a proposed bridge replacement over Alameda Creek in the Sunol Regional Wilderness Park.

Housing-Related Code Amendments DEIR—County of Placer, California, 04/2020 – 10/2020

Biologist. Lisa prepared the vegetation and aquatic resource sections of the draft EIR for proposed amendments to the Placer County General Plan, Placer County Zoning Ordinance, Zoning Combining Districts, and Community Design Manual for Multi-Family and Mixed-Use Development, which would provide a framework for

future housing development in the parts of Placer County that are under County jurisdiction.

Central El Dorado Hills Specific Plan EIR—El Dorado County, CA, 2012 – Present

Biologist. Prepared the vegetation and wetland EIR sections for a 340-acre regional community plan in the El Dorado Hills Community Region. Background technical reports including vegetation mapping, special-status plant surveys, and a delineation of waters of the U.S. were incorporated into the biological resources chapter of the EIR. A reconnaissance survey of the site was conducted to review and verify the data provided. Sensitive resource issues included oak woodlands, riparian habitat, and waters of the U.S.

Village of Marble Valley Specific Plan EIR—El Dorado County, CA, 2012 – Present

Biologist. Lisa prepared the vegetation and wetland sections for the draft EIR for a proposed regional community plan located on 1,875 acres south of SR 50 in El Dorado Hills. Background technical reports, including vegetation mapping, special-status plant surveys, and a delineation of waters of the U.S. were provided and incorporated into the biological resources chapter of the EIR. A reconnaissance survey of the site was conducted to review and verify the data provided. Sensitive resource issues included oak woodlands, riparian habitat, waters of the U.S., and a special-status plant species.

Lime Rock Valley Specific Plan EIR—El Dorado County, California, 2012 – Present

Biologist. Lisa prepared the vegetation and wetland sections for the draft EIR for a proposed regional community plan located on 740 acres south of SR 50 in El Dorado Hills. Background technical reports, including vegetation mapping, special-status plant surveys, and a delineation of waters of the U.S. were provided and incorporated into the biological resources chapter of the EIR. A reconnaissance survey of the site was conducted to review and verify the data provided. Sensitive resource issues included oak woodlands, riparian habitat, waters of the U.S., and two special-status plant species.

U.C. Davis Sacramento Campus Long-Range Development Plan, Hospital Tower, and Aggie Square Projects EIRs—City of Sacramento, California, 02/2020 – 04/2022

Biologist. Lisa conducted botanical surveys and an assessment for aquatic resources in the UCD Health LRDP area, which included two specific project areas. She prepared the botanical and wetland sections of the programmatic EIR for the plan and the project-level EIRs for the Hospital Tower and Aggie Square projects.

West Sacramento General Plan Update EIR and Liberty Island Specific Plan EIR—City of West Sacramento, CA, 8/2015 – 9/2106

Biologist. Lisa reviewed information from the General Plan Public Review Draft Background Report, Liberty Island Specific Plan, the CNDDDB, CNPS Inventory, and USFWS species lists. She conducted program-level and project-level analyses and prepared the vegetation and wetland portions of the General Plan Update EIR and Specific Plan EIR.

University District Specific Plan EIR—City of Rohnert Park, CA, 01/2004 – 11/2005

Biologist. Lisa prepared the botanical and wetland portions of an EIR for a commercial center, affordable housing at a variety of densities, including for-sale affordable housing, parks, open space preserves, trails, school, and a variety of housing types. The EIR will serve as a program-level EIR for the entire specific plan area and as a project-level EIR for a portion of the specific plan area. Major issues included wetlands and endangered species. The EIR was certified in May 2006.

River Park Project EIR—City of West Sacramento, California, 01/2005 – 2006

Biologist. Lisa prepared the botanical and wetland portions of a draft EIR. This project will include approximately 2,788 residential units (including rural residential and low-, medium-, and high-density offerings), a 44-acre regional park, community open space areas, a school, new roads, and a marina. Key biological resource issues include loss of habitat and realignment of an existing agricultural irrigation ditch at the site, which would be expanded and redesigned as an open water/emergent marsh habitat amenity along the Sacramento River.



Lora Holland, MA, RPA

Senior Archaeologist

Lora is a Registered Professional Archaeologist RPA#989173 and is a qualified professional archaeologist (36 CFR 61) with over 19 years of archaeological field and management experience on various project sites, both terrestrial and maritime. Lora has 12 years of cultural resources management experience on projects throughout California and served as a principal investigator, project manager, authored and overseen the drafting of cultural resources studies for local, state, and federal agencies in compliance with CEQA and Section 106. Lora's experience and expertise include survey testing and data recovery; monitoring; contractor training; archival research; artifact analysis and conservation; field staff supervision; cultural resources identification and eligibility evaluations, impact assessments, and mitigations; assisting agencies with Native American outreach and consultation; Section 106 studies for Caltrans, California High Speed Rail Authority, U.S. Department of Housing and Urban Development, U.S. Army Corps of Engineers, U.S. Bureau of Reclamation, and the Federal Aviation Administration.

Years of Experience

Professional start date: 12/2002
ICF start date: 11/2021

Education

MA, Anthropology, University of West Florida, Pensacola, FL, 2006

BA History, Salem College, Winston-Salem, NC, 2000

Project Experience

Bay Area Ridge Trail: Fremont to Garin Project- East Bay Regional Parks District (EBRPD) Fremont and Union City, Alameda County, California, 2017 – 2018

Principal Investigator/Cultural Resources Task Lead (work conducted prior to employment with ICF).

Lora conducted the cultural resources study and authored the report in support of proposed construction of approximately 2.2 miles of new nonmotorized multiuse recreational trail. The study consisted of background research, including a records search and a literature review of the proposed Area of Potential Effects (APE); a pedestrian field survey; and a Sacred Lands File search request with the Native American Heritage Commission. These tasks identified CA-ALA-548H/P-01-000227, an archaeological site with precontact and historic-period components, including the exposed foundations of an 1856 mill. Human remains were also identified during the survey. Lora worked with the EBRPD and the Native American Most Likely Descendant to design the trail access to avoid impacts to this resource. The project required an Army Corps of Engineers (Corps) Section 404 Nationwide Permit; due to this, the study complied with Section 106 of the National Historic Preservation Act (Section 106) and supported the project's obligations under CEQA.

California High-Speed Rail Project, Bakersfield to Palmdale Project Section- California High Speed Rail Authority, Kern and Los Angeles, Counties, 2015 – 2020

Co-Principal Investigator (work conducted prior to employment with ICF). Lora conducted the archaeological background studies, supervised the field survey and site recordation, prepared sections of the Archaeological Survey Reports (ASR) Finding of Effect Report (FOE), APE mapping, and Treatment Plan.



Iron Horse Trail Bollinger Canyon Overcrossing Project-City of San Ramon Public Works Department, San Ramon, Contra Costa County, California, 2019 – 2020

Principal Investigator/Cultural Resources Task Lead (work conducted prior to employment with ICF). Lora managed and conducted the cultural resources study in compliance with Caltrans' regulatory responsibilities under Section 106. The study consisted of background research, including a records search and a literature review of the proposed APE; a pedestrian field survey; a Sacred Lands File search request with the Native American Heritage Commission; and preparation of an Archaeological Survey Report, and Historic Property Survey Report. The City of San Ramon Public Works Department obtained NEPA clearance for the proposed Bollinger Canyon Overcrossing Project in April 2020.

South County Recycled Water Pipeline Phase 1B/2A Project -Santa Clara Valley Water District, City of Gilroy, Santa Clara County, California, 2017 – 2018

Project Manager/Cultural Resources Task Lead (work conducted prior to employment with ICF).

Lora conducted the cultural resources study in support of the proposed installation of 12,200 linear feet of recycled water transmission and distribution pipeline. Because the project was funded by the Bureau of Reclamation, this study was conducted to address requirements of Section 106. The study consisted of background research, including a records search and a literature review of the proposed APE; a pedestrian field survey; and a Sacred Lands File search request with the Native American Heritage Commission. The report was prepared in accordance with BOR Mid-Pacific Region General Scope of Work for Cultural Resources Investigations in California.

Carmel Riverbank Stabilization Project-Monterey Peninsula Water Management District, Carmel, Monterey County, California, 2017 – 2018.

Project Manager/Cultural Resources Task Lead (work conducted prior to employment with ICF).

Lora conducted the cultural resources study proposed Carmel Riverbank stabilization at San Carlos Road Project in Carmel, Monterey County, California. The project required a Corps Section 404 Nationwide Permit; due to this, the study also complied with Section 106. The study consisted of background research, including a records search and a literature review of the proposed APE; a pedestrian field survey; a Sacred Lands File search request with the Native American Heritage Commission; consultation with local Native American tribes and interested parties; eligibility evaluation, and FOE. The study identified the Rancho San Carlos Road Sedge Bed (sedge bed) within the APE. This sedge bed represents a Native American plant gathering area on the bank of Carmel River that is an important source of Santa Barbara sedge (*Carex barbarae*) for the Rumsen Ohlone.

Alpine Road Trail Improvements Project- Midpeninsula Regional Open Space District, San Mateo County, California San Mateo County, California, 2019 – 2020

Project Archaeologist (work conducted prior to employment with ICF). The proposed project consisted of the repairs and rehabilitation of the failed sections of the Alpine Road Trail at Coal Creek Open Space Preserve. Due to lack of maintenance and recent storms, this former vehicle road has been damaged by landslides and other slope failures, including the failure of a 220-foot-long, 48-inch-wide culvert. Ms. Holland conducted the cultural resources survey and assisted in the preparation of the environmental documentation in compliance with California Environmental Quality Act (CEQA) and Section 106.

San Francisco International Airport Runway Safety Project -City and County of San Francisco, San Mateo County, California, 2012 – 2015

Cultural Resources Task Lead (work conducted prior to employment with ICF). The City and County of San Francisco, as owner and operator of San Francisco International Airport (SFO), constructed various improvements to the Runway Safety Areas of runways to enhance safety at SFO. In accordance with the mitigation requirements for development of this project, Lora and monitoring staff conducted archaeological monitoring of excavation activities as required by the Section 106 consultation process between the Federal Aviation Administration and the State Historic Preservation Office. Ms. Holland served as the cultural resources lead for the multi-year project, coordinated the cultural resources monitoring, oversaw monitoring staff, and prepared the Archaeological Monitoring Reports.



Susan Lassell, MA, BS

Senior Historic Preservation Planner

Susan Lassell serves as ICF's cultural resources practice lead for the Northern California and Pacific Northwest region. Susan's technical leadership includes her ability to smoothly navigate highly complex projects and direct multi-disciplinary teams through NEPA, CEQA, and Section 106 compliance. Susan works closely with clients to develop cultural resources regulatory compliance strategies, including developing programmatic agreements under Section 106 and strategies for engaging tribal and historic preservation stakeholders. Susan has authored and directed projects throughout the United States with a focus on the West Coast, Mid-Atlantic, and Texas, including built environment survey and evaluation reports, cultural resources management plans, interpretive media and displays, and the full range of environmental documents. She also develops and teaches environmental education courses for UC Davis Extension and local and state agencies. Through a combination of 25 years of experience and her master's degree in historic preservation planning from Cornell University, Susan meets the Secretary of the Interior's professional qualification standards for architectural history, history, and preservation planning.

Years of Experience

Professional start date: 01/1994
ICF start date: 11/2014 – Present
and 03/1997 – 11/2002

Education

MA, Historic Preservation
Planning, Cornell University, 1994

BS, Environmental Design,
University of California, Davis,
1991

Professional Affiliations

Member, California Preservation
Foundation (2010-present)

Former Vice President of Planning
and Board President, Preservation
Texas, Inc 2004-2010

Project Experience

Parkline Master Plan EIR—City of Menlo Park, California, 12/2022 – Present

Technical Lead. ICF is conducting CEQA analysis for the proposed redevelopment of the former Stanford Research Institute (SRI) campus in Menlo Park and performing a peer review of the historical resources evaluation report prepared by the applicant's consultant. As one of the first detonation points for Silicon Valley's tech boom, the SRI campus represents an important part of Bay Area history; it's redevelopment represents an important part of the region's future. Susan is leading the effort to conduct the peer review, analyze impacts, and develop creative mitigation solutions for the proposed project in support of the CEQA lead agency's goals for disclosing the decision-making process to the public.

Strategic Planning Services—NASA Ames Research Center Moffett Field, Santa Clara, California, 01/2010 – 10/2014

Senior Historic Preservation Planner. While employed by AECOM, Susan served as senior historic preservation planner in support of comprehensive program management for master planning at NASA's Ames Research Center. She provided historic preservation expertise for a wide variety of NASA needs including: the historic preservation chapter of NASA's Facilities Design Guidelines applicable to all NASA facilities nationwide; the survey and evaluation of the landscape, buildings, and structures associated with the former Moffett Federal Airfield; preparation of the facility's first fence-to-fence, comprehensive Integrated Cultural Resources Management Plan; and a variety of Section 106 and Section 110 aspects of leasing a portion of Moffett Field to a private lease holder.

Bay Area Rapid Transit (BART) Systemwide Historical Assessment and Streamlined Review Procedures – BART, Oakland, California, 05/2020 – Present

Project Director and Senior Preservation Planner. As BART turns 50 in 2022, Susan worked closely with BART to chart a road map for assessing BART’s historical significance and identify how BART will address historical status during future planning, design, and maintenance projects. Susan’s team is evaluating the system for historical significance and documenting the National Register of Historic Places and California Register of Historical Resources (CRHR) eligibility and developing a streamlined process for meeting CEQA, NEPA, and Section 106 requirements for consideration of historic properties during project development and environmental reviews.

Sacramento Municipal Utility District (SMUD) Headquarters Campus Master Plan EIR—SMUD, Sacramento, California, 09/2016 – 08/2017

Cultural Resources Lead Reviewer. As a subconsultant to the EIR prime consulting firm, ICF conducted an archaeological and historic built environment technical study to identify any CEQA historical resources present within the headquarters campus. Susan provided quality assurance support and quality control reviews of the study. She also led ICF’s support to SMUD on AB 52 coordination with California Native American tribes.

California High-Speed Train, San Francisco to San Jose and San Jose to Merced Sections—California High-Speed Rail Authority, Northern California, 11/2015 – 06/2022

Cultural Resources Task Manager and Quality Control Manager. Susan directed all cultural resources tasks for this complex, highly controversial design-build project, including archaeological and built resources technical studies, impacts analysis for NEPA and CEQA documents, and findings of effect and mitigation planning for Section 106 compliance. She provided quality control and technical direction for the production of inventory and evaluation reports for more than 3,000 historic properties between San Jose, Gilroy, Pacheco Pass, and Los Banos, California. The technical reports and the Finding of Effect was concurred with by the California SHPO upon their first review.

Long Range Development Plan EIS, and Section 106 Services—San Francisco Veterans Affairs (VA) Medical Center, San Francisco County, California, 12/2010 – 09/2014

Senior Historic Preservation Planner. While employed by AECOM, Susan directed all cultural resources services for NEPA and Section 106 review of VA’s proposed Long Range Development Plan (LRDP). She provided direction on VA’s consultation with the SHPO, solutions for streamlining between the NEPA and Section 106 processes, and methods for identifying historic properties and assessing project effects. She drafted a Programmatic Agreement that includes mitigation programs as well as a process for conducting streamlined individual review of LRDP projects during design development which was successfully executed by the VA and SHPO in January 2015. At the time that it was executed, it was the first Section 106 programmatic agreement successfully executed by the VA for one of their medical campuses. The leadership at SFVAMC and several consulting parties (including SHPO) cited the inclusiveness and transparency of the consultation process as a key to their readiness to proceed with executing the agreement.

California Pacific Medical Center Campuses Seismic Compliance, Hospital Replacement and Campus Renovation Program EIR—California Pacific Medical Center, San Francisco County, California, 12/2010 – 10/2011

Senior Historic Preservation Planner. While employed by AECOM, Susan directed the analysis and documentation of impacts associated with Long Range Development Plan, which proposes a new hospital and renovation of four existing CPMC campuses. She provided senior review of the response to public comments on historic and cultural resources issues in the draft EIR/EIS.



Katrina Sukola

Project Role: Hydrology and Water Quality

Katrina Sukola has experience in water and sediment quality, metal and nutrient analysis, and contaminant analysis in aquatic environments. She has managed and conducted fieldwork including river and coastal assessments such as the U.S. EPA's National Coastal Assessment, habitat assessments for herring, restoration projects including habitat restoration for red-legged frogs in the Eldorado National Forest, and monitoring programs for marine aquatic invasive species. Katrina has also designed and implemented environmental monitoring programs and coordinated fieldwork including surface and stormwater sampling.

Katrina prepares a variety of environmental documents including Environmental Impact Reports/Statements (EIRs/EISs), Environmental Assessments (EAs), Initial Studies (ISs), Mitigated Negative Declarations (MNDs), and technical reports related to water resources, and experienced in environmental compliance pursuant to the National Environmental Policy Act (NEPA) and California Environmental Quality Act (CEQA). She is thoroughly familiar with water resource issues, water quality regulatory compliance, and experienced with review of permits such as wastewater, stormwater, National Pollutant Discharge Elimination System (NPDES), and municipal separate storm sewer system (MS4) permits, and environmental management plans including ocean management plans and fisheries management policy.

Years of Experience

Professional start date: 08/2004
ICF start date: 06/2015

Education

MS Chemistry, University of Manitoba, 2003

BS Environmental Chemistry, University of Waterloo, 2001

Project Experience

Facebook Willow Village EIR—City of Menlo Park, CA, 05/2020 – 04/2022

Hydrology and Water Quality Lead Author. Katrina serves as task lead for surface water hydrology, groundwater resources, water quality, and flood impacts. This project would redevelop an approximately 59-acre industrial site, plus 2 parcels west of Willow Road, as a multiphase, mixed-use development. The project would construct new buildings, establish various open space areas, install infrastructure within a new Residential/Shopping District, Town Square District, and Campus District, alter two parcels to accommodate realignment of Hamilton Avenue, and construct an undercrossing to provide tram and pedestrian access to neighboring Facebook campuses. Primary issues include existing contaminated groundwater, increased flooding, and adaptation to sea level rise.

Bayhill Specific Plan EIR— City of San Bruno, California, 3/2019 – 09/2021

Hydrology and Water Quality Lead Author. The Bayhill Specific Plan Area encompasses approximately 92.2 acres and made up of 17 large parcels. The Plan Area is part of a 20-year campus expansion plan to be developed in five phases. The project considers approximately 2.46 million square feet of new commercial and potentially residential development, with significant subterranean parking. Katrina authored the hydrology and water quality section of the EIR.



Southline Specific Plan EIR — City of South San Francisco, South San Francisco, California, 03/2020 – 04/2022

Hydrology and Water Quality Lead Author. The Southline Specific Plan encompasses the 26-acre Southline Specific Plan Area. The proposed project would demolish all existing industrial uses on-site and construct seven office buildings, an amenities building, underground parking throughout the site, a parking structure, a new road connection, and approximately 369,000-square feet of open space. Development of the proposed project would be phased, including a Phase I. In total, the project is anticipated to have a maximum building area of 2.8 million-square feet. Katrina authored the hydrology and water quality section of the EIR.

751 Gateway Boulevard Project EIR - San Francisco No. 40, LLC, City of South San Francisco, California, 1/2020 – 6/2020

Hydrology and Water Quality Lead Author. Katrina authored the Hydrology and Water Quality section for the 751 Gateway Boulevard Project Environmental Impact Report (EIR). The project included redevelop of a 7.4-acre, irregularly shaped site within the City of South San Francisco's Gateway Specific Plan planning area. The proposed project involves the construction of a seven-story building with approximately 208,8000 sf of usable space (60 percent research and development uses, and 40 percent office uses).

555 & 777 West Middlefield EIRs—City of Mountain View, California, 12/2017 – 10/2021

Hydrology and Water Quality Lead Author. The 555 West Middlefield Project includes retaining the 402 existing multi-family residential units and the construction of 348 new residential units in 2 separate buildings on 7 acres of existing surface parking lot. The 777 West Middlefield Project includes the demolition of the existing on-site buildings and construction of up to 716 new residential units in 3 buildings with subterranean parking.

1075 O'Brien Drive IS/Mitigated Negative Declaration (MND)—City of Menlo Park, CA, 11/2020 – 04/2021

Hydrology and Water Quality Lead Author. Katrina served as technical lead for surface water hydrology, groundwater resources, water quality, and flood impacts. This project would construct an industrial building for research and development, commercial, and office uses, along with a five-level parking structure. Primary issues include changes in stormwater runoff and development within the 100-year floodplain.

1125 O'Brien Drive IS/MND—City of Menlo Park, CA, 10/2019 – 06/2021

Hydrology and Water Quality Lead Author. Katrina served as technical lead for surface water hydrology, groundwater resources, water quality, and flood impacts. This project would construct an industrial building for research and development uses, as well as surface parking. The project includes reduced impervious surfaces and bioretention area and flow-through planter to capture and treat runoff.

SF Giants Mission Rock Seawall Lot 337 Pier 48 EIR—Seawall Los 37 Associates LLC, San Francisco, California, 09/2015 – 06/2017

Hydrology and Water Quality Lead Author. Katrina authored the Hydrology and Water Quality section for the SF Giants Mission Rock Seawall Lot 337 Pier 48 Project EIR documents. The 27-acre mixed use project involves development of residential, commercial, light industrial, active/retail, and open space as well as the rehabilitation and reuse of Pier 48. The project is located adjacent to marginal wharf, China Basin Park and Terry A. Francois Boulevard in the City of San Francisco.

Facebook Constitution Campus Expansion—City of Menlo Park, California, 5/2015 – 08/2016

Hydrology and Water Quality Specialist. Katrina authored the Hydrology and Water Quality section for the Facebook Constitution Campus Expansion Environmental Impact Report (EIR) documents. The project included the redevelopment of an existing industrial site with two new office buildings and a hotel. The project is located within the jurisdiction of the San Francisco Bay Water Board.



Years of Experience

Professional start date: 10/2001
ICF start date: 07/2004

Education

BS, Engineering Technology,
Environmental Technology;
California State University, Long
Beach, 2003

Certifications/Registrations

40-Hour HAZWOPER Training

Mario Barrera, BS

Senior Environmental Planner

Mario Barrera has more than 16 years of experience in the environmental consulting field, including Underground Storage Tank (UST) remediation system (soil vapor and groundwater) operation and maintenance, along with associated management, permitting and reporting; collection of wastewater, groundwater, and soil samples; field data collection and management; National Pollutant Discharge Elimination System (NPDES) rules and regulations enforcement and implementation; Phase I and Phase II Environmental Site Assessments; Initial Site Assessments; Soil Management Plans, California Environmental Quality Act (CEQA), National Environmental Policy Act (NEPA) and State Environmental Policy Act (SEPA) hazardous material and geology technical lead for preparation of environmental documentation. He also has experience in transportation planning and agency sustainability; and stormwater and industrial waste site inspections and permitting. He also assists with project coordination, manages projects and translates outreach documents to Spanish for various projects.

Project Experience

San Francisco Housing Element 2022 Update IS—City of San Francisco, California, 05/2020 – 2/2022

Technical Lead. Mario provided support to ICF's project manager on the project. His responsibilities included the preparation of environmental and regulatory setting pertaining to hazards and hazardous materials throughout the City and the potential for human and environmental exposure to hazardous materials. His responsibilities also consisted of preparation of a Hazards and Hazardous Materials Initial Study (IS) section. The IS document included a programmatic analysis of the hazardous materials conditions in the City and provide measures to reduce potential impacts. The objective of the project was to update the City's General Plan Housing Element for 2022.

Station East Residential/Mixed Use Project EIR—City of Union City, California, October 2020 – January 2021

Technical Lead. Provided support to ICF's project manager on the project. Responsibilities included analyzing a variety of environmental data associated with the project site, including an All Appropriate Inquires Report and multiple Phase I and Phase II Environmental Assessment reports. His responsibilities included the preparation of a Hazards and Hazardous Materials CEQA EIR section. The EIR document was prepared to analyze potentially significant environmental effects associated with the Station East Residential/Mixed Use Project. The project goal was to provide the City of Union City with appropriate technical environmental information prior to redevelopment.

Humboldt Bay Sediment Management Program—Humboldt Bay Harbor District (HBHD), Eureka, California, 09/2019

Technical Lead. Mario provided support to ICF's project manager on the project. Mario was the Hazards and Hazardous Materials primary author for the Programmatic Environmental Impact Report (PEIR) for

the HBHD for the implementation of a regional sediment management plan for the Eureka Littoral Cell. The PEIR describes a long-term program for dredging via hydraulic or mechanical methods, as well as transport, dewatering, processing, and beneficial use or disposal of bay sediments. The primary beneficial use options considered are habitat restoration, sea level rise adaptation and protection, and beach replenishment.

SeaCliff Siding Extension Project EIR/EA Services and Phase I ESA (Contract 75A0398 TO 2)—Caltrans District 7, Ventura County, California. Hazardous Materials Specialist. 06/2016 – 2019

Technical Lead. Mario's responsibilities included peer reviewing hazardous waste/materials content and conclusions included in the Preliminary Environmental Analysis Report (PEAR) for the project. Mario also completed an Initial Site Assessment (ISA) of the project site. The objective of the assessment was to identify and evaluate potential environmental issues associated with past and/or present operations at the project site and adjacent properties. The project consists of the construction of approximately 9,000 feet of new track siding on the existing UPRR Santa Barbara Subdivision main in unincorporated Ventura County.

The Hub Plan 30 Van Ness Avenue Project, 98 Franklin Street Project, and Hub Housing Sustainability District EIR—County of San Francisco, CA, 01/2018 – 3/2018

Technical Lead. Mario provided support to ICF's project manager on the project. His responsibilities included analyzing environmental and historical land use information found in various technical documents along with the preparation of a Hazards and Hazardous Materials CEQA EIR section. The EIR document included a programmatic analysis of the Hub Plan area and a project-level analysis for three individual redevelopment projects contained within the Hub. The objectives of the Hub Plan were to encourage housing, create safer and more walkable streets as well as welcoming and active public spaces; increase transportation options and create a neighborhood with a range of uses and services to meet neighborhood needs.

