



REGULAR MEETING AGENDA

Date: 2/6/2023
Time: 7:00 p.m.
Location: Zoom.us/join – ID# 862 5880 9056 and
City Council Chambers
751 Laurel St., Menlo Park, CA 94025

NOVEL CORONAVIRUS, COVID-19, EMERGENCY ADVISORY NOTICE

Consistent with Cal. Gov. Code §54953(e), and in light of the declared state of emergency, and maximize public safety while still maintaining transparency and public access, members of the public can listen to the meeting and participate using the following methods.

How to participate in the meeting

- Access the live meeting, in-person, at the City Council Chambers
- Access the meeting real-time online at:
zoom.us/join – Meeting ID# 862 5880 9056
- Access the meeting real-time via telephone (listen only mode) at:
(669) 900-6833
Regular Meeting ID # 862 5880 9056
Press *9 to raise hand to speak
- Submit a written comment online up to 1-hour before the meeting start time:
PlanningDept@menlopark.gov*
Please include the agenda item number related to your comment.

*Written comments are accepted up to 1 hour before the meeting start time. Written messages are provided to the Planning Commission at the appropriate time in their meeting.

Subject to change: Given the current public health emergency and the rapidly evolving federal, state, county and local orders, the format of this meeting may be altered or the meeting may be canceled. You may check on the status of the meeting by visiting the city website menlopark.gov. The instructions for logging on to the webinar and/or the access code is subject to change. If you have difficulty accessing the webinar, please check the latest online edition of the posted agenda for updated information (menlopark.gov/agendas).

Regular Meeting

A. Call To Order

B. Roll Call

C. Reports and Announcements

D. Public Comment

Under “Public Comment,” the public may address the Commission on any subject not listed on the agenda. Each speaker may address the Commission once under public comment for a limit of three minutes. You are not required to provide your name or City of residence, but it is helpful. The Commission cannot act on items not listed on the agenda and, therefore, the Commission cannot respond to non-agenda issues brought up under Public Comment other than to provide general information.

E. Consent Calendar

- E1. Approval of minutes from the November 3, 2022, Planning Commission meeting. ([Attachment](#))
- E2. Approval of minutes from the November 7, 2022, Planning Commission meeting. ([Attachment](#))
- E2. Approval of court report transcripts for 123 Independence Drive and Parkline from the December 12, 2022, Planning Commission meeting. ([Independence Drive](#); [Parkline](#))

F. Study Session

- F1. Study session for the Parkline Master Plan project to comprehensively redevelop an approximately 63.2-acre site located at 301 and 333 Ravenswood Avenue and 555 and 565 Middlefield Road. The proposed project would redevelop SRI International’s research campus by creating a new office/research and development, transit-oriented campus with no net increase in commercial square footage, up to 550 new rental housing units (with a minimum of 15% of the units available for below market rate households), new bicycle and pedestrian connections, and approximately 25 acres of publicly accessible open space. The proposed project would demolish all existing buildings, excluding Buildings P, S, and T, which would remain on-site and operational by SRI and its tenants. The proposed project would organize land uses generally into two land use districts within the Project site, including 1) an approximately 10-acre Residential District in the southwestern portion of the Project site; and 2) an approximately 53-acre Office/R&D (research and development) District that would comprise the remainder of the Project site. In total, the Proposed Project would result in a total of approximately 1,898,931 square feet, including approximately 1,380,332 square feet of office/R&D and approximately 518,599 square feet of residential uses (including up to 450 rental residential units). In addition, the proposed project would establish a separate parcel of land that is proposed to be leased to an affordable housing developer for the future construction of a 100 percent affordable housing or special needs project which would be separately rezoned as part of the proposed project for up to 100 residential units (in addition to the residential units proposed within the Residential District), and which is not included in residential square footage calculations as the square footage has not been determined. The EIR will study two potential project variants,

one that includes an approximately 2 million gallon buried concrete water reservoir and associated facilities, and one that includes an additional 50 residential units for a total of up to 600 dwelling units, inclusive of the standalone affordable housing building. The Planning Commission previously held a public hearing on the scope and content of the EIR as part of the 30-day NOP (Notice of Preparation) comment period that ended on January 9, 2023. The project site is zoned “C-1(X)” (Administrative and Professional District, Restrictive) and governed by a Conditional Development Permit (CDP) approved in 1975, and subsequently amended in 1978, 1997, and 2004. The proposed project is anticipated to include the following entitlements: General Plan Amendment (Text and Map), Zoning Ordinance Amendment, Rezoning, Conditional Development Permit, Development Agreement, Architectural Control (for potential future Design Review), Heritage Tree Removal Permits, Vesting Tentative Map, Below Market Rate (BMR) Housing Agreement and Environmental Review. **Continued from the meeting of January 23, 2023.** ([Staff Report #22-073-PC](#); [Correspondence](#))

G. Public Hearing

- G1. Consider and adopt a resolution to approve a use permit to demolish an existing one-story, single-family residence, and construct a new two-story residence on a substandard lot with regard to minimum lot width in the R-1-U (Single Family Urban Residential) zoning district, at 893 Woodland Avenue; determine this action is categorically exempt under CEQA Guidelines Section 15303’s Class 3 exemption for new construction or conversion of small structures. **Continued to a future meeting.**
- G2. Consider and adopt a resolution to approve a use permit to demolish an existing one-story, single-family residence, and construct a new two-story, single-family residence on a substandard lot with regard to minimum lot width and area in the R-1-U (Single Family Urban Residential) zoning district, at 440 University Drive. The project includes an attached accessory dwelling unit (ADU), which is a permitted use not subject to discretionary review; determine this action is categorically exempt under CEQA Guidelines Section 15303’s Class 3 exemption for new construction or conversion of small structures. ([Staff Report #23-010-PC](#))
- G3. Consider and adopt a resolution to approve a use permit to demolish an existing one-story, single-family residence and detached accessory building, and construct a new two-story, single-family residence on a substandard lot with regard to lot width, depth, and area in the R-1-U (Single Family Urban Residential) zoning district, at 167 McKendry Drive; determine this action is categorically exempt under CEQA Guidelines Section 15303’s Class 3 exemption for new construction or conversion of small structures. ([Staff Report #23-011-PC](#))
- G4. Consider and adopt a resolution to approve a use permit to demolish an existing one-story, single family residence and construct two new two-story residences on a substandard lot with regard to minimum lot width in the R-2 (Low Density Apartment) district, at 785 Partridge Avenue. The project would also include excavation in the interior side and rear setbacks for lightwells associated with basements; determine this action is categorically exempt under CEQA Guidelines Section 15303’s Class 3 exemption for new construction or conversion of small structures. Additionally, the proposal includes administrative review of a minor subdivision to subdivide the project into two condominium units. ([Staff Report #23-012-PC](#))

H. Informational Items

- H1. Future Planning Commission Meeting Schedule – The upcoming Planning Commission meetings

are listed here, for reference. No action will be taken on the meeting schedule, although individual Commissioners may notify staff of planned absences.

- Regular Meeting: February 27, 2023
- Regular Meeting: March 13, 2023

I. Adjournment

At every regular meeting of the Planning Commission, in addition to the public comment period where the public shall have the right to address the Planning Commission on any matters of public interest not listed on the agenda, members of the public have the right to directly address the Planning Commission on any item listed on the agenda at a time designated by the chair, either before or during the Planning Commission's consideration of the item.

At every special meeting of the Planning Commission, members of the public have the right to directly address the Planning Commission on any item listed on the agenda at a time designated by the chair, either before or during consideration of the item. For appeal hearings, appellant and applicant shall each have 10 minutes for presentations.

If you challenge any of the items listed on this agenda in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City of Menlo Park at, or before, the public hearing.

Any writing that is distributed to a majority of the Planning Commission by any person in connection with an agenda item is a public record (subject to any exemption under the Public Records Act) and is available by request by emailing the city clerk at jaherren@menlopark.gov. Persons with disabilities, who require auxiliary aids or services in attending or participating in Planning Commission meetings, may call the City Clerk's Office at 650-330-6620.

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CONTINUED PUBLIC HEARING DRAFT MINUTES

Date: 11/03/2022

Time: 7:00 p.m.

Meeting Location: Zoom.us/join – ID# 871 4022 8110

On November 3, 2022, at 7 p.m. Chair Chris DeCardy reopened the public hearing item F1 that was continued from the regular meeting of October 24, 2022

Present: Andrew Barnes, Chris DeCardy (Chair), Linh Dan Do, Cynthia Harris (Vice Chair), Henry Riggs, Michele Tate (all six commissioners were at the October 24, 2022 hearing)

Staff: Nira Dougherty, City Attorney; Kyle Perata, Planning Manager; Matt Pruter, Associate Planner; Anna Shimko, City Attorney's Office

Continued Public Hearing

Adopt a resolution recommending the City Council certify the final environmental impact report (Final EIR), adopt California Environmental Quality Act (CEQA) Findings, adopt a Statement of Overriding Considerations for significant and unavoidable impacts, amend the General Plan Circulation Element, rezone the project site and amend the zoning map to incorporate "X" overlay district and approve the conditional development permit (CDP), approve the vesting tentative maps for the main project site and the Hamilton Avenue Parcels, approve the development agreement (DA), and approve the below market rate (BMR) housing agreements for the proposed Willow Village masterplan project located at 1350-1390 Willow Road, 925-1098 Hamilton Avenue and 1005-1275 Hamilton Court, 1399 and 1401 Willow road, and 871-883 Hamilton Avenue. The proposed project would demolish approximately 1 million square feet of existing office and industrial buildings and redevelop the project site with:

- Up to 1.6 million square feet of office and accessory uses (a maximum of up to 1.25 million square feet of offices with balance for accessory uses);
- Up to 200,000 square feet of retail/commercial uses, including a grocery store, pharmacy, entertainment and restaurant uses;
- Up to 1,730 housing units, including 312 below market rate units (260 inclusionary units plus 52 units per the city's commercial linkage requirement) of which 119 would be age-restricted senior housing units;
- Up to a 193 room hotel and associated retail/dining;
- An approximately 3.5-acre publicly accessible park, a dog park, and additional public open space;
- An approximately 1.5-acre publicly accessible town square;
- An approximately 2-acre publicly accessible elevated park extending over Willow Road providing access at the Hamilton Avenue Parcel North (Belle Haven Shopping Center); and
- A potential publicly-accessible, below grade tunnel for Meta intercampus trams, bicyclists and pedestrians connecting the project with the West and East campuses.

The requested City actions and entitlements for the proposed project include a conditional development permit, development agreement, rezoning, general plan and zoning map amendments, vesting tentative maps, below market rate (BMR) housing agreement, and environmental review.

The proposal includes a request for an increase in height, floor area ratio (FAR), and density under the bonus level development allowance in exchange for community amenities through a conditional development permit and development agreement. The proposed project would be rezoned to combine the "X" (Conditional Development) overlay district with the O and R-MU zoning designations to allow for uses and development regulations as specified in the conditional development permit. The proposed project also includes the realignment of Hamilton Avenue-enabled through the vesting tentative maps. The proposed project requires a general plan circulation element and zoning map amendment to modify the locations of public rights-of-ways and paseos and a new street connection at O'Brien Drive. Through the proposed conditional development permit, the proposed project includes modifications to the City's design standards for specific buildings, BMR guidelines, signage requirements, outdoor seating, on-site and off-site sales of beer, wine, and alcohol, application of its transportation demand management (TDM) requirements, and sets up future architectural reviews for building and site design. The proposed project also includes a request for the use and storage of hazardous materials (diesel fuel) for back up emergency generators on the main Project Site and the Hamilton Avenue Parcels. A development agreement would be entered into between the City and the applicant for the provision of community amenities, development controls, and vested rights. The proposed project includes vesting tentative maps for new parcelization and infrastructure and a BMR housing agreement for the provision of 312 BMR units. The City Arborist conditionally approved the removal of 276 heritage trees on the main project site and 3 heritage trees on the Hamilton Avenue Parcels for the proposed development and 16 trees along O'Brien Drive to accommodate site access and right-of-way modifications along O'Brien Drive. The proposed project also includes a potential project variant that would increase the total number of housing units by up to 200 units for a total of 1,930 units, for consideration by decision makers as part of the requested land use entitlements.

To accommodate the realignment of Hamilton Avenue west of Willow Road, the existing Chevron station at 1399 Willow Road would be demolished. As a separate future project, the environmental analysis considered reconstruction of the existing service station and an approximately 6,700 square foot expansion at the Belle Haven neighborhood shopping center (1401 Willow Road and 871-883 Hamilton Avenue) as a future separate phase that would require separate use permits and architectural control permits. These parcels across Willow Road are referred to as the Hamilton Avenue Parcels. The Hamilton Avenue Parcels are zoned C-2-S (Neighborhood Shopping, Restrictive).

The Final EIR pursuant to CEQA was released on Friday, October 14, 2022. The Final EIR identifies significant and unavoidable impacts in the following topic areas: air quality and noise. The Final EIR identifies potentially significant environmental impacts that can be mitigated to a less than significant level (LTS/M) in the following categories: Air Quality, Energy, Greenhouse Gas Emissions, Noise (Operational), Cultural Resources, Tribal Cultural Resources, Biological Resources, Geology and Soils, Hydrology and Water Quality, Hazards and Hazardous Materials, and Transportation. The Final EIR identifies less than significant (LTS) environmental impacts in the following categories: Land Use, Aesthetics, Population and Housing, Public Services and Recreation, and Utilities and Service Systems. Previously a Notice of Preparation (NOP) was released on September 18, 2019, and included a public review period from September 18, 2019 through October 18, 2019 to solicit comments on the scope and content of the Draft EIR. In accordance with CEQA, the certified program-level ConnectMenlo EIR served as the first-tier environmental analysis. Further, this EIR was prepared in compliance with the terms of the Settlement Agreement between the City of East Palo Alto and the City of Menlo Park. The Draft EIR circulated for a 45-day comment period from

Friday, April 8, 2022 to May 23, 2022 and the Planning Commission held a public hearing on the Draft EIR at its meeting on April 25, 2022. The Final EIR includes responses to all substantive comments received on the Draft EIR. The project location does not contain a toxic site pursuant to Section 65962.5 of the Government Code. ([Staff Report #22-056-PC](#)) ([Presentation – ICF](#)) ([Presentation – Staff](#)) ([Presentation – Willow Village](#))
[Additional comments received after staff report publication](#)

Staff Comment: Planning Manager Kyle Perata highlighted the elements of the Planning Commission’s set of recommendations to make to the City Council through a resolution as shown in Attachment A in the staff report.

Chair DeCardy said public comment was officially closed but noted the extenuating circumstance of the continuation of the item from the October 24, 2022 meeting. He said a number of members of the public spoke previously but might not be present this evening and he thought members of the public unable to attend that meeting might be present at this one. He said it was an important project and it was important to hear as many voices as possible while considering the time needed by the Planning Commission to fully consider the item. He recommended reopening public comment but asked if persons who previously spoke would consider only speaking again if they had a different comment than previously made by them. He recommended limiting public comment to two minutes as done previously. He asked for input from Commissioners about this approach.

Commissioner Riggs suggested not allowing those who spoke before to speak again. Chair DeCardy said opening for public comment meant anyone who wanted to speak had to be allowed to. Commissioner Riggs agreed.

Chair DeCardy reopened the public hearing on Item F1, from the Planning Commission’s October 24, 2022 Agenda.

Public Comment:

- Matt Regan, Senior Vice President of Policy, Bay Area Council, said they represented about 300 of the largest employers in the Bay Area. He said their work scope focused on the economy and quality of life in the region, and housing affordability and availability was key to them. He said their social policy focused on equity, environment and economy, and the project delivered in all areas. He said on equity that the project had 320 Below Market Rate (BMR) units. He said on environment that the project closed the housing/jobs imbalance and reduced vehicle miles traveled (VMT). He said this type of project was critical to keep the region’s economy moving. He said his organization urged the support of staff’s recommendations and to move the project forward as quickly as possible.
- Bryan Shields, Field Representative, Carpenters Union Local 217, said Willow Village was an opportunity for the City and that a project of this magnitude done correctly would be an asset to Menlo Park for years to come. He said the Willow Village team had worked with the community for over four years to ensure that the project would fit the needs of the community. He said the project would deliver 312 homes at low income levels set aside for seniors and further serve seniors’ needs in prioritizing dining and retail including a nearby pharmacy. He said the prioritization for dining and retail also provided small businesses a new foothold in Menlo Park. He said the Willow Village team also committed to using a local carpenter union workforce. He

said that commitment meant the developers were not only invested in the development but also were invested in the community.

- Sean Reese, Field Representative, Local 217 NorCal Carpenters Union, said he represented over 1200 carpenters in San Mateo County. He said he was expressing support of the Willow Village Master Plan project. He said it would provide good paying jobs and be built with union labor, and would provide local jobs for years to come since local businesses would be prioritized for retail and dining. He said building with union labor would give the workforce a livable wage and health care, both vitally important in the Bay Area's current economy. He said being built with an apprenticeship trained workforce meant the construction would be of the highest quality with attention to detail, and would give the community something to be proud of. He said for Belle Haven neighbors it would provide a direct connection to retail services making it easy for residents to utilize the new full-service grocery, pharmacy, cafes, and restaurants as well as community gathering areas like a town square. He said projects like this lifted up the community while they were built and sustained the community for years after being built. He said he supported the project just as he supported his community and asked that the Planning Commission support it too.
- Alexander Melendrez said he was the Organizing Manager for Peninsula for Everyone. He said he strongly supported Willow Village and while he could speak to Regional Housing Needs Assessment (RHNA) or the Housing Element that he had realized that he had never seen a project that had added this amount of housing in one proposal. He said his organization endorsed it as it would provide a significant number of homes toward solving the housing shortage. He said it also had the widest variety of types of housing he had seen in both make up and affordability, noting the deeply affordable and extremely low-income homes that could help their vulnerable community members. He said this opened up a new community for those folks to stay in Menlo Park and that was inclusion and diversity. He urged approval of the proposed project.
- Steve Olivares said he was a journeyman carpenter with Local 217 and was excited about the proposed development. He said it would provide work for the next couple of years for building and construction and sustainable work in the community. He said it was exciting to have work going up near where he lived and to be able to show his kids what they do. He said it was an exciting development that was proposed.
- Ken Chan said he was the senior organizer with the Housing Leadership Council for San Mateo County. He said they worked with communities and leaders to produce and preserve quality affordable homes. He said on behalf of his organization, he wanted to reiterate their support for the project noting that of the 1700-plus homes 312 would be affordable with 119 of those set aside for senior community members at both the very low and extremely low income levels. He said the last would be constructed with an affordable housing developer with 40 plus years of building and operating affordable homes throughout the region. He said those homes with the newly proposed \$5 million additional funding for affordable homes would provide Menlo Park with much needed relief against the jobs/housing imbalance. He urged the Commission to take the necessary steps to move the proposal forward toward reality.
- Ali Sapirman, Housing Action Coalition, said they strongly supported the Willow Village project. She said she echoed previous comments and it was really important that they moved the project

forward without any delays as it had been under consideration going on for five years. She said they had to make efforts to produce housing and not continue to be behind in producing housing stock. She said this project did an incredible job of doing that and specifically in increasing the City's affordable housing stock. She urged the Commission to take the actions recommended by staff to move the project forward to the City Council for approval.

- Jordan Grimes, Peninsula and South Bay Resilience Manager for Green Belt Alliance, said they were an environmental nonprofit organization dedicated to helping create climate resilient sustainable communities throughout the Bay Area. He said they strongly supported Willow Village. He said Menlo Park was badly in need of infill housing and Willow Village provided that abundantly. He said they were excited about the high number of extremely low income units and the use of union paying labor. He said regarding sustainability that amenities like the grocery store and community serving retail would be a huge benefit to all in Menlo Park and that new walking paths and bike lanes would help reduce VMT. He said the 2-acre park and significant new green space was deeply important as well. He said for the Healthy Places Index that Menlo Park had between 15% to 30% tree canopy coverage overall while Belle Haven was at just 7% tree canopy coverage. He said UC Berkeley's local climate policy tool showed that increasing Menlo Park's supply of dense infill housing was the single most effective strategy the City could employ to reduce greenhouse gas emissions. He said the proposal was a really strong project and Green Belt Alliance urged the Commission to support it.
- Colin Bookman, East Palo Alto, said he lived in the Kavanaugh neighborhood directly next to the development and in addition to comments he made previously, he wanted to reaffirm his support and urge the City to approve the project to prevent delay. He said the project would bring many needed necessities and was critical to the future of Belle Haven and East Palo Alto residents.

Chair DeCardy closed the public hearing.

Commission Comment: Commissioner Riggs said he wanted to continue with his questions from October 24. He said in the approvals that reference was made to "up to" 1730 housing units and "up to" so much retail square footage, and grocery store, and so forth. He asked if potentially that meant 50 percent of those maximums might be built and would satisfy the development agreement (DA). He asked if staff could clarify whether there was a minimum percentage required.

Mr. Perata said in referring to "up to" they were setting the maximums being evaluated for the project. He said if something was not built fully and the question would be what specifically were they looking at in terms of the DA's requirements. He said if it was a negotiated community amenity that would be different from whether a certain number of square footage of retail, office or housing were not built, noting a different process through the CDP for potential modifications needed for the overall project.

Commissioner Riggs asked when the City Council made the agreement with the applicants what the minimum number of housing units and hotel rooms would be expected, and what was the minimum grocery store size that would be expected. Mr. Perata said the grocery store did not have a minimum size component but had parameters in terms of being a full service grocery store. He said a space was shown in the plans and they had an evaluation of it based on certain square footage, but it was more about the services provided by the grocery store. He said they studied 193 hotel rooms but that was not in the DA that it had to be built. He said there was a component of the project that

included the hotel. He said if not built that space would go through a process so it could be used for something else. He said in the DA there was gap funding for the hotel so if it were not built that funding would be provided to make up for the difference in revenue. He said that was not the total transient occupancy tax (TOT), but the cost to the City in terms of providing services and that would run with the term of the DA.

Commissioner Riggs said he thought it evident from the fiscal impact analysis (FIA) that the City was fiscally counting heavily on the hotel's construction. He said the FIA indicated a running deficit if there was no hotel and no TOT. He said for example the City received about 1% of sales as its share of the sales tax and as he recalled the past occupant of this particular property had been a major sales tax generator and source of income for Menlo Park. He said that TOT / hotel tax would be approximately 12 times greater revenue. He asked if the gap funding was designed to cover a term of several years and then if there was no hotel whether the project would revert to a net cost to the City for the increased services.

Mr. Perata said the gap payment being referred to would commence and go for a period of years until the hotel was constructed. He said at some point if the hotel was not constructed there would need to be a reevaluation through City staff and the applicant on the use of the space and potentially continuation of the gap payment if the new use would not have a fiscally net neutral effect for the City. Replying further to Commissioner Riggs and whether this was considered a 30 or 100 year project, Mr. Perata said it was a long term project and not necessarily 30 years.

Anna Shimko, City Attorney's office, said the gap payment would come into play after the third office building's certificate of occupancy. She said at that point if the hotel was not started, the gap payment would go into effect to cover the indicated gap in the FIA. She said within two years after the last office building was built and if the hotel still had not been built the City and applicant would meet to try to determine an alternate use for the parcel where the hotel was intended. She said discussion would include what that would mean fiscally. She said the gap payment went for the life of the DA. She said even if there was no extension the applicant had agreed that if the gap payment was in place and if the DA terminated for some reason the gap payment would continue for the full 17 years.

Commissioner Riggs said that the payment would continue 17 years if there was no hotel was great. He said actuarially they were looking at 55 years but he thought that the development once built was probably good for 100 years. He asked about year 18 and if the City would still support sewer, water, library and recreation and parks services. He said should the hotel not get built and the new proposed beneficial use would be an active park targeted for adolescents that would provide a positive use for a wide variety of people including the neighborhood and would reserve the land with no foundations for some future decision that would have a fiscal impact. He asked Ms. Shimko to address. Ms. Shimko said the discussion would not occur at 18 years or the end of the term, but that conversation would occur two years after the final certificate of occupancy for the sixth office building. She said if they got to the point where the hotel was not developed and the City had been receiving gap payment and were 17 years out that if the applicant did not have another use at that point it would be status quo. She said using Commissioner Riggs' example for a use without a lot of revenue her expectation was that would be something the City would take into account in discussions with the developer on what the alternate use might be, and if the City was going to allow an alternate use that did not fill the gap, how would the gap be filled. Commissioner Riggs said they

would not actually have in writing a point of leverage to make sure after 17 years that they filled the gap and fiscally. Ms. Shimko said that was correct that there was nothing in writing on that.

Commissioner Riggs referred to the collaborative space and asked if it was counted as office space. Mr. Perata said it was considered office use for the purpose of the zoning ordinance. Commissioner Riggs asked if it was counted toward generation of housing and traffic impacts as well. Mr. Perata said the analysis was based on the office square footage and also took into account employment within the office campus district.

Commissioner Riggs referred to housing impacts and asked if they were to look at the three projects previously approved for buildings 20, 21, and 22 and with the 1.3 million square feet of this project whether that was somewhere around 3.2 to 3.3 million square feet. Mr. Perata said he thought that was in the ballpark for gross but had not run those numbers. He said it was important to look at the net. He said on the Willow Village campus the nonresidential increase was 800,000 square feet over the 1,000,000 square feet that was currently there or 1.8 million square feet in total. He said the collaboration space had a much lower employment density than office. He said the project was still within the office square footage for gross FAR and FAL zoning but by different use. He said on the Campus Expansion project he did not have the numbers for building 20, but for 21 and 22, it was new square footage of 962,000 but only a net increase in 127,000 square feet. He said there was net and gross square footage and the gross was in the 3 million square foot range.

Commissioner Riggs said not counting building 20's gross square footage they were in the 3 million square foot range. He asked if building 20 was a 500,000 square foot building. Mr. Perata said it was about 500,000 square feet. He said he was trying to research now but did not have the existing square foot numbers for that building. Commissioner Riggs said the net increase was somewhere between 1 to 1.5 million square feet. He asked how they would calculate housing demand for 1.3 million square feet of office. Mr. Perata said they prepared a housing needs assessment (HNA) for Willow Village and one for the Campus Expansion project. He said for this project in its entirety and not just for the commercial non-residential component it created a gross increase in housing demand of 2,545 for the region. He said for the Campus Expansion project with the change of use from lower intensity uses to office that resulted in a net increase in housing demand in the region of 3638 units.

Commissioner Riggs noted about 6000 units as additional demand. He asked if that was gross demand. He asked whether these were expected as Menlo Park residences. Mr. Perata said it was gross demand and the numbers he was speaking about did not capture the fact that the Willow Village project had residential. He said the gross increase in demand for units was regionally within commuting distance. He said housing was a regional matter and many decisions went into that including availability, where people wanted to live, where they wanted to commute from so those units would be distributed throughout the region. He said there was a calculation using census data and some project specific data of kind of a live/work in Menlo Park percentage, which was roughly 7% or so. He said it was 6% for non-Meta employees and about 7-plus % for Meta employees and referenced in the HNA.

Commissioner Riggs said he had seen in some emails on the project a reference to a net housing deficit of 800 units. He asked if that was based on the gross demand of 2545 minus the provision of almost 1740 units on the project. Mr. Perata said it was the 2545 units in the region minus the project's added housing of 1730 units, which was the regional deficit of 815 units.

Commissioner Riggs said he would like to look at one metric and referred to the regional demand of 6000 units they had come up with between Willow Village and the Campus Expansion project. He said he was raising this as they were no longer able to count demand within their City and housing in their City, even if they wanted to. He said they lived in a development world defined possibly more than anything by RHNA. He said RHNA then turned it back around to the City saying people might not be living in your city but you were generating jobs and were part of the regional mess so you will come up with your share of housing. He said effectively it was the City's responsibility whether the deficit was regional or not. He said he did not recall what caused the big jump for the coming RHNA cycle. He said Meta had basically established a range of 6000 dwelling unit load as a result of development, if Willow Village was included. He said he thought this disputed the larger number noted in a piece of correspondence.

Commissioner Riggs checked in with the Chair about continuing with questions for the applicants, and said he had a conversation with the applicant the other day to preview the four or five questions he had for them. He referred to the request for waiver for bird-friendly glass on the collaboration space, which he did not understand.

Eric Morley, Signature Development Group, said the EIR analyzed the project under the City's bird safe design requirements. He said they spent considerable time highly focused on bird friendly and bird safe design by working with expert biologists and under the guidance of the American Bird Conservancy, the leading experts in bird safe design. He said the EIR also looked at the City's bird safe requirement. He said under CEQA it analyzed a bird safe design and concluded that with their project measures and mitigation measures that impacts would be less than significant. He said it also looked at the bird safe guidelines of the City. He said those guidelines allowed for and contemplated waivers. He said when there was a waiver it was also coupled with an alternative measure. He said there was no waiver requested for the glass component of the atrium and 100% of that would be fritted. He said there were certain areas where they had alternative needs and the City's ordinance allowed for consideration of waivers when there was an alternative method. He said the EIR reviewed and was peer reviewed by the City's consultant and concluded that with those alternative measures such as additional fritting, fins and other component features that those impacts would be less than significant.

Commissioner Riggs said that resolved the question for him. He referred to Article 8 in the DA that discussed changes to the project and the process Planning staff would use. He said deviations were in Section 8.5 of that article. He said it was important to the Commission particularly when it came to review of particular buildings and what changes might be made under Article 8. He said this was based on experience with much smaller projects where sometimes the changes were things that would have altered the approval of a design review for example so perhaps the changes might also have an effect on this master plan. He asked how Article 8 changes would be administered and if the Planning Commission would be involved.

Ms. Shimko said Article 8 allowed for administrative amendments of project approvals. She said if there was a minor modification to the project the City Manager's office or City Manager could find that the change substantially conformed to the project approvals and approve administratively. She said the City Manager might find the changes to be outside the approvals and require a discretionary approval which would include Planning Commission review..

Mr. Perata said with the application of the CDP there was a section 8 in it that identified certain processes for different levels of changes to the project including staff level notification of the Planning Commission, or Planning Commission review of revision, or ultimately an amendment. He said they had used similar processes for changes on other CDPs and found those implementations worked pretty well.

Commissioner Do said the design of the buildings and spaces between them were well and thoughtfully designed for humans and was architecture at its best. She said they had heard that the full service grocery store, pharmacy, dining options and housing in particular affordable housing, outdoor space and job opportunities were wanted by the community. She referred to Commissioner Riggs question about the “up to” language in the DA. She said staff had covered well what that meant in terms of the hotel and grocery store but she did not hear what the reply was about “up to” in the context of housing units.

Mr. Perata said “up to” was the maximum 1730 housing units that the project was held to. He referred to requirements in terms of the phasing in the DA to require certain amounts of housing units as the project progressed. He said that was key to answer the questions in that while there was not a minimum requirement for housing there were minimum requirements for certain phases to move to the next office building development including a specific number of housing units that had to be under construction. He said there was timing for community amenities like the grocery store in terms of development timing for office buildings. He said Exhibit D to the DA showed that phasing in more detail including timing for office buildings in relation to milestones about completing certain components of the residential. He said continuing you saw the unlocking of additional office based on the residential projects’ construction start and milestones with those buildings.

Commissioner Do said she would look at that and if she had questions about that she would come back to it later. She said her next question had to do with kind of environmental justice and air quality. She said she understood that the only significant and unavoidable impacts related to air quality was from the reactive organic gases coming from consumer products and that air quality impacts from construction and increased traffic with mitigations were less than significant. She said even with being less than significant she imagined an increase of stuff in the air from those two things.

Mr. Perata said effectively the thresholds of significance for particulate matter and such were not exceeded there. He said there would be an increase related to the construction activity and operations but would not exceed a threshold adopted by the Bay Area Air Quality Management District (BAAQMD).

Commissioner Do asked about air quality monitors. She said she thought a few were set up in Belle Haven and still operating and that the applicant would also provide funding for air quality monitoring. She asked if those readings had been made available or would be made available to the public. Mr. Perata said the data for the air quality monitoring done alongside the project analysis were available in an appendix to the EIR. He said those monitoring stations had been taken down. He said in terms of the future a DA component included funding for an air quality monitor and noise monitor to be located in the Belle Haven neighborhood. Commissioner Do asked if the readings from these would be available to the public perhaps in a more accessible way. Mr. Perata said the intent of having the monitors would be to provide more data if not explicitly written that way in the DA.

Commissioner Do said she had questions on the glass atrium and not regarding the bird safety as that had been well covered but about the effects of light. She said she assumed that even with the frit pattern the glass atrium would still glow and make light. She asked how the 80 foot light trespass was measured. She said she assumed the nature of light was a little fuzzy and would not end abruptly at 80 feet and might reflect off surfaces, walls, the nearby paved pathway or the adjacent storage building. She asked also about the cumulative effects of light in the context of this project and all the other development in the Bayfront Area on the sensitive wildlife habitat. She said the lights were to be turned off at 10 p.m. or midnight for events. She asked if that had some flexibility so if monitoring showed that was too obstructive that times could be adjusted, or adjusted for seasonal variations.

Mr. Morley said lighting was analyzed in the EIR and they spent considerable time with the design team. He said there were a series of very strict mitigation measures and that the City had regulations on down casting of light and light dispersion. He said the project would comply with all of those. He said as their final lighting plan was being completed that would be reviewed by City staff for compliance.

Eric Harrison, Signature Development Group, said he had only one point to add and that was when they were adjacent to the sensitive habitats they had to make certain that they did not have spillage of light greater than 80 feet. He said based on recommendations from their project biological consultant they were required to implement dark sky lighting measures to make certain they did not have any light spillage into the sensitive resource areas.

Commissioner Do asked about the seasonal change in light and the times at which lights were turned off and whether that time was flexible. Mr. Morley said the times of 10 p.m. and midnight were established in the mitigation measures. Mr. Harrison said they understood that their biologists looking at that considered those reasonable times even considering the seasonality of when daylight savings was implemented.

Commissioner Tate referred to construction times mentioned and its disruption and asked how that might be mitigated for Belle Haven and East Palo Alto neighbors considering the number of years that would occur and that residents would only have a small window on Sunday to rest and have quiet. She said the construction hours were far too long and asked about mitigations for that. She also asked whether there was a way to start phasing in the grocery store and some of the housing much earlier in the project. She said most commenters mentioned something about housing importance and that was very important to commissioners. She said housing and a grocery store were dire needs for the community.

Mr. Morley said they had considered the construction related noise and hours and in addition to the EIR review they recognized the importance of minimizing inconvenience. He said they recognized the project would take time but the EIR had also included a number of measures related to noise. He said in the EIR related to noise it was important to note that construction was not getting noisier. He said the City had utilized proposed guidelines from the BAAQMD and others related to noise and they were focused on that. He said a number of measures required would be implemented before the project started including a noise barrier and eight foot fencing surrounding the project space. He said construction equipment would be required to be fitted with both mufflers and sound control. He said stationary equipment would be focused away from sensitive receptors and construction vehicles idling would be limited. He said one of the requirements and mitigations of the project was

to have a detailed construction and noise mitigation and management plan. He said the noisiest activities would be conducted during the day and quieter activities such as interior work or tenant improvement work that were not as ambient from a noise perspective later. He said they were experienced in working well with neighbors on these concerns having had done numerous similar projects over many years.

Mr. Morley said regarding project phasing that early on they had had the grocery store in Phase 3 and heard strongly from the community that they wanted it sooner so they moved it to Phase 1. He said that they also cut office square footage by 30% and employee capacity by 30%. He said they moved both the grocery store and housing into the early phases of the project. He said parcel 2 that included the grocery store and significant residential would be one of the earliest buildings to start as well as the elevated park and collaboration space. He said those were the longest buildings and structures to build so they would be started early and after which they would progress forward with a combination of housing, amenities, and office throughout the project to balance. He said there were triggers in the DA that required delivery or commencement of construction and phasing to move those more quickly. He said they wanted to go as fast as they could and essentially move the project to a single sequential project versus multiple phases.

Commissioner Tate said that the grocery store and housing was in an earlier phase did not really provide a timeline. She said she recalled from the October 24 meeting that this would happen in about two years. She said that might be early in the project but for people supporting and really wanting that to happen and supporting the project because of the essential amenities it was not. She asked if they could help her understand whether the grocery store could occur the first year. She said two or three years out though early in the project was still far out for those living in the area that had been supporting really because of the grocery store.

Mike Ghielmetti, Signature Development Group, said they had to do demolition and install infrastructure and nothing else could get done until that happened. He said they were sensitive to resiliency and climate change and would be building the site up to make it more resilient. He said the existing infrastructure was 50 to 60 years old and needed upgrading and replacement. He said they were not avoiding the grocery store, residential or office but that they had to lay the foundation before doing anything else. He said at that point they would be starting the elevated park just because sequentially that was from where they would be drawing utilities. He said they were anchoring that with the meeting and collaboration space and then moving immediately to office and residential in parcel 2 and that included the grocery store. He said the residential and grocery store would start within two months of the first of the office buildings and so was occurring together. He said the reason it was "year two" was it happened after the foundation including grading and infrastructure.

Commissioner Tate said it appeared in the staff report that the elevated park and tunnel were pending approval. She asked about efforts to get approvals for those to be realized and connect Belle Haven or whether those items were just a wish list.

Mr. Ghielmetti said regarding the elevated park there were two components. He said one portion functionally which was like 90% of it was inside the project and he did not think there was any issue there. He said the part that was outside the project and would functionally connect to Belle Haven would go over Caltrans' right of way and that was the outside approval they needed. He said Menlo Park could or could not approve the project but had no authority over Caltrans.

Mr. Harrison said they had done a fair amount of investigation and identified where utilities and all existing infrastructure including various other encumbrances were within Willow Road where they planned to bridge. He said they had a very high level discussion with Caltrans regarding a proposal for the tunnel for the elevated park. He said they had a fair amount of improvements they would make to Willow Road for example and relocating the alignment of the Hamilton/Willow Road intersection. He said it was not until the project was approved that they could start to move forward and engage Caltrans to advance the necessary permitting for those improvements.

Mr. Ghielmetti said that was one of the reasons why in discussions with the community amenities subcommittee of the City Council that they were encouraged to have a shuttle to connect. He said with or without the other improvements being sought, they would still agree to put in the shuttle to help that connectivity.

Commissioner Tate said it sounded like discussions had occurred about the underground, the tunnel also, and it was a matter of permitting to be able to move forward with trying to secure those. Mr. Morley said they could not advance to that next step until the base approvals were completed. He said they had a long-standing positive relationship developed over decades between their teams and Caltrans. He said they had to, as Commissioner Tate noted get to that next level with them but had already had ongoing discussion. He said they were confident in their ability to move forward

Commissioner Tate referred to the senior housing and noted she had met with Mr. Morley and a few others from the applicant team in 2019. She said at that time the conversation was that the senior housing was in addition to the 15%. She said now it appeared that it was not in addition but the 119 units were part of the 15%. She asked why more units were not being set aside for BMR.

Mr. Morley said the application before the City had been the same since its inception and included 120 BMRs as part of the overall package. He said if there was miscommunication on their part or misunderstanding they apologized.

Commissioner Tate referred to the senior housing and asked if it had been considered to offer more than studio units. Mr. Morley said Mercy Housing, their partner, was working collaboratively with them to define what was the optimal to serve seniors in those 120 units.

Paul Nieto, Signature Development Group, said initially they had the application for the senior housing predominantly in studios. He said they had a dialogue with Mercy Housing and they were looking at having at least 10% as one-bedroom and studying the potentiality of more. He said a lot of it related to how deeply they could get to in affordability as in adding bedrooms the rent increased. He said it was a comment they had heard strongly so they were working with their partner to optimize what the right balance was. He said they understood that the initial estimate of all studios was likely over-aggressive. He said they were trying to figure out in a good way how to get the lowest possible rents to the most people but they understood there were not enough one-bedrooms in the mix so that was being studied.

Commissioner Harris said they heard at the October 24 meeting that the applicant really needed to get approval done by the end of the year and asked what was critical about December 22 other than the fact they had been working on the project for a long time, which she acknowledged. She also asked when they expected to begin construction of the project. She said they could not really talk

about when the grocery store would be finished unless they had an idea of when the applicant might begin the project. She said Meta had lost market cap and was letting go of leases and notably including in the Bay Area. She said many had concerns whether this project would really get built and what insurance they had it would. She asked about their timing and when they planned to start and about Meta's commitment to the project.

Mr. Morley said Meta had been public about its evaluation of the future of work and how it related to its office footprint. He said they remained very committed to well-located, high quality, highly-amenitized space, and the Willow Village vision fit that well. He said Meta was also in a critical phase of evaluating their office strategy for the future. He said they needed clarity and certainty from the City by the end of the year which meant having approvals in place for this vision so they could deliver those amenities and to plan for the future on a project that had been done in collaboration with the City and community over the last five years. He said Meta was at a critical time in their future planning of their space and to advance they needed both certainty and predictability from the City in terms of those approvals so they could plan accordingly.

Mr. Ghielmetti said more broadly there was hybrid work, remote work, and partial work but one thing had been fairly consistent noting he was not speaking specifically to Meta but in general and that was what he would call a flight to quality. He said he thought the offices that were losing ground were Class B and Class C offices. He said if employers were going to be calling employees back to the office one day a week or five days a week that they were wanting to create different types of environments with a lot of collaboration space, hoteling space, a lot of meeting space, conference space and the right kind of floor space. He said they wanted the right kind of interaction and the right kind of experience for employee retention and recruitment. He said the reason they wanted to get to a place here was because Meta had been wanting to figure out how and when to be able to utilize this space and could not without a project approval. He said Meta needed to know if they would be able to use this site or not. He said they would like to use it and the commitment was there as they had not gone away through the entire process. He said they had been public and it was in the news that they had dropped leases but that meant they had needs for other offices. He said in general this was high quality office space and high quality atmosphere with mixed use of parks, transit and proximity to their other campuses.

Mr. Morley said regarding Commissioner Harris' question about next steps and in answer to a portion of Commissioners Riggs and Do's question about "up to" that the Commission had seen on a number of occasions over the last five years the architectural control plans in study sessions in advance of Commissioner Do's arrival. He said all those architectural control plans and what they sought to actually build had been on file with the City. He said they anticipated immediately post approval to complete the additional regulatory outside agency approvals with Caltrans, SFPUC and other agencies. He said in queue and as soon as the schedule permitted they would have the architectural control packages come before the Planning Commission for approval so they could implement the vision. He said he wanted to stress the critical importance of City Council action this year, which needed Planning Commission recommendation this evening.

Commissioner Harris said what she heard was that Meta was really interested in figuring out what the right mix of office was. She said what was nice about this project was it had amenities that might induce workers to want to come back to the office. She said she might not have heard what their thinking was as to when construction would begin.

Mr. Ghielmetti said they had several months of architectural control planning and next year doing master improvement plans. He said the infrastructure had to be designed including sewer, water, roads, grading and drainage, maps and the architectural packages. He said in addition and at the same time as that they needed Caltrans and SFPUC's approvals as well as other outside agencies such as PG&E and West Bay Sanitary District. He said they would be making their best efforts to get all of those approvals in hand so they could start the project as soon as they could. He said he did not have an exact date noting that if they got the go ahead today it would probably be in the 12 to 15 months range before construction could start.

Commissioner Harris said she understood but the community and she as a commissioner wanted a sense of when the grocery store would be built. She suggested that if they received approvals by the end of December for entitlements then three months to get all the other things he discussed meant the infrastructure work would begin perhaps the earliest March 2024.

Mr. Ghielmetti said March or January and yes theoretically. He said they had already gone at risk in this having the architectural control package ready to submit and they had some of the improvement plans and mapping done as they were anxious to move forward. He said a company could not make a promise to an exact time as there were a number of circumstances but through all of the ups and downs of the pandemic and the markets, they continued to be present.

Commissioner Harris said she was not trying to pin them down but to get an idea. Mr. Ghielmetti said it was probable in 12 to 15 months to start and then probably nine to 12 months of infrastructure before vertical development started. Commissioner Harris said if they started Q1 of 2024 and started infrastructure at Q1 2025 the grocery store would start by midyear 2025 and the grocery store would be completed in six years. She said she was going by what they said, and the illustrative construction phasing schedule shown on page 431 of the original Planning Commission agenda packet.

Mr. Morley said at their own risk they had completed the architectural control packages and were ready to advance those. He said they were doing their construction drawings at risk. Mr. Ghielmetti said almost all developers typically waited until a planning commission and council were giving them more feedback before starting those drawings because they were quite expensive. He said if they were not approved that they were out that money.

Commissioner Harris said she appreciated them doing that. She said she was just trying to get a sense of what they were looking at. She said it appeared that year-six, Q1 would be when the grocery store was built but not when it was operating. She said she would assume that they were looking at maybe six years until the grocery store was open and that was assuming they did not have to extend the DA. She said as Commissioner Tate noted the grocery store and the BMR units were basically the two reasons the neighborhood was supportive of the project. She said she reviewed what they had heard from people and they had probably 75 commenters in the last couple of weeks give feedback, and in particular what people who lived in Belle Haven had said was most important to them. She said if those commenting had not included what was most important to them that she had emailed them to get the sense of why they supported the project. She said she wanted to review what one person said, which was: "I look forward to being able to take my girls, currently 4 and 7, to the grocery store walking. That would be a great picture, taking a small cart or wagon, and just stocking up, or taking my girls to check out the retail stores and quick service restaurants, or maybe a night out on the town square." She said someone else said: "I'm most excited to have a

grocery store close by and access to dining. We feel isolated in Belle Haven when it comes to dining or getting our necessities.” She suggested maybe the City could negotiate with the applicants in some way to get the grocery store and some of the BMR housing upfront and suggested that as a stretch goal to get the highly neighborhood desired features done earlier. She said the applicants did not start the problem of no grocery store but to the extent they could help the City help the community that would make a great project.

Commissioner Harris referred to the applicant’s comment about having office that workers wanted to come back to with different amenities and close to collaboration space and retail. She said her one concern was that tech companies often supplied meals to the workers and asked if the retail, restaurants and grocery were being set up for success. She asked how that might be balanced so that on some days workers might have to get their own meals.

Mr. Morley said they had thought critically about that and the critical importance of the retail being successful. He said as part of the office campus culinary and other space those had been far more limited than they would otherwise be in other campuses. He said the whole program within had been reduced and modified even in terms of the offerings to enable the local businesses to be successful and enable that walkability to the retail. He said it was not just whether the campus was exciting but the environment around it and both for the community and the workers.

Mr. Ghielmetti replying further to Commissioner Harris with specificity said the amenities on campus were reduced about 20%. He noted the huge density on the site of workers on this campus and the other ones and leakage from that to go to other eateries at this site. He noted also the population density of residents in the units above. Commissioner Harris noted though the draw of a free lunch and suggested there might be other incentives by Meta for workers to get meals elsewhere but she did not know what that would be. Mr. Ghielmetti said that there were some foods they would not offer on campus such as sushi. Mr. Morley said third party food vendors typical of a campus or other things would be explicitly excluded in the campus as the place to get those would be Willow Village.

Commissioner Harris said looking at the community amenities that in the DA the piece missing were assurances those amenities would be operational. She said for example it stated the applicant was to basically build the grocery store, build the pharmacy, build the bank building and similarly the applicant promised to supply funding for air quality monitoring. She said she would like to see that those amenities were required as operational by the DA so they had a sense when the applicant had a better timeline as to when those would be finished and operational. She said regarding the air quality monitoring that rather than give the City the money it would make more sense if Signature could put the monitoring in sort of commensurate with the start of construction with the infrastructure. She said the amenities that involved the public should prioritize Belle Haven residents and that would include job training, teacher housing, and funding, which she thought could be easily incorporated into the DA.

Chair DeCardy said he had a question about project timing and asked for clarification on the trigger to allow the DA to extend from 10 years to 17 years and the completion of the grocery store and a certain number of housing units as to whether that meant those were operational, occupied and being utilized. He asked additionally why that level of housing was chosen as it was roughly half the total housing in the project.

Mr. Perata said the extension would be subject to the granting of occupancy for the units and not necessarily that they were leased and occupied, and similarly that the grocery store was built and the space had received its certificate of occupancy but that was not tied to the operation of the store. He said regarding the number of residential units that it was about halfway through the project so 865 units and the number referenced would be about half that and tracked with the phasing schedule.

Chair DeCardy referred to page 13 of the DA and a reference to a certain reduction essentially in the housing index by 4% over he thought four quarters. He asked if the project was in the middle of ten years when that happened whether it would automatically pause the project or extend the deadline. He said as he understood when it was no longer at that threshold the applicant would then have three additional quarters to begin construction again. He asked if that was at the applicant's discretion and whether they could decide that they were just going to keep going anyway.

Ms. Shimko said this was a force majeure clause that was pretty standard in DAs and had been made more robust as a result of the pandemic as they had discovered additional force majeure causes over the last few years. She said there had to be a delay in the project as a result of one of those force majeure conditions. She said if one of those conditions occurred, the applicant had to submit something in writing to the City saying there was a delay in the project caused by this circumstance that was one of the force majeure criteria in the DA. She said they were noting specifically that one of those was severe economic recession. She said originally the applicant had proposed a different measure of severe economic recession. She said the City sought advice from its economic consultant that suggested the housing index as shown in the DA before them. She said if any of the force majeure causes occurred and the applicant notified the City of that it would extend the term of the DA and concomitantly the project approval by generally a day for day delay except for severe economic recession that had the provision the housing index had to increase for three quarters before that condition would cease to exist.

Chair DeCardy asked if the project was approved and was at a certain point able to develop and the owner the sold it to another entity that was a developer with a different tax status such as nonprofit education or health institution how that would impact the City. He asked if there were any protections for the City in that circumstance. Ms. Shimko said the City Attorney foresaw that potential and included in the DA that before that entity pulled a building permit there had to be a pilot agreement recorded on the whole property, which would be a payment in lieu of taxes agreement so the City did not lose that revenue.

Commissioner Harris said she was looking at the phasing schedule and the best case was that the project started construction Q1 of 2024. She said it looked like the last set of residences would probably not be completed before the end of their RHNA Cycle 6 commitment. She said they received a letter from HCD saying they were really counting on those units. She said she wanted to register that as a concern. Mr. Perata said it was a possibility that all the housing in this project would be completed by the end of RHNA cycle 6 in 2031.

Commissioner Tate asked how many Belle Haven residents were being served by the JobTraining with the funding provided by Meta. She said they received a letter from someone who worked at JobTrain and they had the program director speak about their construction program, and a gentleman this evening who commented on being part of the apprentice program at JobTrain and

how much he appreciated the construction here. She also asked who the Community Hub had served as it seemed during the pandemic that most users were from west Menlo Park.

Juan Salazar said the Community Hub was not opened until Q2 of 2022 and through restricted guidelines because of Covid. He said for the most part people who had used the space were the Year Up training program they had on their campus and which had not been opened to a wider audience. He said they hoped to be able to open it to a wide audience in the next year once they were over some of the Covid restrictions on their campus. He referred to tracking of Belle Haven residents in the JobTrain programs and said that was not information he had right now but they could work with JobTrain and Year Up to get. He said the guidance they had provided them historically had been that they recruited from the Belle Haven neighborhood. He said the current DA had been written with that purpose in mind. He said it was a priority for them to serve Belle Haven residents. Commissioner Tate said she would definitely be interested in seeing how many people were utilizing the resources they were providing.

Chair DeCardy said regarding the EIR that he heard last time from the consultant that reduced parking was not helpful in addressing any potential environmental impacts from transportation. He said they found it was not helpful for two reasons with the first being what he called leakage. He said within the project if they reduced parking there were opportunities that someone could park offsite so that it was unknown if a car's pollution was avoided. He said he thought they said there was not great transit infrastructure so it was not immediately clear with public transportation where people would go if they decided not to drive. He said he thought those were the two reasons the EIR consultant had talked about. He said he also thought they said effectively that the same purpose was achieved with a really good TDM plan to avoid those same trips and that there was a TDM plan in place to make that happen.

Ms. Black, ICF, said that was a concise overview. She said also there was individual behavior and the question of how easy it would be for people to change their trips based on what the destination was or whether it was to a unique destination, or one where they could easily go to a different grocery store with better parking. She said Chair DeCardy captured the general concept in terms of the content of the EIR.

Chair DeCardy said that he had said in the past he would not vote to certify an EIR that did not adequately look at questions around parking. He said while there was not an alternative put in there he thought the answers to the specific questions raised in the draft EIR answered those questions for him. He said he was prepared to support certification of the EIR and the other associated items.

Chair DeCardy said he could imagine an EIR was indifferent to a TDM plan and building a lot of parking but he imagined that was a different question for a developer. He asked the applicant to remind them what the TDM plan was without great detail. He said he was trying to understand what they would provide Meta employees to get them out of single occupancy vehicles. He noted that Meta had been lifted up for many years as a shining example of moving beyond what many thought you could accomplish in a TDM plan and moving employees at a significant scale out of single occupancy vehicles.

Replying to Chair DeCardy, Mr. Morley said Meta might provide greater detail to his comments. He said that 50% of Meta employees were not traveling to work in a single-occupancy vehicle. He said Meta provided buses/shuttles, ride match programs, a variety of bike/walk infrastructure, wide range

of alternative modes, on and off campus bike program, last mile transit connections to get employees to critical transit infrastructure and with this project reduced office and worker capacity, applied trip caps and had shared parking. Replying further to Chair DeCardy regarding whether Meta offered or promoted alternative transit, Mr. Morley said if he lived in San Francisco and had the choice of driving his own vehicle to Menlo Park to work most likely in traffic or ride a quality efficient and comfortable bus, his preference would be the latter. He said Meta invested in these various programs to offer a quality, comfortable efficient transit alternative. Replying further about employee count, Mr. Morley said Willow Village had an employee capacity of 3500 workers and net addition as part of this project of 3450 workers. He said this assumed that more than 50% of those workers would not travel to and from work in a single-occupancy vehicle. He also said they could dramatically reduce parking for both the campus and residential with shared parking for example.

Chair DeCardy said they were not at the minimum parking number for the office and there were two parking garages and asked the total car capacities of those. Mr. Morley said the total office parking for 6900 workers was 3369 parking spaces. He said the south garage was 1318 spaces and the north garage was 2040 spaces. He said they introduced shared parking in the garage under the Town Square for both the hotel, Town Square and retail to further limit parking on the overall project. He said that it was critical to ensure sufficient parking for successful retail.

Chair DeCardy noted that the project had wonderful transit for employees but not for the community. He referenced the two parking garages and said one had over 1000 spaces. He said it was expensive. He said one thing that should be done was to explore a further ratcheted TDM plan and one that could be allowed to be utilized across all of Meta campuses and not confined to this one, that would reduce another 1000 trips for Meta employees. He said it was up to the applicants if they would want to do that and then request to not build that parking garage for the long range benefit of the community. He said the garage reinforced a barrier on that side of the campus because it was only accessible to Meta employees and was a pinch point for access for people in the community. He said not having it would reduce traffic and create more open space, which he thought would be a great benefit. He said he was not trying to put the applicants on the spot.

Mr. Morley said one element of feedback they received was expanded shuttle for the community and that was why they introduced a neighborhood wide community shuttle. He said the TDM plan was not isolated to this campus but deployed throughout all of the Meta campus. He said regarding the TDM plan Meta had pushed the limit in terms of the TDM plan. He said they understood where Chair DeCardy came from with that idea.

Chair DeCardy said the project had much to love about it. He said the concern for him was that it would not be a hub for a vibrant community across all of Menlo Park but an island oasis. He said regarding a shuttle and permeability that on one side the project had Bayfront Expressway, essentially a massive barrier, and moving to the right clockwise there was no way a person not a Meta employee would get access from essentially the top of the site where you would find some way to the elevated walkway. He said all the way down to O'Brien there was not a single way into the site. He said looping around underneath that were the life science developments as well as the Hetch-Hetchy portion of the cut through. He said right behind that was another barrier where the residents on the other side of life sciences could not get through walking and would have to go to Willow Road to walk around and then walk over and up. He said now they were on three sides of the project and the last side of it was Willow Road. He said that they decided for Willow Road to not work to limit the traffic on it and make it a village main street. He said they were not trying to seize

control and limit that access on Willow Road but to do some convoluted project to build something that kind of looped over it. He said this was his biggest concern and disappointment with the project as it would be very successful for Meta employees and the residents on that campus, but it would be in isolation. He applauded the shuttle but it was not enough and for community amenities the City had to do more than \$5 million for a shuttle and for the DA it had to be more than just saying to have a plan to monitor the shuttle. He said the difference was putting money into something which he imagined was in a shell that kind of circled around the community versus something actually built in, integrated and utilized by community members.

Mr. Ghielmetti said those were great observations but there were no easy answers noting land ownership constraints and regulatory constraints around them. He said they tried to create an inviting atmosphere and suggested comparing and contrasting that with what was there now, which was an old industrial site that basically turned its back on every side with one way in and out. He said they would have five ways in and out and were doing what they could on Willow Road to make it more modern. He said they did not have control of Willow Road and could not shrink it as it was Caltrans' right of way, a regional cut through, and it served a regional purpose. He said they included the shuttle service at the City Council's behest and thought it a great idea. He said with Caltrans' approval they could connect the overhead portion of the park so people walking could take an elevator up and have a beautiful experience walking over the top of Willow Road into the project. He said regarding the corner to the north that they did not control that corridor for any purpose but they made the lower left corner of the big park inviting and they had entrances for vehicles and pedestrians. He said it was impossible to do front on all sides of a project. He said some people drove and they were doing their best to minimize that but they had a life sciences business park on their right to the east and the SFPUC right of way to the south. He said those were regulatory bodies and ownerships they did not control and they were doing their best to make the project as porous as possible through architecture, uses, heights, massing and multimodal transit, which was why the inside streets' design were combinative of pedestrian bikes and cars. He said some of those streets were as wide as El Camino Real

Chair DeCardy said he appreciated the comments and recognized everything Mr. Morley said about the project. He said he appreciated the constraints. He said as a Planning Commission they heard the community's concerns and with every project those things were somebody else's fault then rather than having a wonderful sum of parts they were going to have these isolated parts, which was his concern. He referred to community amenities and said he believed there needed to be more investment in the shuttle. He said he had serious questions about how much of a community amenity the elevated park was. He understood it as a necessity due to what was built on the other side and how it needed to be able to modulate, but from a community standpoint having that as a walkway at grade would work as well as having it be elevated and would be a lot less expensive to build. He said the elevated park was half of the community amenity. He said if it was removed the community amenity would be down \$10 million which was funding he thought would better serve investing in connectivity through the shuttle and other means outside of the project in the ways he had mentioned.

Commissioner Tate asked about the relinquishment of Willow Road or portion of it to Meta noting she had read Google had had a street relinquished to it. Mr. Morley said they looked at that and one of the community amenities included was funding for a study for the City to be able to evaluate that. He said neither Meta nor the City had control over it, noting Willow Road was under the authority of Caltrans, East Palo Alto, and others. Commissioner Tate said she would fall back on staff at some

point to evaluate that but hopefully it was something that could happen and which had been a conversation within the Belle Have community since ConnectMenlo.

Commissioner Riggs referred to 2016 when ConnectMenlo was brought to the Planning Commission and at that time they recognized the housing, traffic and transportation challenges, He said with major projects they tended to use the EIR as the litmus test whether or not the overall benefits of the project outweighed the significant impacts, and if that was not within your legal jurisdiction then it was not your problem. He said ConnectMenlo did not get an approving vote from the Planning Commission primarily because there was not a transportation infrastructure in planning, let alone in place or funded that would handle the additional transportation demand from the ConnectMenlo rezoning. He said as much as he loved this project as an architect and would like to see it realized that as someone serving his community in a capacity of review he could not ignore the transportation and housing impacts. He said whether agreeing with RHNA numbers or not, those were law so the City was doing its best to put in housing in the hundreds for previous RHNA goals and in the thousands for the next RHNA goals. He read the items for recommendation by resolution to the City Council. He said to do this he thought they were asked to conclude whether the project benefits outweighed the impacts. He said the ConnectMenlo rezoning for office, life sciences and housing never had a balance to meet the housing impacts. He said as a Commissioner he was put in the position of approving 59-acres of a new village or as the Chair said a promising and wonderful looking island. He said he was very hard pressed to back a project for which they did not have the transportation infrastructure and that would significantly add to housing demand. He said this beautiful project might not be right for 2022.

Commissioner Harris said she was near the point of a motion or supporting someone's motion but she would have some additions to the DA and an addition to the BMR agreement. She referred to the grocery store rent subsidy. She said there was a paper Signature received and the Commission read that said it would take about 60 months for a grocery store to be successful. She said with the construction on this project and that not all the residences would be built before or at the same time as the proposed grocery store it was going to need more and suggested the condition that the rent subsidy should be for 60 months rather than 24 months. She said additionally in monitoring this project as it went forward as it would be such a big task that she was not sure they had the staff to do it. She suggested having someone as just the project manager for this and not having any other roles to move the project along whether that was a consultant or staff person. She said additionally she wanted to ensure that community amenities were coming on commensurate with the office spaces. She said she would not want a bunch of offices to get built but then not get the community amenities. She said some things were in place with that but she thought it could be made more transparent when it got to the City Council and that would be helpful.

Commissioner Barnes moved to adopt the resolution recommending approval to the City Council and that the commission might recommend slight changes to the items under the resolution. He said he supported the project in its totality, noting the economic vitality it would bring to Menlo Park, which was needed and more jobs, which he believed was good. He said he believed this office would get built somewhere – perhaps in Redwood City or Fremont or it could get built in Menlo Park. He said the difference was they had the opportunity from an owner/user to build here in Menlo Park and not only got the benefit of economic vitality but community amenities and all the jobs associated with the commercial space. He said whether it was built here, Redwood City or Fremont, Menlo Park would get traffic from it. He said this project was what ConnectMenlo intended and noted that the Planning Commission on ConnectMenlo voted 2-2, two votes for and two votes against

recommending approval. He said fortunately the City Council approved ConnectMenlo. He said the Commission was not a policy making body and when he considered the intent of ConnectMenlo which was the standing land use policy for this area that what was being proposed and asked for by the developer aligned with that. He said he found that the benefits of the project outweighed the negatives, and not the least of which was additional housing and economic vitality to the area.

Commissioner Riggs noted the recommendation for approval was for a master plan and not individual buildings. He noted the inevitability of some projects and traffic associated with those and referred to Commissioner Barnes' comments on that. He said he had his concerns and would share those when individual buildings came to the Commissioner for approval but in terms of what the request now was for the Commission that was to make a recommendation. He noted that this was an impressive project and the master plan was admirably well done.

Commissioner Harris said she would like to add conditions of recommendation, including

- Applicant should include a guarantee of operation of the amenities (the grocery store, air quality monitor etc.) and not just to build them;
- Grocery store rent subsidy for 60 months and not 24 months;
- More information from staff on the "up to" items mentioned with a minimum set for each item;
- For BMR, Signature would commit to providing 100% of the gap funds such that their commitment of funds to the senior housing would be equal to if that housing had been, or the cost, if that housing had been all inclusionary.

Chair DeCardy said this was a fabulous project as commented upon by others. He said he was supportive of the four items of recommendation posed by Commissioner Harris. He said for him to support the motion he needed:

- To have the schedule of amenities and requiring community facing ones occurred in some co-equal way with the office space and parking;
- A TDM plan that ratcheted up to avoid another 1000 single occupancy vehicle trips to Menlo Park and leave that to the discretion of the applicant if they would like to apply to not build the smaller of the two parking garages;
- To put \$10 million into the shuttle, connectivity and other ways across the island effect and not do the elevated park;
- For the City Council to review and update ConnectMenlo as its utility had come to a close; and
- If the study to relinquish Willow Road recommended moving forward that the City put the resources and staff, consultants and attorneys to finish that so the City could control Willow Road.

Chair DeCardy asked if the diesel generator would be approved as part of the master plan or be part of future Planning Commission review of building projects. Mr. Perata said the diesel fuel for generators would be part of the Master Plan approval and allowed for in the CDP and the generator themselves would go through the building permit process if they were located on the exterior of a building. He said there might be architectural review if they were integrated with an architectural control package at some point noting 12 generators now in the Master Plan.

Chair DeCardy asked if those were not approved as part of the Master Plan would the applicant have an opportunity to have those come back for considerations at a future date. Mr. Perata said the Planning Commission could recommend removing the use and storage of diesel fuel from the permitted uses and be administratively permitted. He said the fuel for generators in both the RMU and O zoning was administratively permitted. He said the Commission could recommend that was removed from the City Council approval to go through the administrative permit process for the underlying zoning ordinance.

Chair DeCardy said he would not press on this one but thought it was the right thing to do. He said when these buildings came for completion much more would be known about batteries as an alternative. He said it was a shame to use diesel but in the relative scheme of things and where they were he would not push it.

Chair DeCardy said with this project they had another applicant who was going to control the development of a master sign program which he thought was a mistake. He said that should be owned by City staff to have control of the consultant and control of what information was brought before the community for approval on a sign program. He said he would like to see that but it was not a must have for their recommendations for conditions. He said he agreed with Commissioner Harris' recommended four conditions and he had three he was recommending.

Commissioner Harris said she had one more that she was reminded of by the mention of diesel generators. She referenced letter I, the Bayfront Shuttle, in the list of community amenities and recommended that the shuttle be required to be 100% electric and to remove "if feasible as determined in developer's reasonable discretion" regarding that.

Chair DeCardy said he could support that.

Commissioner Riggs referred to the elevated park and agreed it was very expensive, but thought it was a very significant aspect to the project. He said he had walked the High Line in Manhattan and this one would be a good deal nicer. He said it likely would be used as community space. He said it was an all-important way to cross Willow Road. He said he pictured the shuttle, hopefully all electric, waiting and waiting at that traffic light to get across Willow Road. He said he did not see the shuttle as a quick thing to hop on to get to the grocery store or to the other amenities of the project. He said he would be supportive of all the suggested recommended conditions except for the elevated park one, which he hoped could be reconsidered.

Chair Decory said to clarify that it was a lovely solution to a problem and would be helpful as Commissioner Riggs said. He said he believed from a community amenity standpoint that this solved a problem for the developer but it did not solve the broader problem for connectivity. He said they needed additional funding and community amenities that supported connectivity and the shuttle program. He said he was not really commenting on the elevated park as much as noting the shuttle program would get \$5 million and he thought it needed \$10 million more for the shuttle or shuttle like connectivity.

Commissioner Tate said she was comfortable with the suggested amendments to the motion.

Commissioner Do referred to the very real concerns about infrastructure, transportation and connectivity as well as the overall benefit to the community. She said the suggested conditions made were thoughtful and that she could support those.

Commissioner Harris said she agreed with her suggested conditions and Chair DeCardy's and if Commissioner Barnes was as well then she would second his motion.

Commissioner Barnes said in clarification that these suggestions were directions to accompany the resolution. He said a number of those suggestions needed more discussion in terms of feasibility. He said as a conceptual direction he thought any of the suggested conditions could be taken to Council for consideration, but for them to have serious weight with the Council they needed expansion and refinement. He referred to guarantee of amenities operations. He said he thought they were looking for mechanisms to ensure the viability and the long term operations. He said he was uncomfortable with the word "guarantee" as he thought they were looking just for mechanisms and prescriptive language that spoke to the long term operation. He said he would rephrase it that way. He said regarding the increased rent subsidy for the grocery store that from the Commissioners' perspective none of them knew what the appropriate subsidy or duration should be so he would like further refinement of that. He said they could suggest a longer term for that subsidy to support an operable grocery store but not be prescriptive. He said regarding the "up to" that he agreed with minimums and for additional direction on what the minimum amounts would be and that minimum amount mostly being what's needed for project viability or what the City needed for viability from the project. He said gap funding was important and that should be equal to or greater than what would otherwise be BMR contribution. He said they had the discussion about land contribution by the developer and noted that there were complexities to the calculation that deserved attention. He said the idea of the community amenity lagging behind the commercial development was a worthy discussion as to phasing and appropriate delivery. He said there had to be critically massive bodies whether housing on site or at the offices to support the businesses' success. He said the question was the inflection point where you had enough critical mass.

Commissioner Barnes said he was not sure he understood Chair DeCardy's point about the parking garage. He said regarding the elevated park and proposed community amenities his question was what a tenant amenity was versus what was being built specifically for the community. He said it was a gray area. He said he would rather look at bumping up the shuttle monies as opposed to going after something else. He said regarding an update to ConnectMenlo that if that was appropriate, they should go for better land use policies. He said with the relinquishment study he agreed that action beyond the study itself would be the City's responsibility to provide resources and should not be borne by the developer. He said the developer paying for the study was one thing but getting in the discussion of how to effectuate that change was a City responsibility. He said those were his concerns with the recommended conditions. He thought each had merit but needed more refinement to make them good and tangible for the City Council.

Chair DeCardy said regarding the parking garage that he wanted 1000 single-occupancy vehicle trips reduced over the Meta campuses in Menlo Park. He said he chose 1000 as it was essentially equivalent to the parking of the smaller of the two garages for this project. He said it was good to ratchet the TDM and get rid of the associated parking, which was a benefit to the developer and the community. He said he was not recommending a condition to get rid of the parking garage but if the developer wanted the parking garage to reduce single-occupancy vehicle trips by 1000. He said he was trying to link funds to the connectivity piece but he did not want to make a deal about the

elevated park noting that they had heard from numerous community members that they would enjoy it. He said he thought the \$5 million in the shuttle community amenity for connectivity was not enough and it needed an additional \$10 million. He said that included Commissioner Harris' note about electrification and shuttles.

Commissioner Riggs said they were recommending the necessary motions to enact this master plan and additionally posing a series of considerations. He said he did not think they needed to expand on the latter as those were recommendations for Council to consider and it would be appropriate for Council to hear from staff in terms of the details of those. He said he agreed with Commissioner Barnes' caution to not prescribe from the dais. He suggested they relay their concerns to Council and for example clarify that it was not the elevated park that was the issue rather that they believed the shuttle could use considerably more funding. He suggested that if the staff's notes documented the discussion the Council would understand the reasoning behind the recommended conditions.

ACTION: Motion and second (Riggs/Harris) to extend the meeting to 11:20 p.m.; passes 6-0.

Mr. Perata said the first recommendation was to include a guarantee of operation for amenities with mention of the grocery store, pharmacy, bank and air quality monitoring and he also heard discussion about mechanisms. He said the DA, Exhibit D, identified such mechanisms, and the text of the DA had details about mechanisms to ensure that certain things come online or were built by certain dates. He said the DA did not guarantee operation of those amenities that were the use base. He said there were construction guarantees for the spaces in terms of timing in the overall development but not operation of amenities. He said what staff was hearing was to evaluate that and that the City Council consider including an operation guarantee.

Commissioner Harris said that sounded right. She said she was interested in having stronger language for each of those items as well as having air quality and noise quality monitoring operationalized by the applicant. She said she wanted stronger language in the DA so that the grocery store would be operational and not just built.

Commissioner Tate asked if that was for all the amenities. Commissioner Harris said for all of the amenities.

Commissioner Barnes said regarding mechanisms for ensuring operations and over what period of time that for him to agree the language had to be expanded.

Mr. Perata said next regarding the grocery store the recommendation was that the rent subsidy be evaluated for a longer term than 24 months and noted discussion about 60 months.

Commissioner Harris said she had suggested 60 months as was presented in a paper provided by the applicant but she was okay with the term being evaluated. She said she just wanted the record to show that she wanted to make sure they were giving enough runway to the grocery store to become viable and successful. Replying to Chair DeCardy, Commissioner Harris said regarding the 60 months that was not necessary to include specifically if it was clear that the main goal was for the rent subsidy to be enough to ensure the success of the grocery store.

Chair DeCardy confirmed that Mr. Perata could work with that recommendation.

Mr. Perata said the next recommended item he had was to clarify the minimum development requirements such as housing units and square footage in more detail than currently in the staff report. He said the DA included minimums. He said staff could work to clarify those further.

Mr. Perata said the next item was that the gap funding for the senior building should be equivalent as if those were fully inclusionary units. He said that was complicated as that might have legal ramifications. He said the City's BMR guidelines looked at the delivery of the units and not the financing structure per se. He said staff could certainly evaluate but there might be implications.

Chair DeCardy said the BMR was really important and it was supposed to be fully inclusionary. He said for this project it should be determined that the cost would have been to provide fully inclusionary BMR and to make sure those funds went to BMR.

Mr. Perata said to clarify that the applicant would tap into federal, state and potentially county funding for the senior standalone building. He said there was an agreement as part of the BMR and the project entitlements that the City would not contribute any of its BMR trust fund to the project and that portion would be covered by the applicant.

Nira Doherty, City Attorney, said the BMR proposal submitted by the applicant and recommended for approval by the Housing Commission was not premised on the cost to the applicant and providing the units but the value to the City and community in obtaining the units. She said they did come up with a metric to value the commercial linkage fee for the various commercial and office components of the project. She said the applicant had proposed providing units in lieu of paying the fee, which in some context the applicants were entitled to do. She said staff and the Housing Commission evaluated the proposal with an eye toward ensuring the units were provided and not that they might be provided at a lower cost due to financing. She said to incentivize the extremely low and very low income levels, the applicant's proposal involved the provision of units in a standalone affordable building, the senior housing unit, to obtain additional funding.

Chair DeCardy said they heard strongly from the applicant previously about this but to him it was straight forward that BMR was supposed to be inclusionary and the senior housing was not. He asked if it would be cheaper to build than if it had been inclusionary what the delta was between those as he wanted to make sure those resources went to BMR housing.

Commissioner Barnes said respectfully he differed from the Chair in that the provisioning of housing units they got to the same place whether through inclusionary mechanisms with a market rate development providing a prescriptive percent of BMR units at certain AMIs or through a standalone tax credit finance affordable project like this. He said what had him confused or uncomfortable was the utilization of gap financing as the mechanism to solve the problem. He said there were so many layers of financing in such projects including federal, state, county and local that they did not know what the gap number was. He said what they were solving for was making sure the value of the units contributed through the standalone project was not less than what they would have contributed through making more market rate development and having associated BMR units. He said there was a calculation in there and they wanted to true up for a like and similar contribution. He said that could come through the provision of units through what the deferred rents were associated with the offering of different units based on what the market was. He said he agreed with the City Attorney that however you got there the benefit was the same. He said he thought they were talking about what the mechanism for quantifying that was, and he was uncomfortable with the utilization of the

gap financing number as that was an undefined number. He said if they wanted to use gap financing for it and wanted to contribute the land and have that part of it that was fine but just define what their inclusionary requirement would be and what the economic benefit of that was versus the standalone project. He said in his opinion that was what they were trying to solve and he did not think it had to be super complicated.

Commissioner Riggs noted the time and requested through the Chair whether staff could indicate if the intent here was understood.

Mr. Perata said staff understood the intent and could include the recommendation. He said the next was to evaluate the delivery time line for community amenities alongside the office square footage effectively tightening up the timeline. Chair DeCardy said the sentiment was that the community facing pieces were slightly lagging behind the office and to get greater equivalency with the impact that the grocery store, housing, affordable housing and other community amenities show up earlier in the project relative to where they are now.

Mr. Perata said next was an incorporated TDM plan that reduced 1000 additional vehicle trips into Menlo Park, or removing the smaller parking garage for the office. He asked if the Commission was looking for a reduction in trips from this project specifically or from other projects. He said there was an issue with placing a condition for this project on another project so it would need to be from the Willow Village project proposal before the Commission. Chair DeCardy said he was fine with that and fine with the City Council working through with the applicant on what that meant. He said the key thing was the 1000 single occupancy vehicle trip reduction.

Mr. Perata said the next was the elevated park and staff needed clarification as there seemed to be different opinions as to removing the elevated park from the amenities list, or whether it was an increase in shuttle funding. He said to clarify that the DA obligated the applicant to provide a shuttle for a term of 17 years and the estimate for the funding was \$9.7 million but the cost could be higher or lower. He said the obligation was a shuttle that would run through Belle Haven and the broader Bayfront Area including the Jefferson Drive and Haven Avenue areas to bring residents and community members to the project site. He said the dollar amount of the shuttle was not so much in play as the operations of it. Replying further to Chair DeCardy, Mr. Perata said the cost of the Bayfront shuttle should be \$9.7 million. He said there was a \$ 5 million contribution of additional BMR funds for the City to use.

Chair DeCardy said for clarification that this did not have anything to do with the elevated park and everything to do with an additional \$10 million into the shuttle program to make connectivity more robust across the nearby community so there was a greater ability to access and utilize what the project was building.

Commissioner Barnes said he supported solving better connectivity whatever the dollar value was.

Mr. Perata said the recommendation was to add \$10 million to the shuttle or other programs for connectivity to and from the campus.

Mr. Perata said they needed more context on the item recommending evaluation of ConnectMenlo.

Chair DeCardy said ConnectMenlo had been in place for a number of years and the recommendation was for the City Council to reevaluate it and whether it was viable going forward. He said it was City Council discretion, but the sentiment was it needed to be reevaluated because as an instrument guiding these projects it was no longer successful in the underlying guidance of all these products that were coming to them. He said the City Council needed to look at that and determine how to make it work better for their community in the future.

Commissioner Barnes said ConnectMenlo was the land use element update to the General Plan. He said he could support reviewing it but he did not support the premise that it was flawed or not functioning.

Mr. Perata said the Commission's sentiments on ConnectMenlo and the reevaluation of it were known and the statements were on the record for tonight. He said he thought it was difficult to connect it to the project as a recommended modification or as part of the resolution to recommend that as part of this project City Council evaluate ConnectMenlo. He said it did not seem related to the entitlements before the Commission for recommendation to City Council.

Chair DeCardy said staff's recommendation was to leave that item out and that the meeting record would indicate extensive conversation that for different reasons some commissioners believed ConnectMenlo should be reviewed. He said that sentiment would be relayed to the City Council.

Chair DeCardy said Commissioner Barnes would not entertain expansion of a Willow Road relinquishment with additional resources. He said from his perspective the important thing was to get relinquishment on the table and he would withdraw his recommendation for more.

Mr. Perata said another item was that the master sign program be changed to a city-driven process versus an applicant-driven process and a second one was to remove the words "if feasible" for the Bayfront shuttle being a hundred percent electric.

Chair DeCardy confirmed that the latter was fine with Commissioners Barnes and Harris. He noted his feelings about signage and said his recommendation was for process change.

Mr. Perata said the master sign program would be reviewed by staff and then reviewed by the Planning Commission. He said the applicant would be charged with submitting the proposal to the City, which would include the way finding signage, the site advertising parcel by parcel details on square footage and design guidelines similar to some other master sign programs the City had reviewed recently. He said City staff would evaluate this project's sign program but would not draft it. He said the sign program had a public review process that required noticing of the initial application submittal and then noticing of the Planning Commission review and action on it.

Chair DeCardy said he understood the process. He said it did not work the last time they looked at one because the City lacked the resources to provide for independent analysis that would support clear understanding of what the applicant was doing. He said for the record that he heard what Mr. Perata was saying about the master sign program process but it did not work for the City or for the community, which was why he raised the matter. He said though that it did not have to be included in the additional recommendations to the Council.

Commissioner Riggs added that he had found it embarrassing that the City had to lean on two of its applicants to draft a master sign program against which they would then be judged. He said he did not want to affect this vote but he wanted to go on record that a master sign program should be a City responsibility and it was disappointing that it was not.

Commissioner Barnes said he was neutral on the matter of the master sign program. Chair DeCardy said he would leave it off and also to leave off his item about diesel generators.

Replying to Chair DeCardy, Mr. Perata summarized the additional recommendations the Planning Commission wanted to include with the resolution recommending approvals as outlined:

1. Guarantee of operation of community amenities
2. Evaluate longer term grocery rent subsidy
3. Clarify minimums in the staff report from the DA
4. Analyze the delta between the cost of the standalone senior housing and inclusionary units
5. Delivery timeline for community amenities
6. TDM plan to reduce additional 1000 vehicle trips
7. \$10 million for shuttle or other connectivity programs to and from the site
8. Remove "feasible" for the 100% electric Bayfront shuttle

Chair DeCardy confirmed Commissioner Barnes' motion still held with those eight items added. Commissioner Harris seconded the motion.

ACTION: Motion/Second (Barnes/Harris) to adopt a resolution recommending the City Council certify the final environmental impact report (Final EIR), adopt California Environmental Quality Act (CEQA) Findings, adopt a Statement of Overriding Considerations for significant and unavoidable impacts, amend the General Plan Circulation Element, rezone the project site and amend the zoning map to incorporate "X" overlay district and approve the conditional development permit (CDP), approve the vesting tentative maps for the main project site and the Hamilton Avenue Parcels, approve the development agreement (DA), and approve the below market rate (BMR) housing agreements for the proposed Willow Village masterplan project located at 1350-1390 Willow Road, 925-1098 Hamilton Avenue and 1005-1275 Hamilton Court, 1399 and 1401 Willow road, and 871-883 Hamilton Avenue and consider the Planning Commission's eight recommendations as listed here; passes 6-0:

1. Guarantee of operation of community amenities
2. Evaluate longer term grocery rent subsidy
3. Clarify minimums in the staff report from the DA
4. Analyze the delta between the cost of the standalone senior housing and inclusionary units
5. Delivery timeline for community amenities
6. TDM plan to reduce additional 1000 vehicle trips
7. \$10 million for shuttle or other connectivity programs to and from the site
8. Remove "feasible" for the 100% electric Bayfront shuttle

Chair DeCardy adjourned the meeting at 11:30 p.m.

Staff Liaison: Kyle Perata, Planning Manager

Recording Secretary: Brenda Bennett



REGULAR MEETING DRAFT MINUTES

Date: 11/07/2022
Time: 7:00 p.m.
Location: Zoom.us/join – ID# 871 4022 8110 and
City Hall, Downtown Conference Room, 1st Floor
701 Laurel St., Menlo Park, CA 94025

A. Call To Order

Chair DeCardy called the meeting to order at 7:00 p.m. He welcomed Jennifer Schindler, newly appointed Planning Commissioner.

B. Roll Call

Present: Andrew Barnes, Chris DeCardy (Chair), Linh Dan Do, Cynthia Harris (Vice Chair), Henry Riggs, Jennifer Schindler
Absent: Michele Tate
Staff: Calvin Chan, Senior Planner; Fahteen Khan, Associate Planner; Corinna Sandmeier, Acting Principal Planner; Chris Turner, Associate Planner

C. Reports and Announcements

None

D. Public Comment

None

E. Consent Calendar

- E1. Consider and adopt a resolution to approve architectural control for exterior modifications to the front and rear facades of an existing commercial building at 628 Santa Cruz Avenue, in the SP-ECR-D (El Camino Real/Downtown Specific Plan) zoning district. ([Staff Report #22-057-PC](#))

Chair DeCardy opened public comment and closed public comment as no person requested to speak.

ACTION: Motion and second (Riggs/Harris) to adopt a resolution to approve architectural control for exterior modifications to the front and rear facades of an existing commercial building at 628 Santa Cruz Avenue, in the SP-ECR-D (El Camino Real/Downtown Specific Plan) zoning district as submitted; passed 6-0-1 with Commissioner Tate absent.

F. Public Hearing

- F1. Consider and adopt a resolution to deny (1) a variance to exceed the maximum building coverage above limits set by the R-1-U (Single-Family Urban Residential) zoning district and applicable allowances in Chapter 16.79 regarding construction of accessory dwelling units, and (2) a use permit



revision to modify a previously-approved use permit and construct a new covered patio at 6 Greenwood Place. ([Staff Report #22-058-PC](#))

Associate Planner Turner introduced the item and answered Planning Commission's clarifying questions.

Kelly Blythe, property owner, spoke in defense of the application made for a variance and a use permit revision.

Chair DeCardy opened the public hearing and closed it as there were no speakers.

The Planning Commission discussed:

- ADU desirability
- Newness of ADU state law and City's enacting ordinance at the time of the applicant's original use permit application
- Building and permitting sequency
- Variance request

Commissioner Harris moved to approve.

The Commission discussed making the findings for the variance request based on the hardship of the newness of the ADU state law and application of the ADU ordinance.

Commissioner Harris' motion was tabled by the Chair.

ACTION: Motion and second (Barnes/Do) to continue the item to the December 5, 2022 agenda with the following direction to staff; passes 6-0-1 with Commissioner Tate absent.

- Bring back for Commission consideration a resolution to make the findings to approve the variance request based on hardship of misunderstanding of process under newly applied state law for ADUs and City ADU ordinance and approve the use permit revision

- F2. Consider and adopt a resolution to approve a use permit to demolish an existing one-story residence and construct a new two-story residence on a substandard lot with regard to minimum lot width and area at 316 Grayson Court, in the R-1-U (Single Family Urban Residential) zoning district. ([Staff Report #22-059-PC](#))

Associate Planner Fahteen Khan introduced the item.

Anna Felver, Thomas James Homes, applicant, and Katie and John Cromie, property owners, spoke on behalf of the project.

Chair DeCardy opened the public hearing and closed it as no person requested to speak.

Commissioner Barnes moved to approve as recommended in the staff report. Commissioner Riggs noted potential privacy impacts from second floor windows.

The Commission discussed the project and potential privacy impacts from second floor windows along the left and right sides of the residence, and determined with the applicant and property owners an appropriate solution to use obscure glass on the second-floor bathroom windows on the left and right elevations.

ACTION: Motion and second (Barnes/Riggs) to adopt a resolution to approve a use permit to demolish an existing one-story residence and construct a new two-story residence on a substandard lot with regard to minimum lot width and area at 316 Grayson Court, in the R-1-U (Single Family Urban Residential) zoning district with the following modification; passes 5-1-1 with Commissioner DeCardy opposed and Commissioner Tate absent.

- *Simultaneous with the submittal of a complete building permit application, the applicant shall indicate on the elevations that second floor bathroom windows along the left and right sides of the residence will be obscured for the purposes of privacy, subject to review and approval of the Planning Division.*

G. Regular Business

- G1. Consider and adopt a resolution to approve architectural control for modifications to an existing office campus including exterior and interior modifications to the existing fitness center; the addition of a sundeck at the existing restaurant; and hardscaping and landscaping modifications throughout the site, including the addition of five arbors, at 3000 Sand Hill Road, in the C-1-C(X) (Administrative, Professional, and Research District, Restrictive, Conditional Development Permit) zoning district. ([Staff Report #22-060-PC](#))

Senior Planner Calvin Chan introduced the item.

Virginia Calkins, Divco West, property owners, and Jackson Derler, Techcon, landscape architect, spoke on behalf of the project.

Chair DeCardy opened public comment and closed it as no person requested to speak.

The Commission discussed the project and noted the desirable improvements.

ACTION: Motion and second (Do/Harris) adopt a resolution to approve architectural control for modifications to an existing office campus including exterior and interior modifications to the existing fitness center; the addition of a sundeck at the existing restaurant; and hardscaping and landscaping modifications throughout the site, including the addition of five arbors, at 3000 Sand Hill Road, in the C-1-C(X) (Administrative, Professional, and Research District, Restrictive, Conditional Development Permit) zoning district; passes 6-0-1.

- G2. Review of draft 2023 Planning Commission meeting dates. ([Staff Report #22-061-PC](#))

The Commission discussed the proposed 2023 Planning Commission meeting calendar and provide following direction to staff:

- Review schedule for potential conflicts with religious holidays in addition to Christian and Jewish holidays;

- Adjust for lack of quorum upon receipt of individual Commissioners' known conflicts with suggested meeting dates; and
- Approve administratively and send final to Commissioners

H. Informational Items

H1. Future Planning Commission Meeting Schedule

- Regular Meeting: November 14, 2022

Staff indicated that the November 14 agenda would have consideration of the draft Supplemental EIR for the Housing Element Update project and associated discussion on potential rezoning and improvement to the affordable housing overlay, and the Hotel Moxie project.

- Regular Meeting: December 5, 2022

I. Adjournment

Chair DeCardy adjourned the meeting at 9:28 p.m.

Staff Liaison: Corinna Sandmeier, Acting Principal Planner

Recording Secretary: Brenda Bennett

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CITY OF MENLO PARK
Planning Commission

In re:
123 Independence Drive

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**CERTIFIED
TRANSCRIPT**

REPORTER'S TRANSCRIPT OF PROCEEDINGS
AGENDA ITEM F2
MONDAY, DECEMBER 12, 2022

Reported by AMBER ABREU-PEIXOTO
(Via ZOOM Videoconference)
Certified Shorthand Reporter No. 13546
State of California

1 ATTENDEES

2 The Planning Commission:

3 Chris DeCardy - Chairperson
4 Cynthia Harris - Vice Chairperson
5 Jennifer Schindler
6 Andrew Barnes
7 Michele Tate
8 Linh Dan Do
9 Henry Riggs

10 SUPPORT STAFF:

11 Corinna Sandmeier, Acting Principal Planner
12 Matt Pruter, Associate Planner
13 Payal Bhagat, Contract Principal Planner

14 PROJECT PRESENTERS:

15 Peter Tsai, Sobrato Organization
16 Chek Tang, Studio T-SQ
17 Maureen Sedonaen, Habitat for Humanity

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23 BE IT REMEMBERED that, pursuant to Notice of the
24 Meeting, and on December 12, 2022, via ZOOM
25 Videoconference, before me, AMBER ABREU-PEIXOTO, CSR
13546, State of California, there commenced a Planning
Commission meeting under the provisions of the City of
Menlo Park.

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1	MEETING AGENDA	
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9	Maureen Sedonaen, Habitat for Humanity	16
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1 DECEMBER 12, 2022

8:10 p.m.

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P R O C E E D I N G S

4

5 CHAIR DECARDY: We'll move -- we'll now move to
6 Item F2. F2 and G1 are associated items with a single
7 staff report.

8 I have a fair amount to read and, Ms. Sandmeier,
9 I'm going to read that now; is that correct?

10 MS. SANDMEIER: Yes. That's right.

11 CHAIR DECARDY: All right. This is Item F2.
12 This is a public hearing to receive comments on the Draft
13 Environmental Impact Report, the (Draft EIR), for the
14 proposed 123 Independence Drive Project that would
15 redevelop the project site. That's 119 and 123, through
16 125 and 127 Independence Drive, 130 Constitution Drive,
17 and 1205 Chrysler Drive, with a new apartment building
18 with 316 units and 116 three story for-sale townhome
19 condominium units. The five existing office and
20 industrial buildings, totaling approximately 103,000
21 square feet would be demolished. The project site is
22 located in the R-MU-B -- that's the Residential Mixed Use
23 Bonus zoning district.

24 The total gross floor area of residential uses on
25 the site would be approximately 476,962 square feet, with

1 a total floor area ratio of 134 percent.

2 The proposal includes a request for an increase
3 in floor area ratio -- that's the FAR -- and density under
4 the bonus level development allowance in exchange for
5 community amenities.

6 The proposed project includes 48 rental apartment
7 units and 18 for-sale townhome units. 15 percent of the
8 total units affordable to low income households, pursuant
9 to the City's BMR Housing Program Guidelines.

10 The Applicant is currently proposing to provide
11 eight additional rental BMR units affordable to low-income
12 households as a community amenity, in exchange for this
13 bonus level development.

14 The proposal also includes a request for a
15 vesting tentative map for a major subdivision, and a use
16 permit for storage and use of hazardous materials -- in
17 this case, diesel fuel for an emergency back-up generator.
18 The proposed project would remove 29 heritage trees.

19 And with that, I will turn it to -- and I
20 apologize. I don't know who I'm turning it to on staff.

21 Ms. Bhagat?

22 MS. BHAGAT: Yeah. Hi. Yes. You're turning it
23 to me. Thank you.

24 CHAIR DECARDY: Thank you.

25 MS. BHAGAT: I'm just waiting for my presentation

1 to come up. So thank you.

2 Good evening, Commissioners, members of the
3 community. It is my pleasure to provide a brief overview
4 of 123 Independence Drive project this evening.

5 The proposed project is the redevelopment of five
6 existing parcels, totaling 8.15 acres, with 432
7 residential units, and it's made up of 316 rental
8 apartments and 116 for-sale townhomes.

9 Vanh, can you go to the next slide, please.

10 The project -- thank you.

11 The project site is here in the red box and is
12 located south of the Bayfront Expressway, east of Marsh
13 Road. And Highway 101 is to the south of the project
14 site.

15 This slide also shows the other projects that are
16 either approved in the Bayfront area or are currently
17 under construction. As mentioned, this is a bonus-level
18 development, and the applicant is requesting a use permit
19 for the bonus-level development, as well as the use of
20 on-site emergency generator, an architectural control
21 permit for the proposed design, open space, and
22 concessions and waivers associated with the development of
23 the for-sale townhome BMR units; heritage tree removal
24 permit for the 29 heritage trees that will be removed as
25 part of the demolition and prep of the site to receive the

1 project, as well as a major subdivision map to reconfigure
2 the existing property lines to create parcels to receive
3 the apartment building, create an open space parcel. And
4 then the remaining three parcels would receive the
5 townhome buildings.

6 The applicant is proposing to provide 48
7 low-income BMR units that will be rental units and 18
8 for-sale units also affordable to low-income households.

9 Additionally, the project is proposing eight
10 rental units as part of the community amenity. And these
11 units would also be affordable to low-income households.

12 So for tonight's agenda, we're not asking for
13 approval of any entitlements, but we are asking to hold a
14 public hearing and solicit comments on the Draft
15 Environmental Impact Report that was circulated on
16 November 28.

17 Just to remind members of the public, the public
18 comment period ends on January 17, 2023. And the second
19 portion of this meeting is to do a study session on the --
20 the various design aspects of the project, as well as
21 entitlement issues.

22 Next slide, please.

23 So for conducting the two items associated with
24 this project, we have proposed a format. Following the
25 introduction, we request that the Chair invite the

1 applicant to provide a detailed overview of the project,
2 following which, the City's EIR consultant will go over
3 the findings of the Draft Environmental Impact Report, as
4 well as the next steps in the EIR process for the project.

5 After that, we request that the commission invite
6 the members of the community to provide comments on the
7 Draft EIR, following which, we would hear questions from
8 the commission and also take comments from you.

9 And the study session, a portion of this project
10 would also follow a similar format.

11 This concludes my brief presentation. I'm
12 available to answer any questions that you might have as
13 to the various aspects of this project.

14 Thank you.

15 CHAIR DECARDY: Thank you.

16 Any questions of staff before we turn to the
17 applicant presentation?

18 All right. Seeing none, we'll turn to the
19 applicant.

20 Welcome. The floor is yours. We're looking
21 forward to your presentation.

22 MR. TSAI: Thank you, Chair DeCardy. All right.
23 Can everyone hear me?

24 CHAIR DECARDY: Yes, we can.

25 MR. TSAI: Okay. Great. Thank you. Just

1 checking.

2 Good evening, Chair DeCardy, Vice Chair Harris,
3 planning commissioners, staff, as well as members of the
4 public. My name is Peter Tsai, with the Sobrato
5 Organization. I want to thank you for the opportunity to
6 present our all-residential project, 123 Independence
7 Drive. I'm joined by my colleagues, Chek Tang from Studio
8 T-SQ, our design architect; Linda Klein from Cox, Castle &
9 Nicholson, our land use attorney; and Maureen Sedonaen
10 from Habitat for Humanity Greater San Francisco, our
11 affordable housing partner.

12 This project was last before you in September of
13 2021, for our EIR scoping session. Though the project
14 proposal has not changed a great deal since then, there
15 are current commissioners who were not present at that
16 meeting. So we will provide a brief overview on the
17 project sponsor and the project.

18 So about the sponsor. Sobrato is a local
19 organization that has been part of the Bay Area since the
20 1950s. The ethos of the company is making the Bay Area a
21 place of opportunity for all. This is shown throughout
22 philanthropic ventures, as well as our approach towards
23 real estate development.

24 Now to the project. The project is located in
25 the Bayfront, Belle Haven neighborhood between Highway 101

1 and 84 and Marsh Road. The site is bound by Constitution
2 Drive, Chrysler Drive, and Independence Drive. The
3 surrounding area includes mid-rise commercial buildings,
4 parking structures, future multi-family housing, and an
5 11-story hotel.

6 Currently the site contains five old, one-story
7 commercial buildings that will be demolished for the
8 proposed project that you see here; an all-residential
9 project totaling 432 units, 316 apartment units, and 116
10 townhomes over five lots.

11 The project was not always residential. The
12 project zoning is residential mixed-use bonus. The
13 original project was planned to be a mixed-use project
14 with both residential and a 90,000-square-foot office
15 building.

16 After feedback received from our first planning
17 commission study session in January of 2021, and community
18 stakeholders throughout our engagement in 2020 and 2021,
19 Sobrato elected to redesign the project to be all
20 residential. We heard the consistent desire for more
21 housing and specifically more high quality, affordable
22 housing, both for rent and for sale.

23 As long-term holders of real estate, we felt it
24 was important to make this change and be responsive to our
25 community members. At the same time, Sobrato began

1 holding discussions with the Habitat for Humanity Greater
2 San Francisco organization, who we have since partnered
3 with to be the developer for the 18 for-sale affordable
4 townhome units. We will discuss Habitat's portion of the
5 project in more detail when we get into the overall BMR
6 proposal.

7 With that, I will hand it over to Chek Tang from
8 Studio T-SQ, who will walk through the site composition
9 and design.

10 Chek, over to you.

11 MR. TANG: Thank you, Peter. I hope everyone
12 hears me okay. If we can tee up the video walkthrough
13 real quick, please. We'll just briefly go through the
14 impression of the project through a video walkthrough.
15 Thank you.

16 So we can begin -- so just a quick tour of the
17 project, walking from the public paseo on Independence
18 Drive through the townhome districts through a mix -- rich
19 mix of architecture and urban character, arriving at the
20 public park shared by all other resident uses on-site with
21 outdoor and indoor amenities and activities, front porches
22 on the park.

23 Continuing onto the paseo toward Constitutional
24 Drive, the facade of the apartment project creates a
25 strong urban presence on Constitution Drive. And as we

1 come back along paseo, this unit runs onto the paseo,
2 giving security and also coming to the park with the
3 affordable project and the market rate project blended
4 together seamlessly with a whole series of connecting
5 walkways and open space, with varying architectural style.

6 The whole point of the overall massive plan and
7 architectural design is to achieve a thoughtful, balanced
8 and well-integrated neighborhood with an emerging
9 residential mixed-use district.

10 With that, if we can go back to the PowerPoint,
11 please.

12 Thank you. Next slide, please.

13 Happy to report also, since the last time we met
14 with the Planning Commission, we worked very tirelessly
15 with staff to be completely compliant with the R-M-U
16 design guidelines; you know, also the major and minor
17 articulations on the architecture. For the apartment
18 project, we're very focused on creating architecture that
19 is four-sided.

20 In the case of Constitutional frontage, we wanted
21 to have a strong presence that really work in concert with
22 the existing office building across the street. And the
23 four-sided architecture -- also along the proposed paseo
24 that we have architecture that is well articulated, maybe
25 a little bit finer grain to address the pedestrian kind of

1 a scale of the paseo, as well as kind of the internal park
2 that addresses all of the different residential uses that
3 we have, programs and activities that would also address
4 at the park, and also our articulated architecture as
5 well.

6 Next, please. Next slide, please.

7 For the townhomes, we've also heard the comments
8 from the commissions to create more residential scale.
9 This, obviously, is a lower scale residential component.
10 Our idea is to really create a lot of varieties of
11 architecture style with different roof form, with
12 different material and articulation in order to create a
13 finer-grain residential neighborhood.

14 As you can tell, there's a varying combination of
15 townhomes, different module types that would organize
16 around this park, central park.

17 Next, please.

18 And then, obviously, the BMR units with the zero
19 program variations from the townhomes, it also creates a
20 lot of interesting massing and form changes that is to
21 provide overall variations to the overall townhome
22 district, along with the finishes -- the brick, the
23 fiberboard, and also the plaster, coherent with the entire
24 townhome project. It also is complementary to the
25 apartment project as well.

1 Next, please.

2 One key item of the project, as mentioned, is the
3 central park, as well as the paseo. There's some
4 impression of what we are trying to do in terms of
5 programming this space with natural landscaping because
6 it's all in grade. We're planting green lawn space play
7 area, as well as, you know, other picnic areas, a play
8 area, as well as a bike parking area. So it's really an
9 outdoor family room for the entire project.

10 Next, please.

11 In terms of sustainability that -- we are
12 interested in creating a project that would achieve the
13 LEED gold certification. The project will be all
14 electric. It would have EV charging stations. It would
15 have ample bicycle parking and storage, as well as
16 efficient plumbing fixtures, dual plumbing for recycled
17 water use, as well as drought-tolerant landscaping for the
18 landscape and water conservation.

19 With that, I'll turn it back to Peter.

20 MR. TSAI: Thank you, Chek.

21 Even before our first scoping session in January
22 of 2021, we made a commitment to engage a diverse group of
23 Menlo Park and Belle Haven stakeholders to solicit
24 feedback on the project. During the pandemic, we mostly
25 held community meetings online, in small groups, as well

1 as the virtual one-on-ones. That has progressed now as,
2 you know, the pandemic has largely passed. And more
3 recently we held an in-person open house in November of
4 '22. We also held an online meeting forum to engage
5 additional community members on this project.

6 The resounding feedback that we've gotten
7 throughout our years of outreach has been the need for
8 more housing. This was also echoed by the Planning
9 Commission when we came in for a study session back in
10 January of '21, and also was well-received in our
11 September of 20 -- September of '21 study session as well.

12 Next slide.

13 We know the community amenity list is ongoing
14 further refinement, but there is one constant, and that's
15 affordable housing.

16 So in response, our community benefit, we are
17 proposing eight additional rental units. So in total,
18 there will be 74 BMR units, which includes 56 rental
19 apartments and 18 for-sale townhomes. And all will be
20 offered at low levels of AMI.

21 We've also decided to partner with Habitat on the
22 18 affordable townhomes. Sobrato will donate the land to
23 Habitat. And as many of you know, Habitat not only brings
24 a stellar record -- track record, but they also offer
25 residents zero down payment and zero interest rate

1 mortgages. Habitat also caps the homeowner's expenses at
2 30 percent of their income, and that includes property
3 taxes, insurance, and HOA fees.

4 With that, I'll pass it over to Maureen, CEO of
5 Habitat for Humanity Greater San Francisco. She will
6 expand on Habitat and its programs.

7 Maureen, please take it away.

8 MS. SEDONAEN: Thank you, Peter. I'm trying to
9 get my video on. So I don't know -- trying to do that.
10 If the host can ask me -- start my video. Great. Thank
11 you so much.

12 Good evening, everyone. Thank you so much.
13 Thank you to Peter and Sobrato Organization.

14 To the Chair and to all the commission members
15 and all the public tonight, I'm Maureen Sedonaen, CEO of
16 Habitat for Humanity Greater San Francisco. And it's my
17 great honor and pleasure to be with you tonight to talk
18 about our partnership.

19 I think one of the things I want to just kick off
20 and say is one of the incredible, sort of unifying factors
21 for us with Sobrato is they're a family organization,
22 centered in the community, committed to community. And I
23 think our synergy has been incredible since we started the
24 conversation, and I'm pretty proud of where we have it
25 today.

1 Next slide, please.

2 Our Habitat model -- as Peter stated -- does a
3 zero percent down mortgage. We're creating first-time
4 home ownership for community residents and are super proud
5 of our 30-plus year history, including in Menlo Park, for
6 doing this. We cap our homeowner's expenses at 30 percent
7 of their income. We serve people in the 50 to 120 percent
8 area median income. They have to have good credit scores
9 of 650 and above and be willing to also do their 500 hours
10 of sweat equity, with a willingness to partner with us as
11 we create and build these communities. And you see our
12 beautiful picture of some of our current homeowners right
13 now.

14 Next slide, please.

15 Here is our region. We serve Marin, San
16 Francisco, and San Mateo counties. We have 12 homes
17 already in Menlo Park. We look forward to bringing these
18 18 more homes on line here. You can see the rest of our
19 history here.

20 We also have a several-year history and great
21 experience working in the Belle Haven community and
22 partnering with long-time homeowners there to maintain
23 their home ownership through our Critical Repair Program,
24 which we're very proud of as well.

25 Next slide, please.

1 Here's the statistics about our outcomes; why it
2 matters: 96 percent of our Habitat homeowners felt
3 confident that their children are going to finish high
4 school. 95 percent see that their children are going to
5 go on to college. 73 percent have created financial
6 security for their families, and 69 percent are able to
7 save more for the future.

8 Another statistic I'm super proud of is where 21
9 percent of our Habitat homeowners went on to college; 65
10 percent of their children in one generation go on to
11 college. So this kind of transformative opportunity that
12 happens through partnerships with Habitat and the
13 community is really unprecedented.

14 Next slide, please.

15 So project details, we'll go back there. We
16 build a community within a community. I know some of the
17 questions are why do we ask Sobrato to dedicate a separate
18 site for us? This was really our requirement. We build a
19 community within the community where people can put their
20 hands on the clay, if you will, build the homes together
21 and create that kind of community.

22 Secondly, our BMR townhomes will be on an
23 independent timeline, but a timeline none the least. Our
24 townhomes are using a combination of donated materials,
25 volunteer labor; have separate materials in finished

1 packages. But all of this to create a very beautiful
2 experience for our homeowners.

3 Also, our designs are consistent with our other
4 homeowner Habitat homes across the region. We currently
5 are underway for over 140 units in our pipeline that are
6 similar in size, better in count, et cetera.

7 It's just to give you a few examples, for our 20
8 homes that we just completed in Redwood City, we had over
9 700 applicants from the region applying for those homes.
10 And in San Francisco, for our eight townhomes, we had over
11 500 applicants. So the need is there. The community is
12 showing up, and we really are serving the folks who really
13 want to be those first-time homeowners and really
14 transform the community.

15 We're also proud to say that over 85 percent of
16 our homeowners are people of color, and over 90 percent
17 come from within a two-mile radius of the project in which
18 we're building.

19 And, finally, our unique financial model, which
20 is that we have -- we are also the mortgage lender, in
21 addition to being the home builder. So we offer a zero
22 down, zero percent interest mortgage for our homeowners.
23 And we cap their expenses -- as Peter stated earlier -- at
24 30 percent of their income. Many of them right now report
25 between 60 and 70 percent of their income going to

1 housing. So this is a game-changer for them and for their
2 children.

3 Next slide, please.

4 And why do we do this? Because we build a legacy
5 within communities. We build family stability, and we
6 build equity within communities so that in our most
7 expensive region of the Bay Area, we can serve those
8 families who are serving us -- really, our teachers, our
9 first responders, our childcare workers, our folks who are
10 making sure that all our trains move on time; that our
11 families move on time that are served. And we're very
12 proud of this legacy.

13 Next slide, please.

14 I'm happy to also state we have incredible
15 endorsements for this project. From the Housing Action
16 Coalition and the Bay Area Council to the Chamber to the
17 SAMCEDA Group -- everything we do, and I think everything
18 Sobrato does, is done in community and for community. And
19 so together, we brought this incredible synergy across our
20 project.

21 Next slide, please.

22 And we're happy to open up and provide any
23 responses to any partnerships. But I do want to just
24 express our gratitude, express our humility in doing this
25 work. And I really appreciate the Menlo Park community

1 for being such incredible partners.

2 Thank you.

3 MR. TSAI: Thank you, Maureen. With that, that
4 completes the applicant presentation.

5 CHAIR DECARDY: Thank you. Thank you to the --
6 all three of you on the applicant team.

7 This is the hard part of the process we have
8 right now, which is, that's the applicant presentation.
9 We are now going to move first to the EIR portion of the
10 proceedings. So I'm going to hold on any questions for
11 the applicant. I'm going to hand it off to our EIR
12 consultant. We'll move through the EIR portion of the
13 evening with public comment. Commissioner comments will
14 close that. We'll come back then to the broader questions
15 around the project, which the presentation opens up --
16 opens up to.

17 So just as a -- keeping track of where we are,
18 I'm now going to turn to our EIR consultant for their
19 presentation.

20 Thank you.

21 MS. WAUGH: Thank you. Good evening, Planning
22 Commissioners and members of the public. My name is
23 Katherine Waugh. I'm a senior project manager with Dudek,
24 and we are the City's environmental consultant for the
25 project.

1 Also on the call tonight -- or on the meeting
2 tonight is our transportation lead consultant, Dennis
3 Pasquez. So he's available for any questions. But I'm
4 going to handle the presentation by myself, just to keep
5 things efficient.

6 So I don't know -- I'm not sure if I have control
7 of the slide show. So, Vanh, can you advance it to the
8 next slide for me?

9 Thank you.

10 So now, this is just a quick outline of the
11 presentation. And it will -- I'm going to go pretty quick
12 through the project description because you've already
13 heard that. I just wanted to highlight some of the key
14 facts that are relevant to the environmental analysis.

15 So, Vanh, can you go to the next slide?

16 And one more. Thank you.

17 Sorry. When I can do it myself, it's a little
18 bit quicker.

19 So, again, you just heard the project
20 description. So I don't want to go over this slide, but
21 these were the facts of the project proposal that are the
22 most relevant to the environmental analysis.

23 So basically it's a redevelopment project that
24 would demolish the existing structures on the site and
25 repurpose the site for the residential uses that are

1 proposed.

2 Next slide, please. Thank you.

3 And this is just a general site layout, a little
4 bit different from the ones that you've seen previously.
5 But, again, we can see that the apartment structure that
6 has two levels of parking and the 316 dwelling units would
7 be in the northern portion of the site. And the
8 townhouses would be spread throughout the southern portion
9 of the site, with the paseo and park use, you know, kind
10 of along that western edge of the northern portion and
11 then kind of somewhat centrally located through the
12 southern portion.

13 So, then, I have just a couple of quick slides on
14 the overview for the environmental review. For folks that
15 aren't familiar, CEQA refers to the California
16 Environmental Quality Act. And so there's a whole body of
17 state regulations and law under which the EIR,
18 Environmental Impact Report, is prepared.

19 For -- there's, you know, a lot of projects that
20 are going on in the Bayfront area of the city. And some
21 of them require a full EIR. Some of them require more of
22 what we call a focused EIR. And some of them can go under
23 lower levels of CEQA review.

24 For this project, when the project application
25 came in, you know, it's all tied into the General Plan

1 Update that the City recently processed. And for that, a
2 full scope EIR was prepared called the ConnectMenlo EIR.
3 That EIR assumed a certain number of dwelling units, sort
4 of a cap on -- not a cap, but a maximum number of dwelling
5 units that were evaluated within the context of that EIR.

6 And when this project application came through to
7 the City, there were already pending projects and approved
8 projects that added up, you know, and contributed to that
9 maximum number of level of dwelling units. And with this
10 project, with the number of dwelling units proposed, we
11 actually tip over that scale into a level that's beyond
12 what was evaluated in the ConnectMenlo EIR. And so that's
13 why the City staff determined that a full EIR was
14 necessary for this project.

15 In addition, this project proposes a bonus level
16 development. And under the settlement agreement that the
17 City of Menlo Park reached with the City of East Palo
18 Alto, any time that there's a bonus level development, you
19 need to look at the issues of transportation and housing
20 needs. And so those are incorporated within this Draft
21 EIR.

22 Next slide, please. Thank you.

23 This slide just gives you a quick outline of the
24 key steps in the EIR process where public participation,
25 you know, is invited and the mechanisms where that public

1 participation can be provided.

2 So when we first started out with this project,
3 there was a Notice of Preparation released to inform the
4 public and agencies that an EIR would be prepared. And
5 that was first released in January of 2021. And we had a
6 scoping session with the Planning Commission at that time.

7 Then, later in that year, the project applicant,
8 as Peter had reviewed for you, elected to modify the
9 project to eliminate the office -- the office component
10 and replace it with residential. And so we released a
11 revised Notice of Preparation and held a second comment --
12 or scoping meeting.

13 Within the Draft EIR that is out for public
14 review right now, all of the public comments and agency
15 comments that were received on both of the two NOPs,
16 Notices of Preparation, those are documented in Chapter 2
17 of the Draft EIR. Sorry. I had to check my notes. So we
18 have a comment summary there. And then all of the
19 comments, as they were received, are published in the
20 appendices to the Draft EIR.

21 So at this time, we're in the Draft EIR stage.
22 The Draft EIR was released for public review at the end of
23 November and will be out for public review until
24 mid-January. And so comments that public -- members of
25 the public or any public agencies have on the content of

1 the EIR can be received by the City until 5:00 p.m., on
2 January 17th of next year.

3 Once we have all of those comments in hand, we
4 will work through them and provide responses to those
5 comments. And this usually takes the form of direct
6 responses to each individual comment that's received, as
7 well as, we'll make any revisions or additions to the
8 Draft EIR that are necessary to address those comments in
9 full.

10 Next slide, please.

11 Thank you.

12 So this slide just outlines the basic contents
13 that are required in a Draft EIR, based on state law.

14 So the Executive Summary is in Chapter 1, and
15 that provides a very brief overview of the project, the
16 CEQA process that has been followed. And then there's a
17 table in there that documents each of the impacts that we
18 evaluated and whether or not any mitigation measures were
19 required. And if so, what -- you know, the specific
20 content of those mitigation measures. In Chapter 2, which
21 I didn't list on the slide, is just the basic introduction
22 to the EIR. And then the detailed project description
23 follows in Chapter 3.

24 The next several bullets are contained in the
25 individual sections within Chapter 4, which are the

1 environmental impact analysis sections. And so for each
2 topic that's required to be evaluated under CEQA, we go
3 through, you know, the next four bullets -- or, excuse
4 me -- three; the setting, the regulatory framework, what
5 our thresholds of significance are, in terms of how we
6 determine whether an impact is significant or less than
7 significant. We look at both project-specific impacts and
8 cumulative impacts in those sections.

9 And so a cumulative impact refers to when we look
10 at -- in the context of other development that has been
11 proposed within the city and specifically within the
12 Bayfront area. And we -- depending on the topic area, we
13 also might look outside of the city boundaries. We also
14 identify the mitigation measures that are necessary to
15 reduce any significant impacts to a less-than-significant
16 level.

17 And then, in the sort of concluding chapters of
18 the Draft EIR, we look -- we have a summary of the effects
19 that were found not to be significant. We look at topics
20 that are commonly referred to as other CEQA-mandated
21 sections. These kind of amplify some of the content
22 that's already in the Draft EIR.

23 And then we look at project alternatives, which
24 are different ways to design the project or maybe modify
25 the land uses to look to see whether we can avoid or

1 reduce any of the significant impacts that the project
2 would generate.

3 So then, in this section of my presentation, I'm
4 going to really briefly review the major findings of the
5 EIR. So I first have a table on the next slide, Vanh,
6 that -- this one outlines all of the topics that we
7 evaluated where we found that impacts would remain less
8 than significant, with no mitigation measures required.
9 And so we've listed on this table as well the technical
10 studies that were done, where necessary, to support those
11 conclusions.

12 And so I -- I want to keep my presentation brief.
13 I'm happy to answer questions on these, you know,
14 concluding the presentations and the public comment
15 portion of the meeting.

16 In the next, I believe it's three slides, we have
17 the impacts where a mitigation measure or more than one
18 are required to reduce impacts. For the air quality
19 topic, we found that the impacts were really concentrated
20 on the construction period of the project. And they're
21 quite typical for this type of a construction project.

22 And so the mitigation measures require what we
23 typically refer to as "best management practices," BMPs,
24 to make sure that those impacts remain as minimized as
25 possible. And similar is true for the -- for both the

1 biological and the cultural resources.

2 So with respect to biological resources, we found
3 that because of the existing buildings on the site and the
4 existing trees and vegetation, there are potential for a
5 few special status species to occur, but that those
6 impacts can be controlled through the pre-construction
7 surveying process and any additional control measures that
8 are needed, based on the results of those surveys.

9 In regard to cultural resources, there was a
10 cultural resources' analysis and inventory prepared for
11 the project site. But because the project has been -- the
12 property has been developed for quite a long time and no
13 prior subsurface evaluation was done, that phase one
14 archeological inventory report recommended an extended
15 phase one, which is where a little bit more intensive
16 digging is done to determine whether there might be any
17 deposits below the ground surface. And then, in the case
18 that those -- that any such thing were identified, there's
19 protocols identified to evaluate and properly manage any
20 such resources.

21 In terms of the geological resources, the main
22 concern here was that there may be a potential need for
23 de-watering as construction occurs because there would be
24 some excavation. And that -- the way that that system is
25 designed can help avoid any impacts to neighboring

1 properties. And so that's what the first mitigation
2 measure is about.

3 And then the second mitigation measure relates to
4 that as well, in terms of ensuring that -- that
5 construction scheduling is timed such that the geological
6 and soil conditions can settle in between different phases
7 of the construction project.

8 Again, similar in terms of hazards and hazardous
9 materials. Several very standard, best management
10 practice measures were recommended to make sure that both
11 during construction and long-term operation of the
12 project, individuals that are within the site are not
13 exposed to adverse hazardous conditions.

14 And then we have the last two topics here are
15 noise and tribal cultural resources. And these kind of
16 reiterate the same things that I've been saying.

17 Standard best management practices would be used
18 during construction to ensure that neighbors are not
19 exposed to excessive noise levels. And then, if any
20 archeological or tribal cultural resources are identified
21 or potentially encountered during construction, that there
22 are protocols in place to ensure that those resources are
23 managed appropriately.

24 And as I mentioned, at the back end of the
25 Environmental Impact Report, we have a couple of

1 additional sections. So this one, CEQA-mandated
2 sections -- or "Other CEQA Considerations," I think is the
3 title we actually gave it in the EIR, we reiterate some of
4 the discussions on energy conservation, and then we look
5 again at the population and housing analysis. And expand
6 it to whether or not the project could induce additional
7 growth that the City has not planned for. In both cases,
8 we found that the impacts would be less than significant.

9 And then I believe on the next slide, we will
10 look into the project alternatives. So we looked at three
11 different project alternatives. And this is one of -- one
12 of the more essential components of CEQA -- or I shouldn't
13 say "more essential," but one of the critical components
14 of the CEQA requirements to do this environmental analysis
15 is to look at ways that you might modify a project or even
16 change a project to avoid or reduce environmental effects.

17 In this case, it's important to understand that
18 we did not find any significant and unavoidable impacts.
19 So, in other words, that means that all of the
20 environmental effects that we found would result from the
21 project, there were feasible and effective ways to reduce
22 or avoid those effects and bring them to a level of less
23 than significance.

24 But, nonetheless, when you're preparing an EIR,
25 CEQA requires that you look at project alternatives. And

1 so in this case, we took the vein of looking at whether or
2 not any of these alternatives could reduce the need for
3 mitigation measures or just generally reduce the
4 environmental -- you know, comprehensive environmental
5 footprint of a project. And so these are the three
6 alternatives that we looked at:

7 One was just no -- no project, which is required
8 by CEQA. Just, you know, if we leave the project site
9 exactly as it is and continue the current operations.

10 Another was to go back to one of the original
11 project components. As Peter Tsai explained, the original
12 project design included office space. But we also, for
13 this alternatives' analysis, thought that it would be
14 meaningful to incorporate a component of retail uses and
15 see whether or not that kind of a mix of land uses could
16 better achieve any of the City's goals or otherwise reduce
17 environmental effects.

18 And then last we looked at, because this project
19 proposes a bonus level of development, which allows more
20 intensity and more density than what would be allowed
21 under the base zoning designation, is there any
22 environmental benefit to limiting the project to just the
23 base level of development? And so that, we found, would
24 reduce the number of dwelling units.

25 In all of those cases -- sorry, Vanh. I know I

1 paused so you thought I was going on to the next slide.

2 In all of those cases, we found that there would
3 be, you know, sort of a mixed bag. Some cases, we would
4 have fewer effects. In some cases, we might have greater
5 effects. But for the majority of them, we found that the
6 effects would be similar.

7 CEQA does require, though, that we identify which
8 alternative, among these three -- and plus the proposed
9 project -- of those options, which is the most
10 environmentally superior. And we did find that the
11 environmentally superior alternative was the base level
12 development because it would slightly reduce impacts in
13 some of those key impact areas, such as air quality.

14 The base level development wouldn't require a
15 below-grade parking level. It would only necessitate
16 at-grade parking level, so there would be less excavation
17 and less potential to disturb any resources that are below
18 ground. It, you know, reduced the amount of de-watering
19 that would be required.

20 But on the other hand, the City is allowed to
21 consider how that -- how those environmental effects
22 balance against the City's goals for land use development
23 and general, you know, community planning and city-wide
24 planning in the region. And so we found that there were
25 -- while there might be fewer environmental effects, that

1 alternative would be less effective at meeting the project
2 objectives.

3 And so, Vanh, you can go ahead and advance it to
4 the next slide.

5 So that pretty much concludes my comments. The
6 last slide that we have here, if you can go one more,
7 Vahn, is just that the -- as I mentioned, the
8 Environmental Impact Report -- excuse me -- is available
9 for public review until January 17th of next year. And so
10 anybody -- public agencies or public -- you know, members
11 of the public who would like to submit comments,
12 obviously, can make comments tonight during this hearing,
13 but can also submit written comments, whether by mail or
14 e-mail, and they can be addressed to Payal, at the address
15 and e-mail shown below. And they just need to be received
16 before 5:00 p.m., on January 17th.

17 And thank you again. That concludes my
18 presentation.

19 CHAIR DECARDY: Thank you, Ms. Waugh.

20 Any planning commissioner clarifying questions
21 before we open the EIR portion of this program for public
22 comment?

23 All right. Seeing none, Mr. Pruter, off to you
24 to run us through public comment. Again, this is on --
25 we're going to do two bites at this apple, for members of

1 the public. If you have comments around the EIR, which is
2 relevant to the presentation we just saw, this would be
3 the appropriate time.

4 If you have comments that are to the broader
5 project, which likely would be to the initial presentation
6 we saw, that will be coming next.

7 So use your judgment accordingly. And you are
8 more than welcome to speak both times. I'm not trying to
9 tell you not to.

10 Mr. Pruter, please go ahead.

11 MR. PRUTER: Thank you, Chair DeCardy. At this
12 time, members of the public can feel free to press the
13 hand icon on their Zoom interface or dial star nine, if
14 they would like to leave a public comment.

15 And at this time, I do not see any hands raised.
16 And I do not see any members of the public in the council
17 chambers.

18 If anyone is interested in person to come, please
19 feel free to step forward as well. We can wait for a
20 moment at this time. I still see no hands raised.

21 CHAIR DECARDY: Let's wait just a moment.

22 Still none?

23 MR. PRUTER: I still see no hands raised. Thank
24 you.

25 CHAIR DECARDY: All right. We will go ahead and

1 close public comment.

2 We'll come to commissioners now for either
3 clarifying questions or commissioners' comments on the
4 Draft EIR. Again, we are -- there is no motion. There is
5 nothing to vote on here for the commission. It is
6 entirely your feedback to the consultant, to staff.

7 Who would like to begin?

8 Commissioner Riggs.

9 COMMISSIONER RIGGS: Thank you. From Section
10 5.5, the availability of water is one of the items that is
11 considered an less than -- less-than-significant impact.
12 This was based on ConnectMenlo, which was written in I
13 believe, 2016.

14 Have we updated our concerns regarding water over
15 the last six years? And would that be reflected in this
16 EIR?

17 MS. WAUGH: Yes. Commissioner, thank you for the
18 question. The City's Municipal Water District has updated
19 their Urban Water Management Plan. So the last adopted
20 date of that document was 2020. And that is what we
21 relied upon for the analysis in this EIR.

22 We, you know, both reviewed the documentation and
23 contacted the Water District staff to verify our
24 understanding of those -- of that document and the
25 conclusions. And -- yeah.

1 I'm sorry. I'll leave it there.

2 CHAIR DECARDY: Other clarifying questions or
3 comments from commissioners?

4 Commissioner Do.

5 COMMISSIONER DO: Thank you, Chair. Actually, I
6 have a question.

7 I see before the alternatives that you presented
8 in this to the -- but before that, there's also
9 alternatives that were rejected. And so I just had a -- I
10 just get turned around on -- like, on the reduced parking
11 alternative, there's something saying -- let's see.
12 There's a -- the TDM would reduce the VMT by 20 percent.

13 And there's also, later on, a number about
14 reduced parking, reducing it 12 percent. And I just
15 wanted to understand, is that an either/or, or an "and"
16 situation?

17 Is it, like, 12 plus 20, or is it 12 or 20?

18 MS. WAUGH: To be honest, I would need to look
19 back in the text of that section. But from my -- from my
20 recollection, the reduced parking was looked at as sort of
21 an addition to the TDM, or is there an amount that we can
22 reduce parking, in combination with the TDM, that would
23 achieve a better result?

24 And the finding is that, you know, reductions in
25 parking work best in particular situations where there is

1 a, you know, robust amount of other transportation options
2 available in the area and that this project doesn't
3 necessarily meet some of those criteria, to the point
4 being that the reduction in parking -- if you reduce the
5 amount of parking on-site, you're not necessarily going to
6 see a reduction in the amount of trips generated, and more
7 importantly, the total miles of vehicle travel that occur
8 because there are other constraints outside of the project
9 site that limit the effectiveness of that option.

10 But I can -- I will definitely make a note of the
11 question so that we can provide a more-nuanced response.

12 COMMISSIONER DO: Thank you.

13 CHAIR DECARDY: I'm going to use the Chair's
14 discretion to ask a follow-up on that. So this is -- this
15 is familiar. We've seen this before. And the answer
16 about this significantly-reduced parking alternative.

17 So do you look at that based on today's
18 situation, or do you look at it over the lifetime of the
19 project? And how are you making the assessment about
20 alternative -- availability of alternative modes of
21 transportation when you reach that conclusion that you
22 just referenced?

23 MS. WAUGH: Sure. Yeah. I can understand the --
24 you know, the impetus for that question. And it is a
25 difficult spot, in terms of being able to balance what we

1 know today versus what we're -- what our aspirations are
2 for the future. And what we know is planned for the
3 future; right? I mean, sort of our middle ground there.
4 And so it is difficult for us.

5 In the CEQA context, we need to have, you know,
6 pretty solid evidence to allow for any sort of a discount
7 or any kind of a -- you know, an allowance that an impact
8 is less significant than what we expect. And so -- so
9 there is a challenge there in sort of marrying those three
10 different angles.

11 But we do, generally, in terms of CEQA, based on
12 case law and based on how the statute is written and the
13 CEQA guidelines, we typically defer to what is existing on
14 the ground currently. When we look to future conditions,
15 it has to be things that are fairly concretely in place.

16 And so we don't want to engage too -- too far
17 into the realm of supposition or anticipating what may be
18 coming down, if things are not fully funded, in terms of
19 other types of transportation improvements and things
20 along that nature.

21 I'm not sure -- well, I'm sure that doesn't 100
22 percent answer your question. But if you wanted to
23 clarify any further a response that you wanted me to try
24 and elaborate upon...

25 CHAIR DECARDY: No. That's helpful. That was

1 the narrow question I had. That was a good answer. Thank
2 you.

3 Other commissioner questions or ultimately
4 feedback or comments on the Draft EIR?

5 Well, I'm fine to present. This is all I really
6 have. Your presentation was very helpful. The Draft EIR
7 is thorough. The findings are not complicated.

8 I -- I have two comments. The first one is on
9 the parking question. I will say now, my reflection on
10 your answer is not on your answer but on the situation,
11 which is that we're boxed by current policy in the city,
12 which demands parking at a minimum. So there's no need
13 for you to look at parking that is essentially below that
14 minimum. And then we're boxed because we've got terrible
15 transportation policy in place and terrible alternatives,
16 especially in that region of our city. And so we don't
17 look at those.

18 And so the EIR gives us no opportunity,
19 ultimately, to achieve its purpose, which is to provide
20 insight and sunshine so a community can engage in the
21 future-built environment that they live in. And I find
22 that enormously frustrating. But there is nothing that I
23 have found we can do as a Planning Commission. This is on
24 the City Council.

25 And I believe the City Council has to do

1 something about transportation and all the building we're
2 doing. This comes up again and again and again. And if
3 they don't change the parameters, then we're going to keep
4 on getting the same answers. So that's my reflection one,
5 which is more a frustration.

6 My second one is about the alternatives. I think
7 -- as you pointed out, I think you're exactly right. The
8 alternatives are the -- a key element of an EIR. There's
9 something that a community member can easily see and
10 understand and be able to utilize the wealth of
11 information you put behind that that might be in service
12 of their comments about the future of their community.
13 And I -- frankly, I find these alternatives kind of not
14 helpful in that regard for a community member.

15 You have to look at the no-project alternative.
16 Ultimately, it make sense to look at a base level
17 development alternative. We see that all the time,
18 whenever we have bonus-level development. And in this
19 context, the mixed use isn't enormously helpful because
20 everybody in the community wants to have housing.

21 And when we have these three, we end up -- and
22 I've said this before -- we end up with this Goldilocks
23 kind of approach on here, which is, well, if you end up
24 overdeveloping, then that's terrible for the environment.
25 If you end up underdeveloping, then you don't meet the

1 needs of the city. And so you develop just right and
2 turns out, the oatmeal tastes fine because it's warm.

3 And I don't think that's particularly helpful for
4 us as a community in this. So I do have a frustration.
5 This is -- many times, we see EIRs come. Many times, we
6 see three alternatives. And many times they land in
7 exactly this same way. So I will come back to, which is a
8 massive change in a project, like a massively-reduced
9 parking scenario actually would be useful for a city and
10 residents to understand, especially when they've been so
11 frustrated by the impacts in the community of the traffic,
12 which continues to get worse. The only benefit came from
13 the pandemic was knocking that out for a while. But it
14 has come back and will be worse in the future.

15 So it's frustrating me that we can't look at that
16 alternative. But I will say that in future EIRs for these
17 type of projects, if we continue to come back with these
18 three alternatives that are always laid out this way, I'm
19 not sure how useful it is for the community.

20 This is, again, not a criticism of the work of
21 you and your team, Ms. Waugh, which I thought was
22 exemplary, but as a frustration with how we can best
23 utilize this extraordinary amount of expense and work for
24 the benefit of our community. And I just don't see that
25 happening in these instances very often.

1 Other commissioner questions or comments on the
2 Draft EIR this evening?

3 Ms. Bhagat, just to remind me. This is not a
4 command performance. Commissioners do not have to comment
5 before we close this section; is that correct?

6 MS. BHAGAT: Yes. That is absolutely correct.
7 They can always submit comments to me later, if they would
8 like to do so.

9 CHAIR DECARDY: All right. So I will give this
10 one last shot. Again, not a command performance, but any
11 commissioner that would like to offer feedback this
12 evening.

13 All right. I've -- Vice Chair Harris.

14 VICE CHAIR HARRIS: I just want to say that I
15 hear and agree with Chair DeCardy's frustration.

16 CHAIR DECARDY: Thank you for that.

17 All right. Going once, going twice.

18 All right. With that, I am now going to close
19 Item F -- where are we? -- F2, which is the public hearing
20 on the Draft EIR.

21 Thank you very much for the consultant team and
22 for the effort.

23 (WHEREUPON, Item F2 ended.)

24 --o0o--

25

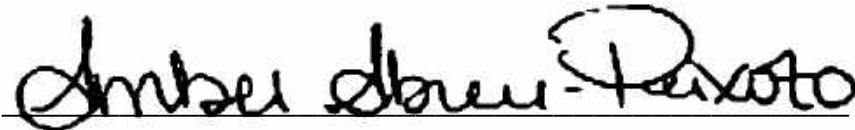
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CERTIFICATE OF REPORTER

I, AMBER ABREU-PEIXOTO, hereby certify that the foregoing proceedings were taken in shorthand by me, a Certified Shorthand Reporter of the State of California, and was thereafter transcribed into typewriting, and that the foregoing transcript constitutes a full, true, and correct report of the proceedings which took place;

That I am a disinterested person to the said action.

IN WITNESS WHEREOF, I have hereunto set my hand this 24th day of January, 2023.



AMBER ABREU-PEIXOTO, CSR No. 13546

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CITY OF MENLO PARK
Planning Commission

In re:
Parkline Project



_____ /

REPORTER'S TRANSCRIPT OF PROCEEDINGS
AGENDA ITEM H1
MONDAY, DECEMBER 12, 2022

Reported by AMBER ABREU-PEIXOTO
(Via ZOOM Videoconference)
Certified Shorthand Reporter No. 13546
State of California

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ATTENDEES

The Planning Commission:

- Chris DeCardy - Chairperson
- Cynthia Harris - Vice Chairperson
- Jennifer Schindler
- Andrew Barnes
- Michele Tate
- Linh Dan Do
- Henry Riggs

SUPPORT STAFF:

- Corinna Sandmeier, Acting Principal Planner
- Matt Pruter, Associate Planner

PROJECT PRESENTERS:

- Mark Murray, Lane Partners
- Thomas Yee, STUDIOS Architecture
- Jessica Viramontes, ICF

---o0o---

BE IT REMEMBERED that, pursuant to Notice of the Meeting, and on December 12, 2022, via ZOOM Videoconference, before me, AMBER ABREU-PEIXOTO, CSR 13546, State of California, there commenced a Planning Commission meeting under the provisions of the City of Menlo Park.

---o0o---

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9	Jessica Viramontes, ICF	15
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23	--o0o--	
24		
25		

1 DECEMBER 12, 2022

10:28 p.m.

2

3

P R O C E E D I N G S

4

This is item H1 -- excuse me. H, Public Hearing

5

2. This is item H1. H1 and I1 are associated items with

6

a single staff report.

7

H1, request for an Environmental Impact Report,

8

an EIR, Scoping Session for the Parkline Master Plan

9

project to comprehensively redevelop an approximately

10

63.2-acre site located at 301 and 333 Ravenswood Avenue,

11

and 555 and 565 Middlefield Road. The proposed project

12

would redevelop SRI International's research campus by

13

creating a new office/research and development,

14

transit-oriented campus with no net increase in commercial

15

square footage, up to 550 new rental housing units (with a

16

minimum of 15 percent of the units available for below

17

market rate households), new bicycle and pedestrian

18

connections, and approximately 25 acres of

19

publicly-accessible open space. The proposed project

20

would demolish all existing buildings, excluding Buildings

21

P, S, and T, which remain onsite and operational by SRI

22

and its tenants.

23

The proposed project would organize land uses

24

generally in two land use districts within the project

25

site including, 1, an approximately 10-acre Residential

1 District in the southwestern portion of the project site;
2 and, 2, an approximately 53-acre Office/R&D -- that's
3 Research and Development District -- that would comprise
4 the remainder of the project site.

5 In total, the proposed project results in a total
6 of approximately 1,898,931 square feet, including
7 approximately 1,380,332 square feet of Office/R&D and
8 approximately 518,599 square feet of residential uses
9 (including up to 450 rental residential units).

10 In addition, the proposed project would establish
11 a separate parcel of land that is proposed to be leased to
12 an affordable housing developer for the future
13 construction of a 100 percent affordable housing or
14 special needs project, which would be separately rezoned
15 as part of the proposed project for up to 100 residential
16 units (in addition to the residential units proposed
17 within the Residential District), and which is not
18 included in the residential square footage calculations as
19 the square footage has not been determined.

20 The EIR will study two potential project
21 variants, one that includes an approximately 2-million
22 gallon buried concrete water reservoir and associated
23 facilities, and one that includes an additional 50
24 residential units for a total of up to 600 dwelling units,
25 inclusive of the standard -- excuse me -- standalone

1 affordable housing building.

2 The project site is zoned C-1(X) -- that's
3 Administrative and Professional District, Restrictive --
4 and governed by a Conditional Development Permit (CDP)
5 approved in 1975, subsequently amended in 1978, 1997, and
6 2004.

7 The proposed project is anticipated to include
8 the following entitlements: The General Plan Amendment
9 (Text and Map), Zoning Ordinance Amendment, Rezoning,
10 Conditional Development Permit, Development Agreement,
11 Architectural Control (for potential future Design Review)
12 Heritage Tree Removal Permits, Vesting Tentative Map,
13 Below Market Rate (BMR) Housing Agreement, and
14 Environmental Review.

15 A Notice of Preparation (NOP) for the proposed
16 project was released on Friday, December 2nd, 2022. The
17 NOP provides a description of the proposed project the
18 location of the proposed project and the probable
19 environmental effects. The EIR will address potential
20 physical environmental effects of the proposed project, as
21 outlined in the California Environmental Quality Act.
22 That's CEQA. An initial study was not completed as it is
23 anticipated this will be a full EIR and no topic areas
24 will be scoped out, with the exception of agriculture and
25 forestry resources, mineral resources, and wildfire.

1 Those topic areas are not anticipated to require further
2 analysis.

3 The project site is located within a "transit
4 priority area," as defined, and thus pursuant to the
5 Public Resources Code section 21099. Aesthetic and
6 parking impacts are not considered significant impacts on
7 the environment. Accordingly, the analysis in the EIR
8 will reflect this statutory directive. Nevertheless, the
9 City retains -- still retains authority to consider
10 aesthetic impacts pursuant to its design review authority.

11 The City is requesting comments on the scope and
12 content of this EIR. The project location does not
13 contain a toxic site pursuant to Section 6596.2 of the
14 Government Code. Comments on the scope and content of the
15 EIR are due by 5:00 p.m., Monday, January 9th, 2023.

16 And with that, I will turn it over to staff.

17 MS. SANDMEIER: Yes. Good evening again, Chair
18 DeCardy and Commissioners. So I have a small
19 presentation -- or try to keep it short.

20 Vanh, can you pull that up?

21 So this is for the Parkline project. And we'll
22 be focusing on the Environmental Impact Report Scoping
23 Session tonight. Next slide.

24 So I'll just kind of focus on the EIR scoping
25 session, since the -- sounds like the study session will

1 be continued.

2 So the purpose of the scoping session is for
3 input on the scope and content of the EIR. And no actions
4 will be taken tonight. And the public comment on the
5 Notice of Preparation ends on January 9th -- that should
6 be 2023. That's a mistake there.

7 So City Council will consider certification of
8 the Final EIR and most of the land use entitlements.

9 And next slide.

10 And this slide just shows the project location.
11 So it's the existing SRI campus. It shows the proximity
12 to downtown, the Caltrain Station, Burgess Park and El
13 Camino Real.

14 Next slide, please.

15 So the existing site is approximately 63 acres in
16 size. It contains 38 buildings. The existing land uses
17 are office, R&D, and supporting uses. And there are
18 approximately 1,100 employees there today.

19 So this is the most recent site plan for the
20 proposed project. 35 of the existing buildings would be
21 demolished. The proposal is for a mixed-use development.
22 The building shown in yellow would be a residential
23 district for approximately 450 residences, with 15 percent
24 below market rate units.

25 And the applicant is also proposing a separate

1 parcel to be dedicated to an affording housing developer.

2 And that would be up to 100 units.

3 And then the remainder of the site would be a
4 nonresidential, basically R&D and office district. And
5 the project includes 25 acres of publicly-accessible open
6 space.

7 So the recommended meeting format for the EIR
8 scoping session is staff overview of the proposed project,
9 presentation by the applicant, presentation by the City's
10 EIR consultant, public comments on the EIR scope,
11 commissioner questions on the scope, commissioner comments
12 on the scope, and then the close of the scoping session
13 public hearing.

14 Next slide, please.

15 And that concludes my presentation. And so next,
16 we'll go to the applicant

17 CHAIR DECARDY: Any questions of Ms. Sandmeier
18 from commissioners?

19 COMMISSIONER BARNES: I do.

20 CHAIR DECARDY: Commissioner Barnes.

21 COMMISSIONER BARNES: So I'm super appreciative
22 of the bifurcation on what we are going to do this
23 evening. Are we, in part two of this, going to hear a
24 redux of the presentation by the applicant? Because
25 depending on when this may come back, I may not be fresh

1 again. And I'd love to -- although it's going to be
2 repetitive, my mind only captures things for a certain
3 period of time. So I'd love to hear a redux of it. And I
4 wanted to check in on that.

5 MS. SANDMEIER: Yes. Through the Chair, that is
6 the plan. It will need to come back next year, 2023. So
7 there'll definitely be an overview again of the project.

8 CHAIR DECARDY: All right. Thank you,
9 Commissioner Barnes. Good question.

10 Any other questions?

11 All right. To the applicant. Thank you for
12 bearing with us this evening. Welcome. The floor is
13 yours.

14 MR. MURRAY: Good evening, Chair DeCardy and
15 members of the Commission, City staff, members of the
16 public. I'm the app -- I represent the applicant, Mark
17 Murray, with Lane Partners.

18 In the interest of time, I'm going to turn things
19 over to Tom Yee, from STUDIOS Architecture, to talk a
20 little bit more about the design, to try to move forward
21 with the scoping session.

22 But, again, we'll be back, probably in a couple
23 months to do the study session presentation, have a more
24 robust presentation there. But, again, here to answer
25 questions as well.

1 Thank you.

2 CHAIR DECARDY: Thanks very much. And appreciate
3 you adjusting on the fly this evening. Thank you.

4 MR. YEE: My name is Thomas Yee. I'm with -- the
5 Principal at STUDIOS Architecture. Thank you for having
6 us this evening, Commissioner DeCardy, Vice Chair Harris.

7 So I'd like to go through the presentation very
8 briefly. Corinna explained the project location and site.

9 Next slide, please.

10 These are some of the goals that we established
11 for the site at the very beginning, over a year-and-a-half
12 ago -- the residential sustainability issues, tree
13 preservation. There are about 1,375 existing trees on the
14 site. We're retaining over half of them through our site
15 planning open space. As we mentioned, 25-acres of
16 publicly-accessible open space because the current site is
17 a fenced-off property. 63 acres, which we're transforming
18 to publicly-accessible land and both programmed, active
19 and passive, open spaces.

20 Next slide, please.

21 And on the Master Plan, as Corinna mentioned, the
22 land uses here are fairly straightforward. A 63-acre
23 site. Ravenswood on the top, Laurel on the left,
24 Middlefield on the right. On the left, part of the site
25 in yellow are three to four buildings of residential

1 apartments in the R1, R2, and R3 buildings. This is all
2 explained in the packet that you received -- and then
3 townhouses to the south, just north of Burgess Park
4 neighborhood -- Burgess Classics neighborhood. Those are
5 two-story townhouses to, again, address the scale
6 transition between Burgess Classics at the residential
7 buildings that work up Laurel, up to Ravenswood.

8 SRI is retaining three existing buildings, as you
9 see in blue there. Building P, S, and T. S and T are at
10 the south portions of the site. Those will -- SRI is
11 consolidating their operations into those three buildings,
12 and -- for their operations in the future.

13 So the 35 remaining buildings to which will be
14 removed will be transformed to office, R&D, and lab/life
15 science uses. You can see, those are situated in the five
16 buildings in light blue.

17 There will be an amenities building for the
18 tenants to the left, above the parking garage No. 3, and a
19 community building on the upper right, next to the church.

20 The open space is accessible. It's being
21 programmed. We've got the active/passive uses. We're
22 proposing a recreational field on the upper right, near
23 Ravenswood and Middlefield.

24 And the other aspect of the property is
25 circulation. We are very -- we've added and included

1 major pedestrian pathways to the site -- north along
2 Ravenswood through the site -- north and south, and
3 diagonally across the site; improved access from the west
4 on Laurel through the site toward the middle.

5 We have Class 1 bike lanes crossing the site
6 along the loop road, which is a private road that you see
7 circulating through the site, as well a consideration of a
8 Class 4 bicycle lane along Laurel.

9 Through our outreach programs with the community,
10 bike safety was a very big concern along -- along Laurel.
11 So Class 4 is a separated bicycle pathway for --
12 especially for kids going up and down Laurel. And they
13 have the opportunity to criss-cross the site over to Menlo
14 Atherton.

15 Again, the idea is to make the open space
16 active/passive, a criss-cross with pedestrian bicycle
17 pathways to create better access through the site, create
18 better safety for bicycle paths and pathways, and folks
19 using those modes of transportation. Located near
20 Caltrain. So taking advantage of the
21 transportation-oriented design aspects.

22 And we're -- both Mark and I are open to any
23 questions. But in the interest of keeping this going this
24 evening, we can conclude here and address any questions
25 you might have.

1 Thank you.

2 CHAIR DECARDY: Thank you very much. And, again,
3 appreciate you adjusting for us on the fly this evening.

4 The next step will be, I believe, to our EIR
5 consultant. Is that right, Ms. Sandmeier? But are there
6 questions for the applicant, in advance of that, from any
7 commissioners?

8 Commissioner Barnes.

9 COMMISSIONER BARNES: So I do have a couple
10 questions on what they presented. But I want to be
11 respectful to the process and the sequencing of how we're
12 going to do this. I mean, I could ask some questions
13 about the site plan -- does it contemplate certain things,
14 and talk further about that.

15 But if we're going to come back to this, you tell
16 me, Chair -- or Chair through staff, how we should
17 progress this.

18 Should we not even go into it and go directly to
19 the EIR? Should we be touching on some of these issues
20 related to the project?

21 How do you want to do this?

22 CHAIR DECARDY: My suggestion, Commissioner
23 Barnes, would go to the EIR. If, after the EIR consultant
24 has spoken, that you've got comments germane to the EIR,
25 where you would like to ask questions of the applicant,

1 then perhaps they could -- you could come back to it at
2 that point.

3 But I think any other questions of the applicant
4 about the project is going to be under I -- what is
5 currently item 11, which we're going to vote to continue
6 until January. So, again, we'll get the full presentation
7 at that point. We'll have the opportunity for full public
8 comment, broad questioning of the applicant at that point.

9 COMMISSIONER BARNES: Okay. So I --

10 CHAIR DECARDY: Does that make sense?

11 COMMISSIONER BARNES: It does.

12 So in the context of clarifying questions, my
13 clarifying questions would be unrelated at this point to
14 the EIR because I haven't heard that yet. So by
15 definition, I won't have anything. But thank you for
16 that.

17 CHAIR DECARDY: All right. Ms. Sandmeier, so
18 we're going to the EIR consultant; is that correct?

19 MS. SANDMEIER: Yes. That's right.

20 CHAIR DECARDY: Thank you.

21 MS. VIRAMONTES: Good evening, Commissioners and
22 members of the public. Thank you for coming to the
23 scoping session for the Parkline Master Plan project. My
24 name is Jessica Viramontes, and I work for the
25 environmental consulting firm, ICF. We will be preparing

1 the environmental review component for the project, and
2 I'm the project manager.

3 Should you have any questions after the
4 presentation regarding the environmental review process, I
5 will respond to them accordingly.

6 Next slide, please.

7 My presentation will cover the scoping process
8 and the environmental review process. I will also explain
9 how to submit comments on the scope of the EIR and
10 describe the next steps.

11 Next slide, please.

12 The EIR team consists of the City of Menlo Park
13 as the lead agency -- meaning, they have principal
14 responsibility for carrying out the project. ICF will be
15 the lead EIR consultant and will prepare all sections of
16 the EIR, with assistance from Hexagon for the
17 transportation analysis, KMA for the housing needs
18 assessment, and West G. Yost for the water supply
19 assessment.

20 Next slide, please.

21 The EIR is a tool for identifying physical
22 environmental impacts by using the analysis conducted by
23 our EIR team. The EIR is also used to inform the public
24 and decisionmakers about a project prior to project
25 approval, recommend ways to reduce impacts, and consider

1 alternatives to lessen identified physical environmental
2 impacts.

3 Next slide.

4 The EIR will summarize the environmental setting
5 and regulatory setting, as well as evaluate potential
6 environmental impacts. With respect to the two scenarios
7 that will be evaluated in the EIR, which are the 100
8 percent office scenario, and the 100 percent R&D scenario,
9 each section in the EIR will evaluate the most intense
10 scenario for the issue being analyzed. This will ensure
11 that the EIR evaluates the proposed project's maximum
12 potential environmental impact and that any future tenant
13 mix is within the scope of the evaluation in the EIR.

14 Variants are variations of a project at the same
15 project site, with the same objectives, background and
16 development controls, but with additions and changes from
17 the project whose inclusion may or may not reduce
18 environmental impacts.

19 As mentioned previously, the EIR will evaluate
20 the variants, which are the emergency reservoir variant
21 and the increased residential variant in detail, equal to
22 that of the proposed project.

23 Next slide, please.

24 The EIR will analyze a proposed project -- will
25 analyze whether the proposed project would have a

1 significant environmental impact related to the issues
2 shown on this slide. With respect to aesthetics, this
3 issue will likely be exempt, but will also likely be
4 analyzed in some capacity for informational purposes.

5 The EIR will also include a section for impacts
6 found less -- found less -- found to be less than
7 significant, including the following issues: Agriculture
8 and forestry resources, mineral resources, and wildfire.

9 In addition, alternatives to the project will be
10 analyzed to potentially reduce identified impacts. CEQA
11 guidelines requires the evaluation of a no-project
12 alternative. Other alternatives will also be considered
13 and will comply with CEQA.

14 Next slide, please.

15 This slide shows the general steps involved with
16 the CEQA process for this project. As most of you know,
17 the NOP, which we'll discuss next, was released earlier
18 this month, on December 2nd. The NOP comment period,
19 which is the scoping period, ends on January 9th, 2023.

20 Following the close of the scoping period, we'll
21 begin preparing the Draft EIR. When the Draft EIR is
22 released for public review, a public hearing will be held
23 to solicit comments on the adequacy of the EIR. Then a
24 Final EIR will be prepared that will address all of the
25 comments received during the Draft EIR review period. A

1 certification hearing for the final EIR will be held
2 before the Planning Commission and City Council.

3 After the EIR is certified, the project can then
4 be approved. Following approval of the project, a Notice
5 of Determination is issued.

6 Next slide.

7 As discussed previously, we are currently in the
8 scoping phase of the project. This is the initial stage
9 of the EIR process. The purpose of the scoping phase is
10 to gather public input, identify key environmental issues,
11 identify possible mitigation measures, and consider
12 possible project alternatives.

13 I want to note that the intent of tonight's
14 meeting, as well as the scoping phase, is not focused on
15 comments on the project itself or its merits. Instead,
16 comments should be focused on the potential environmental
17 impacts of the project.

18 Next slide, please.

19 You can submit comments on the scope of the EIR
20 via e-mail or via letter to Corinna Sandmeier, Acting
21 Principal Planner with the City of Menlo Park. You can
22 also speak tonight, and we will note your comments and
23 consider them during the preparation of the Draft EIR.

24 All comments must be received by January 9th,
25 2023, at 5:00 p.m.

1 Thank you again for coming tonight, and we look
2 forward to receiving your comments.

3 CHAIR DECARDY: Thank you, Ms. Viramontes.

4 Any clarifying questions before we turn to public
5 comment? And then we'll have an opportunity to come back,
6 as commissioners for questions, comments, and input into
7 the EIR. But for right now, before we go to public
8 comment, any clarifying questions?

9 All right. Let's open public comment.
10 Mr. Turner.

11 MR. PRUTER: Thank you, Chair DeCardy. At this
12 time, I see a couple of hands raised. So I'm happy to go
13 through that, with your permission.

14 So we'll have -- looks like three commenters now
15 have raised their hands. Let's start with -- I have
16 someone by the name of Peter.

17 Peter, I'm going to let you un-mute yourself, and
18 we will begin the timer. You will have three minutes to
19 speak. If you could please provide your name and
20 jurisdiction at the start of your comment, that will be
21 greatly appreciated. You'll be able to speak at this
22 time.

23 Thank you.

24 PETER CHOW: Hi, Planning Commission. My name is
25 Peter Chow. I'm a resident here in Burgess community,

1 adjacent to the site.

2 What I wanted to do is just express my continued
3 concern for the number of housing units. The committee
4 here has been very vocal about maintaining the original
5 plan, which was 400 units. And we worked -- and, you
6 know, with Lane Partners and expressing our concern, but
7 now, this additional study is for an additional 50 units.
8 That was not originally contemplated. And so I will be
9 listening and paying attention closely to the impact
10 report, Environment Impact Report, as well as the
11 transportation demand management studies.

12 So want to continue to express my concerns and,
13 you know, for not only the well-being of the local
14 community here in the Burgess community, but all of Menlo
15 Park because we do understand that the rate -- you know,
16 along Ravenswood and Middlefield is a high impact traffic
17 zone area.

18 Thanks.

19 CHAIR DECARDY: Thank you.

20 MR. PRUTER: Thank you for your comment.

21 Our next commenter is the name Jenny Michelle.
22 I'm going to un-mute you. And, again, please provide your
23 name and jurisdiction at this time.

24 Thank you very much. You have three minutes to
25 speak.

1 JENNY MICHELLE: Good evening, Chair,
2 Commissioners, members of the public, neighbors, staff.
3 My name is Jenny Michelle, from the Commonplace
4 Neighborhood blog. And I am very excited about this
5 project.

6 But I want to -- actually, opposite of the
7 previous speaker, want to encourage the applicant to be
8 more aggressive with your housing and your specific
9 approach to meeting and exceeding our residential housing
10 obligations and needs for all residents of all income
11 brackets. Right?

12 But how is the applicant being tied to the Fair
13 Housing Development in this specific way? So I'm just
14 trying to have the applicant and the commissioners and the
15 public tie this together for all the residents who don't
16 understand our obligations here.

17 I'm also interested in pressing the housing -- or
18 I'm sorry. The parking mandates. I think we should
19 reduce the minimums to include loading and ADA parking
20 only.

21 We should encourage slow streets to address the
22 safety concerns that we have with high traffic, with
23 single-use vehicles.

24 And I think there should be robust public
25 outreach, specifically addressing this delta where our

1 population doesn't understand what is being required of
2 us; to develop fair housing in all of our districts and
3 neighborhoods, including the low density neighborhoods
4 that are almost specifically using this vehicle traffic to
5 get through to where the food is; right? So that's where
6 the 10-minute neighborhood comes in.

7 So thank you for allowing me to speak again, and
8 I appreciate your public service. Thank you.

9 CHAIR DECARDY: Thank you.

10 MR. PRUTER: Thank you for your comment.

11 Our next commenter's name is Sue Connelly. I'm
12 going to un-mute you at this time. If you could please
13 provide your name and jurisdiction. You'll have three
14 minutes. Thank you.

15 SUE CONNELLY: Thank you. My name is Sue
16 Connelly. And I, too, am a resident of Burgess Classics.
17 And I grew up in the area here too. So I love Menlo Park.

18 And I'm very much in support of intelligent
19 development, but I am genuinely concerned about the scope
20 of the SRI project. And, again, we here at Burgess
21 Classics, the 33 homes here, are actually a legacy of SRI
22 property that they sold back in '99 to develop in order to
23 raise funds.

24 So I want SRI to be successful. We really
25 appreciate them. Yet, my concern is that there are many,

1 many outside advocacy groups that are pushing very hard to
2 increase the amount of housing in this one lot. And this
3 is also prior to the Stanford project, Middle Plaza
4 opening up and the traffic and school impact, water,
5 infrastructure costs, plus what Springline will be also
6 adding to this very high concentrated area at 400, plus 50
7 to 100, affordable housing units over and above the BMR of
8 15 percent. It already is a monumental amount on an area
9 that's already getting stressed already.

10 My chief concern is also the traffic safety,
11 because Laurel Street is a primary artery, and it's a safe
12 streets, safe bike lanes path. And there are still
13 concerns about driveways for, you know, 450 units dumping
14 right onto Laurel Street, which is already gridlocked and
15 congested.

16 The other issues are that -- you know, the water.
17 I'm really glad that they're planning on building a water
18 reservoir, but just overall, and especially in view of the
19 123 Independent Drive -- Independence Drive earlier spoken
20 about, we have a major drought continuing and probably
21 prolonged for who knows how many decades further. And we
22 keep adding more and more people and such high density.

23 So I think that rather than conceding to all the
24 outside pressures for increasing the amount of housing, we
25 need to reuse and rethink the other areas that we have

1 available around Menlo Park and not make a completely
2 deadlocked and gridlocked Ravenswood and Laurel area
3 corridor.

4 Thank you very much, Planning Commissioners, for
5 staying so late. And thank you for hearing us.

6 CHAIR DECARDY: Thank you.

7 MR. PRUTER: Thank you.

8 Our next speaker is named Brittani Baxter. I'm
9 going to let you un-mute yourself. If you provide your
10 name and jurisdiction. You may now speak. Thank you.

11 BRITTANI BAXTER: Hi. Good evening. I'm
12 Brittani Baxter, District 3 resident. Try to be quick.

13 I think there are a lot of really great
14 structural elements in this project that I hope can be
15 studied in the EIR. So just wanted to ask about a couple
16 of those.

17 Overall, I'm really excited by the project's
18 potential to just kind of be a great example of kind of a
19 future beyond cars. It's so central to downtown. It's so
20 walkable. I think we all hate, you know, car traffic and
21 kind of being stuck in traffic. But I think, with the
22 walkable amenities around that location -- it's an area
23 that I walk to often -- I think it's a really cool
24 opportunity.

25 So having heard earlier in tonight's meeting

1 that, you know, those existing -- kind of existing
2 conditions factor heavily into the EIR. I know I'm
3 personally able to meet a lot of my daily needs by just
4 actually walking around the neighborhood, walking to
5 downtown. So just hoping we can study those existing
6 amenities to the fullest.

7 I also do like the idea of the increased
8 residential variant. To me, it's really appealing because
9 I think this is a once-in-a-multi-generational opportunity
10 for this parcel to turn over. It's been, you know, since,
11 I think, the '60's, when a lot of these buildings were
12 built. And so as I think to the future with more people
13 walking and biking and taking transit.

14 We're right by Caltrain. We're right by the
15 schools. That is really fantastic, too, just to be able
16 to locate those homes in a place that makes sense, again,
17 for people to have other options, other than vehicles.

18 I also wanted to ask if there's an opportunity to
19 study options that do have that reduced parking minimum,
20 again, to sort of create those right conditions for people
21 to ditch their cars, walk or bike around.

22 In terms of circulation impacts, I do really like
23 that the site plan for this location opens up a lot of
24 bike and ped routes that make it easier to kind of
25 criss-cross by Menlo Park, by a lot of our schools; get to

1 the train, get to downtown.

2 And so in terms of circulation benefits, I
3 actually feel that that could be an improvement,
4 especially as we think about, again, alternatives to cars.

5 And according to our housing element, I know that
6 right now, 96 percent of people who work here in Menlo
7 Park, who are already here every day, part of the
8 community, are commuting in to the city from somewhere
9 else. So, again, given that location next to the train,
10 given that there is no net increase in office space, but
11 that we are adding homes to the community, I do wonder if
12 there's any way to kind of study that as well, given that
13 we have people coming in to work, and at the end of the
14 day, you know, maybe driving to an area that doesn't have
15 great public transit. Just seeing if there's any way to
16 kind of map that circulation plan a little bit better.

17 Overall, really excited to have this project in
18 the neighborhood. Really appreciate the open dialogue and
19 just excited to see what transpires.

20 Thank you so much.

21 CHAIR DECARDY: Thank you.

22 MR. PRUTER: Thank you. We have two hands raised
23 that remain. The next is a person named Steve P. I'm
24 going to un-mute you at this time. Provide your name and
25 jurisdiction to start. You have three minutes.

1 Thank you.

2 STEVE PANG: Hi. Can you hear me?

3 CHAIR DECARDY: Yes.

4 MR. PRUTER: Yes.

5 STEVE PANG: Okay. Thanks.

6 Hi. My name is Steve Pang. I'm an owner of one
7 of the Burgess Classic communities since it opened up in
8 1999. And couple quick comments.

9 So with regards to the Parkline project, I've
10 been involved from the start and have attended most of the
11 feedback sessions. And I have to say that most of us are
12 sort of disappointed in Parkline -- that none of the real
13 significant points that we've provided have been adopted
14 and, basically, we feel neglected and ignored.
15 Particularly like the number of units that we're talking
16 about, the egress of the cars of all the units onto Laurel
17 Street, instead of Ravenswood; the bicycle path
18 connectivity behind Burgess Classic communities and the
19 potential gathering of, say, un-homed people behind --
20 which is really a problem right now.

21 So it's funny. We -- I, at least, don't feel
22 like any of our -- my comments have been addressed
23 successfully by Parkline.

24 A couple quick points before I finish. With
25 regards to reducing parking space, parking spaces in these

1 developments, that, to me, seems like a non-starter
2 because these units are rental properties, where people
3 live there maybe two, three years. And, honestly, as a
4 car owner, if I know I'm only going to live in a place
5 only for two, three years, I'm not going to ditch a car
6 and just have to -- just have to buy a new one back
7 several years later. So anyone reasonably renting these
8 place, to me, will seem like -- will hang on to their
9 cars. And so there is the issue of a lot of cars -- you
10 know, up to 600 new cars, maybe a thousand cars, in the
11 neighborhood. And that's a real problem.

12 My final comments are with regards to the
13 Environmental Impact Report. Exactly, there's potentially
14 a thousand more cars in the neighborhood. And, you know,
15 we'd like to know how that's going to be addressed. You
16 know, is that going to be examined? Where is this traffic
17 going to go to on Ravenswood and Laurel? And how is it
18 going to impact our neighborhood, as well as adjoining
19 neighbors?

20 And the last one -- my last comment was with
21 regards to the habit -- the dedication of a certain part
22 of land to a homeless organization or some other
23 organization. So I heard what was happening with
24 Independent Stride, Habitat for Humanity, with a nice
25 plan. And something more definitive needs to be set down,

1 before any approval comes into play. Thank you.

2 CHAIR DECARDY: Thank you.

3 MR. PRUTER: Thank you. And our last hand raised
4 is a person named Gail Gorton. I'm going to let you
5 un-mute yourself at this time. You'll have three minutes.
6 Please provide your name and jurisdiction. Thank you.

7 GAIL GORTON: Good evening. I'm Gail Gorton, a
8 Burgess Classic resident. Thank you for your time
9 tonight.

10 What has been the primary focus of this project
11 is the housing portion. People seem to have forgotten
12 that there will be thousands of employees coming and going
13 from the site five days a week. The additional congestion
14 that this development is going to create is not limited to
15 the housing portion.

16 Traffic light changes at the corner of Laurel and
17 Ravenswood have not helped currently, and there are going
18 to be track changes in the future, train track changes at
19 Alma and Ravenswood. And I'm wondering if these are being
20 taken into consideration in the EIR.

21 In terms of the EIR, it's my understanding it
22 doesn't include the Burgess Classic neighborhood's request
23 to study and include an alternative option of no vehicular
24 access on Laurel Street to the large apartment complex.
25 The fact this was not included, despite what was my

1 understanding from Lane Developers saying it would be
2 studied, is disconcerting.

3 The Parkline project has continued to increase in
4 size. Yet, last month, Stanford's Hoover Institute
5 released a new study, which I suspect you are aware of,
6 stating that in 2021, California lost 152 corporate
7 headquarters. More than double the totals for each of the
8 three years, from 2018 to 2020.

9 I encourage the Planning Commission and the City
10 Council to consider how their current decisions are
11 impacting the future of Menlo Park. I understand you are
12 trying to meet housing element numbers, but those numbers
13 are going to be changing as the business climate changes
14 here in California. With the USGS site opening up, there
15 will be further opportunity to meet the numbers required.

16 I'm asking the Planning Commission to keep the
17 original number of the apartment complex proposal at 400
18 units; not to increase it to 450. The increase in units
19 seems to be driven by a goal to get to 68 units designated
20 as low and moderate income households. 15 percent of 450
21 is 68. Parkline has agreed to this. However, if you
22 increase 15 percent by a mere two points, to 17, and do
23 the math, 17 percent of 400 also equals 68. Considering
24 all that Lane Partners has to gain in this endeavor, I
25 can't imagine they would say no.

1 I'm also asking the Planning Commission to
2 require all apartment parking be underground. This large,
3 three- to five-story apartment complex is not in any way
4 congruent to the neighborhood where all current residences
5 are one or two stories.

6 Lastly, I encourage the commission to emphasize
7 active land use, not just pretty paths for our children
8 and families. Burgess Park is already packed and cannot
9 accommodate our new neighbors. The many individuals and
10 families who will be living in this densely populated
11 development need usable outdoor space for their mental and
12 physical health.

13 Thank you for your consideration.

14 CHAIR DECARDY: Thank you.

15 MR. PRUTER: And, Chair DeCardy, through the
16 Chair, there are no other hands raised at this time. If
17 you'd like to feel free to close, or we could wait for
18 public comment.

19 CHAIR DECARDY: Just give it a second.

20 All right. Still none?

21 MR. PRUTER: That is correct.

22 I apologize. We did not give an opportunity for
23 the members of the public to come forward.

24 CHAIR DECARDY: By all means, please come
25 forward.

1 PHILLIP BAHR: Thank you for having me tonight,
2 Commissioners. And thank you for your presentation
3 tonight. I feel like we've had a great education tonight.

4 I love the 123 Independence, and what they went
5 over and how a housing project -- and how they brought the
6 community together and how detailed it was. That was
7 great.

8 And then we've been talking about this project
9 with Parkline. I appreciate the Classics neighborhood,
10 and I agree with most of the comments that have been made
11 about the size of the project. I'm still a little unclear
12 about the count. I think it's 450, plus 100, plus 50. So
13 a total of 600. But if somebody has a better answer, let
14 me know. But I just look at the documents, and that's
15 what it comes up to.

16 I've commented on some of this before, but I'll
17 just hit the highlights. And one is the traffic and the
18 safety. Yes, it's a big deal about all the traffic coming
19 out onto Laurel, but also onto Pine. Across from Pine
20 Street, that's a disaster right there. Right now, you
21 can't even turn right and turn left as it is. And so with
22 that many more cars, it's never going to work. So they
23 really need to just abort that entry.

24 And I don't have the answer for it. But maybe
25 with some further study and the minds, they can come up

1 with other suggestions because I don't want to say that
2 it's not a great project, and we need the housing. I'm
3 just saying the envisioning of it right now.

4 The second thing is the building setback. It
5 would be good that it's not so close to the road. And I
6 think, along with the building setback, it's the housing
7 height and the number of stories.

8 During the pre-meetings that we had with Lane
9 Partners and with the architect, we went over many things,
10 but one of them was the height of the building along
11 Ravenswood and Laurel and keeping with the neighborhood.
12 One to two stories would be great. And then set back.
13 And then, as you go -- so that you can have the
14 residential character because that side has been on Menlo
15 Park for 70 years. So that's about when those houses were
16 built.

17 And then the final -- so I'm saying that the
18 building height along those streets is just too tall. And
19 I can see it, as an architect, that that is, like, a
20 four-story building. Originally, it was one to two. Then
21 it's three. Now it's four. And it blocks off all the sun
22 in the morning coming onto that intersection at Laurel and
23 Ravenswood.

24 And then the final thing is the site master
25 planning and design of it. I think, get as much housing

1 as you can, but I think, get it in a way that doesn't
2 impact the neighborhood.

3 And also, in terms of a master plan for SRI, I
4 look at it -- and I've done hundreds of master plans for
5 large projects, like hospitals and research labs. And to
6 me, either having an iconic building or something that has
7 the labs with the spaces that are for collaboration. They
8 just have a great opportunity.

9 And right now, they've turned it into a
10 residential, and I'm not sure why. Maybe, if I understood
11 the program better, I could speak better to that.

12 Thank you very much. And my name is Phillip
13 Bahr, and I'm a resident of -- on Pine Street. Thank you.

14 CHAIR DECARDY: Thank you very much.

15 Any more public comment hands, Mr. Pruter?

16 MR. PRUTER: At this time, I see no more.

17 CHAIR DECARDY: All right. We'll go ahead and
18 close public comment.

19 That brings it back to the dias. Again, we're
20 not voting on anything. This is for commissioner feedback
21 or questions relevant to the EIR this evening.

22 Who would like to begin?

23 Commissioner Riggs.

24 COMMISSIONER RIGGS: Thank you. Recognizing the
25 time, I'll try to be brief.

1 I would like to know how we would phrase -- and I
2 guess this would be through the Chair to staff -- how we
3 will address the impacts relative to the current
4 situation.

5 Are we addressing the proposal and their
6 variance, compared with the square footage of SRI or of
7 the actual average occupancy over the last several years?
8 I ask this in the context, remembering that when we
9 studied projects for El Camino Real, going back ten years,
10 we realized we had to compare the impacts with recent
11 usage, not with the fully occupied usage, since the
12 projects had been very much underpopulated for many years.

13 CHAIR DECARDY: That's a question to staff?

14 COMMISSIONER RIGGS: That's a question to staff,
15 yes.

16 Are we comparing with theoretical occupancy or
17 actual occupancy over the last, say, three or four years?

18 MS. VIRAMONTES: Corinna, I can take this, if
19 you'd like.

20 MS. SANDMEIER: Yeah. That would be great.
21 Thank you.

22 MS. VIRAMONTES: Okay. Perfect.

23 So I just want to clarify. The project team, you
24 know, including the City staff, are currently confirming
25 the approach for the CEQA baseline, which will be, you

1 know, what we use to measure the project impacts against
2 -- or as well as the project variants. And so we're still
3 working through those kind of questions. It will likely
4 be the -- you know, the baseline of the timing that the
5 NOP was released.

6 And I just also wanted to clarify that we will be
7 studying an -- we will likely be studying an actual
8 existing conditions at the site.

9 COMMISSIONER RIGGS: I apologize. Our audio has
10 not been what it used to be. And the repetitiveness of your
11 speech, coupled with that, makes it a little bit hard to
12 follow, frankly, what you just said.

13 But I think you ended by saying the baseline
14 would be actual recent usage?

15 MS. VIRAMONTES: Correct.

16 COMMISSIONER RIGGS: All right. Thank you.

17 And then, in terms of the projected occupancy of
18 the -- either office or R&D buildings, am I correct we're
19 using, for office space, 250-square-foot per occupant?

20 MS. VIRAMONTES: I believe that we're still
21 working through those questions as well. But we'll be
22 sure that the generation rate for employees will be
23 conservative enough so that the impacts identified in the
24 EIR will capture the possible future tenant mix and
25 employees that we'll generate by the project.

1 COMMISSIONER RIGGS: I appreciate that because my
2 concern is, these are -- in a sense, these are spec office
3 buildings. And they could just as well be occupied by
4 startups and by other tech-oriented companies with
5 relatively high density use of desks, as they could be by
6 VCs, with very low use of desks.

7 And although we are hearing of companies that are
8 only asking their employees to come in a certain number of
9 days per week -- even, for example, my friend's company,
10 they gather once per week. But on that one day, they all
11 come in. So that would be relevant.

12 And then, of the -- for the project variant with
13 increased housing, I probably read and forgot how much
14 increased housing that would be. I mean, right now, we
15 have 550 as the outside.

16 Would the variant be the 550, or is the variant
17 going to be something like 700 to 800?

18 MS. VIRAMONTES: The variant would be 50 more
19 residential units under the project. So it would be a
20 total of 600 units.

21 COMMISSIONER RIGGS: All right. I would like to
22 suggest that since it's a variant, for the sake of an
23 environmental review, that the difference between the
24 proposed and the variant be significantly different. And
25 so I would suggest at least 150 additional units, if not

1 250, which, you know, to those listening, that does not in
2 any way imply that I think the project should be larger.
3 It does mean that we would like the information that would
4 result from seeing additional housing here.

5 We still don't fully know, until the EIR comes
6 out, whether having more housing here is actually a
7 benefit to transportation, for example. Because if the
8 vast majority of people who work here -- and the SRI
9 campus, until recent years, was a significant draw for
10 people. They've all been driving in.

11 If this changes to more transit-oriented
12 development, sometimes the new housing onsite will have a
13 back effect on those who commute in. And perhaps that's
14 wishful thinking, but the EIR, I think, is more likely to
15 tell us than my guessing or anyone else's.

16 And I'll leave it at that. Thank you.

17 CHAIR DECARDY: Other commissioners?

18 Vice Chair Harris?

19 VICE CHAIR HARRIS: Yes. Thank you so much for
20 that introduction.

21 I would agree with my colleague, Commissioner
22 Riggs, that to study just 50 more units is going to be
23 less -- going to give us less information than studying at
24 least 150 additional units. And I can't remember, but I
25 don't think that that's coupled with reduced office.

1 But I'm wondering if it would be possible to do a
2 variant where we are increasing the housing, coupled with
3 reducing the office, as we struggle with our housing
4 situation because as I was looking at the map, I was
5 thinking that existing building F -- if, after the rest
6 were done, they moved those folks to some of these newer
7 offices, that would provide a nice extra area, right over
8 in the residential zone, to build a lot more housing. So
9 that's a thought.

10 And then the other was to think about reducing
11 the parking. We talk about this about every time. But
12 reducing the parking significantly. So that would be
13 something else that I would want to see studied. Just
14 some thoughts.

15 CHAIR DECARDY: Commissioner Do.

16 COMMISSIONER DO: I agree with the previous
17 comments, and I want to add on to Vice Chair Harris'
18 comment about drastically reducing parking.

19 I think later on in the staff report, I think
20 some parking rates from the Bayfront area were cited. And
21 I just wanted to add, this is an area much closer to
22 transit than the Bayfront, with Caltrain and El Camino
23 Real bus route. So I think even within a half mile.

24 So I just want to echo what Vice Chair Harris
25 said.

1 CHAIR DECARDY: Commissioner Barnes.

2 COMMISSIONER BARNES: Question through the Chair
3 to staff, in particular to the folks who are doing the
4 legwork on the EIR. This is kind of a process question
5 because I don't really understand how this works. And to
6 the extent you can help me understand, it would be
7 fantastic. And what it's specific to is to the question
8 around parking. And more specifically to the extent to
9 which the EIR can illuminate the various discussions
10 around parking.

11 We -- to say more about that, we have a lot of
12 discussions about reducing the number of spaces, and we
13 have assumptions about reductions in greenhouse gases
14 associated with that written reductions, and congestion
15 associated with that.

16 And then we also make assumptions around
17 reductions being doable, feasible; actually, in practice,
18 working. And I don't have any background in this. I
19 think the suppositions around reducing parking are good.

20 What I'd like to know is, is the EIR the
21 mechanism that can illuminate, you know, a database
22 approach to, you know, what happens when you reduce
23 parking? What are the specific impacts of those? Has it,
24 you know, borne out in other jurisdictions? What's the
25 role of the EIR specific to parking and the discussions

1 around parking? I'd love to hear a little bit more about
2 that.

3 MS. VIRAMONTES: Sure. I can tackle that one,
4 and others can add on as needed.

5 I do want to clarify that an EIR is not the
6 mechanism for analyzing the impacts of reducing parking.
7 Specifically, parking is not a topic that is required as
8 an environmental issue that is required to be analyzed
9 under CEQA.

10 And also I want to note that it's been found that
11 generally, reductions of parking do not reduce
12 environmental effects. But I know that my colleague,
13 Kirsten Chapman on this call -- or at this meeting, might
14 have a little bit more to add.

15 Kirsten, is there anything else you want to chime
16 in on?

17 MS. CHAPMAN: Hi. I'm Kirsten Chapman. I'm with
18 ICF. I'm helping Jessica with this EIR.

19 And we actually recently completed the EIR for
20 the Willow Village project. And we did prepare a lengthy
21 master response in the Final EIR that discussed how
22 parking and environmental impacts are not actually
23 correlated. And we explained why this is not a reason
24 that we can use to reduce environmental impacts by
25 reducing parking.

1 So without getting into those details, that is
2 where we recently prepared the response. And, yeah. As
3 Jessica mentioned, it's not a CEQA topic. Parking is not
4 a CEQA topic. And so we generally do not discuss this.

5 But where we will have a robust discussion will
6 be in the alternatives section, and we can discuss why a
7 reduced parking alternative would not actually reduce the
8 environmental impacts.

9 COMMISSIONER BARNES: And if you would just take
10 a moment, define "environmental impacts" in the context
11 with which you're using it, when you say, would not reduce
12 environmental impacts. What's a practical or what's an
13 example of that?

14 MS. CHAPMAN: Well, so transportation impacts
15 like traffic impacts would result in greenhouse gas
16 impacts, air quality impacts, noise impacts. But reducing
17 the parking in and of itself would not reduce the amount
18 of trips to a project site. It would likely result in
19 people driving around neighborhoods, looking for parking.
20 They still need places to park.

21 What is better, rather -- or not better, but what
22 works generally more or what does work more than reducing
23 parking is to have a TDM plan, which is required in the
24 City of Menlo Park, to require the workers on the project
25 site and the residents to take more public transportation

1 or shuttles. That reduces trips.

2 But the reduction in parking generally does not
3 reduce trips, which then has an environmental effect of
4 putting out fewer greenhouse gases and fewer air quality
5 emissions and noise.

6 COMMISSIONER BARNES: Thank you for that.

7 And I assure my fellow commissioners, I wasn't
8 leading the witness on that. I didn't know how it was
9 going to get answered. But I don't know. I always want
10 to come back to testing our assumptions. And that was
11 informative for me, because I didn't -- I didn't know the
12 answer to that.

13 Okay. So I'll probably come back with another
14 one, but thank you for -- for answering that. Appreciate
15 that. And I'll come back with something else.

16 Back to you, Chair.

17 CHAIR DECARDY: Yeah. Sorry. That's red meat
18 for me.

19 So, Ms. Chapman, I don't know if you were there
20 for the Willow EIR, but that -- the answer then was
21 entirely unsatisfactory. The reason is because of a lot
22 of assumptions about leakage, that there's not alternative
23 transportation; and so, therefore, people drive around
24 neighborhoods. And we couldn't do a reduced parking
25 because we've got parking minimums in Menlo Park, which is

1 what we just talked about with the last EIR.

2 So I just -- I encourage you all when you do this
3 EIR, to be as careful as possible when you're explaining
4 why it doesn't have impacts because an answer without that
5 is actually misleading. So that's first point.

6 And then, secondly, for me is an encouragement to
7 find a way in the EIR that can actually tackle this
8 question because it is the one that comes up again and
9 again and again and again. And it just came up in
10 multiples of the public comments with the concerns of the
11 residents who live nearby right now.

12 So, again, I'm tired of EIRs that don't serve the
13 public interest of our community. And I appreciate you
14 all are doing your jobs, and I appreciate you're boxed in
15 by a whole set of stuff. But somebody in this mix has got
16 to do a better job for our community. This is a lot of
17 money, and a lot of time spent on these things.

18 So perhaps the alternative is a
19 massively-increased TDM plan. And I'm fine to do TDM over
20 parking. If the -- if we have a massive TDM plan that
21 says it has to be reduced by 40 or 50 or 60 percent, and
22 then that's a way to be able to look if there's an
23 environmental benefit.

24 And if they want to keep on building the parking
25 garages, when there's going to be no cars in them, that

1 would be a massive mistake. But that's fine, frankly, if
2 that's the answer on this.

3 So I'll just go back to my frustration with just
4 about every EIR I've seen in four years now. And this one
5 is, I'm concerned, headed in that same direction. So I
6 just -- I appreciate the presentation, and I appreciate
7 and understand how -- the way that we have a community
8 that does not have good alternative transportation and
9 because we have parking minimums puts parameters for what
10 you all can do on an EIR.

11 But I would really encourage you to find creative
12 ways around that to actually give a document that would be
13 useful to the community in understanding what those
14 impacts are, and what the benefits might be, if we change
15 those patterns and those behaviors. That would be a true
16 benefit to the discussion of this potentially-fabulous
17 project that is a once-in-a-lifetime opportunity -- that
18 never again are we going to get 62 acres within a block of
19 a train station. And we've got to begin looking at it
20 right with the EIR, if we're going to continue to look at
21 it right through the whole project.

22 So I appreciated Commissioner Barnes, your
23 question. And I assume you knew it was headed toward me
24 on that. But that is the one interest I had is when you
25 do alternatives on this project, and if there's a "no

1 project" alternative, again, I hope we don't come back
2 with three alternatives that ends up with the Goldilocks
3 porridge in the middle that's just warm enough because
4 that's just not useful for us.

5 And I hope you can find ways that can make it
6 useful for our community to use this information that
7 you're going to come up with and your expertise to our
8 benefit.

9 Other commissioner input on the EIR in this
10 scoping session?

11 COMMISSIONER RIGGS: Yes.

12 CHAIR DECARDY: Commissioner Riggs?

13 COMMISSIONER RIGGS: Thank you. I have to admit,
14 I had the same reaction as Chair DeCardy. I think anyone
15 who has worked in Manhattan or, frankly, even San
16 Francisco, yes, you can drive to your office at 6th and
17 Market and then cruise around and look for a surface spot.
18 But that gets really old. And, yes, 60 or 80 people might
19 manage to find street parking spaces until it gets posted
20 two-hour zones. But 600 are not going to. And I think
21 it's quite counter-intuitive for us to hear that reducing
22 -- eliminating places to park is not going to have an
23 effect with how many cars come in to work.

24 And I think we realize that only so many people
25 can take Caltrain because if you're coming in from

1 Hayward, Caltrain simply doesn't go there. And, frankly,
2 if you come in from the Belmont Hills, Caltrain doesn't go
3 there. But a whole lot of people come from San Francisco.
4 A whole lot of people come from San Jose. And if we don't
5 test the waters, as Mr. DeCardy has stressed, we won't
6 have information that we can use. I do not think if it's
7 true that we are not taking reduced parking seriously
8 because of existing codes -- that that should stand in the
9 way. And perhaps this body needs to clarify.

10 When a project comes before us, the result is a
11 change in codes. And the change in codes may be buildable
12 height, it may be density, it may be parking ratios
13 applying to that site. So all items are in flux. And if
14 we can benefit from further information, that would be
15 extremely important.

16 And it may indeed turn out that in real life, if
17 you take away all parking places and have 10,000 people
18 report to work, they'll still drive, then we've learned a
19 very surprising lesson. But I think we have to see it.
20 Thank you.

21 CHAIR DECARDY: Other commissioner comments on
22 any aspect of the scoping of the EIR for input at this
23 time?

24 Commissioner Barnes.

25 COMMISSIONER BARNES: And I must apologize. I'm

1 scrolling furiously back up and down in the staff report.

2 And I'm looking for the specific alternatives. And I
3 guess I don't see it laid out.

4 I'm going to ask this question in real time. Is
5 there a specific matrix that talks to the different
6 alternatives that are being discussed that will be
7 underwritten in the EIR? What am I missing?

8 And I'll ask this question through staff. Thank
9 you.

10 Excuse me. Through Chair.

11 MS. VIRAMONTES: Corinna, would you like me to go
12 first?

13 MS. SANDMEIER: Sure.

14 MS. VIRAMONTES: Okay. I just want to clarify,
15 we haven't yet determined the alternatives for this
16 project. The typical process is to evaluate the project's
17 impact and then develop alternatives that would reduce or
18 avoid any significant environmental issues.

19 So to back up a little bit, you kind of see what
20 the potential impacts of the project are. And then you
21 develop alternatives to kind of help the public understand
22 what alternatives to the project there would be that would
23 reduce the project's environmental impacts.

24 But also to back up again, there are project
25 variants under consideration; one being the emergency

1 reservoir variant, and the other being the increased
2 residential variant. And those will be analyzed
3 throughout the EIR, to similar level of detail as the
4 project. So there's variants, and then there's
5 alternatives.

6 COMMISSIONER BARNES: Got it.

7 So the baseline EIR is based on the project
8 applicant's project description, in terms of densities and
9 intensity; is that right?

10 MS. VIRAMONTES: Exactly. Yes.

11 COMMISSIONER BARNES: Got it. Okay.

12 And I -- this is a -- this is a unique location
13 in Menlo Park that brings together the live, work, play.
14 So thank you for that. This is a commentary. This is a
15 unique portion of Menlo Park that brings together the
16 live, work, play aspect of our city. And I -- I think the
17 commercial -- the office, the commercial pieces of this
18 are very appropriate. And I wouldn't be inclined to see a
19 reduction in that for the purposes just straight up from
20 what the applicant has proposed.

21 I think, from a master plan perspective, it's a
22 net neutral, in terms of space. And I think it's wholly
23 appropriate for this area, for the mix of the different
24 uses for this site and for what it brings to the city.
25 And I wouldn't be inclined to be supportive of a reduction

1 in that component of it. Thank you.