Otay River Restoration Project HMMP IS/MND—Chula Vista, California, 10/2015 – 05/2016

Technical Lead. Mario provided support to ICF's project manager on the project. His responsibilities included analyzing environmental and historical land use information for preparation of a Hazards and Hazardous Materials CEQA IS/MND section. The project involved implementation of the Otay River Restoration Project Habitat Mitigation and Monitoring Plan to guide the restoration and enhancement of approximately 100 acres of aquatic and terrestrial habitat in the Otay River Valley. The IS/MND document was prepared to analyze potentially significant environmental effects associated with the project.

EIS for the Sound Transit Link Operations and Maintenance Satellite Facility Project—Sound Transit, Bellevue/Lynwood, Washington, 07/2015 – 08/2015

Technical Lead. Mario provided support to ICF's project manager on the project. His responsibilities included analyzing environmental and historical land use information for preparation of a Hazardous Materials SEPA EIS section. The Hazardous Materials analysis encompassed four alternatives for construction of a new Light Rail Vehicle (LRV) maintenance facility. The EIS document was prepared to analyze potentially significant environmental effects associated with the Sound Transit Link Operations and Maintenance Satellite Facility Project's four alternatives. The project goal was to provide Sound Transit with appropriate technical environmental information prior to construction of the new facility.

Employment History

ICF Senior Environmental Planner, Irvine, California. 2019 – present.

ICF Senior Associate, Irvine, California. 2017 – 2019.

ICF. Associate. Irvine, California. 2008 – 2017.

ICF. Analyst. Irvine, California. 2006 – 2008.

ICF. Research Assistant. Irvine, California. 2004 – 2006.

Hunter Kennedy and Associates. Environmental Protection Specialist. Santa Fe Springs, California. 2001 – 2003.

Gary K. Black, AICP, President

Education

Master of City Planning in Urban Transportation, University of California at Berkeley

Bachelor of Arts in Geography, University of California at Los Angeles



Professional Associations

American Institute of Certified Planners

Institute of Transportation Engineers

Experience

Since 1982, Mr. Black has directed a number of transportation planning, traffic engineering, parking, and transit studies. He has prepared transportation plans for the Cities of San Jose, Palo Alto, San Mateo, and San Carlos, and areawide plans for reuse of the Bay Meadows racetrack site in San Mateo, the Cargill salt ponds site in Redwood City, and many parts of San Jose (North San Jose, Downtown, Edenvale, and Evergreen). He has prepared traffic studies for new development in most cities within the Bay Area. He also has prepared numerous parking studies, including downtown parking studies for San Carlos, San Mateo, Gilroy, and San Jose.

Representative Projects

• **Areawide Transportation Plans:**

Circulation Elements for General Plans in San Mateo, Sunnyvale, San Carlos, and Palo Alto.

Bay Meadows – Hexagon prepared the transportation plan for redevelopment of the Bay Meadows Race Track in San Mateo into a mixed-use, transit oriented development.

San Carlos - Citywide study involved estimating and analyzing the traffic conditions that would occur from buildout of known development sites within the city. Intersection levels of service were calculated and recommendations were made for possible transportation network improvements.

North San Jose – Hexagon developed a revised development policy for North San Jose that included a long-range forecast of traffic conditions and development of a long list of necessary transportation improvements – both roads and transit. The policy resulted in the adoption of an impact fee to fund transportation improvements.

Redwood City – Hexagon has done the transportation planning for the proposed reuse of the Cargill salt ponds in Redwood City. The potential reuse includes essentially the development of a new town with 12,000 homes, office buildings, a shopping center, and schools.

• **Campus Studies:**

Foothill College –The campus is served by one ring road that is accessed through a single intersection. Hexagon staff recommended that the ring road be made one-way. Other recommendations were also made for better signage and lighting around the ring road.

City College – Hexagon staff was hired to measure parking demand and to determine the amount of new parking needed. Hexagon staff conducted parking occupancy surveys. Student parking in neighborhoods was estimated by comparing overnight occupancy to occupancy at typical student peak times.

IBM Campus - Hexagon staff was hired to address various problems occurring on the internal roads. Many recommendations came out of the study, including modifying speed limits, narrowing streets, channelizing pedestrian crossings, adding signals, and modifying intersection geometries to improve sight distance.



- **Site Traffic Analyses:**

For offices, hotels, restaurants, residential subdivisions, apartments, schools, warehouses, industrial complexes, and mixed-use developments in San Jose, Santa Clara, Sunnyvale, Milpitas, Los Gatos, Fremont, Monterey, Palo Alto, Menlo Park, Redwood City, San Carlos, San Mateo, Los Altos, Santa Rosa, Napa, Hayward, Bakersfield, Richmond, Concord, and Cupertino, California. These included estimation of future trip generation, impacts on adjacent intersections, and site-specific pedestrian and auto circulation issues such as driveway and crosswalk locations.



- **Corridor Studies:**

Route 238 Bypass – Mr. Black evaluated several transportation alternatives to the proposed Rte. 238 Bypass in Hayward. The Planning Area 2 Travel Forecasting model was used to project future traffic in the Central County area. The basis of the evaluation was to analyze the effects of each scenario in terms of (a) changes in traffic volumes on major roads and freeway segments, (b) congested versus uncongested vehicle miles traveled (VMT), and (c) the impacts on major street intersection operations.



- **Parking Studies:**

San Carlos – Staff believed that the available parking spaces were utilized to such an extent that any future development could not be accommodated. It was determined that future development could be accommodated only by planning a parking structure. A suitable site was identified, and a three-level parking structure was designed (one level underground and two levels above). To help the financial feasibility of the parking structure, it was designed to have two levels of housing above.

San Mateo – Due to recent and projected growth, many downtown merchants believed that more parking facilities were needed. Surveys revealed that the existing parking situation was adequate, although during peak times customers sometimes had to settle for less desirable spaces because the prime spaces were taken by employees. The study was able to show that a relatively modest increase in downtown parking meter rates combined with a small property assessment could finance an additional parking structure.



- **Major Developments:**

Valley Fair – Valley Fair is a 1.2 million square foot regional mall that was proposed for enlargement by approximately 300,000 square feet.

Santana Row – This project transformed a 1960's era shopping center into a mixed-use "Main Street" style shopping, entertainment and residential center.

Oakridge Mall – The proposed expansion consisted of the addition of 85,000 square feet of movie theater space plus additional retail and restaurant space.



Evergreen Specific Plan - The plan called for the construction of over 4,000 dwelling units on about 600 acres. Hexagon staff analyzed both on-site and off-site traffic impacts of the plan and developed the circulation element of the EIR.



Ollie Zhou, T.E., Vice President & Principal Associate

Education

Bachelor of Science – Civil & Environmental Engineering, University of California – Berkeley

Professional Associations

Member of the Institute of Transportation Engineers
Registered Professional Traffic Engineer in the State of California (TR 2857)



Experience

Since January 2014, Mr. Zhou has managed a large variety of traffic engineering and transportation planning projects for both the public and private sectors throughout the greater San Francisco Bay Area. These projects mainly include travel demand model validation and application, VMT analysis, general plan updates and area plans, and traffic impact studies. Mr. Zhou is experienced in managing large-scale projects and areawide plans with prolonged schedules and complicated work scopes. Mr. Zhou mainly utilizes the CUBE software package for travel demand model applications, and manage a variety of projects conducted with Synchro, SimTraffic, Vistro, TRAFFIX software.

Representative Projects

• Travel Demand Forecasting Model Development and Application Projects:

- **Menlo Park Citywide Model** – Model refinement and validation. Model application for the Willow Village/Facebook project, VMT policy update, and Housing Element Update
- **Sunnyvale Citywide Model** – Model refinement and validation. Model application for the Moffett Park Specific Plan, Sunnyvale General Plan Update, Lawrence Station Area Plan, Peery Park Specific Plan, and Sunnyvale Traffic Impact Fee.
- **San Mateo Citywide Model** – Model development, refinement and validation. Model application for the San Mateo Traffic Impact Fee.
- **10th St and 11th St two-way street conversion, San Jose** – Local model validation and forecasting link-level and intersection-level volumes.
- **Gilroy Downtown Specific Plan with High-Speed Rail (HSR) Station, Gilroy** – Incorporated boarding-alighting data provided by HSR Authority at Gilroy Caltrain Station into the Gilroy Citywide Model to analyze three downtown specific plan alternatives.

• Vehicle-Miles Travel (VMT) Analysis for residential, office, hotel, school, area plans, Housing Element Updates, and mixed-use developments throughout the greater Bay Area. Representative projects include:

- **Willow Village/Facebook, Menlo Park** – 1.6 million s.f. office, 1,730 housing units, 200,000 s.f. retail, 193-room hotel; project included updating City's VMT policy, and incorporating specific project characteristics into the travel demand model for VMT calculations.
- **Menlo Park Housing Element Update** – CEQA analysis for HEU with 4,000 housing units beyond GP buildout, spread across 16 sub-areas.
- **Avenue School, San Jose** – developed a hybrid VMT methodology using a combination of travel demand model and off-model processes to analyze private school; project included a 2,700 student pre-k to grade 12 private school. Using available private school student data, a custom distribution model was developed for this project.
- **Danville Housing Element Update** – CEQA analysis for HEU with 4,500 housing units among 8 opportunity sub-areas
- **Moffett Park Specific Plan, Sunnyvale** – CEQA analysis for specific plan with 33 million s.f. office/R&D, 20,000 housing units. Project incorporated specific project characteristics into the travel demand model for VMT calculations.



- **Sobel Mixed-Use Development, Salinas** – developed VMT methodology for a mixed-use development with 37,000 s.f. retail, 96-room motel, 4 restaurants, separate service station.
- **Over 50 Traffic Analyses/Traffic Feasibility Studies** for area-wide plans, offices, hotels, apartments, schools, daycare centers and multiple-use developments throughout the Bay Area.
- **Traffic Simulation/Signal Coordination Studies** for various congested corridors in San Mateo, Los Gatos, and Sunnyvale.
- **Traffic Impact Fee (TIF) Update Studies** for the City of San Mateo and the City of Sunnyvale. Conducted nexus studies and calculated appropriate impact fees for the TIF Update projects.
- **Multi-Modal Analysis** for the Palo Alto General Plan Update in Palo Alto, CA. This study included analyzing the existing and future roadway segment level-of-service for all of automobile mode, pedestrian mode, bicycle mode, and transit mode. This study followed the multi-modal analysis guidelines outlined in the *Highway Capacity Manual (HCM), 2010 Edition*.
- **Bicycle Level of Service Analysis** for the Magee Ranch project in Danville, CA. This study utilized the bicycle level of service methodology for two-lane highway segments outlined in the *Highway Capacity Manual (HCM), 2010 Edition*.



Ling Jin, Associate

Education

Master of Science—Civil Engineering, The University of Texas at Austin

Master of Engineering—Civil Engineering, Nanyang Technological University, Singapore

Bachelor of Science—Civil Engineering, Tsinghua University, China



Experience

Since joining Hexagon in 2004, Ms. Jin has participated in a variety of transportation engineering and planning projects throughout the Bay Area. Prior to joining Hexagon, Ms. Jin worked for two years on travel demand modeling and transportation planning projects in Philadelphia, PA. Her projects include transportation impact analyses for environmental impact reports, traffic feasibility studies, multimodal studies, travel behavior analyses, transportation demand management plans, and parking studies. Ms. Jain has experience with TRAFFIX, Synchro/Simtraffic, ArcGIS, and Vistro software packages.

Representative Projects

- **Transportation Analyses** for area-wide plans. Representative projects include Patrick Henry Drive Specific Plan in Santa Clara, CA, and Freedom Circle Focus Area Plan in Santa Clara, CA
- **Traffic Impact Analyses and Vehicle-Miles Travel (VMT) Analysis** for offices, apartments, schools, and mixed-use developments throughout the Bay Area. Representative projects include:
 - 200 Twin Dolphin Drive Office Development - Redwood City, CA
 - 505 East Bayshore Road Residential Development - Redwood City
 - 1350 Adams Court Office EIR - Menlo Park, CA
 - 3723 Haven Avenue Hotel Transportation Analysis - Menlo Park, CA
 - 550 Piercy Road Industrial Development – San Jose, CA
- **Transportation Demand Management (TDM) Plans** for both small and large projects in a variety of settings, incorporating current best practices for reducing single-occupant vehicle trips. Each plan has been tailored to the specific project and the requirements of the community where it is located. Representative projects include 123 Independence Drive TDM Plan in Menlo Park, 610 Walnut Street Office Development TDM Plan in Redwood City, and Greystar Main Street Mixed-use Development TDM Plan in Redwood City.
- **Traffic Operation Analyses and Parking Studies** for office, residential, retail, and school projects. These studies included transportation operation analyses, site access and on-site circulation analyses, conducting surveys of existing parking demand, calculations of required parking supply for the proposed projects, and shared parking analyses. Representative projects include the 3700 Thomas Road Daycare in Santa Clara, 1001 Shoreline Boulevard Residential Development Parking Study in Mountain View, and 4856 El Camino Real Residential Development in Los Altos.
- **Travel Demand Forecasting Model Development and Applications:**
 - **City of Sunnyvale General Plan Update; Sunnyvale, California.** Prepared demographic data for model calibration and development.
 - **City of Palo Alto General Plan Update; Palo Alto, California.** Prepared demographic data for model calibration and development and calculate intersection Level of Services.
 - **C/CAG El Camino Real Corridor Preliminary Traffic Study.** Coded highway networks for project alternatives. Estimated traffic demand and analyzed level of service for studied intersections.
 - **2004/2005 C/CAG Model Update; San Mateo, California.** Prepared highway network and demographic data for the updated travel demand model for San Mateo County, California.

Appendix B Hexagon – Transportation Scope



December 15, 2022

Ms. Kirsten Chapman
ICF
201 Mission Street, Suite 1500
San Francisco, CA 94105

Re: *Proposal to Prepare a Transportation Impact Analysis for the Proposed R&D Development at 1030 O'Brien Drive in Menlo Park, California*

Dear Ms. Chapman:

Hexagon Transportation Consultants, Inc. is pleased to submit this proposal to prepare a Transportation Impact Analysis for the proposed research and development (R&D) project at 1030 O'Brien Drive in Menlo Park, California. The project would combine the 1020 O'Brien Dr and 1030 O'Brien Drive parcels, demolish the existing buildings on site, and build 85,886 square feet of office/R&D space and 5,822 square feet of commercial space. It is unclear whether existing buildings or occupied. Vehicular access to the project site would be provided by driveways on O'Brien Drive.

Scope of Services

The purpose of the transportation study is to satisfy the California Environmental Quality Act (CEQA) requirements for a class 32 infill exemption, and to satisfy the non-CEQA operational analysis requirements for the City of Menlo Park. The CEQA analysis will include an analysis on the project's bicycle, pedestrian and transit impacts, VMT impact, emergency access impact, and potential hazards impacts. The non-CEQA operational analysis will include an analysis of weekday AM and PM peak-hour traffic conditions on key intersections in the project vicinity, daily roadway traffic analysis, and other transportation issues (queuing, site plan review, parking).

The City/County Association of Governments (C/CAG)'s Congestion Management Program (CMP) has additional non-CEQA operational analysis requirements for projects generating over 100 net peak hour trips. This scope assumes that the project's trip generation would receive trip credits for the existing buildings on-site to be demolished. This would lower the project's net peak hour trip generations below the C/CAG thresholds, and would not require the CMP-required freeway or ramp analysis.

The intersections we propose to study are identified below.

Study Intersections:

1. Willow Road and Bayfront Expressway
2. Willow Road and Hamilton Avenue
3. Willow Road and Ivy Drive
4. Willow Road (SR 114) and O'Brien Drive
5. Willow Road and Newbridge Street
6. Willow Road and US 101 NB Off-ramp
7. Willow Road and SB 101 Off-ramp
8. O'Brien Drive and Kavanaugh Drive (unsignalized)
9. University Avenue and Bayfront Expressway



It should be noted that since this project would not trigger the East Palo Alto's Settlement Agreement, East Palo Alto study intersections are not included in this scope.

In addition, the project's effect on neighborhood traffic conditions will be evaluated on the street segments identified below.

Street Segments:

1. O'Brien Drive between Willow Road and Kavanaugh Drive, and
2. O'Brien Drive between University Avenue and Kavanaugh Drive

CEQA Analysis

1. ***Bicycle, Pedestrian, and Transit Facilities.*** A qualitative analysis of the project's impact on transit service in the area and on bicycle and pedestrian circulation in the study area will be included in the traffic report. Any impacts of the project on the nearby facilities will be identified and improvements recommended to mitigate the impacts.
2. ***VMT Analysis.*** The vehicle miles travelled (VMT) associated with the proposed project will be evaluated using the citywide travel demand model. The project is likely to have a VMT impact. It is assumed that the project applicant will provide a Transportation Demand Management (TDM) Plan. Hexagon's TDM Plan peer review findings (Task 3) will inform whether or not additional VMT mitigation measures are necessary. If so, Hexagon will work with City staff to determine the additional mitigation measures necessary to fully mitigate the project VMT impact.
3. ***Peer Review of TDM Plan.*** It is assumed that the project applicant will provide a TDM plan. Hexagon will peer review the TDM Plan and determine a) whether the TDM Plan satisfies the City's TDM ordinance, b) whether the TDM Plan is sufficient to eliminate the project's VMT impact, and c) whether the TDM Plan satisfies the City/County Association of Governments' TDM Checklist requirements. Hexagon will summarize our findings in a separate memorandum.
4. ***Project Impact on Hazards and Emergency Vehicle Access.*** A qualitative analysis of the project's impact on hazards due to a geometric design feature or incompatible use, as well as the project's impact on emergency vehicle access will be conducted. Findings will also be informed by Task 15 below (site access and on-site circulation).
5. ***Provision of Data to Air Quality and Noise Consultants.*** It is anticipated that the Air Quality and Noise consultants will need traffic and VMT data for their analysis. This task includes Hexagon staff time to coordinate with the consultants and the data needs, formats, and to provide the requested data.

Non-CEQA Analysis

6. ***Site Reconnaissance.*** The physical characteristics of the site and the surrounding roadway network will be reviewed to identify existing roadway cross-sections, intersection lane configurations, traffic control devices, and surrounding land uses.
7. ***Observation of Existing Traffic Conditions in the Study Area.*** Field observations of existing traffic conditions will be limited to field-verifying signal timing at signalized intersections during peak hours. Due to COVID conditions, field observations of intersection-level operations issues are not included in this proposal.



8. **Data Collection.** Existing weekday AM (7:00 – 9:00 AM) and PM (4:00 – 6:00 PM) peak-hour traffic volumes at the study intersections and existing average daily traffic volumes for the study roadway segments will be obtained from the City of Menlo Park and previous studies with counts conducted in year 2019 or 2020. This task does not include collection of new turning movement counts.
9. **Evaluation of Existing Conditions.** Existing traffic conditions will be evaluated based on existing traffic volumes at the study intersections. The existing traffic conditions at the study intersections within the City of Menlo Park will be evaluated using the software VISTRO, which employs the *Highway Capacity Manual (HCM) 6th Edition* methodology for intersection analyses.
10. **Evaluation of Background Conditions.** Background traffic volumes represent the existing volumes plus the anticipated volumes from approved developments that have not yet been constructed and occupied. Hexagon proposes to reference the 1075 O'Brien Drive traffic study for background traffic volumes. Intersection levels of service under background conditions will be evaluated.
11. **Project Trip Generation, Distribution, and Assignment.** Estimates of trips to be added to the surrounding roadway network by the proposed development will be based on the trip generation rates recommended by the Institute of Traffic Engineers' *Trip Generation Manual, 11th Edition*. Hexagon will work with City staff on the methodology to estimate trip generation for the ground-floor commercial use, as well as for any potential credits for the existing buildings on site that are to be demolished. A 20% transportation demand management (TDM) reduction will be applied in accordance with Menlo Park requirements.

The directional distribution of site-generated traffic will reference the 1075 O'Brien Drive traffic study. The proposed trip generation estimation and project trip distribution patterns will be sent to the City to review. The site-generated net traffic will be assigned to the roadway network based on the City approved trip generation and distribution pattern.

12. **Evaluation of Background Plus Project Conditions.** Project-generated traffic will be added to the background condition traffic volumes. Intersection level of service calculations will be conducted to estimate project traffic conditions during the AM and PM peak hours after project completion. Intersection adverse effects associated with the development of the proposed project will be evaluated relative to background conditions.
13. **Cumulative Conditions.** The latest 2040 model run, conducted for the City's proposed Housing Element Update, included full buildout of the Menlo Park General Plan and relevant General Plan Amendments, and approved and pending projects within the City of East Palo Alto up to December 2020. This model run will be used to develop intersection volume forecasts at study intersections. Since the proposed project is within the development capacity allowed under the Menlo Park General Plan, the new model run will represent the cumulative plus project conditions for the project. Project traffic will be subtracted to represent the cumulative no project conditions. Intersection adverse effects associated with the development of the proposed project will be evaluated relative to cumulative no project conditions.
14. **Roadway AADT Analysis.** Hexagon will evaluate the project effects on roadway AADT under the cumulative with project scenario. Adverse effects will be identified in accordance with Menlo Park's guidelines.



15. **Site Access and On-Site Circulation.** A review of the project site plan will be performed to determine the overall adequacy of the site access and on-site circulation in accordance with generally accepted traffic engineering standards and to identify any access or circulation issues that should be improved. Sight distance will be checked at the project driveways. Parking will be evaluated relative to the City of Menlo Park Parking Code.
16. **Evaluation of Vehicle Queuing.** For selected locations where the project would add a significant number of left-turning vehicles, the adequacy of existing/planned storage at turn pockets will be assessed by means of comparison with expected maximum vehicle queues. Vehicle queues will be estimated using the Vistro software.
17. **Description of Recommendations.** The results of the level of service calculations will be used to identify the locations and types of necessary improvements or modifications. Improvements could include street widenings, lane additions, changes in lane usage, or modifications to existing traffic signals, which will be consistent with the mitigation measures proposed in the City's General Plan Update - ConnectMenlo. Improvements could also include improvements to transit, bicycle and pedestrian infrastructure, as well as the development of measures that promote multi-modal travel and reduce the use of single-occupant automobile travel for the purpose of VMT reduction goals.

Reports and Meetings

18. **Meetings.** The fee estimate includes Hexagon staff attendance at five staff meetings and one public hearing in connection with the project. Additional meeting attendance would be provided as additional services and will be billed based on staff time plus expenses.
19. **Class 32 Infill Exemption Transportation Section.** Hexagon will author the Transportation section of the Class 32 Infill Exemption, which would include findings of Task 1 through Task 4 above. A summary of the non-CEQA analysis may also be included if deemed necessary. This task includes three rounds of report revisions.
20. **Non-CEQA TIA Report.** Hexagon's findings and recommendations for the non-CEQA analysis tasks (Task 6 through Task 17) will be summarized in a standalone TIA document. This task includes two rounds of report revisions.



Time of Performance

Barring any unforeseen delays, an administrative draft traffic analysis report will be submitted approximately ten weeks after 1) authorization to proceed, and (2) approval of project trip generation, distribution and assignment assumptions. The final traffic report will be delivered one week after receipt of all review comments. The major project milestones are described below.

Major Milestones

Week	Milestone
Week 1	Provide draft trip generation, distribution and assignment for City review and approval (assume 2-week City review time)
Week 2	Provide preliminary VMT results for City review (assume 2-week City review time)
Week 4	Provide preliminary results for non-CEQA analyses (assume 2-week City review time)
Week 6	Provide required data for AQ/Noise consultants
Week 8	Provide administrative draft TIA report
Week 10	Provide administrative draft Class 32 Infill Exemption Transportation Section

Cost of Services

The fee for the scope of services will be based on time and expenses up to a maximum budget of \$69,000 (see Table 1 for budget breakdown). Work not specifically specified in this scope are considered out-of-scope (such as analyzing additional intersections, revised project description after project initiation), and will require additional budget authorization.

We appreciate your consideration of Hexagon Transportation Consultants for this assignment. If you have any questions, please do not hesitate to call.

Sincerely,

HEXAGON TRANSPORTATION CONSULTANTS, INC.