2 CHAIR DECARDY: Commissioner Riggs has left, for
3 those that couldn't see.

4 Other commissioner comments on this item, which
5 is H1, the scoping for the EIR?

6 To staff, have you received what you --

7 COMMISSIONER BARNES: I'm sorry. One more
8 question.

9 CHAIR DECARDY: -- were after this evening?
10 I'm sorry. Commissioner Barnes, please.

11 COMMISSIONER BARNES: Thank you.

12 As it relates to the project itself as being
13 contemplated in the EIR, when we saw the site plan
14 earlier, it had a recreational field at the corner of
15 Middlefield and Ravenswood, and then it seemed to carve
16 out around the church.

17 So my question is, is the project scope
18 contemplating the church site being part of the project or
19 not part of the project?

20 And that's kind of a two-part question. One is,
21 you've got that parking which abuts Ravenswood and
22 Middlefield and another is the actual physical structure
23 of the church itself and the parking that's behind it.

24 What's in the project scope?

25 CHAIR DECARDY: That is a question to the

1 applicant or staff --

2 Ms. Sandmeier?

3 MS. SANDMEIER: Yes. Through the Chair, the
4 church is not part of the project site. There is an
5 agreement between SRI and the church to provide some
6 surface parking to the church.

7 And I know that's -- I think that's influenced
8 the site plan a little bit, that requirement to continue
9 providing some parking there.

10 COMMISSIONER BARNES: Thank you for that.

11 So through the Chair, the -- so the project
12 contemplates a wrap-around, in effect, where you've got --
13 and if we could look at the actual site plan itself, that
14 might provide some quick clarity in this.

15 Can someone pull that up? I think it was on one
16 of the slides in the project introduction.

17 MS. SANDMEIER: Yeah. Vanh, it was slide 5 on my
18 presentation. If you can pull that up.

19 COMMISSIONER TATE: Excuse me. Chair DeCardy,
20 I'm leaving the meeting.

21 CHAIR DECARDY: All right. Thank you,
22 Commissioner Tate.

23 COMMISSIONER BARNES: Okay. So it -- so the
24 proposed project encircles the improvements that are the
25 church, in a sense.

1 MS. SANDMEIER: Yeah. That's right. The church
2 is its own parcel.

3 COMMISSIONER BARNES: And the parking behind the
4 church -- I'm sorry -- runs with the project or doesn't
5 run with the project?

6 MS. SANDMEIER: That parking is part of the
7 Parkline project. But there's an agreement where the SRI
8 -- or Parkline is required to provide parking to the
9 church. And maybe the applicant can speak to that a
10 little bit more.

11 MR. MURRAY: Please. Sure. Just to add a little
12 bit more detail.

13 So kind of that white carve-out on Ravenswood,
14 that's the church-owned property. So there are two
15 buildings there that are owned by the church, not part of
16 the project scope. However, the surface parking around it
17 is part of Parkline. It's owned by SRI.

18 But the church has an easement to 125 parking
19 stalls adjacent to the church. So we're maintaining that
20 in the -- in our project scope, as we're required.

21 COMMISSIONER BARNES: Got it. Thank you.

22 And thank you to our fellow commissioners here
23 for your forebarence with that question.

24 That's all. Thank you.

25 CHAIR DECARDY: Ms. Sandmeier, have you had

1 whatever you need from commissioners on scoping of the EIR
2 this evening?

3 MS. SANDMEIER: Yes. If there's no more comments
4 from commissioners, that's...

5 CHAIR DECARDY: All right. Any final comments or
6 initial comments from any commissioners at this time?

7 All right. I'm going to go ahead and close Item
8 H1 this evening. And thank you.

9 And thank you to the consultant for the
10 presentation, for clearly laying out what's going to
11 happen, and appreciate all the work you're going to be
12 doing.

13 (Whereupon, Agenda Item H1 ended.)

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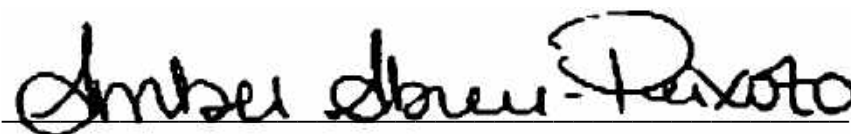
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IN WITNESS WHEREOF, I have hereunto set my hand this 27th day of January, 2023.

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STAFF REPORT

Planning Commission

Meeting Date:

12/12/2022 1/23/2023 2/6/2023

Staff Report Number:

22-073-PC

Public Hearing and Study Session:

Public hearing for the environmental impact report (EIR) scoping session and study session for the proposed Parkline masterplan project to redevelop SRI International's research and development (R&D) campus with a new office/R&D campus with no net increase in commercial square footage, up to 550 new multi-family dwelling units and 25 acres of publicly accessible open space at 333 Ravenswood Avenue

Recommendation

Staff recommends that the Planning Commission conduct the following items for the proposed project to redevelop SRI International's research campus with a new office/R&D, transit-oriented campus with no net increase in commercial square footage, up to 550 new dwelling units at a range of affordability levels, new bicycle and pedestrian connections, and 25 acres of publicly accessible open space:

- EIR scoping session to receive public testimony and provide comments on the scope and content of a EIR for the proposed project; and
- Study session to receive public comments and provide feedback on the proposed project.

The December 12th meeting will not include any project actions. The proposal will be subject to additional review at future Planning Commission and City Council meetings.

Staff recommends the following meeting procedure to effectively and efficiently move through the two items, allowing the public and the Planning Commission to focus comments on the specific project components.

EIR scoping session

- Introduction by Staff
- Presentation by Applicant on Project Proposal
- Presentation by City's EIR Consultant
- Public Comments on EIR scope
- Commissioner Questions on EIR scope
- Commissioner Comments on EIR scope
- Close of Public Hearing

Project proposal study session

- Introduction by Staff
- Public Comments on Project
- Commissioner Questions on Project

- Commissioner Comments on Project

Staff believes that it would be beneficial for the Planning Commission and members of the public to receive the applicant's presentation during the EIR scoping session portion of the public hearing to provide a more robust understanding of the proposed project that will be studied in the EIR. Accordingly, staff recommends that the Planning Commission allow the applicant to present the overall project, followed by a presentation from the City's EIR consultant (ICF) outlining the California Environmental Quality Act (CEQA) process.

Policy Issues

Scoping sessions on the EIR provide an opportunity early in the environmental review process for Planning Commissioners, public agencies, and community members to comment on specific topics that they believe should be addressed in the environmental analysis. Study sessions provide an opportunity for Planning Commissioners and interested community members to provide more general feedback on a proposed project, with comments used to inform future review and consideration of the proposal. The EIR scoping session public hearing and study session should be considered as separate items, as part of the same hearing.

A masterplan project provides a vision and framework for growth and development of the site. The applicant is requesting general plan and zoning ordinance amendments to enable the proposed masterplan development. The new general plan land use designation would allow for residential dwelling units, public and quasi-public uses, office, R&D, and supporting uses. As currently proposed, the designation would apply to the entire site and establish a maximum residential density at 45 dwelling units per acre and a maximum commercial floor area ratio (FAR) of 0.6, based on the amount of existing square footage on-site, to allow up to approximately 1.38 million square feet of non-residential uses. The non-residential square footage would not exceed the current square footage of all buildings on the project site.

The proposed project is anticipated to require the following entitlements and/or City permits:

1. **Environmental Review** to analyze potential environmental impacts of the proposed project through a full EIR, pursuant to CEQA;
2. **General Plan and Zoning Ordinance Amendments** to enable the proposed masterplan development;
3. **Rezoning** to apply the new zoning district(s) to the project site;
4. **Conditional Development Permit (CDP)** to enable comprehensive planning of the project;
5. **Development Agreement (DA)** for vested rights in exchange for community benefits;
6. **Architectural Control** to review the design of the future new buildings and associated site improvements;
7. **Vesting Tentative Map** to merge the existing lots and re-subdivide in a manner consistent with the proposed improvements;
8. **Heritage Tree Removal Permits** to remove heritage trees to enable the proposed project and plant heritage tree replacements per the City's municipal code requirements; and
9. **Below Market Rate (BMR) Housing Agreement** to provide on-site BMR units in accordance with the City's BMR Ordinance.

In addition, a Fiscal Impact Analysis (FIA), Water Supply Assessment (WSA), and Housing Needs Assessment (HNA) will be prepared. Additional actions and entitlements may be required as the project plans are refined.

The City Council would be the decision-making body for the EIR, general plan amendment, zoning ordinance amendment, and rezoning to allow the proposed mix of uses and densities/intensities requested by the applicant, and the CDP to enable comprehensive planning of the project. The City Council would also be the acting body on the development agreement, which would provide vested rights in exchange for community benefits, the vesting tentative map to merge the existing lots and re-subdivide in a manner consistent with the proposed improvements, and the BMR Housing Agreement. The Planning Commission would be the acting body for any future architectural control permits for the proposed new buildings and the recommending body on all other entitlements, and the City Arborist would issue the Heritage Tree Removal Permits.

Background

SRI International (formerly known as the Stanford Research Institute) is an independent, nonprofit research institute located on an approximately 63-acre campus at 333 Ravenswood Avenue. The existing development on the SRI campus is regulated through a CDP, which was first approved in 1975. The most recent amendment to the CDP occurred in 2004 for the construction of Building T, at the southernmost portion of the campus. The CDP establishes standards for the use and development of the campus.

The site of SRI's campus has been reduced over time. In 1978, an amendment to the CDP was approved to remove approximately 10.3 acres from the site for the development of the McCandless office complex on Middlefield Road, and in 1997, the size of the campus was further reduced when part of the property was sold to Classic Communities for the development of 33 single family residential units in the Burgess Classics development.

The existing CDP allows a maximum employee count of 3,308. The applicant indicates approximately 1,100 people are currently employed at the project site, although SRI's headcount has fluctuated between approximately 1,400 and 2,000 workers since 2003.

Lane Partners has been working with staff on this proposal and submitted a pre-application package in April 2021. On June 22, 2021, the applicant gave an introductory presentation on the project to the City Council. Although the Council didn't provide specific feedback, public comment was received. In October 2021, the applicant submitted a formal application package, with a resubmittal package submitted in January 2022. The Planning Commission reviewed the proposed project at a study session during its meeting on March 28, 2022, received public comments and asked clarifying questions. Planning Commissioners discussed the following at the study session:

- Interest in increasing residential densities and inquiries regarding an acre of land being used for a fully affordable project, in addition to the required 15% below market rate (BMR) housing units;
- Interest in reducing proposed parking and/or placing parking underground;
- Questions about the programming for the sports field and potential conflict with the adjacent church;
- Questions about the pre-pandemic parking needs and number of employees at SRI;
- Concerns about traffic congestion and interest in increased transit use for future site occupants; and
- Interest in the potential realignment of Ravenswood Avenue and Ringwood Avenue.

Excerpt minutes from the March 28th Planning Commission meeting are included as Attachment I.

The City Council held a study session on May 10, 2022, reviewed the proposed project, received public comments and asked clarifying questions. City Council members discussed the following general topics at

the study session:

- Options to mitigate noise;
- Security and bike and pedestrian safety;
- Potential shuttle programs, parking ratios, and transit passes;
- BMR requirements and other affordable housing;
- Onsite amenities/community amenities;
- Site density and intensity; and
- Water usage of the proposed project.

Site location

For purposes of this staff report, Ravenswood Avenue is used in an east to west geographic orientation. The project site is located at 333 Ravenswood Avenue and generally bound by Laurel Street to the west, Ravenswood Avenue to the north, Middlefield Road to the east and the Burgess Drive ROW to the south. The site contains 38 existing buildings, totaling approximately 1.38 million gross square feet, which include a mix of office, R&D, and support uses. The surrounding zoning and land uses are provided in Table 1 and a location map is included as Attachment A.

Table 1: Surrounding Land Uses and Zoning		
Item	Existing Land Uses	Zoning
North	Single-Family & Multi-Family Residential/Church	R-1-S/R-2/R-3/City of Atherton
South	City Corp. Yard/USGS/Multi-Family Residential	PF/R-3(A)
East	Menlo Atherton High School/Office (McCandless office complex)	City of Atherton/C-1-X
West	Civic Center/Burgess Park/Single-Family Residential (Classic Communities)	PF/R-3(X)

Most nearby buildings are one to three stories in height. Ravenswood Avenue and Middlefield Road are major city through streets (classified as “Avenues – Mixed Use” in the City’s Circulation Element). The Park Station (Caltrain) and the developing Downtown/EI Camino Real area are within walking and biking distance.

Project overview

The applicant is proposing to comprehensively redevelop the SRI campus with a residential, office, R&D, and retail mixed-use project. The proposed project would be divided into an approximately 53-acre office/R&D campus covering most of the existing project site and a 10-acre residential area along the Laurel Street edge of the project site extending slightly east along Ravenswood Avenue. Site circulation, open space, and landscaping (other than retained trees) would be redesigned and rebuilt per a new comprehensive campus plan, including new bicycle and pedestrian connections. There would be no net increase of non-residential square footage. Primary program elements include:

- Approximately 287,000 square feet of existing office/R&D (retained in Buildings P, S, and T);
- Demolition of 35 structures comprising approximately 1.1 million square feet, to be replaced with new office/R&D space in five main structures, three to five stories in height, along with a smaller amenity building;
- Three new parking structures for the non-residential uses;
- 450 multifamily residential dwelling units (19 townhomes at two stories) and (431 apartments at three to

- six stories) in approximately 500,000 square feet of floor area;
- An approximately one-acre portion of land, proposed to be dedicated to an affordable housing developer for the future construction of a 100 percent affordable housing or special needs project of up to 100 dwelling units;
- Approximately 25 acres of landscaped, publicly-accessible open space, including a large central open space between the office/R&D buildings; and
- A sports field and one-story community building adjacent to the Ravenswood Avenue/Middlefield Road intersection.

As part of the proposed project, the existing 6-megawatt natural gas power plant that generates power and steam energy for the existing SRI International campus would be demolished and the entire project site would be converted to all-electric energy usage, with the exception of two of the existing buildings that would remain (Buildings P and T) and potential backup diesel generators, in compliance with the city Reach Code. (It is possible that limited exceptions may be requested to accommodate additional life science uses.)

The project plans are included in Attachments B, C and D and the applicant's project description letter is included in Attachment E.

CEQA review

An EIR is an informational document that the City must prepare and consider before any discretionary action is taken by the City on the proposed project. The purpose of an EIR is to provide decision makers and the public with detailed information about the effect that the proposed project may have on the environment, list ways in which the significant effects of the proposed project might be minimized and identify alternatives to the proposed project. The main substantive components of an EIR are as follows:

- The project description, which discloses the activities that are proposed for approval;
- Discussion and analysis of the potentially significant environmental effects of the proposed project, including cumulative impacts and growth-inducing impacts;
- Discussion of ways to mitigate or avoid the proposed project's potentially significant environmental impacts; and
- Discussion of alternatives to the project as proposed.

The EIR process begins with the City's decision to prepare an EIR. The City determined that an EIR was required for the proposed project and issued a Notice of Preparation (NOP). The proposed project requires a full EIR instead of a focused EIR as has been prepared for some projects in the Specific Plan and Bayfront areas. Because this will be a full EIR, an initial study was not prepared as has been done for projects that utilize a focused EIR; this is because a full analysis will be done in the EIR of the proposed project's potential impacts. The City released the NOP (Attachment F) on December 2, 2022.

The draft EIR will be prepared and processed in accordance with CEQA and the State CEQA Guidelines in effect at the time of the release of the NOP.

CEQA topic areas included in EIR

The EIR will analyze whether the proposed project would have significant environmental effects in the following topic areas:

- Aesthetics¹
- Air Quality
- Biological Resources
- Cultural Resources
- Energy
- Geology and Soils
- Greenhouse Gas Emissions
- Hazards and Hazardous Materials
- Hydrology and Water Quality
- Land Use and Planning
- Noise
- Population and Housing
- Public Services and Recreation
- Utilities
- Transportation and Traffic
- Tribal Cultural Resources

To help prepare several of these sections and analyze the potential impacts, a transportation impact analysis (TIA) will be prepared in accordance with the City's TIA Guidelines. The EIR will use vehicle miles traveled (VMT) as the CEQA threshold of significance for transportation and traffic. In addition, while not required by CEQA, a housing needs assessment (HNA) will be prepared to inform the population and housing analysis.

CEQA topic areas not requiring further analysis

The proposed project is not anticipated to result in environmental impacts in the following topic areas:

- Agricultural or Forestry Resources
- Mineral Resources
- Wildfire

The project site is fully developed in an urbanized area and within a transit priority area. As such, agricultural and mineral resources do not exist on the site and wildfires are not considered a concern. A detailed analysis of these topics will not be included in the EIR. Therefore, these topic areas are currently scoped out of the EIR. This, however, does not limit the public or Planning Commission's ability to comment on the scope and content of the EIR relative to these topic areas.

Analysis

EIR Scoping Session

The City released a Notice of Preparation (NOP) (Attachment F) for the proposed project on December 2, 2022, beginning an extended review and comment period ending on January 9, 2023 to account for the City Hall closure from December 26, 2022 through January 2, 2023. Hard copies are also available for review at the Menlo Park Main Library and Belle Haven Branch Library. Interested persons should inquire at the library reference desk.

A NOP signals the City plans to prepare an EIR for the proposed project and begins the EIR process. The NOP and scoping process is designed as an early opportunity to seek guidance from interested parties,

¹ The project site is located within a "transit priority area", as defined, and thus pursuant to Public Resources Code Section 21099, aesthetic and parking impacts are not considered significant impacts on the environment. Accordingly, the analysis in the EIR will reflect this statutory directive. Nevertheless, the City still retains authority to consider aesthetic impacts pursuant to its design review authority.

agencies and members of the public on the scope and content of the EIR. The EIR is an informational document the purpose of which is to provide decision makers and the public with detailed information about the potential impacts that the proposed project may have on the environment, list ways in which the potentially significant impacts of the proposed project might be minimized, and identify alternatives to the proposed project.

The December 12, 2022 Planning Commission meeting falls within the required minimum 30-day comment period, and serves as a scoping session for the proposed project. Comments can be made on the scope, content, and focus of the analyses in any of the CEQA topic areas, including the topics proposed to be scoped out of the EIR. Examples of comments include, but are not limited to, suggested mitigation measures, suggested alternatives (e.g. increase or decrease in housing units, commercial square footage, other uses etc.), or areas of study that should not be scoped out. These topics are only examples to help provide context to the Commission, interested agencies, and members of the public on the types of comments that could be provided on the EIR scope and are not intended to limit the scope of comments.

Verbal comments received during the scoping session and written comments received during the NOP comment period on the scope and content of the environmental review will be considered while preparing the draft EIR. NOP comments will not be responded to individually; however, all written comments on the NOP will be included in an appendix of the draft EIR, and a summary of all comments received (both written and verbal) on the NOP will be included in the body of the draft EIR.

Analysis of proposed office and R&D

The Office/R&D District buildings would be flexibly designed to accommodate office or R&D tenants, including life science uses, depending on future tenant and market needs. Likely the proposed project buildout would contain a mixture of these uses. Because future tenants have not been identified, the EIR will evaluate two scenarios: a 100 percent office scenario and a 100 percent R&D scenario. Each section in the EIR will evaluate the most intense scenario for the resource area being analyzed. This will ensure that the EIR evaluates the proposed project's maximum potential impact, and that any future tenant mix is within the scope of the EIR. The applicant indicates anticipated tenant occupancy levels within the Office/R&D District would be consistent with current market demands:

- Office: Approximately one occupant per 250 square feet
- R&D: Approximately one occupant per 350 square feet to 425 square feet for life sciences

Variants

Variants are variations of a project at the same project site, with the same objectives, background, and development controls but with additions and changes from a project, whose inclusion may or may not reduce environmental impacts. Thus, variants are distinct from "alternatives" (discussed below) insofar as CEQA requires the consideration of alternatives to avoid or lessen significant effects of a project. The EIR will include variants proposed by the Project Sponsor or the City and the description and analysis of the variants will be equal in detail to those of the proposed project. The EIR will describe and analyze the following variants:

- Emergency Reservoir Variant: This variant would be similar to the proposed project except it would also include an approximately 2 million gallon below grade concrete water reservoir and associated facilities (including a pump station building, surge tank, and well head) that would be aboveground and surrounded by a fence or screen. The area for the emergency reservoir and associated facilities would be leased by the City. The specific location of the emergency water reservoir and associated facilities within the project site has not yet been determined, but would likely be located on the northeastern portion of the project site.

- **Increased Residential Variant:** This variant would be similar to the proposed project except it would include up to 600 multi-family residential units, 50 more dwelling units than under the Proposed Project. The additional residential dwelling units would be located along Laurel Street within the Residential District. As a result, the proposed building height along Laurel Street would increase and additional subterranean parking may be required.

Alternatives

If there are significant impacts, the alternatives analysis will focus on those alternatives that would reduce identified impacts. If the impacts are less than significant with mitigation, the alternatives analysis is anticipated to focus on those alternatives that would further reduce those impacts or provide policy focused alternatives considering allowable development under the Zoning Ordinance. Section 15126.6(e) of the State CEQA Guidelines requires the evaluation of a No Project Alternative. Other alternatives may be considered during preparation of the EIR and will comply with the State CEQA Guidelines, which call for a “range of reasonable alternatives to the project, or to the location of the project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project.” The City is currently considering analysis of the following alternatives, and is seeking input on these alternatives and any other potential alternative that should be evaluated as part of the EIR:

- CEQA-Required No Project Alternative (maintaining the existing buildings with no new construction);
- Project Alternative that would reduce any environmental impacts; and
- Policy focused project alternative.

Next steps

Following the close of the comment period on the scope and content of the EIR, City staff and the consultant will consider all comments in the development of the draft EIR. The draft EIR is tentatively planned to be released in the summer of 2023 with a minimum 45-day public review and comment period. During the 45-day review and comment period on the draft EIR, the Planning Commission would hold a public hearing to discuss the draft EIR at which interested persons would be able to provide comments. Once the draft EIR comment period is completed, the environmental consultant will review and respond to all comments received in what is referred to as a “Response to Comments” document or final EIR.

Study Session

Planning Commission considerations

The study session portion of this report highlights a variety of topic areas and discussion items for consideration. As the Planning Commission reviews the proposal, staff recommends that the Commission consider the following topics and use these as a guide to ask clarifying questions:

- Proposed land uses and site density and intensity;
- Site access, including vehicular, pedestrian and bicycle;
- Architectural styles;
- Design and layout of open space;
- Parking locations and ratios; and
- Proposed sustainability measures.

Project updates

The addition of a separate parcel of land to be leased to an affordable housing developer and the increase from 400 to 450 proposed dwelling units, not including the separate parcel, are the main revisions to the proposed project since the previous Planning Commission study session on March 28, 2022. Smaller revisions, including orienting proposed buildings to make the main publicly accessible open spaces visible

from the public streets and some square footage updates have also been made and are discussed below.

Office/R&D district

The office/R&D district would be located in the middle of the site and extend to the eastern and southern property lines as well as to a portion of the northern property line. The applicant proposes that the new office/R&D buildings would be designed for established and emerging businesses. The project would consist of approximately 1.1 million square feet of office/R&D in five new buildings, an office amenity building, and a new community building. The project site currently contains approximately 1.38 million square feet of R&D/office uses. Existing Buildings S and T, located to the west of the USGS site, and Building P, located to the east of the proposed new residential buildings along Laurel Street, total approximately 283,826 square feet and would be retained for SRI’s continued operations. Table 2 provides additional information on the buildings that would comprise the non-residential uses and minor square footage adjustments made since the previous submittal.

Table 2: Non-residential buildings		
Building	Gross floor area (March 2022)	Gross floor area (December 2022)
Bldg. 1 (3 stories)	165,000 sf	184,000 sf
Bldg. 2 (5 stories)	244,000 sf	227,300 sf
Bldg. 3 (5 stories)	244,000 sf	227,300 sf
Bldg. 4 (4 stories)	198,000 sf	229,000 sf
Bldg. 5 (4 stories)	198,000 sf	184,000 sf
Office Amenity Bldg. (2 stories)	44,719 sf	40,000 sf
Community Bldg. (1 story)	2,000 sf	2,002 sf
Sub-Total (new)	1,095,719 sf	1,093,602 sf
Bldg. P (existing to remain)	180,519 sf	183,423 sf*
Bldg. S (existing to remain)	21,241 sf	21,241 sf**
Bldg. T (existing to remain)	82,066 sf	82,066 sf**
Sub-total (existing to remain)	283,826 sf	286,730 sf
TOTAL	1,379,545 sf	1,380,332 sf

*This number is a correction of the existing square footage for Building P

** These square footages represent the existing square footages and do not reflect any changes associated with SRI’s separately proposed tenant improvements. The applicant indicates the tenant improvements are estimated to yield approximately 3,000 additional square feet within Building P and a reduction of approximately 6,000 square feet within Building S.

The applicant proposes that the non-residential portion of the project would be accessible to vehicles from two entrances along Ravenswood Avenue and two entrances along Middlefield Road. The applicant indicates the proposed office/R&D buildings would be arranged to form a central aggregated, publicly-accessible open space, and the proposed architectural character of the buildings would be modern, with

building masses defined by main entrances, first floor articulations such as loggias, elevated exterior balconies, and the use of natural materials and integrated indoor/outdoor workspaces. As shown in the preliminary architectural plans exterior cladding systems under consideration include terracotta rainscreen, glass fiber reinforced concrete, metal panel, stone, and other natural materials.
materials.

An approximately 40,000-square-foot, two-story office/R&D amenity building is proposed directly adjacent to Parking Garage 3. The building would contain a full-service café and other amenities, including a possible fitness center, for SRI and the site’s commercial tenants.

The project plans identify that parking for the non-residential uses would be provided in three parking garages, three to four stories tall, and surface parking areas located throughout the site. Parking Garages 1 and 2 would be located along the eastern property line and Parking Garage 3 would be located more centrally near the southwest of the project site, just south of the office amenity building. Buildings 1 and 5 would each have some underground parking spaces as well.

Community building

An approximately 2,000-square-foot, one-story, community-serving building is proposed to be located on the northeast corner of the site, across Middlefield Road from Menlo Atherton High School. The applicant indicates this building would include community-serving retail uses, which may include a bicycle repair shop and juice bar, and publicly-accessible restrooms. As project review continues, the uses within this building would be further refined by the applicant. This building is proposed to be adjacent to a publicly-accessible open space, which could provide community functions, such as a recreational field, public parking, and a children’s play area. The public parking would be available to users of the publically-accessible open space and community building, and the neighboring church would use some spaces, as they currently use some SRI parking spaces per parking agreements. The applicant indicates specific programming functions for the community building and surrounding facilities would be determined in coordination with the City and community.

Residential district

The proposed 450 housing units would consist of approximately 431 apartments and 19 townhomes, with 15 percent of units proposed to be affordable units pursuant to the City’s BMR housing program. Table 3 below indicates the proposed unit types and totals. As currently proposed, the totals include BMR units but the specific numbers of BMR units for each unit type and income level have not been determined.

Table 3: Residential Units		
Unit Type	Unit total (March 2022)	Unit total (Dec. 2022)
Studio	70	75
1 bedroom/1 bath	175	198
2 bedroom/2 bath	125	144
3 bedroom/ 2 bath	11	14

3 bedroom/2bath (townhouse)	19	19
Total	400	450

The residential district would extend from the Burgess Classics neighborhood along Laurel Street north to Ravenswood Avenue and east, partially along Ravenswood Avenue. Approximately 19 rental townhouses would be located between the apartment buildings and the Burgess Classics neighborhood to further diversify the housing mix and provide a scaled transition from the multi-family buildings to the single-family residences. The 431 apartments would be distributed between the three buildings, three to six stories in height, and a total of approximately 500,000 square feet of gross floor area. All residential units are noted in the project description to have some type of exterior deck or patio.

Since the previous study session, the proposed project has been revised to include a separate approximately one-acre portion of land, proposed to be dedicated to an affordable housing developer for the future construction of a 100 percent affordable housing or special needs project which would be separately rezoned as part of the proposed project for up to 100 units. The exact location of this dedicated land area has not been determined.

Parking for the apartments is proposed to be above-grade, in one-story garages, creating a podium on the second floor for common open space for each apartment building. Residential buildings 1 and 2 are also now designed to include one level of subterranean parking. The majority of the garages would not be visible as apartments would partially wrap the sides of the parking structures on the first level. There would also be some surface parking along the private street adjacent to apartment buildings for short-term and visitor parking. The townhome portion of the project would be organized around its vehicle access, with the parking spaces for the townhomes in attached garages.

The project description indicates the buildings would be Mission Style (i.e., Spanish derivative) with white stucco walls, heavy timber brackets and detailing, and clay tile roofs. Building massing would include peaked/sloped rooflines. Additionally, the applicant indicates main building entrances would be highlighted along the street with landscaping, human-scaled plazas, lighting, and trellis structures.

The proposed residential units would be rental units. The applicant indicates that a ground lease for the residential units, and the rest of the project site, is anticipated and this would limit the ability to include for-sale units.

Vehicular access and site circulation

The proposal includes separate vehicular circulation for the residential and office/R&D uses although paths for pedestrian and bicycle access would provide connections between the two elements. A loop road, with access off of Ravenswood Avenue and Middlefield Road, would provide access to the office/R&D buildings and the community building. The apartment buildings are proposed to have their own access road with entry points at Laurel Street and Ravenswood Avenue. The townhomes are proposed to have a separate access directly from Laurel Street, which would not connect to the road between the apartment buildings or the loop road. As shown on the master plan project plans (Attachment B), there would also be emergency vehicle access from the apartment buildings to the loop road and from the loop road to Laurel Street. There would also be emergency vehicle access to the loop road from Burgess Drive. The applicant indicates a security

gate for emergency access and limited service vehicles would likely be located where Burgess Drive intersects the loop road, however, the gate would not impede bicycle or pedestrian circulation.

The applicant's proposal states the circulation design would achieve the following objectives:

- Establishment of private internal streets and roads;
- Separation of office/R&D from residential access and circulation;
- Creation of on-site roads to manage internal vehicular circulation and access to office/R&D and residential buildings;
- Minimization of additional vehicular circulation to and from Laurel Street;
- Three access points to the residential portion of the site (one along Ravenswood Avenue, toward the west side of the site, one along Laurel Street for the multi-family residential buildings, and a separate driveway entrance along Laurel Street for the townhouses);
- An internal road to the three main residential buildings and vehicular access to parking garages and loading areas;
- Four access points to the office/R&D portion of the site (two along Ravenswood Avenue and two along Middlefield Road, with one at Ringwood Avenue and one at Seminary Drive); and
- An internal loop road to provide access to all of the office/R&D buildings, office amenity building, community building, parking garages, surface parking areas, loading areas, as well as emergency vehicle access.

The applicant indicates the project would develop a project-specific TDM (Transportation Demand Management) plan for both the residential and non-residential uses to reduce the total number of single-occupancy vehicle trips affiliated with the project by 20 percent, with a TDM plan that would complement the mixed-use campus' proximity to downtown and the Menlo Park Caltrain station. The applicant indicates that they anticipate the Project would provide electric-powered shuttles for use by employees and residents for access to and from the Caltrain station.

Pedestrian and bicycle circulation

A Class I multiuse bicycle and pedestrian path would be located on the north side of the site along Ravenswood Avenue. This on-site path would create a protected alternative option for bicyclists currently using the bike lane on Ravenswood Avenue. The Class I path would loop southward into the project site toward the east and provide a crossing at Ringwood Avenue and Middlefield Road. This would provide safe access to Menlo Atherton High School and would connect to the existing bicycle path on Middlefield Road. A Class I multi-use bicycle and pedestrian path would extend from Laurel Street at Burgess Drive along Burges and the south side of the project site to connect to Middlefield Road at Seminary Drive. On the west, this path would be situated at Laurel Street to connect to the City's proposed Caltrain undercrossing at El Camino Real.

The proposed bicycle and pedestrian connections through the site would link with a broader network of existing and planned infrastructure, as can be seen on the map included as Attachment G from the City's Transportation Master Plan. The proposed Middle Avenue undercrossing would connect bicycle/pedestrian infrastructure to the west of El Camino Real with the bicycle/pedestrian path along the southern edge of the project site. At Middlefield Road, bicyclists would be able to travel east along Ringwood Avenue to the US 101 bicycle and pedestrian bridge, through the Belle Haven neighborhood and access the Bay Trail through

the recently opened bicycle/pedestrian bridge over Bayfront Expressway at the Meta West Campus.

Parking

Overall, the parking rate for the non-residential uses would be approximately two spaces per 1,000 square feet. According to City records, the current parking rate for the project site is approximately 2.3 spaces per 1,000 square feet. For comparison, the LS (Life Sciences) district in the Bayfront area requires a maximum of 2.5 parking spaces per 1,000 square feet and a minimum of 1.5 parking spaces per 1,000 square feet for R&D uses and the O (Office) district requires a maximum of three spaces and a minimum of two spaces per 1,000 square feet of office space.

The parking rate for the residential dwelling units would be approximately one space per apartment and two spaces per townhome. The applicant indicates in their project description letter that shared parking would be available for residential visitors on evening and weekends at the office/R&D surface lots and parking structures. While parking rates vary throughout the zoning districts, the R-MU (Residential Mixed Use) zoning district has an emphasis on residential and requires a minimum of one parking space per unit. This district also limits permitted parking to a maximum of 1.5 spaces per unit.

Trees, landscaping, and open space

The applicant indicates their landscape concept is to create a network of publicly-accessible pedestrian and bicycle trails, parks, open spaces, and active/passive recreational areas, incorporating many existing and new trees. Additionally, the applicant indicates open space would also be utilized to create welcoming edges along Ravenswood Avenue, Laurel Street and Middlefield Road. The three main open space areas are described below.

Ravenswood Avenue Parklet

The Ravenswood Avenue parklet would be approximately six acres located on the northerly edge of the site along Ravenswood Avenue and would protect the existing heritage trees and provide a landscaped and screened frontage. A shared use path would weave through the existing trees in the setback area to connect with and support pedestrian and bicycle circulation throughout the site. Small scale public spaces, such as picnic areas and exercise stations would be connected to the shared-use path. The parklet would lead to a large multi-use plaza which would provide a visual connection to the Parkline Central Commons.

Parkline Recreational Area

The Parkline Recreational Area would provide a community recreational sports area of approximately two acres, located on the northeast corner of the site at the intersection of Ravenswood Avenue and Middlefield Road. This area would be connected to the Ravenswood shared-use path. This open space area would provide publicly accessible community functions, such as a recreational field, public parking, a children's play area, and other activity areas. In addition, the approximately 2,000-square-foot community amenities building would contain publicly accessible restrooms, and potentially small retail spaces. The City is exploring a possible partnership with Parkline to evaluate opportunities for emergency water supply and/or storage facilities in this area, and this concept is included as a project variant and described under the EIR Scoping Session portion of this report.

Parkline Central Commons

The Parkline Central Commons would provide an approximately 9-acre central open space area located between the Office/R&D buildings and the office amenities building. This space would offer a variety of programmed open space, such as flexible-use lawn areas and a multi-use plaza that can accommodate gatherings. The Parkline Central Commons may also include an event pavilion and landscaped areas. Additionally, smaller landscaped spaces for tenant use would be located adjacent to the buildings, which would provide outdoor seating and shaded tree groves. Primary pedestrian circulation paths would connect all the edges of the site to the Parkline Central Commons.

Since the previous study session, the applicant has revised the plans to increase the visibility of the central publically accessible open space by relocating the office amenity building to be directly adjacent to Parking Garage 3 and reorienting Office Building 5. These revisions to the site plan would allow the Central Commons to be visible from Laurel Street, and to lesser extent from Ravenwood Avenue.

Heritage trees

The site currently contains 565 heritage trees, of which 351 would be retained and 214 would be removed. Including non-heritage trees, approximately 615 trees would be retained on site and an additional 912 trees are proposed to be planted. The size/age of the trees to be planted has not yet been determined but as the plans develop, staff will work with the applicant to determine appropriate tree sizes/ages. A complete tree survey and disposition plan is included as hyperlink Attachment H. The applicant indicates their tree management and retention plan is based on the following:

- The preservation of healthy heritage trees that are of a desirable tree species;
- Special effort to preserve coastal live oaks, valley oaks, and coast redwoods based on their native habitat and ecological significance; and
- Incorporation of existing heritage trees into the overall design.

As the project review continues, the Planning Division and City Arborist team will review and evaluate the arborist report, the tree disposition and removal plans, and determine whether the requested heritage tree removals are supportable based on the information to be provided with heritage tree removal permit applications. If the City Arborist approves some or all of the removals, his or her decision is appealable to the Environmental Quality Commission. Further, as part of that review, the City will evaluate the potential impacts of the project on the heritage trees proposed to remain and work with the applicant team to identify preservation measures. The heritage tree replacement plan would be subject to the City's valuation requirements for replacement trees. The replacement plan will be incorporated into subsequent reviews of the proposed project.

Sustainability

The applicant indicates the project would incorporate the following sustainability measures:

- Pursue certification by the state as an Environmental Leadership Development Project (under SB 7). As part of that certification, the proposed project would need to demonstrate that it would result in no net additional GHG emissions compared to existing conditions.
- Source-separating and tracking waste throughout construction to divert waste away from landfills.
- Demolition of most existing buildings onsite, including the cogeneration plant, and replacement with more energy efficient buildings.
- Incorporation of a range of LEED certification strategies or equivalent standards across the Office/R&D

- and Residential Districts, including minimum LEED Gold certification by the USGBC or equivalency verified through the City of Menlo Park's LEED Performance Program, and related certifications;
- LEED New Construction certification or equivalent standards for multifamily residential buildings;
 - LEED for Homes certification or equivalent standards for residential.

Correspondence

As of the writing of this report, staff has received one item of correspondence regarding the project since the previous study session. The email, included as Attachment J, discusses concerns regarding the sports field.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the proposed project. The project sponsor is also required to fully cover the cost of work by consultants performing environmental review and additional analyses to evaluate potential impacts of the project.

Environmental Review

An EIR will be prepared for the proposed project. On October 18, 2022 the City Council authorized the City Manager to enter into a revised contract with ICF to complete the environmental review and prepare an EIR for the proposed project. The Planning Commission would provide a recommendation to the City Council on the project entitlements including the certification of the EIR, after the completion of the environmental review.

Public Notice

Public notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 1,320-foot radius of the subject property.

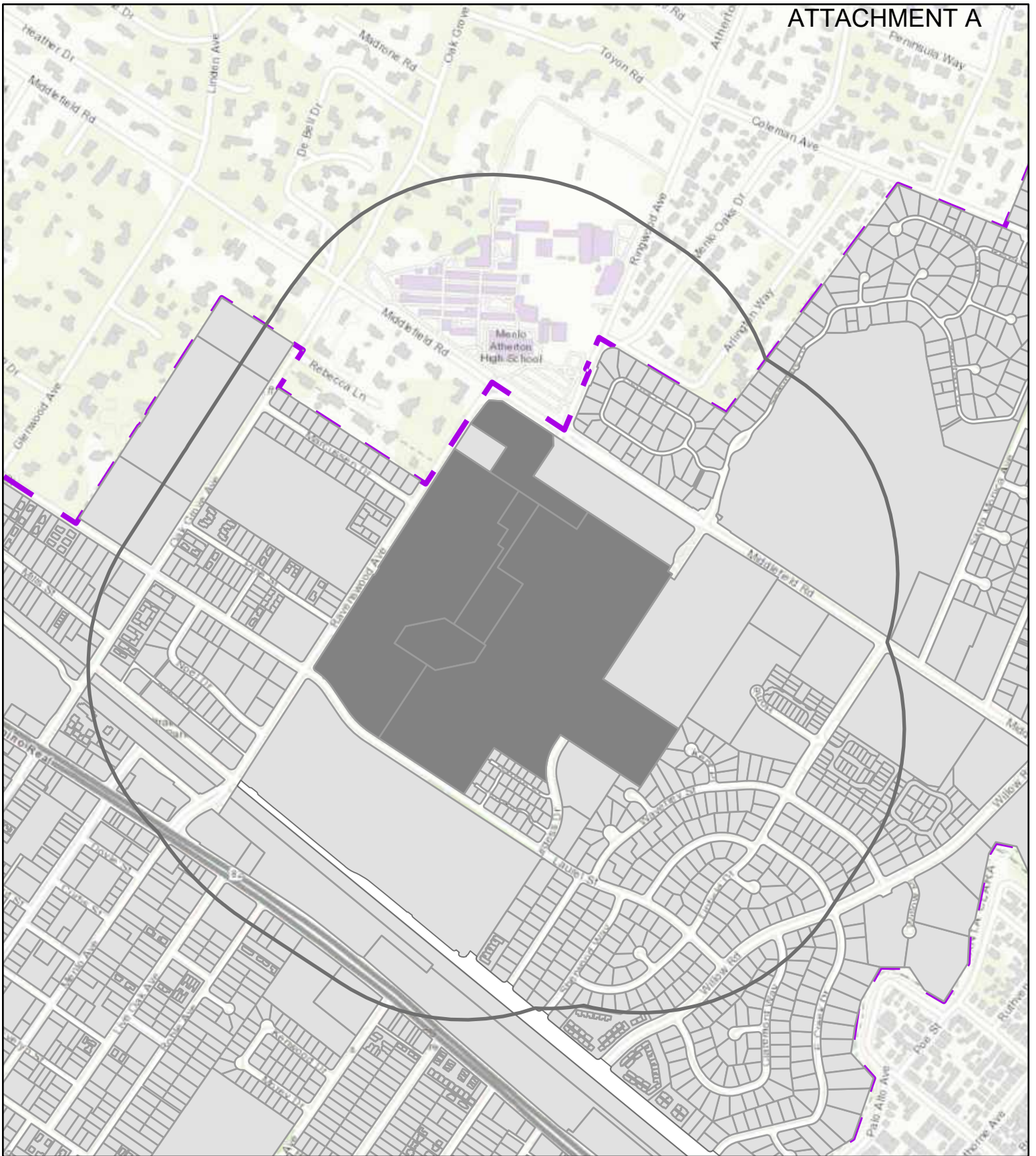
Attachments

- A. Location Map
- B. Masterplan Project Plan: – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/20221031-parkline-masterplan-plan-set.pdf>
- C. Non-residential Project Plans – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/20221031-non-residential-architectural-plans.pdf>
- D. Residential Project Plans – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/20221031-residential-architectural-control-plans.pdf>
- E. Project Description letter – hyperlink: <https://menlopark.gov/files/sharedassets/public/services/community-development/documents/projects/under-review/parkline/20221205-parkline-project-description.pdf>
- F. Notice of Preparation – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/parkline-notice-of-preparation.pdf>
- G. Existing and Proposed Bike Paths from Transportation Master Plans

- H. Tree disposition Plan – hyperlink: <https://menlopark.gov/files/sharedassets/public/community-development/documents/projects/under-review/parkline/parkline-tree-disposition-plan.pdf>
- I. Excerpt minutes of Planning Commission meeting on March 28, 2022
- J. Correspondence

Report prepared by:
Corinna Sandmeier, Acting Principal Planner

Report reviewed by:
Kyle Perata, Planning Manager
Michael Biddle, Special Counsel



CITY OF MENLO PARK

LOCATION MAP

333 RAVENSWOOD AVENUE - PARKLINE



CITY OF
MENLO PARK
A1

Scale: 1:9,000

Drawn By: CDS

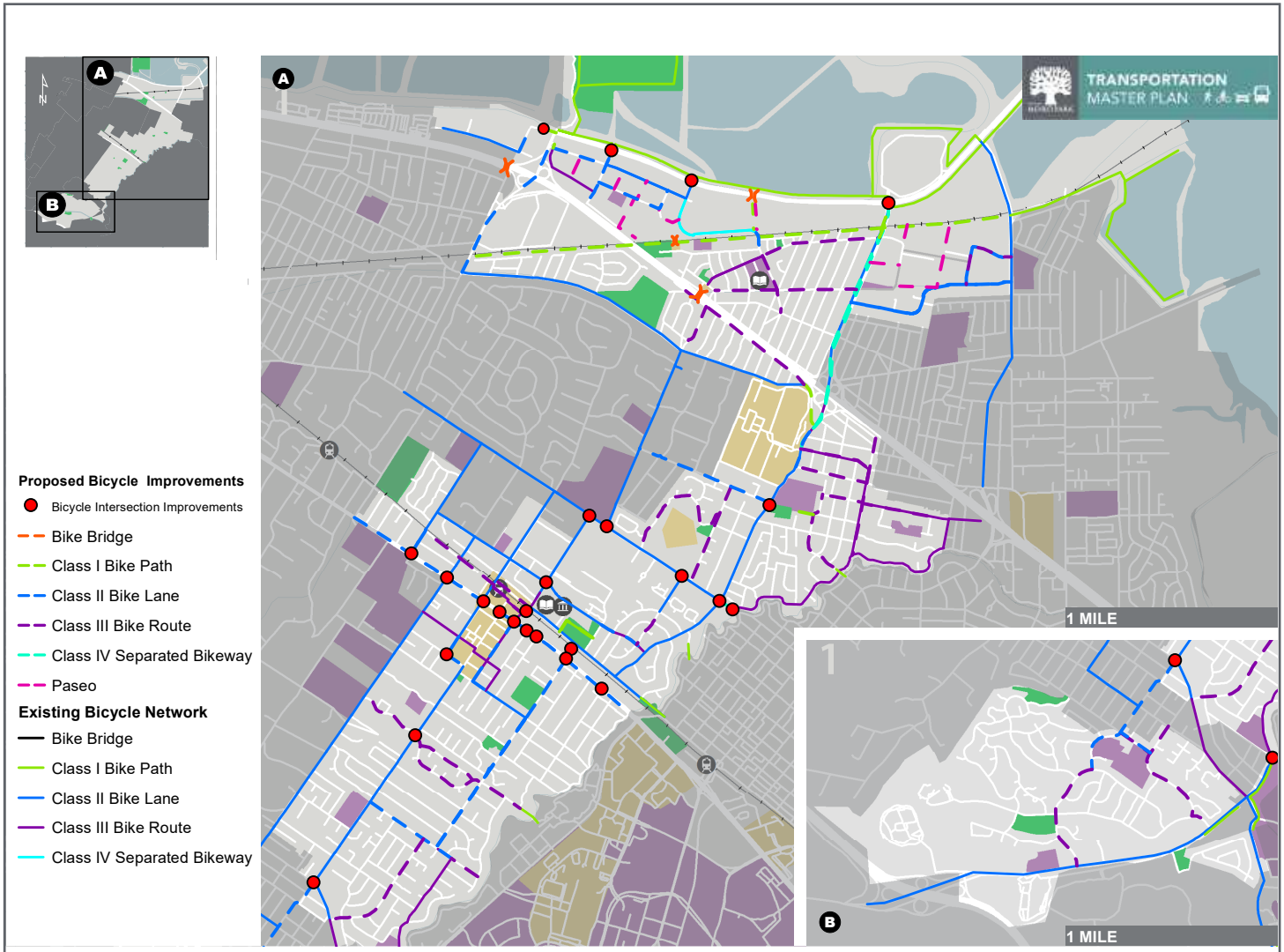
Checked By: JICM

Date: 5/10/2022



Recommended TMP Projects

EXISTING BIKE NETWORK AND RECOMMENDATIONS



Recommendations for people biking at various locations throughout Menlo Park will help improve safety and connections to the existing bicycle network.

Planning Commission



REGULAR MEETING MINUTES - EXCERPT

Date: 03/28/2022

Time: 7:00 p.m.

Meeting Location: Zoom.us/join – ID# 871 4022 8110

Regular Meeting

A. Call To Order

Chair Michael Doran called the meeting to order at 7:01 p.m. At Chair Doran's request, Associate Planner Matt Pruter explained how applicants and the public would be able to participate in the virtual meeting.

B. Roll Call

Present: Andrew Barnes, Chris DeCardy (Vice Chair), Michael Doran (Chair), Camille Gonzalez Kennedy, Cynthia Harris, Henry Riggs, Michele Tate

Staff: Payal Bhagat; Contract Planner; Nira Doherty, City Attorney; Fahteen Khan, Assistant Planner; Eric Phillips, Special Counsel; Matt Pruter; Associate Planner; Corinna Sandmeier, Acting Principal Planner

G. Study Session

- G1. Study Session/Nick Menchel/333 Ravenswood Avenue (Parkline):
Request for a study session on a master plan development to comprehensively redevelop the SRI campus with a residential, office, research and development, and retail mixed-use project. The proposed project includes requests for a general plan amendment, zoning ordinance amendment, rezoning, conditional development permit (CDP), development agreement (DA), architectural control, vesting tentative map, and below market rate (BMR) housing agreement. The project would necessitate the preparation of an environmental impact report (EIR) in compliance with the California Environmental Quality Act (CEQA). (Staff Report #22-018-PC)

Staff Comment: Acting Principal Planner Sandmeier said 23 new emails had been received on the proposed project, and that many were in favor of additional housing and additional BMR housing; and some expressed concerns about the impacts to neighbors and to the church located at 201 Ravenswood Avenue. She said the existing SRI campus was an approximately 63-acre site with 38 buildings and 1.38 million square feet of gross floor area. She said the proposed project had no net increase of nonresidential square footage and that approximately 284,000 square feet would be retained for SRI's use in Buildings P, S and T. She said approximately 1.1 million new square feet of office and research and development uses were proposed in five main structures from three to five stories, a new office amenity building, and three parking structures for nonresidential use. She said the proposal included 400 residential rental units. She said that included 15% Below Market Rate (BMR) units, 19 two-story townhomes with attached two-car garages, 391 apartments in three buildings, three to five stories tall, and approximately one parking space per unit and one-story parking garages with podiums at the second level for private open space for the apartments. She

said the proposal also included a sports field and a one-story community building adjacent to the Ravenswood Avenue and Middlefield Road intersection, 25 acres of landscaped publicly accessible open space, and new pedestrian and bicycle paths and connections through the site.

Ms. Sandmeier highlighted that the proposed circulation was private internal streets, an internal road to the three main residential buildings and parking garages, and an internal loop road to provide access to all nonresidential buildings, parking garages, surface parking areas, loading areas and for emergency vehicle access. She described the entry points for each of the building types. She said the requested entitlements included a General Plan Amendment, Zoning Ordinance Amendment, Rezoning, Development Agreement, Conditional Development Permit, architectural control for the new buildings, and a vesting tentative map to merge existing walks and create new parcels.

Ms. Sandmeier said topics for the Commission's consideration were the proposed land uses including site density and intensity, the site layout including building orientation and site access, conceptual architectural styles, design and layout of open space, parking locations and ratios, and proposed sustainability measures.

Questions of Staff: Commissioner DeCardy asked if this project would be reviewed standardly or whether it would have unique review.

Ms. Sandmeier said it would require a number of public hearings both at the City Council and Planning Commission as the environmental review progressed but it would be similar to what they saw with other projects.

Applicant Presentation: John McIntire, SRI, said that they were collaborating with a local firm Lane Partners to reimagine the site to serve both SRI's and the community's needs.

Mark Murray, Lane Partners, said their firm was Menlo Park based with an office about a half mile from the SRI campus. He said they had met with City staff and the Fire District, with community groups and had one on ones with dozens of residents. He said they held a series of open houses last summer before making their initial submittal in the fall. He said three of those were open to the general public and then they held a fourth specifically for the Burgess Classics neighborhood. He said those 32 homes shared a property line with the SRI site. He said that meeting was focused on the design particularly regarding the buffer zone between those properties and SRI. He said they received constructive feedback and were able to implement changes that responded to that.

Mr. Murray said one of their goals was to open up what currently was kind of a void in the center of town. He said the existing campus was large and for the most part had had security fencing around it. He said they envisioned as the Parkline name implied a new district characterized by open space, noting they planned to have 25 acres of publicly accessible green space. He said the site contained numerous mature heritage trees with some species over 100 years old that many community members had never seen. He said the goal was to preserve many of those heritage trees. He said another goal was to improve pedestrian and bicycle transportation through the area. He said regarding the commercial development component they were doing a one-to-one replacement for the existing 38 buildings. He said SRI would consolidate into three of the existing buildings and the other older 35 ones would be demolished and that same square footage would be consolidated into five new state of the art R&D buildings that were much more efficient and sustainable. He said another goal shared with the community was housing and that was proposed on 10 acres closest to

the downtown and amenities. He said they were proposing 400 units at variable affordability and were open to community feedback on what the appropriate amount and types of housing were.

Thomas Yee, principal architect, Studios Architecture, referred to the site analysis and noted in addition to Mr. Murray's comments that there was an electrical substation near the corner of Ravenswood and Laurel. He said the three buildings, P, S and T that SRI was planning to retain were intended to be included in the master plan effort. He said the existing parking made up about 50% of the entire site area with the building footprint another 23% so 70% of the existing site was hard surface. He said their goal was to convert that into a more amenable resource for the community. He said onsite there were about 1,370 existing trees, a great percentage of which were heritage trees, and that it had been important to incorporate the trees into the plan. He described how in removing the fence the site would be opened up and how it might connect with other parts of the city. He described the pedestrian circulation plan and how the City's bicycle path plan might be extended through the redeveloped campus. He said regarding vehicular circulation they were purposely trying to separate residential from the office R&D and to not have any office R&D traffic go onto Laurel. He described elements of the residential portion of the development that would provide separation and enhanced open space for neighboring residential areas. He said for the residential design they took cues from the Allied Arts neighborhood and the Davis Polk building and were proposing sort of the Mission style. He provided visual imagery of the proposed design starting with Laurel Street and then from the corner of Ravenswood and Laurel toward the east with an alternative pathway that was pedestrian oriented and an alternative bicycle pathway. He showed a view if walking down Ravenswood toward one of the entrances to the office R&D side with entrances clearly defined. He said they would create signals for the public to clearly show that this was a public trail and people were welcome into the site. He showed the proposed commons area of the office R&D site and existing heritage trees and the introduction of both passive and active uses that might be utilized both by tenants and the public. He showed lastly a view to the upper right of the playing field at Ravenswood and Middlefield.

Chair Doran opened for public comment.

Public Comment:

- Sue Connelly said she saw three potential problem areas noting she was a resident of the Burgess Classics community. She said her community's chief concern was the size of the proposed project. She said the elevations shown were only of the lower story and the apartment buildings would be five, five-story buildings and three five-story buildings plus the 20 townhomes. She referred to the office noting those were also five story buildings. She said the project meant the introduction of a great number of people who had not been there before and that would put pressure on the infrastructure and on water. She said safety was another chief concern as having the area fenced for many years had protected her community on one side. She noted they were having problems with the shared gate area with unhoused people. She said they had been trying for three years to resolve this humanely to obtain services and help and had been steadily rejected. She said she and her neighbors proposed that the number and the height of the office buildings be reduced. She said having fewer office buildings meant less of an impact on housing.
- Kalisha Webster, Housing Advocate for Housing Choices, said they were a nonprofit service provider helping people with developmental and other disabilities find and retain affordable housing throughout San Mateo County. She said she was calling in support of the proposed

project but noted the City's draft Housing Element and the development need of around 1700 affordable units, nearly half of which were for very low-income level. She urged the applicants to do more with the project to serve people of all income levels and abilities. She said the site was ideally situated near transit and the downtown that supported a walkable and more sustainable community. She encouraged the city and developer to take advantage of the opportunities at the site to increase heights and densities and to include more affordable homes at all income levels and abilities. She said they supported the request for a one-acre parcel to be donated to an affordable housing developer that could develop more affordable housing at deeper levels of affordability than that under the inclusionary housing ordinance. She said a nonprofit developer was better able to serve the needs of lower income residents for the provision of more onsite support services. She said as of December 2021, 77% of Menlo Park adults with developmental disabilities still lived in the family homes, not by choice, but due to the lack of deeply affordable housing available.

- Kelly Vavor said she was a former public high school teacher and now a community volunteer engaged. She said she felt optimistic about this proposed development and grateful for the thought that had gone into it. She said she was the mother of four children and the public open space and better bicycle and pedestrian routes really resonated with her. She said the project would generate significant new tax revenue that would benefit their elementary and high school districts. She said she supported the project.
- Michal Bortnik, Allied Arts, expressed appreciation for SRI and Lane Partners for bringing a great opportunity and being open to the community's feedback. He said he liked all the open and green space, the trees, the bicycle and walking paths, and the thoughtful layout to work with the surroundings. He said it was great that hundreds of housing units were within easy walking distance of so many things. He said his only request was that more housing be provided. He noted the unfortunate reality of homelessness in the community. He said he made more specific comments in his written letter to the Planning Commission. He said at the last Commission meeting a presentation was made on development in the Bayshore area and how much new development was happening there and how quickly. He said he hoped that a double standard would not be applied here as to what was acceptable versus what was acceptable in other parts of town.
- Anna Zara, Linfield Oaks, said she supported the Parkline project as it was an ideal location due to its proximity to transportation, shopping, entertainment and recreation. She said she also supported higher density apartment buildings as part of the project so that one of those buildings might be made available to people with intellectual, developmental and physical challenges. She said many in this vulnerable population in Menlo Park were forced to relocate away from family, friends and familiar surroundings due to the lack of affordable housing.
- Verle Aebi, Linfield Oaks, said for those who lived on Laurel Street the traffic impact of the proposed project in conjunction with the projects that would be occupied in the near future on El Camino Real, the Stanford project and the other project further north on El Camino Real could put quite a few additional cars on Laurel Street as it was commonly used to cut through. He said when they got to the environmental impact analysis the traffic needed to be analyzed in conjunction with the future grade separation project, which he was sure would happen someday. He said one of those options involved cutting off Alma Street, which would put quite an increase in traffic pressure on Laurel Street. He said he thought it was discussed last summer that there should be no car access from the project even from the residential portion onto Laurel Street and

the access should all be onto Ravenswood Avenue and Middlefield Road as those were much larger streets. He said years ago SRI maintained a “black house” with very toxic gases and chemicals that were used for some of the semiconductor work on campus and if that was the case today that was inconsistent with the density housing proposed.

- Bob MacDonald, Chair, ad hoc Church Committee for the Parkline project for the Menlo Park Christian Science Church on Ravenswood, and a Menlo Park resident said on behalf of his fellow church members attending this evening, that their church had been a neighbor and partner with SRI for over 60 years. He said in the late 1950s their church did a land swap with SRI that led to their current location surrounded by SRI on three sides. He said at that time a perpetual parking agreement was made that provided parking on SRI property for services, meetings and events at their church as well as some mutual traffic flow easements that ensured traffic flow and emergency vehicle access around the perimeter of their property and the ability to exit onto Middlefield Road. He said they had identified a significant issue for their church with the proposed plan, and were requesting that the playing field be moved so it was not adjacent to them to ensure the sanctity and serenity of their religious services, meetings and events. He said they were comfortable with continuing to have parking lots, parking structures, and office buildings adjacent to their property as that would create a buffer similar to what they had enjoyed for over 60 years. He said two of the three existing mutual traffic flow easements, Ravenswood 1 and Ravenswood 2, needed to remain in place to ensure that emergency vehicles were able to get to any location around the periphery of their property. He said they would also like to reach a mutually acceptable agreement regarding the Middlefield Road connection.
- Alex Ho, said he lived near the site. He said it was great that SRI was planning to redevelop the property and help solve the City’s housing shortfall. He said Lane Partners had incorporated much input from the neighbors. He said there were two issues he hoped might be addressed. He noted the egress from Burgess Drive and that it was specified during the presentation as a locked gate but he wondered about assurances that it would remain so in the future. He said the entry would drive additional commute traffic through the Linfield Oaks residential neighborhood and more importantly along Laurel Street, which was the Peninsula Bicycle Corridor and used by numerous children going back and forth to Encinal School. He said it was really important to look at traffic flows along Laurel Street. He asked what could be done to ensure that unhoused people did not start camping along the bicycle path and behind the Burgess Classics adjoining homes. He said currently people were sleeping on the sidewalks back there. He said also there was a history of shopping cars and garbage being left in the neighborhood, and the SRI back fence served as a homeless laundry every weekend. He asked that this be addressed through the project development.
- Emily Simonson, Laurel Street resident, said she supported the proposed project. She noted the thoughtful planning, additional housing, and the addition of better and safer ways to commute by bike and walking. She said as a mother of three young children that was lacking in this area. She said she appreciated the addition of green space as it was a rare opportunity to create more green space while creating more housing.
- Ken Chan said he was an organizer with the nonprofit Housing Leadership Council of San Mateo County. He said they worked with communities and their leaders to produce and preserve quality affordable homes. He expressed appreciation for SRI and their partners for the proposal. He said while the 400 proposed housing units would address the housing and jobs imbalance there was much more that could be done. He said they would like the project proponents to partner with an

affordable housing developer to provide the highest number of affordable homes at the deepest affordability levels that would include services and support for residents such as after school care, computer lab, playgrounds and other amenities.

- Adina Levin, resident, said she served on the Complete Streets Commission but was speaking for herself. She said the proposed development was near amenities and offered paths and green space for people to enjoy and go to and from without really having to use cars for numerous short local needs and potentially near jobs. She said a letter recently sent to the City Council observed that southern California cities were ahead of Menlo Park in developing draft Housing Elements and had had their Housing Elements rejected due to unviable sites and lack of affirmatively furthering fair housing. She said it was pointed out that Menlo Park was at risk of a similar situation. She said she agreed with others to have additional homes particularly deeply affordable homes to accommodate housing needs.
- Karen Grove, Housing Commission, said she was speaking for herself. She said she supported the project noting the bike and walking paths, preservation of the beautiful trees, and the housing. She agreed that the site could be used for more housing and highlighted the comments made by Housing Choices noting the relationship of homelessness to low income. She said she supported the property owner donating land to an affordable housing provider to partner with to provide homes and support services. She said the Housing Element was dependent upon affordable housing. She said she canvassed nearby residents of the project over the weekend and found that many were supportive of more housing, more affordable housing, more extremely low-income homes through the dedication of land and partnership with a nonprofit provider for this proposed project.
- Brittani Baxter, District 3 resident, said she lived within walking distance of the proposed project and loved the idea of opening up the site. She said her neighborhood was walkable and fantastic and she would love for more people to have that opportunity. She said she shared the enthusiasm for the future of this project and what this once in a generation opportunity meant for the city. She said concerns were expressed about traffic and parking and the site was perfectly located wherein a person would not actually need a car to get around. She suggested the site be set up with things in place to encourage people to choose more sustainable, ecofriendly, and congestion-reducing transit. She said using space for homes and people was preferable to using it for car storage. She said given the scale of the site there was a great opportunity to think about everybody in the community and help create that much needed difficult to create affordable housing especially for populations with specific needs. She noted the density of Bayshore projects with 100 units per acre and 40 units here per acre and suggested more could be done.
- Lynne Bramlett, District 3 resident on Mills Court, said she was speaking for herself noting she also led the disaster preparedness organization MPC Ready, which focused on Menlo Park and the unincorporated county islands within or adjacent to Menlo Park. She said their focus was disaster prepared neighborhoods as research showed in a disaster the most immediate source of help was the neighbors living closest. She said there were serious gaps in the local government's disaster preparedness. She said development projects represented opportunities to significantly improve disaster preparedness through the community amenity process. She said she agreed with another speaker's suggestion about the idea of putting underground water cisterns in new development. She said the city had less than one day's worth of stored water for emergency medical drinking and water was also essential for firefighting. She said fires were

secondary consequences of earthquakes and pointed to the Hetch Hetchy water delivery's vulnerability to disruption from an earthquake. She said the local fire district had very little water stored and she thought water storage was much more important than a juice bar or a playing field.

- Rob Willington, Menlo Park resident, said he and his family supported the project. He said the SRI campus land was currently underutilized and it was a great idea to redevelop it into a new neighborhood with open space and new housing.
- Steve Pang, Burgess Classics, said he was opposed to the open space concept of the proposal as it would lead to unhoused people using for encampments. He said over the past three years they had tried to work with SRI to handle the unhoused problem with their back gate and nothing had been done. He said their children used to be able to bike and walk around the neighborhood but it did not feel safe anymore. He said he opposed the bicycle path from Middlefield to Laurel for substantially the same reason. He said he opposed the number and location of housing units proposed as there was potential for a lot of traffic on Laurel Street. He said he had submitted additional comments in a written comment letter.
- Frank Contreras, Menlo Park resident, said he and his family had lived in the area for 40 years and he supported the project proposal. He said he would like his family to be able to stay in the area and affordable housing was needed. He said he agreed about the homelessness and encampments that those needed to be addressed. He said he agreed with housing being provided to special needs population as he thought everybody should have the opportunity to live in Menlo Park as it was such a great area.
- Will Connors, Willows resident, said he strongly supported the project particularly the bicycle and pedestrian access to schools and the downtown. He said his only critique was about the townhomes on Laurel Street as he would like to see more density in that area similar to the other residential units proposed at three to five stories as that was a better use of space near transit.
- Susan Stimson, Linfield Oaks, said she had attended some of the community input sessions and was pleased to see that some of what was recommended by residents had been incorporated. She said she would appreciate consideration of a closed wall for the parking structure to preserve privacy and block headlights at night as well as noise. She said she would like information on how security would be maintained throughout the green space so that the space might be utilized at night. She said that other large mixed-use projects in this area and their impacts on traffic and resources should be determined before adding another large development.
- Kenneth Mah, Burgess Classics, said they generally supported the proposal particularly the bicycle and pedestrian paths. He said they asked that the impact of the development and specifically the housing density be thoroughly considered. He said he and his wife used to bike to Stanford for five years and there was a safety issue at Laurel Street and Ravenswood Avenue as there was no dedicated bike lane. He said this project would worsen that safety issue. He said traffic in general would be increased on Laurel by the project. He said the current proposed designs might decrease the safety of both residents in his neighborhood and the Parkline residents trying to cross Laurel Street to get to Burgess Park. He asked the Commission to mitigate impact to Laurel Street by considering ingress and egress exclusively onto Ravenswood Avenue and Middlefield Road. He said they supported other issues needing attention including

gate access on Burgess Drive, ensuring the intended use of the green space and insuring provision of safety and security of that space.

- Katie Behroozi, Menlo Park, said she served on the Complete Streets Commission, but was speaking for herself. She said this project addressed three big needs. She said one was a direct response to increased density as they needed better connectivity for bikes and pedestrians between Middlefield Road and Laurel Street. She said the project also offered open space noting recent conversations in the city on how to save parks. She said with the housing crisis there were homeless people. She said for several years there had been discussion to have a shelter in the area which was not supported. She said in general this was a great area for denser housing. She supported keeping the proposed openness and ensuring safe crossings at Middlefield Road noting the Vintage Oaks intersection. She said she was a member of the Trinity Church and they had a shared parking agreement with SRI but were also joyfully anticipating the idea of new potential parishioners and members of the community.
- Peter (no last name given) said he met with Mark Murray and Lane Partners and they had listened to the community's opinions. He said he lived in the Classics and loved the quiet nature and the streets. He said his one concern was traffic as although the plan was to replace existing square footage one to one those were primarily currently unoccupied buildings with lower employee density. He said he understood the vision for open space but that had consequences. He said the connectivity to the ingress and egress made sense but did not really address safety issues of the ingress and egress along Burgess. He said there were dedicated bicycle lanes already along Linfield Drive and Ravenswood Avenue so they disagreed with having ingress and egress along Burgess. He said he wanted to make sure that they did not provide programming activities directly behind his and his neighbors' back yards between his community and the parking structure as that would encourage homeless encampments. He referred to comments on safety and unhoused people in the vicinity.
- Gail Gorton, Burgess Classics, said in general she supported the proposed project. She asked that the Commission be sensitive to a huge residential development dropped into a mixed residential area ranging from single family homes to apartment buildings, the tallest of which were only two-story. She said traffic impacts would be huge. She said Laurel Street, Ravenswood Avenue and Middlefield Road were two-lane roads already heavily congested. She said traffic was heavy on Laurel Street with Burgess Park there and she had seen near misses with bicycles from cars exiting the parking lots. She said they had to consider how the schools would absorb additional population and the impacts to natural resources. She said she appreciated the inclusion of a playing field as the fields at Burgess were at maximum usage. She asked how the Parkline playing field would be operated. She said she would prefer to see affordable homes for purchase on the site. She said it was important to provide affordable rental housing too. She said she would like the number of affordable units to remain the same as proposed but for the overall number housing units to be reduced.

Chair Doran closed public comment

Commission Comment: Chair Doran noted the time was 10:24 p.m. and that they would need to stop at 11 p.m. unless they voted to extend beyond that time.

Chair Doran said the first topic staff requested input on was land use. He said overall he thought the project was great and very thoughtful, and the land use was appropriate. He said he liked the

residential uses closest to the train station, the playing field close to Menlo Atherton (MA) because there was not a lot of parkland around MA. He said the application included a request for a zoning ordinance amendment and rezoning so everything was on the table. He said it was a very large site and a great opportunity close to transit. He said he would encourage more housing and was amenable to higher density for housing. He said the proposed site layout seemed respectful to neighbors and he liked the townhouses as a bridge to existing residential neighbors. He said he liked the three stories nearer the front edge of the property and the five stories further behind. He said he would support higher densities especially if they were behind the five stories so height was gradual. He said also he would support more land being used for residential than for office. He said he appreciated the preservation of the heritage trees. He said access seemed well thought out. He said he heard the objections to residential access on Laurel Street but they needed residential development and the applicants had done a good job of keeping at least the commercial access off Laurel Street. He said regarding conceptual architectural styles that he believed it was very appropriate noting it was in early stages but he thought Mission style seemed appropriate. He said the design layout of the open space looked good. He said regarding parking locations and ratios that it was better than what was there now. He said regarding proposed sustainability measures it was still early in the design but he appreciated the LEED gold goal. He said he was generally supportive and would like to see more housing.

Commissioner Kennedy said generally she was supportive of the proposed project. She said she agreed with Chair Doran's comments on increased density and that significantly increasing density would be appropriate for this project. She said they had seen a number of letters contemplating what it would look like to take an acre and partner with an affordable housing developer to provide meaningful affordable housing. She said that might help them to embrace what was starting to happen across both Santa Clara and San Mateo Counties to move toward the attempted zero of homelessness. She said this site was their hope for putting the right amount of housing at the right densities downtown where it belonged.

Commissioner Andrew Barnes asked if the project site was outside of the Downtown / El Camino Real Specific Plan area (Specific Plan) and if so, what community amenities program applied to it. Planner Sandmeier said the site was outside of the Specific Plan and there was no specific community amenities program. She said the applicant was requesting a new general plan designation and new zoning ordinance amendment that the property would be rezoned to. She said as part of those there could be an exchange for some type of community amenity that was negotiated.

Commissioner Barnes said he liked the idea of a sports field but that was not a community amenity in the formal sense. He asked if they were considering have Parks and Recreation program the use of the field. Mr. Murray said they were open to how the field would be programmed. He said in a sense it was a community amenity as that sports field with an adjacent park area and a community building was really a community use rather than an amenity base for their office occupants or residents. He said it was meant for AYSO or other recreational leagues. He said hopefully it could allow for office occupants use as well but they intended it to be truly a community sports field. He said that it was early on and they were open to ideas on management of it.

Commissioner Riggs asked what the approximate occupied density of SRI was currently. Planner Sandmeier said she did not have that information. Mr. Murray said they did not either as occupancy had been significantly disrupted by the pandemic. He said SRI's intent was to consolidate into those three existing buildings totaling about 280,000 square feet but he thought currently employees were

spread out in much more space. Commissioner Riggs said they would have to look at something historic then like a 2019 Google map or something like that. He said his question related to traffic and noted the Meta campus with three office clusters of roughly 500,000 square feet each and the amount of traffic going in and out of those clusters. He said over the 20 years he had regularly traveled down Ringwood and Ravenswood he had never seen even a fraction of that traffic in the SRI parking lots. He asked if that was accurate. Mr. Murray said the last Conditional Use Permit, approved around 2004, showed a headcount cap of about 3,200 people but that had declined significantly due to Covid.

Commissioner Riggs said the public they heard from were supportive of the project because of housing, BMR units and opportunities to create more affordable housing and for special needs populations. He said however the project would have approximately 1.4 million square feet of office space and 400 residential units. He said by comparison Willow Village had over 1700 proposed housing units for roughly the same amount of office use, and that project was providing significantly less housing than the additional workers generated by it. He said the proposed Parkline project was not a housing development project. He said that did not mean he was opposed to it unless it was a housing project – he just thought it should be clear what the project was. He said one letter from the public asked how many workers were expected and how that related to housing / jobs imbalance and traffic. He said five story office buildings here would indicate a higher density. He said there were three parking structures proposed so he expected there was some concept of what kind of density was expected. He said information on that would be expected at the next session.

Commissioner Riggs said regarding the proposed land use, intensity and density, that the most notable thing was this was not a jobs and housing imbalance correcting project. He said the question would be how much it would contribute to the imbalance. He said that this might not be the project that needed to address the imbalance, just that it was something to be noted. He said since the project was predominantly an office space project, he thought it made sense to put the office space as close to the train station as possible. He suggested that office space users might take advantage of transportation much better and more immediately than residents. He said that he did not really have any comments on the site access, design, layout of open space, parking locations or ratios as theoretically those would be rethought to place office closer to transit. He said regarding conceptual architectural styles that they were taking the correct approach, and when that style was done well, it was really exciting.

Commissioner Harris said this was a unique opportunity for the City to transform an aging property with limited use to an open and mixed-use neighborhood. She said with so much community interest there were of course different ideas about what was wanted. She complimented the applicants on the 25 acres of publicly available green space, the retention of heritage trees and locating buildings around them, only the residential entrance on Laurel Street, listening to the community, and the pedestrian / bicycle paths and connectivity. She agreed they could not go wrong with the attractive Mission style architecture and was supportive that the five stories were set back from the three stories, and the 50-foot setback between the site buildings and Burgess Classics. She said her areas of concern included traffic impacts and mitigation. She said regarding a Transportation Demand Management plan (TDM) they had indicated a shuttle to Caltrain and suggested that might be extended to go downtown, maybe circle around to Safeway and then back again. She said she would like the TDM to go even further than that. She said they had had success on other projects with trip caps so she would like to see that. She said she would like Menlo Park to eliminate minimum parking requirements entirely toward significantly reducing the number of people driving and parking on this site as it was close to Caltrain and El Camino Real buses, and close to

downtown amenities. She said the proposed three large parking structures took up too much land that could be used for housing. She asked if they had considered putting the parking underground, which would allow room for additional residences and reduce parking.

Mr. Murray said they considered it and a big drawback was the digging as that increased construction timing by nearly two times, and involved environmental impact and construction noise impact with trucks hauling dirt away. He said while the end result made the parking sort of disappear, it obviously was very costly. He said with this site and being able to provide 25 acres of open space they did not think it was necessary to do underground parking.

Commissioner Harris said the difference between the number of office workers for R&D versus regular office use was a pretty big delta, which might mean a greater parking need. She said when the project came back, she would like information on employee count, to see the parking reduced or ideas of how they might do that. She said to let the Commission know if the city would need to help them with parking reduction. She said they might consider charging for parking both the residents with unbundled parking and also the office workers or give rebates to those who did not drive to work. She said her second suggestion was to increase the number of housing units noting if parking was reduced that they would have more space. She referred to the idea of dedicating an acre to a nonprofit housing group to get more density and housing for people of all abilities and deeply affordable housing, and noted that deeply affordable housing residents were less likely to need cars and that would help the parking. She said as they got closer to a project submittal that she would like to review the recreation site to understand what made the most sense, whether it was really for the community, whether it was truly a recreational field and if so what type.

Chair Doran noted it was 10:59 p.m. and two Commissioners were requesting to speak. He proposed taking a vote on extending the meeting time in a finite amount, and suggested 20 minutes acknowledging that some Commissioners had severe time constraints.

ACTION: M/S (Harris/Doran) to extend the meeting to 11:20 p.m.; passes 7-0.

Commissioner Tate said her biggest concern was the project would not provide enough housing. She said she liked the idea of donating not just one but a couple of acres to a nonprofit or low-income housing developer for affordable housing development. She said additionally she was concerned about the field near the existing church, as she thought the church needed quiet for their activities. She suggested the project team as a good neighbor might consider moving the field or to come to a compromise with the church. She said her assumption was there would be some sort of security to ensure the grounds were safe, but she had not heard that addressed in response to community comments.

Mr. Murray said the 25 acres would be privately owned. He said it was something they were trying to create as an amenity and not to burden the neighbors or the city. He said he envisioned that they would privately develop and maintain the space and there would be some kind of public access license or easement to use it as a park during certain hours. He said they were open to ideas. He said in terms of safety late at night and early morning, as this was private property, they would be responsible for securing it. He said they would have every incentive to secure it as the property owner for the benefit of the residents who lived there. He said that was something they were very confident they could manage.

Commissioner Tate asked if they had given consideration to donating some of the land. Mr. Murray said they were speaking with different groups and others about how to generate more affordable housing. He said the idea had been discussed and they were open to it.

Commissioner Barnes said a couple of areas could use more thought. He said as he conceptualized the 25 acres of green space, he saw that was good for the site and for instance the office users and residents. He said the common area in the middle was underutilizing the site. He noted the dearth of playing fields in the area and suggested two fields on the site that were neither a park or a tenant feature amenity. He said he had no use for in lieu fees but a use for an accretive, material and tangible community benefit. He said he supported parceling out some of the property, an acre or so, for a deeply affordable housing project. He referred to traffic impacts from the project notably to the Willow Road, Middlefield Road and Woodland intersection. He said moving forward he would want discussion on what impacts the project would have transportation and transit infrastructure.

Commissioner Tate said for the record that her request was for one or two acres donated to a low-income housing group but that it was not in lieu of the BMR units the project was providing. She said that integrated housing was better than when it was just in one building but she understood the need for the latter, and they had the property size to make it happen.

Commissioner DeCardy said he appreciated community interest in the project. He said what the applicants were trying to do and the direction they were going could work very well and there were challenging things to sort out. He said two things were not working and those needed to work in a fundamentally different way. He said one was affordable housing. He said with 400 units that 15% BMR would be about 60 units of affordable housing. He said that was one unit of affordable housing per acre on this property. He said the simplest thing would be to set a goal for affordable housing and then they could sort out what that required but the goal needed to be significantly higher than 60 units. He said the second was the congestion that would come with attracting so many people to this area and what to do about that. He said a parking garage would not get them out of the congestion problem. He said the project team proposed shuttles. He said he had the opportunity to have a walkthrough with Mr. Murray and that was helpful. He said an electric shuttle that went from the site down to Caltrain was a beginning point. He said working with City Council they could open this up and as Commissioner Harris had commented, take the opportunity to look across the community and finally get connectivity from Bayfront to the downtown that would get people out of cars, work for this development and act as a catalyst to make that work for the rest of the community. He said the city had major developments from the Bayfront, along Willow Road and downtown not to mention what might come out of the Life Sciences District and the USGS site. He said now was the time as a community to address connecting all that with something other than single occupancy vehicles. He said it was not this project's responsibility to own this but it was their responsibility to catalyze it to help make their project work.

Commissioner Riggs said he supported Commissioner DeCardy's call for action for transit from Bayfront, past SRI and to the Caltrain station and that would require the City Council to do something more locally. He said he was surprised the challenging Ravenswood and Ringwood intersection had not been mentioned as here was an opportunity to bring Ravenwood around the church property and align with Ringwood. He said the current intersection was dangerous for the many pedestrians coming from the high school, particularly dangerous for bicyclists going southbound on Middlefield Road and crossing that loop connector. He said it was an annoyance to everyone who had to navigate those double traffic lights and it was time to fix it.

I. Adjournment

Chair Doran adjourned the meeting at 11:20 p.m.

Staff Liaison: Corinna Sandmeier, Acting Principal Planner

Recording Secretary: Brenda Bennett

Approved by the Planning Commission on August 29, 2022

From: [David Fencil](#)
To: [Sandmeier, Corinna D](#)
Subject: Parkline
Date: Tuesday, December 6, 2022 2:24:23 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

I was looking at the map of the proposed Parkline development...the map was very small but there was green at the corner of Ravenswood and Middlefield...between the church and Middlefield...if that is a park, my experience with the police department would predict a big problem with kids hanging around even during school days and other kids hanging out waiting for the HS kids...
Dominick (650) 269-6279

Sent from [Mail](#) for Windows

Additional Comments Received after Staff Report Publication

Menlo Park City Council, Staff Report #22-091-CC Item G1.

SRI comments on Staff Report. Tuesday 5/10/2022

We applaud SRI and their community involvement and sustainability efforts. Also, we are supportive of the SRI housing proposal.

1. HOUSING FOR OUR CHILDREN, LAW ENFORCEMENT, TEACHERS, ETC. **How can we be assured by the City of Menlo Park, SRI and the Developer that our own children, residents will have housing priority?** Middle class is priced out of Menlo Park.
2. TRAFFIC/SAFETY. Vehicular access from the proposed housing along Ravenswood appears to be aligned with Pine Street. The proposed street intersection of Ravenswood/Pine Street is not acceptable for several reasons.
 - A. There is already a traffic problem with traffic exiting from SRI onto Ravenswood. The Pine/Ravenswood intersection is too close to the intersection of Laurel/Ravenswood. There is gridlock now. Imagine how this will be once the project is complete and all other traffic returns to Menlo Park.
 - B. If cars are permitted to exit from the proposed housing and cross across Ravenswood to Pine Street *this* will create a disastrous and deadly situation to the residents and vehicles. Also, Pine Street can only accommodate one lane of traffic with parking on one side. For example, the existing traffic situation is unsafe and does not allow police or fire truck access. **This point must be addressed by the City of Menlo Park now.**
3. BUILDING SETBACK. Proposed setback for the residential location is too close to Ravenswood and Laurel streets. The housing should be set back at least the same distance as the existing SRI building on Ravenswood.
4. HOUSING LOCATION AND BUILDING HEIGHT. The height of the residential buildings was promised not to exceed two stories on Laurel or Ravenswood, not three to six stories as stated during tonight's presentation. The compact housing development is not in keeping with the surrounding neighborhoods. The density of residential building massing does not reflect the surrounding neighborhoods of Pine, Laurel, etc.
5. PARKING. One space per residential unit is inadequate. **What is the City's residential parking requirement?** No SRI parking can go on to neighborhood streets.

All of these comments have been made by me and others previously as well as some other comments. It appears that SRI and their Developer & Architect have not addressed these community comments made during the outreach process. **Will this change going forward?**

Respectfully Submitted, Phillip Bahr, Menlo Park Resident

From: [Brittani Baxter](#)
To: [Planning Commission](#)
Subject: Support of Parkline, item G1
Date: Monday, January 23, 2023 6:48:11 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning Commission,

Thank you for all of your long hours of service to the community during this recent busy season that's included reviews for the Housing Element and many more projects!

Unfortunately I won't be able to attend tonight's meeting, but wanted to voice my support for the Parkline project and share why I'm so excited for it.

A parcel this large turning over represents a once in a multi-generational opportunity to think ahead to what our community could and should be for the future. I think the homes in general represent an opportunity for our community to remain resilient and vibrant by creating homes at a mix of income and affordability levels, and I encourage us collectively (as a community) to think about all the factors impacting commutes and circulation in our city.

I remain very excited for the increased walkability and bikeability that the redesign will bring to the neighborhood. Not just for people like myself who are already here, walking or biking — but also for the new residents. Who, I suspect, will choose to live near downtown for exactly the same reason I did — its easy access to what I need for much of my daily activities without getting in a car.

In viewing and attending past meetings about this project and others, I hear a strong desire from all sides of the discussion to reduce the traffic impact of new homes. I'm writing because I very much share the desire to reduce traffic — our community is safer, healthier, and friendlier without gridlocked streets. I personally believe that a great way to get people out of cars is to just make it appealing (and as a first step, simply possible) to use other methods of getting around. And therefore, I believe this project represents a gem of an opportunity to do just that — by creating homes in an especially great location that's steps away from existing jobs, schools, and transit. Let's make the most of it!

Our housing element cites a stat saying that, I think (going from memory), 96% of our workforce commutes in. I wish we had good location data on where the individual commuters are coming from, but anecdotally the traffic patterns that I see when out and about seem to indicate lots of cross-bay commutes — i.e. drives from pretty far away. I hope and expect that this project will reduce overall traffic by allowing more community members to live near their work.

I wanted to close by sharing a recent finding published by Arlington, VA's government that I found fascinating. **Despite adding to their population in recent decades, they found that car traffic has steadily declined to 1980s levels.** This seems to be due to their emphasis on fostering walkable communities and clustering of homes near Metro stops — otherwise known as transit-oriented development, just like Parkline. Here's the report:

<https://www.arlingtonva.us/files/sharedassets/public/Projects/Documents/Historic-Traffic-Counts.pdf>

This project is a great opportunity to build in this same direction of vibrancy and energy, with a community focused around seeing each other when out for a walk, rather than being stuck behind the wheel of a car.

Thank you again,

Brittani Baxter

District 3 resident

(Apologies for any typos, writing from mobile)

From: [Nick](#)
To: [Planning Commission](#)
Subject: Comments Proposed SRI/Parkline Project expansion
Date: Monday, January 23, 2023 5:57:15 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Menlo Park Planning Commission,

I have recently learned that in this evening's meeting (Jan 23, 2023), a proposed expansion of the SRI/Parkline development project will be discussed.

As a resident of Linfield Oaks, I am concerned that increasing the number of units from 400 to approximately 600 will place an unsustainable burden and impact on the neighborhood and the community.

We all recognize that we are in a housing crisis, but the project does not address the impact on our local services (schools, transportation, traffic). As a parent and a resident, I am worried about the impact that a project of this size will place on the school infrastructures and on their accessibility: access to the schools (Encinal, Hill View) will become much harder because of the increased traffic on Laurel, Ravenswood and presumably Willow Rd.

I was initially pleased by the community outreach by SRI and Parkline and by their willingness to work with the residents and neighbors to include their feedback; this 11th hour change in plans seems however motivated by other reasons, and I would like for the Planning Commission to encourage SRI/Parkline to resume the work on the previous project that was discussed in 2022.

Best Regards,

Nicola Diolaiti

From: [Jonathan Hahn](#)
To: [PlanningDept; CCIN](#)
Cc: [Wolosin, Jen](#)
Subject: [Sent to Planning]SRI/Parkline
Date: Monday, January 23, 2023 6:40:32 PM

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I have just become informed about the situation with the SRI project under consideration. The state mandates that office development results in creation of housing that the city is having trouble meeting. The burden ends up falling on existing residents in many forms. Why doesn't the city manage and limit office development to manage this mandate? I think the residents deserve to know. Other cities do.

Also, when I saw that the SRI project has two driveways on Laurel, it's clear that's going to generate a lot of cut-through traffic through Linfield Oaks rather than direct it to Ravenswood and Middlefield which are intended for this purpose. Cars cutting through neighborhoods do so at unsafe speeds because all they care about is saving time and avoiding traffic. Traffic that's made significantly worse by these projects! It's just one of the many ways existing residents are burdened by these projects and the city should do more to protect the existing residents and neighborhoods.

Jonathan Hahn
340 Sherwood Way
Menlo Park

From: [Stephen Pang](#)
To: [Planning Commission](#)
Cc: [Sue Connelly](#)
Subject: SRI project feedback
Date: Monday, January 23, 2023 5:59:21 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning Commission,

As an owner in The Classics at Burgess, I with other Classics residences that call for a lower-impact, smaller development for the SRI property.

1. The percentage of units designated as BMR should be increased from 10% to 25%, to address the City's primary concerns for the development. If Parkline is truly attempting to address the housing affordability problem itself, it should understand and accept such an increase. In this way, the number of BMR units will increase and the number of total units, currently 400, can be maintained.
2. Any approval of the SRI project should not even be considered until the "dedication" of one acre to a homeless, transitional shelter, or the like is fully planned. Listening to the discussions regarding the Independence Drive project, I was struck by how well thought-out and planned was the "dedication" of units / land to Habitat for Humanity. It seems to be a mature project that seems to have been fully planned at the same time as the Independence Drive project. In contrast, here in the SRI project, there is a nebulous "donation" to an organization that has not been selected, for a development that has not even been imagined. Any approval for the SRI project should be performed with full knowledge and consent of the commission.
3. The driveways for the SRI project should be maintained on Middlefield road. This road already has a stop light and is a major access to the SRI project. One of Parkline's major talking points is the opening of the SRI campus as a park. Integrating traffic for residents through the campus and onto Middlefield will serve to unify this feeling for residents. Contrary to this, as currently designed, residents are actively funneled away from the campus and onto Laurel street. As previously stated by others, this additional traffic onto Laurel street causes serious safety problems at the Laurel Ravenswood intersection. Additionally, cut-through traffic will greatly increase through Linfield Oaks.

Thank you for your continued attention.

Steve Pang

Sent from [Mail](#) for Windows

Additional Comments Received after Staff Report Publication

From: [Marlene Santoyo](#)
To: [Planning Commission](#)
Subject: Agenda G1
Date: Monday, January 23, 2023 4:57:53 PM
Attachments: [M2G Letter - Agenda G1.pdf](#)

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Members of the Menlo Park Planning Commission,

38 members of your community have signed the following letter about the SRI proposal you will be studying tonight. In addition, twelve neighbors have written a personal note, which I encourage you to read. You will find the full letter and notes attached below.

Please consider the input from these residents who support the increased number of homes and increased affordability of the current proposal and ask you to go even further towards planning for housing equity and sustainability in Menlo Park.

Thank you for your consideration,
Marlene Santoyo

--

Marlene Santoyo | Organizer | (she/hers)
Menlo Together
510-945-7490
<https://menlotogether.org>



January 23, 2023

Members of the Menlo Park Planning Commission,

38 members of your community have signed the following letter about the SRI proposal you will be studying tonight. In addition, twelve neighbors have written a personal note, which I encourage you to read. You will find the notes beneath the letter.

Please consider the input from these residents who support the increased number of homes and increased affordability of the current proposal and ask you to go even further towards planning for housing equity and sustainability in Menlo Park.

Regards,

Marlene Santoyo and The Menlo Together Team

Members of the Menlo Park Planning Commission,

We, and the residents listed below, believe that our city can and must build more homes across all levels of affordability, especially near transit and downtown services, for a variety of household sizes and for people of all abilities.

We are glad to see that the Parkline proposal has increased the number of homes to 550, including a much needed and appreciated 100 deeply affordable homes for people of all abilities. We are pleased that the EIR will study up to 600 homes, and hope that the plans will grow to include that number of homes. Thank you for these important changes to the proposal.

We encourage the city and the developer to do even more.

A sufficient and diverse housing supply is required for a sustainable, welcoming and thriving community. Additionally, state law requires that we meet our fair share of and affirmatively further fair housing by planning for affordable homes in high resource areas. The State will make sure that we achieve our goals - willingly and through our own planning, or unwillingly through by-right development.

To that end, we:

- Celebrate the plan to dedicate an acre of land within the development to be donated to a non-profit housing developer and developed to meet our most pressing needs: deeply affordable housing for families and people of all abilities.

- Support increasing the number of homes beyond 550, and increasing the inclusionary Below Market Rate (BMR) units from 15% to 20%. We encourage reimagining the proposal to produce 100s more homes on this once-in-many-generations opportunity site that is walking distance from downtown services, transit, recreation and schools.
- Support reducing the amount of parking to attract non-driving residents and reduce local traffic, and to leave more space for community-enhancing amenities.

No matter where you begin, success in life starts at home for all ages and all people. When we have safe, secure places to live, parents earn more, kids learn better, health and well being improve, and our community is strengthened because it now has the building blocks needed to thrive.

Let's take full advantage of the Parkline project to build a strong community of people and families of all incomes and abilities who thrive.

Thank you.

Sincerely,

1. Anna Zara (Menlo Park)

I would also like to add that lately most of the new large housing developments in Menlo Park have been clustered in the Belle Haven and Linfield Oaks neighborhoods. It is time to look at adding housing to other Menlo Park neighborhoods as well and to even out the new housing units between the Menlo Park City School District and the Las Lomas School District.

Thank you for your coordination.

Anna

2. Michal Bortnik (Menlo Park)

3. Bridgit Louie (Menlo Park)

4. Beanie Zollweg (Menlo Park)

5. Caroline Beckman (Palo Alto)

6. Caroline Kory (Menlo Park)

7. Connor Gilbert (Menlo Park)

8. Dayna Schocke (Menlo Park)

9. Dennis Irwin (Menlo Park)

I want the benefits of living in a more diverse community. The more affordable housing there is in the Parkline project, the more we'll be going in that direction!