Ollie Zhou, T.E.
 Vice President



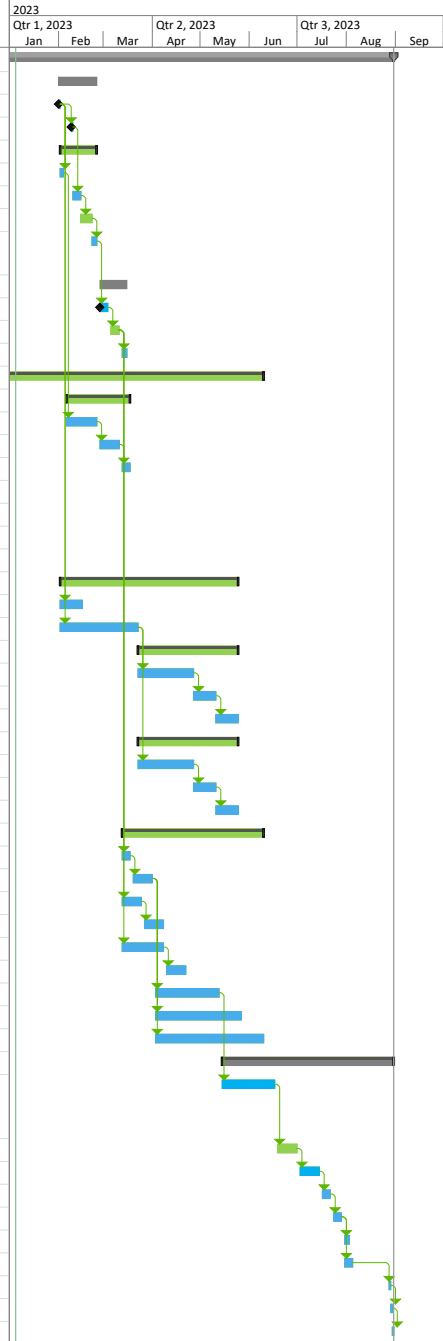
Table 1
Budget Breakdown

#	Task	Hours				Budget
		President	Principal Associate	Associate	Admin/ Graphics	
1	Bicycle, Pedestrian, and Transit Facilities		4	8		\$ 2,900
2	VMT Analysis		8			\$ 2,280
3	Peer Review of TDM Plan	1	20		2	\$ 6,270
4	Project Impact on Hazards and Emergency Vehicle Access		4			\$ 1,140
5	Provision of Data to AQ/Noise		4	4		\$ 2,020
6	Site Reconnaissance			4		\$ 880
7	Traffic Condition Observations			8		\$ 1,760
8	Data Collection			4		\$ 880
9	Evaluation of Existing Conditions		2	8		\$ 2,330
10	Background Conditions			4		\$ 880
11	Trip Generation, Distribution, Assignment	1	4	8	2	\$ 3,470
12	Background + Project Conditions			4		\$ 880
13	Cumulative Conditions		8	8		\$ 4,040
14	Roadway AADT Analysis			4		\$ 880
15	Site Access, On-Site Circulation, Parking		4	8		\$ 2,900
16	Vehicle Queuing			8		\$ 1,760
17	Description of Recommendations	4	8	8		\$ 5,360
18	Meetings	5	10			\$ 4,500
19	Class 32 Infill Exemption Transportation Section	4	20		4	\$ 7,500
20	Non-CEQA TIA Report	4	20	40	8	\$ 16,780
	Total	19	116	128	16	\$ 69,000

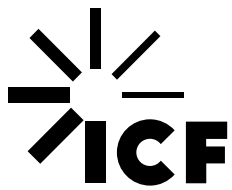
Appendix C Preliminary Schedule

Preliminary Schedule for the 1030 O'Brien Drive Project Class 32 Infill Exemption
 This schedule is preliminary and is subject to change

ID	Task Name	Notes	Duration	Scheduled Start	Scheduled Finish	2023								
						Qtr 1, 2023	Qtr 2, 2023	Qtr 3, 2023	Jan	Feb	Mar	Apr	May	Jun
0	1030 O'Brien Drive Project Class 32 Infill Exemption Document		315 days	Thu 6/16/22	Wed 8/30/23									
1	Project Initiation & Data Needs		18 days	Wed 2/1/23	Fri 2/24/23									
2	ICF is Under Contract	Timing is estimated	1 day	Wed 2/1/23	Wed 2/1/23									
3	Kick-off Meeting	Timing is estimated	1 day	Thu 2/9/23	Thu 2/9/23									
4	Data Needs Process		17 days	Thu 2/2/23	Fri 2/24/23									
5	Project Sponsor Provides Preliminary Project Description & Other Materials Prepared for the Project		2 days	Thu 2/2/23	Fri 2/3/23									
6	ICF Submits Data Needs Request		3 days	Fri 2/10/23	Tue 2/14/23									
7	City & Project Sponsor Respond to Data Needs Request		1 wk	Wed 2/15/23	Tue 2/21/23									
8	ICF Reviews Data Needs Responses & Confirms Everything is Ready for Use in the Infill Exemption Document	Another round of data needs review may be needed if ICF identifies any follow-up questions	3 days	Wed 2/22/23	Fri 2/24/23									
9	Project Description		13 days	Mon 2/27/23	Wed 3/15/23									
10	ICF Submits Draft Project Description		1 wk	Mon 2/27/23	Fri 3/3/23									
11	City Reviews Draft Project Description	Assumes comments are consolidated and conflicting comments are resolved	1 wk	Mon 3/6/23	Fri 3/10/23									
12	ICF Submits Final Project Description		3 days	Mon 3/13/23	Wed 3/15/23									
13	Standalone Technical Reports		257 days	Thu 6/16/22	Fri 6/9/23									
14	Arborist Report Peer Review		30 days	Mon 2/6/23	Fri 3/17/23									
15	ICF Submits Arborist Report Peer Review Memorandum		3 wks	Mon 2/6/23	Fri 2/24/23									
16	Project Sponsor's Consultant Addresses Comments in Arborist Report Peer Review Memorandum	Assumes comments are consolidated and conflicting comments are resolved	2 wks	Mon 2/27/23	Fri 3/10/23									
17	ICF Confirms Comments in Arborist Report Peer Review Memorandum Are Addressed		1 wk	Mon 3/13/23	Fri 3/17/23									
18	Biological Resources Assessment		35 days	Thu 6/16/22	Wed 8/3/22									
19	ICF Submits Draft Biological Resources Assessment		4 wks	Thu 6/16/22	Wed 7/13/22									
20	City & Applicant Review Draft Biological Resources Assessment	Assumes comments are consolidated and conflicting comments are resolved	2 wks	Thu 7/14/22	Wed 7/27/22									
21	ICF Submits Final Draft Biological Resources Assessment		1 wk	Thu 7/28/22	Wed 8/3/22									
22	Built Resources and Archaeological Resources Technical Memoranda		80 days	Thu 2/2/23	Wed 5/24/23									
23	ICF Prepares Notification Letters for Tribal Contacts		2 wks	Thu 2/2/23	Wed 2/15/23									
24	ICF Conducts Records Search		7 wks	Thu 2/2/23	Wed 3/22/23									
25	Built Resources Technical Memorandum		45 days	Thu 3/23/23	Wed 5/24/23									
26	ICF Submits Draft Built Resources Technical Memorandum		5 wks	Thu 3/23/23	Wed 4/26/23									
27	City Reviews Draft Built Resources Technical Memorandum	Assumes comments are consolidated and conflicting comments are resolved	2 wks	Thu 4/27/23	Wed 5/10/23									
28	ICF Submits Final ICF Submits Draft Built Resources Technical Memorandum		2 wks	Thu 5/11/23	Wed 5/24/23									
29	Archaeological Resources Technical Memorandum		45 days	Thu 3/23/23	Wed 5/24/23									
30	ICF Submits Draft Archaeological Resources Technical Memorandum		5 wks	Thu 3/23/23	Wed 4/26/23									
31	City Reviews Draft Archaeological Resources Technical Memorandum	Assumes comments are consolidated and conflicting comments are resolved	2 wks	Thu 4/27/23	Wed 5/10/23									
32	ICF Submits Final ICF Submits Draft Archaeological Resources Technical Memorandum		2 wks	Thu 5/11/23	Wed 5/24/23									
33	Transportation Impact Analysis Report		65 days	Mon 3/13/23	Fri 6/9/23									
34	Hexagon Submits Draft Trip Generation, Distribution, and Assignment		1 wk	Mon 3/13/23	Fri 3/17/23									
35	City Reviews Draft Trip Generation, Distribution, and Assignment		2 wks	Mon 3/20/23	Fri 3/31/23									
36	Hexagon Submits Preliminary VMT results		2 wks	Mon 3/13/23	Fri 3/24/23									
37	City Reviews Preliminary VMT Results		2 wks	Mon 3/27/23	Fri 4/7/23									
38	Hexagon Submits Preliminary Results for Non-CEQA Analyses		4 wks	Mon 3/13/23	Fri 4/7/23									
39	City Reviews Preliminary Results for Non-CEQA Analyses		2 wks	Mon 4/10/23	Fri 4/21/23									
40	Hexagon Provides Required Data for AQ & Noise Analyses		6 wks	Mon 4/3/23	Fri 5/12/23									
41	Hexagon Submits Administrative Draft TIA Report		8 wks	Mon 4/3/23	Fri 5/26/23									
42	Hexagon Submits Administrative Draft Transportation Section of the Infill Exemption Document		10 wks	Mon 4/3/23	Fri 6/9/23									
43	Infill Exemption Document		78 days	Mon 5/15/23	Wed 8/30/23									
44	ICF Submits Administrative Draft Infill Exemption Document	This task includes the preparation of the AQ and Noise analysis as well as the incorporation of the results of the standalone technical reports into the Infill Exemption document	5 wks	Mon 5/15/23	Fri 6/16/23									
45	City Reviews Administrative Draft Infill Exemption Document	Assumes comments are consolidated and conflicting comments are resolved	2 wks	Mon 6/19/23	Fri 6/30/23									
46	ICF Submits Screencheck Draft Infill Exemption Document		2 wks	Mon 7/3/23	Fri 7/14/23									
47	City Reviews Screencheck Draft Infill Exemption Document	Assumes comments are consolidated and conflicting comments are resolved	1 wk	Mon 7/17/23	Fri 7/21/23									
48	ICF Submits Final Infill Exemption Document		1 wk	Mon 7/24/23	Fri 7/28/23									
49	City Approves Final Infill Exemption Document		3 days	Mon 7/31/23	Wed 8/2/23									
50	ICF Submits Administrative Record for the Infill Exemption Document		1 wk	Mon 7/31/23	Fri 8/4/23									
51	Public Hearing	Timing is estimated	1 day	Mon 8/28/23	Mon 8/28/23									
52	ICF Prepares Notice of Exemption		1 day	Tue 8/29/23	Tue 8/29/23									
53	City Files Notice of Exemption		1 day	Wed 8/30/23	Wed 8/30/23									



Appendix D Cost Estimate



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ICF (NASDAQ:ICFI) is a global consulting and digital services company with more than 8,000 full- and part-time employees, but we are not your typical consultants. At ICF, business analysts and policy specialists work together with digital strategists, data scientists, and creatives. We combine unmatched industry expertise with cutting-edge engagement capabilities to help organizations solve their most complex challenges. Since 1969, public and private sector clients have worked with ICF to navigate change and shape the future. Learn more at [icf.com](https://www.icf.com).



STAFF REPORT

City Council

Meeting Date:

2/28/2023

Staff Report Number:

23-040-CC

Consent Calendar:

Award a construction contract to Radius Earthwork Inc. for the Ravenswood Avenue Resurfacing project and determine this action is categorically exempt under California Environmental Quality Act Guidelines §15301 and §15304 exemption for existing facilities

Recommendation

Staff recommends that the City Council award a \$802,590 construction contract, with rubberized asphalt per bid alternate A, to Radius Earthwork Inc. (Attachment A), approve contingency in the amount of \$121,000 (held by the City), approve construction administration fees in the amount of \$150,000, and determine that this action is categorically exempt under California Environmental Quality Act (CEQA) Guidelines §15301 and §15304 for the Ravenswood Avenue Resurfacing project.

Policy Issues

This project is consistent with the City's goal of maintaining and enhancing its municipal infrastructure and extending the life and safety of its roadway network.

Background

Menlo Park uses a Pavement Management System (PMS) that is approved by the Metropolitan Transportation Commission (MTC.) Through grants provided by MTC, a pavement management technical assistance program report is generated every two years. This report evaluates the City's street network and develops a list of potential street maintenance projects, over a five-year span, based on pavement conditions and budgeting. This program is an effective tool for managing the City's street network and to keep streets at an appropriate condition. Ravenswood Avenue was identified for resurfacing in the City's PMS and technical assistance program report, and it is included in the capital improvement program.

Ravenswood Avenue is classified as "Avenue – mixed use" in the General Plan (equivalent to a minor arterial street per the Federal Highway Administration.) The scope of paving work on Ravenswood Avenue is from El Camino Real to Laurel Street. A portion of the work overlaps the jurisdictions of Caltrans (at El Camino Real) and Caltrain (railroad crossing), therefore, encroachment permits from these agencies will be required for these areas. Staff is coordinating encroachment permits from both agencies which are anticipated to be issued before construction in April 2023.

Rubberized asphalt concrete (bid alternate)

In 2021, the City Council held a study session (Attachment B) to review rubberized asphalt concrete and hot mix asphalt pavement resurfacing options for upcoming capital projects. Rubberized asphalt is composed of recycled tires, reduces noise from tires, and has a 20-year typical life expectancy compared to 15 years for hot mix asphalt. However, rubberized asphalt also costs 20 to 35 percent more than hot mix asphalt and is

generally more labor intensive to install. The City Council directed staff to pursue rubberized asphalt concrete as a bid alternative to hot mix asphalt for:

- Projects proposing a 1.2 to 2.4-inch top-lift overlay
- Projects along arterial or collector streets
- Projects planned for summer construction

The project description meets the above criteria, therefore, rubberized asphalt was included as substitute for hot mix asphalt per bid alternate A.

Coordination with related projects

At its April 26, 2022, meeting, City Council directed staff to pursue closing the gaps in the bicycle network on Ravenswood Avenue between Laurel Street and El Camino Real in both directions (Attachment C provides a hyperlink to the staff report from that meeting.) On the south side of Ravenswood Avenue, the resurfacing project will add a continuous bicycle lane between El Camino Real and Laurel Street by making minor adjustments to lane widths. On the north side of the street, between Alma Street and El Camino Real, there is sufficient street width to accommodate bicycle lanes by making minor adjustments to lane widths. From Noel Drive to Alma Street, City Council directed staff to pursue a pilot to add the missing 300 feet of bikeway in place of the existing travel lane. With this change, Ravenswood Avenue would open from one to two automobile lanes at Alma Street instead of at Noel Drive as it does today. The pilot lane modification is anticipated to be installed by March 3, weather permitting, and more information is provided below in the Analysis section.

The City is concurrently working with Caltrans to add the missing crosswalk across El Camino Real at Ravenswood Avenue/Menlo Avenue. Staff are coordinating between the two projects to avoid duplication of effort. The City received a \$200,000 grant from the San Mateo County Transportation Authority (SMCTA) Alternative Commute Reduction/Transportation Demand Management program to help fund this improvement.

Analysis

Project description

The work to be done generally includes the following modifications on Ravenswood Avenue from El Camino Real to Laurel Street:

- Installing a 2.4-inch mill and asphalt overlay using rubberized asphalt (per bid alternate A)
- Installing six-inch deep asphalt base repairs for roadway areas exhibiting signs of settlement
- Adjusting utility structures (manholes, valves etc.) to align with the new roadway surface where overlapping the resurfacing work
- Installing 9 new accessible curb ramps
- Installing 3-inch mountable curb
- Replacing signs and installing new striping (including the bike lane pilot from Noel Drive to Alma Street)

Construction bidding

On December 9, 2022, the City solicited bids from prospective contractors for the project. Hot mix asphalt was included as a base bid item, with rubberized asphalt as a bid alternate. Project bids were opened January 19, 2023 with results listed per Table 1. Of the seven bids received, Radius Earthwork Inc. was identified as the apparent low bidder with a base bid price of \$694,700. Their bid was 17 percent lower than the engineer's estimate of \$840,000. Additionally, the apparent low bidder's total including bid alternate A was \$802,590 which is 14 percent lower than the engineer's estimate of \$932,400.

Table 1: Bid results		
Bidder	Base bid (hot mix asphalt)	Total bid, including bid Alternate A (rubberized asphalt)
Engineer's estimate	\$840,000	\$932,400
Radius Earthwork, Inc.	\$694,700	\$802,590
G. Bortolotto & Company, Inc.	\$816,186	\$838,116
Cato's Paving	\$888,148	\$1,018,468
O'Grady Paving, Inc.	\$896,480	\$925,930
Graniterock Company	\$914,675	\$1,044,475
JV Lucas Paving	\$945,694	\$1,005,694
Granite Construction Company	\$1,178,985	\$1,279,675

City staff has found the lowest bidder to be experienced with projects involving similar scopes of work for asphalt and concrete improvements. Staff also determined the low bidder to be both responsive and responsible per public contracting code requirements. Subject to the City Council's award of contract, the project is tentatively scheduled for construction from April 2023 to early June 2023.

Staff anticipates paving work to occur over two weekends (from 8 a.m. to 5 p.m.) to minimize traffic impacts along Ravenswood Avenue. Furthermore, the paving work may require a full closure of the street to expedite project delivery. Any closures or detours required to facilitate traffic controls during construction will be published on the City's project webpage, and posted on message boards, in advance of work. Staff will also distribute construction notices to nearby businesses, residents, churches and Menlo Atherton High School before construction. Additionally, any impacts to El Camino Real or the Alma Street Rail crossing will be coordinated with Caltrans and Caltrain representatives, respectively.

Bicycle lane pilot

As described above, staff is working to install a pilot segment of bicycle lane on Ravenswood Avenue westbound between Noel Drive and Alma Street. The pilot will begin at the beginning of March and continue through the repaving project. Staff, with support from data collection contractors, will gather the data identified in the April 26, 2022, staff report, including collision data, queuing, and multimodal use and report back to City Council, tentatively planned for May 2023. The main issue of concern is if the small reduction in vehicle capacity required to complete the bicycle lane creates queuing issues at the Ravenswood Avenue and Laurel Street traffic signal. Because spring is a relatively busy time of year, introducing the pilot before paving will allow quick identification of any concerns and to pivot quickly if issues are observed since paving is imminent. Depending on the results of the evaluation, the pilot could be continued for a longer period of time, made permanent or ended, depending on City Council direction.

Impact on City Resources

The project is included in the fiscal year 2022-23 capital improvement program, and has \$940,000 available from the highway user (gas) tax funds remaining.

The estimated construction budget is \$1,073,590 as summarized in Table 2 and includes a 15 percent construction contingency to account for unforeseen field issues (including provisions for additional traffic control measures at El Camino Real and Alma Street.) Construction administration fees are estimated at

\$150,000 and include construction management, consultant services for engineering, inspection and arborist support, and fees pertaining to the procurement of encroachment permits from Caltrans and Caltrain.

To provide sufficient funding for the use of rubberized asphalt, \$140,000 in additional funding would be required and is currently available in the street resurfacing project budget. Staff recommends utilizing \$140,000 of SB 1 funds from the street resurfacing project. The use of this funding will not impact the funding needed to deliver the 2023 street resurfacing project expected to be awarded later this spring and constructed this summer as well.

Table 2: Construction budget		
Item	Base bid (hot mix asphalt)	Total bid, including bid Alternate A (rubberized asphalt)
Construction Subtotal	\$694,700	\$802,590
Contingency	\$104,205	\$121,000
Construction Administration	\$150,000	\$150,000
Total budget	\$948,905	\$1,073,590
Project funds	\$940,000	\$940,000
Additional SB 1 funding from street resurfacing project	-	\$140,000
Total available funding		\$1,080,000

Staff recommends that the City Council award a \$802,590 construction contract, with rubberized asphalt, to Radius Earthwork Inc., approve contingency in the amount of \$121,000 (held by the City), and approve construction administration fees in the amount of \$150,000 for the Ravenswood Avenue Resurfacing project.

The cost to implement the bicycle lane pilot between Noel Drive and Alma Street is anticipated to cost less than \$20,000 and will be implemented through the City’s existing on-call contracts.

Environmental Review

The project is categorically exempt under Section 15301 – Class 1 and Section 15304 – Class 4 of the current CEQA Guidelines. Both sections allow for minor alternations of existing facilities, including existing highways and streets, sidewalks, gutters, bicycle and pedestrian access, and similar facilities, as long as there is negligible or no expansion of use.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

Staff Report #: 23-040-CC

A. Construction contract

B. Hyperlink – April 20, 2021 RAC study session staff report:

menlopark.org/DocumentCenter/View/27934/D1-20210420-CC-Rubbarized-asphalt

C. Hyperlink – Ravenswood pedestrian and bicycle pilot from April 26, 2022:

menlopark.gov/files/sharedassets/public/agendas-and-minutes/city-council/2022-meetings/agendas/20220426-city-council-agenda-packet.pdf

Report prepared by:

Mitchell Supan, Associate Engineer

Report reviewed by:

Tanisha Werner, Assistant Public Works Director – Engineering

CONSTRUCTION AGREEMENT

City Manager's Office
 701 Laurel St., Menlo Park, CA 94025
 tel 650-330-6620



Agreement #:
AGREEMENT FOR SERVICES BETWEEN THE CITY OF MENLO PARK AND FIRST PARTY
THIS CONSTRUCTION CONTRACT ("Contract") is made and entered into this ____ day of _____, _____ ("Execution Date") by and between the CITY OF MENLO PARK, a California municipal corporation, ("City") and Radius Earthwork, Inc ("Contractor").
RECITALS
<p>A. Contractor is a <u>California Corporation</u> duly organized and in good standing in the State of California, License Number <u>1059010</u>. Contractor represents and warrants that it has the background and experience set forth in the Contractor's responses to the notice inviting bids.</p> <p>B. Contractor represents that it is duly licensed by the State of California and has the background, knowledge, experience and expertise to perform the obligations set forth in this Contract.</p> <p>C. On <u>December 9, 2022</u>, the City issued a Notice to Contractors inviting bids for the Project. A copy of the Contractor's Bid proposal and List of Subcontractors is attached herein and incorporated by this reference.</p> <p>D. The City desires to retain Contractor as an independent contractor to provide the construction and other services identified in this Contract for the Project upon the terms and conditions contained herein.</p>
AGREEMENT
<p>NOW, THEREFORE, in consideration of performance by the parties of the promises, covenants and conditions contained herein, the parties hereby agree as follows:</p> <p>1. DEFINITIONS. Capitalized terms used throughout the Contract Documents shall have the meanings set forth in this Contract and/or the Special Provisions. If there is a conflict between the definitions in this Contract and the Special Provisions, the definitions in this Contract shall prevail.</p> <p>2. PROJECT. The project is the construction of <u>Ravenswood Avenue Street Resurfacing Project, No. CPS009</u> ("Project"). The work includes all labor, materials, equipment, services, permits, licenses and taxes, and all other things necessary for Contractor to perform its obligations and complete the Project, including, without limitation, any Change Orders executed by City and Contractor in</p>

accordance with the requirements of the Contract Documents (“Work”).

3. CONTRACT DOCUMENTS.

3.1 List of Documents. The Contract Documents (sometimes collectively referred to as “Agreement” or “Bid Documents”) consist of the following documents which are on file with the Public Works Department and are hereby incorporated by reference.

- 1) Change Orders
- 2) Field Orders
- 3) Contract
- 4) Bidding Addenda
- 5) Special Provisions
- 6) Project Plans and Drawings
- 7) Technical Specifications
- 8) City Standard Details
- 9) State of California Department of Transportation Specifications, 2006 Edition (Cal Trans specifications)
- 10) Notice to Contractors
- 11) Contractor's Bid
- 12 Bidder Certifications, Questionnaire and Statements
- 13) Reports listed in the Contract Documents
- 14) City of Menlo Park Waste Management Form, Waste Management Daily Transport Report
- 15) City of Menlo Park Truck Route Map and Regulations
- 16) Performance, Payment and Maintenance Bonds

3.2 Order of Precedence. For the purposes of construing, interpreting and resolving inconsistencies between and among the provisions of this Contract, the Contract Documents shall have the order of precedence as set forth in the preceding section. If a claimed inconsistency cannot be resolved through the order of precedence, the City shall have the sole power to decide which document or provision shall govern as may be in the best interests of the City.

4. PERMITS. Contractor, at its sole expense, shall obtain and maintain during the term of this Contract, all appropriate permits, licenses and certificates that may be required in connection with the performance of the Work, including, but not limited to, a City business license.

5. DEPARTMENT OF INDUSTRIAL RELATIONS. Contractor and any subcontractor performing Work on this Project shall be registered with the Department of Industrial Relations (“DIR”) pursuant to Labor Code Section 1725.5. No contractor or subcontractor may be listed on a bid proposal for a public works project unless registered with the DIR pursuant to Labor Code Section 1725.5, with limited exceptions from this requirement for bid purposes only under Labor Code Section 1771.1(a). This Project is subject to compliance monitoring and enforcement by the DIR. It is the responsibility of the Contractor to ensure all DIR requirements and regulations are met and stay current. For more information, see <http://dir.ca.gov/Public-Works/SB854.html>.

6. TERM. This Contract is effective on the Execution Date set forth in the initial paragraph of

this Contract and shall remain in effect until the Project has been satisfactorily completed by Contractor, unless earlier terminated pursuant to the terms of this Contract.

7. TIME OF COMPLETION. Time is of the essence with respect to all time limits set forth in the Contract Documents. Contractor shall commence the Work on the date specified in the City's Notice to Proceed. Contractor shall diligently prosecute the Work to Substantial Completion within the working days specified per the contract documents and the City's Notice to Proceed ("Contract Time"). The Contract Time may only be adjusted for extensions of time approved by the City and agreed to by Change Order executed by City and Contractor in accordance with the requirements of the Contract Documents.

8. COMPENSATION. The City agrees to compensate Contractor for its satisfactory completion of the Work in compliance with the Contract Documents for the not to exceed amount of eight hundred and two thousand five hundred ninety (\$802,590) ("Contract Sum"). Payment shall be as set forth in the Plans, Special Provisions and/or Technical Specifications. The Contract Sum may only be adjusted by Change Orders issued, executed and satisfactorily performed by Contractor in accordance with the requirements of the Contract Documents. The Contract Sum shall be adjusted (upward or downward) only to account for Change Orders. The Contract Sum is and shall be full compensation for all Work performed by Contractor. The Contract Sum shall cover all losses arising out of the nature of the Work or from the elements or any unforeseen difficulties or obstructions which may arise or be encountered in performance of the Work until its Acceptance by the City, all risks connected with the Work and any and all expenses incurred due to the suspension or discontinuance of the Work.

9. STANDARD OF PERFORMANCE. As a material inducement to the City to enter into this Contract, Contractor hereby represents and warrants that it has the qualifications and experience necessary to undertake the Work to be provided and the Project to be completed pursuant to this Contract. Contractor agrees that the Work shall be performed by qualified, experienced and well-supervised personnel. The Work performed pursuant to this Contract shall be performed in a manner consistent with the standard of care under California law applicable to those who specialize in providing such services for projects of the type, scope and complexity of the Project.

10. COMPLAINT WITH LAW. This Project constitutes a public work within the meaning of California Labor Code Section 1720 et. seq. and is subject to prevailing wage laws. The Work performed by Contractor pursuant to this Contract shall be provided in accordance with all ordinances, resolutions, statutes, rules and regulations of the City, and any federal, state or local governmental agency having jurisdiction in effect at the time the work is rendered.

11. REPRESENTATIVE. Janamjit Gondara is hereby designated as the project manager/superintendent/foreman of Contractor authorized to act on its behalf with respect to the Work specified in this Contract. It is expressly understood that the experience, knowledge, capability and reputation of Radius Earthwork, Inc. were a substantial inducement for City to enter into this Contract. Therefore, Janamjit Gondara shall be responsible during the term of this Contract for directing all activities of Contractor and devoting sufficient time to personally supervise the services hereunder. The representative may not be changed by Contractor

without the express written approval of the City.

12. LIQUIDATED DAMAGES.

12.1 Entitlement. City and Contractor acknowledge and agree that if Contractor fails to fully and satisfactorily complete the Work within the Contract Time, the City will suffer, as a result of Contractor's failure, substantial damages which are both extremely difficult and impracticable to ascertain. Such damages may include, but are not limited to: (a) loss of public confidence in the City and its contractors; (b) loss of public use of public facilities; and (c) extended disruption to public.

12.2 Daily Amount. City and Contractor have reasonably endeavored, but failed, to ascertain the actual damage that the City will incur if the Contractor fails to achieve Substantial Completion of the Work within the Contract Time. Therefore, the parties agree that in addition to all other damages to which the City may be entitled other than delay damages, in the event the Contractor shall fail to achieve Substantial Completion of the Work within the Contract Time, Contractor shall pay City as liquidated damages the amount of Five Hundred Dollars (\$500) per day for each calendar day after the expiration of the Contract Time until Contractor achieves Substantial Completion of the Work. The liquidated damages amount is not a penalty, but a reasonable estimate of the amount of damages the City will suffer.

12.3 Apportionment. Such liquidated damages shall be subject to reduction for delays for which Contractor is entitled to receive an extension of time under the Contract Documents ("Apportionment"). Such Apportionment shall not be affected by the fact that liquidated damages may not be applied for periods of time during which delays have occurred that are caused by both City and Contractor. It is agreed that the liquidated damages shall not be applied for portions of the Work completed prior to the expiration of the Contract Time.

12.4 Exclusive Remedy. City and Contractor acknowledge and agree that this Section 11, Liquidated Damages, shall be the City's only remedy for delay damages caused by the Contractor's failure to achieve Substantial Completion of the Work within the Contract Time.

12.5 Damages upon Abandonment. In the event that the Contractor either abandons the Work or is terminated for default in accordance with the provisions of this Contract, City shall have the right, in its sole discretion exercised by written notice issued either before or after Substantial Completion, to elect to either assert or waive its right to liquidated damages. If City elects to assert its right to liquidated damages, then the liquidated damages shall be calculated from expiration of the Contract Time to the date that Substantial Completion of the Work is achieved by the City or its replacement contractor employed to complete Contractor's performance. If City elects to waive its right to liquidated damages, then Contractor shall be liable to the City, in lieu of the liquidated damages, for all actual Losses (as defined in the General Conditions) proximately resulting from Contractor's failure to complete the Work within the Contract Time.

12.6 Other Remedies. The parties further acknowledge and agree that the City is entitled to any and all available legal and equitable remedies City may have where City's Losses are

caused by any reason other than Contractor's failure to achieve Substantial Completion of the Work within the Contract Time.

13. INDEPENDENT CONTRACTOR. Contractor is, and shall at all times remain as to the City, a wholly independent contractor and not an agent or employee of the City. Contractor shall receive no premium or enhanced pay for work normally understood as overtime, nor shall Contractor receive holiday pay, sick leave, administrative leave, or pay for any other time not actually worked. The intention of the parties is that Contractor shall not be eligible for benefits and shall receive no compensation from the City except as expressly set forth in this Contract. Contractor shall have no power to incur any debt, obligation, or liability on behalf of the City or otherwise act on behalf of the City as an agent. Neither the City, nor any of its agents shall have control over the conduct of Contractor, any of Contractor's employees, or any subcontractors, except as set forth in this Contract. Contractor shall at no time, or in any manner, represent that it or any of its agents or employees or subcontractors are in any manner employees of the City. Contractor agrees to pay all required taxes on amounts paid to Contractor under this Contract, and to indemnify and hold the City harmless from any and all taxes, assessments, penalties, and interest asserted against the City by reason of the independent contractor relationship created by this Contract. Contractor shall fully comply with the worker's compensation law regarding Contractor, Contractor's employees and subconsultants. Contractor further agrees to indemnify and hold the City harmless from any failure of Contractor and any subconsultants to comply with applicable worker's compensation laws.

14. CONFLICT OF INTEREST. Contractor covenants that it presently has no interest and shall not acquire any interest, direct or indirect, which may be affected by the work to be performed by Consultant under this Contract, or which would conflict in any manner with the performance of its services hereunder. Contractor further covenants that, in performance of this Contract, no person having any such interest shall be employed by it. Furthermore, Contractor shall avoid the appearance of having any interest which would conflict in any manner with the performance of the work pursuant to this Contract. Contractor agrees not to accept any employment during the term of this Contract which is or may make Contractor financially interested, as provided in California Government Code Sections 1090 and 87100, in any decision made by the City on any matter in connection with which Contractor has been retained pursuant to this Contract. However, nothing herein shall preclude Contractor from accepting other engagements with the City.

15. INDEMNIFICATION.

15.1 To the fullest extent permitted by law, Contractor shall indemnify, defend, with independent counsel approved by the City, and hold harmless the City, and its elective or appointive boards, officers, employees agents and volunteers ("Indemnatee") from and against any and all claims, losses, or liability that may arise out of or result from damages to property or personal injury received by reason of, or in the course of work performed under this Contract due to the acts or omissions of Contractor or Contractor's officers, employees, agents or subcontractors. The indemnification provisions survive completion of the Work or the termination of this Contract. The acceptance of such services shall not operate as a waiver of

such right of indemnification. Notwithstanding the foregoing, nothing contained herein shall be construed as obligating Contractor to indemnify any Indemnitee for any claims, losses or liability resulting from the sole or active negligence or willful misconduct of the Indemnitee. Contractor shall pay City for any costs incurred in enforcing this provision.

15.2 The City does not and shall not waive any rights that they may possess against Contractor because of the acceptance by the City or the deposit with the City of any insurance policy or certificate required pursuant to this Contract. This hold harmless and indemnification provision shall apply regardless of whether or not any insurance policies are determined to be applicable to the claim, demand, damage, liability, loss, cost or expense.

15.3 Pursuant to Public Contract Code Section 9201, the City shall timely notify Contractor upon receipt of any third-party claim relating to the Contract.

16. ASSIGNABILITY. The parties agree that the experience and qualifications of Contractor as set forth in the Contractor's Bid are material considerations for the City entering into this Contract. Consultant shall not assign or transfer any interest in this Contract, without the prior written consent of the City, and any attempt by Contractor to do so shall be void and of no effect and a breach of this Contract. For purposes of this section, the sale, assignment, transfer or other disposition of any of the issued and outstanding capital stock of Contractor or of any general partner or joint venturer or syndicate member of Contractor, if a partnership or joint venture or syndicate or co-tenancy exists, which shall result in changing the control of Contractor, shall be construed as an assignment of this Construction Contract. Control means more than fifty percent (50%) of the voting power of the corporation or other entity.

17. INSURANCE AND BOND REQUIREMENTS.

17.1 Prior to the commencement of any Work, the Contractor shall provide the City with evidence that it has obtained the insurance required by this Section and all bonds, including, but not limited to, payment and performance bonds, required in the Special Provisions. Failure to obtain and maintain the required insurance and bonds to so shall be deemed a material breach of this Contract.

17.2 Insurance Requirements. Contractor shall obtain the following insurance.

A. Worker's Compensation and Employer's Liability Insurance: The CONTRACTOR shall have in effect during the entire life of this Contract workers' compensation and Employer's Liability Insurance providing full statutory coverage. In signing this Contract, the CONTRACTOR makes the following certification, required by Section 18161 of the California Labor Code: "I am aware of the provisions of Section 3700 of the California Labor Code which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with the provisions of the Code, and I will comply with such provisions before commencing the performance of the work of this Contract."

B. Commercial General Liability Insurance: The CONTRACTOR shall take out and maintain during the life of this Contract such Bodily Injury Liability and Property Damage Liability

Insurance (Commercial General Liability Insurance) on an occurrence basis as shall protect it while performing work covered by this Contract from any and all claims for damages for bodily injury, including accidental death, as well as claims for property damage which may arise from the CONTRACTOR's operations under this Contract, whether such operations be by CONTRACTOR or by any sub-consultant or by anyone directly or indirectly employed by either of them. The amounts of such insurance shall be not less than two million dollars (\$2,000,000) per occurrence and four million dollars (\$4,000,000) in aggregate, or four million dollars (\$4,000,000) combined single limit bodily injury and property damage for each occurrence. CONTRACTOR shall provide the City with acceptable evidence of coverage, including a copy of all declarations of coverage exclusions.

C. Automobile Liability Insurance: CONTRACTOR shall maintain Automobile Liability Insurance pursuant to this Contract in an amount of not less than one million dollars (\$1,000,000) for each accident combined single limit or not less than one million dollars (\$1,000,000) for any one (1) person, and one million dollars (\$1,000,000) for any one (1) accident, and Three Hundred Thousand Dollars, (\$300,000) property damage.

17.3 CITY and its subsidiary agencies, and their officers, agents, employees and servants shall be named as additional insured on any such policies of Commercial General Liability and Automobile Liability Insurance, (but not for the workers' compensation), which shall also contain a provision that the insurance afforded thereby to the CITY, its subsidiary agencies, and their officers, agents, employees, and servants shall be primary insurance to the full limits of liability of the policy, and that if the CITY, its subsidiary agencies and their officers and employees have other insurance against a loss covered by a policy, such other insurance shall be excess insurance only.

17.4 In the event of the breach of any provision of this Section, or in the event any notice is received which indicates any required insurance coverage will be diminished or canceled, CITY, at its option, may, notwithstanding any other provision of this Contract to the contrary, immediately declare a material breach of this Contract and suspend all further work pursuant to this Contract.

17.5. Before the execution of this Contract, any deductibles or self-insured retentions must be declared to and approved by CITY.

18. SUSPENSION. The City may, at any time and from time to time, without cause, order Contractor, in writing ("Suspension Order"), to suspend, delay, or interrupt the Work in whole or in part for such period of time, up to an aggregate of fifty percent (50%) of the Contract Time, as City may determine, with such period of suspension to be computed from the date of the Suspension Order. Upon receipt of a Suspension Order, Contractor shall, at City's expense, comply with its terms and take all reasonable steps to minimize costs allocable to the Work covered by the Suspension Order during the period of work stoppage. Within the period of the above noted aggregate time, or such extension to that period as is agreed upon by Contractor and City, City shall either cancel the Suspension Order or delete the work covered by the Suspension Order by issuing a Change Order. If a Suspension Order is canceled or expires, Contractor shall resume and continue with the Work. A Change Order will be issued to cover

any adjustments of the Contract Sum or the Contract Time necessarily caused by such suspension.

19. BOOKS AND RECORDS. Contractor shall keep full and detailed accounts and exercise such controls as may be necessary for proper financial management under this Contract in accordance with generally accepted accounting principles and practices consistently applied. City and City's accountants shall be afforded access at all times during normal business hours, to inspect, audit and copy Contractor's records, books, estimates, take-offs, cost reports, ledgers, schedules, correspondence, instructions, drawings, receipts, subcontracts, purchase orders, vouchers, memoranda and other data relating to this Project, and Contractor shall preserve these for a period of three years after the later of (i) final payment or (ii) final resolution of all Contract Disputes and other disputes or for such longer period as may be required by law. Contractor's compliance with any request by City pursuant to this Section 18 shall be a condition precedent to filing or maintenance of any legal action or proceeding by Contractor against City and to Contractor's right to receive further payments under the Contract Documents. Any failure by Contractor to provide access to its business records for inspection or copying by City shall be specifically enforceable by issuance of a writ or a provisional or permanent mandatory injunction by a court of competent jurisdiction based on affidavits submitted to such court, without the necessity of oral testimony.

20. WAIVER. Waiver by either party of any breach or violation of any one or more terms or conditions of this Contract shall not be deemed to be a waiver of any other term or condition contained herein or a waiver of any subsequent breach or violation of the same or any other term or condition. Acceptance by the City of the performance of any work by the Contractor shall not be deemed to be a waiver of any term or condition of this Contract. In no event shall the City's making of any payment to Contractor constitute or be construed as a waiver by the City of any breach of this Contract, or any default which may then exist on the part of Contractor, and the making of any such payment by the City shall in no way impair or prejudice any right or remedy available to the City with regard to such breach or default.

21. DEFAULT. In the event the City determines, in its sole discretion, that Contractor has failed or refused to perform any of the obligations set forth in the Contract Documents, or is in breach of any provision of the Contract Documents, the City may give written notice of default to Contractor in the manner specified for this giving of notices in this Contract. Except for emergencies, Contractor shall cure any default in performance of its obligations under the Contract Documents within two (2) business days after receipt of written notice. However, if the breach cannot be reasonably cured within such time, Contractor will commence to cure the breach within two (2) days and will diligently and continuously prosecute such cure to completion within a reasonable time, which shall in no event be later than ten (10) days after receipt of such written notice.

22. CITY RIGHTS AND REMEDIES.

22.1 Remedies Upon Default. In the event that Contractor fails to cure any default of this Contract within the time period set forth in Section 20, then City may pursue any remedies available under law or equity, including, without limitation, the following: (1) the City may,

without terminating the Contract, delete certain portions of the Work, reserving to itself all rights to losses related thereto; (2) the City may, without terminating the Contract, engage others to perform the Work or portion of the Work that has not been performed by the Contractor and withhold the cost thereof to City from future payments to the Contractor, reserving to itself all rights to Losses related thereto; or (3) the City may, without terminating the Contract and reserving to itself all rights to Losses related thereto, suspend all or any portion of this Construction Contract for as long a period of time as City determines, in its sole discretion, appropriate, in which event City shall have no obligation to adjust the Contract Sum or Contract Time, and shall have no liability to Contractor for damages if City directs Contractor to resume Work; (4) the City may terminate all or any part of this Contract for default, reserving to itself all rights of Losses related thereto; or (5) the City may, without terminating the Contract and reserving to itself all rights to Losses related thereto, exercise its rights under the Performance Bond.

22.2 Additional Provisions. All of City's rights and remedies under this Contract are cumulative, and shall be in addition to those rights and remedies available in law or in equity. Designation in the Contract Documents of certain breaches as material shall not be construed as implying that other breaches not so designated are not material nor shall such designations be construed as limiting City's right to terminate the Contract, or the exercise of its other rights or remedies for default, to only material breaches. City's determination of whether there has been noncompliance with the Contract so as to warrant exercise by City of its rights and remedies for default under the Contract, shall be binding on all parties. No termination or action taken by City after such termination shall prejudice any other rights or remedies of City provided by law or equity or by the Contract Documents upon such termination; and City may proceed against Contractor to recover all liquidated damages and Losses suffered by City.

22.3 Delays by Sureties. Without limitation to any of City's other rights or remedies under the law, City has the right to suspend the performance by Contractor's sureties in the event of any of the following: (1) failure of the sureties to begin Work within a reasonable time in such manner as to insure full compliance with the Contract within the Contract Time; (2) abandonment of the Work; (3) if at any time City is of the opinion the Work is unnecessarily or unreasonably delayed; (4) willful violation of any terms of the Contract; (5) failure to perform according to the Contract Documents; or (6) failure to follow instructions of City for its completion within the Contract Time. City will serve notice of such failure upon the sureties and in the event the sureties neglect or refuse to cure the breach within the time specified in such notice, City shall have the power to suspend the performance or any part thereof of the sureties.

22.4 Damages to the City. The City will be entitled to recovery of all Losses under law or equity in the event of Contractor's default under the Contract Documents. In the event that City's Losses arise from Contractor's default under the Contract Documents, City shall be entitled to withhold monies otherwise payable to Contractor until Final Completion, as defined in the General Conditions, of the Project. If City incurs Losses due to Contractor's default, then the amount of Losses shall be deducted from the amounts withheld. Should the amount withheld exceed the amount deducted, the balance will be paid to Contractor or its designee upon Final Completion of the Project. If the Losses incurred by City exceed the amount withheld,

Contractor shall be liable to City for the difference and shall promptly remit same to City.

22.5 Termination of the Contract for Default. Without limitation to any of City's other rights or remedies at law or in equity, and reserving to itself all rights to Losses related thereto, City shall have the right to terminate this Contract, in whole or in part, upon the failure of Contractor to promptly cure any default. City's election to terminate the Contract for default shall be communicated by giving Contractor a written notice of termination in the manner specified for the giving of notices in the Contract. Any notice of termination given to Contractor by City shall be effective immediately, unless otherwise provided therein.

22.6 Termination Without Cause. City shall have the option, at its sole discretion and without cause, of terminating this Contract in part or in whole by giving thirty (30) days written notice to Contractor. Contractor agrees to accept such sums as allowed under this Section as its sole and exclusive compensation and waives any claim for other compensation or Losses, including, but not limited to, loss of anticipated profits, loss of revenue, lost opportunity, or other consequential, direct, indirect or incidental damages of any kind.

22.7 Compensation. Following termination without cause and within forty-five (45) days after receipt of a billing from Contractor seeking payment of sums authorized by this Section, City shall pay to Contractor as its sole compensation for performance of the Work the following: (1) the amount of the Contract Sum allocable to the portion of the Work properly performed by Contractor as of the date of termination, less sums previously paid to Contractor; (2) reasonable costs of Contractor and its Subcontractors and Sub-subcontractors for demobilizing and administering the close-out of its participation in the Project (including, without limitation, all billing and accounting functions, not including attorney or expert fees) for a period of no longer than thirty (30) days after receipt of the notice of termination in an amount not to exceed the daily sum payable to Contractor for Compensable Delays; (3) previously unpaid cost of any items delivered to the Project Site which were fabricated for subsequent incorporation in the Work.

22.8 Subcontractors. Contractor shall include provisions in all of its subcontracts, purchase orders and other contracts permitting termination for convenience by Contractor on terms that are consistent with this Contract and that afford no greater rights of recovery against Contractor than are afforded to Contractor under this Section.

22.9 Contractor's Duties Upon Termination. Upon receipt of a notice of termination for default or for convenience, Contractor shall, unless the notice directs otherwise, do the following: (1) immediately discontinue the Work to the extent specified in the notice; (2) place no further orders or subcontracts for materials, equipment, services or facilities, except as may be necessary for completion of such portion of the Work as is not discontinued; (3) provide to City a description, in writing no later than fifteen (15) days after receipt of the notice of termination, of all subcontracts, purchase orders and contracts that are outstanding, including, without limitation, the terms of the original price, any changes, payments, balance owing, the status of the portion of the Work covered and a copy of the subcontract, purchase order or contract and any written changes, amendments or modifications thereto, together with such other information as City may determine necessary in order to decide whether to accept assignment

of or request Contractor to terminate the subcontract, purchase order or contract; (4) promptly assign to City those subcontracts, purchase orders or contracts, or portions thereof, that City elects to accept by assignment and cancel, on the most favorable terms reasonably possible, all subcontracts, purchase orders or contracts, or portions thereof, that City does not elect to accept by assignment; and (5) hereafter do only such Work as may be necessary to preserve and protect Work already in progress and to protect materials, plants, and equipment on the Project Site or in transit thereto.

23. CONTRACTOR'S RIGHTS AND REMEDIES. Contractor may terminate this Construction Contract for cause only upon the occurrence of one of the following: (1) the Work is stopped for sixty (60) consecutive days, through no act or fault of Contractor, any subcontractor or any employee or agent of Contractor or any subcontractor, due to issuance of an order of a court or other public authority other than City having jurisdiction or due to an act of government, such as a declaration of a national emergency making material unavailable; or (2) if the City does not make payment of sums that are not in good faith disputed by the City and does not cure such default within ninety (90) days after receipt of notice from Contractor, then upon an additional thirty (30) days' notice to City, Contractor may terminate the Contract.

23.1 Damages to Contractor. In the event of termination for cause by Contractor, City shall pay Contractor the sums provided for in Section 21 above. Contractor agrees to accept such sums as its sole and exclusive compensation and agrees to waive any claim for other compensation or Losses, including, but not limited to, loss of anticipated profits, loss of revenue, lost opportunity, or other consequential, direct, indirect and incidental damages, of any kind.

24. NOTICES. Any notices or other communications required or permitted to be given under this Contract shall be given in writing by personal delivery, by a recognized courier service, or by U.S. mail, postage prepaid, and return receipt requested, addressed to the respective parties as follows:

To City:

Assistant Public Works Director - Engineering
City of Menlo Park
City Hall, 701 Laurel St.
Menlo Park, CA 94025

To Contractor:

Radius Earthwork, Inc.
197 E Hamilton Ave, Suite 204
Campbell, CA 95008

25. Notice shall be deemed communicated on the earlier of actual receipt or 48 hours after deposit in the U.S. mail, or the date of delivery shown on deliverer's receipt. In the event of any change of address, the moving party is obligated to notify the other party of the change of address in writing within a reasonable period of time.

In addition, copies of all Claims by Contractor under this contract shall be provided to the City Attorney as follows:

To City Attorney:

City Attorney
Burke, Williams & Sorensen, LLP
181 Third Street, Suite 200
San Rafael, CA 94901

All claims shall be delivered personally or sent by certified mail.

26. NON-DISCRIMINATION AND EQUAL EMPLOYMENT OPPORTUNITY. In the performance of this Contract, Contractor shall not discriminate against any employee, subcontractor or applicant for employment because of race, color, creed, religion, sex, marital status, sexual orientation, national origin, ancestry, age, physical or mental handicap, or medical condition. Contractor will take affirmative action to ensure that employees are treated without regard to race, color, creed, religion, sex, marital status, sexual orientation, national origin, ancestry, age, physical or mental handicap, or medical condition.

27. CONTRACT DOCUMENTS AND PRECEDENCE. The Contract Documents shall consist of the following documents. In case of inconsistencies between Contract Documents, the documents are listed in order of precedence.

28. PUBLIC WORKS CLAIMS. This Contract is subject to Public Contracts Code Section 9204 governing contractor claims.

29. ATTORNEYS' FEES; VENUE. In the event that any party to this Contract commences any legal action or proceeding to enforce or interpret the provisions of this Contract, the prevailing party in such action or proceeding shall be entitled to recover reasonable attorneys' fees and other costs incurred in that action or proceeding, in addition to any other relief to which the successful party may be entitled. The venue for any litigation shall be San Mateo County.

30. COOPERATION. In the event any claim or action is brought against the City relating to Contractor's performance or services under this Agreement, Contractor shall render any reasonable assistance and cooperation which City might require.

31. NUISANCE. Contractor shall not maintain, commit, nor permit the maintenance or commission of any nuisance in connection with the performance of services under this Contract.

32. GOVERNING LAW. This Contract shall be construed in accordance with and governed by the laws of the State of California.

33. COMPLETE AGREEMENT; SEVERABILITY. This Contract, and any other documents incorporated herein by reference, represent the entire and integrated agreement between the City and Contractor. This Contract supersedes all prior oral and written negotiations, representations or agreements. No prior oral or written understanding shall be of any force or effect with respect to those matters covered hereunder. This Contract may only be modified by

a written amendment duly executed by the parties to this Contract. In case a provision of this Contract is held to be invalid, illegal or unenforceable, the validity, legality and enforceability of the remaining provisions shall not be affected.

34. COUNTERPARTS. This Contract may be signed in multiple counterparts, which shall, when executed by all the parties constitute a single binding contract.

Signatures on next page.

IN WITNESS WHEREOF, the parties hereto have executed this agreement on the day and year first above written.

FOR FIRST PARTY:

Signature

Date

Printed name

Title

Tax ID#

APPROVED AS TO FORM:

Nira F. Doherty, City Attorney

Date

FOR CITY OF MENLO PARK:

Justin I. C. Murphy, City Manager

Date

ATTEST:

Judi A. Herren, City Clerk

Date



STAFF REPORT

City Council Meeting Date: 2/28/2023
Staff Report Number: 23-041-CC

Consent Calendar: **Adopt a resolution supporting the City’s shuttle program for application for the San Mateo County Shuttle Program fiscal year 2023-24 and 2024-25 and authorize the city manager to enter into funding agreements**

Recommendation

Staff recommends that the City Council adopt a resolution (Attachment A) in support of the Citywide shuttle program, for the San Mateo County Transportation Authority Shuttle Program fiscal years 2023-24 and 2024-25 to continue funding for operations and administration of the program and authorize the city manager to enter into necessary funding agreements and any subsequent amendments within the budgeted amounts with grant agencies.

Policy Issues

This project is consistent with the General Plan goal and policies to support local and regional transit that is efficient, frequent, convenient and safe. These policies seek to promote the use of public transit and to promote the use of alternatives to the single-occupant automobile. The grant requires an adopted resolution of support by the City Council as part of the application.

Background

The City of Menlo Park manages a shuttle program that provides transit service to many residents, employees and visitors. The program is primarily funded by grants provided by the San Mateo City/County Association of Governments (C/CAG), San Mateo County Transportation Authority (SMCTA) and Metropolitan Transportation Commission (via the San Mateo County Transit District.) These funds typically include a competitive application process, an executed agreement between parties, and a demonstrated adherence to the agreement details.

On January 13, C/CAG and SMCTA issued a joint call for shuttle projects for fiscal year 2023-24 and 2024-25. The program includes \$11,000,000 for this two-year funding cycle, and direct costs for operations, marketing, and administration of shuttles are eligible for funding. The February 14 City Council agenda included an item to authorize submitting an application, but staff withdrew the item to update the resolution (Attachment A) to be consistent with grant program requirements. The City will submit an application by the February 24 deadline. The City’s shuttles are currently supported by a grant through this program, expiring June 30.

The City’s shuttles provide “around town” transportation to many residents, employees and visitors. The program includes the following services (Attachment B provides a hyperlink to the City’s website, providing additional details):

- The M1-Crosstown shuttle provides an all-day, fixed-route service between Belle Haven and Sharon Heights. It serves several senior housing facilities, Menlo Park senior center facilities (and the Menlo Park Community Campus project once completed), the current Belle Haven library, the Veterans Affairs Medical Center, Menlo-Atherton High School, the main library, Caltrain station, Little House, Safeway, Stanford Medical Center, Stanford Shopping Center, Palo Alto Medical Foundation, and a regional transit hub in downtown Palo Alto.
- The M3-Marsh Road shuttle provides fixed-route, peak-hour service between the Caltrain station and the business parks and office complexes along Marsh Road, Bohannon Drive and the Bayfront Area. This shuttle also currently serves TIDE Academy, housing complexes along Haven Avenue, and will serve new housing currently under construction in the Bayfront Area.
- The M4-Willow Road shuttle provides fixed-route, peak-hour service between the Caltrain station and the business parks and office complexes along Willow Road and O'Brien Drive. This shuttle also serves Mid-Peninsula High School.
- The Shoppers' Shuttle is a door-to-door service, which operates three days per week providing transportation for those not near a shuttle route or that have limited mobility. Destinations include Little House, Menlo Park Senior Center, downtown Menlo Park, the main library, Safeway, Caltrain station, Stanford Shopping Center, and retail destinations in Menlo Park and Redwood City.