10. Hannah Gilbert (Menlo Park)

11. Julian Cortella (Menlo Park)

More housing near downtown is great! Please support the Parkline proposal with the increased number of homes.

12. Jessica Clark (Menlo Park)

13. Jennifer Johnson (Menlo Park)

14. Joseph Grass (Menlo Park)

15. JP Garcia (Menlo Park)

16. Julie Shanson (Menlo Park)

More housing at all income levels near transit and schools helps the whole town.

17. Karen Grove (Menlo Park)

I got interested in local housing issues as a way to "act locally" to achieve racial justice. I know others are interested in housing as a way to minimize our climate impact by reducing local traffic and emissions from people commuting to work in Menlo Park or nearby, because they cannot afford to live here (or near).

I support the increase in number of units, and the dedication of land to a partner who will develop 100 units of homes for those most impacted by housing insecurity.

But this proposal could be SO much more and go a lot further towards achieving fair housing and climate action in our city.

We should be looking at Willow Village - a 59 acre site (as compared to this 64 acre site) as a model. Willow Village is going to produce over 1700 homes including extremely low income affordable senior homes through a partnership similar to the one being contemplated for the SRI site.

What's good near Belle Haven would be even better at the SRI site, which is an easy walk from Caltrain, El Camino busses, downtown, parks, schools and restaurants.

This is a once in more than a generation opportunity to share a vibrant, equitable and sustainable future for Menlo Park.

18. Katie Behroozi (Menlo Park)

I'm enthusiastic about the redevelopment of this centrally located under-utilized land – but I'd like to see less parking, less office space, more housing at all income levels, well-integrated bike-ped

facilities and open space, and public access to all on-site amenities so that adjacent neighbors can use not only the open spaces but also whatever cafes and fitness facilities are developed (I don't think cities benefit from the Google/Meta in-house private amenities that have become the norm.)

19. Katherine Dumont (Menlo Park)

I live just one-half mile from the Parkline site, so I'm very interested in this project. In several meetings with the developer, I've been very impressed by their willingness to build housing for a range of needs and abilities. We should jump at this chance to provide more diverse and affordable housing in this location, which is so close to transit, Burgess Park, the community center, and to downtown shops and services.

This is a great opportunity to reverse the trend of pushing people to live further and further away from their jobs. It's hard on individuals and families, and it's hard on the environment. It's going to cost us all a lot more in the future if we don't take bold steps now.

Thank you for considering more housing on the Parkline site so we can move forward in a more sustainable and equitable way.

20. Lesley Feldman (Menlo Park)

21. Lorri Holzberg (Menlo Park)

22. Mary Kelly (Menlo Park)

I believe in increasing density and affordability!

We all benefit from the diversity!

23. Michael Arruza (Menlo Park)

24. Marijane Leonard (Menlo Park)

25. Margarita Mendez (Menlo Park)

26. Marlene Santoyo (Newark)

27. Nathan Rolander (Menlo Park)

I support this petition to build new homes

28. Nina Wouk (Menlo Park)

29. Jennifer Michel (Menlo Park)

Dear Chair, Vice Chair, Commissioners, Staff, Neighbors,

Thank you for listening to us and granting us much needed vibrancy in our City! I support the Parkline project and applaud the applicant for increasing housing.

Further I support:

Housing at all income levels keeps our community resilient, inclusive, and thriving. Do you have children? Where are they going to live in a few years? Have you been housing unstable? Do you have issues obtaining and retaining labor?

There is a cool recent batch of data from Arlington VA who saw a net decrease in traffic despite adding more units to the city, because of how the units are smartly clustered around transit. We will not meet our Climate Action Plan goals without reducing the number of miles people commute to work in or near Menlo Park, simply because they cannot afford to live here. New York Times also came out with a map of your carbon use mapped by neighborhoods showing that those of us working near where we live, who live within their means, generate climate stability. It's no longer cool or something to boast if you are an empty nester in an SFR on a 10,000 SF lot. Parkline gives us much needed dignity to get out of the car and use much less carbon.

I support local businesses and want them to have a robust, local workforce who are able to thrive and contribute to the community in which they work. Parkline would give a much needed infusion of new mouths to feed and serve. Our local businesses will see a much needed economic lift. Because Parkline is walking distance to downtown and major transportation infrastructure, the residents will also thrive! The current neighbors will feel welcome to walk and get out of their vehicles! What a win win win!!

I value equity and welcome people who have been discriminated against into all neighborhoods, parks and our schools.

Dedicating land in this prime location to a non-profit affordable housing developer is a great way to meet hard-to-meet housing needs: seniors, large families, single-women headed households, people with developmental and physical disabilities. I've mentioned before that I can't get labor to service my buildings because of the overly burdensome commute, but this project would help bridge that gap!

This site will be a strong applicant for federal, state, and county funds because of its proximity to transit and services.

The developer has shown that they are willing and open to building more housing for people of all incomes and abilities. We should take advantage of this opportunity and work with them.

Additionally, we are sending a message to all parties and stakeholders that our residents, workforce, families, and retirees all are incredibly valued and we stand with them, us, to meet the moment with our various housing needs. I'm proud to call Menlo my home and the City where we raise our son.

With all my love,

Jenny Michel from the Coleman Place Neighborhood Block

30. Frances Kieschnick (Menlo Park)

31. Sandy Sloan (Menlo Park)

We need more affordable homes west of Middlefield.

Thank you!

32. Sara Matlin (Redwood City)

33. Sarah Zollweg (Menlo Park)

34. Sharika Thiranagama (Menlo Park)

35. Sarah Brophy (Menlo Park)

This is the type of project that Menlo Park City council should encourage.

36. Tim Clark (Portola Valley)

37. Tom Kabat (Menlo Park)

38. Vikas Maturi (San Mateo)

From: [M. ADHAM](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review
Date: Monday, January 23, 2023 1:39:45 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning commission members:

We have been residents of Linfield Oaks for 30 years, and raised our family here. Please do not approve the proposed changes in the density and size of the SRI/Parkline development as it is unfair for our neighborhood to disproportionately bear the impact of the initial 400 units, not to mention increasing it to 600 units. It's also not fair as we have taken on the additional housing of the Morgan Lane Development that was completed in 2008.

Taking the already extremely large total housing number from 400 units of the SRI Development to 600 jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and increased traffic congestion in this area. Further:

- The apartment complex and townhome driveway should be removed from residential streets.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Residential streets leading to the new development are not used The office traffic can be significantly reduced if Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.
- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage.
- Include the emergency water storage tank , because 1) there

is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development.

Omar and Mary Adham
157 Linfield Dr
Menlo Park, CA. 94025

Sent from my iPhone

From: [larry anderson](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]
Date: Monday, January 23, 2023 1:08:49 AM

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Planning commission members:

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area.

Larry Anderson
321 Linfield Place

From: [Anna Hall](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan
Date: Monday, January 23, 2023 3:12:24 PM

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Planning Commission Members

Adding 400 housing unit in Menlo Park was intended by the State for more housing for people who need to live and work in Menlo Park. On the other hand, adding 200 additional units is questionable, especially if many of those units are earmarked for Office Space. Most people living near SRI know that 400 new units will seriously impact traffic, parking, infrastructure, and quality of life. It will have deleterious effects on students, teachers, and staff who work at Menlo-Atherton high school. Thus, plans to build numerous units so close to M-A should include input by school administration.

Most important, the Planning Commission must not ignore or minimize the impact that tens of thousands of recent job cuts in the Computer Sector in this area will create less need, if any, for more Office Space. Looking around Downtown Palo Alto, or El Camino Blvd., one sees countless signs for empty Office Space.

A responsible Planning Commission will need to go back to the drawing board and re-evaluate the SRI/Parkline Plan before proceeding any further. Failure to do so would indicate that members of the Planning Commission are not beholden to the residents of Menlo Park, but to Real Estate Developers.

Anna Hall
212 Gilbert Avenue
Menlo Park, CA 94025

From: [Judith Asher](#)
To: [Planning Commission](#)
Subject: SRI/Parkline Plan Review - requested changes
Date: Monday, January 23, 2023 7:56:03 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my next door neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

- The **project should net out to provide the state-mandated housing number of 400**, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on the Ravenswood driveways if

Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Quoting from my next door neighbor, Sue Connelly:

" SRI/ParkLine will have highly profitable housing and office revenue annually, but the costs will be borne by the taxpayers.

Based on current Menlo Park office rates, the office project stands to generate \$50M per year. This doesn't include ANY of the apartment rentals, for which most will be at very high rents (see the current rents for the new SpringLine apartments!). There will be some city revenue, but since SRI is a non-profit, this massive development will not offset many of the costs residents must pay for infrastructure (schools, police, fire, water and roads). Yet it will create a significant reduction in our quality of life (and possibly home values), bike/pedestrian safety for school children and residents, and increasing the state-mandated affordable housing units even more.

We need to require that any new office development provides/includes the affordable housing that the office spaces

and employee densities will be required to be built in Menlo Park."

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development .

Judith Saltzman Asher
530 Barron Street
Menlo Park, CA 94025

From: [Christopher Baldwin](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]Planning commission meeting Jan 23, 2023 for the SRI/ParkLane Plan Study Session
Date: Monday, January 23, 2023 3:14:09 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear commission,

As a resident of Menlo Park, I am providing my comments regarding the **SRI/ParkLine Plan Study Session** which is being held tonight to be captured in the public record.

1. **The SRI/ParkLine project should net out to provide the state-mandated housing.**
2. **Reduce the amount of office to comply with the current C1 zoning.**
3. **Remove the apartment complex driveway on Laurel Street to protect bike safety for school children and pedestrians.**
4. **Use the (currently gated) SRI driveway onto Middlefield.**
5. **Increase parking for renters and employees.**
6. **Provide underground parking for the housing units and for the offices.**
7. **Include the emergency water storage tank.**

Thank you.

Christopher Baldwin
345 Claremont Way, Menlo Park, Ca 94025

From: [Susan Bryan](#)
To: [Planning Commission](#)
Subject: Parkline Study session Jan 23, 2023
Date: Monday, January 23, 2023 10:58:10 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning Commissioners: I am writing to remind you that members of Trinity Church, Menlo Park are neighbors of the new Parkline Development. Last year, we submitted the signature of some 30 church members asking for the maximum amount of affordable market rate housing to be included in the developer's plans. That means we would be in favor of the extra 50 units being proposed at the study session tonight.

Thank you - Susan Bryan, church member, Trinity Church, 330 Ravenswood Avenue, Menlo Park

From: [Daryl Camarillo](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/ParkLine project request
Date: Monday, January 23, 2023 7:48:54 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Corrina and Planning Commission,

As a resident of The Classics at Burgess, we are requesting a third level in the EIR scope to review a lower-impact, smaller development option -- especially since the proposed plan **INCREASES the affordable housing deficit.**

In this smaller-scope project, we request the EIR to measure the following:

1. The SRI/ParkLine project should net out to provide the state-mandated housing that the amount of office planned will require Menlo Park to build.
 - o Reduce the amount of office to comply with the current C1 zoning. The planned office use will actually **NEGATIVELY** impact the affordable housing deficit and result in increasing the deficit due to the proposed office use. The risk of the projected lab use FAR being changed to higher employee densities per 1000 square feet will further increase the affordable housing deficit. In short, the office size and density is creating a bigger housing problem.
 - o Keep the housing at 400 apartments, but have 25% of them be BMR (Below Market Rate) units, so the separate one-acre donation being considered for an affordable housing development will not be required.
2. Study the option of removing the apartment complex driveway onto Laurel to preserve bike safety for school children and pedestrians and to reduce the existing gridlock on Laurel Street. The smaller driveway for the townhome residents can remain as indicated in the current plan.
3. Measure the use of the (currently gated) SRI driveway onto Middlefield to redirect traffic flow as a viable alternative to the removal of the Laurel Street for the apartment buildings. The office traffic can be significantly reduced on the Ravenswood driveways if the Middlefield driveway opens (it will reduce Ravenswood gridlock to/from Middlefield and El Camino) and direct commuter traffic closer to Willow and Highway 101.
4. Increase parking for renters and employees since inadequate parking forces apartment renters, visitors and employees to clog residential streets with traffic while looking for parking and for taking up limited residential parking
(Note: In the 12/12 Planning Commission meeting on the SRI EIR, some commissioners wanted to reduce the proposed parking to force renters/employees to use public transit. But the representative from the firm that will conduct the EIR said that studies showed that reducing parking spaces did NOT reduce cars or numbers of car trips. It just pushed drivers to surrounding residential areas to take street parking, which added traffic as well. There were no reductions in Greenhouse Emissions or in number of car trips.)
5. Provide underground parking for the housing units and for the offices to reduce the overall height of the project (notably to reduce the height of the 3-story parking garage behind the Barron Street homes) and the potential of five six-story apartment

buildings if the project is approved for the 600 total housing unit option being reviewed.

6. Include the emergency water storage tank since there is no emergency water for residents and workers west of El Camino (per the latest water report) which said the emergency well in the city yard is not online yet. The risk of toxic contamination of the city yard emergency well makes it a problem since the city's gas tanks and city yard with other toxic substances (oil, pesticides, etc.) are above it could leak into the groundwater, especially in the expected large earthquake event at some point in the future.

Thank you for your help in getting this lower-impact option included in the EIR so we have a solid comparative analysis of the other two scenarios, especially the much larger scope option, that are being proposed in the EIR scope.

Daryl Camarillo/ Yolanda Font
525 Barron Street
Menlo Park, CA 94025
650-269-1493

From: [Angel Chen](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/ParkLine Building Project - Impact on Classics of Burgess Neighborhood
Date: Monday, January 23, 2023 1:01:26 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Attention:
Corrina Sandmeier -- Acting Principal Planner
and the Menlo Park Planning Commission

Dear Corrina and Planning Commission,

As a resident of The Classics at Burgess, we are requesting a **lower-impact, smaller development** -- especially since the proposed plan actually **INCREASES the affordable housing deficit**.

In this smaller-scale project, we request the following:

1. **The SRI/ParkLine project should net out to provide the state-mandated housing** that the amount of office planned will require Menlo Park to build.
 - o **Reduce the amount of office to comply with the current C1 zoning.**The planned office use will actually **NEGATIVELY** impact the affordable housing deficit and result in increasing the deficit due to the proposed office use. The risk of the projected lab use FAR being changed to higher employee densities per 1000 square feet will further increase the affordable housing deficit. In short, the office size and density is creating a bigger housing problem.
 - o **Keep the housing at 400 apartments**, but have 25% of them be BMR (Below Market Rate) units, so the separate one-acre donation being considered for an affordable housing development will not be required.
2. **Remove the apartment complex driveway onto Laurel** to preserve bike safety for school children and pedestrians and to reduce the existing gridlock on Laurel Street. The smaller driveway for the townhome residents can remain as indicated in the current plan.
3. Instead of the Laurel Street driveway, **use the (currently gated) SRI driveway onto Middlefield to redirect traffic flow** as a viable alternative to the removal of the Laurel Street for the apartment buildings. The office traffic can be significantly reduced on the Ravenswood driveways if the Middlefield driveway opens (it will reduce Ravenswood gridlock to/from Middlefield and El Camino) and direct commuter traffic closer to Willow and Highway 101.
4. **Increase parking for renters and employees** since inadequate parking forces apartment renters, visitors and employees to clog residential streets with traffic while looking for parking and for taking up limited residential parking.
(Note: In the 12/12 Planning Commission meeting on the SRI EIR, some commissioners wanted to reduce the proposed parking to force renters/employees to use public transit. But the representative from the firm that will conduct the EIR said that studies showed that reducing parking spaces did NOT reduce cars or numbers of car trips. It just pushed drivers to surrounding residential areas to take street parking, which added traffic as well. There were no

reductions in Greenhouse Emissions or in number of car trips.)

5. **Provide underground parking for the housing units and for the offices** to reduce the overall height of the project (notably to reduce the height of the 3-story parking garage behind the Barron Street homes) and the potential of five six-story apartment buildings if the project is approved for the 600 total housing unit option being considered.

6. **Include the emergency water storage tank** since there is no emergency water for residents and workers west of El Camino (per the latest water report) which stated that the emergency well in the City Yard is not online yet. The risk of toxic contamination of the City Yard emergency well makes it a problem since the city's gas tanks and city yard with other toxic substances (oil, pesticides, etc.) are above it and risk leaking into the groundwater, especially in the expected large earthquake event at some point in the future.

Thank you for your help in seriously considering this lower-impact development solution.

Best,
Angel Chen

From: [Sue Connelly](#)
To: [Planning Commission](#); [PlanningDept](#); [Sandmeier, Corinna D](#)
Subject: [Sent to Planning]Request to reduce the office and housing for SRI/ParkLine
Date: Monday, January 23, 2023 4:45:58 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Ms. Sandmeier and Planning Commissioners,

I'm a Board Member and resident of The Classics at Burgess HOA. I would like to reiterate the requests I submitted for the EIR scoping deadline on January 9th regarding concerns about the massive size of the SRI/ParkLine development.

We are requesting a smaller development that reduces the negative impact of a development of this large scale -- especially since the plan **INCREASES the affordable housing deficit with the quantity of office space and density proposed.**

In this smaller-scale project, the following is requested:

1. **The SRI/ParkLine project should net out** to provide the affordable housing that the amount of offices and workers that the State mandates Menlo Park to build to accommodate the number of new workers.
 - o **Reduce the amount of office space** to comply with the current C1 zoning since the planned office use will actually **NEGATIVELY impact the affordable housing shortage** and result in increasing the number of affordable housing units that will need to be met by yet another development project. The risk of the projected lab use FAR being changed to higher employee densities per 1000 square feet will further increase the affordable housing deficit. Currently, it appears SRI has 1,000 employees on the Menlo Park campus. Even at the lab and biotech use of 4 employees per 1,000 square feet raises the number of workers on the site to 4,000. In short, the office size and density is creating a bigger housing problem. If the office FAR changes to even denser use for start ups and high tech companies, the density of workers per 1,000 square feet will go up significantly, and drive the deficit even deeper.
 - o **Keep the housing at 400 units**, but have 25% of them be BMR (Below Market Rate) units, so the separate one-acre donation considered for an affordable housing development will not be required and the community open space for a soccer field or other public use will be preserved. Also, with a reduction in office space, the housing can be reduced in height and density and spread out more on the SRI campus. With the possibility of five 6-story apartment buildings, in addition to the five 3-story buildings, this height will be 300% higher than any of the surrounding apartments and homes. Also, the apartment complex does not currently have a play area or community area, or pool. Burgess Park across the street is already overbooked and unavailable to soccer and baseball teams. How will we accommodate so many new residents who are in high-density housing without an open space?
2. **Remove the apartment complex driveway onto Laurel** to preserve bike safety for school children and pedestrians and to reduce the existing gridlock on Laurel Street. The smaller driveway for the townhome residents can remain as indicated in the current plan.

3. **Use the currently gated SRI driveway onto Middlefield** to redirect traffic flow as a viable alternative to the removal of the Laurel Street for the apartment buildings. The office traffic can be significantly reduced on the SRI/ParkLine office and apartment driveways on Ravenswood if the Middlefield driveway opens. It will reduce Ravenswood gridlock to/from Middlefield and El Camino and direct commuter traffic more efficiently to Willow Road and Highway 101.

4. **Increase parking for renters and employees** since inadequate parking forces apartment renters, visitors and employees to clog residential streets with traffic while looking for parking and for taking up limited residential parking.
(Note: In the 12/12 Planning Commission meeting on the SRI EIR, some commissioners wanted to reduce the proposed parking to force renters/employees to use public transit. But the representative from the firm that will conduct the EIR said that studies showed that reducing parking spaces did NOT reduce cars or numbers of car trips. It just pushed drivers to surrounding residential areas to take street parking, which added traffic as well. There were no reductions in Greenhouse Emissions or in number of car trips.)

5. **Provide underground parking** for the apartment buildings and for the offices to reduce the overall height of the project (especially to reduce the height of the 3-story parking garage behind the Barron Street homes facing bedrooms and private living spaces on both floors of the homes) and the potential of five six-story apartment buildings if the project is approved for the 600 total housing unit option being considered. Although developers say underground parking is costly, based on current Menlo Park office rental pricing, the one million square feet of office can command an estimated \$50M per year. Considering the negative impact on the surrounding areas of this project, the cost of underground parking for the benefit of the community will be offset by the profits from just the office space alone. The apartment rental income will be another large annual revenue generator since most of the units will be at high market-rate pricing (e.g. SpringLine's rental pricing).

6. **Include an emergency water storage tank** since there is no emergency water for residents and workers west of El Camino (per the latest Menlo Park Municipal Water Report that was mailed to residents) which stated that the emergency well in the City Yard is not online yet. The risk of toxic contamination of the City Yard emergency well makes it a problem since the city's gas tanks and city yard with other toxic substances (oil, pesticides, etc.) are above it and risk leaking into the groundwater, especially in the expected large earthquake event at some point in the future.

Thank you for your serious consideration of a lower-impact development solution,

Sue Connelly

From: [Dr. Harvey Fishman](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]New development comments
Date: Monday, January 23, 2023 4:52:11 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

- The project should net out to provide the state-mandated housing number of 400, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on the Ravenswood driveways if

Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Sent from my iPhone.

Best Harvey
650-387-8481 cell

From: [Patti Fry](#)
To: [Planning Commission](#)
Cc: [CCIN](#)
Subject: SRI Parkline project
Date: Monday, January 23, 2023 8:22:39 AM

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Planning Commissioners --

Please be sure that the project is modified so it improves rather than worsens the jobs/housing imbalance in Menlo Park.

There are decades of precedent with SRI to manage the number of allowed workers on site, well-documented by a submission in the public record by former Council Member Paul Collacchi, The current proposed project blows out prior precedent, including when land was spun off for housing. Managing the number of workers continues to be an important lever.

The proposed EIR scope continues to include worker density metrics that likely would greatly underestimate the potential number of workers and related negative impacts. The staff report describes office worker density assumptions of 250 SF/worker whereas tech companies have allocated 50-150 SF/worker, 66% to 400% more. Be sure that the metrics used will measure realistic impacts. Fix the metrics to be used in the analysis.

Patti Fry, former Menlo Park Planning Commissioner

From: [JoAnne Goldberg](#)
To: [PlanningDept](#)
Cc: [CCIN](#)
Subject: [Sent to Planning]Planning commission meeting January 23: item G1, Parkline Study Session
Date: Monday, January 23, 2023 11:55:13 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning Commissioners and Staff:

Thank you for accepting comments on this important project.

First, I want to endorse the information and analysis that former council member Paul Collacchi sent the Council and Planning Commission two weeks ago, asking for a big picture EIR analysis of the entire project, including the longer-term impact on housing requirements. His analysis points out that the overall project will increase the new housing obligation by over 2,000 units. Long-term consequences always need to be a consideration.

Meanwhile, tonight's study session focuses on the addition of 400-600 housing units in high-rise apartment buildings with few (if any) amenities offered to those new residents, or to current residents of the city. Burgess Park is across the street, but as the only city park with diverse facilities designed to meet the needs of a large segment of the population, it is already fully utilized (until this year, I scheduled practices and games for our local non-profit, all-volunteer youth soccer organization, AYSO. Space all over town is severely limited, especially at Burgess. We don't have enough room for our kids to play as is).

Next, proposals for this housing project specify that it be massively underparked, with (paid) housing advocates suggesting even less housing, holding up visions of a utopian community in which everyone -- no matter their age, physical health, or work/family obligations -- can bike or walk everywhere. In reality, the residents are going to have cars, which will either have to be parked at Burgess or in adjacent neighborhoods.

In the past, the city Planning Commission has rejected projects that did not meet parking requirements. I urge you to continue that tradition with this project.

Although most people in Menlo Park seem unaware of the Parkline project, it will impact almost all neighborhoods and have a deleterious effect on east-west connectivity. I second's Paul's request to expand the EIR to encompass most of the city, with particular note to the fact that Ravenswood and Laurel Street are heavily used by children bicycling to school.

I ask that you consider the needs of all residents and take a long-term approach to this proposal. Once the project has been approved, the change will be irrevocable.

JoAnne Goldberg

From: [Kathy Goodell](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Springline Project Requests
Date: Monday, January 23, 2023 6:30:08 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

For the SRI/Springline project I respectfully request that you not exceed the 400-residential unit plan and keep office at the current C1 level, have the apartment complex not exit onto Laurel, and provide additional (not less) parking --including underground parking for offices and renters.

For those wishing to go west on Ravenswood (to connect to downtown and El Camino) our only street exit from Linfield Oaks is at the Laurel/Ravenswood intersection and in case of emergency and everyday travel (and for vehicles coming from the police station on Laurel) it's important to not have huge traffic bottlenecks at the Laurel/Ravenswood intersection. Opening up the Middlefield gate for the SRI/Springline folks would seem a logical alternative to reroute and help alleviate traffic pressure at Laurel/Ravenswood.

Thank you for your consideration of my requests.

Sincerely,

KATHY

Katherine L. "Kathy" Goodell
21 Willow Road
Menlo Park

From: [Tom Hall](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI Property
Date: Monday, January 23, 2023 8:49:28 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

I am in full agreement with my neighbor Sue Connelly regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area.

Tom Hall
212 Gilbert Ave.
Menlo Park

From: [Betsy Henze](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline
Date: Monday, January 23, 2023 10:46:57 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

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- The **project should net out to provide the state-mandated housing number of 400**, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
- Reduce the amount of office to comply with current C1 zoning. Do NOT increase the jobs-housing imbalance by adding any more office space to this proposal. We need to bring jobs and housing in balance, not keep widening the gap between them.
- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on the Ravenswood driveways if

Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

- Increase parking commensurate with office worker numbers and apartment dwellers. Fewer parking spaces onsite only pushes traffic into nearby neighborhoods, as the research recounted to the Commission during the 12/12/22 meeting indicated. Fewer parking spots than the number of workers' and residents' cars do NOT encourage use of public transit, but to using neighborhood streets for parking.
- Provide underground parking for both offices and housing units, reducing the need for car parking to take up valuable above ground space in the form of an above ground parking garage .
- Include the emergency water storage tank , because 1) there is no options for workers west of El Camino and 2) the city yard emergency well is in danger of possible contamination during an earthquake from existing onsite gas storage and toxic substances in the ground.

Quoting from my neighbor, Sue Connelly, who says it far better than I :

" SRI/ParkLine will have highly profitable housing and office revenue annually, but the costs will be borne by the taxpayers.

Based on current Menlo Park office rates, the office project stands to generate \$50M per year. This doesn't include ANY of the apartment rentals, for which most will be at very high rents (see the current rents for the new SpringLine apartments!). There will be some city revenue, but since SRI is a non-profit, this massive development will not offset many of the costs residents must pay for infrastructure (schools, police, fire, water and roads). Yet it will create a significant reduction in our quality of life (and possibly home values), bike/pedestrian safety for school children and residents, and increasing the state-mandated affordable housing units even more.

We need to require that any new office development provides/includes the affordable housing that the office spaces

and employee densities will be required to be built in Menlo Park."

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development .

Betsy Henze
320 Sherwood Way
Menlo Park

From: [Nancy Hosay](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review - requested changes
Date: Sunday, January 22, 2023 11:22:12 PM

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Planning commission members:

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- The **project should net out to provide the state-mandated housing number of 400**, in the amount required by Menlo Park for the developers planned amount of office space. Keep 400 apartments according to the original plan, but create a BMR (Below Market Rate) number of 25% of those 400 housing units, so no separate acreage for affordable housing will be required.
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- The apartment complex driveway on Laurel St, should be removed to reduce traffic on Laurel St., and to preserve bike and pedestrian safety, such as it is, on Laurel St. The smaller driveway for townhome residents would be less problematic and can remain as is in the current plan.
- Use the currently gated SRI driveway onto Middlefield to redirect traffic flow so Laurel St is not used by the apartment residents (see above point) . The office traffic can be significantly reduced on the Ravenswood driveways if

Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

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Based on current Menlo Park office rates, the office project stands to generate \$50M per year. This doesn't include ANY of the apartment rentals, for which most will be at very high rents (see the current rents for the new SpringLine apartments!). There will be some city revenue, but since SRI is a non-profit, this massive development will not offset many of the costs residents must pay for infrastructure (schools, police, fire, water and roads). Yet it will create a significant reduction in our quality of life (and possibly home values), bike/pedestrian safety for school children and residents, and increasing the state-mandated affordable housing units even more.

We need to require that any new office development provides/includes the affordable housing that the office spaces

and employee densities will be required to be built in Menlo Park."

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development .

Nancy Hosay
325 Linfield Place
Menlo Park

From: [John Henze](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review - Requested Changes
Date: Monday, January 23, 2023 3:11:59 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Planning commission members:

I am in full agreement with my neighbors regarding proposed changes in the density and size of the development. Taking the already extremely large total housing number from 400 units to 600 units, is a 50% increase! At 400 units the density of this development far outstrips anything in the adjoining neighborhoods, and jeopardizes basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area .

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significantly reduced on the Ravenswood driveways if Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

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We need to require that any new office development provides/includes the affordable housing that the office spaces and employee densities will be required to be built in Menlo Park."

Please don't forget about all of the long-time Menlo Park residents that value the quality of life that Menlo Park has long afforded. Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development.

Thanks,

John Henze

31 year Menlo Park resident
320 Sherwood Way

Confidentiality notice: This message may contain confidential information. It is intended only for the person to whom it is addressed. If you are not that person, you should not use this message. We request that you notify us by replying to this message, and then delete all copies including any contained in your reply. Thank you.

From: [Lauren John](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]
Date: Monday, January 23, 2023 9:50:19 AM

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Planning commission members:

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the Ravenswood driveways if Middlefield driveway opens, providing more egress options, and directing traffic closer to their destinations of Middlefield and 101 access.

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We need to require that any new office development

provides/includes the affordable housing that the office spaces and employee densities will be required to be built in Menlo Park."

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development .

George and Lauren John
331 Laurel Street
Menlo Park 94025

From: [John Kadwany](#)
To: [Planning Commission](#)
Cc: [CCIN](#)
Subject: Parkline/SRI proposal comments
Date: Monday, January 23, 2023 11:11:08 AM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning Commissioners:

Following are comments on the land use policies implied by the Parkline/SRI redevelopment proposal, followed by recommendations.

– This project presents as a large office park with some housing included. The parking including three multi-story parking garages is significantly out of scale for a transit-oriented proposal. There is a commercial-to-housing ratio of about 2:1 or 3:1 (including old buildings) by square footage. Given that the Specific Plan major developments (Stanford, 1300 ECR) are about 50:50 residential compared to office + retail, for square footage, that amount of commercial space is out of step with recent transit-oriented development.

– Given the scarcity of housing in the Bay Area, this proposed office-residential ratio should not be encouraged by the PC or the city. A better use of this site would be to include more housing and less commercial and parking space. I do not know of city policy or resident preferences for this projected level of commercial space, especially given over-built office capacity today.

– The current proposal is not that of a 'neighborhood' or 'mixed-use' as stated in the Master Plan. This is principally an office park. While pedestrian and bicycle circulation through the project is good, the site space is dominated by the commercial and parking buildings. The two amenity buildings do not create a mixed-use plan. (That's not to suggest significant retail should be included, so the 'mixed-use' goal needs clarification. Certainly the office + residential design is not 'mixed-use'.) The 'open space' is numerically generous, and the designated use areas are good, but the overall layout is not that of an inviting public space. The plan does provide desirable benefits including the planned affordable housing area and the playing field.

- The current configuration of commercial buildings and parking garages, while apparently (and gratefully) not designed as 'secure' areas, are not oriented to encourage interaction with the community, or even the planned residences. The busy scenes full of pedestrians or office workers shown enjoying walkways in the project slides will not likely materialize.

– The rezoning and General Plan amendments options are open-ended. I do not agree with changes which would allow the development as proposed. It's a poor use of this site, more appropriate to urban planning now several decades past. I would not want amendments or zoning allowing new or existing buildings to be sold off to others, at least for significant periods of time. Plans for existing buildings including 'P', 'T' and 'S', and options for the affordable housing plan area, should be clarified.

- I understand the applicant is assuming that existing commercial entitlements, based on square footage, justify the proposed commercial space and parking. Instead, the applicant should acknowledge the very low intensity uses SRI has enjoyed in Menlo Park for decades. The applicant, PC and CC should use past site use intensities as a point of comparison for overall benefit-cost comparisons. A smaller total commercial use target should be considered.

RECOMMENDATIONS:

- The plan needs a different balance of residential-commercial use of the site, and reduction of multi-story parking. For that, the site perimeter and large site size are sufficient to accommodate higher buildings for the site interior, keeping in mind existing streets and neighborhoods. For comparison, San Mateo and Palo Alto have several higher and older residential buildings mixed in smaller scale neighborhoods or downtowns. Consideration should be given where relevant to additional height for residential and commercial buildings to add floor area. Affordable housing plans could be integrated with these changes.

- Given fewer and possibly taller buildings, the remaining open space can be consolidated into a larger space shared by commercial and residence buildings. Such an approach could create a genuine shared open space, and a distinctive neighborhood less isolated from the adjoining residences, streets and neighborhoods.

Sincerely,
John Kadvany / College Avenue

From: [Kenneth Everett Mah](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/ParkLine Study Session with Planning Commission public comment
Date: Monday, January 23, 2023 4:33:26 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Menlo Park Planning Commission and Planning Department,

My wife and I are writing to express our concerns about the project overall and EIR, and request additional items be added to the scope and be studied/changed. We, along with our 4.5yo daughter and 7mo son, bought our home in the Burgess Classic neighborhood ~1 year ago (November 2021) and live directly on Laurel St across from Burgess Pool. We have lived on Laurel St for now 6+ years total.

Generally, we are concerned about the impact of the size of the residential and commercial development on local safety and resources. Specifically, traffic on Laurel St, safety of biking and walking on Laurel street especially for children since it's a safe route to school, and utilization of Burgess Park amenities.

- Entrances/exits on Laurel St
 - These should all be removed. All traffic, both residential and commercial, should be routed to Middlefield and Ravenswood. There is an opportunity to create an additional network of roads within SRI to either offload current traffic or at a minimum keep new traffic that will be added by this project off Laurel St, which is residential. We requested this in writing and verbally to both the City Council/Planning Commission and Lane Partners, but continue to be ignored and have not received any explanations on why they want to direct the new residential traffic onto Laurel as opposed to the internal SRI roads or Ravenswood. Furthermore, not having driveways onto Laurel would encourage new residents to use alternative modes of transportation rather than drive.
 - Request: Please remove all entrances/exits on Laurel St, or study the impact on traffic on Laurel St and demonstrate there will be no difference from the current state. Also, study the impact at the different variations of housing density.
- Safety on Laurel St
 - Laurel St is a residential street that is designated a safe route to school. Any increase in car traffic or driveway use (the current SRI driveways on Laurel have minimal traffic to no traffic) will compromise the safety of children. Walking and biking will be more dangerous due to traffic and more intersections. We have verbally requested Lane Partners extend truly protected (by physical barriers such as curb, and not just paint) bike lanes in both directions on Laurel from Ravenswood to Burgess, and they verbally agreed, but we don't see it on the proposal.
 - Request: Please remove all entrances/exits on Laurel St, or study the impact on traffic on Laurel St and demonstrate that traffic accidents (car vs car, car vs bike, car vs pedestrian) will not increase, and the impact of at the different variations of housing density.
 - Request: Install truly protected (by physical barrier such as curb or

- immobile ballard) bike lanes in both directions on Laurel St from Ravenswood to Burgess.
 - Request: Install truly protected (by physical barrier such as curb or immobile ballard) bike lanes in both directions on Burgess Drive from Laurel St to SRI/Menlo Park Corporation Yard (since this will be open to bike/pedestrian traffic).
 - Also, would like protected bike lanes the full length of Burgess between Alma and SRI whether as part of this Parkline Project or the Middle Tunnel.
- Utilization of Burgess Park amenities
 - Adding 400+ units and commercial space will severely overcrowd the amenities at Burgess Park, and decrease how current residents can use them. These include the pool, tennis courts, playground, library, gymnastics center, etc. and the associated classes with them, such as gymnastic and dance classes, swim lessons, etc.
 - Request: Study the impact on Burgess amenities by specific amenities, not generally, and class/course offerings at each amenity, and demonstrate there will be no difference than current state. Also, study the impact at the different variations of housing density.
 - Request: Give Burgess Classics residents priority and discounted/free access to Burgess Park amenities if the Parkline development will impact access in any way.
- Menlo Park Corporation Yard Parking lot
 - This parking lot is primarily used by MP staff during the day, and Burgess Classics residents at night. We are currently not allowed to get annual overnight parking passes despite our limited street parking, but we can use the lot and tennis court. We are concerned that Parkline residents and workers will use the lot, as will other people who come to use the public space and amenities in Parkline as it is the closest parking lot to SRI/Parkline.
 - Request: Study the impact of the development on use of the Corporation Yard parking lot during the day, evening, and overnight, and demonstrate there will be no impact.
 - Request: If there is an impact, make lot not accessible to Parkline residents or workers nor the public, and give Burgess Classic residents access to overnight annual parking permits for free so we can park on the streets of Burgess Classics (Thurlow, Hopkins, and Barron) and the Corporation Yard parking lot.

Please let me know if you have questions or need clarification about these concerns or requests.

Thank you for your time and consideration,
Kenneth Mah

From: [Rob McCool](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]please reconsider SRI/ParkLine site specifics
Date: Monday, January 23, 2023 4:24:44 PM

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Dear Menlo Park City Council,

Reducing housing to office space imbalance by increasing housing should be a priority for us all. Increasing the housing to 600 units at this site, from 400, while still allowing 4000 more employees into the site, does nothing to relieve this imbalance.

I am also disappointed to see that parking is being reduced in an attempt to reduce car traffic. Our peninsula cities are simply not correctly set up for this to be realistic at this time, meaning that anyone living in these new properties will absolutely have a car, as will many of the employees commuting into the site each day. I urge the council to be realistic as to how people will get around our city from this new development, which is going to remain car-based due to the last mile problem associated with caltrain.

Finally I would also urge the council to consider Laurel Street, and not include a driveway onto Laurel from this complex. Middlefield is far more well set up to handle this increased traffic, and would be the more appropriate way to direct traffic. Our police frequently use Laurel Street to get to and from various parts of town and introducing more traffic blockage on Laurel is not going to be positive.

Thanks, Rob McCool 360 Sherwood Way

From: [Peter C](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]Traffic at SRI
Date: Sunday, January 22, 2023 5:07:35 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning Department,

It is apparent that the Planning Department and City Council are acting counter to the concerns of the Burgess Classics neighbors. Yes, we have a housing deficit in the Bay Area, but replacing it with this project does not solve the area's housing problem.

My concerns are as follows:

- 1) Major traffic along Laurel, Ravenswood and Middlefield. We need to make sure the trip caps are low enough to manage this large project.
- 2) This project will create an imbalance to jobs to housing units, further exacerbating the region's housing crisis. Let's not use tax receipt collections as a smoke screen to endorse the project. We need to ensure it does not impact schools and our local infrastructure.
- 3) 600-unit mid-rises don't conform to the area's existing uses.

I'm generally supportive, but let's go back to 400 units the original proposal by the developer.

Thank you

Peter C (District 3 resident)

From: [Susan Stimson](#)
To: [PlanningDept](#); [_CCIN](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review - requested changes
Date: Monday, January 23, 2023 3:50:22 PM

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City Council and Planning Commission Members,

As a 14 year resident of Menlo Park, I urge you to curtail the scope of the Parkline housing project to protect safety and accessibility in Menlo Park.

As you know from past examination of the railroad crossings, the crossing at Ravenswood is especially tenuous during high traffic hours which surround both business hours AND very importantly school hours.

In addition, the accessibility to and from Highway 101 via Willow road has deteriorated. Of course, there was respite amidst the pandemic, however, the existing two lane road is insufficient to accommodate future growth.

The city has expressed interest in forward and future thinking which I think is apt. Preparing for additional housing is an important part of that for certain.

That said, the plans must be coupled with forward thinking and planning regarding infrastructure to accommodate additional neighbors such as above/below grade railroad crossings and additional routes to access highways 101 and 280. Not doing so puts current and future neighbors at risk and lacks prudence.

The Parkline project is scoped to add over twice as many units as the 2 large developments yet to be inhabited (Springline is open but not at capacity and the Stanford project is still under construction). Despite how the city chooses to draw district lines, all properties are adjacent to downtown. While convenience to public transit is a benefit, it is not realistic or fair to assume that new residents will give up their freedom of owning and using an automobile. People have lives off of El Camino... kids sports activities, jobs off highways vs downtown, jobs like sales or construction that require daily driving, hiking in the hills, volunteering on the coast for example.

While I understand that speculative models have been generated regarding the potential effects to traffic and safety, I urge the city to "digest" the new additions from other downtown adjacent developments before adding extensively to them.

I am fully supportive of adding new housing on the SRI campus and the campus development overall. I also support stipulating that a higher percentage become affordable housing.

My asks:

- Perform a traffic and safety assessment subsequent to the large developments on El Camino being inhabited. That will be possible very soon if the need for housing near downtown is dire.
- Perform a survey of those new neighbors to see how they in fact are commuting and using / not using public transit.
- Ensure city of the future planning includes near term investments in infrastructure to improve access to highways 101 and 280 and also above or below grade RR track crossings

Thank you for your consideration regarding rejecting this enlarged and negatively impactful proposal for this development.

Susan Stimson

From: [Karen Wang](#)
To: [PlanningDept](#)
Subject: [Sent to Planning]SRI/Parkline Plan Review - requested changes
Date: Monday, January 23, 2023 4:29:25 PM

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Planning Commission and City Council members:

I object to proposed changes in the density and size of the SRI/Parkline development for the following reasons.

- At even 400 housing units, never mind 600, the density of this development far outstrips anything in the adjoining neighborhoods and will negatively impact basic quality of life issues including resultant lack of parking, crowding, school and infrastructure impacts and traffic in this area.
- We should not increase the jobs-housing imbalance by adding any more office space to this proposal. We need to stop big office development until we meet the affordable housing deficit for the offices already built and others already approved in the pipeline. We need to bring jobs and housing in balance, not keep widening the gap between them.
- It is fantasy to believe workers and residents will exclusively use public transit and not have cars. The traffic and parking impact on the surrounding neighborhoods will be terrible.

I hope you reject this enlarged and negatively impactful proposal for this development. Thank you for your consideration.

Karen Wang
29 Willow Road
Menlo Park



STAFF REPORT

Planning Commission

Meeting Date:

2/6/2023

Staff Report Number:

23-010-PC

Public Hearing:

Consider and adopt a resolution to approve a use permit to demolish an existing single-story, single-family residence and construct a new two-story residence with an attached garage on a substandard lot with regard to minimum lot width and area in the R-1-U (Single Family Urban Residential) zoning district, at 440 University Drive

Recommendation

Staff recommends that the Planning Commission adopt a resolution approving a use permit to demolish an existing one-story, single-family residence and construct a new two-story residence on a substandard lot with regard to minimum lot width and area in the R-1-U (Single Family Urban Residential) zoning district. The proposal also includes an attached accessory dwelling unit (ADU), which is not subject to discretionary review. The draft resolution, including the recommended actions and conditions of approval, is included as Attachment A.

Policy Issues

Each use permit request is considered individually. The Planning Commission should consider whether the required use permit findings can be made for the proposal.

Background

Site location

The subject property is located on the northeastern side of University Drive, between Middle Avenue and College Avenue in the Allied Arts neighborhood. All neighboring properties are also located in the R-1-U zoning district. A location map is included as Attachment B. This block of University Drive features many one-story homes in the ranch and Spanish architectural styles, but several properties have been redeveloped with newer two-story homes with primarily craftsman architectural styles.

Analysis

Project description

The applicant is proposing to demolish the existing one-story, single-family residence, and construct a new two-story, single-family residence with an attached ADU. A data table summarizing parcel and project characteristics is included as Attachment C. The project plans and project description letter are included as Attachment A Exhibits A and B, respectively.

The proposed residence would be a three-bedroom, three and one half-bathroom home. The attached ADU

would also be located on the first floor. The required parking for the primary dwelling would be provided by an attached, front-loading, two-car garage. The proposed residence would meet all Zoning Ordinance requirements for setbacks, lot coverage, floor area limit (FAL), daylight plane, parking, and height. Of particular note, the project would have the following characteristics with regard to the Zoning Ordinance:

- The main residence would be 2,755.8 square feet and the attached ADU would be 277.3 square feet and would exceed the maximum floor area limit. The proposed floor area would be 3,033.1 square feet, where 2,800 square feet is the maximum permitted; however, the maximum FAL is permitted to be exceeded by up to 800 square feet in order to accommodate the ADU;
- The main residence would be 34.2 percent, and the attached ADU would exceed the maximum building coverage by 236.1 square feet. The proposed residence and ADU would have a building coverage of 39.5 percent where 35 percent is the maximum; however, the maximum building coverage is permitted to be exceeded by up to 800 square feet in order to accommodate the ADU;
- The proposed second floor would be below the second floor limit with 1,287 square feet proposed where the maximum allowable second-story floor area is 1,400 square feet; and
- The proposed residence would be below the maximum height, with approximately 26 feet, four inches proposed where 28 feet is the maximum permitted height.

The proposed residence would have a front setback of 20 feet, two inches and a rear setback of 20 feet, 10 inches, where 20 feet is required in either case. The residence is proposed to be set back approximately eight feet, two inches on the left side and approximately five feet, five inches on the right side where five feet, three inches is the minimum side setback. The proposed second story would be stepped back from the first story on the front and right side, and a portion of the left side. The second story would be stepped back to 26 feet, two inches on the front, approximately nine feet, three inches on the right side, and to approximately 12 feet, two inches along the front two-thirds of the left side. The attached ADU, while not subject to the use permit request, would conform to the required front and side setbacks of the main residence, where four feet is the minimum required side setback.

Design and materials

The applicant states that the proposed residence would be constructed in a traditional architectural style. The siding material would be primarily horizontal lap siding with brick veneer siding accents on the front and right elevations. Roofing is proposed to be composition shingle roofing material. There would be several wood accent elements, including wood fascia trim, front porch columns, front bay window, and the rear trellis. The garage door is proposed to be steel, and the front door would be fiberglass. Windows would be fiberglass windows with no grid pattern.

All second-story windows would have sill heights with a minimum of three feet. The stairwell window would have a sill height of three feet, six inches from the stair landing. Given the additional setback of most areas of the second floor, and proposed landscape screening (addressed later in the report), staff believes potential privacy issues would be adequately addressed.

Staff believes that the scale, materials, and style of the proposed residence would result in a consistent aesthetic approach and are generally consistent with the broader neighborhood, given the similar architectural styles and sizes of structures in the area.

Trees and landscaping

The applicant has submitted an arborist report (Attachment D), detailing the species, size, and conditions of on-site and nearby heritage and non-heritage trees. The arborist report lists a total of 22 trees on and

around the subject property. Six of the trees (Trees #1-6) are heritage, and all are located on neighboring properties. There are 14 trees of various species located on the subject property. None of the on-site trees are heritage in size. Eight non-heritage trees of various species (Trees # 14-17, 19, 21, and 22) are proposed for removal.

The applicant has submitted a preliminary landscape plan. The proposed planting plan includes four trees (western redbud, Chinese flame tree, crape myrtle, and Japanese zelkova) along the left side property line. Two of the trees would be placed in front of the stair well landing. Existing trees on the neighboring property to the right would provide screening on the right side of the property. The remainder of the property would be landscaped with a variety of shrubs and ground cover.

The arborist report includes tree protection recommendations for the pre-construction, construction, and post-construction phases of the project. As part of the project review process, the arborist report was reviewed by the City Arborist. Implementation of all recommendations to mitigate impacts to the heritage trees identified in the arborist report would be ensured as part of condition 1h.

Correspondence

The applicant states in their project description letter that they conducted outreach via a virtual neighborhood meeting. The applicant received comments from two neighbors regarding construction staging and concerns about the location of a two-story home. The applicant provides their responses in the project description letter. Staff received one item of written correspondence from a neighbor expressing concerns with the height of the proposed residence and the location of the second story relative to the neighboring property (Attachment E).

Conclusion

Staff believes that the design, scale, and materials of the proposed residence are generally compatible with the surrounding neighborhood. The traditional style would be generally attractive and well-proportioned, and the inset of the second floor would help increase privacy, alleviating potential privacy concerns. Staff recommends that the Planning Commission approve the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 3 (Section 15303, "New Construction or Conversion of Small Structures") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City

Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Draft Planning Commission Resolution Adopting Findings of Approval for project Use Permit, including project Conditions of Approval
 - Exhibits to Attachment A
 - A. Project Plans
 - B. Project Description Letter
 - C. Conditions of Approval
- B. Location Map
- C. Data Table
- D. Arborist Report
- E. Correspondence

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings, and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:
Chris Turner, Associate Planner

Report reviewed by:
Kyle Perata, Planning Manager

PLANNING COMMISSION RESOLUTION NO. 2023-XX**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF MENLO PARK APPROVING A USE PERMIT FOR THE DEMOLITION OF AN EXISTING ONE-STORY, SINGLE-FAMILY RESIDENCE AND CONSTRUCTION OF A NEW TWO-STORY, SINGLE-FAMILY RESIDENCE ON A SUBSTANDARD LOT WITH REGARD TO MINIMUM LOT WIDTH AND AREA IN THE R-1-U (SINGLE FAMILY URBAN RESIDENTIAL) ZONING DISTRICT**

WHEREAS, the City of Menlo Park (“City”) received an application requesting to demolish an existing one-story, single-family residence, and construct a new two-story residence on a substandard lot with regard to minimum lot width and area in the Single Family Urban Residential (R-1-U) zoning district (the “Project”) from Thomas James Homes (“Owner” and “Applicant”) located at 440 University Drive (APN 071-403-280) (“Property”). The Project use permit is depicted in and subject to the development plans and project description letter which are attached hereto as Exhibit A and Exhibit B, respectively, and incorporated herein by this reference; and

WHEREAS, the Property is located in the Single Family Urban Residential (R-1-U) district. The R-1-U district supports single-family residential uses; and

WHEREAS, the proposed Project complies with all objective standards of the R-1-U district; and

WHEREAS, the proposed Project was reviewed by the Engineering Division and found to be in compliance with City standards; and

WHEREAS, the Applicant submitted an arborist report prepared by California Tree and Landscaping Consulting, Inc. which was reviewed by the City Arborist and found to be in compliance with the Heritage Tree Ordinance and proposes mitigation measures to adequately protect heritage trees in the vicinity of the project; and

WHEREAS, the Project, requires discretionary actions by the City as summarized above, and therefore the California Environmental Quality Act (“CEQA,” Public Resources Code Section §21000 et seq.) and CEQA Guidelines (Cal. Code of Regulations, Title 14, §15000 et seq.) require analysis and a determination regarding the Project’s environmental impacts; and

WHEREAS, the City is the lead agency, as defined by CEQA and the CEQA Guidelines, and is therefore responsible for the preparation, consideration, certification, and approval of environmental documents for the Project; and

WHEREAS, the Project is categorically except from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures); and

WHEREAS, all required public notices and public hearings were duly given and held according to law; and

WHEREAS, at a duly and properly noticed public hearing held on December 5, 2022, the Planning Commission fully reviewed, considered, and evaluated the whole of the record including all public and written comments, pertinent information, documents and plans, prior to taking action regarding the Project.

NOW, THEREFORE, THE MENLO PARK PLANNING COMMISSION HEREBY RESOLVES AS FOLLOWS:

Section 1. Recitals. The Planning Commission has considered the full record before it, which may include but is not limited to such things as the staff report, public testimony, and other materials and evidence submitted or provided, and the Planning Commission finds the foregoing recitals are true and correct, and they are hereby incorporated by reference into this Resolution.

Section 2. Conditional Use Permit Findings. The Planning Commission of the City of Menlo Park does hereby make the following Findings:

The approval of the use permit for the construction of new two-story residence on a substandard lot is granted based on the following findings which are made pursuant to Menlo Park Municipal Code Section 16.82.030:

1. That the establishment, maintenance, or operation of the use applied for will, under the circumstance of the particular case, not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing in the neighborhood of such proposed use, or injurious or detrimental to property and improvements in the neighborhood or the general welfare of the city because:
 - a. Consideration and due regard were given to the nature and condition of all adjacent uses and structures, and to general plans for the area in question and surrounding areas, and impact of the application hereon; in that, the proposed use permit is consistent with the R-1-U zoning district and the General Plan because two-story residences are allowed to be constructed on substandard lots subject to granting of a use permit provided that the proposed residence conforms to applicable zoning standards, including, but not limited to, minimum setbacks, maximum floor area limit, and maximum building coverage.
 - b. The proposed residence would include the required number of off-street parking spaces because one covered and one uncovered parking space would be required at a minimum, and two covered parking spaces are provided.

Section 3. Conditional Use Permit. The Planning Commission approves Use Permit No. PLN2022-00050, which use permit is depicted in and subject to the development plans and project description letter, which are attached hereto and incorporated herein by this reference as Exhibit A and Exhibit B, respectively. The Use Permit is conditioned in conformance with the conditions attached hereto and incorporated herein by this reference as Exhibit C.

Section 4. ENVIRONMENTAL REVIEW. The Planning Commission makes the following findings, based on its independent judgment after considering the Project, and having reviewed and taken into consideration all written and oral information submitted in this matter:

- A. The Project is categorically except from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures)

Section 5. SEVERABILITY

If any term, provision, or portion of these findings or the application of these findings to a particular situation is held by a court to be invalid, void or unenforceable, the remaining provisions of these findings, or their application to other actions related to the Project, shall continue in full force and effect unless amended or modified by the City.

I, Corinna Sandmeier, Principal Planner and Planning Commission Liaison of the City of Menlo Park, do hereby certify that the above and foregoing Planning Commission Resolution was duly and regularly passed and adopted at a meeting by said Planning Commission on February 6, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS THEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this 6th day of February, 2023

Corinna Sandmeier
Principal Planner and Planning Commission Liaison
City of Menlo Park

Exhibits

- A. Project Plans
- B. Project Description Letter
- C. Conditions of Approval

PLANNING SUBMITTAL FOR:

440 University Dr.

MENLO PARK, CA



VICINITY MAP:



NOT TO SCALE

PROJECT TEAM INFO:

Developer
Thomas James Homes
 255 Shoreline Dr. Suite 428
 Redwood City, CA 94065
 Tel: (408) 402-3024

Architect
Dahlin Group
 5865 Owens Drive
 Pleasanton, CA 94588
 Tel: (925) 251-7200
 Contact: Jaime Matheron
 jaime.matheron@dahlingroup.com

Landscape
Van Dorn Abed Landscape Architects, Inc.
 81 14th Street,
 San Francisco, CA 94103
 Tel: (415) 864-1921
 Contact: Hoanglan Nguyen
 hoanglan@valainc.com

DEVELOPMENT SUMMARY

LOCATION	440 UNIVERSITY DR.		
ASSESSOR'S PARCEL NUMBER	071-403-280		
PARCEL AREA - GROSS	5,200 SQ. FT.	0.12 AC	
ZONING DESIGNATION	R-1-U		
OCCUPANCY GROUP	R-3		
CONSTRUCTION TYPE	V-8		
MAX. FLOOR AREA LIMIT	2,800.00 SQ. FT.	PROPOSED FLOOR AREA LIMIT	2755.84 SQ. FT.
		FAL (INCLUDING ADU EXCEEDANCE)	3033.12 SQ. FT.
MAX. BUILDING COVERAGE (5,200/.35)	1,820.00 SQ. FT.	PROPOSED BUILDING COVERAGE (INCLUDING ADU EXCEEDANCE)	1,778.84 SQ. FT.
MAX. BUILDING HEIGHT	28'	PROPOSED BUILDING HEIGHT	28'-3 1/2" FROM ANG

REQUIRED SETBACKS

FRONT - STREET (FT)	20'
FRONT - STREET - AT GARAGE (FT)	20'
SIDE (FT)	5'-3"
REAR (FT)	20'

PROPOSED SETBACKS

FRONT - STREET (FT)	21'-8"
FRONT - STREET - AT GARAGE (FT)	24'-8"
SIDE - RIGHT (FT)	5'-4 1/2"
SIDE - LEFT (FT)	8'-1 1/2"
REAR (FT)	20'-10"

PARKING REQUIRED:
 2 TOTAL SPACES
 MIN. GARAGE DIMENSIONS: 10' X 20' PER SPACE

EXISTING USE: ONE SINGLE FAMILY DETACHED RESIDENCE OF TO BE DEMOLISHED.
PROPOSED USE: ONE NEW SINGLE FAMILY DETACHED RESIDENCE OF 3033.12 SQ. FT.

CODES AND REGULATIONS GOVERNING THE PROJECT: CURRENT 2022 CALIFORNIA CODES

SHEET INDEX:

- ARCHITECTURAL:**
 A.0 TITLE SHEET
 A.1 SITE AERIAL & PHOTOS
 AP1 AREA PLAN
 A.3 SITE PLAN
 A.4 FLOOR PLANS
 A.5 ROOF PLAN
 A.6 FLOOR AREA DIAGRAMS
 A.7 ELEVATIONS
 A.8 ELEVATIONS
 A.9 SECTIONS
 A.10 COLORS & MATERIALS
 RENDERING & COLOR BOARD

- AS-BUILTS:**
 1 FLOOR PLAN
 2 ROOF PLAN
 3 EXTERIOR ELEVATIONS

- CIVIL:**
 TO1 TOPOGRAPHIC SURVEY

- LANDSCAPE:**
 L1.1 CALLOUT/ LAYOUT PLAN
 L1.2 CONSTRUCTION DETAILS
 L2.1 IRRIGATION NOTES & WELO CHECKLIST
 L3.1 PLANTING PLAN & LEGEND
 L3.2 PLANTING DETAILS
 L3.3 TREE PROTECTION PLAN & NOTES

FRONTAGE IMPROVEMENTS

ALL EXISTING CRACKED OR DAMAGED FEATURES ALONG THE PROPERTY FRONTAGE MUST BE REPAIRED IN KIND. ADDITIONALLY, ANY FRONTAGE IMPROVEMENTS WHICH ARE DAMAGED AS A RESULT OF CONSTRUCTION WILL BE REQUIRED TO BE REPLACED. ALL FRONTAGE IMPROVEMENT WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CITY STANDARD DETAILS.

ANY ENCROACHMENT PERMIT FROM THE ENGINEERING DIVISION IS REQUIRED PRIOR TO ANY CONSTRUCTION ACTIVITIES, INCLUDING UTILITY LATERALS, IN THE PUBLIC RIGHT OF WAY.

3 BEDROOMS / 3.5 BATH +
 1 BEDROOM / 1 BATH ADU

440 UNIVERSITY DR.

FIRST FLOOR	1031.59 SQ. FT.
SECOND FLOOR	1287.00 SQ. FT.
TOTAL LIVING	2318.59 SQ. FT.
GARAGE	437.25 SQ. FT.
ADU	277.28 SQ. FT.
PORCH	54.50 SQ. FT.
LANAI	247.50 SQ. FT.

TOTAL (LIVING + GARAGE)	2755.84 SQ. FT.
FAL: (LIVING + GARAGE + ADU)	3033.12 SQ. FT.
MAX. FAL:	2800.00 SQ. FT.
THOMAS JAMES HOMES STANDARD 5.0 (LIVING + ADU + 34)	2619.87 SQ. FT.

BUILDING COVERAGE

FIRST FLOOR	1039.59 SQ. FT.
GARAGE	437.25 SQ. FT.
PORCH	54.50 SQ. FT.
LANAI	247.50 SQ. FT.
ADU	54.50 SQ. FT.
FIREPLACE	8.00 SQ. FT.
TOTAL (W/O ADU)	1778.84 SQ. FT.
TOTAL (W/TH ADU)	2056.12 SQ. FT.
MAX. BLDG. COVERAGE	1820.00 SQ. FT.

COVER SHEET

440 UNIVERSITY DR., MENLO PARK
 D28M200-TR

THOMAS JAMES HOMES



DATE 01-10-2023
 JOB NO. 1641.044

5865 Owens Drive
 Pleasanton, CA 94588
 925-251-7200

A.0



NOT TO SCALE

SITE AERIAL & PHOTOS

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

THOMAS JAMES HOMES

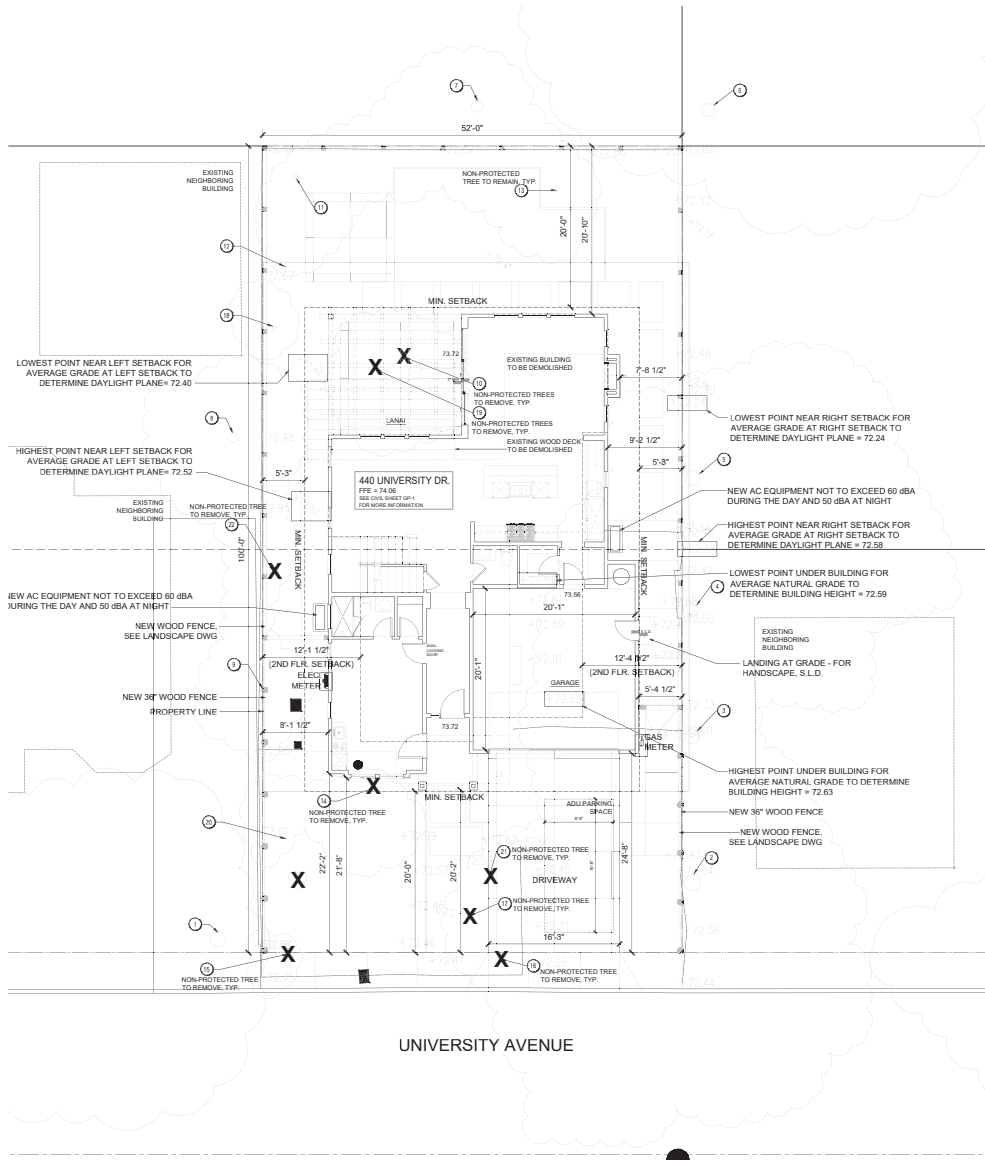


DATE 01-10-2023
JOB NO. 1641.044



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925-251-7200

A.1



TREE PROTECTION CHART NOTE: SEE ARBORIST REPORT FOR ADDITIONAL INFORMATION

KEY NOTE	TAG #	STATUS	LOCATION	SCIENTIFIC NAME	COMMON NAME	DBH (in)	ACTION
1	1340	PROTECTED	OFF-SITE	MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA	30	RETAIN
2	1341	PROTECTED	OFF-SITE	LIQUIDAMBAR	SWEET GUM	15	RETAIN
3	1342	PROTECTED	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	20	RETAIN
4	1343	PROTECTED	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	20	RETAIN
5	1344	PROTECTED	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	25	RETAIN
6	1345	PROTECTED	OFF-SITE	CEDRUS DEODARA	DEODAR CEDAR	35	RETAIN
7	1346	NON-PROTECTED	OFF-SITE	FIGUS CARICA	FIG	10	REMOVE
8	1347	NON-PROTECTED	OFF-SITE	DIOSPYROS VIRGIN	PERSIMMON	8	RETAIN
9	1348	NON-PROTECTED	ON-SITE	PITOSPORUM TOBIRA	SWEET TOBIRA	10	RETAIN
10	1349	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	9	REMOVE
11	1350	NON-PROTECTED	ON-SITE	CINNAMOMUM CAMPHORA	CAMPHOR	8	RETAIN
12	1352	NON-PROTECTED	ON-SITE	LYCIANTHES RANTONNETII	POTATO BUSH	6	RETAIN
13	1354	NON-PROTECTED	ON-SITE	CITRUS LIMON	LEMON	9	RETAIN
14	1355	NON-PROTECTED	ON-SITE	ARBUS UNEDO	STRAWBERRY	9	REMOVE
15	1357	NON-PROTECTED	ON-SITE	PRUNUS CERASIFERA	PURPLE LEAF PLUM	5	REMOVE
16	1358	NON-PROTECTED	ON-SITE	PRUNUS CERASIFERA	PURPLE LEAF PLUM	4	REMOVE
17	1359	NON-PROTECTED	ON-SITE	LAGERSTROEMIA INDICA	GRAPE MYRTLE	4	REMOVE
18	1351	NON-PROTECTED	ON-SITE	CITRUS LIMON	LEMON	3	RETAIN
19	1353	NON-PROTECTED	ON-SITE	CAMELLIA SP.	CAMELLIA	3	REMOVE
20	1356	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	3	RETAIN
21	1360	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	3	REMOVE
22	N/A	NON-PROTECTED	ON-SITE	STUMP	STUMP	10	REMOVE

REFER TO LANDSCAPE SHEETS FOR TREE DETAILS

This Site Plan contains information beyond the scope of work of the Architect. Information provided by Civil Engineer, Landscape Architect, and Arborist shall be verified in their respective documents.

3 BEDROOMS / 3.5 BATH +
1 BEDROOM / 1 BATH ADU

440 UNIVERSITY DR.	
FIRST FLOOR	1031.59 SQ. FT.
SECOND FLOOR	1287.00 SQ. FT.
TOTAL LIVING	2318.59 SQ. FT.
GARAGE	437.25 SQ. FT.
ADU	277.28 SQ. FT.
PORCH	54.50 SQ. FT.
LANAI	247.50 SQ. FT.
TOTAL: (LIVING + GARAGE)	2755.84 SQ. FT.
TOTAL: (LIVING + GARAGE + ADU)	3033.12 SQ. FT.
MAX. FAL:	2800.00 SQ. FT.
THOMAS JAMES HOMES STANDARD S.F. (LIVING + ADU + 1/2)	2619.87 SQ. FT.

BUILDING COVERAGE	
FIRST FLOOR	1039.59 SQ. FT.
GARAGE	437.25 SQ. FT.
PORCH	54.50 SQ. FT.
LANAI	247.50 SQ. FT.
ADU	54.50 SQ. FT.
FIREPLACE	8.00 SQ. FT.
TOTAL (w/o ADU):	1778.84 SQ. FT.
TOTAL (WITH ADU):	2056.12 SQ. FT.
MAX. BLDG. COVERAGE	1820.00 SQ. FT.

SITE PLAN

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

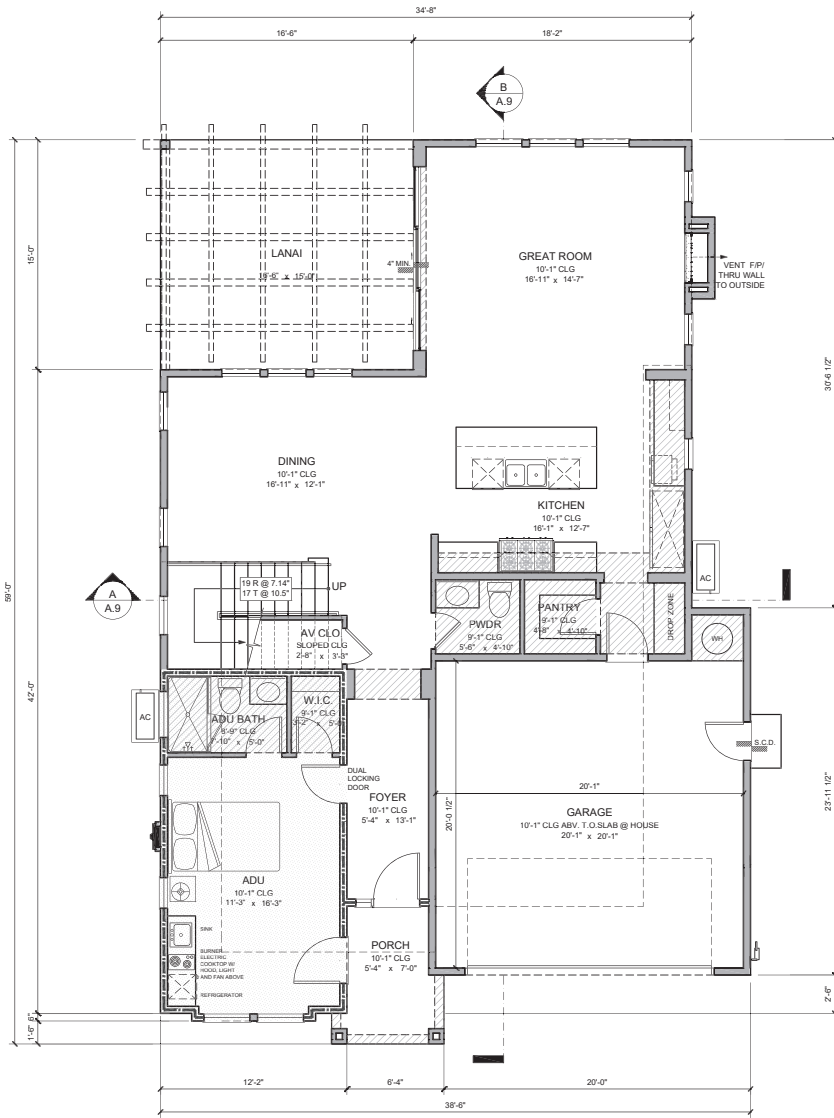
THOMAS JAMES HOMES



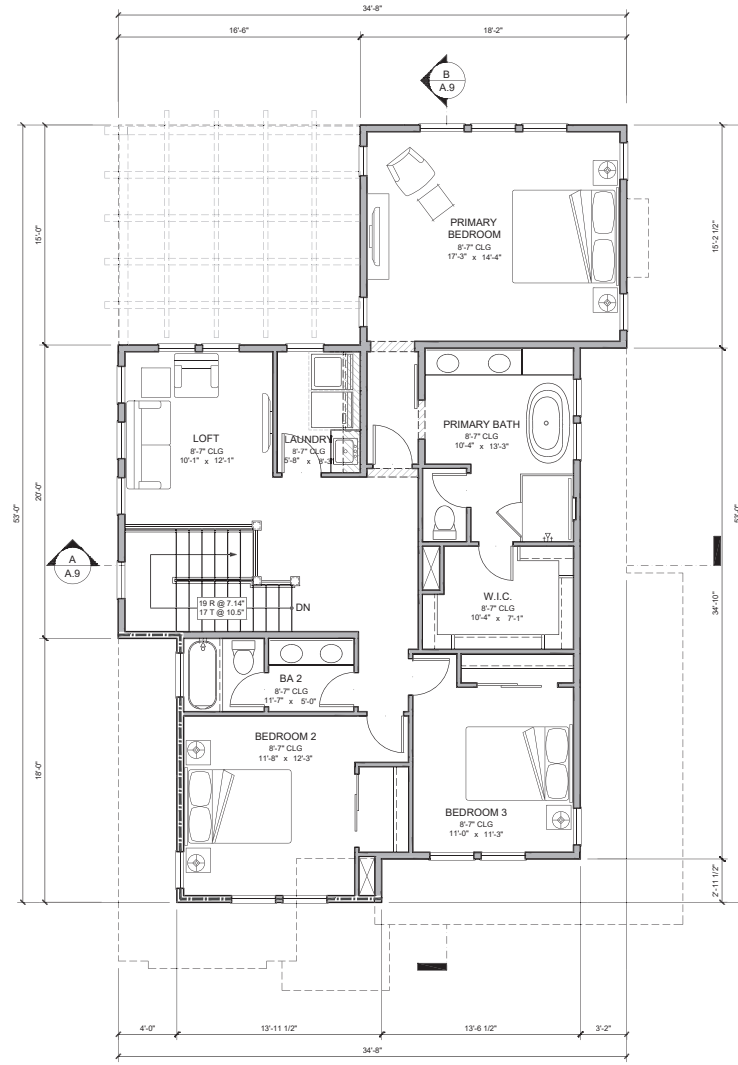
DATE 01-10-2023
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Pleasanton, CA 94588
925-251-7200

A.3



FIRST FLOOR PLAN
SCALE: 1/4" = 1'-0"



SECOND FLOOR PLAN
SCALE: 1/4" = 1'-0"

3 BEDROOMS / 3.5 BATH +
1 BEDROOM / 1 BATH ADU

440 UNIVERSITY DR.	
FIRST FLOOR	1031.59 SQ. FT.
SECOND FLOOR	1287.00 SQ. FT.
TOTAL LIVING	2318.59 SQ. FT.
GARAGE	437.25 SQ. FT.
ADU	277.28 SQ. FT.
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LANAI	247.50 SQ. FT.
TOTAL (LIVING + GARAGE)	2755.84 SQ. FT.
FAL (LIVING + GARAGE + ADU)	3033.12 SQ. FT.
MAX. FAL:	2800.00 SQ. FT.
THOMAS JAMES HOMES STANDARD S.F. (LIVING + ADU + 1/2)	2619.87 SQ. FT.

BUILDING COVERAGE	
FIRST FLOOR	1039.59 SQ. FT.
GARAGE	437.25 SQ. FT.
PORCH	54.50 SQ. FT.
LANAI	247.50 SQ. FT.
ADU	54.50 SQ. FT.
FIREPLACE	8.00 SQ. FT.
TOTAL (w/o ADU):	1778.84 SQ. FT.
TOTAL (with ADU):	2056.12 SQ. FT.
MAX. BLDG. COVERAGE	1820.00 SQ. FT.

FIRST FLOOR PLAN

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

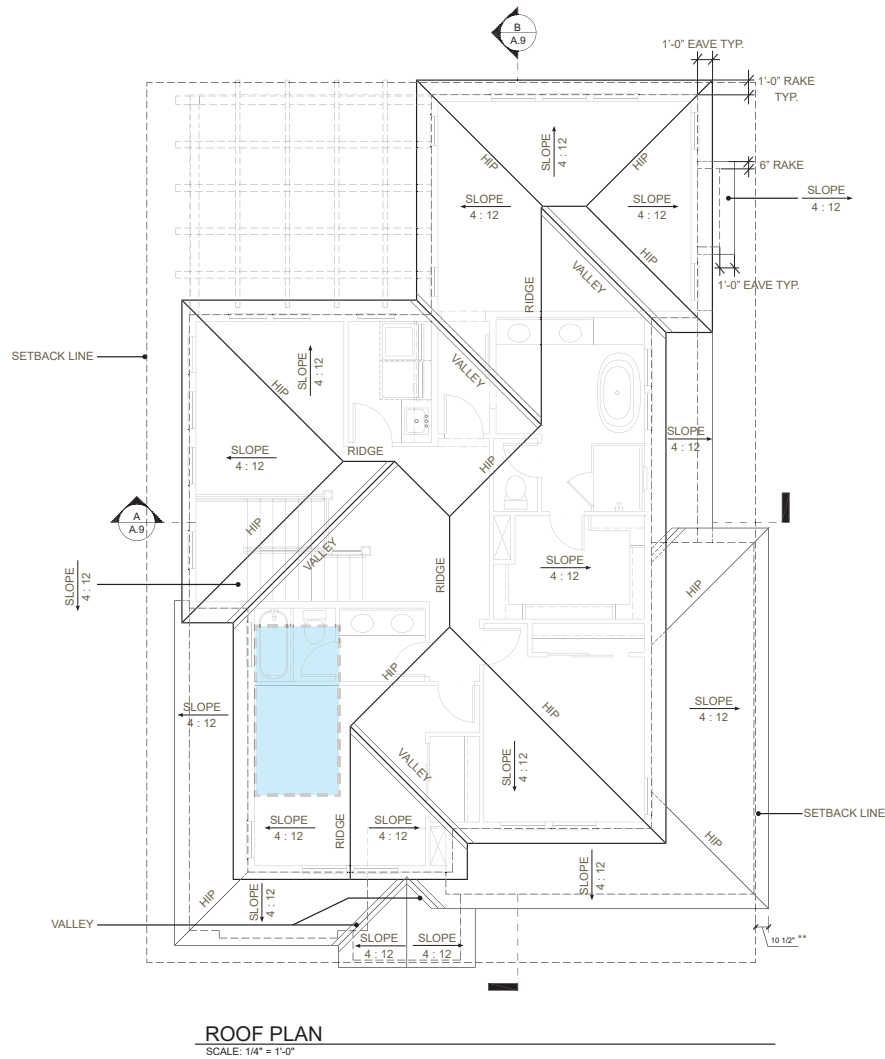
THOMAS JAMES HOMES



DATE 01-10-2023
JOB NO. 1641.044

5865 Owens Drive
Pleasanton, CA 94588
925-251-7200

A.4



** AS PER MENLO PARK GUIDELINES, 18" INTRUSION OF ARCHITECTURAL FEATURES, SUCH AS EAVES, IS ALLOWABLE INTO ANY YARD OF LESS THAN 10'. 3' INTRUSION OF ARCHITECTURAL FEATURES, SUCH AS EAVES, IS ALLOWABLE INTO ANY YARD OF 10' OR GREATER.



ALL ROOFING MATERIAL IS COMPOSITION SHINGLE UNLESS OTHERWISE NOTED.



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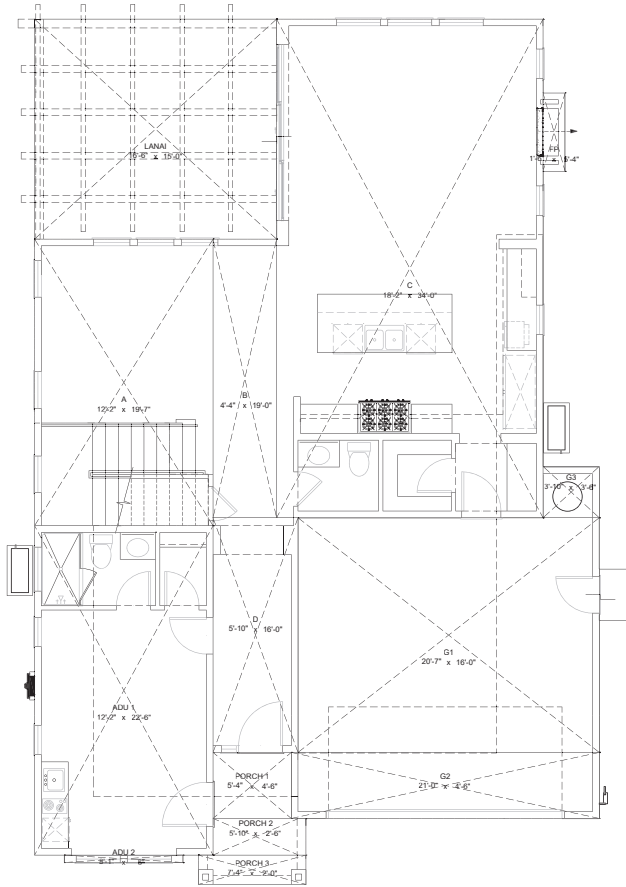
A.5

ROOF PLAN

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

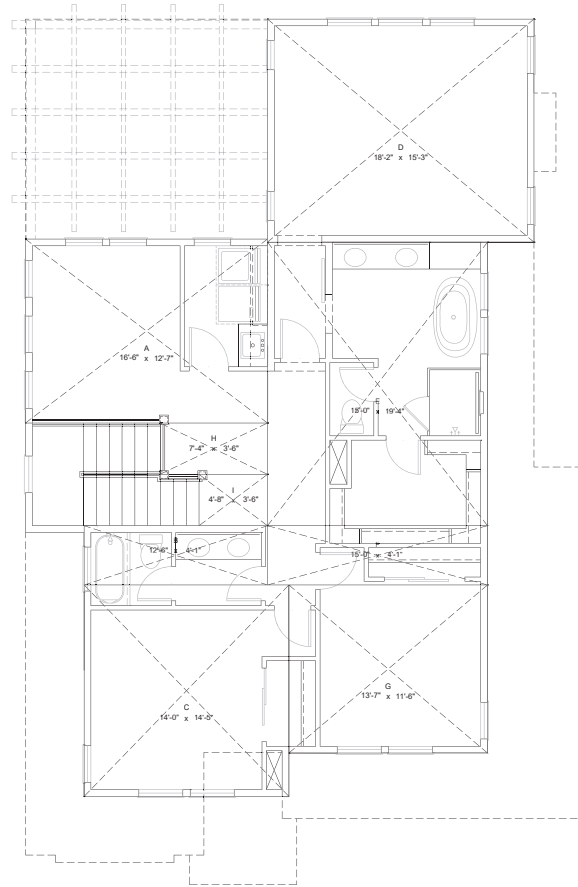
THOMAS JAMES HOMES





FIRST FLOOR AREA DIAGRAM

SCALE: 1/4" = 1'-0"



SECOND FLOOR AREA DIAGRAM

SCALE: 1/4" = 1'-0"

FIRST FLOOR AREA		
A	238.26 SQ. FT.	12'-2" X 19'-7"
B	82.33 SQ. FT.	4'-4" X 19'-0"
C	617.67 SQ. FT.	18'-2" X 34'-0"
D	93.33 SQ. FT.	5'-10" X 16'-0"
TOTAL	1031.59 SQ. FT.	
GARAGE		
G1	329.33 SQ. FT.	20'-7" X 16'-0"
G2	94.50 SQ. FT.	21'-0" X 4'-6"
G3	13.42 SQ. FT.	3'-10" X 3'-6"
TOTAL	437.25 SQ. FT.	
ADU		
ADU 1	273.24 SQ. FT.	12'-2" X 22'-6"
ADU 2	4.04 SQ. FT.	8'-1" X 6'-1"
TOTAL	277.28 SQ. FT.	
PORCH		
PORCH 1	24.00 SQ. FT.	5'-4" X 4'-6"
PORCH 2	15.83 SQ. FT.	5'-10" X 2'-6"
PORCH 3	14.67 SQ. FT.	7'-4" X 2'-0"
TOTAL	54.50 SQ. FT.	
SECOND FLOOR AREA		
A	207.63 SQ. FT.	16'-6" X 12'-7"
B	51.04 SQ. FT.	12'-6" X 4'-11"
C	201.83 SQ. FT.	14'-0" X 14'-5"
D	277.04 SQ. FT.	18'-2" X 15'-3"
E	290.00 SQ. FT.	15'-0" X 19'-4"
F	61.25 SQ. FT.	15'-0" X 4'-11"
G	156.21 SQ. FT.	13'-7" X 11'-6"
H	25.67 SQ. FT.	7'-4" X 3'-6"
I	16.33 SQ. FT.	4'-8" X 3'-6"
TOTAL	1287.00 SQ. FT.	
FLOOR AREA LIMIT		
FIRST FLOOR	1031.59 SQ. FT.	
SECOND FLOOR	1287.00 SQ. FT.	
GARAGE	437.25 SQ. FT.	
ADU	277.28 SQ. FT.	
TOTAL	3033.12 SQ. FT.	
LIVING + GARAGE	2755.84 SQ. FT.	
MAX. F.A.L.	2800.00 SQ. FT.	
BUILDING COVERAGE		
FIRST FLOOR	1031.59 SQ. FT.	
LANAI	247.50 SQ. FT.	16'-6" X 15'-0"
PORCH	54.50 SQ. FT.	
GARAGE	437.25 SQ. FT.	
ADU	277.28 SQ. FT.	
FIREPLACE	8.00 SQ. FT.	1'-6" X 5'-4"
TOTAL	2056.12 SQ. FT.	
TOTAL W/O ADU	1778.84 SQ. FT.	
MAX. BUILDING COVERAGE	1820.00 SQ. FT.	

FLOOR AREA DIAGRAMS

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

THOMAS JAMES HOMES



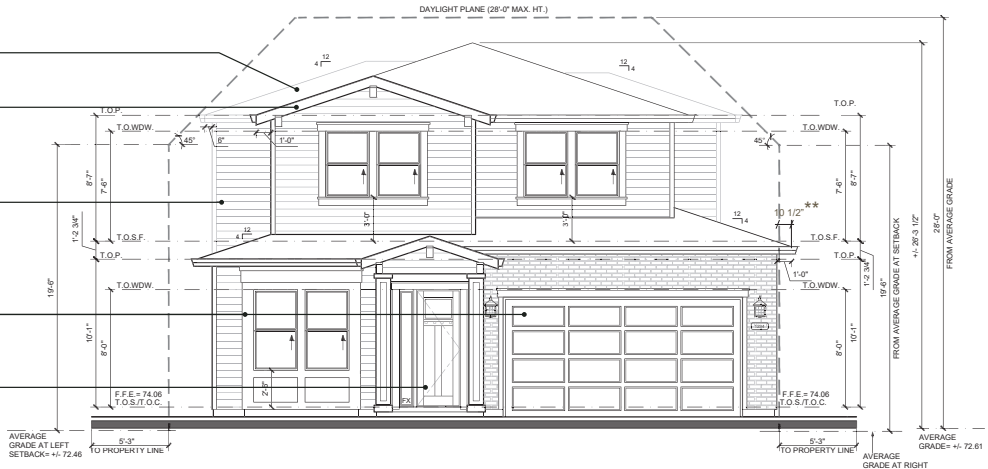
DATE 01-10-2023
JOB NO. 1641.044



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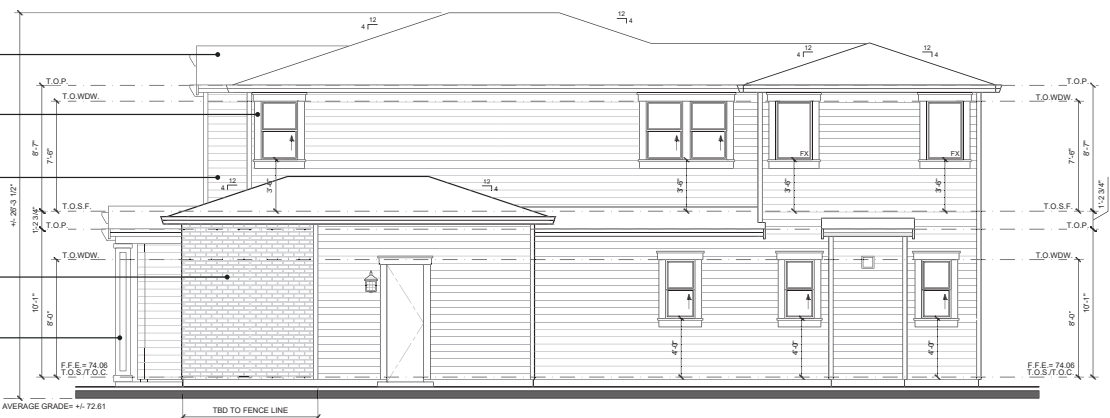
A.6

- ROOF MATERIAL
- COMPOSITION SHINGLE ROOF
- BODY COLOR
- WOOD FASCIA TRIM
- BODY COLOR
- CEMENTITIOUS HORIZONTAL SIDING
- BODY COLOR
- CEMENTITIOUS WINDOW & DOOR TRIM
- WOOD BAY WINDOW
- STEEL GARAGE DOOR
- ACCENT COLOR
- FIBERGLASS FRONT DOOR
- WOOD COLUMN & TRIM



FRONT ELEVATION
SCALE: 1/4" = 1'-0"

- ROOF MATERIAL
- COMPOSITION SHINGLE ROOF
- BODY COLOR
- CEMENTITIOUS WINDOW & DOOR TRIM
- BODY COLOR
- CEMENTITIOUS HORIZONTAL SIDING
- ACCENT MATERIAL
- BRICK VENEER
- ACCENT COLOR
- WOOD PORCH COLUMN



RIGHT ELEVATION
SCALE: 1/4" = 1'-0"

** AS PER MENLO PARK GUIDELINES, 18" INTRUSION OF ARCHITECTURAL FEATURES, SUCH AS EAVES, IS ALLOWABLE INTO ANY YARD OF LESS THAN 10'. 3" INTRUSION OF ARCHITECTURAL FEATURES, SUCH AS EAVES, IS ALLOWABLE INTO ANY YARD OF 10' OR GREATER.

WINDOWS
MARVIN ESSENTIAL ALL ULTREX WINDOWS TYP. - NO SIMULATED DIVIDED LITE

ELEVATIONS

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

THOMAS JAMES HOMES



DATE 01-10-2023
JOB NO. 1641.044

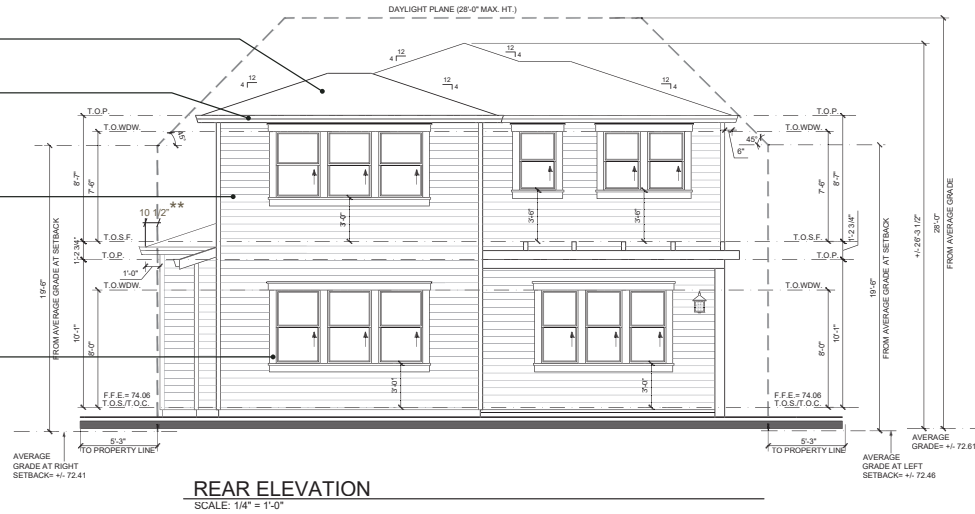
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Pleasanton, CA 94588
925-251-7200

A.7

ROOF MATERIAL
COMPOSITION SHINGLE ROOF
BODY COLOR
WOOD FASCIA TRIM

BODY COLOR
CEMENTITIOUS HORIZONTAL SIDING

BODY COLOR
CEMENTITIOUS WINDOW & DOOR TRIM



ROOF MATERIAL
COMPOSITION SHINGLE ROOF
BODY COLOR
CEMENTITIOUS WINDOW & DOOR TRIM
BODY COLOR
CEMENTITIOUS
HORIZONTAL SIDING

ACCENT COLOR
WOOD TRELLIS & WOOD POST



** AS PER MENLO PARK GUIDELINES,
18" INTRUSION OF ARCHITECTURAL
FEATURES, SUCH AS EAVES, IS
ALLOWABLE INTO ANY YARD OF
LESS THAN 10'. 3" INTRUSION OF
ARCHITECTURAL FEATURES, SUCH
AS EAVES, IS ALLOWABLE INTO ANY
YARD OF 10' OR GREATER.