The City is about to start a shuttle evaluation study, which is funded in part by a Caltrans sustainable transportation planning grant. This grant application was authorized by City Council October 12, 2021, with a successful award in spring 2022. This comprehensive study will look at existing shuttle routes and ridership, existing transit services, new shuttle technologies (such as on demand or flex route service), reach out to the community for feedback and input, and provide recommendations for route changes or new service, and identify funding opportunities and partnerships. Staff will also coordinate with the Willow Village development to identify how the shuttle approved as a condition of that development could be integrated with or impact operations of the City's shuttles. The Willow Village shuttle is not expected to begin operation within the timeframe of the current grant cycle. The findings from this study will inform future budgets and funding for the shuttle program. Staff is currently reviewing proposals from consultants and anticipates bringing an item on approval of the consultant agreement to the City Council in the coming months.

Analysis

By applying for funds through the San Mateo County Shuttle Program Call for projects, the City of Menlo Park is seeking to ensure continuity of the City's current shuttle services.

The effectiveness of the City's shuttle program is evaluated by considering ridership, cost and how it enhances accessibility, especially for groups with significant need. SMCTA considers two numerical metrics, the average number of riders per service hour and the cost per rider, compared to defined benchmarks for the type of service. Tables 1 and 2 summarize these metrics for each route from fiscal year 2018-19 (Table 1) and fiscal year 2021-22 (Table 2.) The benchmarks for service are set bi-annually by C/CAG and SMCTA based on operations of the current routes in San Mateo County and shuttle management best practice information. These data are used by the grant selection committee to compare shuttles against one another.

The Shoppers' shuttle and Crosstown shuttle also provide enhanced accessibility for the groups with significant transportation needs within Menlo Park. The Shopper's shuttle provides a travel option for populations that have limited mobility or access to fixed-route transit. The M1-Crosstown shuttle connects Belle Haven and senior residents with vital services. Similar to many transit services in the Bay Area,

shuttles continue to provide vital transportation links that would be difficult to recover and implement again if they were cut.

Tables 1 and 2 reflect two different realities, one of a world pre-pandemic and one during the pandemic. As shown in Table 1, the program’s cost effectiveness (cost per rider) and ridership (passengers per hour) exceeded the County’s standards for commuter shuttles (M3-Marsh Road and M4-Willow Road.) The performance of the M1-Menlo Midday, M2-Belle Haven and Shoppers’ shuttles did not meet the benchmarks, and reflect several challenges experienced during that period, including a driver shortage, and service disruptions, and suspension of a second vehicle for both the M2-Belle Haven and M3-Marsh shuttles between October 2017 to April 2019.

Table 2 reflects ridership numbers that are still stabilizing as we adjust to the pandemic. Since March 2020, transit ridership plummeted 90 percent in the Bay Area. The rise of work-from-home and the need for shelter-in-place kept transit ridership low until 2021, when the advent of vaccines and decrease in hospitalizations led to slight increases in transit ridership. With long term employer work-from-home policies continuing to evolve, the near-term demand for transit remains unclear.

Table 1: Benchmark metrics, by shuttle route (Before COVID-19 Pandemic)				
Shuttle route	Passengers per service hour		Cost per rider	
	Actual (FY 2018-19)	Service benchmark (FY 2018-20)	Actual (FY 2018-19)	Service benchmark (FY 2018-20)
M3-Marsh Road	17.4	Above 15	\$ 6.60	Less than \$8.00
M4-Willow Road	26.1	Above 15	\$ 6.90	Less than \$8.00
M1-Menlo Midday ¹	1.1	Above 10	\$74.31	Less than \$10.00
M2-Belle Haven	6.3	Above 10	\$14.11	Less than \$10.00
Shoppers’	2.3	Above 2	\$36.45	Less than \$20.00

Note: FY 2018-19 data are presented as the most recent complete fiscal year.

¹ Entire route suspended as of September 2018; benchmarks reflect poor ridership, drivers not complying with schedules, and ridership data from only two months of the fiscal year.

Table 2: Benchmark metrics, by shuttle route (COVID-19 Pandemic)				
Shuttle route	Passengers per service hour		Cost per rider	
	Actual (FY 2021-22)	Service benchmark (FY 2020-22)	Actual (FY 2021-22)	Service benchmark (FY 2020-22)
M3-Marsh Road	5.0	Above 15	\$ 23.33	Less than \$9.00
M4-Willow Road	3.8	Above 15	\$ 58.03	Less than \$9.00
M1-Crosstown ¹	1.1	Above 10	\$89.60	Less than \$11.00
Shoppers’	2.8	Above 2	\$36.81	Less than \$22.00

Note: FY 2021-22 data are presented as the most recent complete fiscal year.

¹ M2-Belle Haven took over part of M1-Menlo Midday route, renamed to M1-Crosstown in November 2020.

Many other shuttles in San Mateo County are experiencing similar ridership decreases, with shuttles serving industries such as biotechnology and the medical field performing better because work-from-home is less common. The M3-Marsh Road and M4-Willow Road shuttles continue to have lower ridership due to fewer workers, but the return of high schools to in-person education has helped ridership rebound slightly. The M1-Crosstown shuttle has lower ridership, but provides vital connections between Belle Haven and senior centers, retail, medical and transit facilities. The Shoppers’ shuttle recovered the fastest during the pandemic, exceeding pre-pandemic ridership in 2021 and continuing to date.

The San Mateo County Shuttle Program grant criteria include the ridership and cost metrics, assessments of impact on greenhouse gas metrics and vehicle miles of travel, and assessments of how the shuttles provide enhanced accessibility to equity priority communities and amenities to riders.

Proposed program budget

The City’s shuttle program is currently funded through several sources, including grants from C/CAG, SMCTA, the Metropolitan Transportation Commission’s (MTC) Lifeline Grant Program, and the City’s Development Shuttle Fee. Table 3 summarizes the program budget for the three prior fiscal years, and the estimated budget for fiscal years 2023-24 and 2024-25, the two years covered in this San Mateo County Shuttle Program grant cycle.

Table 3: Program budget					
Shuttle route	FY 2020-21	FY 2021-22	FY 2022-23	FY 2023-24 Proposed	FY 2024-25 Proposed
M1-Crosstown	\$408,600	\$414,200	\$421,700	\$432,700	\$441,200
M3-Marsh Road ¹	\$310,200	\$314,600	\$189,100	\$188,000	\$192,000
M4-Willow Road	\$164,700	\$167,500	\$171,200	\$169,100	\$172,800
Shoppers’	\$68,300	\$69,600	\$70,600	\$74,300	\$76,100
Total	\$951,800	\$965,900	\$852,600	\$864,100	\$882,100

Note: San Mateo County Shuttle Program FY 2020-22 grant extended one year for FY 2022-23 due to COVID-19.
¹ Funding request for FY 2022-23 reduced due to second vehicle being suspended since March 2020.

If awarded, the San Mateo County Shuttle Program will fund up to 75 percent of program costs, with remaining program funds coming from sources outlined in Tables 4 and 5 below. As in past years, staff recommends not applying for funding for the Shoppers’ shuttle, as this route has not met the benchmarks to be competitive in the call for projects. Despite the Shoppers’ shuttle returning to its pre-pandemic ridership that ridership is still well below the recommended benchmark metrics.

Staff has prepared two funding allocation scenarios for the shuttle program: one where the City continues to be awarded funds from the MTC Lifeline grant program (Table 4), and one where the match is drawn entirely from local sources (Table 5.) Shuttles serving elderly and low-income residents, such as the M1-Crosstown are eligible for Lifeline funding. These two scenarios are presented since the Lifeline program funding is determined on a different cycle, and cannot be guaranteed for the two fiscal years in the San Mateo County Shuttle Program call for projects. Grant applications for Lifeline Cycle 7 funding are expected later in 2023. Historically, Lifeline has funded up to 40 percent of the M1-Crosstown shuttle, with the other 60 percent funded by the San Mateo County Shuttle Program. If the City does not receive Lifeline funding, the City would need to contribute 25 percent from local funds to continue running the service, with the remaining 75 percent coming from the San Mateo County Shuttle Program. Otherwise, the service would

need to be suspended.

If the City continues to receive Lifeline funding, the City’s contribution toward shuttle operations is estimated at up to \$98,200 in fiscal year 2023-24 and \$102,100 in fiscal year 2024-25, for a total of \$200,300 during the current grant cycle (Table 4.) If the City does not receive Lifeline funding, the City’s contribution toward shuttle operations is estimated at \$206,400 in fiscal year 2023-24 and \$212,400 in fiscal year 2024-25, totaling \$418,800 during the current grant cycle (Table 5.) This portion of the local match has historically been funded by San Mateo County Measure A funds. An additional \$65,000 a year comes from developer contributions to the City’s shuttle program. These estimates would maintain current service levels. If the City is not successful with these grants, staff would return to City Council to receive direction on whether and how to continue shuttle services within the City.

**Table 4: Allocation of program budget by source fund
(with MTC Lifeline award)**

Funding source	SMCTA and C/CAG request	MTC Lifeline ¹ award	Developer fees ²	Local match	Total
FY 2023-24	\$527,500	\$173,100	\$65,300	\$98,200	\$864,100
FY 2024-25	\$538,200	\$176,500	\$65,300	\$102,100	\$882,100
2 Year Total	\$1,065,700	\$349,600	\$130,600	\$200,300	\$1,746,200

¹ MTC Lifeline funding is anticipated to be a two-year cycle.

² Approximately \$65,300 per year comes from developer contributions to the City’s shuttle program. The local match includes 100 percent of the cost for the Shoppers’ shuttle (approximately \$75,000 annually.)

**Table 5: Allocation of program budget by source fund
(without MTC Lifeline award)**

Funding source	SMCTA and C/CAG request	Developer fees ¹	Local match ²	Total
FY 2023-24	\$592,400	\$65,300	\$206,400	\$864,100
FY 2024-25	\$604,400	\$65,300	\$212,400	\$882,100
2 Year Total	\$1,196,800	\$130,600	\$418,800	\$1,746,200

¹The City collects approximately \$65,300 per year from developer-required contributions to the City’s shuttle program.

²The local match includes 100 percent of the cost for the Shoppers’ shuttle (approximately \$75,000 annually.)

As the City pursues funding to continue operation of the shuttles, staff have also been coordinating with Commute.org about the operation of the commuter shuttles. The City officially joined Commute.org in 2022 and Mayor Wolosin currently sits on their Board. Commute.org operates most of the other commuter shuttles in San Mateo County and the existing shuttles all use Commute.org branded vehicles, for ease of deployment and efficiency in contracting. Commute.org is willing to take on the operations of the City’s two commuter shuttles starting with the new grant funding, subject to a 4 percent fee (of the total operating cost) to cover their administrative expenses. Some activities currently conducted by staff related to coordination, shuttle management, and marketing would be handled by Commute.org, which would offset the additional expense by freeing up about 20 hours of staff time per month for other transportation-related activities. The City would maintain responsibility for the Crosstown and Shoppers’ shuttles, which are outside the purview of Commute.org.

The current contract between SamTrans and the shuttle operator, which expires at the end of fiscal year

2024-25, includes the use of gas-powered vehicles. Funding from this grant source can only be used to fund shuttles contracted via this existing contract. After this contract, the City can work with Samtrans and Commute.org to pursue a zero emission bus (ZEB) fleet. Starting in 2026, the California Air Resources Board (CARB) will require smaller transit agencies to purchase ZEBs for at least 25 percent of their fleet, growing to 100 percent in 2029. Attachment C provides a hyperlink to a CARB fact sheet about the CARB Innovative Clean Transit regulations.

Impact on City Resources

If the City is successful with these grant applications, staff will include funding for the shuttles in each of the next two annual budget cycles. The estimated total annual cost of the M1-Crosstown, M3-Marsh Road, M4-Willow Road, and Shoppers' shuttle services is \$864,100 in fiscal year 2023-24 and \$882,100 in fiscal year 2024-25. The funding for the City's share of 25 percent comes from the City's Development Shuttle Fee (approximately \$65,000 per year), Measure A funds, and the MTC Lifeline Grant Program (this program provides a maximum of 40 percent for the M1-Crosstown shuttles.)

Environmental Review

This action is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §§ 15378 and 15061(b)(3) as it will not result in any direct or indirect physical change in the environment.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

- A. Resolution
- B. Hyperlink: menlopark.gov/shuttles
- C. Hyperlink: ww2.arb.ca.gov/resources/fact-sheets/innovative-clean-transit-ict-regulation-fact-sheet

Report prepared by:
Nicholas Yee, Associate Transportation Planner

Report prepared by:
Hugh Louch, Assistant Public Works Director - Transportation

RESOLUTION NO. XXXX**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO PARK
SUPPORTING THE CITY'S SHUTTLE PROGRAM AND SUBMITTING AN
APPLICATION FOR SAN MATEO COUNTY SHUTTLE PROGRAM FUNDING**

WHEREAS, there is a need for “around town” transportation to serve many residents, employees and visitors; and

WHEREAS, City of Menlo Park manages a Shuttle Program to provide commuter service to and from the Menlo Park Caltrain station and community service to link residents to vital community services and destinations; and

WHEREAS, the cost of the City's Shuttle Program is estimated to be \$1,746,200 over fiscal years 2023-24 and 2024-25; and

WHEREAS, the City wishes to sponsor the City's Shuttle Program (the Project); and

WHEREAS, the City seeks up to \$1,196,800 for the Project in fiscal years 2023-24 and 2024-25; and

WHEREAS, June 7, 1988, the voters of San Mateo County approved a ballot measure to allow the collection and distribution by the San Mateo County Transportation Authority (TA) of a half-cent transactions and use tax in San Mateo County for 25 years, with the tax revenues to be used for highway and transit improvements pursuant to the Transportation Expenditure Plan presented to the voters (Original Measure A); and

WHEREAS, November 2, 2004, the voters of San Mateo County approved the continuation of the collection and distribution by the TA the half-cent transactions and use tax for an additional 25 years to implement the 2004 Transportation Expenditure Plan beginning January 1, 2009 (New Measure A); and

WHEREAS, the Board of Directors of the City/County Association of Governments (C/CAG) of San Mateo County at its February 14, 2002 meeting approved the Congestion Relief Plan and subsequently reauthorized the Congestion Relief Plan in 2007, 2010, 2015, and 2019; and

WHEREAS, a component of the C/CAG Congestion Relief Plan is to support Local and Employer Based Shuttle Programs; and

WHEREAS, the TA and C/CAG issued a joint Call for Projects for the San Mateo County Shuttle Program on January 13, 2023; and

WHEREAS, the TA and C/CAG require a governing board resolution from the City in support of the City's application for \$1,196,800 from the San Mateo County Shuttle Program for the Project; and

WHEREAS, TA and C/CAG require the City's a governing board to adopt a resolution:

1. Supporting the Project and the City's application for \$1,196,800 in TA Measure A Shuttle Program funds for the Project;

2. Committing the City to the completion of the Project, including the commitment of matching funds in the amount of \$549,400 needed for implementation;
3. Certifying that any funds awarded by the TA or C/CAG will be used to supplement existing funds for program activities, and will not replace existing funds or resources; and
4. If funds are awarded, authorizing the City Manager, or designee, to sign a funding agreement or memorandum of understanding (MOU) with the TA and, if necessary, C/CAG, for Shuttle Program funding for the Project and to take any other actions necessary to give effect to this resolution.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Menlo Park

1. Directs staff to submit an application for funding from the San Mateo County Shuttle Program for \$1,196,800 for the Project.
2. Authorizes the City Manager, or designee, to execute a funding agreement or MOU with the San Mateo County Transportation Authority and, if necessary, the City/County Association of Governments of San Mateo County to encumber San Mateo County Shuttle Program funds.
3. Commits to completing the Project, including contributing \$549,400 of matching funds needed for Project implementation, if the requested funds are awarded.
4. Certifies that any funds awarded by the SMCTA or C/CAG will be used to supplement existing funds for program activities, and will not replace existing funds or resources.
5. Authorizes the City Manager, or designee, to take any other actions necessary to give effect to this resolution.

I, Judi A. Herren, City Clerk of Menlo Park, do hereby certify that the above and foregoing City Council Resolution was duly and regularly passed and adopted at a meeting by said City Council on the twenty-eighth day of February, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this __ day of ____, 2023.

Judi A. Herren, City Clerk



STAFF REPORT

City Council

Meeting Date:

2/28/2023

Staff Report Number:

23-046-CC

Consent Calendar:

Adopt a resolution to accept and appropriate a San Mateo County 2023 Summer Enrichment Grant in the total amount of \$13,000 to support and expand summer camp enrichment programs for children at the Belle Haven Youth Center

Recommendation

Staff recommends that City Council adopt a resolution (Attachment A) accepting and appropriating a San Mateo County 2023 Summer Enrichment Grant in the total amount of \$13,000 to support and expand summer camp enrichment programs for children at the Belle Haven Youth Center.

Policy Issues

City Council authorizes the acceptance of grant awards to support City of Menlo Park operations, services and projects.

Background

On February 6, 2023, San Mateo County awarded the City of Menlo Park a San Mateo County 2023 Summer Enrichment Grant in the total amount of \$13,000 to support and expand summer camp enrichment programs for 12 middle school youth at the Belle Haven Youth Center (Attachment B.)

The City of Menlo Park was awarded and accepted a similar grant from San Mateo County in 2022. The 2022 award included additional funding for Camp Menlo, however the Camp Menlo program was not selected for an award in 2023.

Analysis

The San Mateo County 2023 Summer Enrichment Grant Program's goals are:

- Expand access to out-of-school care programs for low-income youth and youth from historically marginalized or underrepresented backgrounds or communities (e.g., students with special needs, in foster care, English language learners, youth experiencing homelessness);
- Support the social-emotional well-being of youth to mitigate the setbacks and hardships associated with COVID-19; and
- Provide opportunities for youth to re-inspire and propel learning to counterbalance the loss of learning opportunities attributed to the COVID-19 pandemic and distance learning.

City staff submitted a successful application for \$13,000 from the San Mateo County 2023 Summer Enrichment grant funding to support and expand programs for 12 middle school youth in the Middle Camp summer camp program at Belle Haven Youth Center. This funding will be used to support the continued and

expanded program serving middle school youth.

The program provides community service opportunities and summer programming to rising sixth through eighth grade children. Funding will be utilized to subsidize tuition, partial costs of personnel salaries, and program supplies (i.e., apparel, materials to support service projects, and general supplies.) This program is especially important as some of the elementary school participants will be aging out of the camp programs and have limited structured options for the summer which has a potential to increase learning loss.

Impact on City Resources

There is no new impact to the City's general fund operating budget associated with this grant as general costs will be covered by the program fees and existing budgets. Should the City Council adopt the resolution in Attachment A, then the City will receive a grant award in the amount of \$13,000 toward the Middle Camp summer camp program at Belle Haven Youth Center.

Environmental Review

This action is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §15378 and §15061(b)(3) as it will not result in any direct or indirect physical change in the environment.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

- A. Resolution
- B. Preliminary grant award notification letter

Report prepared by:
Sheriann Chaw, Library and Community Services Supervisor
Nick Szegda, Assistant Library Service Director

Report reviewed by:
Sean S. Reinhart, Library and Community Services Director

RESOLUTION NO. XXXX

**RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO PARK
ACCEPTING AND APPROPRIATING A SAN MATEO COUNTY 2023 SUMMER
ENRICHMENT GRANT IN THE TOTAL AMOUNT OF \$13,000 TO SUPPORT
MIDDLE CAMP SUMMER PROGRAM.**

WHEREAS, the City of Menlo Park operates the Belle Haven Youth Center and its associated summer programs including Middle Camp; and

WHEREAS, the purpose of the summer programs operated out of the Belle Haven Youth Center, in part, is to provide a safe and healthy environment for children to learn and develop; and

WHEREAS, the City of Menlo Park has been awarded a San Mateo County 2023 Summer Enrichment Grant award in the amount of \$13,000 to support the Middle Camp Summer Program.

NOW, THEREFORE, BE IT RESOLVED, that the City Council accepts and appropriates the San Mateo County 2023 Summer Enrichment Grant award in the amount of \$13,000 to support the Middle Camp Summer Program in the calendar year 2023.

I, Judi A. Herren, City Clerk of Menlo Park, do hereby certify that the above and foregoing City Council Resolution was duly and regularly passed and adopted at a meeting by said City Council on the twenty-eighth day of February, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this ___ day of ___, 2023.

Judi A. Herren, City Clerk

From: [SMC_summerenrichment](#)
To: [Chaw, Sheriann J](#)
Subject: Award Notification: 2023 SMC Summer Enrichment Grant
Date: Monday, February 6, 2023 5:02:17 PM
Attachments: [2023 Grant Agreement-Summer Enrichment - REVIEW COPY.pdf](#)
Importance: High

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear City of Menlo Park,

Congratulations! Middle Camp (AKA Summer of Service) has been selected to receive a San Mateo County 2023 Summer Enrichment grant in the amount of \$13,000. Unfortunately, Camp Menlo was not selected for an award.

The grant is contingent on completing the following steps **by 5:00pm on Wednesday, February 15, 2023.**

1. **Review the attached Grant Agreement and provide contact information for page 4.** Once we receive the contact information, the County will finalize the grant agreement and send it back to you for signature by your authorized representative. *The attached version is for review only—please do not fill or sign the attached file.*
2. **Submit an electronic W-9** available online [Form W-9 \(Rev. October 2018\) \(irs.gov\)](#). Once we have executed the grant agreement, the funding check will be mailed via priority express mail to the address provided on the W-9 form.

If you have any questions, please email us at smc_summerenrichment@smcgov.org.

Sincerely,

San Mateo County Grant Team



STAFF REPORT

City Council
Meeting Date: 2/28/2023
Staff Report Number: 23-047-CC

Regular Business: Identify a preferred aquatics operator and authorize the city manager to negotiate an agreement for an aquatics operator at Burgess Pool and the future Menlo Park Community Campus aquatics center; and form an ad hoc City Council subcommittee to advise the agreement negotiation process

Recommendation

City staff recommends that City Council:

- Identify request for proposals (RFP) respondent, Team Sheep, Inc., as the preferred aquatics operator for Burgess Pool and the future Menlo Park Community Campus (MPCC) aquatics center
- Authorize the city manager to enter negotiations with Team Sheep, Inc., for a draft aquatics operator agreement at Burgess Pool and the future MPCC aquatics center, to take effect September 1, at terms in accordance with City Council's direction regarding desired elements of the aquatics program and as specified in the RFP. The resulting draft agreement would be subject to City Council authorization, tentatively scheduled March 28.
- Form an ad hoc subcommittee comprised of two City Councilmembers appointed by the City Council to advise and support City staff during the agreement negotiation process.

Policy Issues

City Council provides policy direction to the city manager regarding service provision to the community; provides authorization to the city manager to negotiate and execute professional services agreements with service providers; and sets prioritization for the use of City resources to serve the community.

Background

On February 8, 2022, City Council directed staff to prepare an RFP to be issued in autumn 2022 for an aquatics operator at Burgess Pool and the future MPCC aquatics center.

On August 23, 2022, City Council reviewed an aquatics program analysis and provided direction to staff regarding desired elements of the RFP.

On October 11, 2022, City Council reviewed a draft RFP and process timeline and authorized staff to issue the RFP. The RFP was issued October 24, 2022

The City received three proposals in response to the RFP by the November 30, 2022 submittal deadline.

A review panel comprised of experienced aquatics managers from East Bay municipalities that have no prior connections or preferences for any of the RFP respondents, City staff, and the Parks and Recreation

Commission chairperson independently evaluated all three proposals, then met December 15, 2022, to jointly evaluate the proposals.

On December 21, 2022, the review panel conducted follow-up interviews with two of the respondents to seek clarification to various aspects of their respective proposals, and in January 2023 conducted initial reference checks with municipalities where those respondents are currently operating aquatics programs.

On February 14, 2023, City Council reviewed the three proposals and preliminary considerations for selecting an aquatics operator (Attachment A.) Several public comments were received in support of the Team Sheeper, Inc. proposal. Some public comments were received that expressed concerns about Team Sheeper, Inc.'s current programs and schedule. City Councilmembers offered a number of questions and comments about specific desired programming and schedules, community input, city-operated aquatics programs, resident fees and non-resident fees, prioritizing residents' access to pools, and forming an ad hoc subcommittee to advise the negotiation process.

Analysis

RFP key priorities

Per City Council's feedback and direction when the RFP was authorized October 11, 2022, key priorities were incorporated into the RFP. The RFP was structured to offer some flexibility to RFP respondents to develop proposals that balance the City's key priorities with respondents' operational needs. Proposals from qualified aquatics operators that demonstrated a high degree of responsiveness to most or all these priorities in addition to the basic capabilities and competencies that are required to operate quality aquatics programs were deemed more competitive for selection:

1. Operate the Burgess Pool and the future MPCC aquatics center for public access year-round, seven days per week, no fewer than 63 hours per week at each location as calculated by average applied over the course of a full calendar year—with exceptions for closures to observe major holidays or to complete necessary maintenance or repair work.
2. Operate Burgess Pool and MPCC aquatics center with comparable or equivalent operating schedules and programs at both locations, with allowance for some variances to respond to hyperlocal needs and other unique considerations of each site and the neighborhoods in which they are located.
3. Seek City approval for new aquatics user fees or modifications to existing aquatics user fees, with the mutual understanding by City and operator that: a) user fees are the primary source of revenue necessary for operator to deliver and sustain safe, quality aquatics operations for the community; and, b) user fees can and do present barriers to entry for some residents, especially residents who are most vulnerable, and barriers can persist even with the availability of scholarship or subsidy programs.
4. Enter into a revenue sharing agreement with the City to offset a portion of the City's facility maintenance costs at Burgess Pool and the MPCC aquatics center, preferably at a "medium" cost recovery rate to the City of 30 percent to 70 percent. The City's total costs to maintain the Burgess Pool facility were approximately \$645,000 in fiscal year 2021-2022.
5. Dedicate operating hours and pool space to open swim/community swim for play and social time during times that are deemed convenient and accessible to Menlo Park resident children and families, as measured in part by community satisfaction surveys to be jointly administered by operator and the City.
6. Offer aqua wellness and/or therapeutic classes in formats and at times that are deemed convenient and accessible to Menlo Park for seniors and others who benefit from such programs, as measured in part by community satisfaction surveys to be jointly administered by operator and the City.
7. Recognize the City's desire to ensure that every Menlo Park resident child has meaningful access to effective water safety instruction at City aquatics centers regardless of their family's ability to pay user fees. Respondent proposes to provide these services to individuals or families who cannot afford the market rate fees.