**WINDOWS
MARVIN ESSENTIAL ALL ULTREX
WINDOWS TYP. - NO SIMULATED
DIVIDED LITE**

ELEVATIONS

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

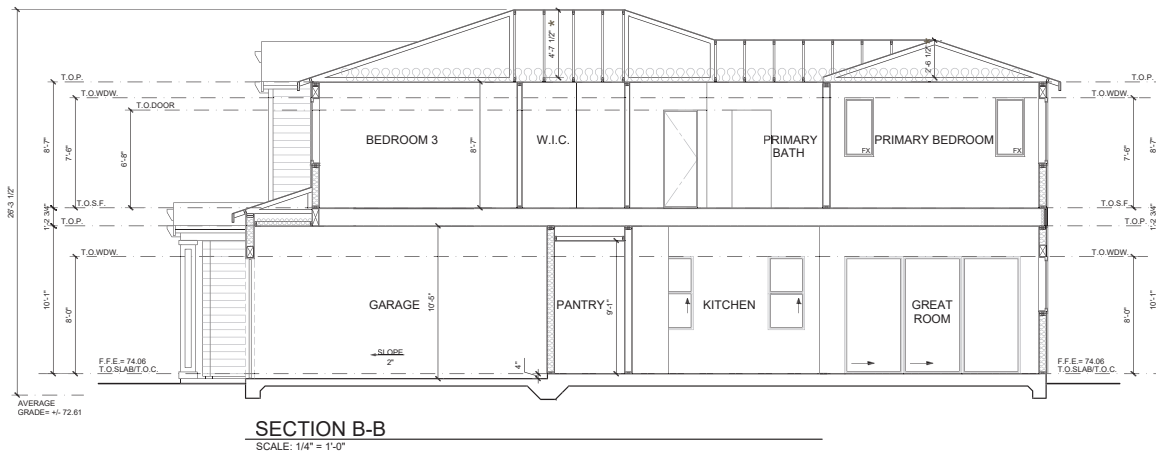
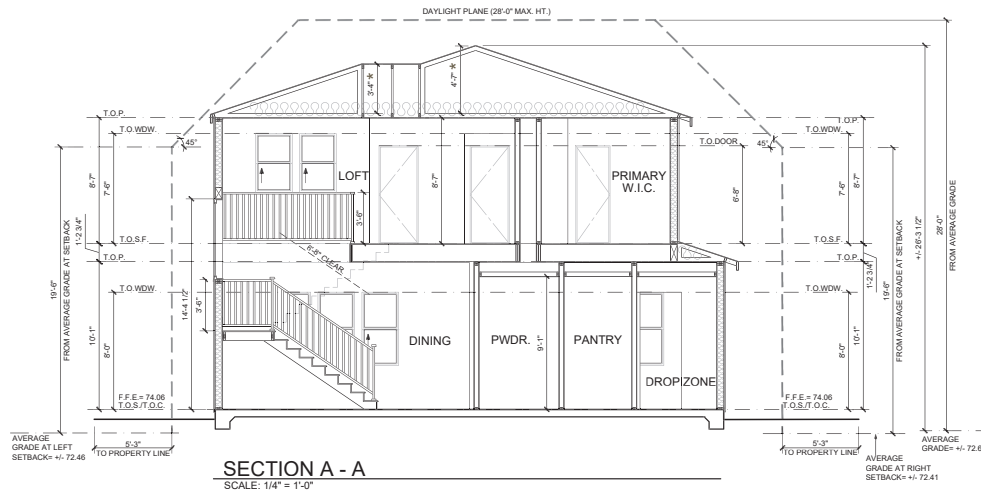
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DATE 01-10-2023
JOB NO. 1641.044

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Pleasanton, CA 94588
925-251-7200

A.8



* AS PER THE MENLO PARK MUNICIPAL CODE (SECTION 16.04.313 FLOOR AREA) ATTIC SPACE WHERE THE DISTANCE BETWEEN THE TOP OF THE CEILING JOIST AND THE BOTTOM OF THE ROOF SHEATHING MEASURES LESS THAN FIVE FEET (5') IS EXCLUDED FROM THE FLOOR AREA.

** AS PER MENLO PARK GUIDELINES, 18" INTRUSION OF ARCHITECTURAL FEATURES, SUCH AS EAVES, IS ALLOWABLE INTO ANY YARD OF LESS THAN 10'. 3" INTRUSION OF ARCHITECTURAL FEATURES, SUCH AS EAVES, IS ALLOWABLE INTO ANY YARD OF 10' OR GREATER.

SECTIONS

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

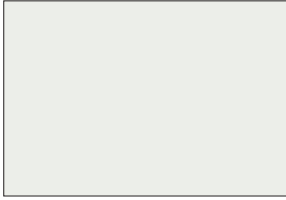
THOMAS JAMES HOMES



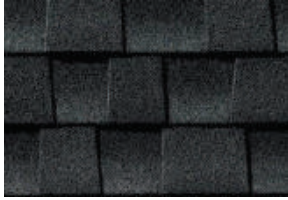
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A.9



BODY COLOR
HORIZONTAL SIDING, WINDOW & DOOR TRIM
BAY WINDOWS, PANELLING, EAVES, FASCIA,
GARAGE DOOR, BACK TRELLIS



ROOF MATERIAL
COMPOSITION SHINGLE



ACCENT COLOR
FRONT DOOR, FRONT PORCH COLUMN &
TRIM



ACCENT MATERIAL
BRICK VENEER

COLORS & MATERIALS

440 UNIVERSITY DR., MENLO PARK
D28M200-TR

THOMAS JAMES HOMES



DATE 01-10-2023
JOB NO. 1641.044

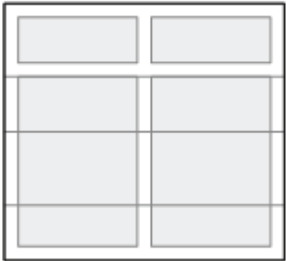
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Pleasanton, CA 94588
925-251-7200

A.10

WINDOW FRAMES: BLACK



FRONT DOOR
 MASONITE - HERITAGE - WINSLOW
 FIBERGLASS
 1/4 LITE 2 PANEL DOOR
 FULL SIDELITE



GARAGE DOOR
 CLOPAY GRAND HARBOR
 DESIGN 11, INSULATED
 SOLID TOP 11



FENCE STAIN
 SEMI-TRANSPARENT
 SPANISH MOSS



EXTERIOR RENDERINGS
 (NOT TO SCALE)

440

HOUSE NUMBERS



EXTERIOR LIGHT FIXTURE
 8"W X 15.25"H

PURE WHITE

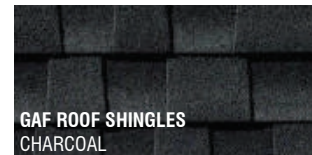
- SW 7005
- o SIDING
 - o WINDOW & DOOR TRIM
 - o PANELING, COLUMNS, & TRIM
 - o FASCIA, EAVES, & CORBELS
 - o BACK TRELLIS

TRICORN BLACK

- SW 6258
- o FRONT DOOR
 - o GARAGE DOOR



ELDORADO STONE
 TUNDRA BRICK - CHALK DUST



GAF ROOF SHINGLES
 CHARCOAL



440 UNIVERSITY DRIVE
 MENLO PARK, CALIFORNIA 94025

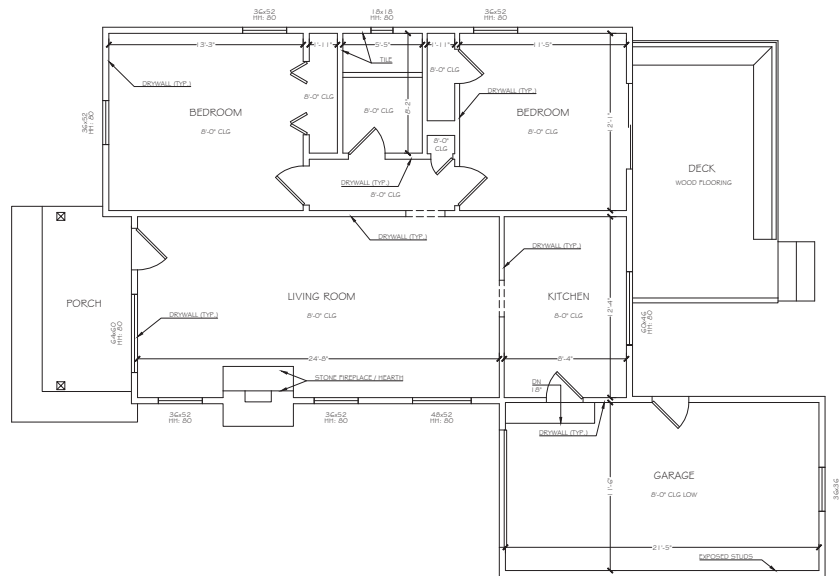
This is an example of design specifications for this particular plan and elevation. Detailed specifications, finishes and fixtures are subject to change, on homes prior to sale. All in any time without notice or obligation. Square footage and dimensions are approximate and may vary in construction and depending on the standard measurement and rounding. All items are subject to change without notice. © 2018 Thomas James Homes, LLC. All rights reserved. CA DRE License #0207297.

NOTE: DIMENSIONS PROVIDED IN THIS DOCUMENT ARE BASED OFF THE ARCHITECTURAL PLANS AND ARE TO BE VERIFIED IN FIELD. ACTUAL FIELD CONDITIONS MAY EFFECT THESE DIMENSIONS. PROJECT RANGER TO NOTIFY DESIGNER OF DIMENSIONS AND FIELD CONDITIONS THAT DIFFER FROM THE DESIGN PACKAGE AND ARCHITECTURAL PLANS.

DATE: 09.09.22
 DESIGNER: KRISTIN LASKY
 ARCHITECT: DAHLIN

NOTE: RENDERINGS SHOWN ARE FOR ILLUSTRATION PURPOSES ONLY AND ARE NOT INTENDED TO BE AN ACTUAL DEPICTION OF THE HOME OR IT'S SURROUNDINGS

**D28 M200 A
 TRADITIONAL**



LEGEND

- LOW CASEWORK
- UPPER CASEWORK
- FULL HEIGHT CASEWORK
- WDR = WASHER/DRYER COMBO
- WY = WASHER
- DR = DRYER
- R = RANGE
- REF = REFRIGERATOR
- OV = OVEN
- DW = DISH WASHER
- TC = TRASH COMPACTOR
- FURN = FURNACE
- W = WALL HEATER
- GM = GAS METER
- EM = ELECTRIC METER
- SC = SOLAR COMPONENTS
- EP = ELECTRICAL PANEL
- TWH = TANKLESS WATER HEATER
- WH = WATER HEATER
- WS = WATER SOFTNER
- FD = FLOOR DRAIN
- CLG = CEILING HEIGHT
- HT = HEADER HEIGHT

PPM
PRECISION PROPERTY
MEASUREMENTS

3626 E. PACIFIC COAST
HIGHWAY | 2ND FLOOR
LONG BEACH CA | 90804
T 562.621.9100
F 888.696.2966
WWW.PPMCO.NET



**WORRY FREE
RENOVATIONS**

PREPARED FOR

THOMAS JAMES
HOMES

PLAN TYPE

FLOOR PLAN

PROJECT NAME

440 UNIVERSITY
DRIVE
RESIDENCE

PROJECT ADDRESS

440 UNIVERSITY DRIVE,
MIRALTA PARK, CA 94025

PPM is not responsible for the accuracy of the information provided in this plan. The user of this plan is responsible for verifying the accuracy of the information provided in this plan. PPM is not responsible for the accuracy of the information provided in this plan. The user of this plan is responsible for verifying the accuracy of the information provided in this plan. PPM is not responsible for the accuracy of the information provided in this plan. The user of this plan is responsible for verifying the accuracy of the information provided in this plan.



SCALE
1/4" = 1'-0"

PROJECT
3385_BA

APPROVED BY
JS

DATE
06/14/22

SHEET
1 of 3



LEGEND
 FINISHED GRADE LINE
 FINISHED FLOOR LINE
 1/2" X
 ROOF FITCH LABEL (RISE/RUN)

PPM
 PRECISION PROPERTY
 MEASUREMENTS
 3626 E. PACIFIC COAST
 HIGHWAY | 2ND FLOOR
 LONG BEACH, CA 90804
 T 562.621.9100
 F 562.621.9100
 WWW.PPMCO.NET



PREPARED FOR

THOMAS JAMES
 HOMES

PLAN TYPE

EXTERIOR
 ELEVATIONS

PROJECT NAME

440 UNIVERSITY
 DRIVE
 RESIDENCE

PROJECT ADDRESS

440 UNIVERSITY DRIVE,
 MENDOTA, CA 94025

Notes:
 1. All dimensions are in feet and inches.
 2. All elevations are shown as finished conditions.
 3. All materials are to be installed in accordance with the manufacturer's instructions.
 4. All work is to be done in accordance with the applicable building codes.
 5. All work is to be done in accordance with the applicable local, state and federal laws.
 6. All work is to be done in accordance with the applicable local, state and federal regulations.
 7. All work is to be done in accordance with the applicable local, state and federal standards.
 8. All work is to be done in accordance with the applicable local, state and federal practices.
 9. All work is to be done in accordance with the applicable local, state and federal procedures.
 10. All work is to be done in accordance with the applicable local, state and federal protocols.

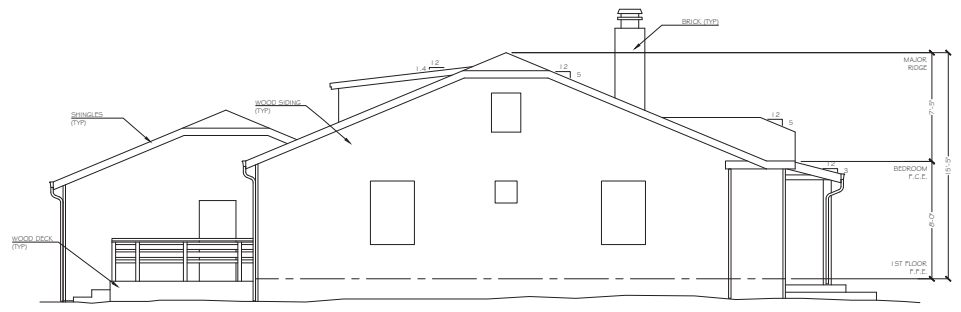
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PROJECT
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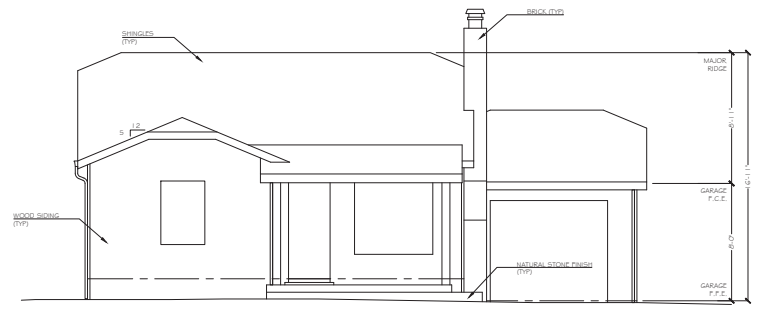
APPROVED BY
 JS

DATE
 06/14/22

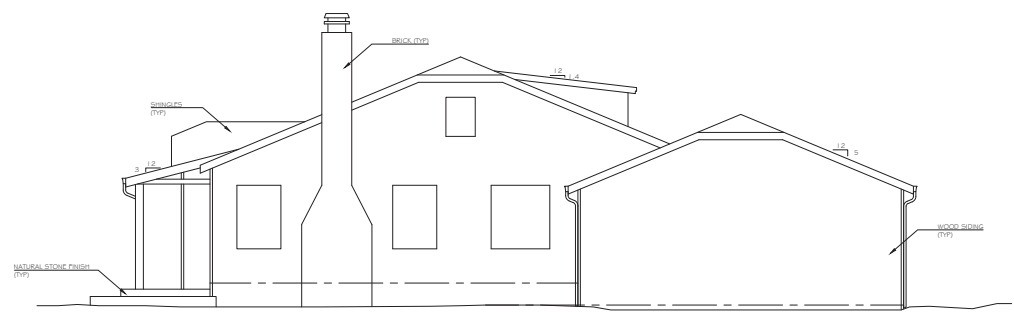
SHEET
 3 of 3



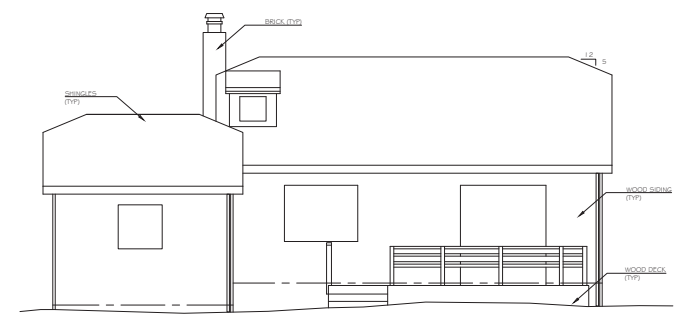
NORTHWEST



SOUTHWEST



SOUTHEAST



NORTHEAST

PROJECT INFO

- 1. TITLE REPORT: LAWYERS TITLE
FILE NUMBER: FLNP-006220065
DATED: MAY 13, 2022
- 2. PROPERTY ADDRESS: 440 UNIVERSITY DRIVE
MENLO PARK, CALIFORNIA
- 3. ASSESSOR'S PARCEL NO.: 071-403-280
- 4. AREA: 5,200± SF

BASIS OF BEARINGS

THE BEARING NORTH 56°37'00" WEST, OF THE SOUTHWESTERLY LINE OF LOTS 13 & 28 WHICH IS THE CURRENTLY THE NORTHEASTERLY BOUNDARY OF UNIVERSITY DRIVE BETWEEN MIDDLE AVENUE AND COLLEGE AVENUE ESTABLISHED BY SPLITTING THE PHYSICAL CURB IMPROVEMENTS AS SHOWN ON THAT CERTAIN SUBDIVISION MAP ENTITLED "MAP NO. 2-STANFORD PARK-MENLO PARK" RECORDED APRIL 2, 1913 IN BOOK 8 OF MAPS, AT PAGE 46, SAN MATEO COUNTY RECORDS, WAS USED AS THE BASIS OF ALL BEARINGS SHOWN ON THIS SURVEY.

BENCHMARK

CITY OF MENLO PARK BENCHMARK & CENTER STAR ON TOP OF CATCH BASIN, AT THE INTERSECTION OF HEMOSA WAY AND MIDDLE AVENUE; AT THE NORTHWESTERLY END OF THE NORTHWESTERLY CURB RETURN
NAVD88 ELEVATION: 88.09

FLOOD ZONE

THE SUBJECT PROPERTY IS SHOWN ON THE FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NUMBER 060810C0308E, DATED OCTOBER 16, 2012, AS BEING LOCATED IN FLOOD ZONE "X".

ZONE X: AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.

LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF MENLO PARK, COUNTY OF SAN MATEO, STATE OF CALIFORNIA AND IS DESCRIBED AS FOLLOWS:

PORTION OF LOTS 28 AND 29 IN BLOCK 2 AS SHOWN ON THAT CERTAIN MAP ENTITLED "MAP NO. 2 STANFORD PARK, SAN MATEO COUNTY, CALIFORNIA," FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN MATEO COUNTY, STATE OF CALIFORNIA ON APRIL 2, 1913 IN BOOK 8 OF MAPS AT PAGE 46, MORE PARTICULARLY DESCRIBED AS:

BEGINNING AT A POINT ON THE SOUTHWESTERLY LINE OF LOT 28 IN BLOCK 2, BEING THE NORTHEASTERLY LINE OF UNIVERSITY DRIVE EXTENDED DISTANT THEREON SOUTHEASTERLY 104 FEET FROM THE SOUTHWESTERLY LINE OF MIDDLE AVENUE AS SHOWN ON THE MAP ABOVE, REFERRED TO, RUNNING THENCE NORTHEASTERLY AND PARALLEL WITH THE SOUTHEASTERLY LINE OF MIDDLE AVENUE, 100 FEET TO THE NORTHEASTERLY LINE OF LOT 29 IN BLOCK 2; THENCE SOUTHEASTERLY ALONG SAID NORTHEASTERLY LINE, 52 FEET TO THE MOST EASTERLY CORNER OF SAID LOT 29; THENCE SOUTHWESTERLY ALONG THE SOUTHWESTERLY LINE OF LOTS 29 AND 28, 100 FEET TO THE MOST SOUTHERLY CORNER OF LOT 28; THENCE NORTHWESTERLY ALONG SOUTHWESTERLY LINE OF LOT 28, 52 FEET TO THE POINT OF BEGINNING.

EXCEPTIONS

THERE ARE NO PLOTTABLE EXCEPTIONS LISTED IN THE TITLE REPORT.

NOTES

- 1. EXISTING UNDERGROUND UTILITIES AND IMPROVEMENTS ARE SHOWN IN THEIR APPROXIMATE LOCATIONS BASED UPON RECORD INFORMATION AVAILABLE TO THE ENGINEER AT THE TIME OF PREPARATION OF THESE PLANS. ENGINEER IS NOT RESPONSIBLE FOR ACCURACY OR COMPLETENESS OF THE INFORMATION SHOWN.

SURVEYOR'S STATEMENT

I CERTIFY THAT THIS PARCEL'S BOUNDARY WAS ESTABLISHED BY ME OR UNDER MY SUPERVISION AND IS BASED ON A FIELD SURVEY IN CONFORMANCE WITH THE LAND SURVEYOR'S ACT. NO STREET MONUMENTATION WAS DISCOVERED DURING OUR SURVEY AND NONE IS REFERENCED IN THE SUPPORTING MAP OF RECORD. THEREFORE, WE HAVE UTILIZED THE PHYSICAL LINES OF OCCUPATION NOTED TO PREPARE THIS SURVEY.

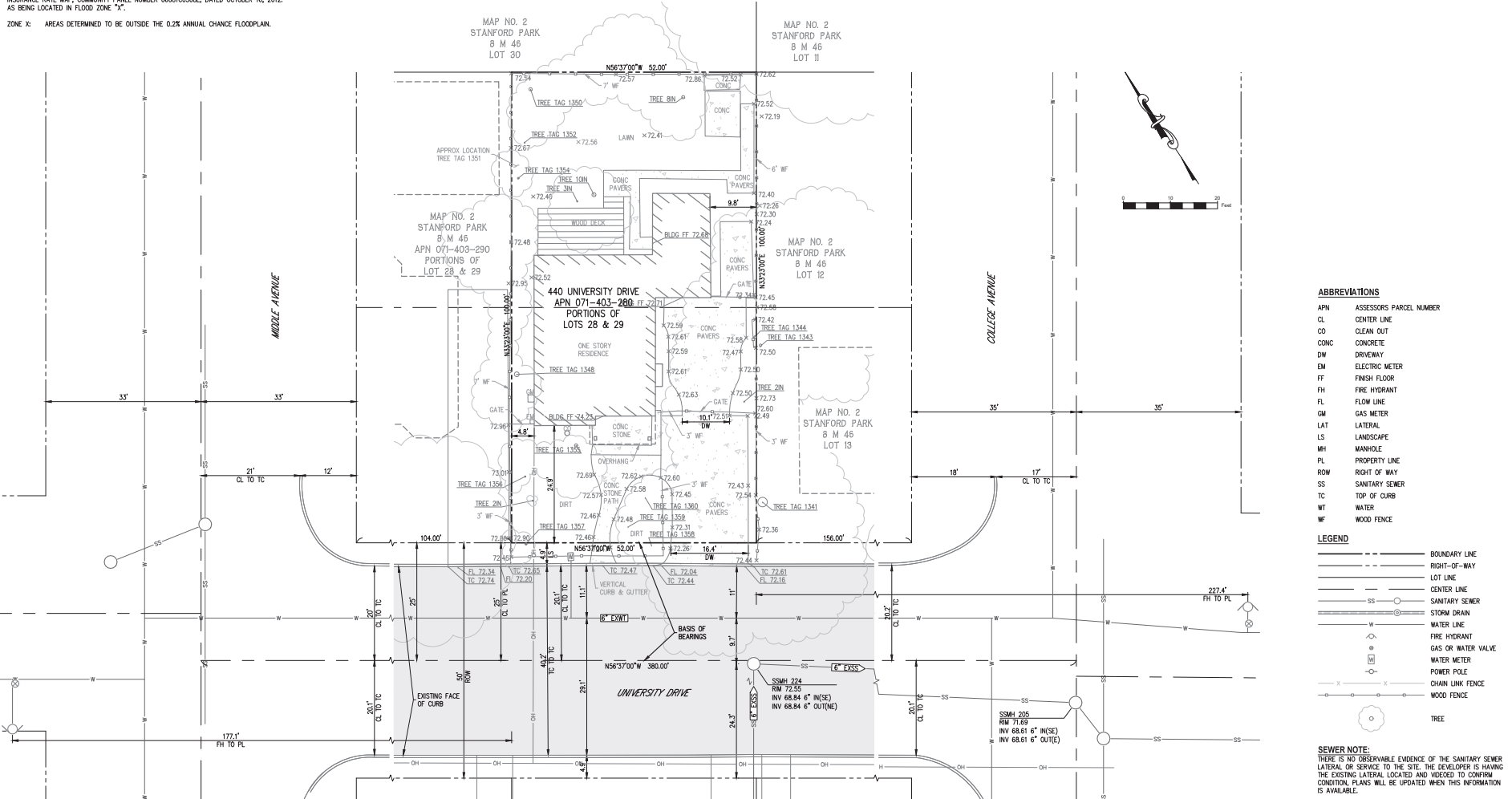
NAME: ANDREW TURNER
COMPANY: CIVIL ENGINEERING ASSOCIATES, INC.
LS No. 9107



11/11/2022
DATE



VICINITY MAP
NOT TO SCALE



ABBREVIATIONS

- APN ASSESSORS PARCEL NUMBER
- CL CENTER LINE
- CO CLEAN OUT
- CONC CONCRETE
- DW DRIVEWAY
- EM ELECTRIC METER
- FF FINISH FLOOR
- FH FIRE HYDRANT
- FL FLOW LINE
- GM GAS METER
- LAT LATERAL
- LS LANDSCAPE
- MH MANHOLE
- PL PROPERTY LINE
- ROW RIGHT OF WAY
- SS SANITARY SEWER
- TC TOP OF CURB
- WF WATER
- WF WOOD FENCE

LEGEND

- BOUNDARY LINE
- - - RIGHT-OF-WAY
- LOT LINE
- CENTER LINE
- SS SANITARY SEWER
- STORM DRAIN
- W WATER LINE
- FIRE HYDRANT
- GAS OR WATER VALVE
- WATER METER
- POWER POLE
- CHAIN LINK FENCE
- WOOD FENCE
- TREE

SEWER NOTE:
THERE IS NO OBSERVABLE EVIDENCE OF THE SANITARY SEWER LATERAL OR SERVICE TO THE SITE. THE DEVELOPER IS HAVING THE EXISTING LATERAL LOCATED AND VIDEOED TO CONFIRM CONDITION. PLANS WILL BE UPDATED WHEN THIS INFORMATION IS AVAILABLE.

DATE: 06/22/2022
SCALE: SEE PLAN
DESIGNED: N/A
DRAWN: RS
JOB NO.: 22-158
SHEET: T01
1 OF 1 SHEETS

PREPARED FOR:
THOMAS JAMES HOMES
255 SHORELINE DRIVE, SUITE 428
REDWOOD CITY, CA 94065
877-381-4092

CIVIL ENGINEERING ASSOCIATES
Civil Engineers • Planners • Surveyors

CIVIL IMPROVEMENT PLANS
BOUNDARY AND TOPOGRAPHIC SURVEY
TOPO SURVEY
MENLO PARK, CALIFORNIA

LAYOUT LEGEND

ADJ.	ADJACENT	NATIVE GRASS	NATIVE GRASS
EQ	EQUAL DISTANT	OH	OVERHANG
BOC	BACK OF CURB	PA	PLANTING AREA
CL	CONSTRUCTION/ COLD JOINT	PL	PROPERTY LINE
CLR	CENTERLINE	PMB	POINT OF BEGINNING
EJ	EXPANSION JOINT	SIM	SIMILAR TO
EQ	EQUAL DISTANT	SYM	SYMMETRICAL
ILO	IN LIEU OF	TYP	TYPICAL
MAX	MAXIMUM	TURF	TURF AREA
MN	MINIMUM	UNO	UNLESS NOTED OTHERWISE
		VIF	VERIFY IN FIELD

PAVING AND FENCING LEGEND

A	B	CONCRETE PAVERS PER DETAIL A/L1.2: STANDARD GRAY CONCRETE WITH ACID ETCH FINISH WITH TOP CAST #01 SURFACE RETARDANT MANUFACTURED BY GRACE PRODUCTS. PAVERS TO BE SPACED EQUALLY AT FRONT YARD - FILL GAP WITH P2 CRUSHED AGGREGATE AND AT BACKYARD - FILL GAP WITH PLANTING. SEE SHEET L3.1.
C	D	CONCRETE DRIVEWAY PER DETAIL D/L1.2: STANDARD GRAY CONCRETE WITH ACID ETCH FINISH WITH TOP CAST #01 SURFACE RETARDANT MANUFACTURED BY GRACE PRODUCTS.
P2		DECORATIVE GRAVEL PER DETAIL D/L1.2: LYNXSO 3/4" CRUSHED AGGREGATE "YOSEMITE TAN", 2" OVER COMPACTED SUBGRADE OVER FILTER FABRIC, WITH 8" GALVANIZED WIRE STAPLES.
P3		CONCRETE PAVING (PEDESTRIAN) PER DETAIL B/L1.2: STANDARD GRAY CONCRETE WITH ACID ETCH FINISH WITH TOP CAST #01 SURFACE RETARDANT MANUFACTURED BY GRACE PRODUCTS.
P4		CONCRETE PAVING (VEHICULAR) PER DETAIL C/L1.2: STANDARD GRAY CONCRETE WITH ACID ETCH FINISH WITH TOP CAST #01 SURFACE RETARDANT MANUFACTURED BY GRACE PRODUCTS. TOOLED SCORE JOINTS AS SHOWN ON PLANS.
P5		CONCRETE TO BE POURED WITH ARCHITECTURE. REFER TO STRUCTURAL DRAWINGS.
		SIDEYARD FENCE: PER DETAIL F/L1.2: 205 LF (CONTRACTOR TO VERIFY, INCLUDES ONE 4'-0" GATE).
		36" FENCE SIMILAR TO SIDEYARD FENCE: PER DETAIL F/L1.2: 60 LF (CONTRACTOR TO VERIFY).
		STEEL HEADER, TYP. REFER TO PLAN FOR EXACT LOCATIONS AND CONDITIONS.
1		EXISTING TREES TO REMAIN/ EXISTING TREES TO REMOVE. SEE TREE PROTECTION PLAN & NOTES ON SHEET L3.3.

SITE CALCULATIONS (PERFORMANCE APPROACH)

440 UNIVERSITY DRIVE	SF	% OF LOT AREA
EXISTING		
GROSS LOT SF AREA	5,200 SF	100%
TOTAL PERMEABLE AREA	2,294 SF	44%
TOTAL LANDSCAPE AREA (% OF TOTAL AREA)	2,093 SF	100%
SHRUB AREA (% OF TOTAL LANDSCAPE AREA)	1,769 SF	85%
TURF AREA (% OF TOTAL LANDSCAPE AREA)	324 SF	15%
AGGREGATE PATHS AND PATIOS	201 SF	
TOTAL IMPERMEABLE AREA	2,954 SF	57%
RESIDENCE/ GARAGE FOOTPRINT	1,752 SF	34%
CONCRETE DRIVEWAY	400 SF	
COVERED CONCRETE REAR PATIO	210 SF	
UNCOVERED CONCRETE PATIO	121 SF	
FRONT PORCH	48 SF	
CONCRETE PAVERS	205 SF	
TRASH AREA PAD	218 SF	
20' FRONT YARD SETBACK	1,040 SF	
PERMEABLE	628 SF	60%
IMPERMEABLE	412 SF	40%

NOTE: WATER SUPPLY IS DOMESTIC.

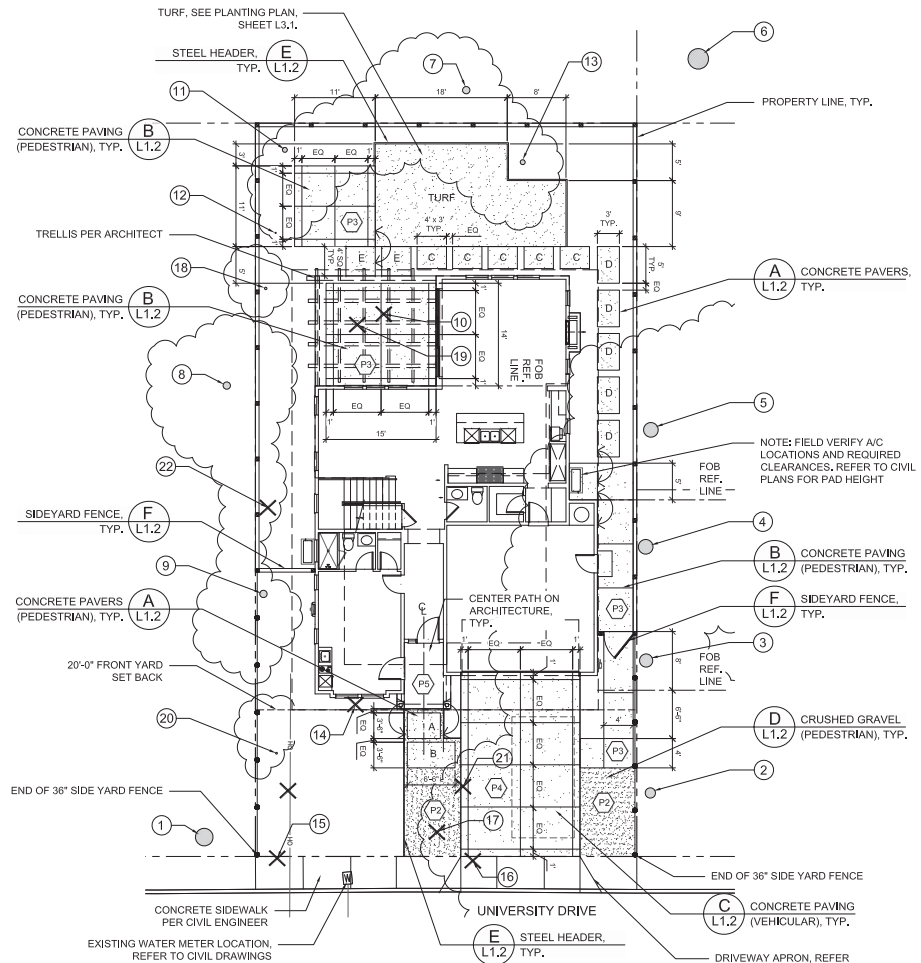
I have complied with the criteria of the Water Efficient Landscape Ordinance and applied them for the efficient use of water in the Landscape Design Plan

[Signature] 11/16/22
SIGNATURE DATE

TREE PROTECTION CHART

NOTE: SEE ARBORIST REPORT FOR ADDITIONAL INFORMATION AND TREE PROTECTION PLAN ON SHEET L3.3

TREE #	TAG #	STATUS	LOCATION	SCIENTIFIC NAME	COMMON NAME	DBH (In)	ACTION
1	1340	HERITAGE	OFF-SITE	MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA	30	RETAIN
2	1341	HERITAGE	OFF-SITE	LIQUIDAMBAR	SWEET GUM	15	RETAIN
3	1342	HERITAGE	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	20	RETAIN
4	1343	HERITAGE	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	20	RETAIN
5	1344	HERITAGE	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	25	RETAIN
6	1345	HERITAGE	OFF-SITE	CEDRUS DEODARA	DEODAR CEDAR	35	RETAIN
7	1346	NON-PROTECTED	OFF-SITE	FICUS CARICA	FIG	10	RETAIN
8	1347	NON-PROTECTED	OFF-SITE	DIOSPYROS VIRGIN	PERSIMMON	8	RETAIN
9	1348	NON-PROTECTED	ON-SITE	PITTSOPORUM TOBIRA	SWEET TOBIRA	10	RETAIN
10	1349	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	9	REMOVE
11	1350	NON-PROTECTED	ON-SITE	CINNAMOMUM CAMPHORA	CAMPHOR	8	RETAIN
12	1352	NON-PROTECTED	ON-SITE	LYCIANTHES RANTONNETII	POTATO BUSH	6	RETAIN
13	1354	NON-PROTECTED	ON-SITE	CITRUS LIMON	LEMON	9	RETAIN
14	1355	NON-PROTECTED	ON-SITE	ARBUTUS UNEDO	STRAWBERRY	9	REMOVE
15	1357	NON-PROTECTED	ON-SITE	PRUNUS CERASIFERA	PURPLE LEAF PLUM	5	REMOVE
16	1358	NON-PROTECTED	ON-SITE	PRUNUS CERASIFERA	PURPLE LEAF PLUM	4	REMOVE
17	1359	NON-PROTECTED	ON-SITE	LAGERSTROEMIA INDICA	GRAPE MYRTLE	4	REMOVE
18	1351	NON-PROTECTED	ON-SITE	CITRUS LIMON	LEMON	3	RETAIN
19	1353	NON-PROTECTED	ON-SITE	CAMELLIA SP.	CAMELLIA	3	REMOVE
20	1356	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	3	RETAIN
21	1360	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	3	REMOVE
22	N/A	NON-PROTECTED	ON-SITE	STUMP	STUMP	10	REMOVE



CONSTRUCTION NOTES

- LOCAL CODES AND ORDINANCES: WORK SHALL CONFORM TO ALL LOCAL CODES, ORDINANCES, AND REQUIREMENTS, INCLUDING FEDERAL ACCESSIBILITY GUIDELINES. NOTHING IN THE CONTRACT DOCUMENTS SHALL BE CONSTRUED AS AN EXEMPTION TO APPLICABLE CODES OR OTHER JURISDICTIONAL REQUIREMENTS.
- UTILITIES: CONTACT COMMON GROUND ALLIANCE (C.G.A.) AT 811, AT LEAST TWO WORKING DAYS IN ADVANCE OF WORK (PER CA GOV. CODE 4216). THE CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES, WHETHER SHOWN OR NOT, AND SHALL PAY FOR ANY REPAIRS REQUIRED DUE TO THE CONTRACTOR'S OPERATIONS AT NO ADDITIONAL EXPENSE TO THE OWNER.
- DISCREPANCIES: NOTIFY DISTRICT'S REPRESENTATIVE OF ANY VARIATIONS BETWEEN THE CONTRACT DOCUMENTS AND FIELD CONDITIONS. DO NOT PROCEED WHERE DIFFERENCES EXIST THAT WOULD AFFECT THE WORK. ALL ADJUSTMENTS DUE TO FIELD CONDITIONS MUST BE APPROVED BY THE DISTRICT'S REPRESENTATIVE PRIOR TO CONTINUING.
- LAYOUT NOTES: THE WRITTEN DIMENSION SUPERCEDES SCALED OR GRAPHIC DENOTATION. DIMENSIONS ARE BETWEEN PARALLEL OR PERPENDICULAR POINTS UNLESS NOTED OTHERWISE. DIMENSIONS ARE TO CENTERLINE OR FACE OF MASONRY, CONCRETE, OR FRAMING SUBSTRATE FINISH SURFACES, UNLESS NOTED OTHERWISE.
- COORDINATION: CONTRACTOR SHALL COORDINATE WORK BETWEEN TRADES. ALL REQUIRED SLEEVING SHALL BE COORDINATED WITH SITE WORK, INCLUDING OTHER UNDERGROUND UTILITIES, CURBS, AND CONCRETE.
- VERTICAL WORK: ALL VERTICAL CONSTRUCTION SHALL BE INSTALLED TRUE AND PLUMB. ALL UNIT COURSING AND TOPS OF WALLS, FENCES, ETC. SHALL BE LEVEL UNLESS NOTED OTHERWISE. ALL CURVES SHALL BE CONTINUOUS AND EVEN, WITH NO BREAKS OR ANGLES AT POINTS OF TANGENCY OR FORMWORK JOINTING.
- LEAD TIME: SPECIFIED MATERIALS MAY REQUIRE A SIGNIFICANT LEAD TIME. CONTRACTOR IS SOLELY RESPONSIBLE TO LEAD TIMES AND TO PROVIDE SUBMITTALS, AND ORDER MATERIAL, AND ENSURE DELIVERY TO THE JOB SITE TO ALLOW TIMELY PROGRESSION OF WORK.
- EXISTING WORK: WHERE NEW CONSTRUCTION ADJUTS EXISTING WORK, ALL EXISTING WORK SHALL BE PROTECTED. CONTRACTOR SHALL REPLACE ANY DAMAGED EXISTING WORK AT NO ADDITIONAL EXPENSE TO THE OWNER. ALL NEW WORK WILL CONFORM TO EXISTING WORK, INCLUDING FLATWORK JOINTS, ELEVATIONS, COLOR, AND FINISH.
- FENCING: FENCE LOCATIONS SHOWN ARE DIAGRAMMATIC AND FINAL LOCATIONS ARE TO BE COORDINATED IN THE FIELD BY THE LANDSCAPE CONTRACTOR.
- ADDITIONAL NOTES: SEE SHEET L1.2 FOR CONSTRUCTION DETAILS.



CLIENT:
THOMAS JAMES HOMES
1800 TREAT BLVD.
WALNUT CREEK
CALIFORNIA
(925) 849-1824



VAN DORN ABED
LANDSCAPE ARCHITECTS, INC.
3015 UNIVERSITY DRIVE, SUITE 200
BERKELEY, CA 94704
TEL: (415) 841-1111
WWW.VANDORNABED.COM

PROJECT NAME/LOCATION:
440 UNIVERSITY DRIVE
MERCLO PARK
Berkeley, CA

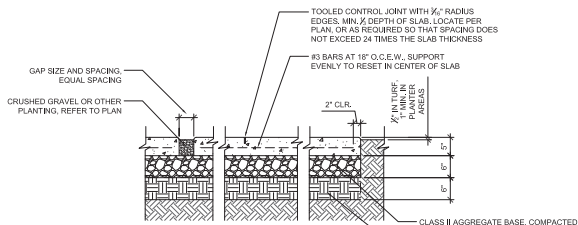
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SCALE: 1/8" = 1'-0"

ISSUE DATE: 11/16/22

PROJECT NO: Y2212

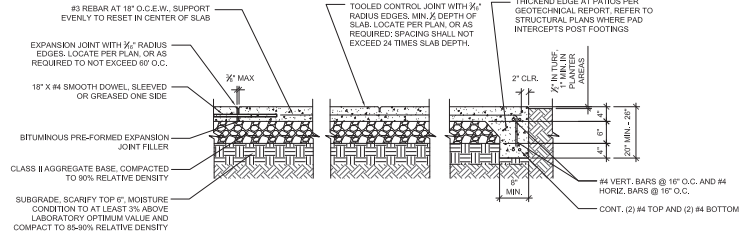
SHEET NO: L1.1



NOTES:

1. CONCRETE PAVERS - STANDARD GRAY CONCRETE WITH ACID ETCH FINISH WITH TOP CAST #01 SURFACE RETARDANT MANUFACTURED BY GRACE PRODUCTS. PAVERS TO BE SPACED EQUALLY AT FRONT YARD - FILL GAP WITH (CRUSHED AGGREGATE) AND AT BACKYARD - FILL GAP WITH PLANTING, SEE SHEET L3.1.
2. PAVEMENT DEPTHS AND REQUIREMENTS HEREIN ARE SUPERCEDED BY THE PROJECT GEOTECHNICAL REPORT AND RECOMMENDATIONS

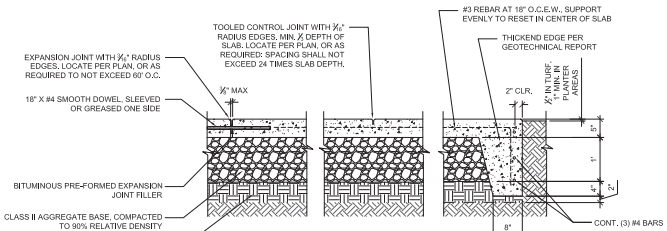
A CONCRETE PAVERS
3/4"-1'-0"



NOTES:

1. CONCRETE PAVING (PEDESTRIAN) - STANDARD GRAY CONCRETE WITH ACID ETCH FINISH WITH TOP CAST #01 SURFACE RETARDANT MANUFACTURED BY GRACE PRODUCTS
2. PAVEMENT DEPTHS AND REQUIREMENTS HEREIN ARE SUPERCEDED BY THE PROJECT GEOTECHNICAL REPORT AND RECOMMENDATIONS

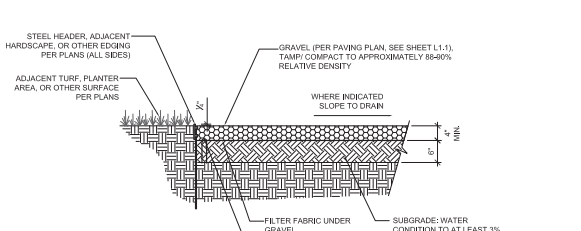
B CONCRETE PAVING (PEDESTRIAN)
3/4"-1'-0"



NOTES:

1. CONCRETE PAVING (VEHICULAR) - STANDARD GRAY CONCRETE WITH ACID ETCH FINISH WITH TOP CAST #01 SURFACE RETARDANT MANUFACTURED BY GRACE PRODUCTS. TOOLED SCORE JOINTS AS SHOWN ON PLANS
2. PAVEMENT DEPTHS AND REQUIREMENTS HEREIN ARE SUPERCEDED BY THE PROJECT GEOTECHNICAL REPORT AND RECOMMENDATIONS

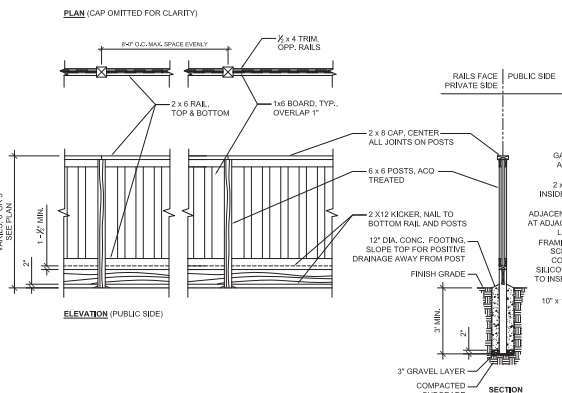
C CONCRETE PAVING (VEHICULAR)
3/4"-1'-0"



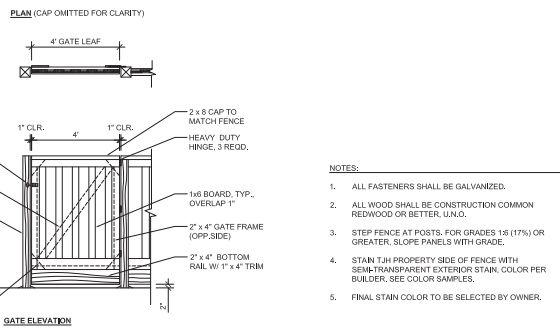
NOTES:

1. DECORATIVE GRAVEL - LYNOSO 3/4" CRUSHED AGGREGATE "YOSEMITE TAN", 2" OVER COMPACTED SUBGRADE OVER FILTER FABRIC, WITH 6" GALVANIZED WIRE STAPLES.
2. WITHIN EXISTING TREE CANOPY, OR IN AREAS OF UNDISTURBED SUBGRADING, NO COMPACTION NEEDED.

D CRUSHED GRAVEL (PEDESTRIAN)
3/4"-1'-0"

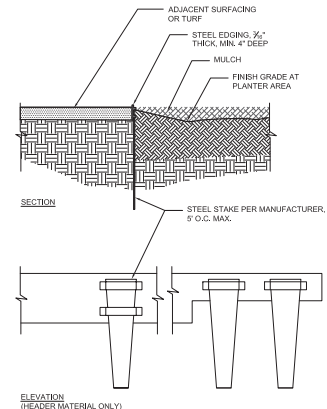


F SIDYARD FENCE WITH GATE
3/8"-1'-0"



NOTES:

1. ALL FASTENERS SHALL BE GALVANIZED.
2. ALL WOOD SHALL BE CONSTRUCTION COMMON REDWOOD OR BETTER, U.N.O.
3. STEP FENCE AT POSTS. FOR GRADES 1% (17%) OR GREATER, SLOPE PANELS WITH GRADE.
4. STAIN T/H PROPERTY SIDE OF FENCE WITH SEMI-TRANSPARENT EXTERIOR STAIN, COLOR PER BUILDER, SEE COLOR SAMPLES.
5. FINAL STAIN COLOR TO BE SELECTED BY OWNER.



NOTE:

1. BLACK ENAMEL PAINTED FINISH, TYP. U.N.O.

E STEEL HEADER
3/4"-1'-0"

CLIENT:
THOMAS JAMES HOMES
1800 TREAT BLVD,
WALNUT CREEK
CALIFORNIA
(925) 844-1824



VAN DORN ABED
LANDSCAPE ARCHITECTS, INC.
3015 UNIVERSITY DRIVE, SUITE 200
BERKELEY, CA 94704
TEL: (415) 841-1111
WWW.VANDORNABED.COM

PROJECT MANAGER:
JIN LI
DESIGNED BY:
JIN LI

PROJECT NAME/LOCATION:
440 UNIVERSITY DRIVE
MERLO PARK
DRAINAGE FILE

SCALE:
CONSTRUCTION DRAWINGS

NO.	DESCRIPTION	BY	DATE

SCALE:
AS NOTED
ISSUE DATE:
11/16/22
PROJECT NO:
Y2212

SHEET NO.:
L1.2

CITY OF MENLO PARK WATER EFFICIENT LANDSCAPE ORDINANCE (WELO) COMPLIANCE:

THIS LANDSCAPE PLANTING AND IRRIGATION DESIGN IS UTILIZING:

PRESCRIPTIVE OPTION A:

- FOR RESIDENTIAL PROJECTS IMPACTING 22,500 SF OF LANDSCAPE
- TOTAL TURF MUST BE 25% OR LESS OF TOTAL DISTURBED LANDSCAPE AREA
- REMAINING 75% OF PROPOSED PLANTING MUST BE OF LOW WATER SPECIES (AVERAGE WUCOLS' 0,3)

SEE CHART BELOW FOR THE LANDSCAPE AREA CHART.

LANDSCAPE AREA CHART:

LOW (88.5%) & MEDIUM (11.5%) WATER USE TREE, SHRUB, GROUND COVER & VINES:	1,749 SF
HIGH WATER USE TURF:	324 SF
TOTAL LANDSCAPE AREA:	2,073 SF
PERCENT TURF: 324 / 2073:	15.6% TURF

TOTAL LANDSCAPE AREA IS LESS THAN 2,500 SF & TURF AREA IS LESS THAN 25% OF TOTAL LANDSCAPE AREA. LANDSCAPE COMPLIES WITH PRESCRIPTIVE OPTION A.

LANDSCAPE DESIGN INTENT STATEMENT:

THE PLANTING AND IRRIGATION HAS BEEN DESIGNED FOR MAXIMUM EFFICIENCY AND WATER CONSERVATION:

- SMART E.T. WEATHER BASED IRRIGATION CONTROLLER WITH AUTOMATIC WATER SCHEDULE ADJUSTMENTS DAILY BASED UPON LOCAL SITE CLIMATIC CONDITIONS.
- RAIN SHUTOFF DEVICE.
- LOW VOLUME DRIP EMITTERS AT TREE, SHRUB AND GROUND COVER PLANTING AREAS.
- ALL TREE, SHRUB AND GROUND COVER AREAS PLANTING AREAS UTILIZE 75% OR MORE LOW WATER USE PLANT MATERIALS FOR PRESCRIPTIVE OPTION A WELO COMPLIANCE.
- SEPARATE HYDROZONE VALVE CIRCUITS FOR SUN AND PART SHADE AREAS.
- HIGH WATER USE TURF AREA IS LESS THAN 25% OF THE LANDSCAPE AREA.
- THE LANDSCAPE PLANTING AND IRRIGATION SYSTEM COMPLIES WITH THE CITY'S WATER EFFICIENT LANDSCAPE ORDINANCE (WELO).

CITY OF MENLO PARK WATER EFFICIENT LANDSCAPE ORDINANCE (WELO) CHECKLIST:

City of Menlo Park - Water Efficient Landscape Ordinance (WELO) Landscape Application Checklist		Project No. 2212	
Project Information	Project Name: 440 UNIVERSITY DRIVE	Project No.:	2212
Project Location	Address: 440 UNIVERSITY DRIVE	City:	MENLO PARK
Project Description	Project Type: LANDSCAPE	Project Status:	IN PROGRESS
Project Owner	Client Name: THOMAS JAMES HOMES	Client Phone:	650-249-1234
Project Designer	Designer Name: VAN DORN ABED	Designer Title:	LANDSCAPE ARCHITECT
Project Date	Issue Date: 11/16/22	Project Start:	11/16/22
Project Status	Project Phase: PRELIMINARY	Project End:	11/16/22
Project Notes	Project Description: LANDSCAPE	Project Location:	440 UNIVERSITY DRIVE
Project Details	Project Area: 2,073 SF	Project Budget:	15.6% TURF
Project Compliance	Project Code: WELO	Project Status:	COMPLIANT
Project Checklist	Project Item: PRESCRIPTIVE OPTION A	Project Status:	COMPLIANT
Project Checklist	Project Item: WELO COMPLIANCE	Project Status:	COMPLIANT
Project Checklist	Project Item: LANDSCAPE AREA CHART	Project Status:	COMPLIANT
Project Checklist	Project Item: LANDSCAPE DESIGN INTENT STATEMENT	Project Status:	COMPLIANT
Project Checklist	Project Item: IRRIGATION SYSTEM	Project Status:	COMPLIANT
Project Checklist	Project Item: MATERIALS LIST	Project Status:	COMPLIANT
Project Checklist	Project Item: IRRIGATION CONTROLLER	Project Status:	COMPLIANT
Project Checklist	Project Item: GENERAL NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: WATER PRESSURE AT P.O.C. NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: SLEEVE & CONDUIT NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: ATMOSPHERIC VACUUM BREAKER REMOTE CONTROL VALVE NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: IRRIGATION CONTROLLER NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: SPECIAL REQUIREMENTS AT EXISTING TREE NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: IRRIGATION CONTROLLER NOTES	Project Status:	COMPLIANT

City of Menlo Park - Water Efficient Landscape Ordinance (WELO) Checklist		Project No. 2212	
Project Information	Project Name: 440 UNIVERSITY DRIVE	Project No.:	2212
Project Location	Address: 440 UNIVERSITY DRIVE	City:	MENLO PARK
Project Description	Project Type: LANDSCAPE	Project Status:	IN PROGRESS
Project Owner	Client Name: THOMAS JAMES HOMES	Client Phone:	650-249-1234
Project Designer	Designer Name: VAN DORN ABED	Designer Title:	LANDSCAPE ARCHITECT
Project Date	Issue Date: 11/16/22	Project Start:	11/16/22
Project Status	Project Phase: PRELIMINARY	Project End:	11/16/22
Project Notes	Project Description: LANDSCAPE	Project Location:	440 UNIVERSITY DRIVE
Project Details	Project Area: 2,073 SF	Project Budget:	15.6% TURF
Project Compliance	Project Code: WELO	Project Status:	COMPLIANT
Project Checklist	Project Item: PRESCRIPTIVE OPTION A	Project Status:	COMPLIANT
Project Checklist	Project Item: WELO COMPLIANCE	Project Status:	COMPLIANT
Project Checklist	Project Item: LANDSCAPE AREA CHART	Project Status:	COMPLIANT
Project Checklist	Project Item: LANDSCAPE DESIGN INTENT STATEMENT	Project Status:	COMPLIANT
Project Checklist	Project Item: IRRIGATION SYSTEM	Project Status:	COMPLIANT
Project Checklist	Project Item: MATERIALS LIST	Project Status:	COMPLIANT
Project Checklist	Project Item: IRRIGATION CONTROLLER	Project Status:	COMPLIANT
Project Checklist	Project Item: GENERAL NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: WATER PRESSURE AT P.O.C. NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: SLEEVE & CONDUIT NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: ATMOSPHERIC VACUUM BREAKER REMOTE CONTROL VALVE NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: IRRIGATION CONTROLLER NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: SPECIAL REQUIREMENTS AT EXISTING TREE NOTES	Project Status:	COMPLIANT
Project Checklist	Project Item: IRRIGATION CONTROLLER NOTES	Project Status:	COMPLIANT

GENERAL NOTES:

- THIS DESIGN IS DIAGRAMMATIC. ALL PIPING, VALVES, ETC., SHOWN WITHIN PAVED AREAS ARE FOR DESIGN CLARIFICATION ONLY AND SHALL BE INSTALLED IN PLANTING AREAS WHERE POSSIBLE, UNLESS OTHERWISE NOTED. AVOID ANY CONFLICTS BETWEEN THE IRRIGATION SYSTEM, PLANTING AND ARCHITECTURAL FEATURES. LOCATE TURF AREA REMOTE CONTROL VALVE(S) IN SHRUB PLANTING AREAS - DO NOT LOCATE IN TURF AREAS OR BIOSWALE/BIORETENTION AREAS.
- CONTRACTOR SHALL VERIFY P.O.C. METER SIZE AND PRESSURE ON-SITE PRIOR TO BEGINNING WORK. SEE IRRIGATION NOTES FOR TEST REQUIREMENTS. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY CORRECTIVE MEASURES REQUIRED TO IRRIGATION SYSTEM AT NO ADDITIONAL COST TO THE OWNER. IF IRRIGATION SYSTEM IS INSTALLED WITHOUT REQUIRED TESTS, AND DISCREPANCIES IN PRESSURE AND P.O.C. METER SIZE ARE DISCOVERED THAT PREVENT THE IRRIGATION SYSTEM FROM FUNCTIONING CORRECTLY.

WATER PRESSURE AT P.O.C. NOTES:

- CONTRACTOR SHALL VERIFY WATER PRESSURE ON SITE. IF PRESSURE IS 65 PSI OR HIGHER AT P.O.C., CONTRACTOR SHALL INSTALL A PRESSURE REDUCER AT THE IRRIGATION SYSTEM POINT OF CONNECTION (P.O.C.), AND SET PRESSURE REDUCER TO 55 PSI. PRESSURE REDUCER SHALL BE WILKINS LEAD FREE 500XL-YSBR (INCLUDES PRESSURE REDUCER & FILTER), LINE SIZE. SEE IRRIGATION DETAILS.
- IF PRESSURE IS LESS THAN 65 PSI OMIT PRESSURE REDUCER.
- IF PRESSURE IS LESS THAN 50 PSI NOTIFY OWNER'S REPRESENTATIVE AND LANDSCAPE ARCHITECT FOR CORRECTIVE MEASURES.

SLEEVE & CONDUIT NOTES:

FOR DESIGN CLARITY DUO TO THE SCALE OF THE DRAWINGS, NOT ALL SLEEVES MAY BE SHOWN IN THE SMALLER AREAS OF THE SITE. FIELD CONDITIONS MAY ALSO VARY, AND MORE SLEEVES MAY BE REQUIRED THAN WHAT IS SHOWN ON THE DRAWINGS. CONTRACTOR SHALL VERIFY FIELD CONDITIONS. CONTRACTOR IS RESPONSIBLE FOR INSTALLING SLEEVES AT ALL PIPES AND CONTROL WIRES CROSSING UNDER PAVED AREAS.

- SLEEVE AND CONDUIT DEPTHS SHALL BE AS FOLLOWS: 24" MIN. BURY. UNDER PEDESTRIAN PAVING & 36" MIN. BURY UNDER VEHICULAR PAVING.
- WHERE LATERAL LINES WITH SLEEVES CROSS ROADS OR DRIVEWAYS, CONTRACTOR SHALL INSTALL ONE SPARE 4" CLASS SCH.40 PVC SLEEVE.
- WHERE MAIN LINES WITH SLEEVES CROSS ROADS OR DRIVEWAYS, CONTRACTOR SHALL INSTALL ONE SPARE 6" SCH.40 PVC SLEEVE.
- WHERE LOW VOLTAGE CONTROL WIRES CROSS UNDER PAVED AREAS, INSTALL IN SCH.40 CONDUIT. CONDUIT SIZE SHALL BE 1" OR LARGER SO WIRES CAN BE EASILY PULLED THROUGH CONDUIT.
- COORDINATE WITH OTHER TRADES FOR ALL SLEEVING, ETC.

ATMOSPHERIC VACUUM BREAKER REMOTE CONTROL VALVE NOTES:

- ATMOSPHERIC VACUUM BREAKER (AVB) REMOTE CONTROL VALVES MUST BE INSTALLED IN A LOCATION SO THAT THEY ARE 12" MINIMUM ABOVE THE HIGHEST ELEVATION SPRINKLER/EMITTER HEAD IN EACH YARDS IRRIGATION SYSTEM.
- CONTRACTOR SHALL FIELD VERIFY LOCATION OF HIGHEST SPRINKLER/EMITTER HEAD(S) AND INSTALL THE RCV'S AT A LOCATION WHERE THEY WILL BE 12" MINIMUM ABOVE THE HIGHEST ELEVATION SPRINKLER/EMITTER HEAD IN THE IRRIGATION SYSTEM. THIS INCLUDES LOCATING RCV'S AT THE TOP OF SLOPE AREAS ADJACENT TO FENCES, LOCATING RCV'S AT A HIGHER LOCATION/PAVED ELEVATIONS IN THE REAR YARDS. DO NOT LOCATE RCV'S IN THE MIDDLE OF OPEN AREAS - LOCATE THEM ADJACENT TO FENCES, WALLS, HOUSE, ETC. DO NOT LOCATE RCV MORE THAN 24" ABOVE FINISH GRADE.
- THE RCV LOCATIONS INDICATED ON THE IRRIGATION PLANS ARE DIAGRAMMATIC/APPROXIMATE ONLY. CONTRACTOR SHALL FIELD VERIFY CORRECT INSTALLATION LOCATIONS AS NOTED ABOVE.
- RVC'S THAT ARE NOT INSTALLED 12" ABOVE THE HIGHEST ELEVATION SPRINKLER/EMITTER HEAD IN THE IRRIGATION SYSTEM WILL NOT BE ACCEPTED. SEE IRRIGATION DETAILS.

SPECIAL REQUIREMENTS AT EXISTING TREE NOTES:

- IF EXISTING TREES ARE PRESENT, ALL UNDERGROUND IRRIGATION LINES SHALL BE ROUTED OUTSIDE THE DRIP LINES WHERE POSSIBLE.
- IF UNDERGROUND IRRIGATION LINES MUST TRAVERSE THROUGH THE DRIP LINE AREA, LOCATION OF IRRIGATION LINES SHALL BE REVIEWED WITH PROJECT ARBORIST AND MODIFIED AS NEEDED PRIOR TO INSTALLATION, WHEN LINES ARE PROPOSED WITHIN A DISTANCE FROM THE TRUNKS OF FIVE (5) TIMES THEIR DIAMETER, THE PROJECT ARBORIST MAY RECOMMEND THAT A PNEUMATIC AIR DEVICE IS USED TO EXCAVATE THE TRENCH.

IRRIGATION CONTROLLER NOTES:

- CONTRACTOR SHALL CREATE THE BASELINE PROGRAM, AND CREATE A SEPARATE PROGRAM FOR THE PLANT ESTABLISHMENT PERIOD.
- IRRIGATION CONTROLLER IS AN ET WEATHER BASED SMART CONTROLLER THAT UTILIZES BASELINE PROGRAM AND ADJUSTS THE RUN TIME SCHEDULE DAILY BASED UPON LOCAL WEATHER CONDITIONS, FOR MAXIMUM WATER EFFICIENCY.
- CONTRACTOR SHALL SETUP AND PROGRAM HUNTER HYDRAWISE CONTROLLER ACCOUNT AND PROVIDE ACCESS INFORMATION TO OWNER'S REPRESENTATIVE.
- CONTRACTOR SHALL PROVIDED OWNER'S REPRESENTATIVE WITH WRITTEN VERIFICATION THE FOLLOWING HAS BEEN DONE:
 - IRRIGATION CONTROLLER IS COMMUNICATION WITH WIRED/WIRELESS RAIN/FREEZE/WEATHER SENSORS,
 - IRRIGATION CONTROLLER IS CONNECTED TO AND OPERATING ALL VALVES CORRECTLY.
 - IRRIGATION CONTROLLER IS COMMUNICATING WITH MASTER VALVE AND FLOW SENSOR AND CORRECT 'K' VALUES HAVE BEEN UTILIZED TO ENSURE ACCURATE WATER FLOW MEASUREMENTS.
 - NORMALLY CLOSED MASTER VALVE SHALL BE PROGRAMMED TO TURN ON WHEN CONTROLLER VALVES OPERATE.
 - IRRIGATION CONTROLLER'S FLOW MONITORING HAS BEEN ENABLED AND PROGRAMMED FOR LEAK DETECTION ALERT NOTIFICATION TO PROPERTY OWNER.

CLIENT: THOMAS JAMES HOMES, 1800 WILSON BLVD., WILSON, CALIFORNIA (650) 249-1234

811

LANDSCAPE ARCHITECT: VAN DORN ABED, 440 UNIVERSITY DRIVE, MENLO PARK, CA 94025

VAN DORN ABED LANDSCAPE ARCHITECTS, INC. 3000 UNIVERSITY AVENUE, SUITE 200, MENLO PARK, CA 94025

PROJECT NO.: 2212

DATE: 11/16/22

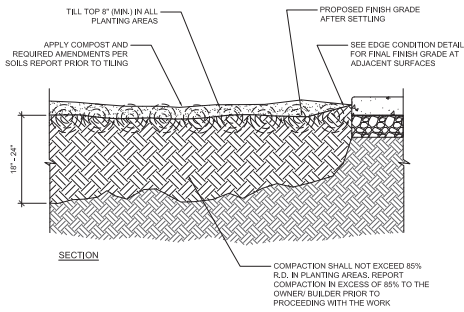
PROJECT LOCATION: 440 UNIVERSITY DRIVE, MENLO PARK, CA 94025

SCALE: N/A

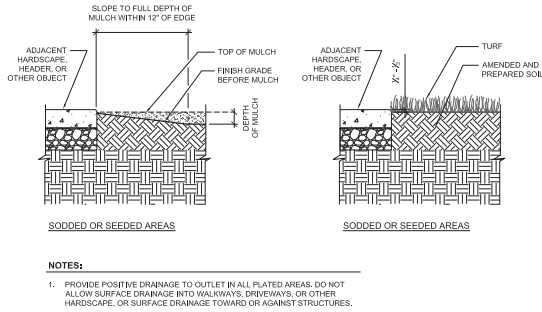
ISSUE DATE: 11/16/22

PROJECT NO.: Y2212

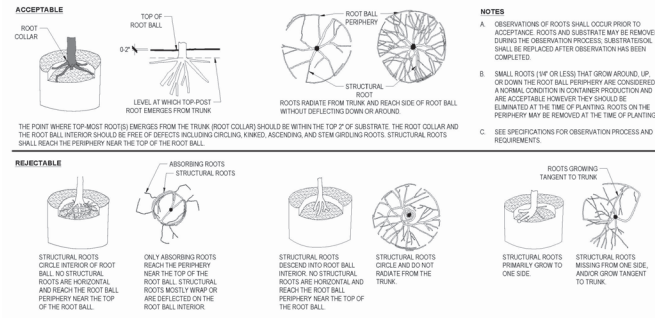
SHEET NO.: 12.1



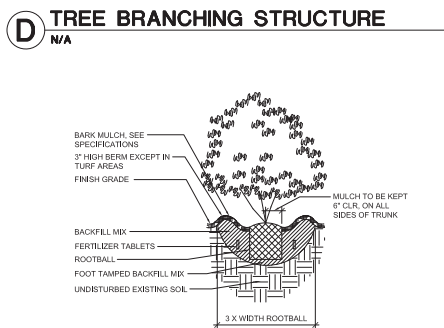
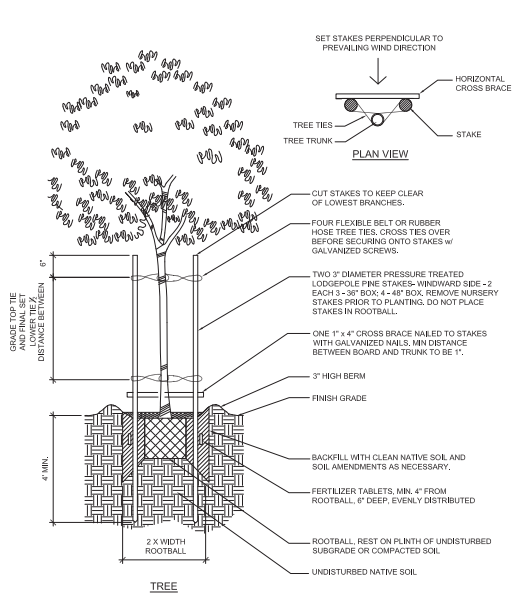
A PLANTING AREA SOIL PREPARATION
3/4"-1'-0"



B PLANTED AREA EDGE CONDITION AT HARDSCAPE
3/4"-1'-0"



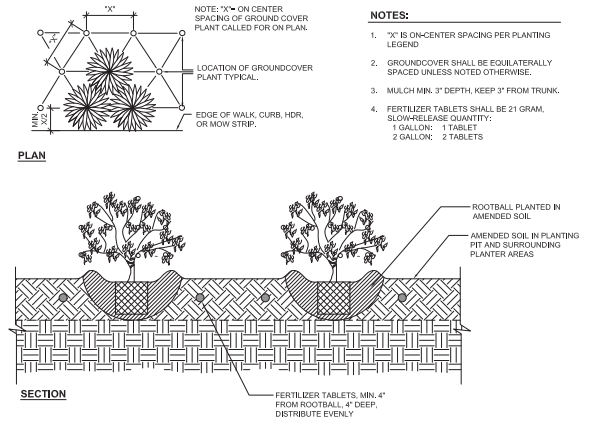
C ROOT STRUCTURE: CONTAINERIZED PLANTS
N/A



- NOTES:**
- MULCH MIN. 3" DEPTH, KEEP 6" FROM TRUNK.
 - PLANTING PIT DIAMETER MIN. 2X DIAMETER OF CONTAINER.
 - PLANT CROWN SHALL BE 1" ABOVE FINISH GRADE FOR SHRUBS AFTER WATERING AND SETTLING.
 - ONE 2x2 STAKE FOR 5 GAL. SHRUB STANDARDS AND TWO 2x2 STAKES FOR 15 GAL. SHRUB STANDARDS.
 - LOCATE STAKES AT OUTSIDE EDGE OF ROOTBALL.
 - SCARIFY SIDES OF PIT.
 - FERTILIZER TABLETS SHALL BE 21 GRAM, SLOW-RELEASE, QUANTITY AS FOLLOWS:
1 GALLON: 1 TABLET
2 GALLON: 2 TABLETS
5 GALLON: 3 TABLETS
15 GALLON: 6 TABLETS

E TREE PLANTING: STANDARD UP TO 36" BOX
3/4"-1'-0"

F SHRUB PLANTING
3/4"-1'-0"



G GROUNDCOVER PLANTING
3/4"-1'-0"

CLIENT: **THOMAS JAMES HOMES 1800 TREAT WALKIT CREEK CALIFORNIA (800) 840-1824**

811 CALL BEFORE YOU DIG

LICENSED LANDSCAPE ARCHITECT Lic. No. 3402 State of California

VAN DORN ABED LANDSCAPE ARCHITECTS, INC. 2500 UNIVERSITY DRIVE SUITE 100 SAN ANTONIO, TX 78205-5000

PROJECT MANAGER: **MINO PARRA** PROJECT ARCHITECT: **MINO PARRA**

PROJECT NAME/LOCATION: **440 UNIVERSITY DRIVE MESA PARK PHASE 11**

LANDSCAPE CONSTRUCTION DRAWINGS

NO.	DESCRIPTION	BY	DATE

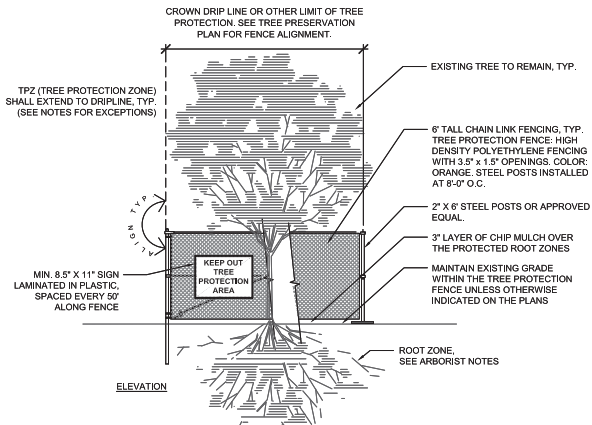
SHEET TITLE: **PLANTING DETAILS**

SCALE: **AS NOTED**

ISSUE DATE: **11/16/22**

PROJECT NO.: **Y2212**

SHEET NO.: **L3.2**



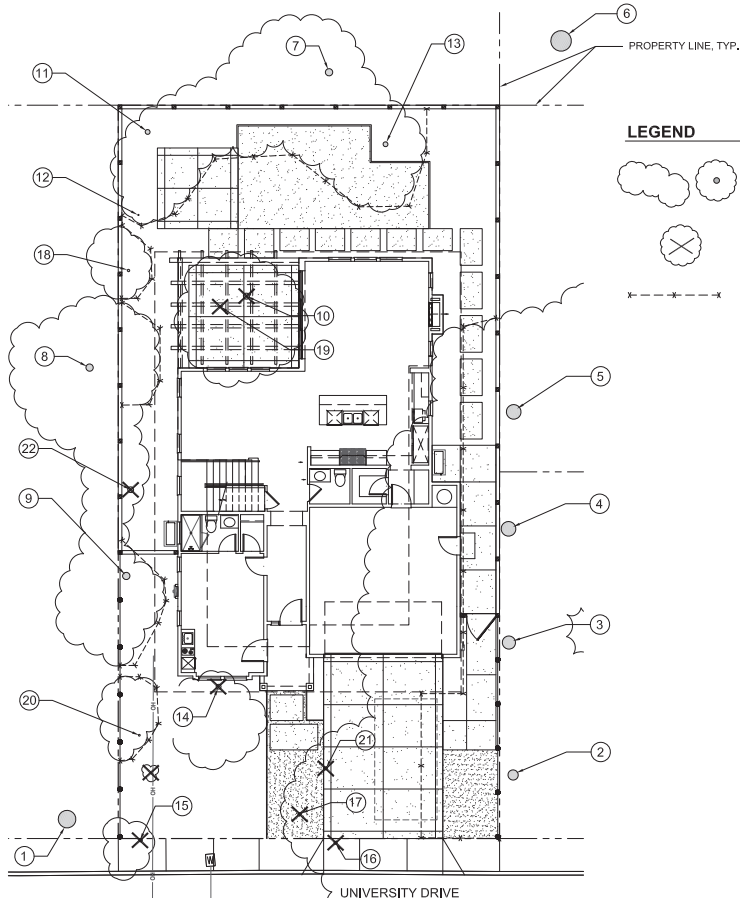
- NOTES:**
- SEE ARBORIST REPORT FOR ADDITIONAL PROTECTION REQUIREMENTS. COMPLY WITH ALL TREE PROTECTION REQUIREMENTS PER JURISDICTION.
 - IRRIGATE AS NEEDED TO MAINTAIN HEALTH OF TREE.
 - KEEP EXPOSED ROOTS MOIST.
 - NO PRUNING SHALL BE PERFORMED EXCEPT UNDER THE DIRECTION OF APPROVED ARBORIST.
 - NO EQUIPMENT SHALL OPERATE INSIDE THE PROTECTIVE FENCING INCLUDING DURING FENCE INSTALLATION AND REMOVAL.
 - NO MATERIALS SHALL BE STORED INSIDE FENCE.

A TREE PROTECTION FENCING
N/A

TREE PROTECTION CHART NOTE: SEE ARBORIST REPORT FOR ADDITIONAL INFORMATION

TREE #	TAG #	STATUS	LOCATION	SCIENTIFIC NAME	COMMON NAME	DBH (in)	ACTION
1	1340	HERITAGE	OFF-SITE	MAGNOLIA GRANDIFLORA	SOUTHERN MAGNOLIA	30	RETAIN
2	1341	HERITAGE	OFF-SITE	LIQUIDAMBAR	SWEET GUM	15	RETAIN
3	1342	HERITAGE	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	20	RETAIN
4	1343	HERITAGE	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	20	RETAIN
5	1344	HERITAGE	OFF-SITE	QUERCUS LOBATA	VALLEY OAK	25	RETAIN
6	1345	HERITAGE	OFF-SITE	CEDRUS DEODARA	DEODAR CEDAR	35	RETAIN
7	1346	NON-PROTECTED	OFF-SITE	FICUS CARICA	FIG	10	RETAIN
8	1347	NON-PROTECTED	OFF-SITE	DIOSPYROS VIRGIN	PERSIMMON	8	RETAIN
9	1348	NON-PROTECTED	ON-SITE	PITTOSPORUM TOBIRA	SWEET TOBIRA	10	RETAIN
10	1349	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	9	REMOVE
11	1350	NON-PROTECTED	ON-SITE	CINNAMOMUM CAMPHORA	CAMPHOR	8	RETAIN
12	1352	NON-PROTECTED	ON-SITE	LYCIANTHES RANTONNETII	POTATO BUSH	6	RETAIN
13	1354	NON-PROTECTED	ON-SITE	CITRUS LIMON	LEMON	9	RETAIN
14	1355	NON-PROTECTED	ON-SITE	ARBUTUS UNEDO	STRAWBERRY	9	REMOVE
15	1357	NON-PROTECTED	ON-SITE	PRUNUS CERASIFERA	PURPLE LEAF PLUM	5	REMOVE
16	1358	NON-PROTECTED	ON-SITE	PRUNUS CERASIFERA	PURPLE LEAF PLUM	4	REMOVE
17	1359	NON-PROTECTED	ON-SITE	LAGERSTROEMIA INDICA	CRAPE MYRTLE	4	REMOVE
18	1351	NON-PROTECTED	ON-SITE	CITRUS LIMON	LEMON	3	RETAIN
19	1353	NON-PROTECTED	ON-SITE	CAMELLIA SP.	CAMELLIA	3	REMOVE
20	1356	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	3	RETAIN
21	1360	NON-PROTECTED	ON-SITE	ACER PALMATUM	JAPANESE MAPLE	3	REMOVE
22	N/A	NON-PROTECTED	ON-SITE	STUMP	STUMP	10	REMOVE

- NOTES**
- REFER TO THE ARBORIST REPORT "PRELIMINARY ARBORIST REPORT AND TREE INVENTORY: 440 UNIVERSITY DRIVE, MENLO PARK, CALIFORNIA" PREPARED BY CALIFORNIA TREE AND LANDSCAPE CONSULTING, INC. DATED JUNE 22, 2022 FOR FULL DETAILS AND TREE PROTECTION MEASURES.
 - TREES AND SHRUBS NOT IDENTIFIED WITHIN THE REPORT, BUT AS PART OF THE TOPOGRAPHICAL SURVEY, ARE INCLUDED FOR REFERENCE ONLY.
 - PROTECT ALL EXISTING ITEMS NOTED TO REMAIN OR OTHERWISE UNLABELLED.
 - EXISTING TREES TO REMAIN UNLESS NOTED OTHERWISE. DO NOT STOCKPILE, DRIVE OVER, OR OTHERWISE DISTURB SOIL UNDER DRIP LINES OF EXISTING TREES, EXCEPT AS REQUIRED FOR PLANTING OPERATIONS.
 - USE HAND TOOLS ONLY FOR SOIL CULTIVATION UNDER DRIP LINES OF EXISTING TREES TO REMAIN.
 - TREES NOTED TO BE REMOVED SHALL BE COMPLETELY REMOVED, INCLUDING STUMP AND ROOT MASS. REFER TO ARBORIST REPORT FOR INSTRUCTIONS ON REMOVING TREE STUMPS WITHIN PROTECTED TREE ROOT ZONES.
 - NO ROOTS OVER 2" IN DIAMETER SHALL BE CUT EXCEPT UNDER THE DIRECTION OF AN ARBORIST. ALL CUT ROOTS SHALL BE COVERED WITH BURLAP OR STRAW AND SHALL REMAIN MOIST UNTIL RE-BURIED IN SOIL.
 - CALL COMMON GROUND ALLIANCE (811) AT LEAST TWO WORKING DAYS PRIOR TO BEGINNING WORK. CONTRACTOR IS RESPONSIBLE TO PROTECT FOR ALL EXISTING UTILITIES. SEE GENERAL NOTES, SHEET L1.1, FOR MORE INFORMATION.



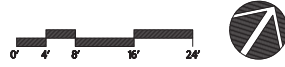
LEGEND

- EXISTING TREE CANOPY, TYP.
- TREES TO BE REMOVE, TYP.
- TREE PROTECTION FENCING; REFER TO ARBORIST REPORT

I have complied with the criteria of the Water Efficient Landscape Ordinance and applied them for the efficient use of water in the Landscape Design Plan

SIGNATURE

11/16/22
DATE



CLIENT:
THOMAS JAMES HOMES
1800 TREAT BLVD.
WALNUT CREEK
CALIFORNIA
(925) 849-1824



VAN DORN ABED
LANDSCAPE ARCHITECTS, INC.
2000 W. ELGIN AVENUE, SUITE 200
MENLO PARK, CA 94025
TEL: (650) 321-1111
WWW.VANDORNABED.COM

PROJECT MANAGER:
DESIGNED BY:
CHECKED BY:

PROJECT NAME/LOCATION:
440 UNIVERSITY DRIVE
MENLO PARK
DRAWING TITLE:
LANDSCAPE CONSTRUCTION DRAWINGS

NO.	DESCRIPTION	BY:	DATE

SHEET TITLE:
TREE PROTECTION PLAN & NOTES

SCALE:
1/8" = 1'-0"

ISSUE DATE:
11/16/22

PROJECT NO. Y2212

SHEET NO.:
L3.3



THOMAS JAMES HOMES
255 Shoreline Dr Suite 428,
Redwood City, CA 94065

440 University Drive Project Description

January 19, 2023

PARCEL GENERAL INFORMATION

The parcel located at 440 University Drive is a substandard lot, which is the reason a Use Permit is required for the proposed two-story residence. The R-1-U zoning ordinance requires a minimum of 7,000 sq ft in area, 65 ft in width and 100ft in depth. The depth complies with the zoning ordinance; however, the lot area of 5200sf and width of 52ft is less than the required.

There are 22 trees analyzed including 6 Heritage trees and 2 non protected trees offsite and 14 non protected trees onsite. 8 of the 14 non protected trees are proposed to be removed. 5 new trees are proposed onsite: 1-36" Box Chitalpa at the front; 1- 24" Box Crape Myrtle and 1- 24" Box Western Redbud at the rear; and 2-24"box Japanese Zelkovas at the left side. Tree protection will be provided for the trees to remain during construction through fencing as well as construction methods to save the trees from being impacted.

EXISTING HOME TO BE DEMOLISHED

The existing house is a one story single-family minimal traditional home built in 1940. The home is 1130sf including a 260sf garage.

PROPOSED SINGLE FAMILY RESIDENCE

The parcel's block typically consists of homes facing either College Avenue or Middle Avenue. The corner lots have been merged with their adjacent lots and then subdivided into 3 parcels that are reoriented to face the cross-street of University Drive. The parcels maintain a 50ft width yet are smaller in depth in comparison to the original lots. The homes on these parcels have L shaped footprints to create private rear outdoor spaces. In the immediate context along University Drive, the neighborhood consists of both 2-story and 1-story residences of varying styles including Traditional, Transitional, and Spanish. Main design features include horizontal siding, stucco, brick/stone accents, gable/shed roofs of either metal, comp shingle or S-tile materials, decorative shutters, and bay windows. Porches with columns define some of the entries creating inviting front elevations. Multiple 2-car garage configurations are found including recessed, side facing and front facing.

Continuing the 2-story evolution using the patterns and aesthetics found in the neighborhood, the new home proposed is a 2-story single family Traditional style residence featuring 3beds/3.5 baths and a 1bed/1bath attached ADU to appeal to families. A combination of horizontal siding and brick are used on the first floor with a smooth panel bay window treatment at the front living space. Square columns and a gable roof form distinguishes the front porch and offers a more traditional aesthetic. The second-floor massing steps back from the first story to reduce appearance from the street and provide relief along the right and left edges. A front gable provides interest and echoes the surrounding gable/shed roof forms in the neighborhood. The second-floor plate height is lower than typical 9ft and reduces massing. The window sills at the second floor have been raised to 3'-6" for



THOMAS JAMES HOMES
255 Shoreline Dr Suite 428,
Redwood City, CA 94065

privacy while still providing adequate light and ventilation to the living spaces. Additionally, two privacy screening trees have been proposed between existing tree canopies at the left stair window. A front facing 2-car garage and 2 off street parking spaces are provided. The garage is slightly setback from the main living space to be less prominent. The footprint of the home aligns with the adjacent homes as it uses a similar L shape form stepping back on the left side to create a usable private yard.

A light color palette is proposed to blend well with the neighborhood using an off white for the horizontal siding and a matching brick to provide a warm texture change. Minimalist windows with black window frames compliment the dark accent color for the garage, front door and shingle roof.

NEIGHBOR RELATIONS

Thomas James Homes has reached out to neighbors within 300-ft. of this property with a copy of the site plan, floor plan, elevations and a letter addressing our project. A virtual neighbor meeting was held on October 5, 2022 to collect feedback from the immediate neighbors. In addition, we have coordinated with two neighbors who attended the meeting and have reached out with concerns. Please see below for a summary of responses to neighbor concerns.

Neighbor at 444 University (left adjacent neighbor)

Concerns: 1) Requested more information on traffic management during construction and results on the asbestos report for demolition of the existing home. 2) Concerned if there was going to be a sidewalk installed at the property to continue the existing sidewalk.

Response: Thomas James Homes responded to the neighbor via email on 11/3/22 to follow up with requested information regarding traffic management and the asbestos report. We also confirmed the Engineering Department does indeed require a new sidewalk on the property and are proposing that improvement in our plans.

Neighbor at 883 Middle Avenue (Rear neighbor)

Concerns: 1) Interested in selling her home to TJH. 2) Requested more information on allowable fence heights. 3) Concerned the house is too close to the rear of the yard and does not desire the two story home close to the shared property line regardless of the compliant setbacks. The Neighbor claims the home and trees will affect/block solar gain.

Response: Thomas James Homes responded to the neighbor via phone multiple times and an email on 11/3/22 to follow up with requested information and concerns. We confirmed the proposal is well within the allowable setbacks and allowable height per the City municipal code and guidelines. In addition, the proposed home is 78ft from the main home on the 883 property. See the attached thread for email reference.

We look forward to adding to the charm and sense of community in Menlo Park and welcome any questions the City may have as we go through the Use Permit Application process.

Anna Felver, Planning Manager at **Thomas James Homes**
afelver@tjhusa.com | 650. 402.3024

THE RIGHT HOME. RIGHT WHERE YOU WANT IT.
255 Shoreline Drive, Suite 428, Redwood City, CA 94065

Re: University in Menlo Park set back

Anna Felver <afelver@tjhusa.com>

Thu 11/3/2022 11:45 AM

To: Elizabeth Houck <elhouck@gmail.com>

Cc: Jon Tattersall <jtattersall@tjhusa.com>; Tommy Beadel <tommy@tjhusa.com>; Cynthia Thiebaut <cthiebaut@tjhusa.com>

Elizabeth,

Please see the attached civil area plan for your reference of our proposed house location as you can see the dimensions documented. I am providing numbers from our professional plans.

If you have additions to your home, then we would like to adjust our plans to reflect those additions. If desired, we can coordinate a survey on your site to document additional existing buildings in the plans.

Understood and noted that you are objecting to the current proposal unless modifications are made. If/when changes are made, we will notify you.

Best,

Anna Felver

Senior Planning Manager, Northern California Division



Thomas James Homes

1255 Treat Blvd, Suite 800, Walnut Creek, CA 94597

(650) 402-3024 | TJH.com

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From: Elizabeth Houck <elhouck@gmail.com>

Sent: Thursday, November 3, 2022 11:25 AM

To: Anna Felver <afelver@tjhusa.com>

Cc: Jon Tattersall <jtattersall@tjhusa.com>; Tommy Beadel <tommy@tjhusa.com>; Cynthia Thiebaut <cthiebaut@tjhusa.com>

Subject: Re: University in Menlo Park set back

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Thank you for getting back to me.

The home you are proposing is not "over 20 feet from the property line" it is 20 feet. Words matter. It is also extraordinarily tall, blocking my solar access.

It is not "over 78 feet from my existing residence" - it is much closer to my ADU and would be much closer to any future house I chose to build on my property - again words matter.

Your first photograph is not relevant nor is your second as they do not show my property at all.

I will be objecting to your plan with both planning and the planning commission unless and until you address my concerns.

Again, thank you for getting back to me and do chose your words more carefully in the future or one might conclude you are lacking factual information.

Warmest regards,

Elizabeth

[linkedin.com/in/elizabethhouck/](https://www.linkedin.com/in/elizabethhouck/)

[facebook.com/elizabethhouck.737](https://www.facebook.com/elizabethhouck.737)

650.323.0313

"Process shouldn't be more complicated than what you're trying to accomplish."

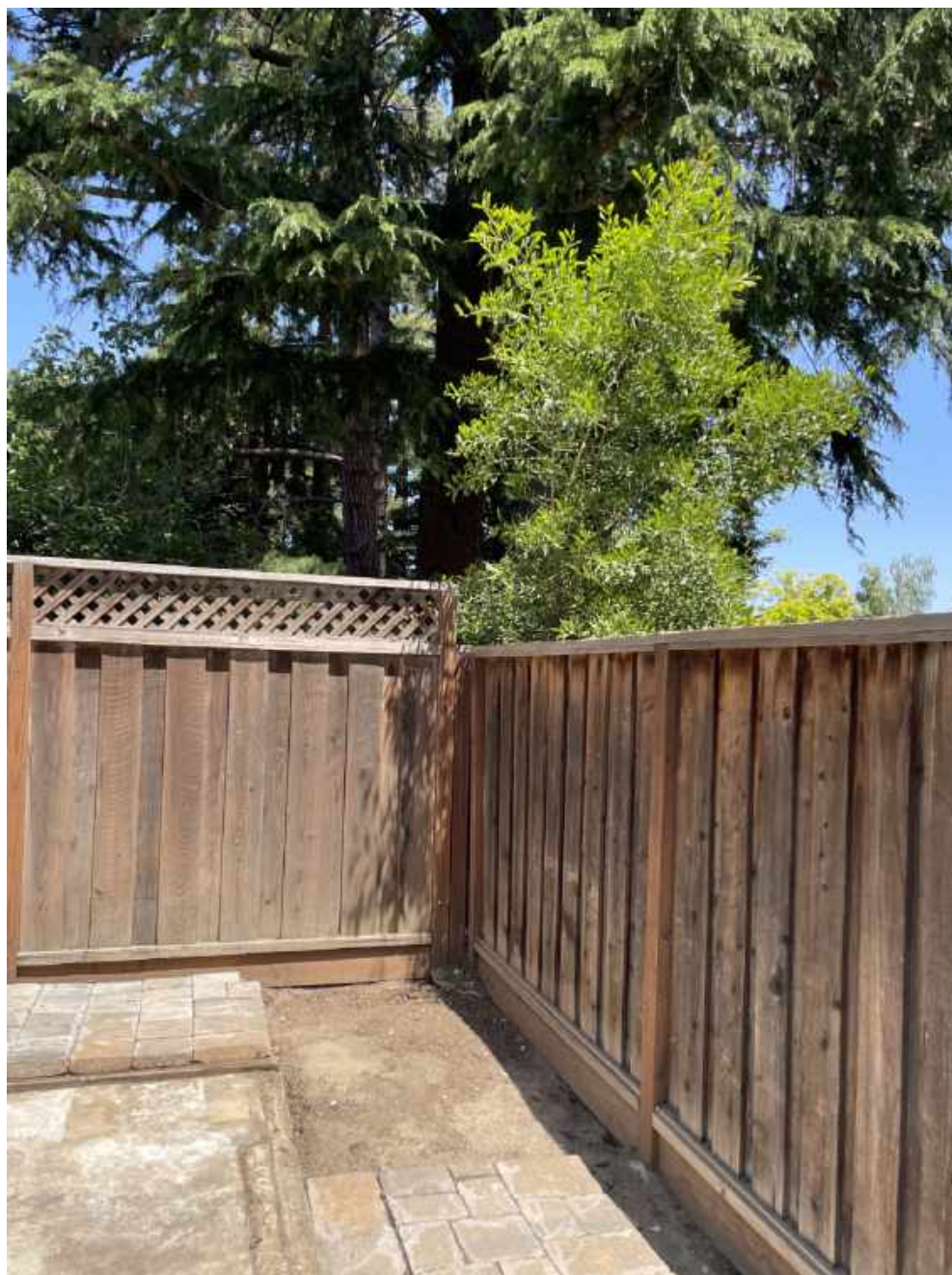
On Thu, Nov 3, 2022 at 11:07 AM Anna Felver <afelver@tjhusa.com> wrote:

Elizabeth,

Per our last phone conversation and meeting, we have taken note of your concerns. As you are aware, we are proposing a new two story that will be compliant with the city municipal code and guidelines. The proposed home is over 20ft from the rear property line and over 27ft from the left property line and over 78ft from your existing residence. As we also discussed previously, there are no trees being removed at the rear property line nor the dense tree canopy offsite be impacted.