8. Diversity, equity, inclusion, belonging
 - A. Provide meaningful employment opportunities in City aquatics facilities to qualified Menlo Park residents, especially residents who live in the immediate vicinity of City aquatics centers.
 - B. Foster an organizational culture that is based on foundations of equity, inclusion, belonging and justice to create a safe and welcoming environment in City aquatics facilities for all Menlo Park residents regardless of background, income, race, religion, sexual orientation, gender identity, and other lived experiences that contribute to a vibrant and accepting community.
 - C. Reduce and/or remove barriers to entry for City residents who are most vulnerable, including children and families who reside in low-income households, seniors and people with disabilities.

Proposals

Three proposals were received in response to the RFP. The proposals were provided to City Council February 14 to give City Council and the community sufficient time to review the proposals and to identify preliminary considerations for City Council feedback and direction (Attachment A.) Because no operator has been selected and no agreement has been negotiated, some sensitive information was redacted from the proposals such as contact phone numbers and confidential financial details in respect to the privacy of individuals and proprietary business information.

During the review of the proposals, the RFP review panel identified several considerations for City Council's awareness, which were previewed to City Council February 14.

- Two of the proposals (Team Sheeper, Inc., and The Swimming Swan, LLC) are from respondents that currently operate public aquatics facilities, and in the opinion of City staff and the RFP review panel, demonstrate sufficient capabilities and experience to operate the Burgess Pool and future MPCC aquatics center
- Team Sheeper, Inc. is the City's current operator that has operated Burgess Pool since 2006 and previously operated Belle Haven Pool from 2011 until 2021 when the Belle Haven Pool was demolished to make way for construction of a new aquatics center as part of the MPCC project
- The Swimming Swan, LLC is a qualified operator that is based in southern California and recently expanded its operations to Santa Clara County and Las Vegas, Nevada
- SOLO Aquatics is from a local aquatics program that currently operates as a program subcontractor to the current operator of Burgess Pool
- Based on the key priorities and requirements that City Council established in the RFP, some changes to the current operations and programs are expected and desired, however, minimizing disruption and preserving the continuity of popular services and programs to the extent possible also is important
- Remaining with the current qualified operator (Team Sheeper, Inc.) would probably result in relatively less disruption to current programs and operations from the perspective of current pool users, and negotiating a new agreement with Team Sheeper, Inc. would provide the opportunity for the City to implement some desired changes to the aquatics program
- Selecting a new qualified operator (e.g., The Swimming Swan, LLC) would probably result in relatively more disruption to current programs and operations from the perspective of current pool users, particularly in regard to the unique challenges and opportunities of transitioning from one operator to another operator.

Based on the information received during the RFP process, City staff is confident and prepared to work with either qualified aquatics operator and/or execute any direction City Council may provide regarding the desired future operations of the City's aquatics program.

RFP review panel evaluation

Proposals were evaluated by the review panel for the necessary qualifications, background, and experience operating municipal aquatics centers, and according to the selection criteria outlined in the RFP. For reference, the proposals and RFP are included with this report in Attachment A.

The review panel found that Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals both demonstrated the necessary qualifications, background and experience operating municipal aquatics centers, and found both proposals to be sufficiently responsive to the RFP selection criteria to be deemed competitive for selection. The proposal from SOLO Aquatics demonstrated experience running aquatics programs like swim teams, however it did not demonstrate experience operating aquatics centers of the scale and complexity of Burgess Pool and the future MPCC aquatics center, and was significantly less comprehensive overall, and was not deemed competitive for selection.

What follows is a summary of the RFP review panel's evaluation of the proposals that were deemed competitive, from Team Sheeper, Inc. and The Swimming Swan, LLC.

- Basic qualifications, experience and organizational capacity. The review panel found that both Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals demonstrated sufficient qualifications and experience to effectively operate the Burgess Pool and future MPCC aquatics center. Team Sheeper, Inc., is the current operator of Burgess Pool and operated the former Belle Haven Pool, demonstrating experience operating aquatics centers in the local area and specifically Menlo Park. The Swimming Swan, LLC, is a qualified operator that is based in southern California and recently expanded its operations to Santa Clara County and Las Vegas, Nevada. The review panel also found that both Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals demonstrated sufficient staffing and management capacity, staff training and development, and related organizational and administrative systems necessary to effectively operate the Burgess Pool and future MPCC aquatics center.
- Quality assurance and risk management. The review panel found that both Team Sheeper, Inc.'s and The Swimming Swan's proposals demonstrated the necessary qualifications and capacity to provide sufficient quality assurance and risk management for operating the Burgess Pool and future MPCC aquatics center. Team Sheeper, Inc.'s proposal noted its existing relationship and experience with the San Mateo County Environmental Health agency, which oversees quality assurance and conducts inspections at aquatics facilities in the county, including Burgess Pool and the future MPCC aquatics center. The Swimming Swan, LLC's proposal provided general assurances, backed by its experience operating aquatics centers in other communities that it follows all local, state, and federal regulations and is in compliance with health department guidelines.
- Schedule. The review panel found that both Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals demonstrated responsiveness to the RFP priority of operating both pools year-round, seven days per week, no fewer than 63 hours per week at each location. Team Sheeper, Inc.'s proposal included a greater number of operating hours than the other proposals, and proposed to open at 6 a.m. most days of the year. The Swimming Swan, LLC's proposal included hours above the minimum required, however its proposal was to open the Burgess Pool later in the morning most days of the year, at 7 a.m. or 10 a.m.
- Programs. The review panel found that both Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals demonstrated responsiveness to the RFP priority to operate Burgess Pool and MPCC aquatics center with comparable or equivalent operating schedules and programs. The review panel noted that both proposals included a variety of aquatics programs that overall meet these criteria, and that the details of specific programs likely could be refined or revised during a subsequent agreement negotiation process with the selected operator. One distinction noted by the review panel is that Team Sheeper,

Inc.'s proposal included sample programs for both pools, whereas The Swimming Swan, LLC's proposal included a sample program for the Burgess Pool and indicated that the MPCC aquatics program would follow a similar model. The review panel found that both Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals demonstrated some responsiveness to the requirement to dedicate operating hours and pool space to open swim for play and social time for Menlo Park resident children and families. The review panel also found that both Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals demonstrated responsiveness to the RFP priority to offer aqua wellness and/or aquafit classes in formats and at times that are convenient and accessible. The review panel additionally noted that The Swimming Swan, LLC, appears to offer more robust programming in this area and employs its own in-house dedicated staff for aqua wellness programs, whereas Team Sheeper, Inc., proposed to offer such programs through a qualified subcontractor.

- Provide meaningful access to effective water safety instruction for every Menlo Park child. The review panel found that both Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals demonstrated responsiveness to the minimum requirements of this priority. Both proposals address this priority through third party nonprofits that offer scholarships to cover user fees for eligible children.
- Public communication, marketing and registration. The review panel found that both Team Sheeper, Inc.'s and Swimming Swan, LLC's proposals demonstrated sufficient public communication, marketing and registration capabilities to effectively promote and inform the public about aquatics programs in Menlo Park. One distinction noted by the review panel regarding registration capabilities is that Team Sheeper, Inc.'s proposal indicated that it uses the Xplor Recreation platform (formerly known as PerfectMind), which the City independently selected in 2021-22 for the City's new recreation registration platform, whereas The Swimming Swan, LLC's proposal indicated that it uses an unspecified "secure and encrypted," "custom proprietary software built on a multi-site platform."
- Fees and revenue sharing. The review panel found that both Team Sheeper, Inc.'s and Swimming Swan, LLC's proposals demonstrated some responsiveness to these criteria. The review panel found that both proposals included similar fee structures, and both expressed interest and willingness to work with the City before implementing new aquatics user fees or modifying existing aquatics user fees. Team Sheeper, Inc.'s proposal includes a specific revenue sharing plan in tiers, between 1 percent to 2.5 percent of revenues depending on the revenue tier; whereas The Swimming Swan, LLC's proposal appears to indicate some willingness to explore a revenue sharing arrangement of 30 percent to the City above an unspecified minimum revenue threshold but offers no specific details.
- Financial Information. The review panel found that both Team Sheeper, Inc.'s and The Swimming Swan, LLC's proposals demonstrated sufficient responsiveness and administrative capabilities to meet the requirements of this criteria. Team Sheeper, Inc., included summary unaudited financial statements, and proposed operating budgets for both Burgess Pool and the future MPCC aquatics center. The Swimming Swan, LLC, included detailed profit and loss statements, and included summary revenue, pricing and attendance projections but no specific operating budget proposal for either aquatic center.

Overview - The Swimming Swan, LLC

Based on the evaluations provided by the RFP review panel, City staff finds that the proposal from The Swimming Swan, LLC, demonstrates sufficient capabilities and experience to effectively operate the Burgess Pool and future MPCC aquatics center, and is competitive for selection. Major factors in this finding include that The Swimming Swan, LLC, currently operates aquatics facilities in southern California, Santa Clara County and Las Vegas, Nevada; has proven experience operating aquatics programs in multiple locations and communities; and has experience navigating the transitions into new communities as incoming aquatics operator. However, a transition to a new aquatics operator would inevitably result in

some disruption to current pool users, and would create some economic uncertainty for the aquatics program depending on how well the operator transition is received by the community and how well the transition is managed by the new aquatics operator with the support of City Council and staff. Had the review panel's evaluation found that the proposal from The Swimming Swan, LLC, had achieved a significantly stronger level of competitiveness than Team Sheeper, Inc.'s proposal, then City staff would have been comfortable recommending The Swimming Swan, LLC, for selection despite the above-noted risks. However, the review panel did not find The Swimming Swan, LLC's proposal to be significantly more competitive than Team Sheeper, Inc.'s proposal, and therefore City staff does not recommend The Swimming Swan, LLC, for selection at this time. That said, if City Council ultimately selects The Swimming Swan, LLC, as the preferred operator, City staff is confident in the City's ability to work with The Swimming Swan, LLC, and help manage a successful transition to a new aquatics operator.

Overview – Team Sheeper, Inc.

City staff recommends that City Council identify Team Sheeper, Inc., as the preferred aquatics operator. Major determining factors in this recommendation include that Team Sheeper, Inc., is headquartered in Menlo Park; possesses deep experience providing aquatics programs to the Menlo Park community; has proven capacity to simultaneously operate two City-owned aquatics centers in Menlo Park; and has developed a large and loyal clientele of local aquatics users which is essential to sustain an aquatics center. In its proposal, Team Sheeper, Inc. provided clear, specific responses to the key City priorities and requirements outlined in the RFP, and was the only proposal that provided detailed and well-considered operating budgets for both Burgess Pool and the future MPCC aquatics center. Negotiating a new agreement with Team Sheeper, Inc. would provide the City the opportunity to implement some desired changes to the aquatics program, while minimizing disruption to current pool users.

Overview – City-operated aquatics

At City Council's request, City staff prepared a rough-order-of-magnitude (ROM) estimate of the cost, staffing, and other resources that would be necessary for the City to directly operate Burgess Pool and the future MPCC aquatics center. The ROM estimate was first provided to City Council August 23, 2022, and is reiterated in the Impact on City Resources section of this report. Based on that preliminary analysis, and after having received and evaluated proposals from qualified aquatics operators in response to the RFP, City staff does not recommend pursuing a City-operated aquatics program at this time.

Requirements and agreement terms

Per City Council's feedback and direction, various terms were incorporated into the RFP and staff will advise the selected operator that these terms should be included in the operator agreement. A partial list of these terms includes:

1. Provider will prepare and submit a detailed annual performance report to City staff no later than January 30 of each year, including but not limited to the following information:
 - A. Total program hours by program area
 - B. Participation statistics by program area including resident and non-resident percentages
 - C. Community satisfaction survey results
 - D. User group feedback by program area
 - E. Pool schedule and allocation by program for previous year and projections to the upcoming year
 - F. Fees by program area and a fee comparison to other public pools in the region
 - G. Annual audits and reviews demonstrating standards of care are met
 - H. Risk management documentation
 - I. Training certifications listed by staff member.
2. Provider will coordinate with City staff to present operator's annual performance report to City Council no later than March 31 of each year.
3. Provider will submit to City staff quarterly financial statements no later than 45 days after the end of

each fiscal quarter, and annual financial statements no later than 45 days after the close of the fiscal year. Financial statements submitted to the City shall be considered public records subject to applicable government transparency law.

4. Provider will consult with City staff before altering the operating hours, launching new aquatics programs, making significant changes to existing programs, or significantly altering any other substantive aspects of Provider's operations and programs at City aquatics centers.
5. Provider will seek City approval before engaging the services of subcontractors or other parties not directly employed by Provider to deliver aquatics programs and/or services in City aquatics facilities.
6. Provider will assign sufficient qualified staffing to maintain safe and effective operations at City aquatics facilities. Provider's staff will have the required qualifications and certifications for each position.
7. Provider will maintain appropriate and safe ratios of lifeguards to pool users at all times, consistent with industry best practices and applicable regulations.
8. Provider will maintain reasonable evidence and documentation of its hiring practices, background checks, certifications and training, including documentation of pre-service/employment orientation, on-the-job training, regular in-service training, and certification training for each employee. Provider will furnish these records to the City for inspection on request.
9. Provider will demonstrate adequate risk management planning and practices by conducting annual audits and reviews by qualified external experts in coordination and with oversight by City staff.

Ad hoc subcommittee – aquatics operator agreement

City staff recommends that the City Council appoint an ad hoc subcommittee comprised of two City Councilmembers to advise and support City staff during the agreement negotiation process. The ad hoc subcommittee members would be tasked with working with City staff to review the agreement terms and requirements with City staff, provide detailed feedback and suggestions to City staff during the negotiation process, and aid the development of the draft agreement's terms and scope of work. City staff anticipates that the ad hoc subcommittee would meet on 2 or 3 occasions beginning March 1, in addition to reviewing the draft terms and scope of work, and could provide a written subcommittee report to the City Council before City Council authorization of the draft agreement tentatively March 28.

Impact on City Resources

There is no new direct impact to the General Fund associated with the recommended action. Potential impacts to the General Fund will be associated with the authorization of a new operator agreement by City Council tentatively March 28. The City is responsible for maintenance of the Burgess Pool facility including recurring expenditures for mechanical maintenance and repairs, custodial and landscaping services, utilities (water, sewer, electricity, gas, telephone and internet), and pool maintenance chemicals and supplies; and one-time expenditures for equipment replacements and minor facility renovations. Total expenses to the City related to maintenance of Burgess Pool were approximately \$645,000 in fiscal year 2021-22. City Council's adopted fiscal year 2022-23 operating budget has sufficient resources allocated toward these anticipated expenditures in the current fiscal year. The City also will be responsible for maintenance of the MPCC aquatics center when it is completed; the new center is tentatively scheduled to open in early 2024. Budgetary considerations related to maintaining that facility will be addressed during City Council's fiscal year 2023-24 budget deliberations in the spring of 2023. The aquatics operator RFP identifies revenue sharing as a key priority to help offset a portion of the City's cost of maintaining its aquatics facilities at a preferred "medium" cost recovery rate to the City of 30 percent to 70 percent.

Rough order of magnitude (ROM) estimate – City-operated aquatics

City staff does not recommend pursuing a City-operated aquatics program at this time. Should City Council direct staff to directly operate Burgess Pool and/or the future MPCC aquatics center, it is within the City's capabilities to do so, given the necessary time, staff and resources. The City of Menlo Park has not directly

operated Burgess Pool since 2006 nor the former Belle Haven Pool since 2011, and currently lacks the necessary staff and resources to operate an aquatics program. Major, ongoing appropriations by City Council of limited City resources would be necessary for the City to operate the aquatics program, during a time of significant economic uncertainty.

Several preparatory steps would be needed before the City assumed direct operations of Burgess Pool and/or the new MPCC pool, including substantial budget authorizations, adjustments to the salary schedule for new classifications, personnel recruitments, and regulatory certifications in a process that would extend several months and involve hundreds of hours of staff time and effort. Staff's preliminary review of other area jurisdictions' staffing models and expenditures, along with analysis of Menlo Park's current salary schedule, job classifications, facility maintenance and capital expenditures, indicates that in order to directly operate Burgess Pool and the MPCC pool seven days per week, year-round, the City of Menlo Park would need to:

- Increase benefited personnel by 4.0 to 7.0 full time equivalent (FTE), including a full-time aquatics supervisor, two full-time aquatics coordinators, two to four office and program assistants, and between 90-150 part-time, temporary non-benefited employees such as lifeguards, instructors and attendants, depending on season – approximately \$0.75 million to \$1.0 million per year for benefited personnel, plus approximately \$1.0 million to \$1.7 million per year for non-benefitted personnel.
- Continue to bear the costs to operate and maintain the Burgess Pool and MPCC Pool facilities, including the costs of utilities, equipment replacement and repair, minor renovation projects, and supplies like pool chemicals – approximately \$0.65 million to \$1.0 million per year.
- Bear new administrative costs related to pool operator certifications, risk management, records management and financial administration – approximately \$0.10 million per year.

Based on this preliminary analysis, a ROM estimate for the City to directly operate Burgess Pool and MPCC pool year-round, seven days per week is total annual expenditures between \$2.5 million to \$3.8 million per year; with potential annual revenue (cost recovery) between \$0.75 million to \$2 million per year. These estimated ranges of expenditures and revenues track with each other, that is, the lower end of the expenditure range would result in the lower end of the potential revenues, and the higher end of the expenditure range would result in the higher end of the potential revenues.

Should City Council so direct, staff can develop a budget proposal for a City-run aquatics program including specific staff positions and compensation, hours of operation, programs, certifications, supplies and services, facility maintenance and capital expense, revenue projections, and other financial and operating considerations, and present it to City Council in April. City staff estimates that developing a detailed budget proposal would require approximately 80 total hours of staff time and effort, which would significantly divert limited resources away from other priority projects including the development of the MPCC facility's programming, staffing, and budgetary proposals tentatively set for City Council consideration during the fiscal year 2023-24 budget process in spring 2023.

Environmental Review

The requested action is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §§ 15378 and 15061(b)(3) as it will not result in any direct or indirect physical change in the environment.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72

Staff Report #: 23-047-CC

hours prior to the meeting.

Attachments

- A. Hyperlink – February 14, City Council Staff Report #23-036-CC:
menlopark.gov/files/sharedassets/public/agendas-and-minutes/city-council/2023-meetings/agendas/20230214-city-council-regular-agenda-packet.pdf#page=187

Report prepared by:

Tricia Mullan, Library and Community Services Supervisor
Sean S. Reinhart, Library and Community Services Director



SELECT A PREFERRED AQUATICS OPERATOR; AUTHORIZE NEGOTIATION; AD HOC SUBCOMMITTEE

City Council – February 28, 2023



Recommendation

- Identify Team Sheeper, Inc., as the preferred aquatics operator for Burgess Pool and the future Menlo Park Community Campus (MPCC) aquatics center
- Authorize the city manager to enter negotiations with Team Sheeper, Inc., for a draft aquatics operator agreement. The resulting draft agreement would be subject to City Council authorization, tentatively scheduled March 28
- Form an ad hoc subcommittee comprised of two City Councilmembers appointed by the City Council to advise and support City staff during the agreement negotiation process.



Timeline*

- February 14 – Proposals released for City Council and community review and preliminary City Council discussion and feedback
- February 28 – City Council recommend preferred operator; authorize the city manager to negotiate agreement; form ad hoc subcommittee
- March 1 – City enter negotiations with the preferred operator identified by City Council, with support from ad hoc subcommittee
- March 28 – City Council review and approve the aquatics operator agreement; agreement executed shortly thereafter
- September 1 – Agreement takes effect.

* All dates are tentative and subject to change





Proposals

- Team Sheepar, Inc. proposal is from the City's current operator that has operated Burgess Pool since 2006 and previously operated Belle Haven Pool from 2011 until 2021 when the Belle Haven Pool was demolished to make way for construction of a new aquatics center as part of the MPCC project
- Swimming Swan, LLC proposal is from a qualified operator that is based in southern California and recently expanded its operations to Santa Clara County and Las Vegas, Nevada
- SOLO Aquatics proposal is from a local aquatics program that operates as a program subcontractor to the current operator of Burgess Pool.



RFP review panel evaluation

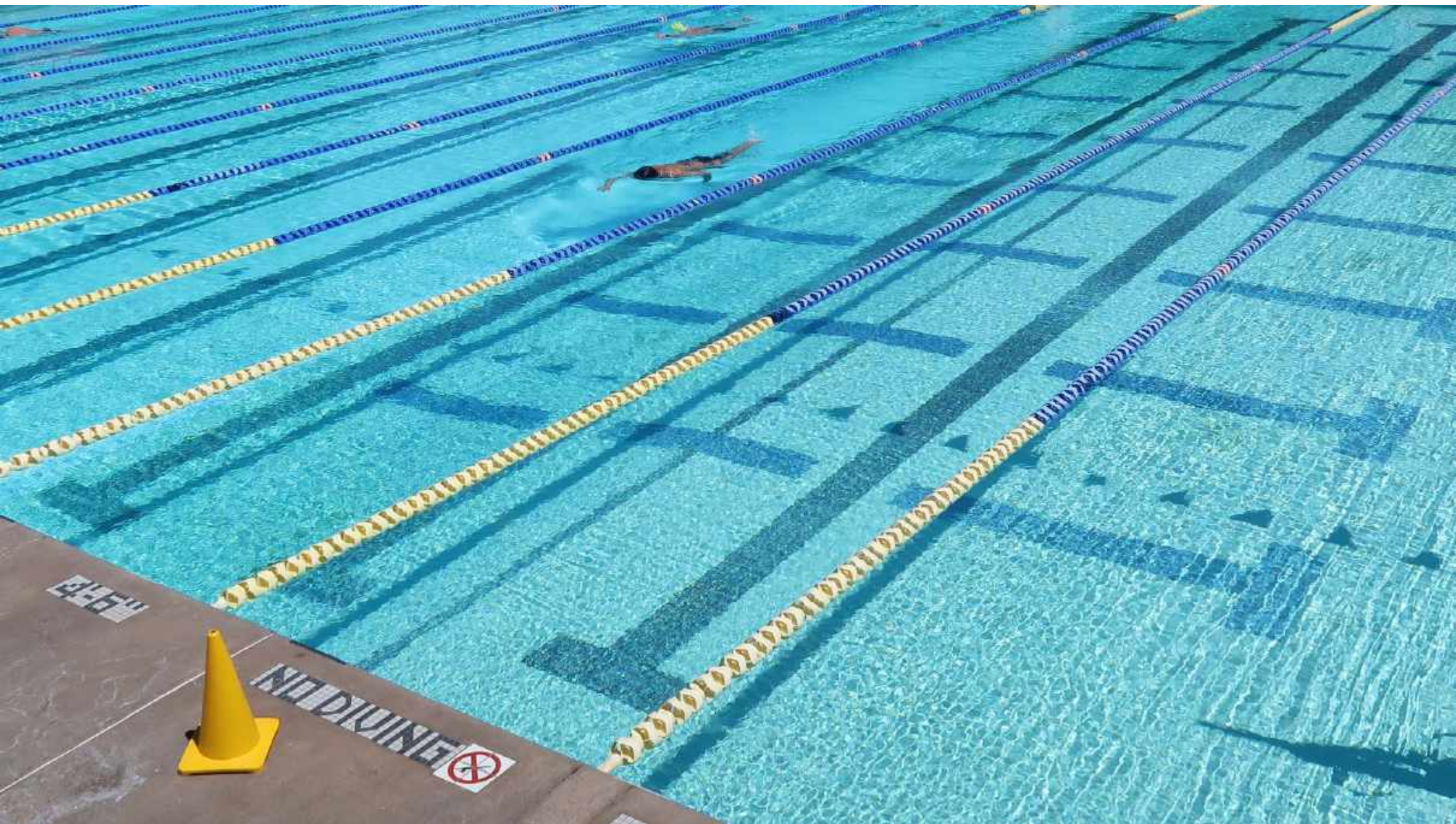
- Proposals were evaluated by the review panel for the necessary qualifications and experience operating municipal aquatics centers, and according to the selection criteria outlined in the RFP
- The review panel found that Team Sheepar, Inc.'s and The Swimming Swan, LLC's proposals both demonstrated the necessary qualifications, background and experience operating municipal aquatics centers, and found both proposals to be deemed competitive for selection
- The proposal from SOLO Aquatics demonstrated experience running aquatics programs like swim teams, however it did not demonstrate experience operating aquatics centers, and was significantly less comprehensive overall, and was not deemed competitive for selection.



RFP review panel evaluation

Summary evaluations of the two competitive proposals based on key RFP criteria are in the staff report:

- Basic qualifications, experience and organizational capacity
- Quality assurance and risk management
- Schedule and programs
- Meaningful access to water safety instruction for children
- Public communication, marketing, registration
- Fees and revenue sharing
- Financial information





Overview – The Swimming Swan, LLC

- Proposal demonstrates sufficient capabilities and experience to effectively operate the Burgess Pool and future MPCC aquatics center, and is competitive for selection
- Proven experience operating aquatics programs in multiple locations and communities, and navigating transitions into new communities
- Transition to new operator in Menlo Park would cause some disruption to current pool users and would create some economic uncertainty
- If City Council ultimately selects The Swimming Swan, LLC, as the preferred operator, City staff is confident in the City's ability to work with The Swimming Swan, LLC, and help manage a successful transition.



Overview – Team Sheeper, Inc.

- City staff recommends that City Council identify Team Sheeper, Inc., as the preferred aquatics operator
- Headquartered in Menlo Park; experience providing aquatics programs to the Menlo Park community; has proven capacity to simultaneously operate two City-owned aquatics centers in Menlo Park; and has developed a large and loyal clientele of local aquatics users which is essential to sustain an aquatics center
- Negotiating a new agreement with Team Sheeper, Inc. would provide the City the opportunity to implement some desired changes to the aquatics program, while minimizing disruption to current pool users.



Overview – City-operated aquatics

- At City Council’s request, City staff prepared a rough-order-of-magnitude (ROM) estimate of the cost, staffing, and other resources that would be necessary for the City to directly operate Burgess Pool and the future MPCC aquatics center
- ROM estimate was first provided to City Council August 23, 2022, and is reiterated in the Impact on City Resources section of the staff report
- Based on that preliminary analysis, and after having received and evaluated proposals from qualified aquatics operators in response to the RFP, City staff does not recommend pursuing a City-operated aquatics program at this time.



Ad hoc subcommittee

- City staff recommends that the City Council appoint an ad hoc subcommittee comprised of two City Councilmembers to advise and support City staff during the agreement negotiation process
- Would work with City staff to review the agreement terms and requirements with City staff, provide detailed feedback and suggestions to City staff during the negotiation process, and aid the development of the draft agreement's terms and scope of work
- City staff anticipates that the ad hoc subcommittee would meet on 2 or 3 occasions beginning March 1, in addition to reviewing the draft terms and scope of work, before City Council authorization of the draft agreement tentatively March 28.





Recommended actions and next steps

- Identify Team Sheeper, Inc., as the preferred aquatics operator for Burgess Pool and the future Menlo Park Community Campus (MPCC) aquatics center
- Authorize the city manager to enter negotiations with Team Sheeper, Inc., for a draft aquatics operator agreement. The resulting draft agreement would be subject to City Council authorization, tentatively scheduled March 28
- Form an ad hoc subcommittee comprised of two City Councilmembers appointed by the City Council to advise and support City staff during the agreement negotiation process.



STAFF REPORT

City Council
Meeting Date: 2/28/2023
Staff Report Number: 23-045-CC

Regular Business: **Appoint a City Council liaison to the Finance and Audit Committee**

Recommendation

Staff recommends that the City Council appoint a City Council liaison to the Finance and Audit Committee.

Policy Issues

According to CC-23-004 (Attachment A), there shall be seven members on each commission/committee and City Councilmembers are assigned to serve in a liaison capacity with one or more City commission/committee.

Background

On January 10, the City Council made annual appointments to represent the City or liaison for City Council (Attachment B.) During that deliberation, the City Council directed staff to return the composition of the Finance and Audit Committee (FAC) membership for City Council consideration. At that meeting, the City Council also approved the regular FAC meeting schedule as the third Thursday of every quarter at 5:30 p.m.

The FAC was established in early 2008 consisting of two City Councilmembers and three residents. In April 2019, the City Council approved the FAC membership expansion from five to seven members with the addition of two residents. This action created a FAC consisting of two City Councilmembers and five residents (Attachments C and D.)

On February 14, the City Council directed staff to recruit to fill the FAC with seven resident members and remove the two City Councilmembers as voting members (Attachment E.) No update to CC-23-004 is required because it does not specify that City Councilmembers must serve as members on the FAC.

Analysis

Staff is recommending the City Council appoint one City Councilmember to serve as a liaison to the FAC. The purpose of the liaison assignment is to facilitate communication between the City Council and the advisory body. The liaison also helps to increase the City Council's familiarity with the membership, programs and issues of the advisory body. In fulfilling their liaison assignment, City Councilmembers may elect to attend commission/committee meetings periodically to observe the activities of the advisory body or simply maintain communication with the commission/committee chair on a regular basis.

Currently, the City's annual advisory body recruitment is underway through April 7 at 5 p.m. to fill expiring terms. If at the end of annual recruitment the City Council is unable to fill seven FAC resident member seats, staff will return to the City Council recommending that the FAC membership be reduced to five resident members.

Impact on City Resources

There is no impact on City resources.

Environmental Review

This action is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §§ 15378 and 15061(b)(3) as it is a minor change that will not result in any direct or indirect physical change in the environment.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

- A. CC-23-004
- B. 2023 City Council assignments to regional boards and advisory bodies
- C. Hyperlink – June 4, 2019, City Council staff report: menlopark.org/DocumentCenter/View/21743/H3-20190604-FAC-update-and-appoint-CC
- D. Hyperlink – June 4, 2019, City Council minutes: menlopark.org/AgendaCenter/ViewFile/Minutes/_06042019-3287
- E. Hyperlink – February 14, 2023, City Council staff report: menlopark.gov/files/sharedassets/public/agendas-and-minutes/city-council/2023-meetings/agendas/20230214-city-council-regular-agenda-packet.pdf#page=526

Report prepared by:

Judi A. Herren, Assistant to the City Manager/City Clerk

COMMISSIONS/COMMITTEES POLICIES AND PROCEDURES, ROLES AND RESPONSIBILITIES

City Council Policy #CC-23-004

Adopted January 10, 2023

Resolution No. 6803



<p>Purpose</p> <p>To define policies and procedures and roles and responsibilities for Menlo Park appointed commissions and committees.</p>
<p>Authority</p> <p>Upon its original adoption, this policy replaced the document known as “Organization of Advisory Commissions of the City of Menlo Park.”</p>
<p>Background</p> <p>The City of Menlo Park currently has seven active Commissions and Committees. The active advisory bodies are: Complete Streets Commission, Environmental Quality Commission, Finance and Audit Committee, Housing Commission, Library Commission, Parks and Recreation Commission, and Planning Commission. Those not specified in the City Code are established by City Council ordinance or resolution. Most of these advisory bodies are established in accordance with Resolution No. 2801 and its amendments. Within specific areas of responsibility, each advisory body has a primary role of advising the City Council on policy matters or reviewing specific issues and carrying out assignments as directed by the City Council or prescribed by law.</p> <p>Six of the seven commissions and committees listed above are advisory in nature. The Planning Commission is both advisory and regulatory and organized according to the City Code (Ch. 2.12) and State statute (Government Code 65100 et seq., 65300-65401).</p> <p>The City has an adopted Anti-Harassment and Non-Discrimination Policy (CC-21-0022), and a Travel, Meal, and Lodging Policy (CC-19-002), which are also applicable to all advisory bodies.</p>
<p>Policies and Procedures</p> <p><u>Relationship to City Council, staff and media</u></p> <ul style="list-style-type: none"> • Upon referral by the City Council, the commission/committee shall study referred matters and return their recommendations and advise to the City Council. With each such referral, the City Council may authorize the City staff to provide certain designated services to aid in the study. • Upon its own initiative, the commission/committee shall identify and raise issues to the City Council’s attention and from time to time explore pertinent matters and make recommendations to the City Council. • At a request of a member of the public, the commission/committee may consider appeals from City actions or inactions in pertinent areas and, if deemed appropriate, report and make recommendations to the City Council. • Each commission/committee is required to develop an annual work plan which will be the foundation for the work performed by the advisory body in support of City Council annual work plan. The plan, once finalized by a majority of the commission/committee, will be formally presented to the City Council for direction and approval no later than September 30 of each year and then reported out on by a representative of the advisory body at a regularly scheduled City Council meeting at least annually, but recommended twice a year. The proposed work plan must align with the City Council’s adopted work plan. When modified, the work plan must be taken to the City Council for approval. The Planning Commission is exempt from this requirement as its functions are governed by the Menlo Park municipal code (Chapter 2.12) and State law (Government Code 65100 et seq, 65300-65401). • Commissions and committees shall not become involved in the administrative or operational matters of City departments. Members may not direct staff to initiate major programs, conduct large studies or establish department policy. City staff assigned to furnish staff services shall be available to provide general staff assistance, such as preparation of agenda/notice materials and minutes, general review of department programs and activities, and to perform limited studies, program reviews, and other services of a general staff nature. Commissions/Committees may not establish department work programs or determine department program priorities. The responsibility for setting policy and allocating scarce City resources rests with the City’s duly elected representatives, the City Council. • Additional or other staff support may be provided upon a formal request to the City Council. • The staff liaison shall act as the commission/committee’s lead representative to the media concerning matters before the commission/committee. Commission/Committee members should refer all media inquiries to their respective liaisons for response. Personal opinions and comments may be expressed so long as the commission/committee member clarifies that their statements do not represent the position of the City Council. • Commission/Committee members will have mandatory training every two years regarding the Brown Act and

COMMISSIONS/COMMITTEES POLICIES AND PROCEDURES, ROLES AND RESPONSIBILITIES

City Council Policy #CC-23-004

Adopted January 10, 2023

Resolution No. 6803

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parliamentary procedures, anti-harassment training, ethics training, and other training required by the City Council or State Law. The commission/committee members may have the opportunity for additional training, such as training for chair and vice chair. Failure to comply with the mandatory training will be reported to the City Council and may result in replacement of the member by the City Council.

- Requests from commission/committee member(s) determined by the staff liaison to take one hour or more of staff time to complete, must be directed by the City Council.

Role of City Council commission/committee liaison

City Councilmembers are assigned to serve in a liaison capacity with one or more city commission/committee. The purpose of the liaison assignment is to facilitate communication between the City Council and the advisory body. The liaison also helps to increase the City Council's familiarity with the membership, programs and issues of the advisory body. In fulfilling their liaison assignment, City Councilmembers may elect to attend commission/committee meetings periodically to observe the activities of the advisory body or simply maintain communication with the commission/committee chair on a regular basis.

City Councilmembers should be sensitive to the fact that they are not participating members of the commission/committee, but are there rather to create a linkage between the City Council and commission/committee. In interacting with commissions/committee, City Councilmembers are to reflect the views of the City Council as a body. Being a commission/committee liaison bestows no special right with respect to commission/committee business.

Typically, assignments to commission/committee liaison positions are made at the beginning of a City Council term in December. The Mayor will ask City Councilmembers which liaison assignments they desire and will submit recommendations to the full City Council regarding the various committees, boards, and commissions which City Councilmembers will represent as a liaison. In the rare instance where more than one City Councilmember wishes to be the appointed liaison to a particular commission, a vote of the City Council will be taken to confirm appointments.

City Staff Liaison

The City has designated staff to act as a liaison between the commission/committee and the City Council. The City shall provide staff services to the commission/committee which will include:

- Developing a rapport with the Chair and commission/committee members
- Providing a schedule of meetings to the city clerk's office and commission/committee members, arranging meeting locations, maintaining the minutes and other public records of the meeting, and preparing and distributing appropriate information related to the meeting agenda.
- Advising the commission/committee on directions and priorities of the City Council.
- Informing the commission/committee of events, activities, policies, programs, etc. occurring within the scope of the commission/committee's function.
- Ensuring the city clerk is informed of all vacancies, expired terms, changes in offices, or any other changes to the commission/committee.
- Providing information to the appropriate appointed official including reports, actions, and recommendations of the committee/commission and notifying them of noncompliance by the commission/committee or chair with City policies.
- Ensuring that agenda items approved by the commission/committee are brought forth in a timely manner taking into consideration staff capacity, City Council priorities, the commission/committee work plan, and other practical matters such as the expense to conduct research or prepare studies, provided appropriate public notification, and otherwise properly prepare the item for commission/committee consideration.
- Take action minutes; upon agreement of the commission, this task may be performed by one of the members (staff is still responsible for the accuracy and formatting of the minutes)
- Maintain a minute book with signed minutes

Recommendations, requests and reports

As needed, near the beginning of City Council meetings, there will be an item called "Advisory Body Reports." At this time, commissions/committees may present recommendations or status reports and may request direction and support from the City Council. Such requests shall be communicated to the staff liaison in advance, including any written materials, so that they may be listed on the agenda and distributed with the agenda packet. The materials being

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provided to the City Council must be approved by a majority of the commission/committee at a commission/committee meeting before submittal to the City Council. The City Council will receive such reports and recommendations and, after suitable study and discussion, respond or give direction.

City Council referrals

The city clerk shall transmit to the designated staff liaison all referrals and requests from the City Council for advice and recommendations. The commissions/committees shall expeditiously consider and act on all referrals and requests made by the City Council and shall submit reports and recommendations to the City Council on these assignments.

Public appearance of commission/committee members

When a commission/committee member appears in a non-official, non-representative capacity before the public, for example, at a City Council meeting, the member shall indicate that they are speaking only as an individual. This also applies when interacting with the media and on social media. If the commission/committee member appears as the representative of an applicant or a member of the public, the Political Reform Act may govern this appearance. In addition, in certain circumstances, due process considerations might apply to make a commission/committee member's appearance inappropriate. Conversely, when a member who is present at a City Council meeting is asked to address the City Council on a matter, the member should represent the viewpoint of the particular commission/committee as a whole (not a personal opinion).

Disbanding of advisory body

Upon recommendation by the Chair or appropriate staff, any standing or special advisory body, established by the City Council and whose members were appointed by the City Council, may be declared disbanded due to lack of business, by majority vote of the City Council.

Meetings and officers

1. *Agendas/notices/minutes*

- All meetings shall be open and public and shall conduct business through published agendas, public notices and minutes and follow all of the Brown Act provisions governing public meetings. Special, canceled and adjourned meetings may be called when needed, subject to the Brown Act provisions.
- Support staff for each commission/committee shall be responsible for properly noticing and posting all regular, special, canceled and adjourned meetings. Copies of all meeting agendas, notices and minutes shall be provided to the City Council, city manager, city attorney, city clerk and other appropriate staff, as requested.
- Original agendas and minutes shall be filed and maintained by support staff in accordance with the City's adopted records retention schedule.
- The official record of the commissions/committees will be preserved by preparation of action minutes.

2. *Conduct and parliamentary procedures*

- Unless otherwise specified by State law or City regulations, conduct of all meetings shall generally follow Robert's Rules of Order.
- A majority of commission/committee members shall constitute a quorum and a quorum must be seated before official action is taken.
- The chair of each commission/committee shall preside at all meetings and the vice chair shall assume the duties of the chair when the chair is absent.
- The role of the commission/committee chair (according to Roberts Rules of Order): To open the session at the time at which the assembly is to meet, by taking the chair and calling the members to order; to announce the business before the assembly in the order in which it is to be acted upon; to recognize members entitled to the floor; to state and put to vote all questions which are regularly moved, or necessarily arise in the course of the proceedings, and to announce the result of the vote; to protect the assembly from annoyance from evidently frivolous or dilatory motions by refusing to recognize them; to assist in the expediting of business in every compatible with the rights of the members, as by allowing brief remarks when undebatable motions are pending, if they think it advisable; to restrain the members when engaged in debate, within the rules of order, to enforce on all occasions the observance of order and decorum among the members, deciding all questions of order (subject to an appeal to the assembly by any two members) unless when in doubt he prefers to submit the question for the decision of the assembly; to inform the assembly when necessary, or when referred to for the purpose, on a point of order to practice pertinent to pending business; to authenticate by their signature, when necessary, all the acts, orders, and proceedings of the assembly declaring it will and in all things obeying its commands.

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3. *Lack of a quorum*

- When a lack of a quorum exists at the start time of a meeting, those present will wait 15 minutes for additional members to arrive. If after 15 minutes a quorum is still not present, the meeting will be adjourned by the staff liaison due to lack of a quorum. Once the meeting is adjourned it cannot be reconvened.
- The public is not allowed to address those commissioners present during the 15 minutes the commission/committee is waiting for additional members to arrive.
- Staff can make announcements to the members during this time but must follow up with an email to all members of the body conveying the same information.
- All other items shall not be discussed with the members present as it is best to make the report when there is a quorum present.

4. *Meeting locations and dates*

- Meetings shall be held in designated City facilities, as noticed.
- All commissions/committees with the exception of the Planning Commission, and Finance and Audit Committee shall conduct regular meetings once a month. Special meetings may also be scheduled as required by the commission/committee. The Planning Commission shall hold regular meetings twice a month and the Finance and Audit Committee shall hold quarterly meetings.
- Monthly regular meetings shall have a fixed date and time established by the commission/committee. Changes to the established regular dates and times are subject to the approval of the City Council. An exception to this rule would include any changes necessitated to fill a temporary need in order for the commission/committee to conduct its meeting in a most efficient and effective way as long as proper and adequate notification is provided to the City Council and made available to the public.

The schedule of Commission/Committee meetings is as follows:

- Complete Streets Commission – Every second Wednesday at 6:30 p.m.
- Environmental Quality Commission – Every third Wednesday at 6:00 p.m.
- Finance and Audit Committee – Third Thursday of every quarter at 5:30 p.m.,
- Housing Commission – Every first Wednesday at 6:30 p.m.
- Library Commission – Every third Monday at 6:30 p.m.
- Parks and Recreation Commission – Every fourth Wednesday at 6:30 p.m.
- Planning Commission – Twice a month on a Monday at 7 p.m.

Each commission/committee may establish other operational policies subject to the approval of the City Council. Any changes to the established policies and procedures shall be subject to the approval of the City Council.

5. *Off-premises meeting participation*

While technology allows commission/committee members to participate in meetings from a location other than the meeting location (referred to as “off-premises”), off-premises participation is discouraged given the logistics required to ensure compliance with the Brown Act and experience with technological failures disrupting the meeting. In the event that a commission/committee member believes that their participation is essential to a meeting, the following shall apply:

- Any commission/committee member intending to participate from an off-premise location shall inform the staff liaison at least two weeks in advance of the meeting.
- The off-premise location must be identified in the notice and agenda of the meeting.
- Agendas must be posted at the off-premise location.
- The off-premise location must be accessible to the public and be ADA compliant.
- The commission/committee member participating at a duly noticed off-premises location does not count toward the quorum necessary to convene a meeting of the commission/committee.
- For any one meeting, no more than one commission/committee member may participate from an off-premise location.
- All votes must be by roll call.

6. *Selection of chair and vice chair*

- The chair and vice chair shall be selected in May of each year by a majority of the members and shall serve for one year or until their successors are selected.
- Each commission/committee shall annually rotate its chair and vice chair.

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G. Memberships

Appointments/Oaths

- The City Council is the appointing body for all commissions/committees. All members serve at the pleasure of the City Council for designated terms.
- All appointments and reappointments shall be made at a regularly scheduled City Council meeting, and require an affirmative vote of not less than a majority of the City Council present.
- Before taking office, all members must complete an Oath of Allegiance required by Article XX, §3, of the Constitution of the State of California. All oaths are administered by the city clerk or their designee.
- Appointments made during the middle of the term are for the unexpired portion of that term.

Application and selection process

- The application process begins when a vacancy occurs due to term expiration, resignation, removal or death of a member.
- The application period will normally run for a period of four weeks from the date the vacancy occurs. If there is more than one concurrent vacancy in a Commission, the application period may be extended. Applications are available from the city clerk's office and on the City's website.
- The city clerk shall notify members whose terms are about to expire whether or not they would be eligible for reappointment. If reappointment is sought, an updated application will be required.
- Applicants are required to complete and return the application form for each commission/committee they desire to serve on, along with any additional information they would like to transmit, by the established deadline. Applications sent by email are accepted.
- After the deadline of receipt of applications, the city clerk shall schedule the matter at the next available regular City Council meeting. All applications received will be submitted and made a part of the City Council agenda packet for their review and consideration. If there are no applications received by the deadline, the city clerk will extend the application period for an indefinite period of time until sufficient applications are received.
- Upon review of the applications received, the City Council reserves the right to schedule or waive interviews, or to extend the application process in the event insufficient applications are received. In either case, the city clerk will provide notification to the applicants of the decision of the City Council.
- If an interview is requested, the date and time will be designated by the City Council. Interviews are open to the public.
- The selection/appointment process by the City Council shall be conducted at a City Council meeting. The city clerk will ask each City Councilmember for their nominations; the number of nominations is limited to the number of vacancies. The candidate that receives a majority of nominations will be appointed. If there is a tie, multiple rounds of voting will occur.
- Following a City Council appointment, the city clerk shall notify successful and unsuccessful applicants accordingly, in writing. Appointees will receive copies of the City's Non-Discrimination and Sexual Harassment policies, and disclosure statements for those members who are required to file under State law as designated in the City's Conflict of Interest Code. Copies of the notification will also be distributed to support staff and the commission/committee chair.
- An orientation will be scheduled by the city clerk following an appointment (but before taking office) and a copy of this policy document will be provided at that time.

Attendance

- A compilation of attendance will be submitted to the City Council at least annually listing absences for all commissions/committee members.
- Absences, which result in attendance at less than two-thirds of their meetings during the calendar year, will be reported to the City Council and may result in replacement of the member by the City Council.
- Any member who feels that unique circumstances have led to numerous absences can appeal directly to the City Council for a waiver of this policy or to obtain a leave of absence.
- While it is expected that members be present at all meetings, the chair and staff liaison should be notified if a member knows in advance that they will be absent.
- When reviewing commissioners for reappointment, overall attendance at full commission meetings will be given significant consideration.

Compensation

- Members shall serve without compensation (unless specifically provided) for their services, provided, however,

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members shall receive reimbursement for necessary travel expenses and other expenses incurred on official duty when such expenditures have been authorized by the City Council (See Policy CC-19-002).

Conflict of interest and disclosure requirements

- A Conflict of Interest Code has been updated and adopted by the City pursuant to Government Code §87300 et seq. Copies of the conflict of interest code are filed with the city clerk. Pursuant to the adopted Conflict of Interest Code, members serving on the Complete Streets Commission, Housing Commission, and Planning Commission are required to file a Statement of Economic Interest with the city clerk to disclose personal interest in investments, real property and income. This is done within 30 days of appointment and annually thereafter. A statement is also required within 30 days after leaving office.
- If a public official has a conflict of interest, the Political Reform Act may require the official to disqualify himself or herself from making or participating in a governmental decision, or using their official position to influence a governmental decision. Questions in this regard may be directed to the city attorney.

Qualifications, compositions, number

- In most cases, members shall be residents of the City of Menlo Park and at least 18 years of age.
- Current members of any other City commission/committee are disqualified for membership, unless the regulations for that advisory body permit concurrent membership. Commission/Committee members are strongly advised to serve out the entirety of the term of their current appointment before seeking appointment on another commission/committee.
- Commission/Committee members shall be permitted to retain membership while seeking any elective office. However, members shall not use the meetings, functions or activities of such bodies for purposes of campaigning for elective office.
- There shall be seven (7) members on each commission/committee.

Reappointments, resignations, removals

- Incumbents seeking a reappointment are required to complete and file an application with the city clerk by the application deadline. No person shall be reappointed to a commission/committee who has served on that same body for two consecutive terms; unless a period of one year has lapsed since the returning member last served on that commission/committee (the one-year period is flexible subject to City Council's discretion).
- Resignations must be submitted in writing to the city clerk, who will distribute copies to City Council and appropriate staff.
- The City Council may remove a member by a majority vote of the City Council without cause, notice or hearing.

Term of office

- Unless specified otherwise, the term of office for all commission/committee shall be four (4) years unless a resignation or a removal has taken place. The Finance and Audit Committee term of office shall be two (2) years.
- If a person is appointed to fill an unexpired term and serves less than two years, that time will not be considered a full term. However, if a person is appointed to fill an unexpired term and serves two years or more, that time will be considered a full term.
- Terms are staggered to be overlapping four-year terms, so that all terms do not expire in any one year.
- If a member resigns before the end of their term, a replacement serves out the remainder of that term.

Vacancies

- Vacancies are created due to term expirations, resignations, removals or death.
- Vacancies are posted by the city clerk in the City Council Chambers bulletin board and on the city website.
- Whenever an unscheduled vacancy occurs in any commission/committee, a special vacancy notice shall be posted within 20 days after the vacancy occurs. Appointment shall not be made for at least 10 working days after posting of the notice (Government Code 54974).
- On or before December 31 of each year, an appointment list of all regular advisory commissions/committees of the City Council shall be prepared by the city clerk and posted in the City Council Chambers bulletin board and on the City's website. This list is also available to the public. (Government Code 54972, Maddy Act).

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Roles and Responsibilities

Complete Streets Commission

The Complete Streets Commission is charged primarily with advising the City Council on realizing the City's adopted goals for complete streets, vision zero, climate action plan, and provide input on major land use and development projects as it relates to transportation. The Complete Streets Commission's responsibilities include:

- To advance the goals of the city's newly adopted climate action plan by making alternatives to driving safer and more attractive
- Advise City Council on the implementation of the transportation master plan.
- Continue to advocate for and advise the City Council on planning and installing pedestrian and bicycle rail crossing and safe cycling/pedestrian infrastructure.
- Continue to support City Council in ongoing initiatives to improve access to Downtown and support downtown businesses.
- Continue to support the implementation of the Safe Routes to School strategy and advocate for community engagement, program continuity and engineering implementation.
- Continue to support City Council's role as a stakeholder with regard to regional multi-modal and transportation demand management programs projects to increase

Environmental Quality Commission

The Environmental Quality Commission is charged primarily with advising the City Council on matters involving environmental protection, improvement and sustainability. Specific focus areas include:

- Preserving heritage trees
- Using best practices to maintain city trees
- Preserving and expanding the urban canopy
- Making determinations on appeals of heritage tree removal permits
- Administering annual Environmental Quality Awards program
- Organizing annual Arbor Day Event; typically, a tree planting event
- Advising on programs and policies related to protection of natural areas, recycling and waste reduction, environmentally sustainable practices, air and water pollution prevention, climate protection, and water and energy conservation.