We are still in the design process of the project so if there are updates and changes, we will notify you.





Take Care,

Anna Felver

Senior Planning Manager, Northern California Division



Thomas James Homes

1255 Treat Blvd, Suite 800, Walnut Creek, CA 94597

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From: Elizabeth Houck <elhouck@gmail.com>

Sent: Thursday, November 3, 2022 5:05 AM

To: Anna Felver <afelver@tjhusa.com>

Cc: Jon Tattersall <jtattersall@tjhusa.com>; Tommy Beadel <tommy@tjhusa.com>; Cynthia Thiebaut <cthiebaut@tjhusa.com>

Subject: Re: University in Menlo Park set back

Some people who received this message don't often get email from elhouck@gmail.com. [Learn why this is important](#)

This message was sent from outside the company by someone with a display name matching a user in your organization.

Please do not click links or open attachments unless you recognize the source of this email and know the content is safe.

When can I expect to hear from you?

Warmest regards,

Elizabeth

[linkedin.com/in/elizabethhouck/](https://www.linkedin.com/in/elizabethhouck/)

[facebook.com/elizabethhouck.737](https://www.facebook.com/elizabethhouck.737)

650.323.0313

"Process shouldn't be more complicated than what you're trying to accomplish."

On Sat, Oct 29, 2022 at 11:20 AM Elizabeth Houck <elhouck@gmail.com> wrote:

Hello Anna,

Since you have not gotten back to me, since our phone call, I am writing to express my concern about your proposed home on University in Menlo Park and my desire not to have a 24' hi two story house 20' from my property line.

Please consider moving the master suite to the main floor and the second story to the front of the house/property.

I would appreciate you getting back to me and not blocking my solar gain.

Warmest regards,

Elizabeth Houck

883 Middle Avenue

[linkedin.com/in/elizabethhouck/](https://www.linkedin.com/in/elizabethhouck/)

[facebook.com/elizabethhouck.737](https://www.facebook.com/elizabethhouck.737)

650.323.0313

"Process shouldn't be more complicated than what you're trying to accomplish."

440 University Avenue - Follow up Responses

Anna Felver <afelver@tjhusa.com>

Thu 11/3/2022 1:01 PM

To: sbitler27@gmail.com <sbitler27@gmail.com>

Cc: Andy Cost <acost@tjhusa.com>; Cynthia Thiebaut <cthiebaut@tjhusa.com>

Good afternoon, Steve,

Thank you for joining us at the neighbor meeting. We are still in the design review process with the city so no updates or changes to the home.

I did follow up with our team to respond to the questions/concerns you had at the meeting. Please see below for those responses:

Concern #1: Safety Concern about the big trucks on the street. Can you help us back out of the driveway?

Response #1: There is a Project manager onsite (who will reach out at the beginning and notify you when larger trucks are expected), they can accommodate this request.

Concern #2: This is a busy Street in the city and there is a lot of traffic.

Response #2: Thank you for the insight. Our team creates a traffic management plan for appropriate access of the site prior to construction.

Concern #3: Will there be a sidewalk provided?

Response #3: The city engineering department is indeed requiring a new sidewalk to conform with your property 444 University. This is expected to be in the building grading drawings in order to receive our building permit. We will also show this in our civil area plan for design review. See our city comment below.

Frontage Improvements:

- a. Add/revise the following note on the cover sheet:
 - *Any frontage improvements which are damaged either as an existing condition or as a result of construction will be required to be replaced. All frontage improvement work shall be in accordance with the latest version of the City Standard Details.*
- b. Construct a new sidewalk along entire project frontage. Conform to adjacent property, 444 University Drive.
- c. Remove and replace vertical curb and gutter along entire project frontage.

Concern #4: How do you demo when there is asbestos? When will we have results if there is asbestos and lead? Can we have a copy of the report?

Response #4: We actually had the home tested for asbestos in October. The existing house was built in 1940. Most likely this will have asbestos to report. However, the only asbestos found was in (2) Transit pipes and were removed immediately. I have attached that letter to you for reference or in case you wanted to reach out to this company for more detailed information on their process of removal.

For further info on the demolition process: Demolition is scheduled for around January 2023. Prior to start of demo, the City of Menlo Park requires a dust control plan and erosion control measures that they verify are installed. The demolition team will use hoses to control the dust during demo. Straw waddles will remain in place through construction along the perimeter of the fencing to control dirt/mud from leaving the site.

Let me know if you have any more concerns or need further clarification.
Best,

Anna Felver

Senior Planning Manager, Northern California Division



Thomas James Homes

1255 Treat Blvd, Suite 800, Walnut Creek, CA 94597

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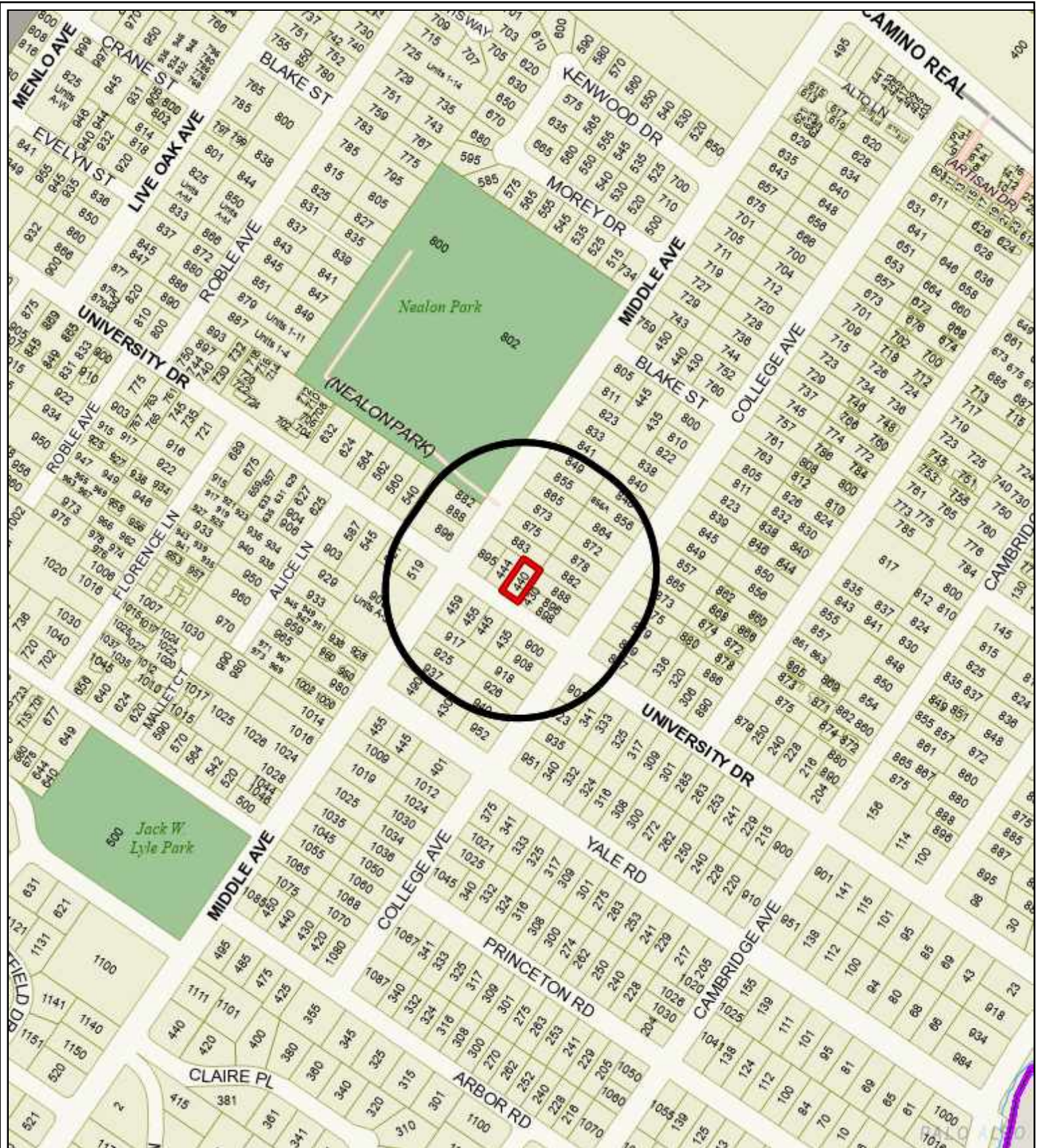


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LOCATION: 440 University Drive	PROJECT NUMBER: PLN2022-00050	APPLICANT: Thomas James Homes	OWNER: Thomas James Homes
---------------------------------------	--------------------------------------	--------------------------------------	----------------------------------

PROJECT CONDITIONS:

1. The use permit shall be subject to the following standard conditions:
 - a. The applicant shall be required to apply for a building permit within one year from the date of approval (by February 6, 2024) for the use permit to remain in effect.
 - b. Development of the project shall be substantially in conformance with the plans prepared by Dahlin Group Architecture consisting of 22 plan sheets, dated received January 19, 2023 and approved by the Planning Commission on February 6, 2023, except as modified by the conditions contained herein, subject to review and approval of the Planning Division.
 - c. Prior to building permit issuance, the applicant shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies’ regulations that are directly applicable to the project.
 - d. Prior to building permit issuance, the applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
 - e. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
 - f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division.
 - g. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits.
 - h. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the arborist report prepared by California Tree and Landscape Consulting, Inc. dated August 30, 2022.
 - i. Prior to building permit issuance, the applicant shall pay all fees incurred through staff time spent reviewing the application.
 - j. The applicant or permittee shall defend, indemnify, and hold harmless the City of Menlo Park or its agents, officers, and employees from any claim, action, or proceeding against the City of Menlo Park or its agents, officers, or employees to attack, set aside, void, or annul an approval of the Planning Commission, City Council, Community Development Director, or any other department, committee, or agency of the City concerning a development, variance, permit, or land use approval which action is brought within the time period provided for in any applicable statute; provided, however, that the applicant’s or permittee’s duty to so defend, indemnify, and hold harmless shall be subject to the City’s promptly notifying the applicant or permittee of any said claim, action, or proceeding and the City’s full cooperation in the applicant’s or permittee’s defense of said claims, actions, or proceedings.



City of Menlo Park
 Location Map
 440 UNIVERSITY DRIVE



	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	5,200 sf	5,200 sf	7,000 sf min
Lot width	52 ft	52 ft	65 ft min
Lot depth	100 ft	100 ft	100 ft min
Setbacks			
Front	20.2 ft	24.9 ft	20 ft min
Rear	20.8 ft	25.6 ft	20 ft min
Side (left)	8.1 ft	4.8 ft	10 percent of minimum lot width, minimum 5 feet
Side (right)	5.4 ft	9.8 ft	
Building coverage*	2,056.2* sf 39.5* %	1,130 sf 21.7 %	1,820 sf max 35.0 % max
FAL (Floor Area Limit)*	3,033.2* sf	1,130 sf	2,800 sf max
Square footage by floor	1,031.6 sf/1 st 1,287 sf/2 nd 437.3 sf/garage 277.3 sf/ADU 302 sf/porches 8 sf/fireplace	870 sf/1 st 260 sf/garage	
Square footage of buildings	3,343.2 sf	1,130 sf	
Building height	26.3 ft	16.9 ft	28 ft max
Parking	2 covered spaces	1 covered space	1 covered and 1 uncovered space
Note: Areas shown highlighted indicate a nonconforming or substandard situation			
Trees	Heritage trees 6**	Non-Heritage trees 16***	New trees 4
	Heritage trees proposed for removal 0	Non-Heritage trees proposed for removal 8	Total Number of trees 18

* Floor area and building coverage for the proposed project includes the ADU, which is allowed to exceed the maximum floor area and building coverage by up to 800 square feet

** These trees are located on neighboring properties.

*** Of these trees, two are located on neighboring properties and 14 are located on the subject property.



California Tree and Landscape Consulting, Inc.

August 30, 2022

Cynthia Thiebaut, Director of Development

Thomas James Homes

255 Shoreline Drive, Suite 428

Redwood City, California 94065

Via Email: cthiebaut@tjhusa.com

FINAL ARBORIST REPORT, TREE INVENTORY, CONSTRUCTION IMPACT ASSESSMENT AND TREE PROTECTION PLAN

RE: 440 University Drive, Menlo Park, California [APN 071-40-3280]

EXECUTIVE SUMMARY

Thomas James Homes contacted California Tree and Landscape Consulting, Inc. to document the trees on the property for a better understanding of the existing resource and any potential improvement obstacles that may arise, and to review the provided development plans and assessment of construction impacts with preservation recommendations. Thomas James Homes requested an Arborist Report, Tree Inventory, Construction Impact Assessment and Tree Protection Plan suitable for submittal to the City of Menlo Park. This is a Final Arborist Report, Tree Inventory, Construction Impact Assessment and Tree Protection Plan for the initial filing of plans to develop the property.

Thomas M. Stein, ISA Certified Arborist WE-12854A, visited the property on May 27, 2022, to provide species identification, measurements of DBH and canopy, field condition notes, recommended actions, ratings, and approximate locations for the trees. A total of 22 trees were evaluated on this property, 9 of which are protected trees according to the City of Menlo Park Municipal Code, Chapter 13.24.¹ Eight trees are located off the parcel but were included in the inventory because they may be impacted by development of the parcel.

TABLE 1: Trees 4"+ DBH

Tree Species	Total Trees Inventoried	Trees on this Site ²	Protected Heritage Oak Trees	Protected Heritage Other Trees	Street Tree	Trees Proposed for Removal	Total Proposed for Retention ³
Camphor, <i>Cinnamomum camphora</i>	1	1	0	0	0	0	1
Crape Myrtle, <i>Lagerstroemia indica</i>	1	1	0	0	0	1 (CR)	0
Deodar Cedar, <i>Cedrus deodara</i>	1	0	0	1	0	0	1
Fig, <i>Ficus carica</i>	1	0	0	0	0	0	1
Japanese Maple, <i>Acer palmatum</i>	1	1	0	0	0	1 (CR)	0
Lemon, <i>Citrus limon</i>	1	1	0	0	0	0	1
Persimmon, <i>Diospyros virginiana</i>	1	0	0	0	0	0	1
Potato Bush, <i>Lycianthes rantonnetii</i>	1	1	0	0	0	0	1

¹ Any tree protected by the City's Municipal Code will require replacement according to its appraised value if it is damaged beyond repair as a result of construction. In addition, any time development-related work is recommended to be supervised by a Project Arborist, it must be written in the report to describe the work plan and mitigation work. The Project Arborist shall provide a follow-up letter documenting the mitigation has been completed to specification.

² CalTLC, Inc. is not a licensed land surveyor. Tree locations are approximate and we do not determine tree ownership. Trees which appear to be on another parcel are listed as off-site and treated as the property of that parcel.

³ Trees in close proximity to development may require special protection measures. See Appendix/Recommendations for specific details.

Tree Species	Total Trees Inventoried	Trees on this Site ²	Protected Heritage Oak Trees	Protected Heritage Other Trees	Street Tree	Trees Proposed for Removal	Total Proposed for Retention ³
Purple Leaf Plum, <i>Prunus cerasifera</i>	2	2	0	0	0	2 (CR)	0
Southern Magnolia, <i>Magnolia grandiflora</i>	1	0	0	1	0	0	1
Strawberry, <i>Arbutus unedo</i>	1	1	0	0	0	1 (CR)	0
Sweet Gum, <i>Liquidambar</i>	1	0	0	1	0	0	1
Sweet Tobira, <i>Pittosporum tobira</i>	1	1	0	0	0	0	1
Valley Oak, <i>Quercus lobata</i>	3	0	3	0	0	0	3
TOTAL	17	9	3	3	0	5	12

TABLE 2: Trees 2"-3.99" DBH, including stumps

Tree Species	Total Trees Inventoried	Trees on this Site ⁴	Protected Heritage Oak Trees	Protected Heritage Other Trees	Street Tree	Trees Proposed for Removal	Total Proposed for Retention ⁵
Camellia, <i>Camellia sp.</i>	1	1	0	0	0	1 (CR)	0
Japanese Maple, <i>Acer palmatum</i>	2	2	0	0	0	1 (CR)	1
Lemon, <i>Citrus limon</i>	1	1	0	0	0	0	1
Stump	1	1	0	0	0	1 (AR & CR)	0
TOTAL	5	5	0	0	0	3	2

CR=Construction Removal; AR=Arborist Recommended Removal

ASSIGNMENT

Perform an examination of the site to document the presence and condition of trees protected by the City of Menlo Park. The study area for this effort includes the deeded parcel as delineated in the field by the property fences and any significant or protected trees overhanging from adjacent parcels.

Prepare a report of findings. All trees protected by the City of Menlo Park are included in the inventory.

METHODS

Appendix 2 in this report is the detailed inventory and recommendations for the trees. The following terms and Table A – Ratings Descriptions will further explain our findings.

The protected trees evaluated as part of this report have a numbered tag that was placed on each one that is 1-1/8" x 1-3/8", green anodized aluminum, "acorn" shaped, and labeled: CalTLC, Auburn, CA with 1/4" pre-stamped tree number and Tree Tag. They are attached with a natural-colored aluminum 10d nail, installed at approximately 6 feet above

⁴ CalTLC, Inc. is not a licensed land surveyor. Tree locations are approximate and we do not determine tree ownership. Trees which appear to be on another parcel are listed as off-site and treated as the property of that parcel.

⁵ The preservation of existing trees will be determined after review of improvement/building plans when the TPP is prepared.

ground level on the approximate north side of the tree. The tag should last ~10-20+ years depending on the species, before it is enveloped by the trees' normal growth cycle.

The appraisals included in this report (see Appendix 4) is based on the 10th Edition of the *Guide for Plant Appraisal*.⁶ The trunk formula technique of appraisal provides a basic cost to replace a tree, determined by its species and size. The tree costs are extrapolated from that of the most commonly available and used tree for landscaping, which at this time in Northern California has been determined to be a 24" box specimen.⁷ Based on the size and value of the tree as a 24" box, the species are valued at \$66.49 TO \$112.78 per square inch of trunk area. Per the request of the city of Menlo Park, multi-stem trees are measured as a single trunk, just below the lowest point of branching.

The basic value is depreciated by the tree's condition, which is considered a function of its health, structure and form and expressed as a percentage of the basic value. The result is termed the deterioration of the tree.

The trees are further depreciated by the functional and external limitations that may impact their ability to grow to their normal size, shape and function. Functional limitations include limited soil volume, adequate growing space, poor soil quality, etc. External limitations include easements, government regulations and ownership issues beyond the control of the tree's owner.

The final value is rounded to the nearest \$100 to obtain the assignment result. If the tree is not a complete loss, the value of loss is determined as a percentage of the original value. **It should be noted that Trees # 1-8 (Tags # 1340-1347) are offsite and inspected only from one side, from ground level. The lower to mid-trunks were obscured by fencing. The appraised value shown in the appraisal table and inventory summary should be considered only a rough estimate of the tree values. If an accurate appraisal is required, the trees will need re-appraisal without the observation limitations, and may require more advanced inspection techniques to determine the extent of the defects.**

TERMS

Species of trees is listed by our local common name and botanical name by genus and species.

DBH (diameter breast high) is normally measured at 4'6" (54" above the average ground height, but if that varies then the location where it is measured is noted here. A steel diameter tape was used to measure the trees.

Canopy radius is measured in feet. It is the farthest extent of the crown composed of leaves and small twigs measured by a steel tape. This measurement often defines the Critical Root Zone (CRZ) or Protection Zone (PZ), which is a circular area around a tree with a radius equal to this measurement.

Actions listed are recommendations to improve health or structure of the tree. Trees in public spaces require maintenance. If a tree is to remain and be preserved, then the tree may need some form of work to reduce the likelihood of failure and increase the longevity of the tree. Preservation requirements and actions based on a proposed development plan are not included here.

Arborist Rating is subjective to condition and is based on both the health and structure of the tree. All of the trees were rated for condition, per the recognized national standard as set up by the Council of Tree and Landscape Appraisers and the International Society of Arboriculture (ISA) on a numeric scale of 5 (being the highest) to 0 (the worst condition,

⁶ 2018. Council of Tree and Landscape Appraisers. *Guide for Plant Appraisal*, 10th Edition, 2nd Printing. International Society of Arboriculture, Atlanta, GA

⁷ 2004. *Western Chapter Species Classification and Group Assignment*. Western Chapter, International Society of Arboriculture. Porterville, CA

dead). The rating was done in the field at the time of the measuring and inspection.

Table A – Ratings Descriptions

No problem(s)	5	excellent
No apparent problem(s)	4	good
Minor problem(s)	3	fair
Major problem(s)	2	poor
Extreme problem(s)	1	hazardous, non-correctable
Dead	0	dead

Rating #0: This indicates a tree that has no significant sign of life.

Rating #1: The problems are extreme. This rating is assigned to a tree that has structural and/or health problems that no amount of work or effort can change. The issues may or may not be considered a dangerous situation.

Rating #2: The tree has major problems. If the option is taken to preserve the tree, its condition could be improved with correct arboricultural work including, but not limited to: pruning, cabling, bracing, bolting, guying, spraying, mistletoe removal, vertical mulching, fertilization, etc. If the recommended actions are completed correctly, hazard can be reduced and the rating can be elevated to a 3. If no action is taken the tree is considered a liability and should be removed.

Rating #3: The tree is in fair condition. There are some minor structural or health problems that pose no immediate danger. When the recommended actions in an arborist report are completed correctly the defect(s) can be minimized or eliminated.

Rating #4: The tree is in good condition and there are no apparent problems that a Certified Arborist can see from a visual ground inspection. If potential structural or health problems are tended to at this stage future hazard can be reduced and more serious health problems can be averted.

Rating #5: No problems found from a visual ground inspection. Structurally, these trees have properly spaced branches and near perfect characteristics for the species. Highly rated trees are not common in natural or developed landscapes. No tree is ever perfect especially with the unpredictability of nature, but with this highest rating, the condition should be considered excellent.

Notes indicate the health, structure and environment of the tree and explain why the tree should be removed or preserved. Additional notes may indicate if problems are minor, extreme or correctible.

Remove is the recommendation that the tree be removed. The recommendation will normally be based either on poor structure or poor health and is indicated as follows:

Yes H – Tree is unhealthy

Yes S – Tree is structurally unsound

OBSERVATIONS AND CONCLUSIONS

The site is located in an existing subdivision with single-family residences, and the vegetation is comprised of ornamental landscape plants. The site is located in an existing subdivision with single-family residences, and the vegetation is comprised of ornamental landscape plants. The existing single-story home has a reported area of 870 sq. ft. and a reported lot size of 5,183 sq. ft. The home is connected to electrical, communication, gas, water, and sanitary sewer infrastructure. The development plans include demolition of the existing home, and construction of a new two-story home with a reported area of 2,311 sq. ft. (livable, plus a 273 sq. ft. accessory dwelling unit), new landscape and hardscape. Refer to Appendix 2 – Tree Data for details

RECOMMENDED REMOVALS OF HAZARDOUS, DEFECTIVE OR UNHEALTHY TREES

At this time, one stump on the property has been recommended for removal from the proposed project area due to the nature and extent of defects, compromised health, and/or structural instability noted at the time of field inventory

efforts. If this tree was retained within the proposed project area, it is our opinion that it may be hazardous depending upon its proximity to planned development activities. For reference, the tree which has been recommended for removal is highlighted in green within the accompanying Tree Data (Appendix 2) and briefly summarized as follows:

Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	DBH	Circ.	Diameter Measured At	Arborist Rating
22	n/a	No	No	No	No	Stump	Stump	10	31	2	0 Dead

CONSTRUCTION IMPACT ASSESSMENT

This Arborist Report, Tree Inventory, Construction Impact Assessment and Tree Protection Plan is intended to provide to Thomas James Homes, the City of Menlo Park, and other members of the development team a detailed *pre-development review* of the species, size, and current structure and vigor of the trees within and/or overhanging the proposed project area. We have reviewed the Draft Plan, prepared by Dahlin, dated August 11, 2022; the Landscape Construction Drawings, prepared by Van Dorn Landscape Architects, Inc, dated August 25, 2022; and the Grading and Drainage Plans (Grading Plan), prepared by Civil Engineering Associates, date June 17, 2022. The perceived construction impacts to protected trees are summarized below. **Refer to Appendix 2 – Tree Data for protective measures to be taken for trees that will remain.**

Trees 4"+ DBH

Tree # 1 (Tag # 1340): No impact is expected from development to this off-site tree.

Tree # 2 (Tag # 1341): Slight impact to the tree's CRZ is expected from excavation for the paved walkway. No impact is expected to the tree's canopy.

Tree # 3 (Tag # 1342): Moderate impact to the tree's CRZ is expected from excavation for the paved walkway. Consider an alternative design to reduce the CRZ impacts. No impact is expected to the tree's canopy.

Tree # 4 (Tag # 1343): Moderate impact to the tree's CRZ is expected from excavation for the paved walkway. Consider an alternative design to reduce the CRZ impacts. No impact is expected to the tree's canopy.

Tree # 5 (Tag # 1344): Moderate impact to the tree's CRZ is expected from excavation for the paved walkway. Consider an alternative design to reduce the CRZ impacts. No impact is expected to the tree's canopy. Slight impact to the tree's canopy is expected due to building encroachment.

Tree # 6 (Tag # 1345): No impact is expected from development to this off-site tree.

Tree # 7 (Tag # 1346): Slight impact to the tree's canopy is expected due to clearance needs for the new landscaping.

Tree # 8 (Tag # 1347): Slight impact to the tree's canopy is expected due to clearance needs for the new landscaping.

Tree # 9 (Tag # 1348): Moderate impact to the tree's CRZ is expected due to foundation excavation and fence installation. No impact is expected to the tree's canopy.

Tree # 10 (Tag # 1349): The developer proposes removal of this tree. It is located in the rear patio hardscape area. It is not protected.

Tree # 11 (Tag # 1350): Slight impact to the tree's CRZ is expected due to hardscape installation. Slight impact to the tree's canopy is expected due to clearance requirements.

Tree # 12 (Tag # 1352): Slight impact to the tree's CRZ is expected due to hardscape installation. Slight impact to the tree's canopy is expected due to clearance requirements.

Tree # 13 (Tag # 1354): Slight impact to the tree's CRZ is expected due to hardscape installation. Slight impact to the tree's canopy is expected due to clearance requirements.

Tree # 14 (Tag # 1355): The developer proposes removal of this tree. It is located in the building envelope.

Tree # 15 (Tag # 1357): The developer proposes removal of this tree. It is incompatible with the proposed new landscape. It is not protected.

Tree # 16 (Tag # 1358): The developer proposes removal of this tree. It is incompatible with the proposed new landscape. It is not protected.

Tree # 17 (Tag # 1359): The developer proposes removal of this tree. It is incompatible with the proposed new landscape. It is not protected.

Trees 2"-3.99" DBH, including stumps

Tree # 18 (Tag # 1351): Slight impact to the tree's CRZ is expected due to hardscape installation. Slight impact to the tree's canopy is expected due to clearance requirements.

Tree # 19 (Tag # 1353): The developer proposes removal of this tree. It is located in the rear patio hardscape area. It is not protected.

Tree # 20 (Tag # 1356): Slight impact to the tree's CRZ is expected due to foundation excavation. Slight impact to the tree's canopy is expected due to clearance requirements.

Tree # 21 (Tag # 1360): The developer proposes removal of this non-protected tree. It is located in the proposed driveway area.

Tree # 22 (no tag #): The developer proposes removal of this stump.

The Menlo Park Tree Ordinance requires any work directed by the Project Arborist should follow a written work plan and mitigation plan. The Project Arborist shall provide a letter documenting the work and mitigation has been completed to specification.

A tree protection verification letter is required from the Project Arborist prior to the start of construction. The letter shall include photos of the tree protection installed to specification. The letter should also specify that monthly inspections are required.

DISCUSSION

Trees need to be protected from normal construction practices if they are to remain healthy and viable on the site. Our recommendations are based on experience, and County ordinance requirements, so as to enhance tree longevity. This requires their root zones remain intact and viable, despite heavy equipment being on site, and the need to install foundations, driveways, underground utilities, and landscape irrigation systems. Simply walking and driving on soil has serious consequences for tree health.

Following is a summary of Impacts to trees during construction and Tree Protection measures that should be incorporated into the site plans in order to protect the trees. Once the plans are approved, they become the document that all contractors will follow. ***The plans become the contract between the owner and the contractor, so that only items spelled out in the plans can be expected to be followed. Hence, all protection measures, such as fence locations, mulch requirements and root pruning specifications must be shown on the plans.***

RECOMMENDATIONS: SUMMARY OF TREE PROTECTION MEASURES

Hire a Project Arborist to help ensure protection measures are incorporated into the site plans and followed. The Project Arborist should, in cooperation with the Engineers and/or Architects:

- Identify the Root Protection Zones on the final construction drawings, prior to bidding the project.
- Show the placement of tree protection fences, as well as areas to be irrigated, fertilized and mulched on the final construction drawings.
- Clearly show trees for removal on the plans and mark them clearly on site. A Contractor who is a Certified Arborist should perform tree and stump removal. All stumps within the root zone of trees to be preserved shall be ground out using a stump router or left in place. **No trunk within the root zone of other trees shall be removed using a backhoe or other piece of grading equipment.**
- Prior to any grading, or other work on the site that will come within 50' of any tree to be preserved:
 1. Irrigate (if needed) and place a 6" layer of chip mulch over the protected root zone of all trees that will be impacted.
 2. Erect Tree Protection Fences. Place boards against trees located within 3' of construction zones, even if fenced off.
 3. Remove lower foliage that may interfere with equipment PRIOR to having grading or other equipment on site. The Project Arborist should approve the extent of foliage elevation, and oversee the pruning, performed by a contractor who is an ISA Certified Arborist.
- For grade cuts, expose roots by hand digging, potholing or using an air spade and then cut roots cleanly prior to further grading outside the tree protection zones.
- For fills, if a cut is required first, follow as for cuts.
- Where possible, specify geotextile fabric and/or thickened paving, re-enforced paving, and structural soil in lieu of compacting, and avoid root cutting as much as possible, prior to placing fills on the soil surface. Any proposed retaining wall or fill soil shall be discussed with the engineer and arborist in order to reduce impacts to trees to be preserved.
- Clearly designate an area on the site outside the drip line of all trees where construction materials may be stored, and parking can take place. No materials or parking shall take place within the root zones of protected trees.
- Design utility and irrigation trenches to minimize disturbance to tree roots. Where possible, dig trenches with hydro-vac equipment or air spade, placing pipes underneath the roots, or bore the deeper trenches underneath the roots.
- Include on the plans an Arborist inspection schedule to monitor the site during (and after) construction to ensure protection measures are followed and make recommendations for care of the trees on site, as needed.

General Tree protection measures are included as Appendix 3. These measures need to be included on the Site, Grading, Utility and Landscape Plans. A final report of recommendations specific to the plan can be completed as part of, and in conjunction with, the actual plans. This will require the arborist working directly with the engineer and architect for the project. If the above recommendations are followed, the amount of time required by the arborist for the final report should be minimal.

Report Prepared by:



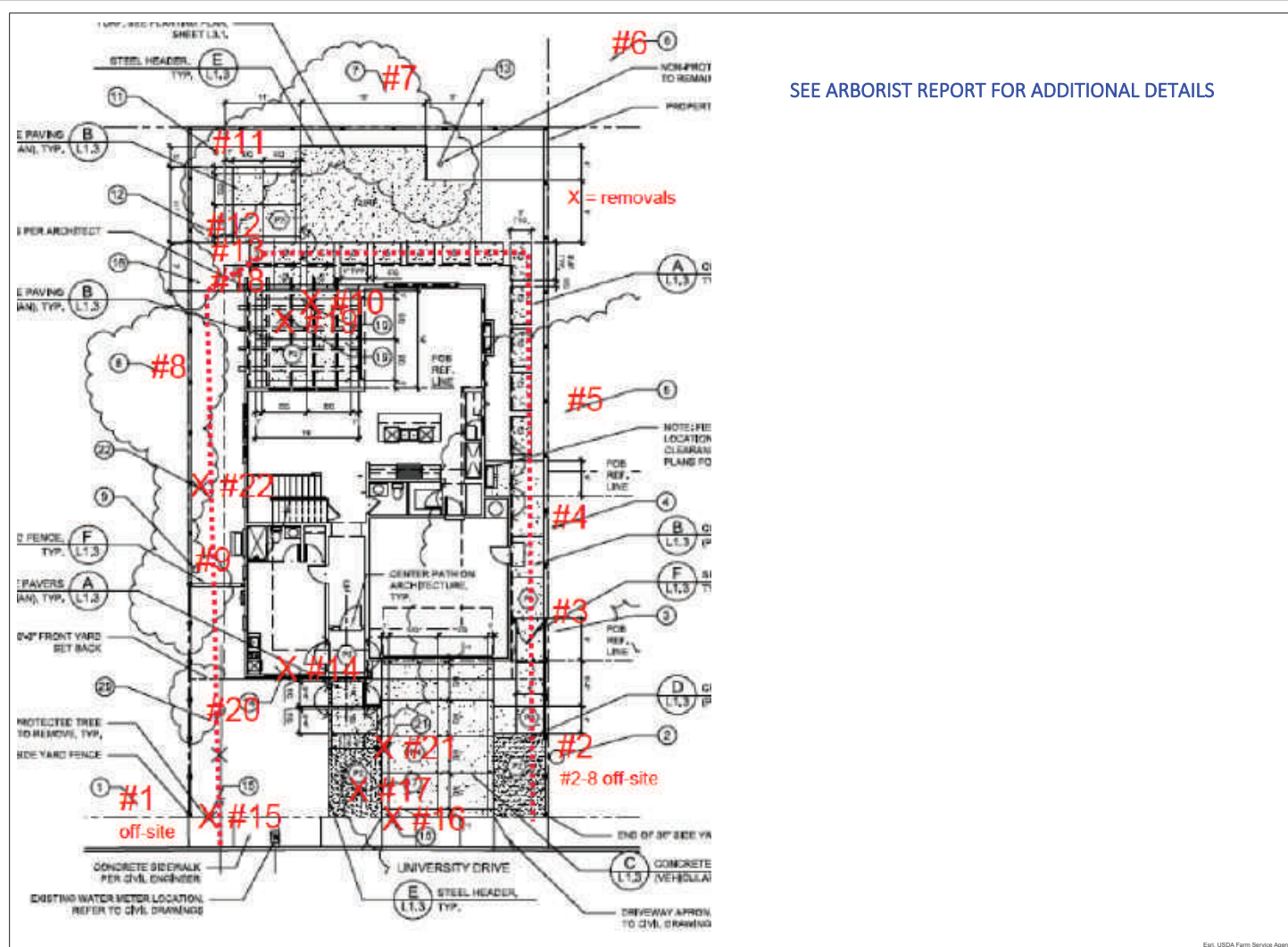
Edwin E. Stirtz, Consulting Arborist
International Society of Arboriculture
Certified Arborist WE-0510A
ISA Tree Risk Assessment Qualified
Member, American Society of Consulting Arborists

Report Reviewed by:



Gordon Mann
Consulting Arborist and Urban Forester
Registered Consulting Arborist #480
ISA Certified Arborist and Municipal Specialist #WE-0151AM
CaUFC Certified Urban Forester #127
ISA Qualified Tree Risk Assessor

- Enc.: Appendix 1 – Tree Protection Plan
Appendix 2 – Tree Data
Appendix 3 – General Practices for Tree Protection
Appendix 4 – Appraisal Value Table
Appendix 5 – Tree Protection Specifications
Appendix 6 – Photographs



SEE ARBORIST REPORT FOR ADDITIONAL DETAILS

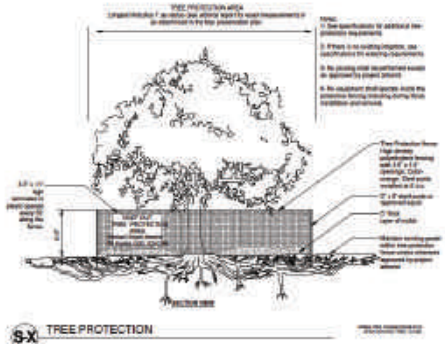


California Tree & Landscape Consulting, Inc.

359 Nevada St. Suite 201
Auburn, CA 95603

TREE PROTECTION GENERAL REQUIREMENTS

- The project arborist for this project is California Tree & Landscape Consulting. The primary contact information is Florida Arburists (FLA) 888-640-0000. The project arborist may refer to the project arborist and make all the final tree protection decisions during the construction process if and when additional requests occur or tree responses occur. Monitoring and construction oversight by the project arborist is recommended for all projects and required when a final letter of assessment is required by the jurisdiction.
- The project arborist shall inspect the preliminary root protection fencing installed by the contractor prior to any grading and/or grading for compliance with the recommended protection zones. Additionally, the project arborist shall inspect the fencing at the start of each phase of construction. The root protection zone for trees is specified as the "varying radius" in Appendix 2 in the arborist report unless otherwise specified by the arborist. Note: Varying radius is not an acceptable location for installation of tree protection fencing.
- The project arborist shall identify to provide any necessary pruning, digging, fertilization, placement of mulch and/or electrical treatments. If clearance pruning is required, the Project arborist should approve the extent of foliage removal and oversee the pruning to be performed by a contractor who is an ISA Certified Arborist. Clearance pruning should include removal of all the lower foliage that may interfere with equipment ROPS to having grading or other equipment inside.
- No trunk within the root protection zone of any trees shall be removed using a chainsaw or other form of grading equipment.
- Clear designated areas on the site that is outside of the protection area of all trees where construction materials may be stored, and parking can take place. The materials or parking shall take place within the protection zone of any trees on or off the site.
- Any and all work to be performed inside the protected root zone fencing, including all grading and all tree trenching, shall be approved and/or supervised by the project arborist.
- Staking, if required, inside the protected root zone shall be approved and/or supervised by the project arborist and may be required to be performed by hand, by hydraulic or air spade, or other method which will place pipes underneath the trees without damage to the roots.
- The root protection zone for trees is specified as the "varying radius" in Appendix 2 in the arborist report unless otherwise specified by the arborist. Note: Varying radius is not an acceptable location for installation of tree protection fencing.



TREE PROTECTION PLAN

Page 1 of 1



440 University Drive

City of Menlo Park, California

Sheet No.
TPP 1.1

Prepared by Thomas M. Stein, ISA Cert #WE-12854A

Date: 08/28/2022

APPENDIX 2 – TREE DATA

Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
Trees 4"+ DBH																					
1	1340	No	Yes	No	Yes	Southern Magnolia	<i>Magnolia grandiflora</i>		30	94	54	26	2 Major Structure or Health Problems	Preserve	Lower trunk & root crown obscured by hedge. Growing ~19' W of property line & overhanging 5'. Branches at 5' above grade into 7 main scaffolds. Sloughing bark throughout. Crossing branches throughout. Wires in canopy. Multiple pruning wounds with decay. DBH estimated.	None at this time.	No impact from development is expected.	Install PTF as show in App. 1. Perform clearance pruning (if needed) prior to demo.	G	8550	N/A



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
2	1341	No	Yes	No	Yes	Sweet Gum	<i>Liquidambar</i>		15	47	54	18	3 Fair - Minor Problems	Preserve	Offsite growing 5' E of property line 5' N of pavement; no sidewalk. Exposed buttress roots S. Stem girdling root N. Codominant branching 6' above grade. Partially callused pruning wound W at 5'. Reduction pruning throughout. Growing underneath Tree # 1342. Dimensions estimated.	None at this time.	Slight CRZ impact from paved walkway excavation.	Perform excavation by hand within CRZ. Perform any root pruning under direction of project arborist.	G	4050	N/A
3	1342	Yes	No	No	Yes	Valley Oak	<i>Quercus lobata</i>		20	63	54	28	2 Major Structure or Health Problems	Preserve	Offsite growing on E property line, 29' from existing home, and 15' from next-door house. Codominant at 9 and 12'. Out of balance W & N. Lower trunk covered by ivy. Leaning N. Dead branches to 2" dia. in upper canopy throughout.	None at this time.	Moderate CRZ impact from paved walkway excavation. Consider alternative designs to reduce CRZ impacts.	Perform excavation by hand within CRZ. Perform any root pruning under direction of project arborist.	G	5450	N/A



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
4	1343	Yes	No	No	Yes	Valley Oak	<i>Quercus lobata</i>		20	63	54	29	2 Major Structure or Health Problems	Preserve	Offsite growing on E property line, 20' to house and overhanging site 20'. Trunk and root crown obscured by fence. Codominant branching at 10' into 3 scaffolds. Sparse canopy with dead branches to 3" in dia. May require clearance pruning for development. Tag on W trunk. Dimensions estimated.	None at this time.	Moderate CRZ impact from paved walkway excavation. Consider alternative designs to reduce CRZ impacts.	Perform excavation by hand within CRZ. Perform any root pruning under direction of project arborist.	G	5450	N/A
5	1344	Yes	No	No	Yes	Valley Oak	<i>Quercus lobata</i>		25	79	54	32	2 Major Structure or Health Problems	Preserve	Offsite growing 15 ft E of property line. Overhanging site 18'. Dimensions estimated. Codominant branching at 12'. Lower trunk & root crown obscured by fence. Leaning E. Sparse upper canopy. Requires clearance pruning over garage.	None at this time.	Moderate CRZ impact from paved walkway excavation. Consider alternative designs to reduce CRZ impacts. Slight impact to canopy due to building encroachment.	Perform clearance pruning prior to demo. Perform excavation by hand within CRZ. Perform any root pruning under direction of project arborist.	G	17550	N/A



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
6	1345	No	Yes	No	Yes	Deodar Cedar	<i>Cedrus deodara</i>		35	110	54	25	3 Fair - Minor Problems	Preserve	Growing 25' NE of N property line and 48' from house. Dimensions estimated. Tag on fence. Negligible overhang and no encroachment expected.	None at this time.	No impact from development is expected.	None; off-site tree.	G	29550	N/A
7	1346	No	No	No	Yes	Fig	<i>Ficus carica</i>		10	31	24	17	3 Fair - Minor Problems	Preserve	Offsite growing 10' N of N property line and overhanging site 7'. Dimensions estimated. Tag on fence. Lower trunk & root crown obscured by fence. Codominant branching at 5'.	None at this time.	Slight impact to canopy due to clearance for new landscape.	Perform clearance pruning prior to demo.	G	N/A	N/A



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
8	1347	No	No	No	Yes	Persimmon	<i>Diospyros virginiana</i>		8	25	54	12	3 Fair - Minor Problems	Preserve	Offsite growing 3' W of property line and overhanging site 7'. Dimensions estimated. Lower trunk & root crown obscured by fence. Tag on fence. Codominant branching at 11'. No encroachment expected after pruning.	None at this time.	Slight impact to canopy due to clearance for new landscape.	Perform clearance pruning prior to demo.	G	N/A	N/A
9	1348	No	No	No	No	Sweet Tobira	<i>Pittosporum tobira</i>		10	31	6	11	2 Major Structure or Health Problems	Unknown	Exposed buttress roots N & S. Growing 3' from house, 1' from fence and 5' from gas meter. Clearance pruned E. Branches at 1'. Topped at 13' with weak attachments. Mechanical damage from climbing spikes.	None at this time.	Moderate impact to CRZ due to foundation excavation and fence installation.	Perform excavation within CRZ by hand. Perform any root pruning under direction of project arborist. Modify fence post location if structural roots are encountered while augering post holes.	M	N/A	N/A



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
10	1349	No	No	No	No	Japanese Maple	<i>Acer palmatum</i>		9	28	24	11	3 Fair - Minor Problems	Unknown	Growing 12' from house and 2' from deck. Branches at 3'. Codominant branching at 4.8' with included bark. Exposed root S & NW. Bark split at grade to 5' N and additional splitting on S side to ~7'. Minor amount of deadwood.	Evaluate for transplanting.	Developer proposes removal.	N/A	G	N/A	In patio hardscape area.
11	1350	No	No	No	No	Camphor	<i>Cinnamomum camphora</i>		8	25	54	11	2 Major Structure or Health Problems	Unknown	Exposed roots NE. Leaning SE. Out of balance SE. Dead branches in lower canopy. Located 32' from house and 4' from NW corner.	None at this time.	Slight impact to CRZ due to hardscape installation. Slight impact to canopy due to clearance requirements.	Perform excavation for hardscape by hand w/in CRZ. Perform any root pruning under direction of project arborist. Perform clearance pruning, as needed, prior to demo. Install PTF as shown in App. 1. Monitor irrigation needs 2x/mo-irr. as needed.	G	N/A	N/A



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
12	1352	No	No	No	No	Potato Bush	<i>Lycianthes rantonnetii</i>		6	19	6	2	2 Major Structure or Health Problems	Unknown	Tag on branch. Dimensions estimated. Decay in lower trunk. One-sided E. Growing 16' from house.	None at this time.	Slight impact to CRZ due to hardscape installation. Slight impact to canopy due to clearance requirements.	Perform excavation for hardscape by hand w/in CRZ. Perform any root pruning under direction of project arborist. Perform clearance pruning, as needed, prior to demo. Install PTF as shown in App. 1. Monitor irrigation needs 2x/mo-irr. as needed.	G	N/A	N/A

Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
13	1354	No	No	No	No	Lemon	<i>Citrus limon</i>		9	28	12	8	2 Major Structure or Health Problems	Unknown	Branches at grade. Located 5' S of property line and 19' N of house. Out of balance W. Bearing fruit.	None at this time.	Slight impact to CRZ due to hardscape installation. Slight impact to canopy due to clearance requirements.	Perform excavation for hardscape by hand w/in CRZ. Perform any root pruning under direction of project arborist. Perform clearance pruning, as needed, prior to demo. Install PTF as shown in App. 1. Monitor irrigation needs 2x/mo-irr. as needed.	G	N/A	N/A
14	1355	No	No	No	No	Strawberry	<i>Arbutus unedo</i>	4,5	9	28	24	11	3 Fair - Minor Problems	Unknown	Branches at 3'. Clearance pruned N. Located 4' from house and 3' W of front porch steps. Slight lean NE. Adjacent to water service entrance.	None at this time.	Developer proposes removal.	N/A	G	N/A	In building envelope.



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
15	1357	No	No	No	No	Purple Leaf Plum	<i>Prunus cerasifera</i>		5	16	18	6	3 Fair - Minor Problems	Unknown	Growing 5' N of curb, 3' E of W property line and 24' from house. Branches at 2'. Slight amount of dead branches mid to upper canopy.	None at this time.	Developer proposes removal.	N/A	G	N/A	Incompatibility with proposed new landscape.
16	1358	No	No	No	No	Purple Leaf Plum	<i>Prunus cerasifera</i>		4	13	24	4	3 Fair - Minor Problems	Unknown	Growing 4' N of curb, 3' W of driveway and 28' from house. Branches at 2.5'. Upper canopy has 10-15% dieback.	None at this time.	Developer proposes removal.	N/A	G	N/A	Incompatibility with proposed new landscape.
17	1359	No	No	No	No	Crape Myrtle	<i>Lagerstroemia indica</i>		4	13	36	6	3 Fair - Minor Problems	Unknown	Growing 9' N of curb, 24' from house and 9' W of driveway. Branches codominant at 4'. 2' E of walkway.	None at this time.	Developer proposes removal.	N/A	G	N/A	Incompatibility with proposed new landscape.

Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
Trees 2"-3.99" DBH, including stumps																					
18	1351	No	No	No	No	Lemon	<i>Citrus limon</i>		3	9	12	2	2 Major Structure or Health Problems	Unknown	Tag on branch. Growing 5' E of property line. Moderate lean S. Out of balance E. Deadwood. Bearing fruit.	None at this time.	Slight impact to CRZ due to hardscape installation. Slight impact to canopy due to clearance requirements.	Perform excavation for hardscape by hand w/in CRZ. Perform any root pruning under direction of project arborist. Perform clearance pruning, as needed, prior to demo. Install PTF as shown in App. 1. Monitor irrigation needs 2x/mo-irr. as needed.	G	N/A	N/A
19	1353	No	No	No	No	Camellia	<i>Camellia sp.</i>		3	9	12	4	2 Major Structure or Health Problems	Unknown	Growing 3' from Tree #1349 and irrigation box, and 1' from deck. Tag on branch. Under canopy of Tree #1349.	Use caution during stump grinding & removal if removed to transplant Tree # 1349.	Developer proposes removal.	N/A	G	N/A	In patio hardscape area.



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
20	1356	No	No	No	No	Japanese Maple	<i>Acer palmatum</i>		3	9	6	3	3 Fair - Minor Problems	Unknown	Branches at 12". Located 10.5' from house and 3' E of W property line.	None at this time.	Slight impact to CRZ due to foundation excavation. Slight impact to canopy due to clearance requirements.	Perform excavation for hardscape by hand w/in CRZ. Perform any root pruning under direction of project arborist. Perform clearance pruning, as needed, prior to demo. Install PTF as shown in App. 1. Monitor irrigation needs 2x/mo-irr. as needed.	G	N/A	N/A
21	1360	No	No	No	No	Japanese Maple	<i>Acer palmatum</i>		3	9	3	5	3 Fair - Minor Problems	Unknown	Branches at 6". Growing 16.5' from house, 7' from driveway and 3' from walkway. Tag on branch. Purple leaf cultivar.	None at this time.	Developer proposes removal.	N/A	G	N/A	Located in new proposed driveway.
22	n/a	No	No	No	No	Stump	<i>Stump</i>		10	31	2		0 Dead			Recommend removal.	Developer proposes removal.	N/A	N/A	N/A	Stump

TOTAL INVENTORIED TREES = 22 trees (782 aggregate circumference inches)
TOTAL RECOMMENDED REMOVALS = 1 stump (31 aggregate circumference inches)
TOTAL RECOMMENDED REMOVALS FOR DEVELOPMENT= 8 trees (1 is a stump); (147 aggregate circumference inches)
Rating (0-5, where 0 is dead) = 0=1 tree; 2=10 trees; 3=11 trees
Total Protected Street Trees = None



Tree #	Tag #	Heritage Oak Tree 31.4"+ circ.	Heritage Other Tree 47.1"+ circ.	Street Tree	Off-site	Common Name	Botanical Name	Multi-Stems	DBH	Circ.	Diameter Measured At	Measured Canopy Radius	Arborist Rating	Dvlpmt Status	Notes	Recommendations	Construction Impact	Protective Measures to be Taken	Suitability for Preservation	Appraised Value, Rounded (\$)	Justification for Removal
Total Protected Oak Trees 31.4"+ = 3 trees (205 aggregate circumference inches)																					
Total Protected Other Trees 47.1"+ = 3 trees (251 aggregate circumference inches)																					
TOTAL PROTECTED TREES = 6 trees (456 aggregate circumference inches)																					

APPENDIX 3 – GENERAL PRACTICES FOR TREE PROTECTION

Definitions:

Root zone: The roots of trees grow fairly close to the surface of the soil, and spread out in a radial direction from the trunk of tree. A general rule of thumb is that they spread 2 to 3 times the radius of the canopy, or 1 to 1½ times the height of the tree. It is generally accepted that disturbance to root zones should be kept as far as possible from the trunk of a tree.

Inner Bark: The bark on large valley oaks and coast live oaks is quite thick, usually 1” to 2”. If the bark is knocked off a tree, the inner bark, or cambial region, is exposed or removed. The cambial zone is the area of tissue responsible for adding new layers to the tree each year, so by removing it, the tree can only grow new tissue from the edges of the wound. In addition, the wood of the tree is exposed to decay fungi, so the trunk present at the time of the injury becomes susceptible to decay. Tree protection measures require that no activities occur which can knock the bark off the trees.

Methods Used in Tree Protection:

No matter how detailed Tree Protection Measures are in the initial Arborist Report, they will not accomplish their stated purpose unless they are applied to individual trees and a Project Arborist is hired to oversee the construction. The Project Arborist should have the ability to enforce the Protection Measures. The Project Arborist should be hired as soon as possible to assist in design and to become familiar with the project. He must be able to read and understand the project drawings and interpret the specifications. He should also have the ability to cooperate with the contractor, incorporating the contractor’s ideas on how to accomplish the protection measures, wherever possible. It is advisable for the Project Arborist to be present at the Pre-Bid tour of the site, to answer questions the contractors may have about Tree Protection Measures. This also lets the contractors know how important tree preservation is to the developer.

Root Protection Zone (RPZ): Since in most construction projects it is not possible to protect the entire root zone of a tree, a Root Protection Zone is established for each tree to be preserved. The minimum Root Protection Zone is the area underneath the tree’s canopy (out to the dripline, or edge of the canopy), plus 1’. The Project Arborist must approve work within the RPZ.

Irrigate, Fertilize, Mulch: Prior to grading on the site near any tree, the area within the Tree Protection fence should be fertilized with 4 pounds of nitrogen per 1000 square feet, and the fertilizer irrigated in. The irrigation should percolate at least 24 inches into the soil. This should be done no less than 2 weeks prior to grading or other root disturbing activities. After irrigating, cover the RPZ with at least 12” of leaf and twig mulch. Such mulch can be obtained from chipping or grinding the limbs of any trees removed on the site. Acceptable mulches can be obtained from nurseries or other commercial sources. Fibrous or shredded redwood or cedar bark mulch shall not be used anywhere on site.

Fence: Fence around the Root Protection Zone and restrict activity therein to prevent soil compaction by vehicles, foot traffic or material storage. The fenced area shall be off limits to all construction equipment, unless there is express written notification provided by the Project Arborist, and impacts are discussed and mitigated prior to work commencing.

A protective barrier of 6’ chain link fence shall be installed around the dripline of protected tree(s). The fencing can be moved within the dripline if authorized by the project arborist or city arborist, but not

closer than 2' from the trunk of any tree. Fence posts shall be 1.5" in diameter and are to be driven 2' into the ground. The distance between posts shall not be more than 10'. Movable barriers of chain link fencing secured to cement blocks can be substituted for "fixed" fencing if the project arborist and city arborist agree that the fencing will have to be moved to accommodate certain phases of construction. The builder may not move the fence without authorization from the project or city arborist.

Where the city or project arborist has determined that tree protection fencing will interfere with the safety of work crews, tree wrap may be used as an alternative form of tree protection. Wooden slats at least 1" thick are to be bound securely, edge to edge, around the trunk. A single layer or more of orange plastic construction fencing is to be wrapped and secured around the outside of the wooden slats. Major scaffold limbs may require protection as determined by the city or project arborist. Straw waddle may also be used as a trunk wrap by coiling waddle around the trunk up to a minimum height of 6' from grade. A single layer or more of orange plastic construction fencing is to be wrapped and secured around the straw waddle.

Signage should be placed on the protective tree fence no further than 30' apart. The signage should present the following information:

- The tree protection fence shall not be moved without authorization of the Project or City Arborist.
- Storage of building materials or soil is prohibited within the Tree Protection Zone.
- Construction or operation of construction equipment is prohibited within the tree protection zone.

In areas with many trees, the RPZ can be fenced as one unit, rather than separately for each tree.

Do not allow run off or spillage of damaging materials into the area below any tree canopy.

Do not store materials, stockpile soil or park or drive vehicles within the TPZ.

Do not cut, break, skin or bruise roots, branches, or trunks without first obtaining authorization from the city arborist.

Do not allow fires under and adjacent to trees.

Do not discharge exhaust into foliage.

Do not secure cable, chain or rope to trees or shrubs.

Do not trench, dig, or otherwise excavate within the dripline or TPZ of the tree(s) without first obtaining authorization from the city arborist.

Do not apply soil sterilant under pavement near existing trees.

Only excavation by hand, compressed air or hydro-vac shall be allowed within the dripline of trees.

Elevate Foliage: Where indicated, remove lower foliage from a tree to prevent limb breakage by equipment. Low foliage can usually be removed without harming the tree, unless more than 25% of the foliage is removed. Branches need to be removed at the anatomically correct location in order to prevent decay

organisms from entering the trunk. For this reason, a contractor who is an ISA Certified Arborist should perform all pruning on protected trees.⁸

Expose and Cut Roots: Breaking roots with a backhoe, or crushing them with a grader, causes significant injury, which may subject the roots to decay. Ripping roots may cause them to splinter toward the base of the tree, creating much more injury than a clean cut would make. At any location where the root zone of a tree will be impacted by a trench or a cut (including a cut required for a fill and compaction), the roots shall be exposed with either a backhoe digging radially to the trunk, by hand digging, or by a hydraulic air spade, and then cut cleanly with a sharp instrument, such as chainsaw with a carbide chain. Once the roots are severed, the area behind the cut should be moistened and mulched. A root protection fence should also be erected to protect the remaining roots, if it is not already in place. Further grading or backhoe work required outside the established RPZ can then continue without further protection measures.

Protect Roots in Deeper Trenches: The location of utilities on the site can be very detrimental to trees. Design the project to use as few trenches as possible, and to keep them away from the major trees to be protected. Wherever possible, in areas where trenches will be very deep, consider boring under the roots of the trees, rather than digging the trench through the roots. This technique can be quite useful for utility trenches and pipelines.

Route pipes outside of the area that is 10 times the diameter of the protected tree to avoid conflicts with roots. Where it is not possible to reroute pipes or trenches, the contractor shall bore beneath the dripline of the tree. The boring shall take place not less than 3' below the surface of the soil in order to avoid encountering feeder roots. Alternatively, the trench can be excavated using hand, pneumatic or hydro-vac techniques within the RPZ. The goal is to avoid damaging the roots while excavating. The pipes should be fed under the exposed roots. Trenches should be filled within 24 hours, but where this is not possible the side of the trench adjacent to the trees shall be kept shaded with 4 layers of dampened, untreated burlap, wetted as frequently as necessary to keep the burlap wet.

Protect Roots in Small Trenches: After all construction is complete on a site, it is not unusual for the landscape contractor to come in and sever a large number of "preserved" roots during the installation of irrigation systems. The Project Arborist must therefore approve the landscape and irrigation plans. The irrigation system needs to be designed so the main lines are located outside the root zone of major trees, and the secondary lines are either laid on the surface (drip systems), or carefully dug with a hydraulic or air spade, and the flexible pipe fed underneath the major roots.

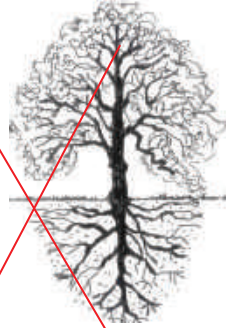
Design the irrigation system so it can slowly apply water (no more than ¼" to ½" of water per hour) over a longer period of time. This allows deep soaking of root zones. The system also needs to accommodate infrequent irrigation settings of once or twice a month, rather than several times a week.

Monitoring Tree Health During and After Construction: The Project Arborist should visit the site at least once a month during construction to be certain the tree protection measures are being followed, to monitor the health of impacted trees, and make recommendations as to irrigation or other needs.

⁸ International Society of Arboriculture (ISA), maintains a program of Certifying individuals. Each Certified Arborist has a number and must maintain continuing education credits to remain Certified.

Root Structure

The majority of a tree's roots are contained in a radius from the main trunk outward approximately two to three times the canopy of the tree. These roots are located in the top 6" to 3' of soil. It is a common misconception that a tree underground resembles the canopy (see Drawing A below). The correct root structure of a tree is in Drawing B. All plants' roots need both water and air for survival. Surface roots are a common phenomenon with trees grown in compacted soil. Poor canopy development or canopy decline in mature trees is often the result of inadequate root space and/or soil compaction.



Drawing A

Common misconception of where tree roots are assumed to be located



Drawing B

The reality of where roots are generally located

Structural Issues

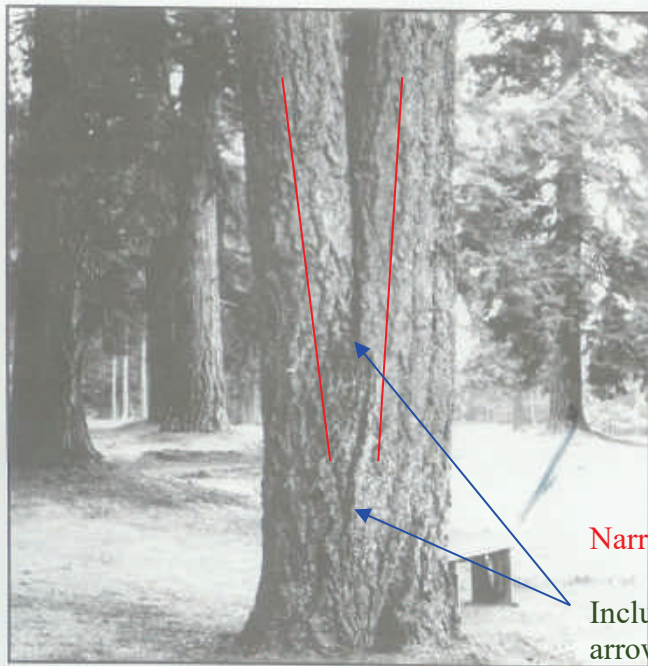
Limited space for canopy development produces poor structure in trees. The largest tree in a given area, which is 'shading' the other trees is considered Dominant. The 'shaded' trees are considered Suppressed. The following picture illustrates this point. Suppressed trees are more likely to become a potential hazard due to their poor structure.

Dominant Tree
Growth is upright
Canopy is balanced by limbs and foliage equally



Suppressed Tree
Canopy weight all to one side
Limbs and foliage grow away from dominant tree

Co-dominant leaders are another common structural problem in trees.



The tree in this picture has a co-dominant leader at about 3' and included bark up to 7 or 8'. Included bark occurs when two or more limbs have a narrow angle of attachment resulting in bark between the stems – instead of cell to cell structure. This is considered a critical defect in trees and is the cause of many failures.

Narrow Angle
Included Bark between the arrows

Figure 6. Codominant stems are inherently weak because the stems are of similar diameter.

Photo from Evaluation of Hazard Trees in Urban Areas by Nelda P. Matheny and James R. Clark, 1994 International Society of Arboriculture

Pruning Mature Trees for Risk Reduction

There are few good reasons to prune mature trees. Removal of deadwood, directional pruning, removal of decayed or damaged wood, and end-weight reduction as a method of mitigation for structural faults are the only reasons a mature tree should be pruned. Live wood over 3” should not be pruned unless absolutely necessary. Pruning cuts should be clean and correctly placed. Pruning should be done in accordance with the American National Standards Institute (ANSI) A300 standards. It is far better to use more small cuts than a few large cuts as small pruning wounds reduce risk while large wounds increase risk.

Pruning causes an open wound in the tree. Trees do not “heal” they compartmentalize. Any wound made today will always remain, but a healthy tree, in the absence of decay in the wound, will ‘cover it’ with callus tissue. Large, old pruning wounds with advanced decay are a likely failure point. Mature trees with large wounds are a high failure risk.

Overweight limbs are a common structural fault in suppressed trees. There are two remedial actions for overweight limbs (1) prune the limb to reduce the extension of the canopy, or (2) cable the limb to reduce movement. Cables do not hold weight they only stabilize the limb and require annual inspection.



Photo of another tree – not at this site.

Normal limb structure

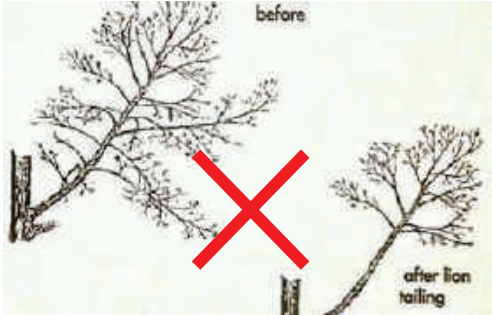
Over weight, reaching limb with main stem diameter small compared with amount of foliage present



Photo of another tree – not at this site

Lion's – Tailing is the pruning practice of removal of “an excessive number of inner and/or lower lateral branches from parent branches. Lion's tailing is not an acceptable pruning practice” ANSI A300 (part 1) 4.23. It increases the risk of failure.

Pruning – Cutting back trees changes their natural structure, while leaving trees in their natural form enhances longevity.



Arborist Classifications

There are different types of Arborists:

Tree Removal and/or Pruning Companies. These companies may be licensed by the State of California to do business, but they do not necessarily know anything about trees;

Arborists. Arborist is a broad term. It is intended to mean someone with specialized knowledge of trees but is often used to imply knowledge that is not there.

ISA Certified Arborist. An International Society of Arboriculture Certified Arborist is someone who has been trained and tested to have specialized knowledge of trees. You can look up certified arborists at the International Society of Arboriculture website: isa-arbor.org.

Consulting Arborist. An American Society of Consulting Arborists Registered Consulting Arborist is someone who has been trained and tested to have specialized knowledge of trees and trained and tested to provide high quality reports and documentation. You can look up registered consulting arborists at the American Society of Consulting Arborists website: <https://www.asca-consultants.org/>

Decay in Trees

Decay (in General): Fungi cause all decay of living trees. Decay is considered a disease because cell walls are altered, wood strength is affected, and living sapwood cells may be killed. Fungi decay wood by secreting enzymes. Different types of fungi cause different types of decay through the secretion of different chemical enzymes. Some decays, such as white rot, cause less wood strength loss than others because they first attack the lignin (causes cell walls to thicken and reduces susceptibility to decay and pest damage) secondarily the cellulose (another structural component in a cell walls). Others, such as soft rot, attack the cellulose chain and cause substantial losses in wood strength even in the initial stages of decay. Brown rot causes wood to become brittle and fractures easily with tension. Identification of internal decay in a tree is difficult because visible evidence may not be present.



According to Evaluation of Hazard Trees in Urban Areas (Matheny, 1994) decay is a critical factor in the stability of the tree. As decay progresses in the trunk, the stem becomes a hollow tube or cylinder rather than a solid rod. This change is not readily apparent to the casual observer. Trees require only a small amount of bark and wood to transport water, minerals and sugars. Interior heartwood can be eliminated (or degraded) to a great degree without compromising the transport process. Therefore, trees can contain significant amounts of decay without showing decline symptoms in the crown.



additional cells. The weakest of the vertical wall. Accordingly, decay progression inward at large are more than one pruning cut trunk of the tree, the likelihood of decay progression and the associated structural loss of integrity of the internal wood is high.

Compartmentalization of decay in trees is a biological process in which the cellular tissue around wounds is changed to inhibit fungal growth and provide a barrier against the spread of decay agents into the barrier zones is the formation of while a tree may be able to limit pruning cuts, in the event that there located vertically along the main

Oak Tree Impacts

Our native oak trees are easily damaged or killed by having the soil within the Critical Root Zone (CRZ) disturbed or compacted. All of the work initially performed around protected trees that will be saved should be done by people rather than by wheeled or track type tractors. Oaks are fragile giants that can take little change in soil grade, compaction, or warm season watering. Don't be fooled into believing that warm season watering has no adverse effects on native oaks. Decline and eventual death can take as long as 5-20 years with poor care and inappropriate watering. Oaks can live hundreds of years if treated properly during construction, as well as later with proper pruning, and the appropriate landscape/irrigation design.

APPENDIX 4 – APPRAISAL VALUE TABLE

Tree #	Species	trunk dia. (in.)	x-sect area (sq. in.)	Condition rating	Health rating	Structure rating	Form rating	Functional limitation	External limitation	Replacement species	nursery trunk dia (in.)	nursery x-sect area (sq. in)	Replacement tree cost (24" box)	unit tree cost (\$/sq. in.)	basic reproduction cost (\$)	Depreciated reproduction cost (\$)	Depreciated rep. cost rounded to \$100	Installed cost (\$)
1	Southern Magnolia	30	706.5	0.366666667	0.5	0.3	0.3	0.6	0.8	Southern Magnolia	2.2	3.8	252.63	66.49	46,976.65	8,267.89	8,300.00	8,550.00
2	Sweet Gum	15	176.625	0.4	0.5	0.4	0.3	0.6	0.8	Sweet Gum	1.69	2.24	252.63	112.78	19,919.99	3,824.64	3,800.00	4,050.00
3	Valley Oak	20	314	0.366666667	0.3	0.4	0.4	0.5	0.8	Valley Oak	1.69	2.24	252.63	112.78	35,413.31	5,193.95	5,200.00	5,450.00
4	Valley Oak	20	314	0.366666667	0.3	0.4	0.4	0.5	0.8	Valley Oak	1.69	2.24	252.63	112.78	35,413.31	5,193.95	5,200.00	5,450.00
5	Valley Oak	25	490.625	0.433333333	0.4	0.4	0.5	0.8	0.9	Valley Oak	1.69	2.24	252.63	112.78	55,333.30	17,263.99	17,300.00	17,550.00
6	Deodar Cedar	35	961.625	0.566666667	0.5	0.6	0.6	0.9	0.9	Deodar Cedar	2.2	3.8	252.63	66.49	63,940.44	29,348.66	29,300.00	29,550.00

*The value of the trees was determined using the Trunk Formula Method, described in the *Guide for Plant Appraisal*⁹, and on the *Species Classification and Group Assignment* published by the Western Chapter, International Society of Arboriculture (ISA).

⁹ Council of Tree and Landscape Appraisers, 2018. *Guide for Plant Appraisal*, 10th Edition. International Society of Arboriculture, Champaign, IL.



COMMUNITY DEVELOPMENT DEPT.

701 Laurel Street
Menlo Park, CA 94025
650.330.6704
2/28/2011

TREE PROTECTION SPECIFICATIONS

1. A 6” layer of coarse mulch or woodchips is to be placed beneath the dripline of the protected trees. Mulch is to be kept 12” from the trunk.
2. A protective barrier of 6’ chain link fencing shall be installed around the dripline of protected tree(s). The fencing can be moved within the dripline if authorized by the Project Arborist or City Arborist but not closer than 2’ from the trunk of any tree. Fence posts shall be 1.5” in diameter and are to be driven 2’ into the ground. The distance between posts shall not be more than 10’. This enclosed area is the Tree Protection Zone (TPZ).
3. Movable barriers of chain link fencing secured to cement blocks can be substituted for “fixed” fencing if the Project Arborist and City Arborist agree that the fencing will have to be moved to accommodate certain phases of construction. The builder may not move the fence without authorization from the Project Arborist or City Arborist.
4. Where the City Arborist or Project Arborist has determined that tree protection fencing will interfere with the safety of work crews, Tree Wrap may be used as an alternative form of tree protection. Wooden slats at least one inch thick are to be bound securely, edge to edge, around the trunk. A single layer or more of orange plastic construction fencing is to be wrapped and secured around the outside of the wooden slats. Major scaffold limbs may require protection as determined by the City Arborist or Project Arborist. Straw waddle may also be used as a trunk wrap by coiling the waddle around the trunk up to a minimum height of six feet from grade. A single layer or more of orange plastic construction fencing is to be wrapped and secured around the straw waddle.
5. **Avoid the following conditions.**
DO NOT:
 - a. Allow run off of spillage of damaging materials into the area below any tree canopy.
 - b. Store materials, stockpile soil, or park or drive vehicles within the TPZ.
 - c. Cut, break, skin, or bruise roots, branches, or trunks without first obtaining authorization from the City Arborist.
 - d. Allow fires under and adjacent to trees.
 - e. Discharge exhaust into foliage.
 - f. Secure cable, chain, or rope to trees or shrubs.
 - g. Trench, dig, or otherwise excavate within the dripline or TPZ of the tree(s) without first obtaining authorization from the City Arborist.
 - h. Apply soil sterilants under pavement near existing trees.
6. Only excavation by hand or compressed air shall be allowed within the dripline of trees. Machine trenching shall not be allowed.

7. Avoid injury to tree roots. When a ditching machine, which is being used outside of the dripline of trees, encounters roots smaller than 2", the wall of the trench adjacent to the trees shall be hand trimmed, making clear, clean cuts through the roots. All damaged, torn and cut roots shall be given a clean cut to remove ragged edges, which promote decay. Trenches shall be filled within 24 hours, but where this is not possible, the side of the trench adjacent to the trees shall be kept shaded with four layers of dampened, untreated burlap, wetted as frequently as necessary to keep the burlap wet. Roots 2" or larger, when encountered, shall be reported immediately to the Project Arborist, who will decide whether the Contractor may cut the root as mentioned above or shall excavate by hand or with compressed air under the root. Root is to be protected with dampened burlap.
8. Route pipes outside of the area that is 10 times the diameter of a protected tree to avoid conflict with roots.
9. Where it is not possible to reroute pipes or trenches, the contractor shall bore beneath the dripline of the tree. The boring shall take place not less than 3' below the surface of the soil in order to avoid encountering "feeder" roots.
10. Trees that have been identified in the arborist's report as being in poor health and/or posing a health or safety risk, may be removed or pruned by more than one-third, subject to approval of the required permit by the Planning Division. Pruning of existing limbs and roots shall only occur under the direction of a Certified Arborist.
11. Any damage due to construction activities shall be reported to the Project Arborist or City Arborist within six hours so that remedial action can be taken.
12. An ISA Certified Arborist or ASCA Registered Consulting Arborist shall be retained as the Project Arborist to monitor the tree protection specifications. The Project Arborist shall be responsible for the preservation of the designated trees. Should the builder fail to follow the tree protection specifications, it shall be the responsibility of the Project Arborist to report the matter to the City Arborist as an issue of non-compliance.
13. Violation of any of the above provisions may result in sanctions or other disciplinary action.

MONTHLY INSPECTIONS

It is required that the site arborist provide periodic inspections during construction. Four-week intervals would be sufficient to access and monitor the effectiveness of the Tree Protection Plan and to provide recommendations for any additional care or treatment.

APPENDIX 6 – PHOTOGRAPHS



Tree # 1 off-site



Tree #'s 3, 2 off-site







Tree #'s 12, 13



Tree # 14



Tree # 15



Tree #'s 17, 16



Turner, Christopher R

From: Turner, Christopher R
Sent: Monday, January 30, 2023 5:35 PM
To: Turner, Christopher R
Subject: FW: 440 University, Menlo Park, CA 94025



Christopher R. Turner

Associate Planner
City Hall - 1st Floor
701 Laurel St.
tel 650-330-6724

menlopark.gov

*Note our emails have changed to @menlopark.gov

From: elizabeth elizabethhouck.com [<mailto:elizabeth@elizabethhouck.com>]
Sent: Saturday, January 21, 2023 6:13 PM
To: _Planning Commission <planning.commission@menlopark.gov>
Cc: elhock@gmail.com
Subject: 440 University, Menlo Park, CA 94025

CAUTION: This email originated from outside of the organization. Unless you recognize the sender's email address and know the content is safe, DO NOT click links, open attachments or reply.

Dear Commissioners,

I need your help and advice. My house has three properties to my southwest along one fence line, one of which is 440 University - Thomas James Homes (TJH) is proposing a two story McMansion that will loom over my house and yard a mere 20 feet from my property at 883 Middle Avenue.

This will block my sunlight plane overshadowing my home and property.

I have reached out to TJH and they tell me this is within the city building requirements regarding the height and 20 foot setback and they steadfastly refuse to consider design alterations that would lessen the impacts. It would be my hope at the very least that the house be similar in size and setback to 444 Middle Avenue.

Do I have any recourse? Is there nothing I can do?

Your help and advice is greatly appreciated.

Elizabeth Houck
650 323-0313



STAFF REPORT

Planning Commission

Meeting Date:

2/6/2023

Staff Report Number:

23-011-PC

Public Hearing:

Consider and adopt a resolution to approve a use permit to demolish an existing one-story, single-family residence and one detached accessory building, and construct a new two-story residence on a substandard lot with regard to lot width, depth, and area in the R-1-U (Single Family Urban Residential) zoning district, at 167 McKendry Drive.

Recommendation

Staff recommends that the Planning Commission adopt a resolution approving a use permit to demolish an existing one-story, single-family residence and one detached accessory building, and construct a new two-story residence on a substandard lot with regard to lot width, depth, and area in the R-1-U (Single Family Urban Residential) zoning district. The draft resolution, including the recommended actions and conditions of approval, is included as Attachment A.

Policy Issues

Each use permit request is considered individually. The Planning Commission should consider whether the required use permit findings can be made for the proposal.

Background

Site location

Using McKendry Drive in the north-south orientation, the subject property is located on the western side of McKendry Drive, between Marmona Drive to the north and Blackburn Avenue to the south, in the Willows neighborhood. A location map is included as Attachment B.

Houses along McKendry Drive include both one- and two-story residences, developed in a variety of architectural styles, including ranch and craftsman. The neighborhood features predominantly single-family residences that are also in the R-1-U zoning district.

Analysis

Project description

The subject property is currently occupied by a one-story residence with a detached one-car garage. The property has a substandard lot width of 55 feet, where 65 feet is required, a substandard lot depth of 91 feet, where 100 feet is required, and a substandard lot area of 5,005 square feet, where 7,000 square feet

is required. The relatively narrow lot configuration results in the existing residence being nonconforming with regard to the left side and rear setbacks. The applicant is proposing to demolish the existing residence and detached garage and construct a new two-story, single-family residence, along with an attached two-car garage.

The proposed residence would include a total of four bedrooms and 4.5 bathrooms. The first floor of the proposed residence would include the attached garage, a bedroom, a bathroom, a powder room, and an open kitchen, dining room, and living room. The second floor would include three bedrooms and three bathrooms. The required parking for the residence would be provided by an attached two-car garage. The proposed two-car garage would be located in line with the front façade of the building and face the street.

The proposed residence would meet all Zoning Ordinance requirements for setbacks, lot coverage, floor area limit (FAL), daylight plane, parking, and height. Of particular note with regard to Zoning Ordinance requirements:

- The proposed floor area would be 2,774.5 square feet, where 2,800 square feet is the FAL for the site.
- The second floor would be limited in size relative to the development, with a floor area of 1,077.5 square feet, where 1,400 square feet is the maximum permitted.
- The proposed building coverage would be 1,743.9 square feet, approximately 34.8 percent of the lot area, where 35 percent is the maximum allowed.
- The proposed residence would be 26.7 feet in height, where 28 feet is the maximum allowed.
- The property contains a five-foot public utility easement (PUE) along the rear and left side property lines, and no structures are proposed in these areas.

The proposed residence would be set back 20 feet from the front property line and 24 feet from the rear property line, where a minimum 20-foot setback is required for both. The left side would have a 6.5-foot setback, and the right side would have an 8.8-foot setback. In the R-1-U zoning district, the minimum side setbacks are 10 percent of the lot width, but no less than five feet and no greater than 10 feet. As such, the required setback for each side of the property is 5.5 feet. The proposed second story would be stepped back from the first story on portions of the left sides, and would feature varied wall depths to minimize massing and increase separation from neighboring properties.

A data table summarizing parcel and project attributes is included as Attachment C. The project plans and the applicant's project description letter are included as Exhibits A and B within Attachment A, respectively.

Design and materials

The applicant states in their project description letter that the proposed new residence is designed in a modern farmhouse style. The exterior of the proposed residence would predominantly feature board and batten siding and trim and standing seam metal roofing. In addition, the applicant has stated that stained and painted wood lintels are proposed to soften the exterior and offer an accent feature.

The front façade features a generally centralized covered porch above the front entry, with a separate pathway into the residence, parallel with the driveway. The position of the garage massing and its facade are less impactful, as the porch and right portion of the front elevation are aligned on the same plane.

The windows and doors would be aluminum-clad with wood. The left-side elevation would feature four second-floor windows with sill heights of approximately three feet, six inches above the finished floor. The right-side elevation would feature two second-floor windows with sill heights of five feet above the finished floor, as well as a staircase containing a sill height of three feet, two inches above the stair landing. The windows with a sill height of five feet were a result of revisions by the applicant to increase privacy along the right-side elevation. According to the applicant, the adjacent neighbor expressed privacy concerns, and the applicant made adjustments along the right side to minimize those concerns. Proposed new trees along the right side and existing trees along the left side would also provide additional screening to address potential privacy concerns for neighboring properties on both sides of the subject property.

Staff believes that the scale, materials, and style of the proposed residence would result in a consistent aesthetic approach and are generally consistent with the broader neighborhood, given the similar architectural styles and sizes of structures in the area.

Flood zone

The subject property is located within the “AE” zone established by the Federal Emergency Management Agency (FEMA). Within this zone, flood-proofing techniques are required for new construction and substantial improvements of existing structures. Stated in general terms, the finished floor must be at least one foot above the base flood elevation (BFE). The Engineering Division has reviewed and tentatively approved the proposal for compliance with FEMA regulations. The sections (Plan Sheets A4.1, A4.2, and A4.3 in Attachment A, Exhibit A) show the BFE (52.2 feet) in relation to the existing average natural grade (approximately 51.54 feet) and the finished floor elevation (53.2 feet).

Trees and landscaping

The applicant has submitted an arborist report (Attachment D), detailing the species, size, and conditions of the nearby heritage and non-heritage trees. The report discusses the impacts of the proposed improvements and provides recommendations for tree maintenance and protection. As part of the project review process, the arborist report was reviewed by the City Arborist. Table 1 below summarizes the project trees by species, size, condition, and whether the trees are proposed to be preserved or removed.

Table 1: Project tree summary				
Tree Number	Species	Size (DBH, in inches)	Condition	Removal/Reason
1	Coast redwood	66 (heritage size)	Fair	To be preserved
2	Southern magnolia	15.1 (heritage size and street tree)	Poor	To be preserved
3	California bay laurel	7 (non-heritage size)	Fair	Removed due to demolition impacts
4	Grapefruit	12 (non-heritage size)	Fair	To be preserved
5	Almond	10 (non-heritage size)	Poor	To be preserved
6	Coast redwood	50 (heritage size)	Fair	To be preserved

* Of the three heritage trees, one is a street tree in front of the subject property, one is located in a neighboring property, and one is located on the subject property.

** Of the three non-heritage trees, two are located in neighboring properties and one is located on the subject property.

The applicant is also proposing to plant three strawberry trees on site, specifically along the right side of the property to increase privacy along the right elevation. To protect the heritage and non-heritage trees on site, the arborist report has identified such measures as tree protection fencing, exploratory trenching by hand, requiring arborist supervision for any construction within a tree protection zone (TPZ), revising plans due to root locations, requiring arborist supervision for the removal of roots greater than two inches in diameter, and placing geogrid or aggregate infill above tree roots. All recommended tree protection measures identified in the arborist report would be implemented and ensured as part of condition 1h.

Correspondence

The applicant states in their project description letter that the property owner has completed outreach efforts, which involved mailing plans and project details to neighbors. The applicant attached correspondence received from one neighbor to the project description letter, and describes feedback received from neighbors and steps the applicant has taken to address the feedback.

As of the writing of this report, staff has received no direct correspondence.

Conclusion

Staff believes that the design, scale, and materials of the proposed residence are generally compatible with the surrounding neighborhood, and would result in a consistent aesthetic approach. The architectural style would be generally attractive and well-proportioned, and the additional side setback distances and varied positioning of the second floor would help increase privacy while reducing the perception of mass. In addition, screening trees along the right side elevation would further enhance the privacy along that side. Staff recommends that the Planning Commission approve the proposed project.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 3 (Section 15303, "New Construction or Conversion of Small Structures") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Draft Planning Commission Resolution
 - Exhibits to Attachment A
 - A. Project Plans
 - B. Project Description Letter
 - C. Conditions of Approval
- B. Location Map
- C. Data Table
- D. Arborist Report

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings, and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:
Matt Pruter, Associate Planner

Report reviewed by:
Kyle Perata, Planning Manager

PLANNING COMMISSION RESOLUTION NO. 2023-XX**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF MENLO PARK APPROVING A USE PERMIT FOR THE DEMOLITION OF AN EXISTING ONE-STORY, SINGLE-FAMILY RESIDENCE AND ONE DETACHED ACCESSORY BUILDING AND CONSTRUCTION OF A NEW TWO-STORY, SINGLE-FAMILY RESIDENCE ON A SUBSTANDARD LOT WITH REGARD TO MINIMUM LOT WIDTH, DEPTH, AND AREA IN THE R-1-U (SINGLE FAMILY URBAN RESIDENTIAL) ZONING DISTRICT**

WHEREAS, the City of Menlo Park (“City”) received an application requesting to demolish an existing one-story, single-family residence, and detached accessory building, and construct a new two-story residence on a substandard lot with regard to minimum lot width, depth, and area in the Single Family Urban Residential (R-1-U) zoning district (collectively, the “Project”) from Eiki Tanaka (“Applicant”), on behalf of the property owner Ryan Chang (“Owner”), located at 167 McKendry Drive (APN 062-311-600) (“Property”). The Project use permit is depicted in and subject to the development plans and project description letter, which are attached hereto as Exhibit A and Exhibit B, respectively, and incorporated herein by this reference; and

WHEREAS, the Property is located in the Single Family Urban Residential (R-1-U) district. The R-1-U district supports single-family residential uses; and

WHEREAS, the proposed Project complies with all objective standards of the R-1-U district; and

WHEREAS, the proposed Project was reviewed by the Engineering Division and found to be in compliance with City standards; and

WHEREAS, the Applicant submitted an arborist report prepared by Davey Resource Group, Inc., which was reviewed by the City Arborist and found to be in compliance with the Heritage Tree Ordinance and proposes mitigation measures to adequately protect heritage trees in the vicinity of the project; and

WHEREAS, the Project, requires discretionary actions by the City as summarized above, and therefore the California Environmental Quality Act (“CEQA,” Public Resources Code Section §21000 et seq.) and CEQA Guidelines (Cal. Code of Regulations, Title 14, §15000 et seq.) require analysis and a determination regarding the Project’s environmental impacts; and

WHEREAS, the City is the lead agency, as defined by CEQA and the CEQA Guidelines, and is therefore responsible for the preparation, consideration, certification, and approval of environmental documents for the Project; and

WHEREAS, the Project is categorically except from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures); and

WHEREAS, all required public notices and public hearings were duly given and held according to law; and

WHEREAS, at a duly and properly noticed public hearing held on February 6, 2023, the Planning Commission fully reviewed, considered, and evaluated the whole of the record including all public and written comments, pertinent information, documents and plans, prior to taking action regarding the Project.

NOW, THEREFORE, THE MENLO PARK PLANNING COMMISSION HEREBY RESOLVES AS FOLLOWS:

Section 1. Recitals. The Planning Commission has considered the full record before it, which may include but is not limited to such things as the staff report, public testimony, and other materials and evidence submitted or provided, and the Planning Commission finds the foregoing recitals are true and correct, and they are hereby incorporated by reference into this Resolution.

Section 2. Conditional Use Permit Findings. The Planning Commission of the City of Menlo Park does hereby make the following Findings:

The approval of the use permit for the construction of new two-story residence on a substandard lot is granted based on the following findings which are made pursuant to Menlo Park Municipal Code Section 16.82.030:

1. That the establishment, maintenance, or operation of the use applied for will, under the circumstance of the particular case, not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing in the neighborhood of such proposed use, or injurious or detrimental to property and improvements in the neighborhood or the general welfare of the city because:
 - a. Consideration and due regard were given to the nature and condition of all adjacent uses and structures, and to general plans for the area in question and surrounding areas, and impact of the application hereon; in that, the proposed use permit is consistent with the R-1-U zoning district and the General Plan because two-story residences are allowed to be constructed on substandard lots subject to granting of a use permit and provided that the proposed residence conforms to applicable zoning standards, including, but not limited to, minimum setbacks, maximum floor area limit, and maximum building coverage.
 - b. The proposed residence would include the required number of off-street parking spaces because one covered and one uncovered parking space would be required at a minimum, and two covered parking spaces are provided.

- c. The proposed Project is designed to meet all the applicable codes and ordinances of the City of Menlo Park Municipal Code and the Commission concludes that the Project would not be detrimental to the health, safety, and welfare of the surrounding community as the new residence would be located in a single-family neighborhood and designed such that privacy concerns would be addressed through right-side, left-side, and rear setbacks greater than the minimum required setbacks in the R-1-U district.

Section 3. Conditional Use Permit. The Planning Commission approves Use Permit No. PLN2022-000014, which use permit is depicted in and subject to the development plans and project description letter, which are attached hereto and incorporated herein by this reference as Exhibit A and Exhibit B, respectively. The Use Permit is conditioned in conformance with the conditions attached hereto and incorporated herein by this reference as Exhibit C.

Section 4. ENVIRONMENTAL REVIEW. The Planning Commission makes the following findings, based on its independent judgment after considering the Project, and having reviewed and taken into consideration all written and oral information submitted in this matter:

- A. The Project is categorically except from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures)

Section 5. SEVERABILITY

If any term, provision, or portion of these findings or the application of these findings to a particular situation is held by a court to be invalid, void or unenforceable, the remaining provisions of these findings, or their application to other actions related to the Project, shall continue in full force and effect unless amended or modified by the City.