Finance and Audit Committee

The Finance and Audit Committee is charged primarily to support delivery of timely, clear and comprehensive reporting of the City's fiscal status to the community at large. Specific focus areas include:

- Review the process for periodic financial reporting to the City Council and the public, as needed
- Review financial audit and annual financial report with the City's external auditors
- Review of the resolution of prior year audit findings
- Review of the auditor selection process and scope, as needed

Housing Commission

The Housing Commission is charged primarily with advising the City Council on housing matters including housing supply and housing related problems. Specific focus areas include:

- Community attitudes about housing (range, distribution, racial, social-economic problems)
- Programs for evaluating, maintaining, and upgrading the distribution and quality of housing stock in the City
- Planning, implementing and evaluating City programs under the Housing and Community Development Act of 1974
- Review and recommend to the City Council regarding the Below Market Rate (BMR) program
- Initiate, review and recommend on housing policies and programs for the City
- Review and recommend on housing related impacts for environmental impact reports
- Review and recommend on State and regional housing issues
- Review and recommend on the Housing Element of the General Plan

Library Commission

The Library Commission is charged primarily with advising the City Council on matters related to the maintenance and operation of the City's libraries and library systems. Specific focus areas include:

- The scope and degree of library activities

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- Maintenance and protection of City libraries
- Evaluation and improvement of library service
- Acquisition of library materials
- Coordination with other library systems and long range planning
- Literacy and ESL programs

Parks and Recreation Commission

The Parks and Recreation Commission is charged primarily with advising the City Council on matters related to City programs and facilities dedicated to recreation. Specific focus areas include:

- Those programs and facilities established primarily for the participation of and/or use by residents of the City, including adequacy and maintenance of such facilities as parks and playgrounds, recreation buildings, facilities and equipment
- Adequacy, operation and staffing of recreation programs
- Modification of existing programs and facilities to meet developing community needs
- Long range planning and regional coordination concerning park and recreational facilities

Planning Commission

The Planning Commission is organized according to State Statute.

- The Planning Commission reviews development proposals on public and private lands for compliance with the General Plan and Zoning Ordinance.
- The Commission reviews all development proposals requiring a use permit, architectural control, variance, minor subdivision and environmental review associated with these projects. The Commission is the final decision-making body for these applications, unless appealed to the City Council.
- The Commission serves as a recommending body to the City Council for major subdivisions, rezoning's, conditional development permits, Zoning Ordinance amendments, General Plan amendments and the environmental reviews and Below Market Rate (BMR) Housing Agreements associated with those projects.
- The Commission works on special projects as assigned by the City Council.

Special Advisory Bodies

The City Council has the authority to create standing committees, task forces or subcommittees for the City, and from time to time, the City Council may appoint members to these groups. The number of persons and the individual appointee serving on each group may be changed at any time by the City Council. There are no designated terms for members of these groups; members are appointed by and serve at the pleasure of the City Council.

Any requests of city commissions or committees to create such ad hoc advisory bodies shall be submitted in writing to the city clerk for City Council consideration and approval.

Procedure history

Action	Date	Notes
Procedure adoption	1991	Resolution No. 3261
Procedure adoption	2001	
Procedure adoption	2011	
Procedure adoption	2013	Resolution No. 6169
Procedure adoption	2017	Resolution No. 6377
Procedure adoption	6/8/2021	Resolution No. 6631
Procedure adoption	3/1/2022	Resolution No. 6706
Procedure adoption	3/8/2022	Resolution No. 6718

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Procedure adoption	9/20/2022	Resolution No. 6776
Procedure adoption	1/10/2023	Resolution No. 6803

MAYOR ASSIGNMENTS	2023 Primary	2023 Alternate
Association of Bay Area Governments (ABAG)	Jen Wolosin	Cecilia Taylor
League of California Cities (Peninsula Division)	Jen Wolosin	Cecilia Taylor
Menlo Park Chamber of Commerce / City liaison position	Jen Wolosin	Cecilia Taylor
Palo Alto Community Fund Advisory Board	Jen Wolosin	
San Mateo County Council of Cities and City Selection Committee	Jen Wolosin	Cecilia Taylor
REGIONAL BOARD MEMBERS	2023 Member	
Bay Area Water Supply & Conservation Agency (BAWSCA) - through June 25, 2025 and San Francisco Bay Area Regional Water System Financing Authority	Maria Doerr	
REGIONAL ASSIGNMENTS	2023 Primary	2023 Alternate
Caltrain Modernization Local Policy Group	Jen Wolosin	Betsy Nash
City/County Association of Governments of San Mateo County (C/CAG)	Cecilia Taylor	Betsy Nash
Emergency Services Council (San Mateo County JPA)	Cecilia Taylor	Drew Combs
Facebook Local Community Fund	Betsy Nash	Cecilia Taylor
Grand Boulevard Task Force		
HEART Board Member Agency Committee (MAC)	Drew Combs	Maria Doerr
Home for All	Cecilia Taylor	n/a
Peninsula Clean Energy (PCE) Community Choice Energy	Betsy Nash	Maria Doerr
Peninsula Traffic Congestion Relief Alliance (Commute.org)	Jen Wolosin	Drew Combs
San Francisquito Creek Joint Powers Authority	Drew Combs	Cecilia Taylor
San Mateo County Mosquito and Vector Control District	Catherine Carlton*	n/a
SFO Airport/Community Roundtable	Cecilia Taylor	Drew Combs
South Bayside Waste Management Authority (SBWMA)	Cecilia Taylor	Drew Combs
Stanford Community Resources Group	Betsy Nash	Maria Doerr
CITY COUNCIL ADVISORY BODY MEMBERS	2023 Member 1	2023 Member 2
Finance and Audit Committee		
ADVISORY BODY LIAISONS	2023 Member	
Complete Streets Commission	Cecilia Taylor	
Environmental Quality Commission	Betsy Nash	
Housing Commission	Maria Doerr	
Library Commission	Jen Wolosin	
Parks and Recreation Commission	Drew Combs	
Planning Commission	Maria Doerr	
OUTSIDE AGENCY LIAISONS	2023 Member 1	2023 Member 2
Menlo Park City School District		
Menlo Park Fire Protection District		
Searsville Advisory Group	Betsy Nash	
San Mateo County Flood and Sea Level Rise District (FSLR, OneShoreline)	Cecilia Taylor	Betsy Nash

*appointed on December 14, 2021 to serve through December 31, 2024



STAFF REPORT

City Council Meeting Date: 2/28/2023
Staff Report Number: 23-042-CC

City Council Initiated Item **Direction on City Council role in San Mateo County related animal control efforts**

Recommendation

Provide direction to the city attorney and staff on the role of the City Council in San Mateo County (County) related animal control efforts in Menlo Park.

Background

On February 14, City Councilmember Combs requested an addition to the City Council agenda for animal control matters. Specific questions pertain to parameters in which the County enforces and engages with the public and the role of the City Council after County determination in animal control matters.

Analysis

Pursuant to City Council Procedure #CC-20-013 (Attachment A), the City Council may discuss the item and ask staff questions regarding preliminary scope, analysis and resource requirements. After discussion, with a motion and second, the City Council may take one of the following actions:

- Direct the city manager to prioritize staff resources to prepare a formal staff report for further City Council consideration and/or action, or
- Direct the item to an advisory body for preparation of a formal staff report with no additional staff support required, or
- Direct the city manager to prepare a formal staff report for further City Council consideration as resources are available, or
- Defer action to the City Council's annual goal setting process.

Impact on City Resources

Determined by City Council direction.

Environmental Review

This action is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §§ 15378 and 15061(b)(3) as it will not result in any direct or indirect physical change in the environment.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72

Staff Report #: 23-042-CC

hours prior to the meeting.

Attachments

A. City Council Procedure #CC-20-013

Report prepared by:
Justin Murphy, City Manager

CITY COUNCILMEMBER REQUESTS

City Council Policy #CC-20-013
Adopted August 25, 2020



Purpose

The purpose of this procedure is to provide transparency into requests by individual City Councilmembers that result in the use of staff time. The policy applies to all City Councilmembers equally and allows the full City Council to determine how to use limited city resources.

For this procedure, a “City Councilmember request” is defined as a request to use city resources in a manner that exceeds the City Council approved budget, priorities, or work plan. This includes requests directed to the city manager, city attorney, and all city staff members. This procedure also applies to City Council appointed commissions and committees.

Requests to add items to a future agenda

To make a request

To request consideration of an item at future City Council meetings, City Councilmembers may send the request via email to the city manager, with a copy to the Mayor and Vice Mayor, or via email to city.council@menlopark.org. The request must be received no later than two (2) business days prior to publication of the meeting agenda. The request will automatically appear under “City Council initiated items” at the end of the City Council’s regular agenda.

Initial City Council consideration of request

As an agenda item under “City Council initiated items” the City Council may discuss the item and ask staff questions regarding preliminary scope, analysis, and resource requirements. After discussion, with a motion and second, the City Council may take one of the following actions:

- Direct the city manager to prioritize staff resources to prepare a formal staff report for further City Council consideration and/or action, or
- Direct the item to an advisory body for preparation of a formal staff report with no additional staff support required, or
- Direct the city manager to prepare a formal staff report for further City Council consideration as resources are available, or
- Defer action to the City Council’s annual goal setting process.

If the request does not receive sufficient City Council support, the item is not considered further.

City Council action

When the staff report is available, the report will be placed under “City Council initiated items” for City Council discussion and action at the next City Council meeting, regardless of agenda load management exercised by the Mayor, Vice Mayor, and city manager.

Request to modify operations or for special projects

To make a request

To request consideration of a change in operations or for a special project, a City Councilmember may send the request via email to the city manager, with a copy to the Mayor and Vice Mayor, or via email to city.council@menlopark.org. The request must be received no later than two (2) business days prior to publication of the meeting agenda. The request will automatically appear under “City Council initiated items” at the end of the City Council’s regular agenda.

Initial City Council consideration of request

As an agenda item under “City Council initiated items” the City Council may discuss the item and ask the city manager the preliminary assessment of the scope, analysis, and resource requirements of the request. After discussion, with a motion and second, the City Council may take one of the following actions:

- Direct the city manager to prioritize staff resources to prepare a formal staff report for further City Council consideration and/or action, or
- Direct the city manager to prepare a formal staff report for further City Council consideration as resources are available, or
- Defer action to the City Council’s annual budget process.

If the request does not receive sufficient City Council support, the item is not considered further.

City Council action

When the staff report is available, the report will be placed under “City Council initiated items” for City Council discussion and action at the next City Council meeting, regardless of agenda load management exercised by the Mayor, Vice Mayor, and city manager.

CITY COUNCILMEMBER REQUESTS

City Council Policy #CC-20-013

Adopted August 25, 2020

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and action at the next City Council meeting, regardless of agenda load management exercised by the Mayor, Vice Mayor, and city manager.

Emergency and non-agendized items

Emergency and non-agendized items may be added to an agenda only in accordance with state law. Emergency items are only those matters affecting public health or safety such as work stoppages, disasters and other severe emergencies. Adding an emergency item requires a majority vote. Emergency items are very rare. An item that the City Council would like to act on after agenda posting is considered a non-agendized item.

Non-agendized items may be added to the agenda only if the City Council makes findings that (1) the need to consider the item arose after the posting of the agenda, and; (2) there is a need to take immediate action at this meeting of the City Council. These findings must be approved by a four-fifths vote; if less than five members of the City Council are present, the findings require a unanimous vote of those present.

Emergency and non-agendized items are not be used to bypass the City Councilmember request process above.

Procedure history

Action	Date	Notes
Draft procedure presented	July 18, 2020	City Council continued item to August 25, 2020
Procedure adoption	August 25, 2020	Draft procedure amended at City Council direction. Staff edit to clarify definition of a "non-agendized item"



STAFF REPORT

City Council

Meeting Date: 2/28/2023
Staff Report Number: 23-051-CC

Informational Item: City Council agenda topics: March 14 – March 28, 2023

Recommendation

The purpose of this informational item is to provide the City Council and members of the public access to the anticipated agenda items that will be presented to the City Council. The mayor and city manager set the City Council agenda so there is no action required of the City Council as a result of this informational item.

Policy Issues

In accordance with the City Council procedures manual, the mayor and city manager set the agenda for City Council meetings.

Analysis

In an effort to provide greater access to the City Council's future agenda items, staff has compiled a listing of anticipated agenda items, Attachment A, through March 28, 2023. The topics are arranged by department to help identify the work group most impacted by the agenda item.

Specific dates are not provided in the attachment due to a number of factors that influence the City Council agenda preparation process. In their agenda management, the mayor and city manager strive to compile an agenda that is most responsive to the City Council's adopted priorities and work plan while also balancing the business needs of the organization. Certain agenda items, such as appeals or State mandated reporting, must be scheduled by a certain date to ensure compliance. In addition, the meeting agendas are managed to allow the greatest opportunity for public input while also allowing the meeting to conclude around 11 p.m. Every effort is made to avoid scheduling two matters that may be contentious to allow the City Council sufficient time to fully discuss the matter before the City Council.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

A. City Council agenda topics: March 14 – March 28, 2023

Report prepared by:

Judi A. Herren, Assistant to the City Manager/City Clerk

Tentative City Council Agenda

#	Title	Department	Item type	City Council action
1	December 2022 quarterly investment report	ASD	Consent	Receive and file
2	Mid-year budget amendment	ASD	Regular	Adopt resolution
3	Receive the annual comprehensive financial report for the fiscal year ended June 30, 2022	ASD	Consent	Receive and file
4	Report out of 2/28 closed session	CAO	Closed Session	Report out
5	Accept the 2022 Housing Element annual progress report and the annual housing successor report	CDD	Consent	Approve
6	Authorize the City Manager to execute a contract amendment with M-Group for the Housing Element Update	CDD	Consent	Approve
7	Introduce an ordinance-Senate Bill 9 (SB 9)	CDD	Public Hearing	First read/intro ordinance
8	Parkline/SRI NOP comment review and Parkline - Senate Bill 7	CDD	Regular	Approve
9	SB 9 - second reading	CDD	Consent	Second read/adopt ordinance
10	Vesting Tentative Map Extension for 201 El Camino Real	CDD	Public Hearing	Adopt resolution
11	Adopt a resolution authorizing the annual destruction of obsolete records	CMO	Consent	Adopt resolution
12	Annual conflict of interest code update	CMO	Consent	Adopt resolution
13	Appoint City Councilmembers to various standing and ad hoc subcommittees, and disband inactive ad hoc subcommittees	CMO	Regular	Decide
14	City Council procedure manual	CMO	Informational	No action
15	Environmental Quality Commission Chair Report	CMO	Presentation	No action
16	First reading/intro: Outdoor dining ord	CMO	Regular	Second read/adopt ordinance
17	Receive and file City Council and advisory body annual attendance report for March 2022 – February 2023	CMO	Consent	Receive and file
18	Reach Codes 2.0	CMO	Study Session	No action
19	Transmittal of city attorney billing (Jan 2023)	CMO	Informational	No action
20	Grand Jury PRA report policies response	CMO, CA	Informational	No action
21	Adopt a resolution to update City Council Procedure CC-86-0001, "Naming and/or changing the name of facilities"	LCS	Consent	Adopt resolution
22	Belle Haven School renovation project update	LCS	Informational	Info from staff
23	Budget amendments related to and supporting the Menlo Park Community Campus project	LCS	Regular	Adopt resolution
24	Reimagining Public Safety Update	PD	Informational	No action
25	Adopt a resolution initiating the Menlo Park landscape assessment district proceedings for fiscal year 2023-24	PW	Consent	Adopt resolution
26	Adopt resolution - Priority Water Service for Affordable Housing Developments	PW	Consent	Adopt resolution
27	Approve service agreement with Caltrain for Middle Avenue undercrossing project	PW	Consent	Contract award or amend
28	Authorize the city manager to execute an agreement to develop a shuttle evaluation study	PW	Consent	Contract award or amend
29	Consider an appeal of the Complete Streets Commission decision to remove two parking spaces on Roble Avenue	PW	Regular	Adopt resolution, Decide

Tentative City Council Agenda

#	Title	Department	Item type	City Council action
30	Street Resurfacing Program	PW	Consent	Approve
31	Update on five-year capital program	PW	Informational	No action
32	Water Storage/Supply Project - Info Item + Corp Yard Well Update	PW	Informational	No action
33	Goal setting	Various	Special	Direction to staff



STAFF REPORT

City Council

Meeting Date: 2/28/2023

Staff Report Number: 22-043-CC

Informational Item: Annual City Council priority setting workshop
March 18, 2023

Recommendation

This is an informational item and does not require City Council action.

Policy Issues

The City Council holds an annual priority setting workshop to provide direction to the city manager on priority projects for the next year. The City Council identifies top priorities to receive significant attention and resources, as well as a variety of work plan projects that will potentially receive resources based on workforce capacity and funding availability.

Background

The City Council last held a priority setting workshop January 30, 2021, and carried over those identified priorities through 2022. The City Council received the final 2021-2022 work plan progress report at the January 24 City Council meeting (Attachment A.)

Analysis

The City Council is scheduled to hold the annual priority setting workshop Saturday, March 18, from 10 a.m. to 2 p.m. in a hybrid format. Priority setting is informed through public participation, input from City Councilmembers, input from staff based on City Council actions and direction, needs identified in various master planning efforts, and review of remaining work plan items from 2022. A professional facilitator will develop the workshop agenda, present findings on priority topics, facilitate a priority setting discussion, and produce a workshop final report.

Tentative agenda topics (subject to change):

- Welcome
- Public comment
- Introduction and review of agenda
- Context for priority setting discussions
- Break
- Establish top priorities through fiscal year 2023-24
- Next steps

Public engagement:

Staff will launch a dedicated webpage to provide transparency on priority setting actions by the City Council and updates on progress over the next year.

An online web form will be released to the public to solicit input on community-identified priorities. Results will be available to City Council during the priority setting workshop.

Staff will encourage members of the public to attend and share their input through a variety of public communication platforms leading up to the workshop.

Impact on City Resources

This informational item has no impact on city resources.

Environmental Review

This action is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §§ 15378 and 15061(b)(3) as it will not result in any direct or indirect physical change in the environment.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

- A. Hyperlink – January 24 Staff Report #23-015-CC: menlopark.gov/files/sharedassets/public/agendas-and-minutes/city-council/2023-meetings/agendas/20230124-city-council-agenda-packet.pdf#page=45

Report prepared by:
Stephen Stolte, Assistant City Manager



STAFF REPORT

City Council Meeting Date: 2/28/2023
Staff Report Number: 23-044-CC

Informational Item: Proposed process and timeline to develop a focused addendum to the Parks and Recreation Facilities Master Plan to include pickleball

Recommendation

No City Council action is requested at this time. City staff recommends that City Council review this informational report outlining a proposed process and timeline for developing a focused addendum to the Parks and Recreation Facilities Master Plan (PRFMP) to include emerging community interest in pickleball.

Policy Issues

City Council provides policy direction to the city manager regarding services to the community; allocates resources to support City services and capital projects; reviews and approves master plans; and sets prioritization for the use of City resources to serve the community.

Background

The PRFMP was accepted by City Council October 15, 2019 (Attachment A.) The PRFMP is designed to provide a comprehensive road map to guide decision-making for the City of Menlo Park to maintain, manage and develop its parks, open spaces and recreation facilities for the next 20 years, and to help provide the community with the recreational opportunities that they desire.

At the time the PRFMP was developed and approved in 2019, pickleball had not yet emerged as a sport requiring use of current and/or future recreation facilities in Menlo Park, and as a result, pickleball is not included in the PRFMP.

In September 2020, in response to community member requests for rapid implementation of a new service not included in the PRFMP, the Parks and Recreation Commission (PRC) recommended a pickleball pilot program to gauge community interest in pickleball. The pickleball pilot program was launched in November 2020, then was later extended and expanded. The pilot program currently includes eight pickleball courts located on converted tennis courts – four at Kelly Park and four at Nealon Park.

On October 26, 2022, the PRC recommended converting additional Nealon Park tennis courts to pickleball. Due to concerns about potential impacts to tennis users including the Hillview Middle School tennis team, the recommendation has not been implemented, pending the development of a focused addendum to address pickleball needs in a holistic manner that is integrated into the context of the overall PFRMP.

Analysis

Key considerations

The success of the pickleball pilot program indicates that there exists sufficient interest and demand for pickleball to warrant developing a focused addendum to the PRFMP to include pickleball. Developing a focused addendum to the PRFMP to include pickleball is an essential preparatory step to place future pickleball-related capital projects or expansions in the context of the City's overall system of recreation and sports facilities, and will include the following key considerations:

1. Alignment of potential future pickleball-related capital projects with the goals, prioritization, and timelines in the Parks and PRFMP
2. Evaluate options for user fees related to pickleball and tennis, including but not limited to the following considerations:
 - A. Ensure that Menlo Park residents of all backgrounds, income levels and lived experiences have meaningful, equitable, inclusive access to public recreation amenities, including tennis courts and pickleball courts
 - B. Prioritize use by Menlo Park residents and deprioritize use by non-residents
 - C. Evaluate current and potential user fees, including pickleball court fees, tennis court fees and court reservation fees
 - D. Evaluate user fees for City-sponsored tennis and pickleball programming
 - E. Evaluate rental fees for use of City courts for private instruction
 - F. All fees are subject to City Council authorization in the City of Menlo Park Master Fee Schedule
3. Estimated costs and timelines for potentially creating additional purpose-built pickleball courts in city parks
4. Impacts to tennis users of potentially converting additional existing tennis courts to pickleball
5. Changes in pickleball usage resulting from the potential construction of new, purpose-built pickleball courts at Willow Oaks Park during the planned second phase of park improvements.

Proposed process and timeline

City staff proposes the following process and timeline to develop a focused addendum to the PRFMP to include pickleball. All dates are tentative and subject to change.

- February 22 – PRC reviewed informational item outlining the proposed process and timeline, and expressed general assent with the process and timeline as presented
- February 28 – City Council reviews informational item outlining the proposed process and timeline
- March 22 – PRC reviews pickleball usage and facility data, including benchmark data from other Peninsula cities that provide pickleball courts, and evaluation of tennis and pickleball user fees
- March-April– City staff conducts robust outreach and engagement with community stakeholders, including current pickleball and tennis users, to collect input and data about current and future community needs related to pickleball play in Menlo Park
- April 26 – PRC reviews community input and makes recommendations for a draft PRFMP addendum to include pickleball
- May 9 – City Council reviews the progress of the PRFMP addendum's development, including considerations for potential changes to tennis and/or pickleball user fees
- May 24 – PRC recommends PRFMP addendum to City Council for approval
- Summer – City Council approves the PRFMP addendum and any changes to the master fee schedule related to tennis and/or pickleball user fees.

Impact on City Resources

There are no substantive direct impacts to the City's General Fund associated with the proposed PRFMP

addendum.

Environmental Review

This informational report is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §§ 15378 and 15061(b)(3) as it will not result in any direct or indirect physical change in the environment.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting.

Attachments

A. Hyperlink – PRFMP (2019): menlopark.gov/Government/Open-government/Master-plans

Report prepared by:

Tricia Mullan, Library and Community Services Supervisor

Sean S. Reinhart, Library and Community Services Director



STAFF REPORT

City Council

Meeting Date: 2/28/2023

Staff Report Number: 23-049-CC

Informational Item: Expiration of local emergencies

Recommendation

This is an informational item and does not require City Council action.

Policy Issues

City Council sets policy and goals and provides direction to staff regarding municipal services to the Menlo Park community; allocates resources to support and maintain city facilities and operations and provide services to residents; and ratifies and/or rescinds local emergency orders.

Background

COVID-19

On March 11, 2020, the Director of Emergency Services proclaimed the existence of a local emergency in the City of Menlo Park based on COVID-19. On March 11, 2020, the City Council of the City of Menlo Park ratified the Director's proclamation. As a result of the local emergency, the City is empowered to take actions to preserve and protect the health and safety of the community. On March 23, 2020, the Director issued the City of Menlo Park director of emergency services/city manager order ("Order") No. 1 temporarily suspending all construction activity in the City of Menlo Park. On March 27, 2020, the City of Menlo Park issued Emergency Order No. 2 that closed all City facilities to the public, including the Arrillaga Family Gymnasium, City Council Chambers, City Hall, and the Arrillaga Family Gymnastics Center. This precaution was necessary to protect public health by minimizing opportunities for congregation, both public and employee, during the COVID-19 pandemic. On April 17, 2020, the Director issued Order No. 3 closing additional public facilities (Bedwell Bayfront Park.) On June 10, 2020, the Director issued Order No. 6 opening certain public facilities (Burgess and Belle Haven Pools) subject to the County safety precautions.

On April 27, 2021, the City Council reviewed a four-phase plan for safely reopening City facilities to limited indoor public access, including the Arrillaga Family Gymnasium, City Council Chambers, City Hall, and the Arrillaga Family Gymnastics Center tentatively in Fall 2021.

On June 15, 2021, the State of California rescinded multiple public health restrictions and issued new guidance which allows local communities to reopen these facilities with some precautions to prevent the spread of COVID-19 in workplaces.

Rain events

On January 5, 2023, the City Council adopted a resolution ratifying the Director of Emergency Services proclamation of local emergency, pursuant to Government Code Section 54956.5 related to the extreme atmospheric river rain events, which occurred in the final days of December 2022 and threatened to continue in the coming hours and days of January 2023, which events had caused flooding in and around San Francisquito Creek and the Atherton Channel, as well as areas that drain to these waterways.

Analysis

COVID-19

Pursuant to Government Code §8630, local authorities must renew a local emergency every 60 days or the local emergency will expire. However, the Governor's March 4, 2020, Proclamation of a State of Emergency regarding COVID-19 provided "the 60-day time period in Government Code §8630, within which local government authorities must renew a local emergency, is hereby waived for the duration of this statewide emergency. Any local emergency proclaimed will remain in effect until each local governing authority terminates its respective local emergency." Because of this proclamation, the City did not need to renew its local COVID-19 emergency declaration every 60 days. However, once the Statewide emergency ends, the City will be required to renew its local COVID-19 emergency declaration, or the local emergency will too expire for lack of renewal every 60 days.

Rain events

Resolution No. 6800, adopted January 5, 2023, declared a local emergency due to extreme weather conditions and will expire 60 days after adoption.

Impact on City Resources

There is no impact on City resources.

Environmental Review

This action is not a project within the meaning of the California Environmental Quality Act (CEQA) Guidelines §§ 15378 and 15061(b)(3) as it is a minor change that will not result in any direct or indirect physical change in the environment.

Public Notice

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Attachments

None.

Report prepared by:

Judi A. Herren, Assistant to the City Manager/City Clerk