I, Corinna Sandmeier, Principal Planner and Planning Commission Liaison of the City of Menlo Park, do hereby certify that the above and foregoing Planning Commission Resolution was duly and regularly passed and adopted at a meeting by said Planning Commission on February 6, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS THEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this 6th day of February, 2023

Corinna Sandmeier
Principal Planner and Planning Commission Liaison
City of Menlo Park

Exhibits

- A. Project Plans
- B. Project Description Letter
- C. Conditions of Approval

DRAWING INDEX

- ARCHITECTURAL:
- A0.0 PROJECT INFORMATION
 - A1.0 AREA PLAN / STREETScape ELEVATION
 - A1.1 EXISTING / PROPOSED SITE PLANS
 - A2.0 PROPOSED FLOOR PLAN - 1ST FLOOR
 - A2.1 PROPOSED FLOOR PLAN - 2ND FLOOR
 - A2.2 PROPOSED ROOF PLAN
 - A2.3 FLOOR AREA / IMPERVIOUS SURFACE CALCULATION
 - A3.0 PROPOSED ELEVATIONS - FRONT / REAR
 - A3.1 PROPOSED ELEVATIONS - LEFT SIDE / RIGHT SIDE
 - A4.1 BUILDING SECTIONS
 - A4.2 BUILDING SECTIONS
 - A4.3 BUILDING SECTIONS

- CIVIL:
- CD BOUNDARY AND TOPOGRAPHIC SURVEY

LOCATION MAP



PROJECT TEAM

Owner:
 Ryan Chang
 167 McKendry Drive, Menlo Park, CA 94025
 Contact: Kelvin Chan
 (t) 650.862.8682 (e) kchan9775@gmail.com

Architect:
 Studio 02, Inc.
 1136 E Hamilton Ave #100, Campbell, CA 95008
 Contact: Eiki Tanaka
 (t) 408.730.8877 (e) eiki@studio02.net

Surveyor:
 Triad/Holmes Associates, Inc.
 777 Woodside Road, Suite #2A, Redwood City, CA 94061
 (t) 650.366.0216 (e) sf@thainc.com

Arboret:
 Davey Resource Group
 PO Box 5321, Larkspur, CA 94977
 Contact: Matthew Thomas
 (t) 408.805.9511 (e) matthew.thomas@davey.com

PROJECT NOTES

1. WORK IN THE PUBLIC RIGHT-OF-WAY: ANY WORK PROPOSED IN THE PUBLIC RIGHT-OF-WAY, SUCH AS SIDEWALK REPLACEMENT, DRIVEWAY APPROACH, OR UTILITY LATERALS, TO BE DONE PER CITY OF MENLO PARK STANDARDS AND CONTRACTOR TO PERFORM WORK MUST OBTAIN AN 'EXCAVATION PERMIT' FROM PUBLIC WORKS. SEE CIVIL PLANS AND SITE PLAN ON A1.
2. APPROVED NUMBERS OR ADDRESS SHALL BE PLACED IN SUCH A POSITION AS TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE STREET FRONTING THE PROPERTY. SAID NUMBERS SHALL CONTRAST WITH THEIR BACKGROUND. ADDRESS NUMBERS SHALL BE ILLUMINATED AND SIZED AS FOLLOWS:
 - a. WHEN THE STRUCTURE IS BETWEEN THIRTY-SIX (36) AND FIFTY (50) FEET FROM THE ROAD OR OTHER EMERGENCY MEANS OF ACCESS, A MINIMUM OF ONE-HALF INCH [0.5"] STROKE BY SIX INCHES [6"] HIGH IS REQUIRED.
 - b. WHEN THE STRUCTURE IS FIFTY (50) OR MORE FEET FROM THE ROAD OR OTHER EMERGENCY MEANS OF ACCESS, A MINIMUM OF ONE INCH [1"] STROKE BY NINE INCHES [9"] HIGH IS REQUIRED.

FIRE PROTECTION NOTES

1. SHOP DRAWINGS FOR FIRE SPRINKLER SYSTEM TO BE SUBMITTED BY LICENSED FIRE SPRINKLER CONTRACTOR AND APPROVED UNDER A SEPARATE PERMIT
2. FIRE SPRINKLERS REQUIRED, OBTAIN SEPARATE FIRE PERMIT. DESIGN AND INSTALL IN ACCORDANCE WITH NFPA 13D, 2016, AND CMV RESIDENTIAL FIRE SPRINKLER SYSTEM REQUIREMENTS (SEE CITY WEBSITE UNDER 'FIRE HANDOUTS'). COORDINATE WATER METER AND WATER MAIN SIZES WITH APPROVED FIRE SPRINKLER SHOP DRAWINGS. (MINIMUM 1" METER AND 1" SERVICE UNLESS HYDRAULIC CALCULATIONS PROVE EXISTING ADEQUATE)
3. HOMES, INCLUDING ATTACHED GARAGES, ON LOTS SHALL BE PROVIDED WITH AN APPROVED AUTOMATIC FIRE SPRINKLER SYSTEM IN ACCORDANCE WITH NFPA 13D. THE DEVELOPER AND CONTRACTOR SHALL ENSURE THAT THE UNDERGROUND WATER LINES AND WATER METERS ARE SIZED TO ACCOMMODATE THE AUTOMATIC FIRE SPRINKLER SYSTEMS. COORDINATE WITH THE FIRE SPRINKLER SYSTEM CONTRACTOR.
4. FIRE SERVICE INSTALLATION - THE FIRE SERVICES, INCLUDING WATER SERVICES SUPPLYING NFPA 13D FIRE SPRINKLERS, SHOWN ON THESE PLANS ARE PRELIMINARY AND SHALL NOT BE INSTALLED UNTIL AFTER THE FIRE SPRINKLER PLANS HAVE BEEN APPROVED BY THE CITY. IF THE FIRE SPRINKLER PLANS REQUIRE CHANGES TO THE UTILITIES SHOWN ON THESE PLANS, REVISIONS TO THESE PLANS MUST BE APPROVED BY THE PUBLIC WORKS DEPARTMENT PRIOR TO THE INSTALLATION OF THE FIRE UTILITIES

PROJECT DATA

PROJECT DESCRIPTION:
 New two-story single-family residence with attached garage on an existing single-family property. Existing one-story single-family home and detached garage to be demolished. Scope of work:
 1. Demolish existing one-story single family residence and detached garage
 2. New two-story single-family residence
 3. Existing utilities to be disconnected, stubbed at property line, reconnected to new residence
 4. New residential fire sprinklers
 5. New driveway - existing driveway to be removed
 6. Existing heritage and street trees to be protected

Project Address: 167 McKendry Drive
Lot / Block Number: Lot 174, 25 Maps 38
APN Number: 062-311-600
Zone Designation: R1U SUBSTANDARD LOT
Occupancy: R-3/U
Construction Type: V-B - SPRINKLERED

Lot Area: 5,005 sf (7,000 sf min.)
Lot Width: 55' (60' min.)
Lot Depth: 91' (100' min.)
Flood Zone: AE (BFE 52.20)
Base Flood Elevation: 52.20
Design Flood Elevation: 53.20

FEMA Note:
 The project will be designed to comply with the City's Flood Damage Prevention Ordinance, Chapter 12, Section 42

	EXISTING	PROPOSED	ALLOWED / REQUIRED
BUILDING FLOOR AREA			
1st Floor:	1,385.8 sf	1,392.7 sf	-
Garage:	245.1 sf	454.3 sf	-
2nd Floor:	-	1,077.5 sf	1,400 sf (50% FAL)
Total:	1,630.9 sf	2,774.5 sf	2,800 sf FAL
BUILDING COVERAGE:	1,630.9 sf 32.6 %	1,743.9 sf 34.8 %	1,751.8 sf MAX. 35% MAX.
BUILDING HEIGHT:	-	26'-8"	28'-0" MAX.
PARKING:	1 COVERED / 1 UNCOVERED	2 COVERED	2 (1 COVERED) MIN.
NO. BEDROOMS/BATHROOMS	2 BD / 1 BA	4 BD / 4.5 BA	-
SETBACKS:			
FRONT:	24'-10"	20'-0" (1ST) / 23'-6" (2ND)	20' MIN.
REAR:	18'-3"	24'-0" (1ST) / 26'-0" (2ND)	20' MIN.
RIGHT SIDE:	14'-2"	8'-10" (1ST) / 8'-10" (2ND)	5'-6" MIN. (10% LOT WIDTH)
LEFT SIDE:	4'-10"	6'-6" (1ST) / 13'-2" (2ND)	5'-6" MIN. (10% LOT WIDTH)

CODE REFERENCE

2019 CBC (2018 IBC)	2019 CMC (2018 UMC)	2019 CPC (2018 UPC)
2019 CFC (2018 IFC)	2019 CEC (2018 NEC)	2019 CRC (2018 IRC)
2019 CALIFORNIA GREEN BUILDING CODE	TITLE-24 ENERGY	2019 CALIFORNIA ENERGY CODE
CITY OF MENLO PARK MUNICIPAL CODE		



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NEW TWO-STORY RESIDENCE
 FOR
**MENLO PARK
 RESIDENCE**
 167 MCKENDRY DRIVE
 MENLO PARK, CA 94025

- 03.04.2022 USE PERMIT SUBMITTAL
- 08.30.2022 RESPONSE TO UP COMMENTS #1
- 12.14.2022 RESPONSE TO UP COMMENTS #2

PROJECT NO. : 21-7650

PROJECT INFORMATION

A0.0

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FRONT - RIGHT SIDE



FRONT



DETACHED GARAGE



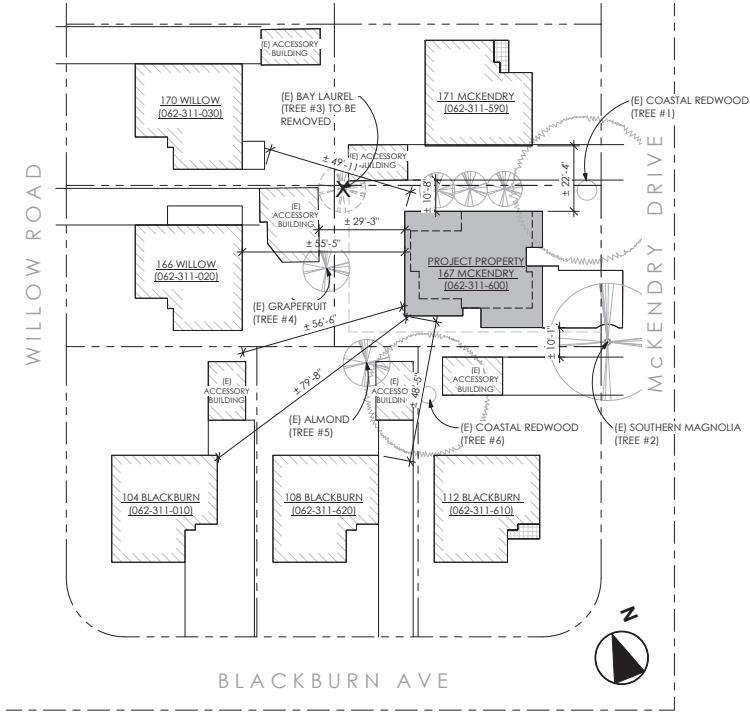
RIGHT SIDE



REAR - LEFT SIDE



REAR



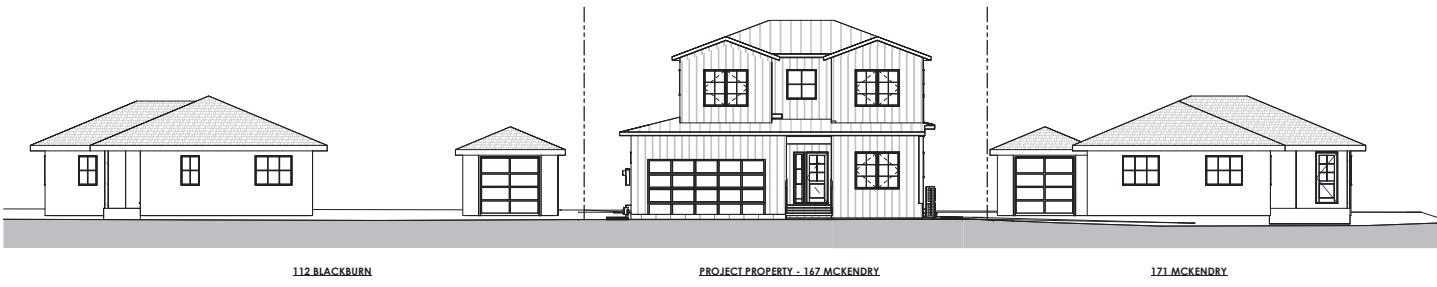
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EXISTING PHOTOS 1/2" = 1'-0" 2

AREA PLAN 1" = 20'-0" 1

PROJECT NO.: 21-7650



112 BLACKBURN

PROJECT PROPERTY - 167 MCKENDRY

171 MCKENDRY



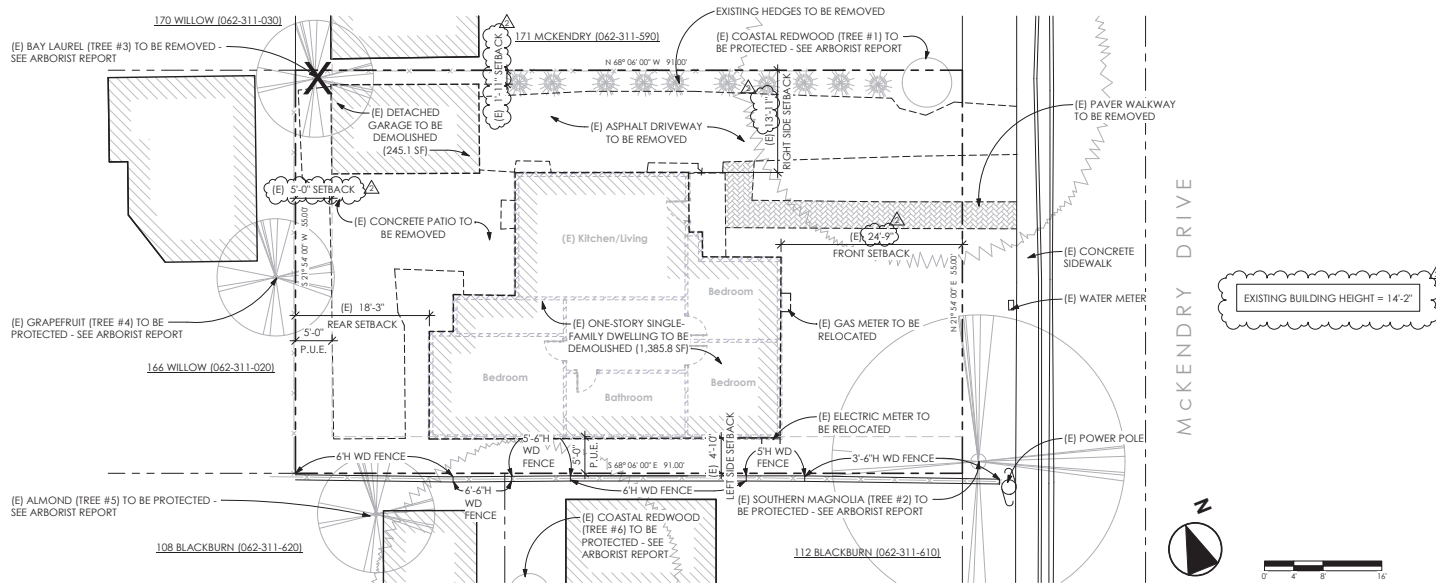
STREETSCAPE ELEVATION 1/8" = 1'-0" 3

AREA PLAN / STREETSCAPE ELEVATION



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EXISTING SITE PLAN 1/8" = 1'-0" 1

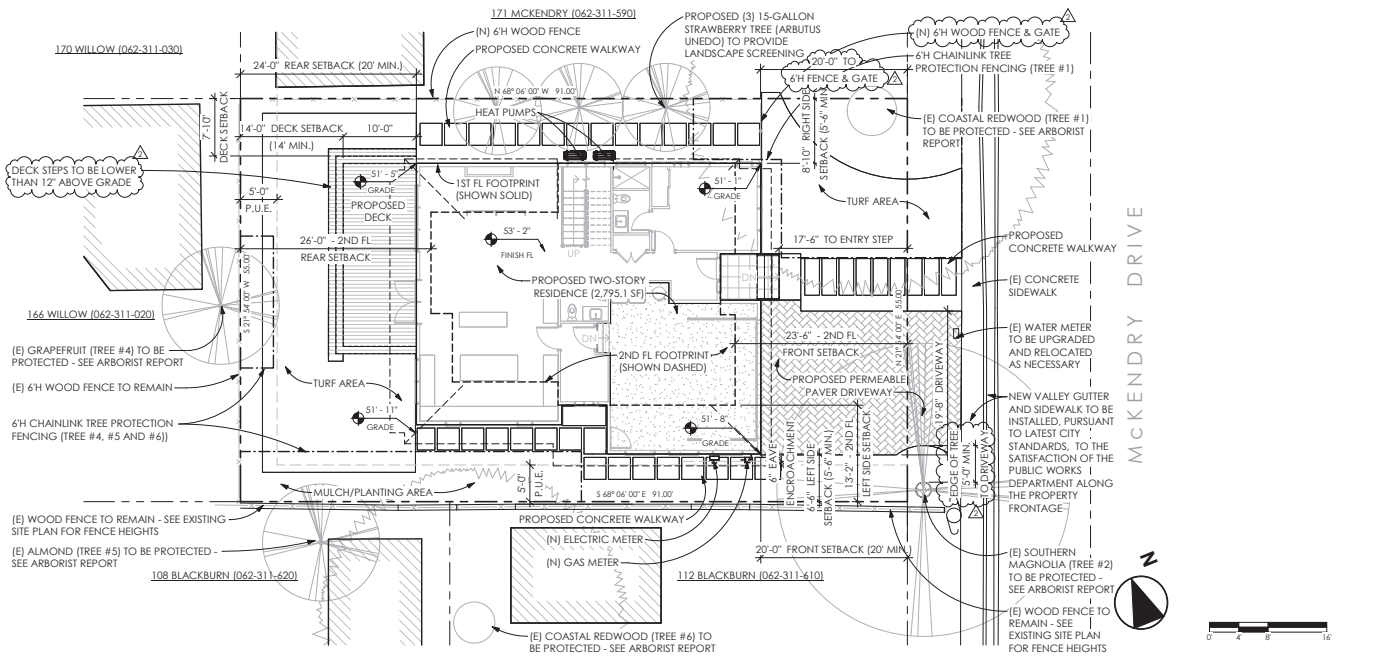
NOTE:
FOR ANY RESIDENTIAL PROPERTY, SOUND SHALL NOT EXCEED 60 dBA DURING THE DAYTIME HOURS OR 50 dBA DURING THE NIGHTTIME HOURS AT THE NEAREST RESIDENTIAL PROPERTY LINE

SITE ANALYSIS

LOT AREA: 5,005 SF
 ALLOWABLE FLOOR AREA: 2,800 SF
 PROPOSED FIRST FLOOR AREA: 1,709.1 SF
 PROPOSED SECOND FLOOR AREA: 1,075.4 SF
 TOTAL PROPOSED FLOOR AREA: 2,784.5 SF
 LAND COVERED BY STRUCTURES: 34.8%
 LANDSCAPING: 40.5%
 PAVING SURFACE/DECKS: 24.7%
 PARKING: 2 COVERED

AVERAGE GRADE CALCULATION

REFER TO SITE PLAN FOR SPOT ELEVATIONS
 FRONT RIGHT CORNER: 51'-1"
 FRONT LEFT CORNER: 51'-8"
 REAR RIGHT CORNER: 51'-6"
 REAR LEFT CORNER: 51'-11"
 AVERAGE GRADE = 51'-6 1/2"



PROPOSED SITE PLAN 1/8" = 1'-0" 2



NEW TWO-STORY RESIDENCE FOR MENLO PARK RESIDENCE
 167 MCKENDRY DRIVE
 MENLO PARK, CA 94025

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PROJECT NO.: 21-7650

EXISTING / PROPOSED SITE PLANS

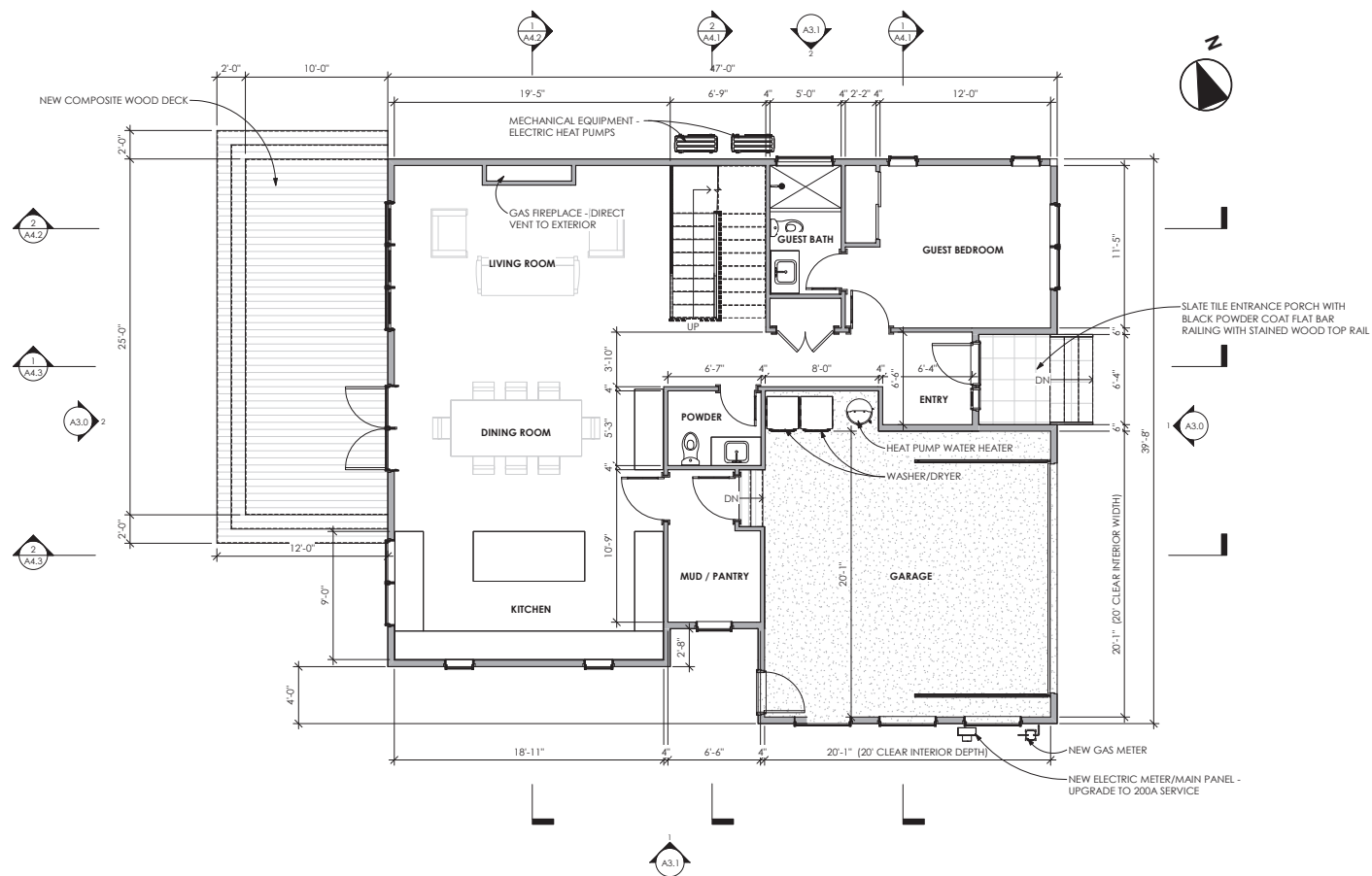


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**NEW TWO-STORY RESIDENCE
 FOR
 MENLO PARK
 RESIDENCE
 167 MCKENDRY DRIVE
 MENLO PARK, CA 94025**

03.04.2022 USE PERMIT SUBMITTAL
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PROJECT NO. : 21-7650

PROPOSED FLOOR PLAN - 1ST FLOOR



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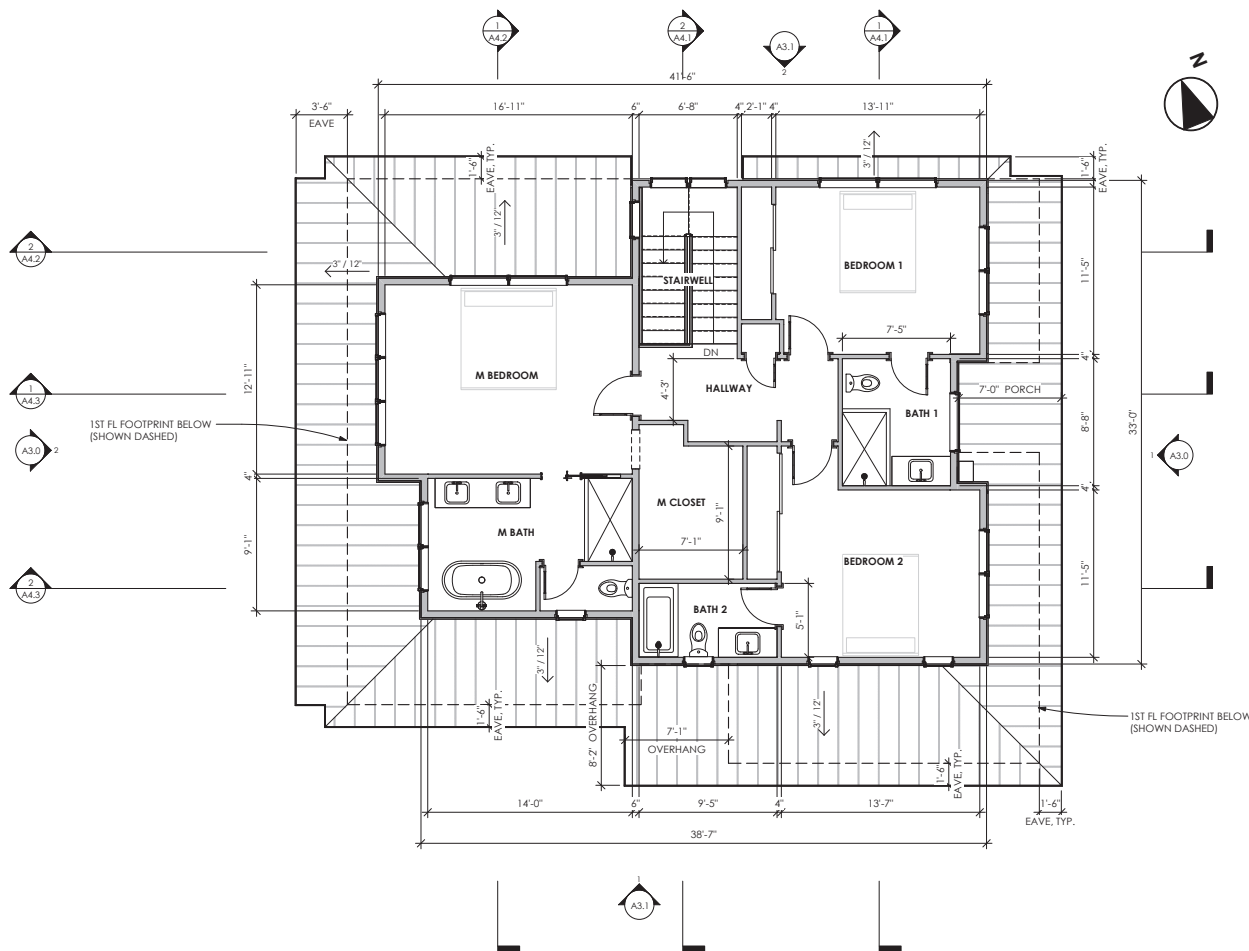
PROPOSED 1ST FLOOR PLAN 1/4" = 1'-0" 1





NEW TWO-STORY RESIDENCE
 FOR
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 RESIDENCE**
 167 MCKENDRY DRIVE
 MENLO PARK, CA 94025

03.04.2022 USE PERMIT SUBMITTAL
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PROPOSED 2ND FLOOR PLAN 1/4" = 1'-0" 1

PROPOSED FLOOR PLAN - 2ND FLOOR



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FLOOR AREA CALCULATION - 1ST FLOOR

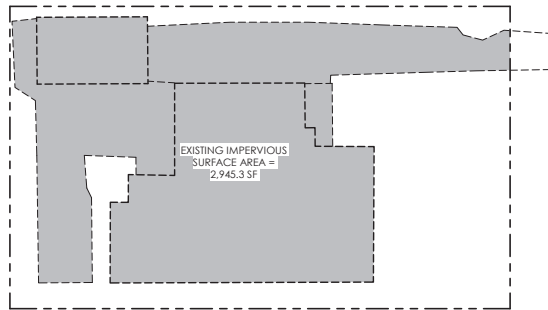
	DIMENSIONS	AREA (SF)
A	27'-2" x 12'-4"	335.1 SF
B	6'-9" x 6'-4"	42.8 SF
C	8'-9" x 3'-8"	32.1 SF
D	8'-2" x 20'-5"	127.4 SF
E	(19'-10" x 35'-8") - (6'-7" x 1'-10")	695.3 SF
F (GARAGE)	8'-9" x 2'-8"	23.3 SF
G (GARAGE)	21'-0" x 21'-0"	441.0 SF
H (PORCH)	6'-4" x 6'-4"	41.0 SF
P	4'-7" x 1'-10"	12.1 SF
TOTAL 1ST FLOOR AREA:		1497.0 SF

BUILDING COVERAGE CALCULATION

	DIMENSIONS	AREA (SF)
A	27'-2" x 12'-4"	335.1 SF
B	6'-9" x 6'-4"	42.8 SF
C	8'-9" x 3'-8"	32.1 SF
D	8'-2" x 20'-5"	127.4 SF
E	(19'-10" x 35'-8") - (6'-7" x 1'-10")	695.3 SF
F (GARAGE)	8'-9" x 2'-8"	23.3 SF
G (GARAGE)	21'-0" x 21'-0"	441.0 SF
H (PORCH)	6'-4" x 6'-4"	41.0 SF
P	4'-7" x 1'-10"	12.1 SF
TOTAL BUILDING COVERAGE:		1743.9 SF

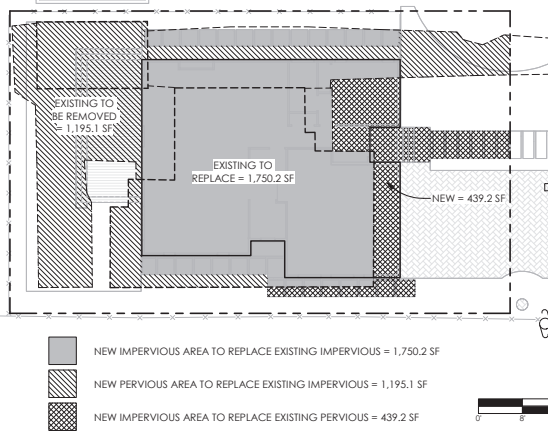
FLOOR AREA CALCULATION - 2ND FLOOR

	DIMENSIONS	AREA (SF)
I	17'-0" x 12'-4"	209.8 SF
J	22'-2" x 8'-4"	184.7 SF
K	24'-2" x 12'-4"	298.1 SF
L	7'-2" x 1'-2"	8.5 SF
M	17'-10" x 4'-6"	79.7 SF
N	17'-4" x 9'-4"	162.2 SF
O	14'-5" x 9'-4"	134.6 SF
TOTAL 2ND FLOOR AREA:		1077.5 SF



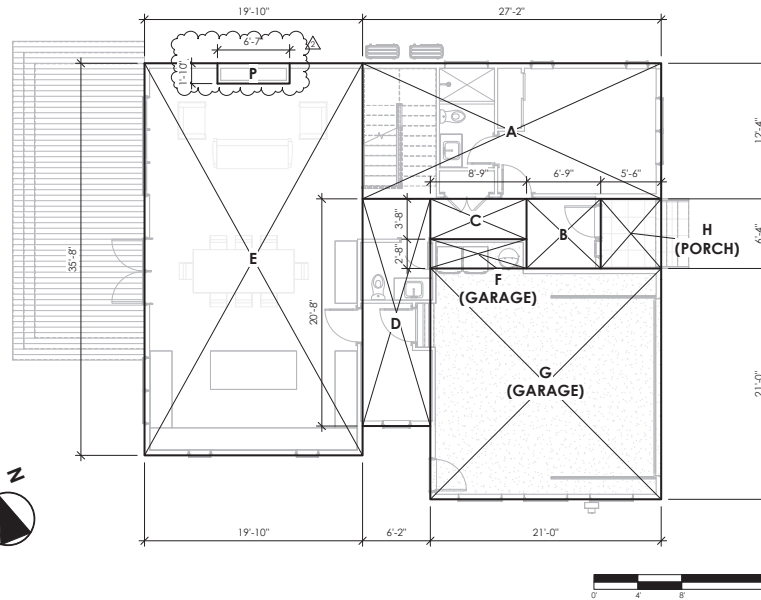
EXISTING IMPERVIOUS AREA = 2,945.3 SF

IMPERVIOUS SURFACE DIAGRAM - EXISTING 3/32" = 1'-0" 3

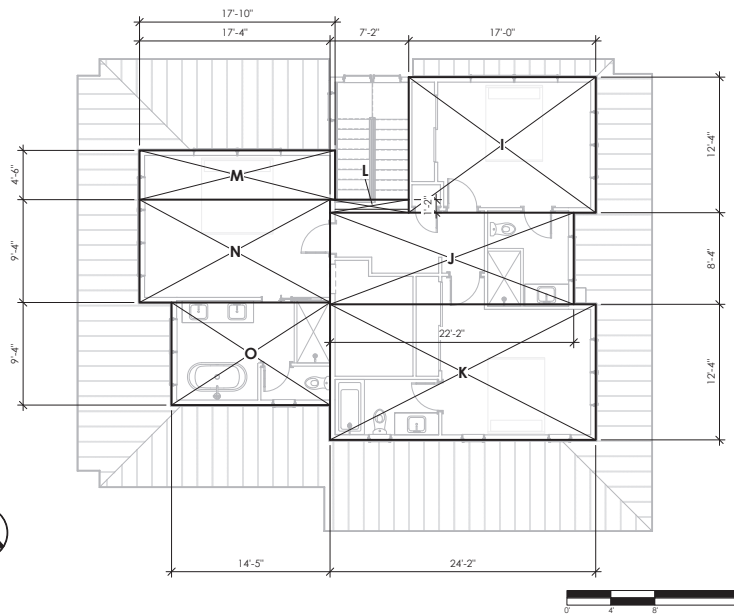


NEW IMPERVIOUS AREA TO REPLACE EXISTING IMPERVIOUS = 1,750.2 SF
 NEW IMPERVIOUS AREA TO REPLACE EXISTING IMPERVIOUS = 1,195.1 SF
 NEW IMPERVIOUS AREA TO REPLACE EXISTING IMPERVIOUS = 439.2 SF

IMPERVIOUS SURFACE DIAGRAM - NEW 3/32" = 1'-0" 4



AREA CALCULATION DIAGRAM - 1ST FLOOR 3/16" = 1'-0" 1



AREA CALCULATION DIAGRAM - 2ND FLOOR 3/16" = 1'-0" 2



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PROJECT NO.: 21-7650

FLOOR AREA / IMPERVIOUS
 SURFACE CALCULATION



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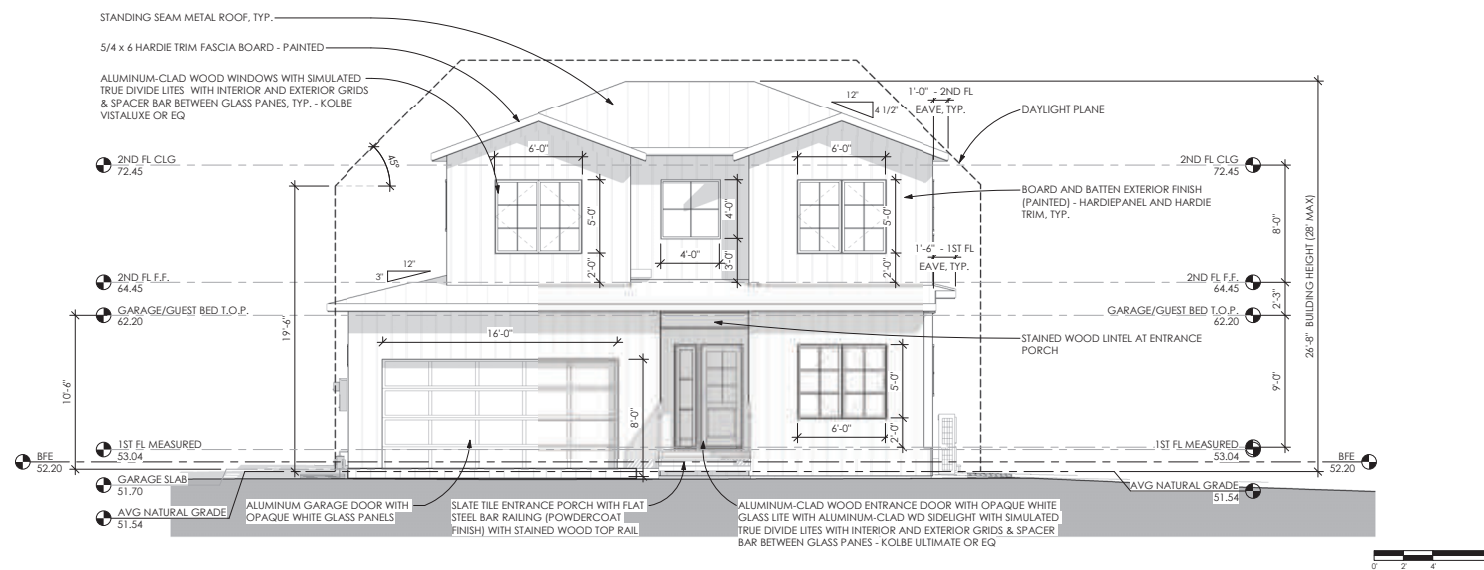
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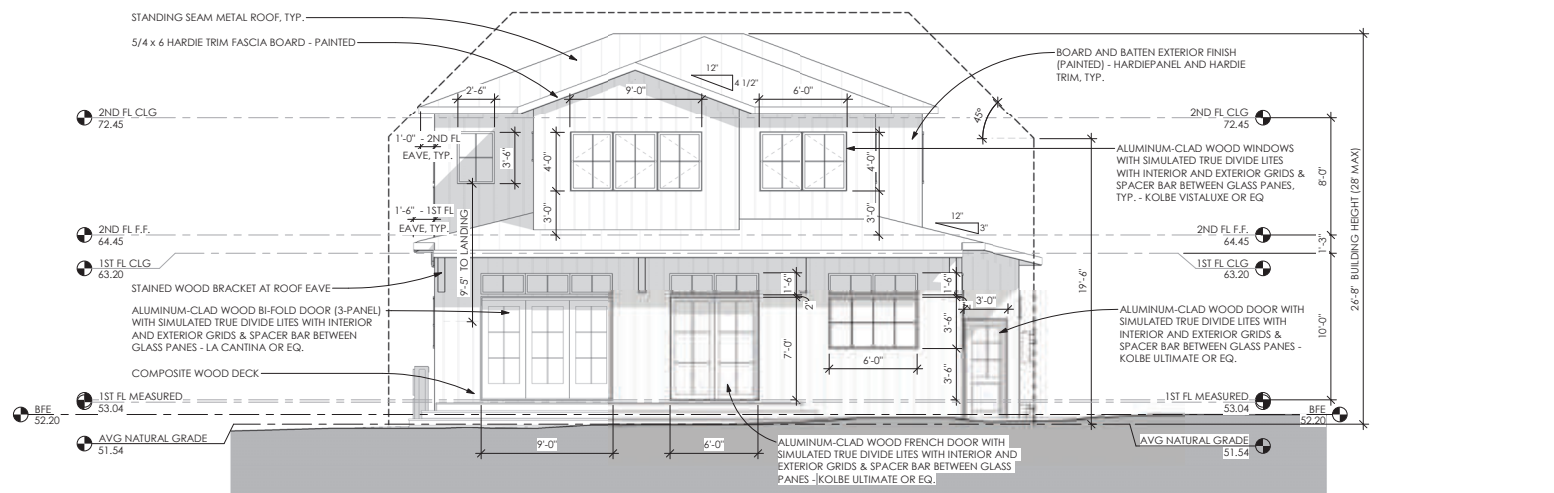
NEW TWO-STORY RESIDENCE

FOR
**MENLO PARK
 RESIDENCE**
 167 MCKENDRY DRIVE
 MENLO PARK, CA 94025



PROPOSED FRONT (EAST) ELEVATION 1/4" = 1'-0" 1

03.04.2022 USE PERMIT SUBMITTAL
 08.30.2022 RESPONSE TO LIP COMMENTS #1



PROPOSED REAR (WEST) ELEVATION 1/4" = 1'-0" 2

PROJECT NO. : 21-750

PROPOSED ELEVATIONS -
 FRONT / REAR



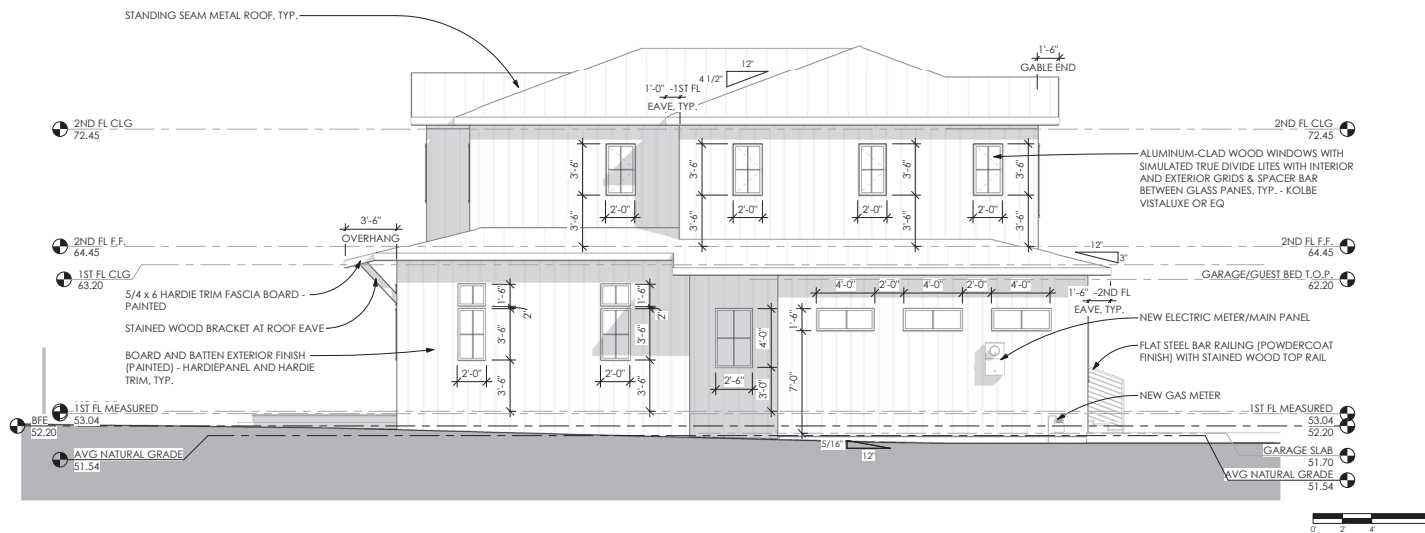
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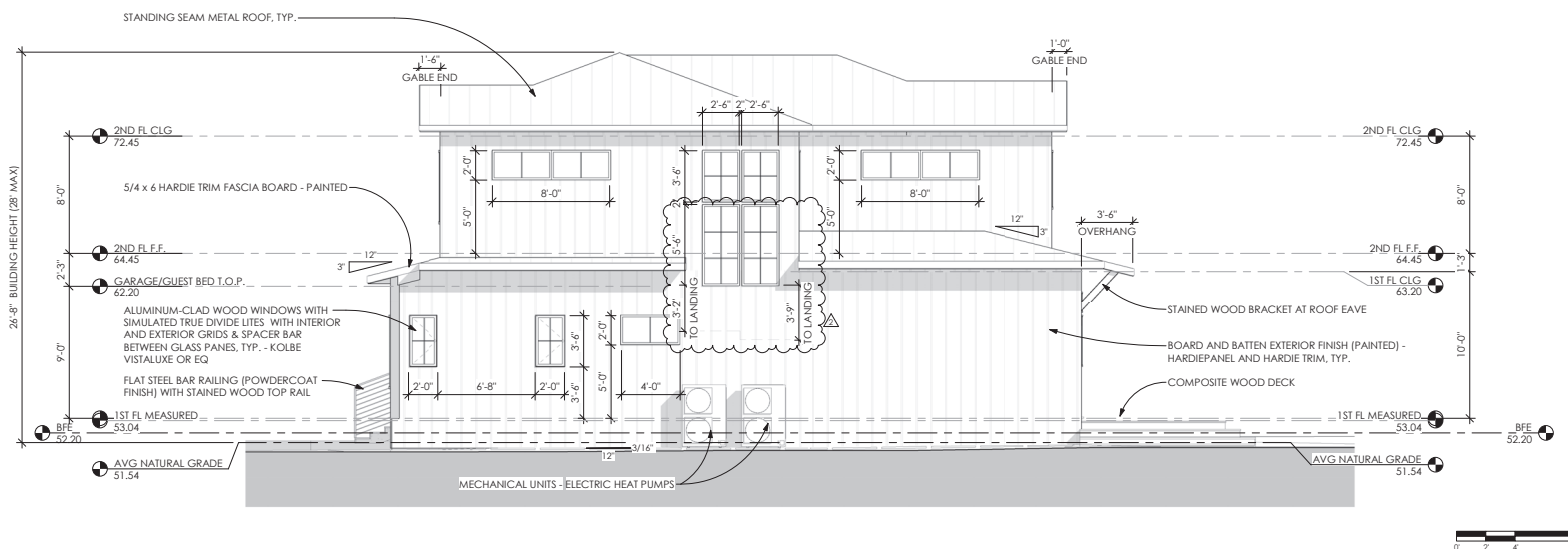


NEW TWO-STORY RESIDENCE

FOR
MENLO PARK RESIDENCE
 167 MCKENDRY DRIVE
 MENLO PARK, CA 94025



PROPOSED LEFT SIDE (SOUTH) ELEVATION 1/4" = 1'-0" 1



PROPOSED RIGHT SIDE (NORTH) ELEVATION 1/4" = 1'-0" 2

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PROJECT NO. : 21-7650

PROPOSED ELEVATIONS - LEFT SIDE / RIGHT SIDE



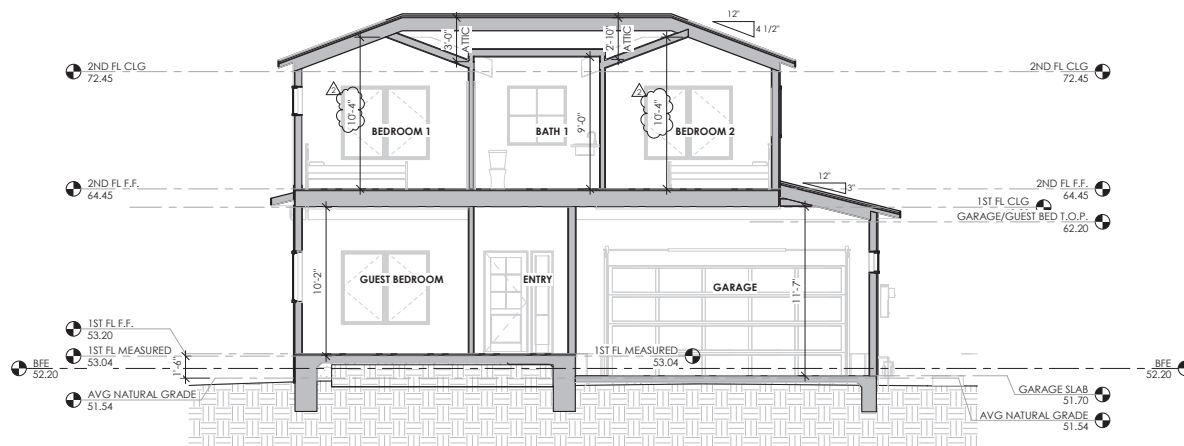
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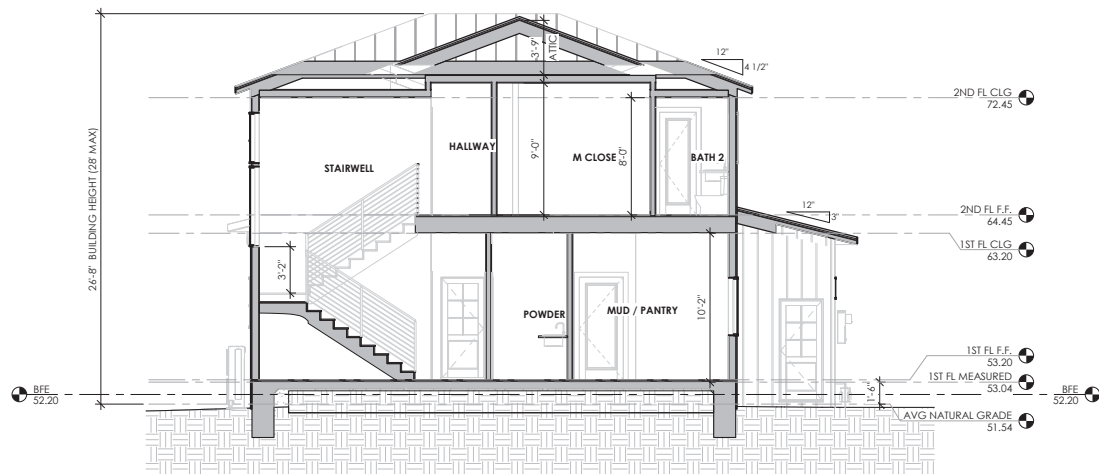


NEW TWO-STORY RESIDENCE

FOR
**MENLO PARK
RESIDENCE**
167 MCKENDRY DRIVE
MENLO PARK, CA 94025



BUILDING SECTION A 1/4" = 1'-0" 1



BUILDING SECTION B 1/4" = 1'-0" 2

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BUILDING SECTIONS

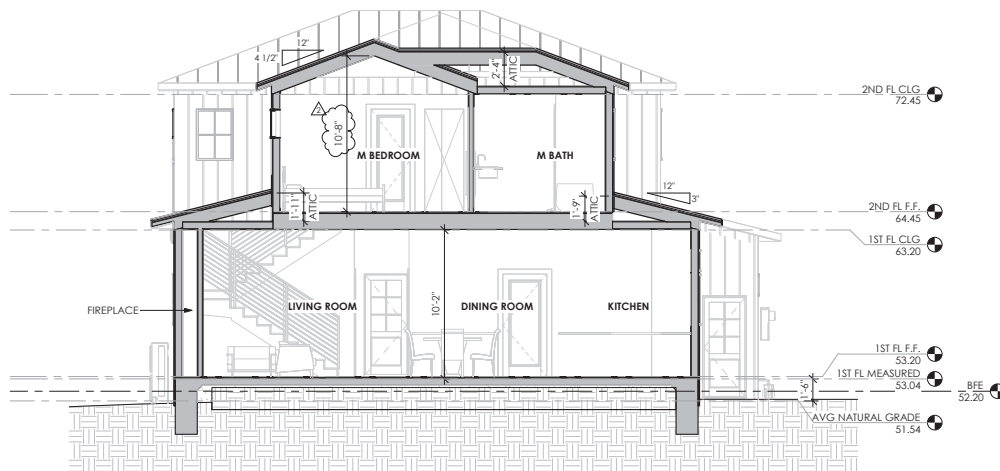


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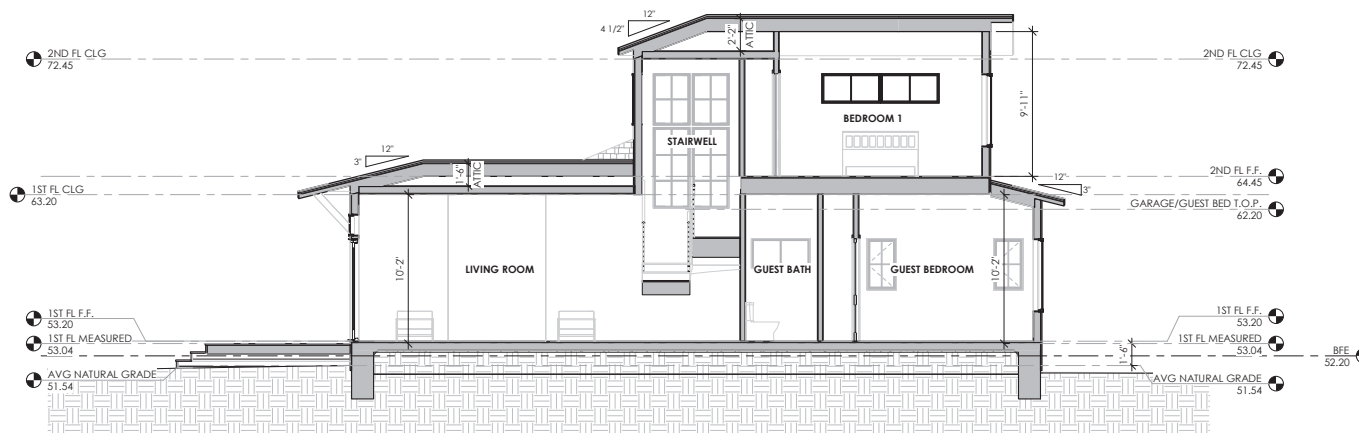


NEW TWO-STORY RESIDENCE
FOR
MENLO PARK RESIDENCE
167 MCKENDRY DRIVE
MENLO PARK, CA 94025



BUILDING SECTION C 1/4" = 1'-0" 1

- 03.04.2022 USE PERMIT SUBMITTAL
- 08.30.2022 RESPONSE TO UP COMMENTS #1
- 12.14.2022 RESPONSE TO UP COMMENTS #2



BUILDING SECTION D 1/4" = 1'-0" 2

PROJECT NO. : 21-7650

BUILDING SECTIONS

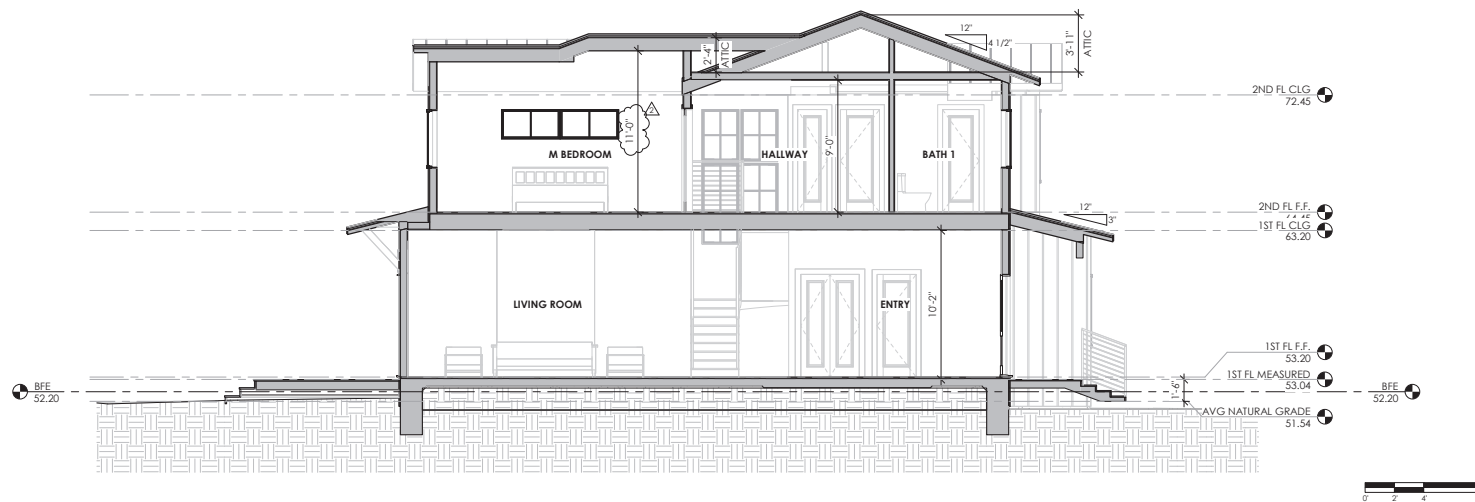


A4.2

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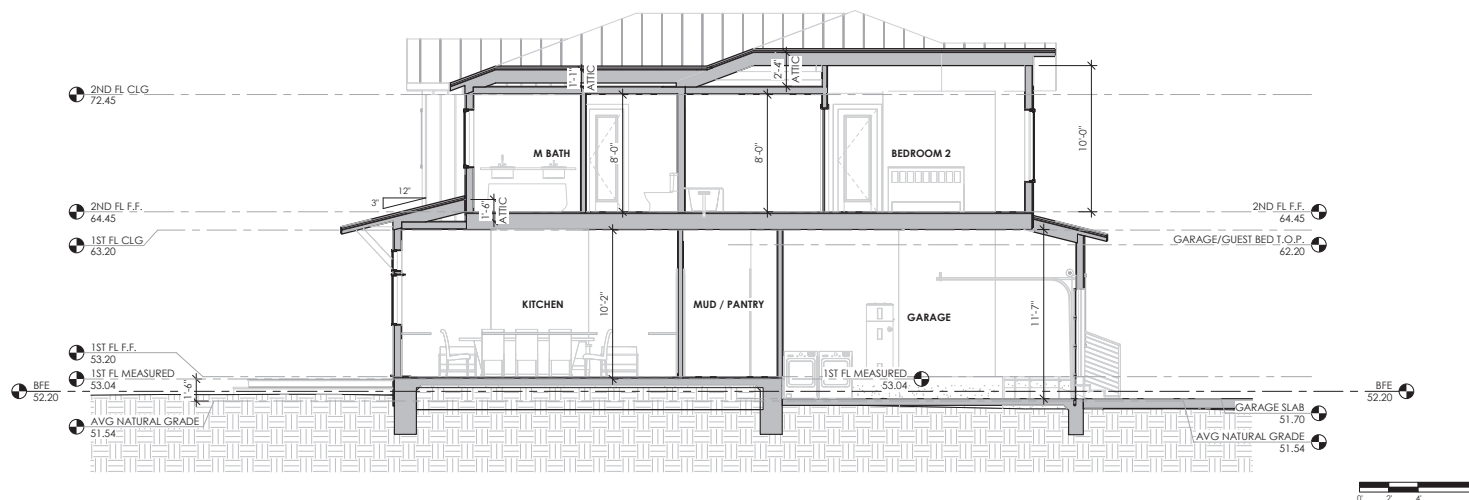


NEW TWO-STORY RESIDENCE
FOR
MENLO PARK RESIDENCE
167 MCKENDRY DRIVE
MENLO PARK, CA 94025



BUILDING SECTION E 1/4" = 1'-0" 1

- 03.04.2022 USE PERMIT SUBMITTAL
- 08.30.2022 RESPONSE TO UP COMMENTS #1
- 12.14.2022 RESPONSE TO UP COMMENTS #2



BUILDING SECTION F 1/4" = 1'-0" 2

PROJECT NO.: 21-7650

BUILDING SECTIONS



A4.3

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P R O J E C T S T A T E M E N T
M E N L O P A R K R E S I D E N C E
1 6 7 M C K E N D R Y D R I V E
S E P T E M B E R 1 , 2 0 2 2

Project Scope of Work

- Demolish existing 1,386 sf one-story single family residence and 245 sf detached garage
- Construct a new 2,310 sf two-story single family residence with 464 sf attached garage
- New permeable paver driveway, rear yard deck and other concrete walkways
- Removal of 1 non-heritage tree on project property

Existing Conditions

The existing home on the lot is a 1,386 sf one-story bungalow with a detached 245 sf garage resembling most of the original homes within the neighborhood. The home has not been maintained well and showing its age while also being damaged extensively from a Redwood tree that grew against the rear side of the house.

The property is also in a flood zone and the current home doesn't meet the base flood elevation nor is it constructed to meet FEMA flood protection requirements.

Existing hardscaping and landscaping is also showing the need for replacement. There is one (1) heritage tree (Coastal Redwood) on the property and one (1) heritage tree (Magnolia) in the public right-of-way that will be protected and retained. There is one (1) heritage tree (Coastal Redwood) on an adjacent lot (112 Blackburn) which will be minimally impacted but tree protection fencing will be provided. 1 non-heritage tree (bay laurel) is proposed to be removed due to proximity to the existing/proposed construction and health.

Proposed Project

Proposed Use

The new home will be a 4 bedroom, 4.5 bathroom two-story residence. The first floor area will have mostly gathering spaces (living/dining/kitchen) while 2nd floor will have private bedrooms. A two-car garage is provided to mitigate street parking concerns on

McKendry Drive. A rear deck is proposed to make use of the rear yard for the homeowner.

Site layout

- Right side setback given extra space to accommodate the Coastal Redwood located at the side property line at the front of the lot
- Rear yard given space for future use for the owner

Architectural Design

- The proposed two-story home will have a modern farmhouse aesthetic using board and batten siding to touch on the traditional farmhouse design and using standing seam metal roofing and aluminum-clad windows with narrow trims to give the house an updated design. Stained/painted wood lintels and brackets are used to soften the exterior and give an accent feature
- Main large windows face the front and rear. Privacy concerns for rear-facing large windows at the 2nd floor is proposed to be mitigated with tall landscaping

Neighborhood Outreach

- The property owner personally delivered the attached letter along with copies of the plans on June 16th, 2022 to the following addresses:
 - 171 McKendry
 - 202 McKendry
 - 205 McKendry
 - 104 Blackburn
 - 108 Blackburn
 - 112 Blackburn
 - 116 Blackburn
 - 200 Robin
 - 166 Willow
 - 170 Willow
 - 204 Willow

A response was received from the owners at 171 McKendry – see correspondence attached. The following changes were made:

- Sill heights for windows in Master Bedroom and Bedroom 1 windows facing the neighboring property at 171 McKendry have been raised to 5'-0" AFF and made shorter. See 1/A2.1 Proposed 2nd Floor Plan and 2/A3.1 Proposed Right Side Elevation
- Screening trees added along property line between project property and 171 McKendry to provide privacy screening from stairwell

From

The Chang family
167 McKendry Drive
Menlo Park, CA 94025

Dear Neighbor,

We are the owners of 167 McKendry Drive. We are planning to rebuild the house and would like to know if you have comments and feedbacks to the designs of the new house. We have enclosed with this letter the architectural plans for your review. If you have any comments or feedbacks, please kindly provide them to my email at ccrprime@yahoo.com. We want to thank you for your time in this matter.

Sincerely,

Ryan Chang
510-269-7163

Fw: 167 McKendry Updated Plan

From: Ryan (ccrcprime@yahoo.com)

To: eiki@studio02.net

Date: Thursday, August 18, 2022 at 06:09 PM PDT

FYI

----- Forwarded Message -----

From: Alexandre Lee <alexandrelee@yahoo.com>

To: Ryan <ccrcprime@yahoo.com>

Sent: Wednesday, August 17, 2022 at 12:58:39 AM PDT

Subject: Re: 167 McKendry Updated Plan

Hi Ryan!

Thanks for the additional information regarding the staircase area. We appreciate the value that the proposed staircase windows provide and its importance to your design. In the spirit of finding a mutually beneficial solution, we'd like to request the use of obscure glass for the staircase windows. This seems to be a common approach to both preserve the light coming into that area while providing additional privacy for both houses, as this would also make it harder for folks to see your upstairs area from the street and from our side.

There is also one additional issue that we wanted to raise upon closer examination of the plans. Currently, there is no 2nd floor side setback on the side that we share. This will be a problem when we build a 2nd floor, as our 2nd floors will be very close together.

Since the house on the other side of our home will have a 2nd floor, we will be very limited on our options for 2nd floor setbacks. Can you accommodate a 2-3 feet 2nd floor setback on the side we share? We hope this only shifts your 2nd floor plans, without impacting your desired square footage. With the small densely packed lots in our area, we hope we can find a solution that can allow for privacy and breathing room while maximizing the square footage.

Thanks for your continued collaboration!

-Alex

On Thursday, August 11, 2022 at 01:56:08 AM PDT, Ryan <ccrcprime@yahoo.com> wrote:

Hello Alex,

We spoke with the architect re your questions. There is no plan for seating area, just up and down for the stairs. 1st level finished height is at 10ft and 2nd floor is 9ft.

We plan to plant some screening trees between the houses to block the view, but honestly speaking, we do not expect any normal person to stand at the stairs and trying to looking into your rooms, and very likely you would have some window coverings for the evening as well?

Again, we appreciate your understanding that we will accommodate as much as possible but there are some design details that we would like to keep.

Thank you,
Ryan

On Friday, August 5, 2022 at 01:48:52 AM PDT, Alex Lee <alexandrelee@yahoo.com> wrote:

Hi Ryan!

Apologies for the delayed response, currently out of town with more limited connectivity till next Thu.

Thanks so much for listening to our concerns and incorporating the changes to the bedroom windows, hugely appreciated!

Regarding the stairway windows, thanks for explaining the importance of the window element. I was wondering if you could provide some details on how the stairway will be configured. As you go up the stairs, it looks like there will be a landing space half way up as

you turn to continue going up. What is the height between the bottom of the stairway window and that landing area? Forgive me if that number is somewhere in the plans already and I completely missed it. Also, were you thinking to configure this landing space with a seating area by the window, or would it just be a walk-through landing space? These details will help us get a better sense for how the stairway traffic will have visibility into our bedroom windows on that side.

Thanks for your continued patience and collaboration as we work out these last details!

Alex

On Aug 1, 2022 12:01 PM, Ryan <ccrcprime@yahoo.com> wrote:

Hi Alex,

Hope this email finds you well.

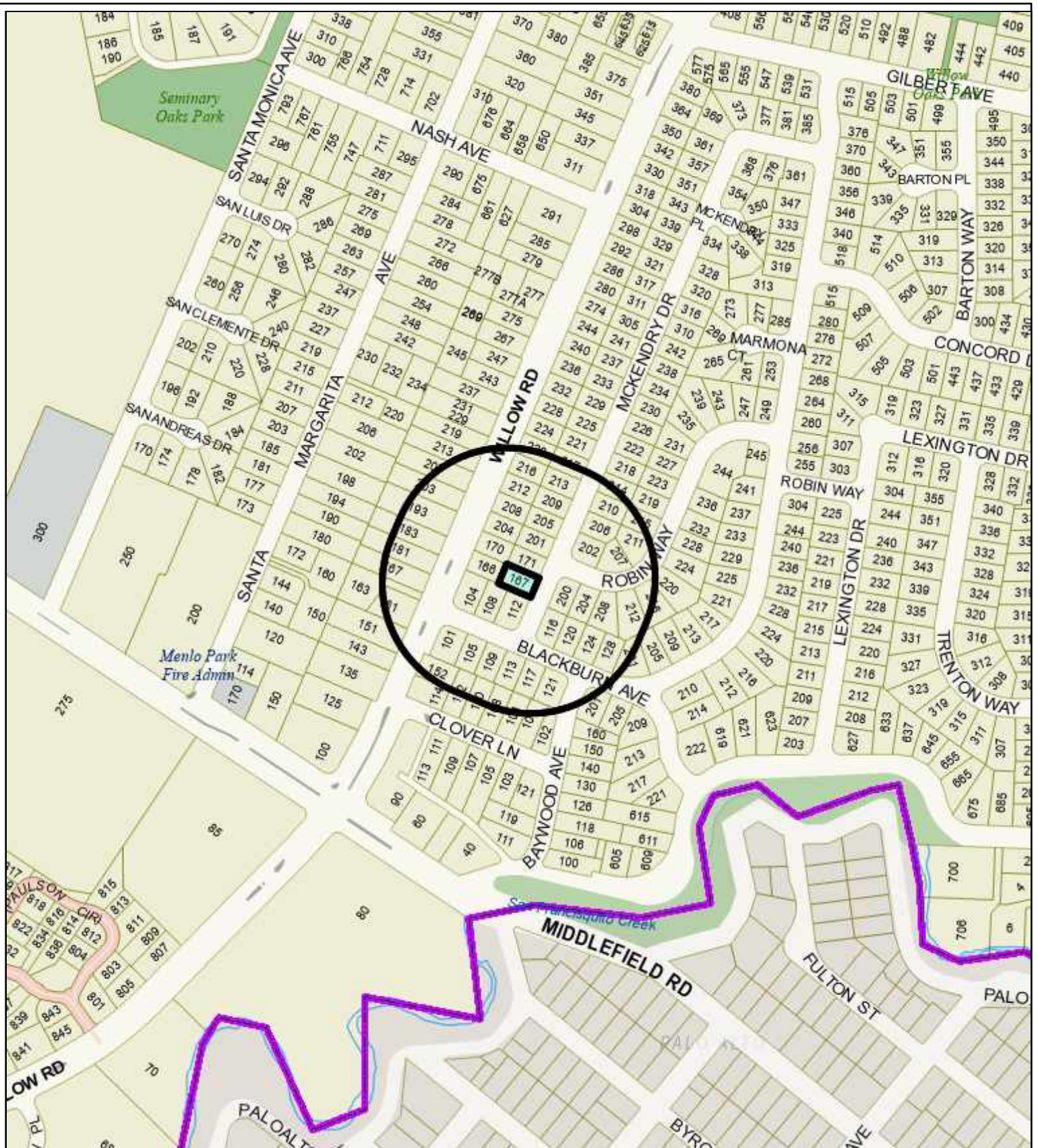
After our discussion, we dived in with the architect to address the comments you raised. Please see the enclosed updated drawing for your review. In short, the architect changed the window orientation/profile on the 2nd floor bedroom to address the privacy concerns. However, the stairs window is a very important design element to the house and as such we could not change the design. We hope this is a workable proposal and we greatly appreciate your understanding.

We are happy to further discuss once you have a chance to review the updated drawing. Thank you again and look forward to your feedback.

Best,

Ryan

LOCATION: 167 McKendry Drive	PROJECT NUMBER: PLN2022-00014	APPLICANT: Eiki Tanaka	OWNER: Ryan Chang
<p>PROJECT CONDITIONS:</p> <ol style="list-style-type: none"> 1. The use permit shall be subject to the following standard conditions: <ol style="list-style-type: none"> a. The applicant shall be required to apply for a building permit within one year from the date of approval (by February 6, 2024) for the use permit to remain in effect. b. Development of the project shall be substantially in conformance with the plans prepared by Studio 02, Inc., consisting of 13 plan sheets, dated received January 19, 2023 and approved by the Planning Commission on February 6, 2023, except as modified by the conditions contained herein, subject to review and approval of the Planning Division. c. Prior to building permit issuance, the applicants shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies' regulations that are directly applicable to the project. d. Prior to building permit issuance, the applicants shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project. e. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes. f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division. g. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits. h. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the arborist report prepared by Davey Resource Group, Inc., dated received September 8, 2022. i. Prior to building permit issuance, the applicant shall pay all fees incurred through staff time spent reviewing the application. j. The applicant or permittee shall defend, indemnify, and hold harmless the City of Menlo Park or its agents, officers, and employees from any claim, action, or proceeding against the City of Menlo Park or its agents, officers, or employees to attack, set aside, void, or annul an approval of the Planning Commission, City Council, Community Development Director, or any other department, committee, or agency of the City concerning a development, variance, permit, or land use approval which action is brought within the time period provided for in any applicable statute; provided, however, that the applicant's or permittee's duty to so defend, indemnify, and hold harmless shall be subject to the City's promptly notifying the applicant or permittee of any said claim, action, or proceeding and the City's full cooperation in the applicant's or permittee's defense of said claims, actions, or proceedings. 			



City of Menlo Park
 Location Map
 167 McKendry Drive



Scale: 1:4,000

Drawn By: MAP

Checked By: KTP

Date: 2/6/2023

Sheet: 1

167 McKendry Drive – Attachment C: Data Table

	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	5,005.0 sf	5,005.0 sf	7,000 sf min.
Lot width	55.0 ft.	55.0 ft.	65 ft. min.
Lot depth	91.0 ft.	91.0 ft.	100 ft. min.
Setbacks			
Front	20.0 ft.	24.7 ft.	20 ft. min.
Rear	24.0 ft.	18.2 ft.	20 ft. min.
Side (left)	6.5 ft.	4.7 ft.	5 ft. min.
Side (right)	8.8 ft.	13.9 ft.	5 ft. min.
Building coverage	1,743.9 sf 34.8 %	1,630.9 sf 32.6 %	1,751.8 sf max. 35 % max.
FAL (Floor Area Limit)	2,774.5 sf	1,630.9 sf	2,800 sf max.
Square footage by floor	1,232.7 sf/1st 1,077.5 sf/2nd 464.3 sf/garage 12.1 sf/fireplaces 34.8 sf/porches	1,385.8 sf/1st 245.1 sf/garage	
Square footage of buildings	2,821.4 sf	1,630.9 sf	
Building height	26.7 ft.	14.2 ft.	28 ft. max.
Parking	2 covered	1 covered	1 covered/1 uncovered
Note: Areas shown highlighted indicate a nonconforming or substandard situation.			

Trees	Heritage trees*	3	Non-Heritage trees**	3	New Trees	3
	Heritage trees proposed for removal	0	Non-Heritage trees proposed for removal	1	Total Number of Trees	8

* Of the three heritage trees, one is a street tree in front of the subject property, one is located in a neighboring property, and one is located on the subject property.

** Of the three non-heritage trees, two are located in neighboring properties and one is located on the subject property.



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Fax: 330-673-0860

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PO Box 5321
Larkspur, CA 94977
916-204-7902



Arborist and Tree Protection Report

167 McKendry Dr.

Menlo Park, CA

February, 2022

Revised September 7, 2022

Prepared For:

Ryan Chang

Prepared By:

Davey Resource Group, Inc.

Brenda Wong

ISA Certified Arborist

WE-12933A

ISA Tree Risk Assessment Qualified

Notice of Disclaimer

Inventory data provided by Davey Resource Group is based on visual recording at the time of inspection. Visual records do not include testing or analysis and do not include aerial or subterranean inspection. Davey Resource group is not responsible for discovery or identification of hidden or otherwise non-observable risks. Records may not remain accurate after inspection due to variable deterioration of inventoried material and site disturbance. Davey Resource Group provides no warranty with respect to the fitness of the urban forest for any use or purpose whatsoever or for future outcomes of the inventoried trees.

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Summary

In October 2021, Davey Resource Group (DRG) was contacted by Studio 02 Inc. to conduct a tree inventory and develop a tree protection plan for the trees in the area of impact on the property at 167 McKendry Dr. in Menlo Park, CA. The request was made to assess the current condition of the trees and establish a protection plan based on the findings.

On February 11, 2022, an International Society of Arboriculture (ISA) Certified Arborist (Brenda Wong, #WE-12933A) from Davey Resource Group conducted the evaluation of six (6) trees that may be impacted by development. The trees were assessed by their location, size, current condition, health, structure, and form (and functional and external limitations for appraisal values). The current site development scheme was used to estimate the construction footprint in relation to the critical root zones (CRZ) of the trees in order to help guide construction, and to reduce potential impacts on the trees. Current plans include demolition of the existing house and detached garage and the addition of a two-story, 2,798 square foot house with an attached garage and possible ADU. Tree information is summarized as follows:

- The six (6) trees inventoried comprise five (5) species: Coast redwood (2 trees), Southern magnolia (1 tree), California bay laurel (1 tree), grapefruit (1 tree), and almond (1 tree).
- The inventory encompasses the trees that may be impacted by the proposed construction (any trees with construction occurring within 10 times the trunk diameter or canopies that overhang the site).
- Two (2) of the trees are considered Heritage trees according to the City of Menlo Park.
- Three (3) of the trees are on adjacent properties with canopies that overhang the site.
- Four (4) trees were in fair condition and two (2) trees were in poor condition.
- Tree heights ranged from 20 to 115 feet.
- Tree diameters at four and a half feet above grade/breast height (DBH) ranged from 7 inches to 66 inches.
- Two (2) trees are recommended for removal under the current design plans, and four (4) trees may be retained; tree protection measures are provided.
- The total appraisal value (rounded) of the inventoried trees was \$15,370.00.
- Replacement tree plantings and design options are discussed in the Summary and Recommendations section below.

This report focuses on tree protection recommendations for tree preservation and provides the CRZs and SRZs of these trees for planning purposes. DRG has provided general site preservation recommendations based on the provided site scheme. Arborist monitoring of construction is required whenever work is performed within the critical root zones and work in structural root zones should be excavated by hand or with pneumatic air spade excavation tools. The trees identified for preservation should be monitored by a Certified Arborist at the end of construction and ongoing as needed.

Introduction

Background

Current plans for construction at 167 McKendry Dr. in Menlo Park include demolition of the existing house and detached garage as well as removal of an asphalt driveway, concrete walkway to the existing front door, and a concrete patio area in the rear. New construction includes a two-story, 2,798 square foot house with an attached garage and possible ADU. The proposed project has the potential to impact trees on the property and on adjacent property. All trees over 4 inches in diameter on the property and adjacent properties with construction or excavation occurring within 10 times the DBH of the tree were assessed and evaluated for impacts, and to determine if any trees meet criteria for Heritage status as defined by the City of Menlo Park.

Assignment

The arborist visually assessed each tree on the site, and the required tree data were collected using a portable tablet device. Following data collection, specific tree preservation plan elements were calculated that identified each tree's critical and structural root zones (CRZ and SRZ) to better ensure survivability during the planned development. This report establishes the condition of the trees and canopy within the project area. The trees were visually assessed, and photo documented so that change in condition can be evaluated if needed.

Limits of the Assignment

Many factors can limit specific and accurate data when performing evaluations of trees, their conditions, and potential for failure or response to site disturbances. No soil or tissue testing was performed. All observations were made from the ground on February 11, 2022, and no soil excavation to expose roots was performed. The most recent development scheme was available to assist in determining potential construction impacts. The determinations and recommendations presented here are based on current data and conditions that existed at the time of the evaluation and cannot be a predictor of the ultimate outcome for the evaluated trees in the future. No physical inspection of the upper canopy, sounding, resistance drilling, or other technologies were used in the evaluation of the trees.

Purpose and Use of Report

The purpose of this report is to provide a summary inventory of all trees within the project area of impact, including an assessment of the current condition and health, as well as providing a tree protection plan for all evaluated trees/canopies that may be impacted by construction plans. The findings in this report can be used to make informed decisions on design planning and be used to guide long-term care of the trees. This report and detailed tree protection plan can also be submitted to the City of Menlo Park for permitting purposes.

Observations

Methods

Only a visual inspection was used to develop the findings, conclusions, and recommendations found in this report. Data collection included measuring the diameter of significant trees at approximately 54 inches above grade (DBH), height estimation, a visual assessment of tree condition, structure, and health, and a photographic record. A rating percentage (0-100%) was assigned for each tree's health, structure, and form, and the lowest percentage was used as the overall tree condition. A preservation priority was assigned to each tree on a scale of 1 to 4: a rating of 1 representing the highest priority for protection due to excellent overall condition, unique specimen, or high value tree; a rating of 2 for a good to fair condition tree worthy of protection but not uniquely value; a rating of 3 for a fair condition tree that can be easily replaced; and a rating of 4 for trees in poor to critical condition that should be removed under most circumstances.

Site Observations

The project site is located at 167 McKendry Dr. in Menlo Park, CA. The parcel is a privately owned lot with an existing single family house and detached garage. The lot parcel contains three trees; one (1) coast redwood, one (1) southern magnolia, and one (1) California bay laurel. The redwood is situated in the front between the driveways of 167 and 171 McKendry Dr. The southern magnolia is in the front, opposite the coast redwood. The redwood and magnolia are located under a high voltage utility line. The California bay laurel is in the rear between the garage and the property fence. The canopies of three (3) trees on adjacent properties overhang 167 McKendry Dr. and were assessed for potential impact; one (1) grapefruit, one (1) almond, and one (1) coast redwood. The stump of a large redwood tree was observed in the rear of the house.

Tree Observations

Six (6) trees were assessed within the project area, comprising five (5) species: coast redwood (2 trees), Southern magnolia (1 tree), California bay laurel (1 tree), grapefruit (1 tree), and almond (1 tree). The trees were established, and tree condition ratings were fair for four (4) trees and poor for two (2) trees. Tree diameters ranged from 7 inches to 66 inches with an average of approximately 28 inches. Tree heights ranged from 20 feet to 115 feet, with an average height of 55 feet. All tree measurements of trees on adjacent properties were estimated.

A map of tree locations can be found in Appendix A. Tree photographs can be found in Appendix B and a complete Tree Inventory, Condition Assessment, and Tree Appraisal Values can be found in Appendix C.

Root Zone Calculations

The trunk diameters of the assessed trees are often used to determine the Critical Root Zone (CRZ). The CRZ is considered the ideal preservation area of a tree. It can be calculated by adding 1 foot of radius for every inch of trunk diameter measured at 4.5 feet from grade/breast height (DBH). For example; a tree with a DBH of 10 inches has a calculated CRZ radius of 10 feet from the trunk. The CRZ represents the typical rooting area required for tree health and survival. As this project is located in the City of Menlo Park, CRZ was substituted with the city standard of 10 times DBH to determine the Tree Protection Zone (TPZ) as seen in Table 1. Some impact (25% or less) within this zone is typically acceptable for average to good condition trees with basic mitigation/stress reduction measures. Construction activities should not occur within the TPZ of any tree to be retained. This includes but is not limited to the storage of materials, parking of vehicles, contaminating soil by washing out equipment, (concrete, paint, etc.), or changing soil grade.

The structural root zone was calculated using a commonly accepted method established by Dr. Kim Coder in *Construction Damage Assessments: Trees and Sites*.¹ In this method, the root plate size (i.e. pedestal roots, zone of rapid taper area, and roots under compression) and limit of disruption based upon tree DBH is considered as a minimum distance that any disruption should occur during construction. Significant risk of catastrophic tree failure exists if structural roots within this given radius are destroyed or severely damaged. The SRZ is the area where minimal or no disturbance should occur without arborist supervision. The TPZ and SRZ for the surveyed trees are listed in Appendix B, Table 2.

Conclusion and Recommendations

Based on visual evaluations and the impacts of proposed development, four (4) trees that have the potential to be impacted may be retained and two (2) trees are recommended for removal.

- Three (3) trees are considered Heritage trees (trees #1, 2, 6). The total replacement cost (rounded) for the Heritage trees was \$9,180 (appraisal values can be found in Appendix C). Any heritage tree damaged beyond repair by construction will require replacement according to its appraised value. Replacement tree plantings are another option and are discussed below.
- Tree #1, a heritage coast redwood, was in fair health with good structure and form and is located between 167 and 171 McKendry Dr. Proposed demolition of the existing asphalt driveway and the new proposed footprint is within the TPZ of the tree. New construction will be within 20 feet of the trunk (TPZ radius is 66ft). Impacts to the tree could be moderate to severe. Care and attention should be taken during pre-construction planning to minimize the impacts in the TPZ. Exploratory trenching with hand tools should be done to determine the root quantity and size in the area. Tree protection fencing should be installed along the TPZ and moved in when excavation is to occur in the TPZ, and moved to the farthest extent possible when excavation is complete. Any excavation that occurs within the TPZ should be done under arborist supervision. Any roots over 2-inches in diameter should only be removed with the approval of the onsite arborist, and if

¹ Dr. Kim D. Coder, University of Georgia June 1996

substantial roots are discovered the arborist may recommend the removal of the tree if design plans cannot be changed. Additional tree preservation methods may be recommended by the arborist completing the work based on the quality and quantity of roots found. The architect or builder should be willing to accommodate tree roots using construction techniques such as pier post construction and floating slab technologies. Plans may need to be adjusted depending on root locations determined in the field. The protected tree should be monitored and reassessed by a Certified Arborist six (6) months and one (1) year after construction, or if any changes in condition are observed. The replacement tree requirement per the City of Menlo Park is to plant one 60-inch box tree (such as a Catalina ironwood [*Lyonothamnus floribundus*], Cajeput tree [*Melaluca quinquenervia*], or Saratoga laurel [*Laurus nobilis* 'Saratoga']) or pay the appraised value of \$3,520.00.

- Tree #2, a heritage Southern magnolia, was in poor health with good structure and poor form and is located on the southwestern corner of the parcel. The property owner indicates intent to retain this tree and install a new permeable paver driveway while following necessary tree protection measures. Demolition impacts to the tree will be minimal, however, excavating or grading for the proposed driveway could impact the tree moderately to severely. The driveway will extend an estimated 15 feet within the TPZ, therefore, exploratory trenching with hand tools should be done to determine the root quantity and size in this area. Tree protection fencing should be installed along the TPZ and moved in when excavation is to occur in the TPZ, and moved to the farthest extent possible when excavation is complete. Any excavation that occurs within the TPZ should be done under arborist supervision. Any roots over 2-inches in diameter should only be removed with the approval of the onsite arborist, and if substantial roots are discovered the arborist may recommend the removal of the tree if design plans cannot be changed. An additional alternative to retain the magnolia by reducing root impacts is to build the new driveway up above grade. This can be done by adding a geogrid with aggregate infill on top of the roots. This will avoid cutting large roots and the geogrid prevents soil compaction by creating a spongy base that absorbs impact.
- Tree #3, a California bay laurel, was in fair health with good structure and form and is located in the rear of the property between the existing detached garage and the property fence. Demolition impacts to the tree could be moderate to severe. Due to this and its poor location, this tree is recommended to be removed. No permit is required for removal.
- Tree #4, a grapefruit, was in fair health with fair structure and form and is located on adjacent property to the west. Impacts should be minimal to none. The east side of the canopy has been pruned where it overhangs the fence, however, tree protection fencing should be installed on the east side of the tree.
- Tree #5, an almond, was in poor health with poor structure and form and is located on adjacent property to the southeast (108 Blackburn Ave.). Impacts should be minimal to none. The portion of the canopy that overhangs the property fence is dead. Pruning to remove deadwood is recommended.
- Tree #6, a coast redwood, was in fair health with good structure and form and is located on adjacent property to the southeast (112 Blackburn Ave.). Impacts should be minimal. The canopy overhangs the property fence. Before excavation, exploratory trenching should be done to determine the root quantity and size in the area. Tree protection fencing should be installed along the TPZ on the north side of the fence, moved in when excavation is to occur in the TPZ, and moved to the farthest extent possible when excavation is complete. Any excavation that occurs within the TPZ should be done under arborist supervision. Any roots over 2-inches in diameter should only be removed with the approval of the onsite arborist. If this tree is damaged or destroyed during construction, the appraised value that would be owed is \$3,800.00. Alternatively, the tree could be replaced with a 48-inch box tree.
- Monitoring of the tree protection specifications by an ISA Certified Arborist is required at monthly intervals.
- TPZ fencing must be in place before any equipment is on-site, and it must be inspected by a Certified Arborist who shall provide a verification letter summarizing the conditions. The fencing must remain in place for the entirety of the project and only removed, temporarily or otherwise, by a Certified Arborist while activities are directly supervised, and replaced immediately after.
- TPZ fencing should be 6 feet in height, constructed of chain link fencing. The fencing may be moved within the dripline if directed by the on-site or City Arborist but cannot be moved to within 2 feet of the trunk. Fence posts must be 2-inch in diameter and galvanized, and installed 2 feet below grade. Posts may be movable rather than below grade and may not be spaced more than 10 feet apart. Signs must be posted stating: "TREE PROTECTION FENCE - DO NOT MOVE OR REMOVE WITHOUT APPROVAL FROM CITY ARBORIST." The fence may not be moved without authorization from the on-site or City Arborist.
- Activities prohibited within the TPZ include: grade changes, trenching, root cutting, equipment and foot traffic that could compact the soil or damage roots, parking vehicles or equipment, and burning of any kind.

- No material shall be stored, nor concrete basins washed, or any chemical materials or paint stored within the TPZ of trees, and no construction chemicals or paint should be released into landscaped areas, as these can be toxic to trees and contaminate soil.
- If there is to be entry into the TPZ, a 4-inch layer of mulch topped with plywood or other soil-compaction-protection materials appropriate for the proposed activities shall be installed.
- If clearance is necessary for equipment access, lower branches should be lifted with ropes, or straps, or pruned by an ISA certified arborist.
- After construction is complete, the property owner should monitor the trees for at least one year and contact a Certified Arborist to inspect if any lean, limb die-back, leaf drop, or foliage discoloration develops.

Appendix A – Location Map



Appendix B – Tree Photos



Photo 1. Tree #1, a coast redwood, has been side pruned for utilities. The well formed canopy is showing signs of stress. There is the potential for moderate to severe impact to this tree. The trunk of tree #2 is behind the utility pole on the left (see photo 11 for more detail).



Photo 2. View of tree #1 from across McKendry Dr., facing southwest.



Photo 3. Shown of the right side of the photo, tree #1 has grown over the existing driveway. Care and attention to preconstruction planning are essential to reduce impacts to the TPZ of this tree.



Photo 4. View of tree #3. Due to its poor location and potential impacts from demolition of the building on the left, removal is necessary.



Photo 5. Closer view of the proximity of the trunk of tree #3 to the existing detached garage.



Photo 6. The canopy of tree #5, on adjacent property, overhangs the fence line. Impacts should be minimal. TPZ fencing should be placed on the east side of the property fence.



Photo 7. View of tree #5 (smaller tree with white flowers) facing southwest. It is located on adjacent property. Impacts should be minimal.



Photo 8. View along the south fence line. The canopy of tree #5 (seen in the background with leafless branches) overhangs the property fence. The portion of the canopy that overhangs the property line is dead. Pruning of deadwood is recommended.

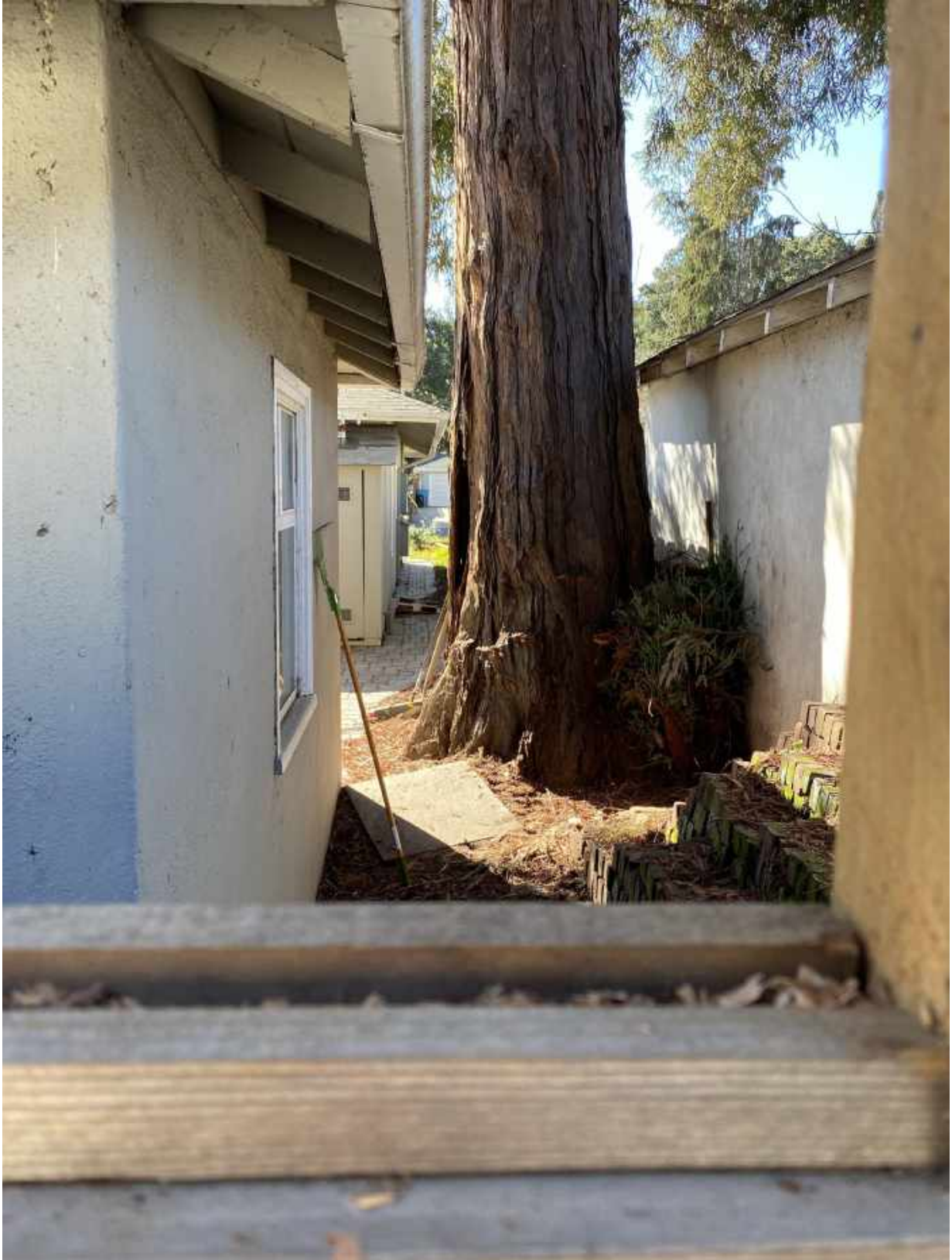


Photo 9. Photo looking over the fence to the trunk of tree #6 in the rear of 112 Blackburn Ave. The canopy slightly overhangs the fence (see branches in the foreground of photo 8). TPZ fencing should be installed on the north side of the fence and any excavation taking place in the TPZ should be supervised by an arborist.

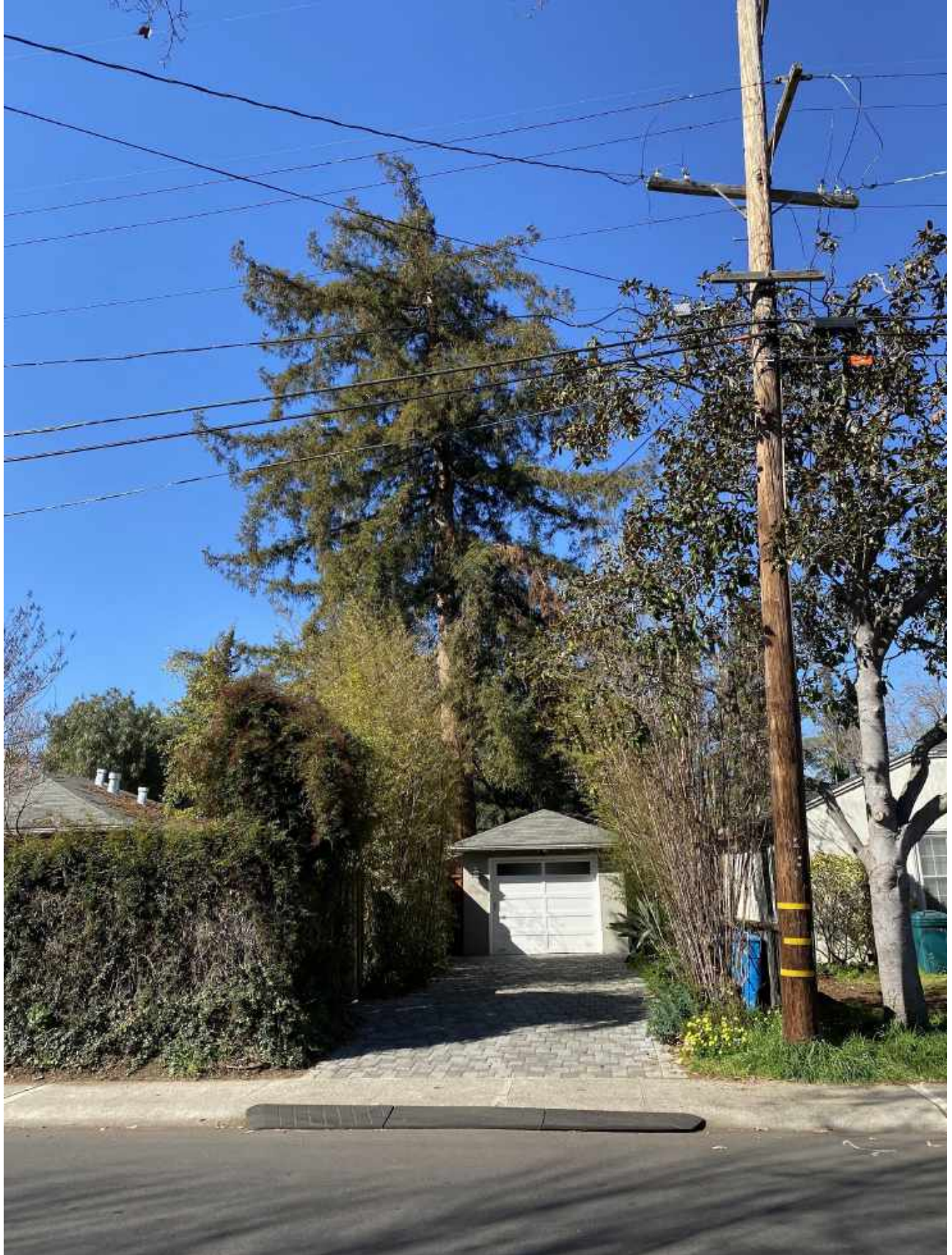


Photo 10. View of tree #6 (on the left) from McKendry Dr. Tree #2 can be seen in the foreground (right) of the photo.



Photo 11. View of tree #2 from McKendry Dr. This species of tree is unsuitable under utility lines and was showing signs of stress. Construction impacts may be moderate to severe.

Appendix C – Tables

Table 1. Tree Inventory and Root Zones

Tree #	DBH	Stems	Botanical Name	Common name	Preservation Priority	Height (ft)	Canopy Radius (ft)	SRZ (Radius in ft)	TPZ (Radius in ft)
1	66	1	<i>Sequoia sempervirens</i>	Coast redwood	1	115	18	30	66
2	15.1	1	<i>Magnolia grandiflora</i>	Southern magnolia	3	30	12	7	15
3	7	1	<i>Umbellularia californica</i>	California bay laurel	4	30	4	3	7
4	12 (est)	1	<i>Citrus paradisi</i>	Grapefruit	2	20	4 (est)	5	12
5	10,10 (est)	2	<i>Prunus dulcis</i>	Almond	4	35	10 (est)	6	14
6	50 (est)	1	<i>Sequoia sempervirens</i>	Coast redwood	1	100	18 (est)	23	50

Table 2. Condition Assessment February 2022

Tree #	Common name	Condition	Health (%)	Structure (%)	Form (%)	Heritage/ Street Tree (Y/N)	Removal Required (Y/N)	Notes
1	Coast redwood	Fair	45	70	80	Y	N	Located between properties. Utility pruned. Signs of stress in the canopy. Potential impacts moderate to severe.
2	Southern magnolia	Poor	41	70	30	Y	Y	Located under utility lines. Signs of stress in the canopy. Poor site suitability.
3	California bay laurel	Fair	60	70	60	N	Y	Located between the existing garage and property fence.
4	Grapefruit	Fair	60	60	60	N	N	Neighbor's tree. Canopy overhangs property fence. Impacts minimal.
5	Almond	Poor	30	40	40	N	N	Neighbor's tree. Canopy overhangs property fence. Impacts minimal.
6	Coast redwood	Fair	45	70	80	Y	N	Neighbor's tree. Canopy overhangs property fence. Impacts minimal

Table 3. Tree Appraisal Values*

Tree #	Common name	Condition	External Limitations (%)	Functional Limitations (%)	Heritage/ Street Tree (Y/N)	Removal (Y/N)	Total Functional Replacement Cost (\$)	Rounded Functional Replacement Cost (\$)
1	Coast redwood	Fair	70	80	Y	N	3,516.00	3,520.00
2	Southern magnolia	Poor	70	60	Y	Y	1,862.88	1,860.00
3	California bay laurel	Fair	80	90	N	Y	1,923.36	1,920.00
4	Grapefruit	Fair	80	80	N	N	2,605.92	2,610.00
5	Almond	Poor	90	20	N	N	1,655.52	1,660.00
6	Coast redwood	Fair	80	80	Y	N	3,804.00	3,800.00

*Appraisal values include \$1,500/tree in additional costs for replacement tree installation, aftercare, and cleanup. All values calculated using the Trunk Formula Method as described in the 10th edition of the *Guide for Plant Appraisal* by the Council of Tree and Landscape Appraisers.

Appendix D – Tree Appraisal Calculation Methodology

The valuation of the assessed trees for the site was calculated using the trunk formula method described in the 10th edition of the *Guide for Plant Appraisal* by the Council of Tree and Landscape Appraisers. The basic formula is as follows:

$$\text{Unit Tree Cost} \times \text{Condition Rating (\%)} \times \text{Functional Limitations (\%)} \times \text{External Limitations (\%)}$$

The basic tree cost is the sum of the installed tree cost and the cost of the difference between the adjusted trunk area and the replacement tree size (appraised tree size increase multiplied by unit tree cost). Size was measured as trunk cross-sectional area (square inches), calculated by $0.785 \times (\text{DBH})^2$; where a circular cross-section was assumed.

Species size and cost data were obtained from the ISA Western Chapter Species Classification for Landscape Tree Appraisal (2004). The Western rating was used. No nursery group data were used as the Basic Tree Cost was calculated using the above formula(s). The condition rating was based on field observations already described. The functional limitation and external limitation ratings were based on field and aerial imagery observations. The basic functional replacement tree cost was then calculated by multiplying the functional replacement tree cross section area by the unit tree cost. The depreciated functional replacement tree (calculated using the basic functional replacement cost, the overall condition rating (%), the functional limitations rating (%), and the external limitations rating (%)) is then added to the total additional costs. The additional cost includes installation cost, replacement tree aftercare cost, and cleanup costs.

Regional Data - Western	
State or Region	Northern California
Replacement Tree Size (in. diam @ 12" Above Grade)	2
Installation Cost \$	\$800.00
Replacement Tree Aftercare Cost \$	\$500.00
Other Costs (Hardscape, Cleanup, etc.) \$	\$200.00
Unit Tree Cost (\$/sq in)	\$172.73

Additional Comments Received after Staff Report Publication

Alex Lee

171 McKendry Drive
Menlo Park CA 94025

February 6, 2023

Menlo Park Planning Commission

701 Laurel Street
Menlo Park, CA 94025

Dear members of the Menlo Park Planning Commission,

We are writing in regards to the proposed renovation plans on 167 McKendry Drive. We currently live next door on 171 McKendry Drive.

As noted in Exhibit B of the submitted plans, we have raised privacy concerns with the 4 stairwell windows that face our property. When coming down on that stairway, or when standing on the stairwell landing area between floors, we are concerned that there is a direct line of sight to our backyard and our bedroom windows that face 167 McKendry (*see Pictures 1 and 2 attached below*). Given that there is only 3'2"-3'9" clearance between the stairwell landing area and the window, it is easy for any person to have this direct line of sight. This issue will be worse if we choose to expand our current home with a 2nd floor in the future, as the top of the stairway on 167 McKendry would have direct line of sight to our 2nd floor windows (*see Picture 3 below*). That is why we had discussed with Mr. Ryan Chang about using some form of obscure glass for the stairwell windows last August.

Last week we just learned that the latest submitted plans only address our privacy concerns with three 15-gallon Strawberry Trees to provide landscape screening. We have informed Mr. Ryan Chang that this submitted proposed solution is not adequate. A 15-gallon Strawberry Tree is only generally around 6ft tall, and we calculated that we would need a tree that is at least 13ft tall to block the direct line of sight towards our current backyard & windows (*see Picture 4 below*). We also raised our concern that Strawberry Trees can grow wide and drop significant fruits on our driveway where we park our car and where our kids play, requiring us significant ongoing maintenance.

We have reiterated to Mr. Chang our preference for an obscure glass solution, as we see it as a better longer term solution that addresses both the current and future privacy needs. We have requested that if any landscape screening solution is pursued, it would need to provide adequate coverage from the time when the construction is completed, without having to wait for the trees to grow to the

appropriate height over time. In the past, we have also proposed raising the height of the stairwell windows as an alternative.

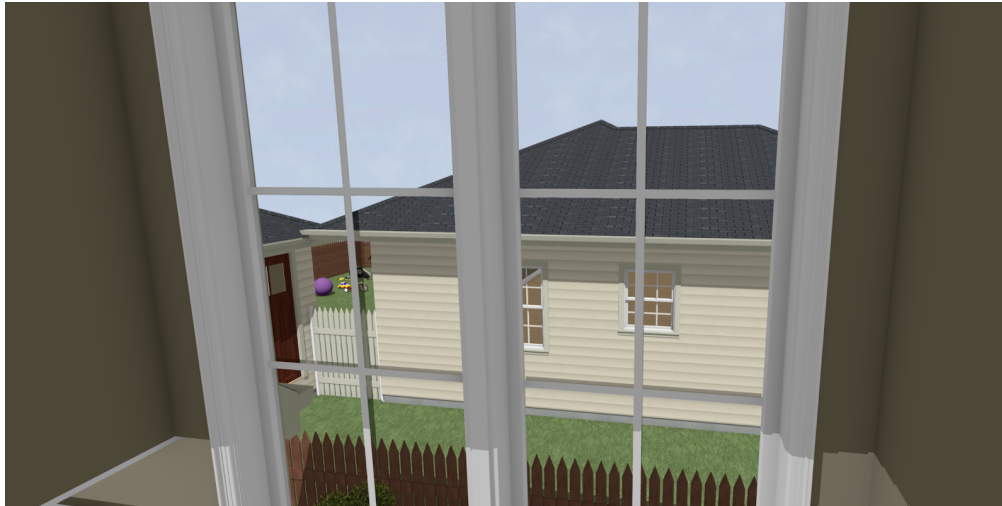
As of this morning, we have not come to an agreement yet. My purpose with this letter is primarily to make you aware that the privacy concerns with the stairwell windows have not yet been resolved, and that we're still in discussions to arrive at a mutually agreed upon solution.

Thank you for your time and consideration.

Sincerely,

Alex Lee

Picture 1 - Simulated view from stairwell landing on 167 McKendry Dr., looking towards the back of our current 1-story home (based on latest submitted plans for 167 McKendry and our current home dimensions)



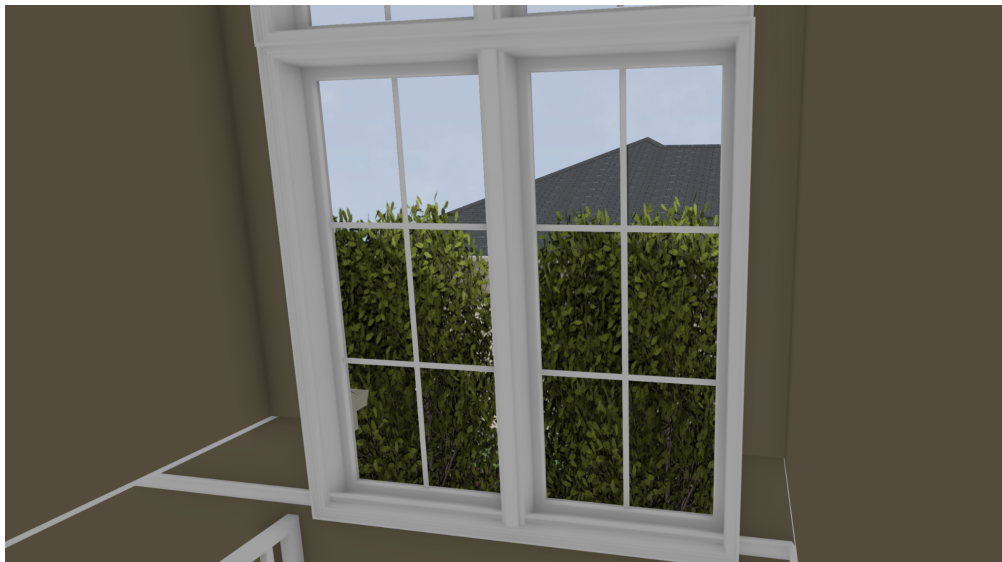
Picture 2 - Simulated view from stairwell landing on 167 McKendry Dr., looking at the side of our current 1-story home



Picture 3 - Simulated view from top of stairwell on 167 McKendry Dr., looking at the side of our home in the scenario where we expand our home with a 2nd floor



Picture 4 - Simulated view from stairwell landing on 167 McKendry Dr., looking towards the back of our current 1-story home WITH a 13ft hedge tree planted in between the properties





STAFF REPORT

Planning Commission

Meeting Date:

2/6/2023

Staff Report Number:

23-012-PC

Public Hearing:

Consider and adopt a resolution to approve a use permit to demolish an existing one-story single-family residence and construct two new two-story residences on a substandard lot with regard to minimum lot width in the R-2 (Low Density Apartment) zoning district. The project would also include excavation in the interior side and rear setbacks for lightwells associated with basements.

Recommendation

Staff recommends that the Planning Commission approve a use permit to demolish an existing one-story single-family residence and construct two new two-story residences on a substandard lot with respect to minimum lot width in the R-2 (Low Density Apartment) zoning district. The project would also include excavation in the interior side and rear setbacks for lightwells associated with basements. Additionally, the proposal includes administrative review of a minor subdivision to subdivide the project into two condominium units. The draft resolution, including the recommended actions and conditions of approval, is included as Attachment A.

Policy Issues

Each use permit request is considered individually. The Planning Commission should consider whether the required use permit findings can be made for the proposal.

Background

Site location

The subject property is located at 785 Partridge Avenue. Using Partridge Avenue in the north-south orientation, the subject property is located on the eastern side of Partridge Avenue, between El Camino Real and University Drive, in the Allied Arts neighborhood. A location map is included as Attachment B.

Houses along Partridge Avenue include both one- and two-story residences and the area contains a mixture of single-family and multifamily residences, with a mix of older and newer homes. Most of the residences surrounding the property are two-story multi-family residences, with single-story single-family residences closer to University Drive. The neighborhood features various architectural styles ranging from craftsman, ranch, and farmhouse.

Most parcels on Partridge Avenue are also zoned R-2, however, the properties at the end of Partridge

Avenue, to the north where it intersects El Camino Real and the south where it intersects University Drive, are part of the El Camino Real Downtown Specific Plan and the R-1-U (Single-Family Urban) zoning district, respectively. At the north end of Partridge Avenue, the parcels immediately adjacent to the specific plan parcels are zoned R-3 (Apartment).

Analysis

Project description

The property is currently occupied by a one-story, single-family residence with an attached two-car garage. The lot is substandard with regard to lot width (50 feet where 65 feet is required by the zoning district). The applicant is proposing to demolish all existing structures and construct two, two-story, residences on the site. The required parking for each unit would be provided via an attached one-car garage for each unit and one uncovered parking space for each unit. A data table summarizing parcel and project attributes is included as Attachment C. The project plans and the applicant's project description letter are included as Attachment A, Exhibits A and B, respectively.

The front residence (Unit A) and rear residence (Unit B) would both contain four bedrooms, and four bathrooms. Both homes would have a typical layout of shared spaces on the ground level, with an attached single-car garage and most of the bedrooms on the upper level and one on the basement level. The existing driveway on the left side would be removed to allow for a new driveway with permeable pavers on the right side of the property which would be shared by both residences. Uncovered parking would be located in between the units on the lot, which would fulfill the two parking space per dwelling unit requirement.

Of note with regard to Zoning Ordinance development standards:

- The proposed floor area for the both residences combined is 3,199 square feet, where 3,200 square feet is the maximum allowable FAL.
- The second-story would have combined floor area of 1,198.8 square feet, where 1,200 square feet (15 percent) is the maximum allowed.
- The proposed combined building coverage, would be 2,621 square feet, approximately 32.8 percent of the lot area, where 35 percent is the maximum allowed.
- The proposed unit A would be 27.9 feet in height, and unit B would be 27.7 feet in height where 28 feet is the maximum allowed.
- The buildings would comply with the daylight plane.
- The second floors for both units would feature greater setbacks than required on all four sides, and the overall structure would be within the daylight plane, reducing potential privacy concerns.
- Second floor window sill heights would have minimum of three feet and maximum six feet sill heights.
- Larger windows on the second floor are located on the front and rear, and smaller windows are proposed on the sides, to reduce any potential privacy impacts.

In addition to the use permit, the applicant is also requesting approval of a tentative map for a minor subdivision into two residential condominium units. The minor subdivision can be reviewed and approved administratively by the Public Works Department, if the Planning Commission approves the use permit request.

Design and materials

The applicant has indicated that the proposed residences would be constructed in a farmhouse style. Although, the proposed units are identical through floor plans, massing, and similar material usage; the architectural styles of the two proposed homes, however, are not identical but do complement each other. Both units, would feature front entries consisting of a covered porch supported by painted wooden posts with solid wood entry doors with translucent glass, and front facing single-car garages. Both homes have been designed with larger second-story windows to the front and rear of the property for the purposes of egress, and smaller windows along the sides, to address any privacy concerns. Proposed windows for both units would be gridded, with aluminum-clad wood trims with simulated true divided lites.

The front residence (unit A) would be designed as in a traditional farmhouse style, with board and batten siding proposed for both lower and upper levels. To provide variation in the use of exterior material, horizontal siding on the second floor right side is proposed. Roofing is proposed to be standing seam metal over the first floor and asphalt composite shingles over the second floor. Most second-story windows would have sill heights between three feet two inches to six feet. The garage door would be designed as a carriage-style door made of solid wood with a herringbone pattern and high-paned windows. The impact of the front-facing single-car garage would be minimized by the usage of concrete slabs intermittent with landscape for the driveway access to reduce the hardcape, whereas the driveway portion leading to the rear would be pavers. A covered front porch would be located adjacent to the garage, limiting the impact of the garage on the street. Further, a separate pathway would provide direct access from the street to the front door.

The rear residence (unit B) would be designed in a modern farmhouse style, featuring traditional forms which would complement the front unit, and proposed contemporary details and materials, would allow for a modern take on the traditional farmhouse home. The proposed exterior material for unit B would be fiber cement stucco on both levels, and portions of the front and right elevation would feature colored horizontal wood siding. Standing seam metal roofing is proposed above the first floor, with composition asphalt shingle roofing on the second floor. The garage door would feature a modern aluminum frame with non-translucent window panes for privacy. The front door is designed as a custom wood door with a sleek vertical window pane. Aluminum trim, with simulated true divided lite windows, are proposed with horizontal muntins for a modern aesthetic.

Staff believes the increased second-story setbacks, taller sill heights on the sides, and fewer windows on the sides are sufficient to alleviate potential privacy concerns. Staff believes that the scale, materials, and style of the proposed residence would result in a consistent aesthetic approach and are generally consistent with the broader neighborhood.

Excavation

The project proposes two lightwell encroachments, one for each proposed residence (unit A and B). The proposed lightwells would require excavation within the required left side setback for unit A and rear yard setback for unit B. Specifically, unit A's proposed lightwell would encroach two feet into the left-side

setback. Unit B’s proposed lightwell would encroach three feet, 11 inches into the rear setback. The lightwells would be designed for egress and would not substantially exceed the minimum size required for code-compliant egress. Staff believes the proposed excavations into the left side and rear setbacks for the lightwells would be modest due to their limited size, and would not result in any visible effects as it would be located on the interior side and rear respectively.

Trees and landscaping

The applicant has submitted an arborist report (Attachment D), detailing the species, size, and conditions of on-site and nearby heritage/non-heritage trees. As part of the project review process, the City Arborist reviewed the report. As described in the report, there are a total of 11 heritage and non-heritage trees on and around the subject property. Tree protection measures related to the proposed work have been included in the report. There are four heritage sized trees (Trees #4, 5, 6 and 8) that are proposed for removal, as well as a non-heritage sized tree (Tree #1). A total of five replacement trees are proposed on the subject property, which would help with privacy. The removal justifications of the four heritage trees are summarized in Table 1, as noted in the arborist report:

Table 1: Tree Dispositions				
Tree Number	Species	Size (DBH, in inches)	Condition	Removal/Reason
4	Canary Island Date Palm	40	Good	To accommodate the proposed driveway. (Development, criteria #5)
5	Avocado	20.4	Good	To accommodate unit B. (Development, criteria #5)
6	Monterey Pine	47.3	Poor	Declining health due to beetle damage. (Tree Health rating, Criteria #3)
8	Bay Laurel	17.3	Fair	To accommodate unit B. (Development, criteria #5)

The City Arborist has reviewed and approved a heritage tree removal permit (HTR2022-00011) for the applicable trees on the subject property. The arborist report includes tree protection recommendations for pre-construction, construction, and post-construction phases of the project. These arborist recommendations include the establishment of tree protection zones for Trees #7 and #10, guidance for preventing root damage, and guidance for pruning branches for tree #11 to allow for large vehicle access, amongst other specifications. Implementation of all recommendations to mitigate impacts to the heritage trees identified in the arborist report would be ensured as part of condition 1.h. As part of the review process the Engineering Division has reviewed the frontage improvements and recommended a 24 inch box street tree be planted on the street frontage, which is included as condition 2.a.

Correspondence

Within the project description letter (Attachment A, Exhibit B), the applicant states that they have reached out to the neighbors, including mailed letters to the apartment complex; and have received comments from

two neighbors that raised concerns about asbestos during the demolition of the existing home and the cost of a new fence. Staff has received neighbor comments regarding the project on issues pertaining to a tree on another neighbor's property, lightwell construction impacts to their existing foundation, and privacy impacts from the proposed second-story bedroom windows.

Conclusion

Staff believes that the scale, materials, and style of the proposed residences are compatible with those of the overall neighborhood, as well as the proposed lightwell sizes. Visibility of the lightwells from the street will also be limited. The proposed variation in the forms would lessen the perceived massing and add visual interest to the project. The proposed five replacement trees on the property along the left side and rear of property would provide screening. Onsite circulation would meet all Transportation Division requirements for covered and uncovered parking while meeting the minimum required landscaping requirements. Staff recommends that the Planning Commission approve the proposed use permit.

Impact on City Resources

The project sponsor is required to pay Planning, Building and Public Works permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project.

Environmental Review

The project is categorically exempt under Class 3 (Section 15303, "New Construction or Conversion of Small Structures") of the current California Environmental Quality Act (CEQA) Guidelines.

Public Notice

Public Notification was achieved by posting the agenda, with the agenda items being listed, at least 72 hours prior to the meeting. Public notification also consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants within a 300-foot radius of the subject property.

Appeal Period

The Planning Commission action will be effective after 15 days unless the action is appealed to the City Council, in which case the outcome of the application shall be determined by the City Council.

Attachments

- A. Draft Planning Commission Resolution of Approval Adopting Findings for project Use Permit, including project Conditions of Approval
 - Exhibits to Attachment A
 - A. Project Plans
 - B. Project Description Letter
 - C. Conditions of Approval
- B. Location Map
- C. Data Table

- D. Arborist Report
- E. Neighbor Correspondence

Disclaimer

Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings, and exhibits are available for public viewing at the Community Development Department.

Exhibits to Be Provided at Meeting

None

Report prepared by:
Fahteen Khan, Associate Planner

Report reviewed by:
Kyle Perata, Planning Manager

PLANNING COMMISSION RESOLUTION NO. 2023-XX

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF MENLO PARK APPROVING A USE PERMIT TO DEMOLISH AN EXISTING ONE-STORY RESIDENCE AND CONSTRUCT TWO NEW TWO-STORY RESIDENCES ON A SUBSTANDARD LOT WITH REGARD TO MINIMUM LOT WIDTH IN THE R-2 (LOW DENSITY APARTMENT) ZONING DISTRICT. THE PROJECT WOULD ALSO INCLUDE EXCAVATION IN THE INTERIOR SIDE AND REAR SETBACKS FOR LIGHTWELLS ASSOCIATED WITH BASEMENTS.

WHEREAS, the City of Menlo Park (“City”) received an application requesting to demolish an existing one-story residence and construct two new two-story residences on a substandard lot with regard to minimum lot width in the R-2 (Low Density Apartment) zoning district. The project would also include excavation in the interior side and rear setbacks for lightwells associated with basements. Additionally, the proposal includes administrative review of a minor subdivision to subdivide the project into two condominium units. The above referenced entitlements are collectively referred to as, the “Project”, submitted by Calvin Smith (“Applicant”) and Xiaoyan Liu (“Owner”), and located at 785 Partridge Avenue (APN 071-413-010) (“Property”). The Project use permit is depicted in and subject to the development plans and documents which are attached hereto as Exhibit A and incorporated herein by this reference; and

WHEREAS, the Property is located in the Low Density Apartment (R-2) district. The R-2 district supports multi-family residential uses; and

WHEREAS, the proposed Project complies with all objective standards of the R-2 district; and

WHEREAS, the proposed Project was reviewed by the Engineering Division, and Transportation Division, and found to be in compliance with City standards; and

WHEREAS, the Applicant submitted an arborist report prepared by Aesculus Arboricultural Consulting which was reviewed by the City Arborist and found to be in compliance with the Heritage Tree Removal permit number HTR2022-00011, Heritage Tree Ordinance, and determined to include mitigation measures to adequately protect heritage trees in the vicinity of the project; and

WHEREAS, the Project, requires discretionary actions by the City as summarized above, and therefore the California Environmental Quality Act (“CEQA,” Public Resources Code Section §21000 et seq.) and CEQA Guidelines (Cal. Code of Regulations, Title 14, §15000 et seq.) require analysis and a determination regarding the Project’s environmental impacts; and

WHEREAS, the City is the lead agency, as defined by CEQA and the CEQA Guidelines, and is therefore responsible for the preparation, consideration, certification, and approval of environmental documents for the Project; and

WHEREAS, the Project is categorically exempt from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures); and

WHEREAS, all required public notices and public hearings were duly given and held according to law; and

WHEREAS, at a duly and properly noticed public hearing held on February 6, 2023, the Planning Commission fully reviewed, considered, and evaluated the whole of the record including all public and written comments, pertinent information, documents and plans, prior to taking action regarding the Project.

NOW, THEREFORE, THE MENLO PARK PLANNING COMMISSION HEREBY RESOLVES AS FOLLOWS:

Section 1. Recitals. The Planning Commission has considered the full record before it, which may include but is not limited to such things as the staff report, public testimony, and other materials and evidence submitted or provided, and the Planning Commission finds the foregoing recitals are true and correct, and they are hereby incorporated by reference into this Resolution.

Section 2. Conditional Use Permit Findings. The Planning Commission of the City of Menlo Park does hereby make the following Findings:

The approval of the use permit to demolish an existing one-story residence and construct two new two-story residences on a substandard lot, with excavation in the interior side and rear setbacks for lightwells associated with basements, is granted based on the following findings which are made pursuant to Menlo Park Municipal Code Section 16.82.030:

1. That the establishment, maintenance, or operation of the use applied for will, under the circumstance of the particular case, not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing in the neighborhood of such proposed use, or injurious or detrimental to property and improvements in the neighborhood or the general welfare of the city because:
 - a. Consideration and due regard were given to the nature and condition of all adjacent uses and structures, and to general plans for the area in question and surrounding areas, and impact of the application hereon; in that, the proposed use permit is consistent with the R-2 zoning district and the General Plan because the construction of two, two-story residences is allowed to be constructed on a substandard lot subject to granting of a use

- permit and provided that the proposed residences conform to applicable zoning standards, including, but not limited to, maximum floor area limit, and maximum building coverage.
- b. The proposed residences would include the required number of off-street parking spaces because one covered and one uncovered parking space would be required at a minimum and is provided as such for each proposed residence.
 - c. The proposed Project is designed to meet all the applicable codes and ordinances of the City of Menlo Park Municipal Code and the Commission concludes that the Project would not be detrimental to the health, safety, and welfare of the surrounding community as the new residences would be located in a multi-family neighborhood and designed such that privacy concerns would be addressed through landscaping and second story setbacks greater than the minimum required setbacks in the R-2 district.
 - d. The proposed project would include one lightwell for each unit associated with basements. Unit A's lightwell is proposed to be on the interior setback (left) and unit B's lightwell is proposed in the rear setback. Consideration and due regard were given to the nature and condition of both lightwells, and that they would be limited in size, for the purposes of egress, and the proposed excavations are consistent with the municipal code and are allowed to be constructed, subject to granting of a use permit.

Section 3. Conditional Use Permit. The Planning Commission approves Use Permit No. PLN2021-00032, which Use Permit is depicted in and subject to the development plans which are attached hereto and incorporated herein by this reference as Exhibit A and the project description letter, attached hereto and incorporated herein by this reference as Exhibit B. The Use Permit is conditioned in conformance with the conditions attached hereto and incorporated herein by this reference as Exhibit C

Section . ENVIRONMENTAL REVIEW. The Planning Commission makes the following findings, based on its independent judgment after considering the Project, and having reviewed and taken into consideration all written and oral information submitted in this matter:

- A. The Project is categorically exempt from environmental review pursuant to Cal. Code of Regulations, Title 14, §15303 et seq. (New Construction or Conversion of Small Structures).

Section 5. SEVERABILITY

If any term, provision, or portion of these findings or the application of these findings to a particular situation is held by a court to be invalid, void or unenforceable, the remaining provisions of these findings, or their application to other actions related to the Project, shall continue in full force and effect unless amended or modified by the City.

I, Corinna Sandmeier, Principal Planner and Planning Commission Liaison of the City of Menlo Park, do hereby certify that the above and foregoing Planning Commission Resolution

was duly and regularly passed and adopted at a meeting by said Planning Commission on February 6, 2023, by the following votes:

AYES:

NOES:

ABSENT:

ABSTAIN:

IN WITNESS THEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this 6th day of February, 2023.

Corinna Sandmeier
Principal Planner
City of Menlo Park

Exhibits

- A. Project Plans
- B. Project Description Letter
- C. Conditions of Approval

PARTRIDGE HOMES

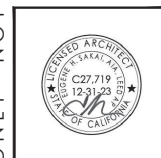
TWO NEW SINGLE FAMILY HOMES

785 PARTRIDGE AVE, MENLO PARK



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San Jose, CA 95128
P : (408) 998 - 0983

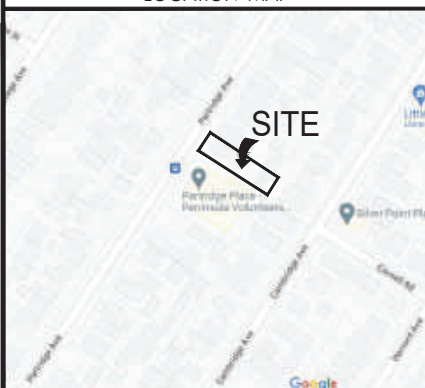
22011 Monterey Development
Residence
TWO NEW SINGLE FAMILY HOMES
Menlo Park, 785 Partridge Avenue
ADAM TOH & XIAOYAN LIU



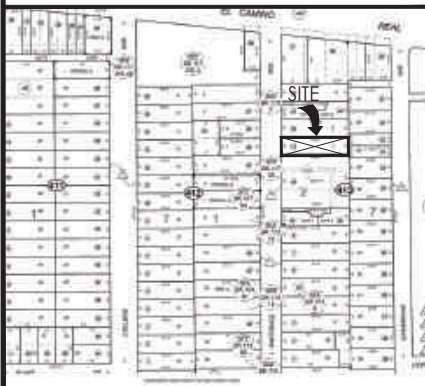
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2022.11.15	LIST PLANNING SUBMITTAL	MC
2022.12.16	PLANNING REV. SUBMITTAL	CS/MC

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LOCATION MAP



ASSESSOR'S PARCEL MAP



SCOPE OF WORK

USE PERMIT TO DEMOLISH AN EXISTING ONE-STORY, SINGLE FAMILY RESIDENCE AND CONSTRUCT TWO NEW TWO-STORY SINGLE FAMILY RESIDENCES

PROJECT SUMMARY

Assessor's Parcel No.	071-413-010	
Zoning	R-2	
Jurisdiction	City of Menlo Park	
Type of Construction:	TYPE V-B, SPRINKLERED	
Building Occ. Groups:	???	
Total	8,000	
Required Property Setbacks (1st / 2nd):		
Front	20'	
Rear	20'	
Right Side	5'	
Left Side	5'	
Max. Area of sunning reign:	5'	
Max. Floor Area	3,200 (40%)	
Max. Second Floor Area	1,200 (15%)	
Max. Lot Coverage	2,800 (35%)	
Total Existing Living Area	1,403	
Total Existing Garage	385	
Total Existing Residence	1,788	
Existing Lot Coverage	1,765	
Proposed Property Setbacks (1st / 2nd):	Bldg A (front)	Bldg B (rear)
Front	21'-9"	105'-11/2"
Rear	103'-5"	20'-1"
Right Side	13'-3"	5'-0"
Left Side	5'-3"	13'-6"
Proposed Building Height	27'-11"	26'-8 1/2"
Floor Area:		
Total Floor Area (Excluding Basement):	1,600	
Total Second Floor Area:	600	
Total New Residence Including Basement	2,517	
Total Lot Floor Area (Excluding Basement)	3,199	
Max. F.A.L.	3,200	
Proposed Site Landscape Coverage	47.2%	
Proposed Lot Coverage	2,621	
Lot Coverage Percentage	32.8%	

DEFERRED SUBMITTALS

REQ'D CONTRACTOR SUBMITTALS TO ARCHITECT

REQ'D CONTRACTOR SUBMITTALS TO BUILDING DEPT. PRIOR TO PERMIT ISSUANCE

APPLICABLE CODES

SHEET INDEX

- ARCHITECTURAL
 - A0.0 COVER SHEET
 - A0.2 FLOOR AREA CALCULATIONS
 - A0.3 AREA PLAN
 - A0.4 STREETScape
 - A1.0 DEMO SITE PLAN & PROPOSED SITE PLAN
 - A2.0a DEMO PLANS & ELEVATIONS
 - A2.1a BLDG A&B - BASEMENT FLOOR PLAN
 - A2.2a BUILDING A - FIRST FLOOR PLAN
 - A2.2b BUILDING B - FIRST FLOOR PLAN
 - A2.3a BLDG A&B - SECOND FLOOR PLAN
 - A2.4a BUILDING A - ROOF PLAN
 - A2.4b BUILDING B - ROOF PLAN
 - A3.0 BLDG. A - FRONT & REAR EXTERIOR ELEVATIONS
 - A3.1 BLDG A. - SIDE EXTERIOR ELEVATIONS
 - A3.2 BLDG B. - FRONT & REAR EXTERIOR ELEVATIONS
 - A3.3 BLDG B. - SIDE EXTERIOR ELEVATIONS
 - A3.4 EXTERIOR PERSPECTIVES
 - A5.0 SECTIONS - BUILDING "A"
 - A5.1 SECTIONS - BUILDING "B"
- ARBORIST REPORT
 - T1.0 ARBORIST REPORT
- TOPOGRAPHIC SURVEY
 - C.0 TOPOGRAPHIC SURVEY
- CIVIL
 - C-1 TITLE SHEET
 - C-2 TENTATIVE PARCEL MAP
 - C-3 PRELIMINARY GRADING & DRAINAGE PLAN
 - C-4 OFFSITE IMPROVEMENT PLAN
 - C-5 PRELIMINARY UTILITY PLAN
 - C-6 DETAILS PLAN
 - C-7 HYDROLOGY MAP
- LANDSCAPE
 - L-1 LANDSCAPE CONSTRUCTION LAYOUT
 - L-2 LANDSCAPE IRRIGATION PLAN
 - L-3 LANDSCAPE PLANTING PLAN
 - L-4 WELO WORKSHEET

PROJECT TEAM

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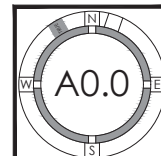
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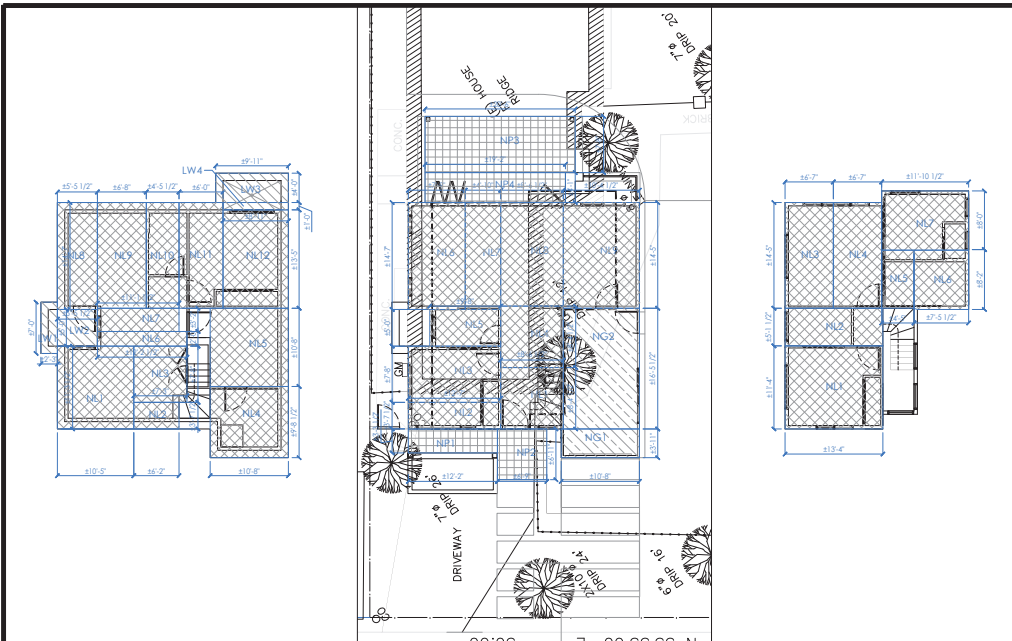
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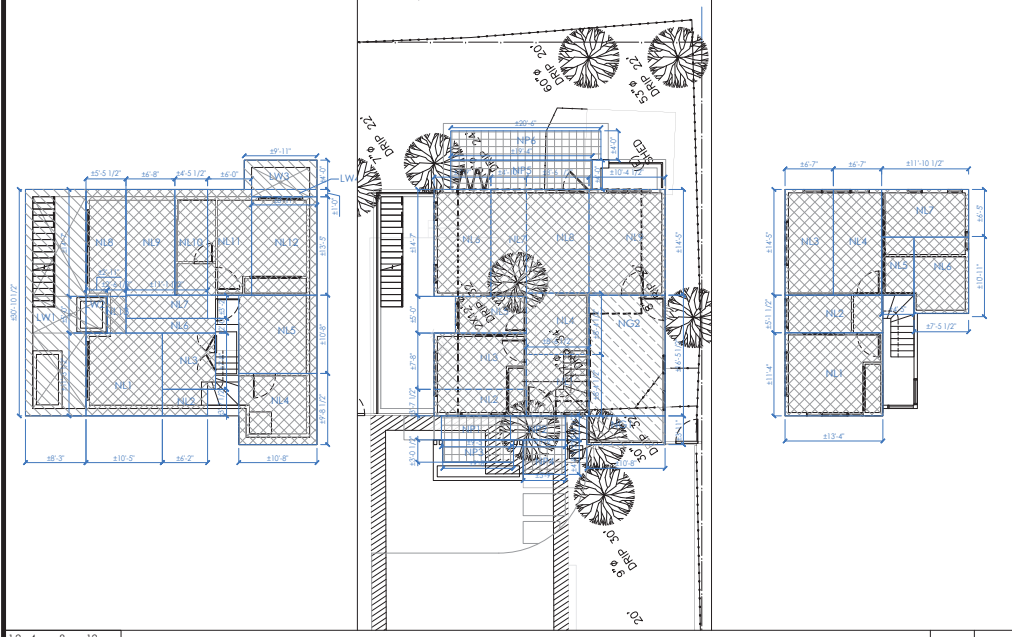
COVER SHEET



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Bldg A (front) FLOOR AREA CALC 1/8" = 1'



Bldg B (rear) FLOOR AREA CALC 1/8" = 2'

New Basement Area/NOT COUNTED TO FAL		Bldg A (front)
NL1	10'-5" x 11'-3 1/2"	117.62
NL2	6'-2" x 3'-7 1/2"	22.35
NL3	7'-8" x 7'-3"	55.58
NL4	10'-8" x 9'-5 1/2"	103.11
NL5	10'-8" x 10'-8"	113.78
NL4	2'-0" x 12'-2 1/2"	24.42
NL7	3'-2" x 11'-1 1/2"	35.23
NL8	14'-7" x 5'-5 1/2"	79.60
NL9	14'-7" x 6'-5"	94.17
NL10	14'-7" x 4'-5 1/2"	64.27
NL11	14'-7" x 6'-0"	86.50
NL12	13'-5" x 8'-11"	119.63
NL13		0.00
NBL Total		916.27

New Lightwell Area/NOT COUNTED TO FAL		Bldg A (front)
LW1	2'-3" x 7'-0"	15.75
LW2	5'-5 1/2" x 5'-0"	27.29
LW3	9'-11" x 4'-0"	39.67
LW4	8'-11" x 1'-0"	8.92
LW Total		91.63

New First Floor Living Area		Bldg A (front)
NL1	8'-4 1/2" x 8'-4 1/2"	71.51
NL2	3'-7 1/2" x 2'-7"	45.61
NL3	7'-8" x 12'-7"	96.47
NL4	8'-4 1/2" x 8'-4 1/2"	71.52
NL5	9'-8" x 5'-0"	48.33
NL6	14'-7" x 7'-5"	113.02
NL7	14'-7" x 4'-10"	70.64
NL8	14'-7" x 8'-6 1/2"	120.50
NL9	14'-7" x 10'-4 1/2"	149.93
NFL Total		787.55

New Living Second Floor Area		Bldg A (front)
NL1	13'-4" x 11'-4"	151.1
NL2	13'-4" x 5'-1 1/2"	68.3
NL3	14'-5" x 6'-7"	94.9
NL4	14'-5" x 6'-7"	94.9
NL5	8'-2" x 4'-5"	35.0
NL6	8'-2" x 7'-5 1/2"	60.1
NL7	11'-10 1/2" x 8'-0"	95.2
NSL Total		600.20

New Porch Area/NOT FAR		Bldg A (front)
NP1	3'-3 1/2" x 12'-2"	39.69
NP2	6'-9" x 6'-1"	46.53
NP3	20'-6" x 7'-8"	157.01
NP4	19'-2" x 4'-"	78.18
NP5		
NP6		
NP Total		321.41

New Garage Area		Bldg A (front)
NG1	10'-8" x 3'-11"	41.73
NG2	10'-8" x 16'-5 1/2"	170.79
NG Total		212.53

LA	Lol Area:	8,000
TEL	Total Existing Living Area	1,403
EG	Total Existing Garage	385
NG	Total New Garage	425
NL	Total New Living Area	2,774
TNL=NFL+NSL	Total New Living Area	1,788
TER=TEL+EG	Total Existing Residence	1,788
TNR=TNL+NG	Total New Residence	3,199
	Max. FAL	3,200
ELC	Existing Lol Coverage	1,965
PLC=NFL+NG+NP	Proposed Lol Coverage	2,621
PLC/LA	Lol Coverage Percentage	32.8% <35% (OK)

New Basement Area/NOT COUNTED		Bldg B (rear)
NL1	10'-5" x 11'-3 1/2"	117.62
NL2	6'-2" x 3'-7 1/2"	22.35
NL3	7'-8" x 7'-3"	55.58
NL4	10'-8" x 9'-5 1/2"	103.11
NL5	10'-8" x 10'-8"	113.78
NL4	2'-0" x 12'-2 1/2"	24.42
NL7	3'-2" x 11'-1 1/2"	35.23
NL8	14'-7" x 5'-5 1/2"	79.60
NL9	14'-7" x 6'-5"	94.17
NL10	14'-7" x 4'-5 1/2"	64.27
NL11	14'-7" x 6'-0"	86.50
NL12	13'-5" x 8'-11"	119.63
NL13	2'-6 1/2" x 5'-0"	12.71
NBL Total		928.98

New Lightwell Area/NOT FAR		Bldg B (rear)
LW1	8'-3" x 30'-10 1/2"	254.72
LW2	5'-5 1/2" x 5'-0"	14.58
LW3	9'-11" x 4'-0"	55.58
LW4	8'-11" x 1'-0"	55.58
LW Total		380.47

New First Floor Living Area		Bldg B (rear)
NL8	4'-1/2" x 8'-4 1/2"	71.51
NL2	3'-7 1/2" x 12'-7"	45.61
NL3	7'-8" x 12'-7"	96.47
NL4	8'-4 1/2" x 8'-4 1/2"	71.52
NL5	9'-8" x 5'-0"	48.33
NL6	14'-7" x 7'-9"	113.02
NL7	14'-7" x 4'-10"	70.64
NL8	14'-7" x 8'-6 1/2"	120.50
NL9	14'-7" x 10'-4 1/2"	149.93
NFL Total		787.55

New Living Second Floor Area		Bldg B (rear)
NL1	13'-4" x 11'-4"	151.1
NL2	13'-4" x 5'-1 1/2"	68.3
NL3	14'-5" x 6'-7"	94.9
NL4	14'-5" x 6'-7"	94.9
NL5	8'-2" x 4'-5"	35.0
NL6	8'-2" x 7'-5 1/2"	77.25
NL7	11'-10 1/2" x 8'-0"	95.2
NSL Total		598.55

New Porch Area/NOT FAR		Bldg B (rear)
NP1	3'-3 1/2" x 12'-2"	39.69
NP2	3'-1/2" x 7'-5 1/2"	24.43
NP3	0-1/2" x 9'-8"	29.34
NP4	5'-9" x 4'-8 1/2"	27.07
NP5	4'-0" x 19'-4"	77.25
NP4	4'-0" x 20'-6"	81.92
NP Total		299.47

New Garage Area		Bldg B (rear)
NG1	10'-8" x 3'-11"	41.73
NG2	10'-8" x 16'-5 1/2"	170.79
NG Total		212.53



FLOOR AREA LEGEND



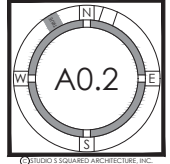
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FLOOR AREA CALCULATION



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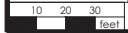
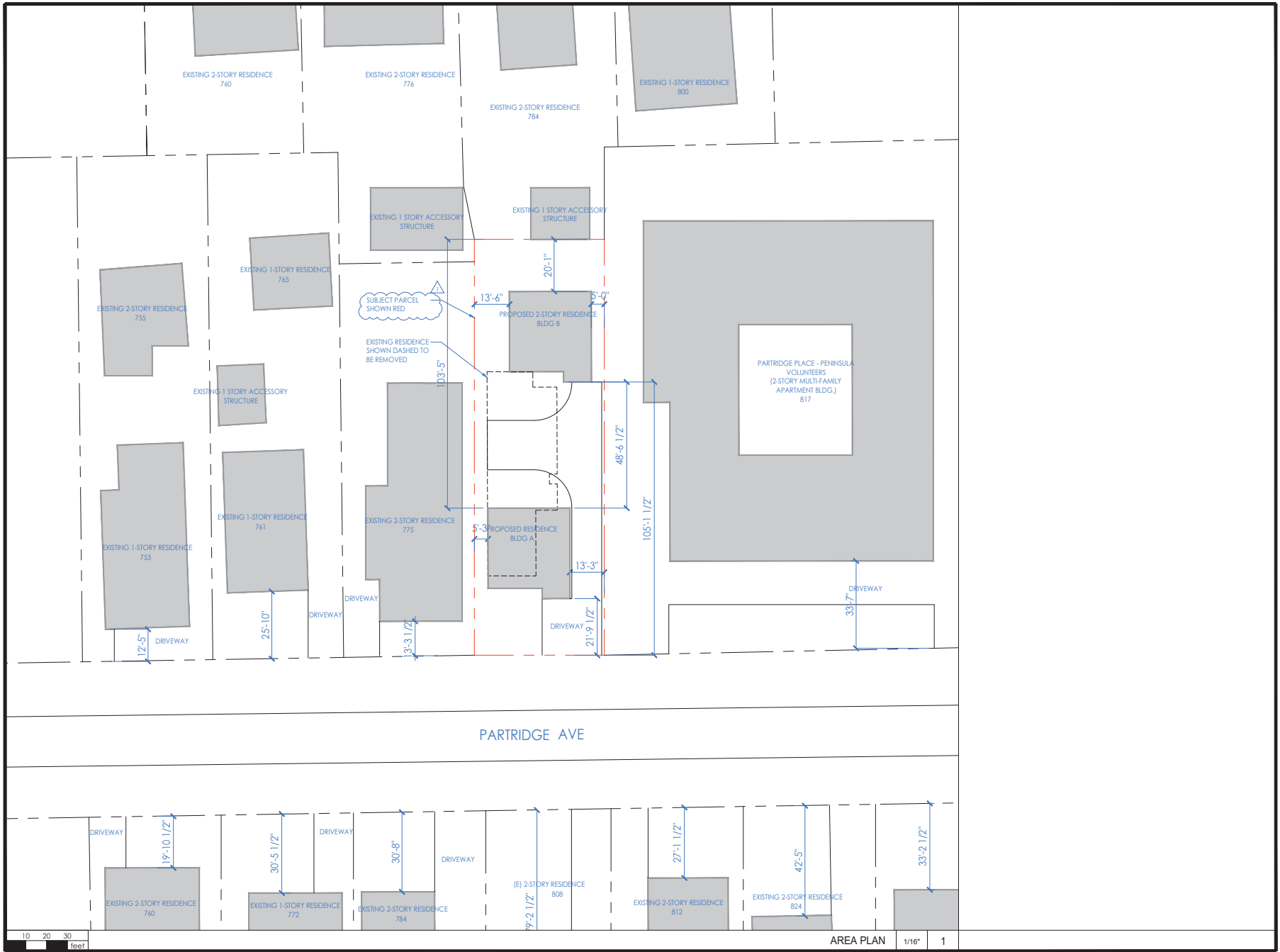
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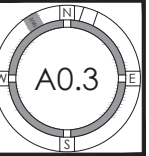
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22011	2022.11.15	1ST PLANNING SUBMITTAL	MC	
	2022.12.16	PLANNING REV. SUBMITTAL	CS/MC	

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AREA PLAN 1/16" = 1'

AREA PLAN





775 PARTRIDGE AVE 785 PARTRIDGE AVE 817 PARTRIDGE AVE PROPOSED BLDG A CONTEXT ELEVATION 1/8" 1



775 PARTRIDGE AVE 785 PARTRIDGE AVE 817 PARTRIDGE AVE PROPOSED BLDG B CONTEXT ELEVATION 1/8" 2



775 PARTRIDGE AVE

761 PARTRIDGE AVE

817 PARTRIDGE AVE

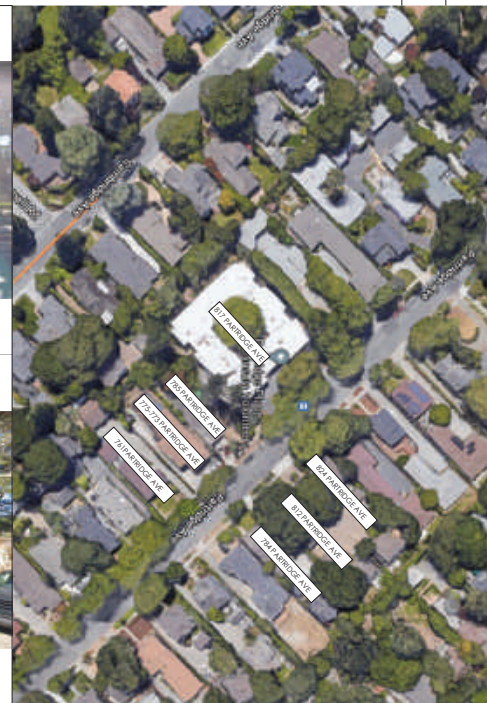


784 PARTRIDGE AVE

812 PARTRIDGE AVE

804 PARTRIDGE AVE

EXTERIOR PERSPECTIVE BACK RIGHT - 6



SITE MAP - 3



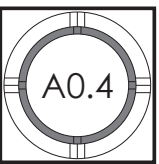
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MONTEREY DEVELOPMENT
TWO NEW SINGLE FAMILY HOMES
MENLO PARK, 785 PARTRIDGE
MONTEREY DEVELOPMENT, LLC

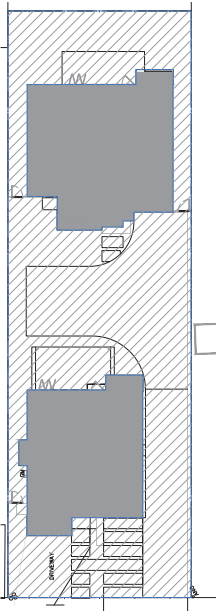


REVISION	DATE	DESCRIPTION	DRAWN BY	CHKD BY
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	2022.12.16	PLANNING REV1 SUBMITTAL SET		

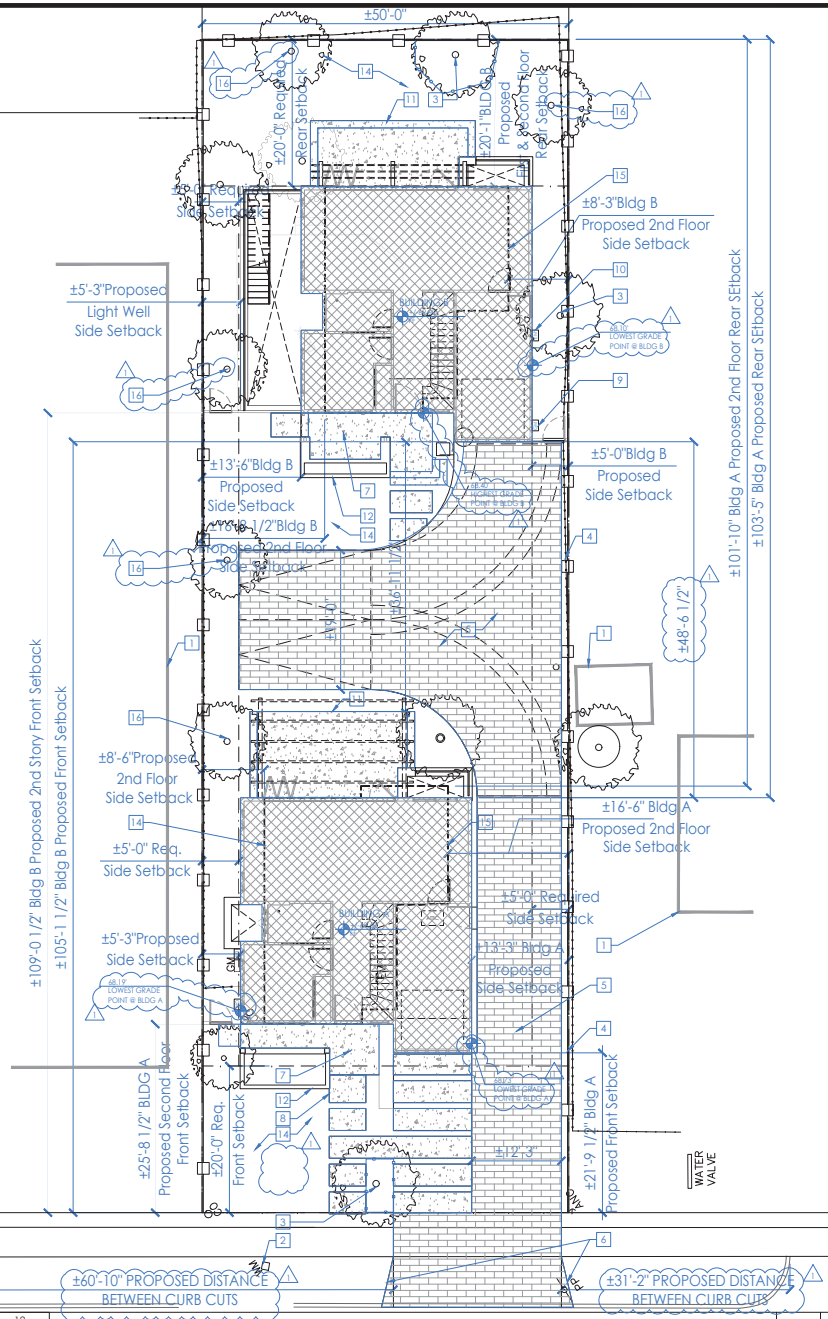
STREETSCAPE



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LANDSCAPE AREA CALCULATION	
Lot Area:	8,000
Building A - First Floor	1,000.07
Building A - Covered Porches	81.60
Building A - Lightwells	91.63
Building B - First Floor	1,000.07
Building A - Covered Porches	81.60
Building B - Lightwells	380.47
Landscape Area	5,364.56
Landscape Percentage	67.1% >40% (OK)



- # = NUMBER TO KEY NOTE BELOW
 APPROXIMATE LOCATION OF NEIGHBORING STRUCTURE
- (E) WATER METER--CONTRACTOR TO COORDINATE (N) METER WITH LOCAL WATER COMPANY IF REQUIRED BY INCREASED FIXTURE LOAD
 - (E) TREE(S) TO REMAIN- PROTECT AS REQUIRED DURING CONSTRUCTION - DO NOT LEAVE MATERIALS OR EQUIPMENT IN ROOT AREAS FOR EXTENDED PERIODS OF TIME. SEE ARBORIST REPORT PREPARED BY AESCULLUS ARBORICULTURAL CONSULTING DATED MARCH 26, 2021 FOR ADDITIONAL TREE PROTECTION MEASURES AND INFORMATION.
 - (E) 6' HIGH WOOD FENCE AT PROPERTY LINE TO REMAIN AND BE PROTECTED DURING CONSTRUCTION.
 - (N) PAVER DRIVEWAY -- VERIFY PAVER DESIGN WITH OWNER & LANDSCAPE ARCHITECT
 - (N) CONCRETE CURB CUT, TO CONFORM TO CITY OF MENLO PARK REQUIREMENTS.
 - (N) ENTRY PORCH, TILE OVER CONCRETE, REFER TO PROPOSED FLOOR PLANS
 - (N) HARDSCAPE, REFER TO LANDSCAPE PLANS
 - (N) GAS METER LOCATION -- INSTALL TWO 2" DIAMETER X 30' TALL STEEL PIPE BOLLARDS EMBEDDED IN 2 FT DEEP CONCRETE FOOTINGS IF GAS METER IS WITHIN 3 FT OF DRIVEWAY
 - (E) ELECTRICAL METER LOCATION -- VERIFY ALL REQUIREMENTS WITH P.G. & E. AND CITY OF MENLO PARK
 - (N) CONCRETE PATIO, TILE OVER CONCRETE, REFER TO PROPOSED FLOOR PLANS
 - (N) BUILT-IN PLANTER
 - (N) 4'-0" HIGH WOOD FENCE WITHIN 20 FT. OF PROPERTY.
 - (N) PLANTING AND LANDSCAPE, REFER TO LANDSCAPE PLANS FOR LAYOUT
 - PROPOSED SECOND FLOOR FOOTPRINT OF BUILDING SHOWN DASHED, TYP.
 - (N) 24" BOX STREET TREE FROM CITY OF MENLO PARK APPROVED TREES, REFER TO LANDSCAPE PLANS FOR ADDL INFO, VERIFY FINAL SELECTION AND LOCATION W/ OWNER, LANDSCAPE ARCHITECT, AND CITY OF MENLO PARK.

SITE PLAN KEYNOTES	
---	PROPERTY LINE--SEE TOPO SURVEY FOR MORE INFO
---	REQUIRED YARD SETBACK/EASEMENT
---	TREE PROTECTION FENCING
---	(E) 6'-0" WOOD FENCE TO REMAIN
---	(N) 4'-0" WOOD FENCE

[Hatched Box]	NEW BUILDING AREA
[Dotted Box]	NEW HARDSCAPE--SEE FLOOR PLAN FOR MORE INFO
⊙	SPOT ELEVATION, SEE CIVIL DRAWINGS FOR MORE INFO
①	TREE NUMBER--REFER TO ARBORIST REPORT FOR SPECIES AND OTHER INFO

- NOTES:
 1. (E) WATER SUPPLY TO BE REPLACED FROM METER IN.
 2. (E) SEWER LATERAL TO BE REPLACED FROM PROPERTY LINE IN.
 3. SEE LS PLANS FOR ALL SITE CONCRETE AND HARDSCAPE DETAILS--CO-ORDINATE WITH CIVL & GEOTECH. REQUIREMENTS

24 8 16 24 feet LANDSCAPE CALC PLAN 1/16" 1

12 4 8 12 feet SITE PLAN 1/8" 1

SITE PLAN LEGEND



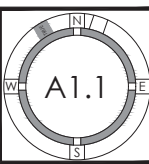
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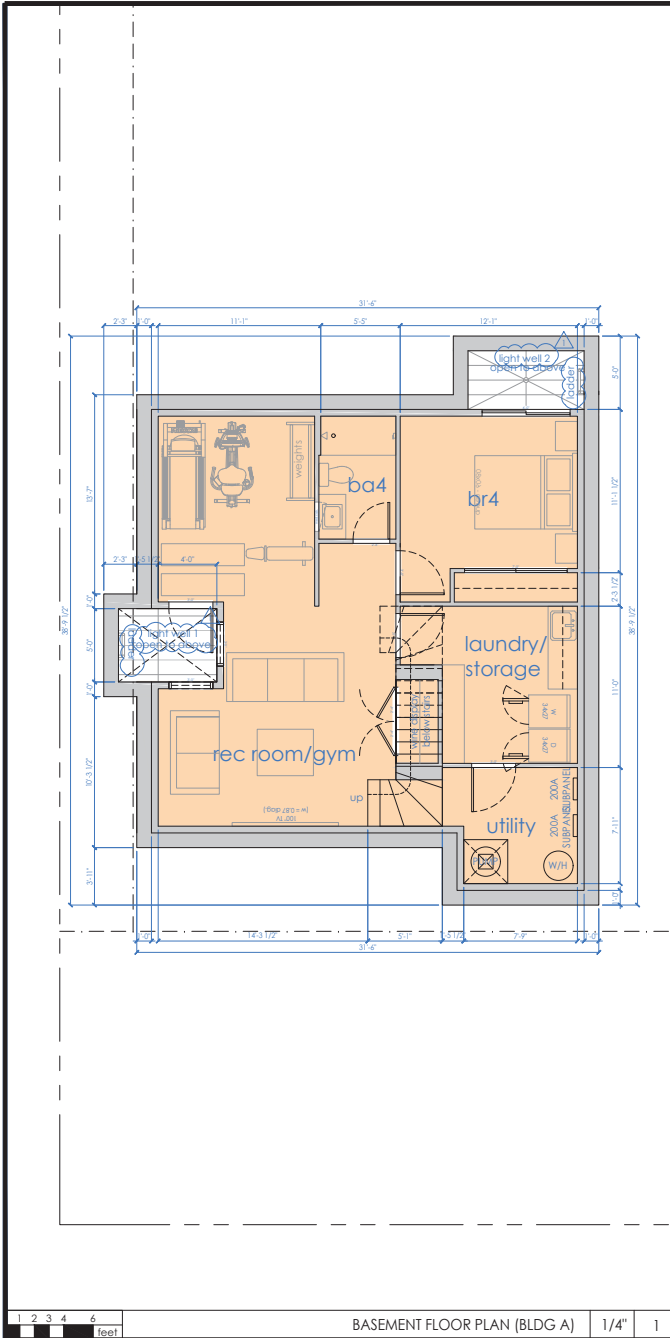
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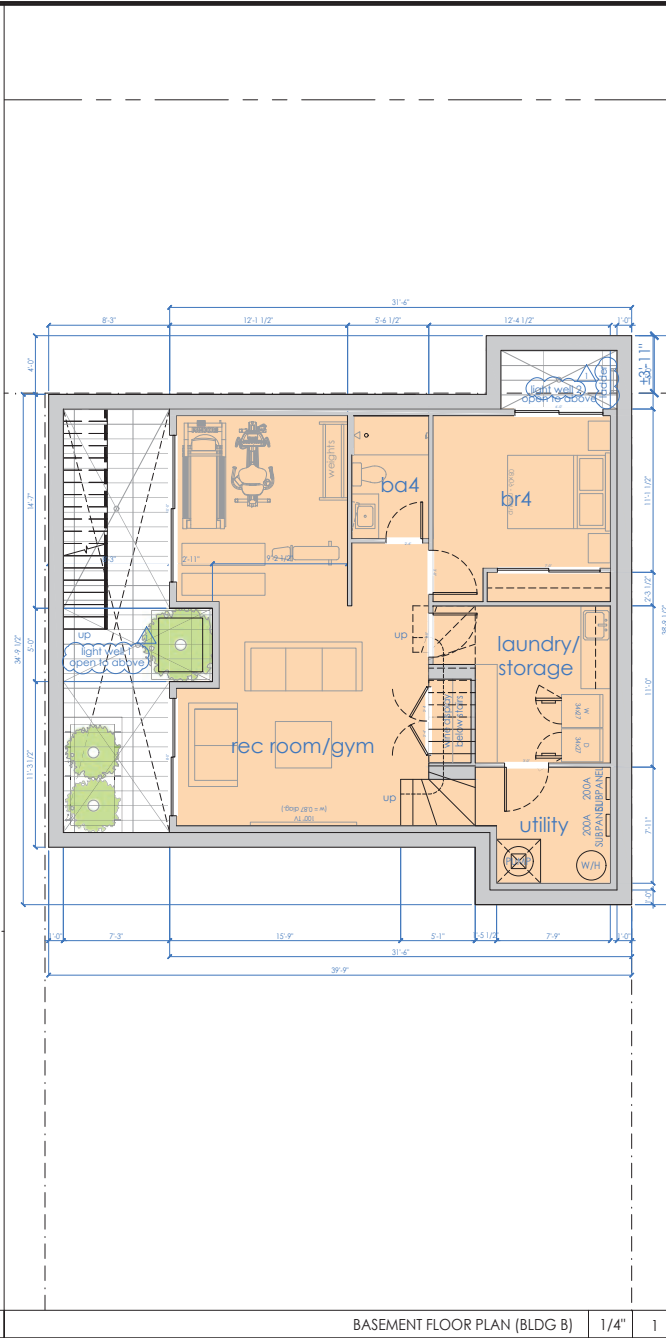
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2022.11.15	1ST PLANNING SUBMITTAL	MC	2022.11.15	1ST PLANNING SUBMITTAL	MC
2022.12.16	PLANNING REVI. SUBMITTAL	CS/MC	2022.12.16	PLANNING REVI. SUBMITTAL	CS/MC

SITE PLAN





BASEMENT FLOOR PLAN (BLDG A) 1/4" 1



BASEMENT FLOOR PLAN (BLDG B) 1/4" 1

FLOOR PLAN KEYNOTES

(N) WALL: EXTERIOR: 2x6 STUDS @ 16" O.C.; INTERIOR 2x4 STUDS @ 16" O.C. - SEE ELEVATIONS AND STRUCTURAL DRAWINGS FOR EXTERIOR WALL MATERIAL ASSEMBLIES. INSTALL 2 LAYERS OF WEATHER RESISTIVE BARRIER (TYVEK HOUSE WRAP OR EQ.) OVER EXTERIOR WALLS SHEATHING PER CRC 703.2 - INSTALL PER MANUF. INSTRUCTIONS. PROVIDE 5/8" TYPE 'X' GYPSUM BOARD EACH SIDE @ INTERIOR PARTITIONS. PROVIDE CEMENT BOARD OR TILE BACKER BOARD AT SHOWER/TUB LOCATIONS. ALL WALLS TO RECEIVE (N) PAINT FINISH. ALL CEILINGS AT TUB/SHOWERS TO BE M.R. BOARD

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2022.12.16	PLANNING REV. SUBMITTAL	CS/MC	

FLOOR PLAN LEGEND



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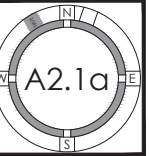
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DATE	DESCRIPTION	DATE	DESCRIPTION
2021	2021.11.15	2021.11.15	2021.11.15

BASEMENT

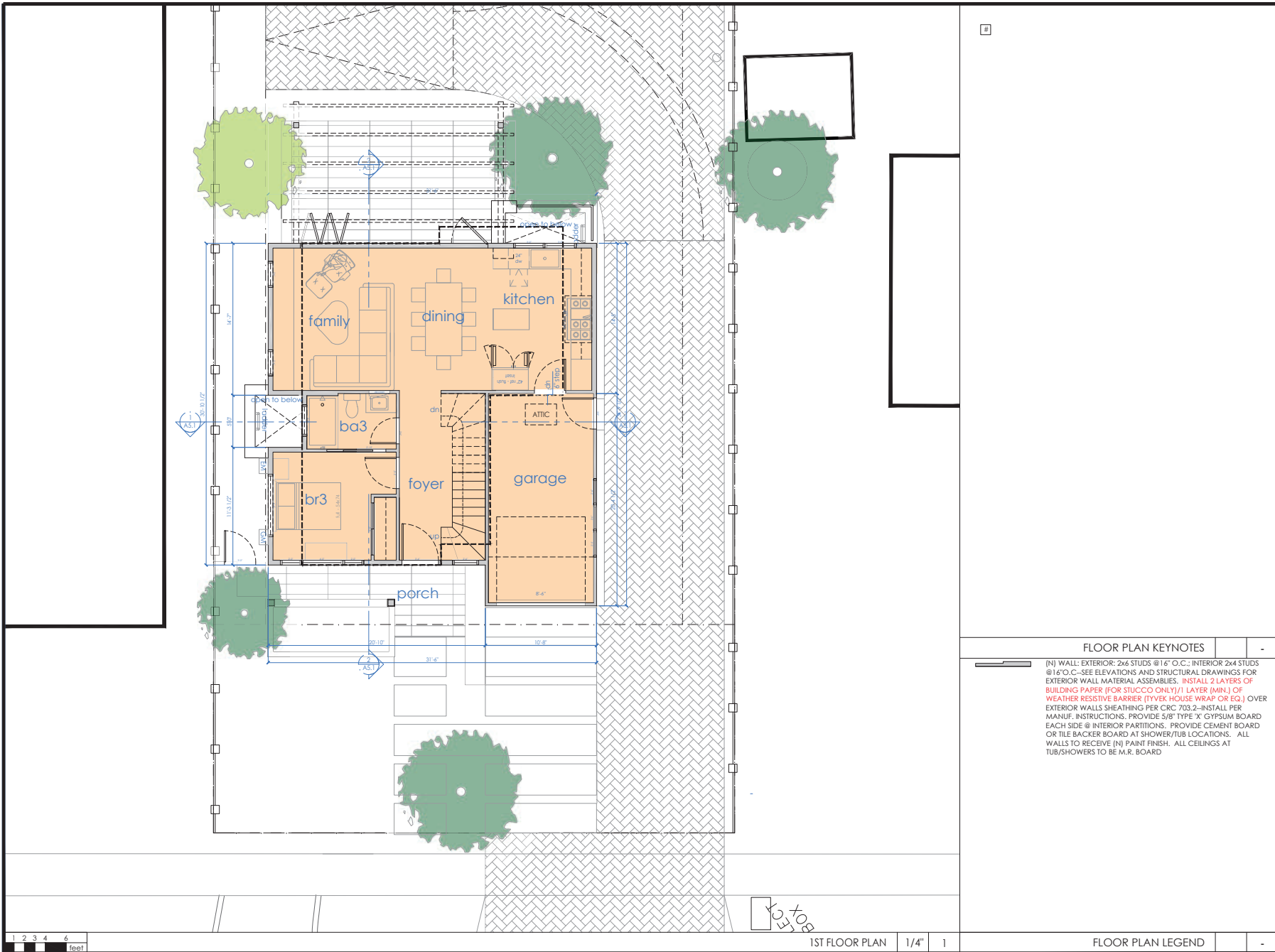
FLOOR PLAN



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FLOOR PLAN KEYNOTES

(N) WALL: EXTERIOR: 2x6 STUDS @ 16" O.C.; INTERIOR 2x4 STUDS @ 16" O.C.—SEE ELEVATIONS AND STRUCTURAL DRAWINGS FOR EXTERIOR WALL MATERIAL ASSEMBLIES. INSTALL 2 LAYERS OF BUILDING PAPER (FOR STUCCO ONLY)/1 LAYER (MIN.) OF WEATHER RESISTIVE BARRIER (TYVEK HOUSE WRAP OR EQ.) OVER EXTERIOR WALLS SHEATHING PER CRC 703.2—INSTALL PER MANUF. INSTRUCTIONS. PROVIDE 5/8" TYPE 'X' GYPSUM BOARD EACH SIDE @ INTERIOR PARTITIONS. PROVIDE CEMENT BOARD OR TILE BACKER BOARD AT SHOWER/TUB LOCATIONS. ALL WALLS TO RECEIVE (N) PAINT FINISH. ALL CEILINGS AT TUB/SHOWERS TO BE M.R. BOARD

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1ST FLOOR PLAN BLDG A





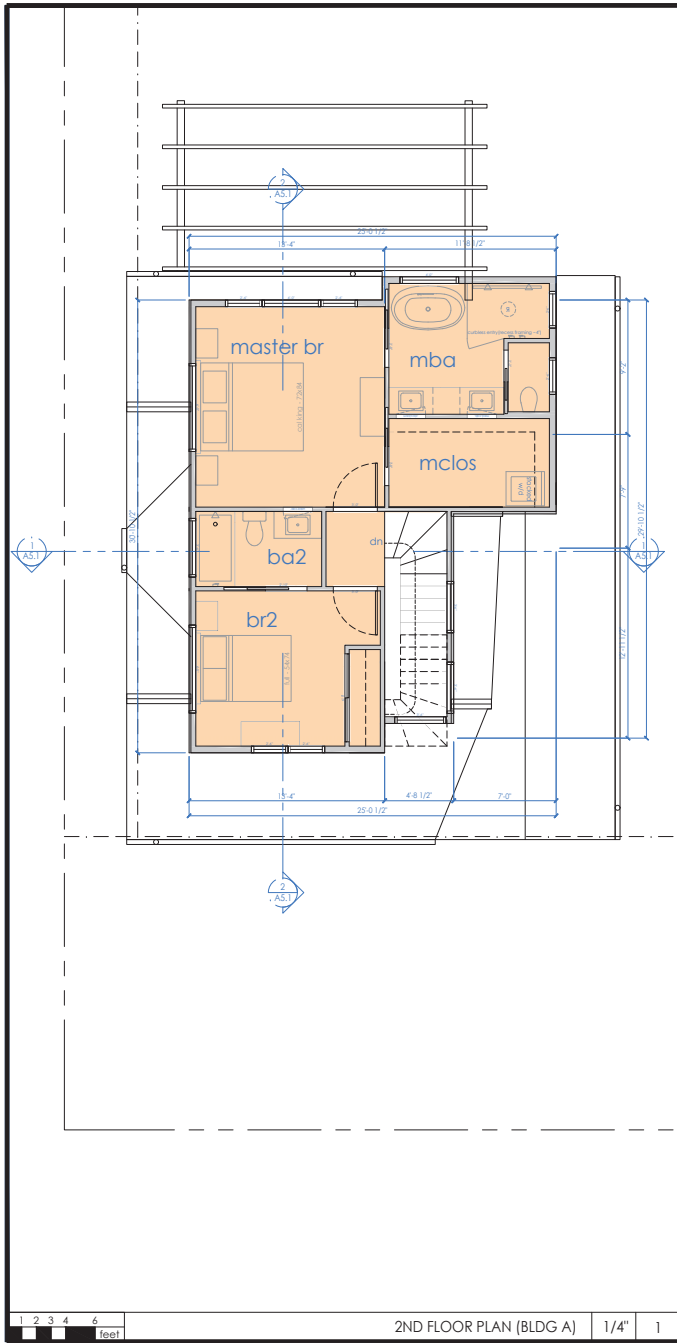
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 San Jose, CA 95128
 P : (408) 998 - 0983

22011 Monterey Development
 Residence
 TWO NEW SINGLE FAMILY HOMES
 Menlo Park, 785 Partridge Avenue
 ADAM TOH & XIAOYAN LIU

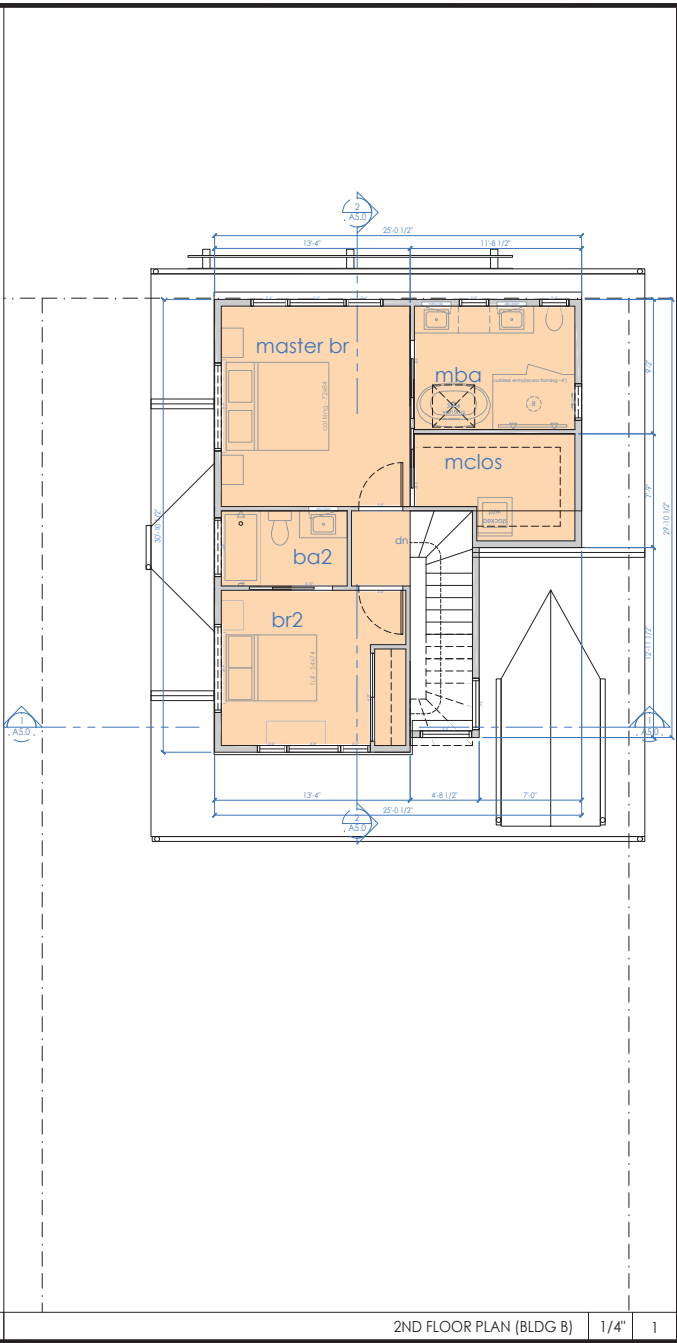


DATE	DESCRIPTION
2022.11.15	1ST PLANNING SUBMITTAL
2022.12.16	PLANNING REV. SUBMITTAL
	CS/MC

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2ND FLOOR PLAN (BLDG A) 1/4" 1



2ND FLOOR PLAN (BLDG B) 1/4" 1

FLOOR PLAN KEYNOTES

(N) WALL: EXTERIOR: 2x6 STUDS @ 16" O.C.; INTERIOR 2x4 STUDS @ 16" O.C.—SEE ELEVATIONS AND STRUCTURAL DRAWINGS FOR EXTERIOR WALL MATERIAL ASSEMBLIES. **INSTALL 2 LAYERS OF BUILDING PAPER (FOR STUCCO ONLY)/1 LAYER (MIN.) OF WEATHER RESISTIVE BARRIER (TYVEK HOUSE WRAP OR EQ.) OVER EXTERIOR WALLS SHEATHING PER CRC 703.2—INSTALL PER MANUF. INSTRUCTIONS. PROVIDE 5/8" TYPE 'X' GYPSUM BOARD EACH SIDE @ INTERIOR PARTITIONS. PROVIDE CEMENT BOARD OR TILE BACKER BOARD AT SHOWER/TUB LOCATIONS. ALL WALLS TO RECEIVE (N) PAINT FINISH. ALL CEILINGS AT TUB/SHOWERS TO BE M.R. BOARD**

FLOOR PLAN LEGEND -



2ND
 FLOOR PLAN

A2.3a

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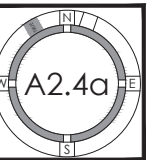
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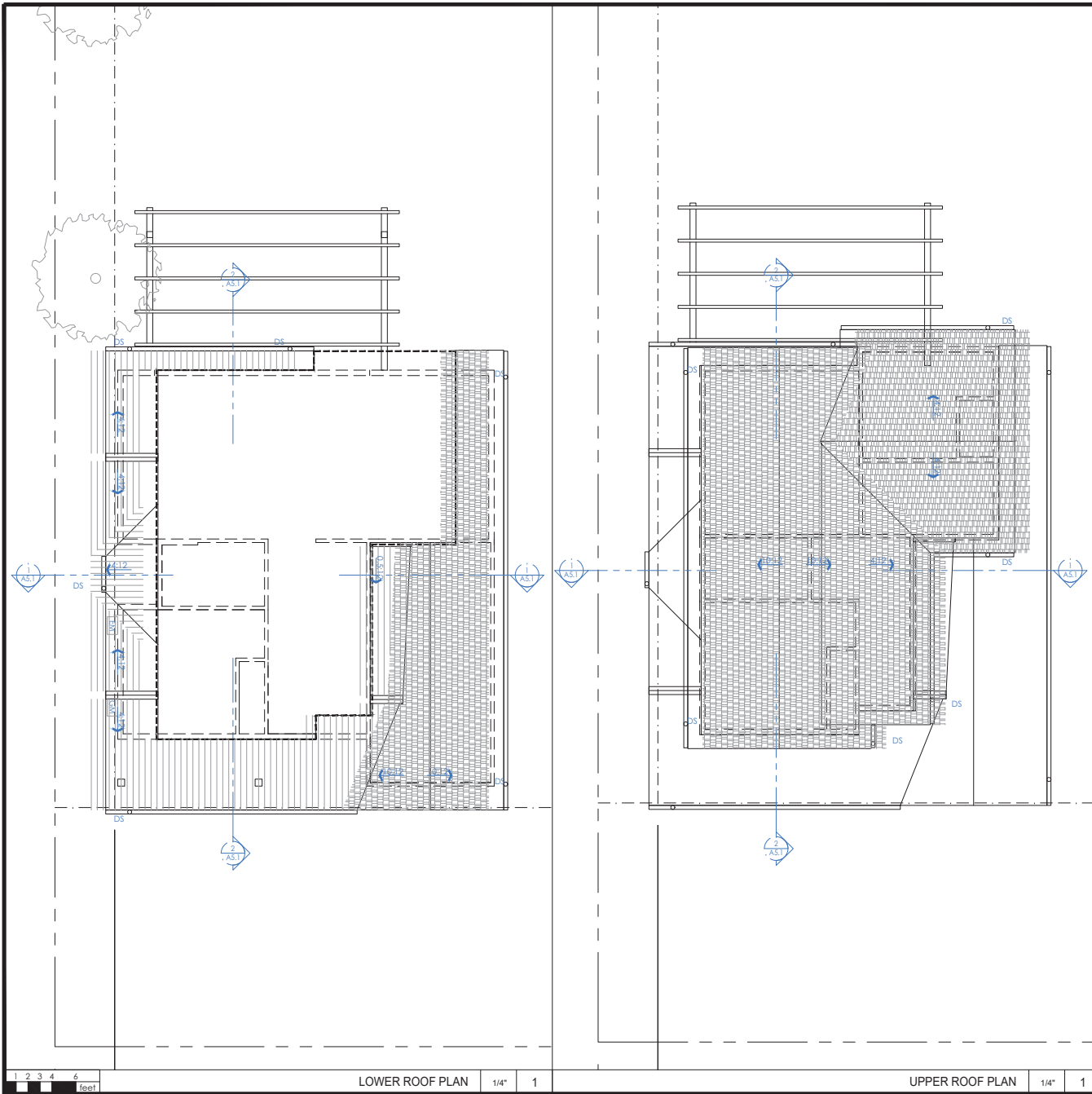
ROOF PLAN

BLDG A



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ROOF GENERAL NOTES

ATTIC VENTILATION CALCULATIONS AND NOTES

- (N) ASPHALT COMPOSITION SHINGLES @ 1 LAYER 15# ROOF FELT (EXCEPT FOR AT ROOF SLOPES BETWEEN 2-4:12, INSTALL 2 LAYERS) PER CRC 905.2.7--MIN. CLASS [C]--MANUF. [CERTAINTED]; STYLE: PRESIDENTIAL; COLOR: [BLACK]; LIFETIME EXPECTANCY--VERIFY FINAL SELECTION WITH OWNER PRIOR TO PLACING ORDER. INSTALL PER MANUF. WARRANTY INSTRUCTIONS AND ICC-ES EVALUATION REPORT #ESR-1389
- (N) STANDING SEAM METAL ROOF, MIN CLASS C--MANUF. [AEP SPAN OR SIMILAR]; STYLE: SELECT NARROW BATTEN; COVERAGE: [16']; GAUGE: [22]; COLOR: ZINC GREY--VERIFY FINAL SELECTION WITH OWNER PRIOR TO PLACING ORDER. INSTALL PER MANUF. WARRANTY INSTRUCTIONS AND [UES EVALUATION REPORT #0309]

- DS DENOTES GUTTER DRAIN (3" DIA.) AND DOWNSPOUT (2" X 3") 26 GA ALUMINUM - PAINTED TO MATCH TRIM COLOR-- VERIFY SPEC. W/ OWNER. INSTALL PER MFR. INSTRUCTIONS
- ← DENOTES DIRECTION OF SLOPE FROM HIGH TO LOW--ROOF SLOPE APPROX. REFER TO ELEVATIONS FOR MAX HT AND VERTICAL CONTROL
- LINE OF BLDG. BELOW

1 2 3 4 6
feet

LOWER ROOF PLAN 1/4" 1

UPPER ROOF PLAN 1/4" 1

ROOF PLAN LEGEND



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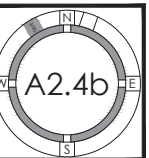
22011 Monterey Development
Residence
TWO NEW SINGLE FAMILY HOMES
Menlo Park, 785 Partridge Avenue
ADAM TOH & XIAOYAN LIU



DATE	DESCRIPTION	22011	DATE	DESCRIPTION
2022.11.15	1ST PLANNING SUBMITTAL	MC		
2022.12.16	PLANNING REVI. SUBMITTAL	CS/MC		

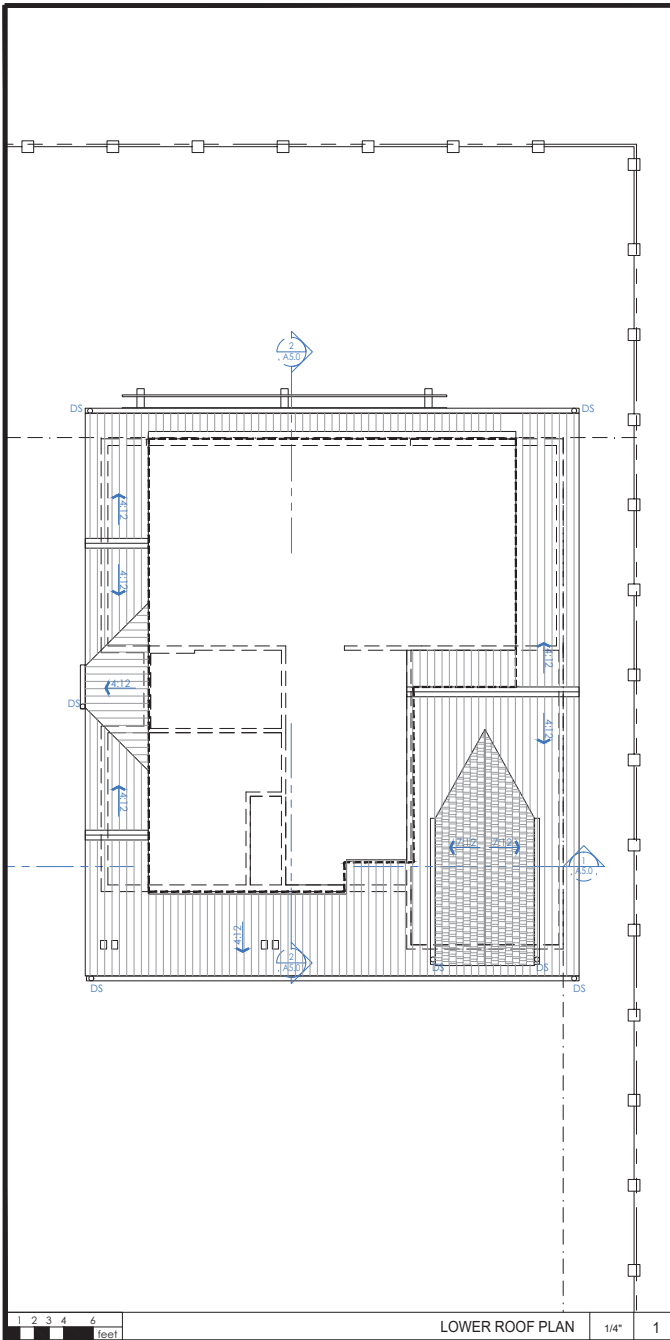
ROOF PLAN

BLDG B

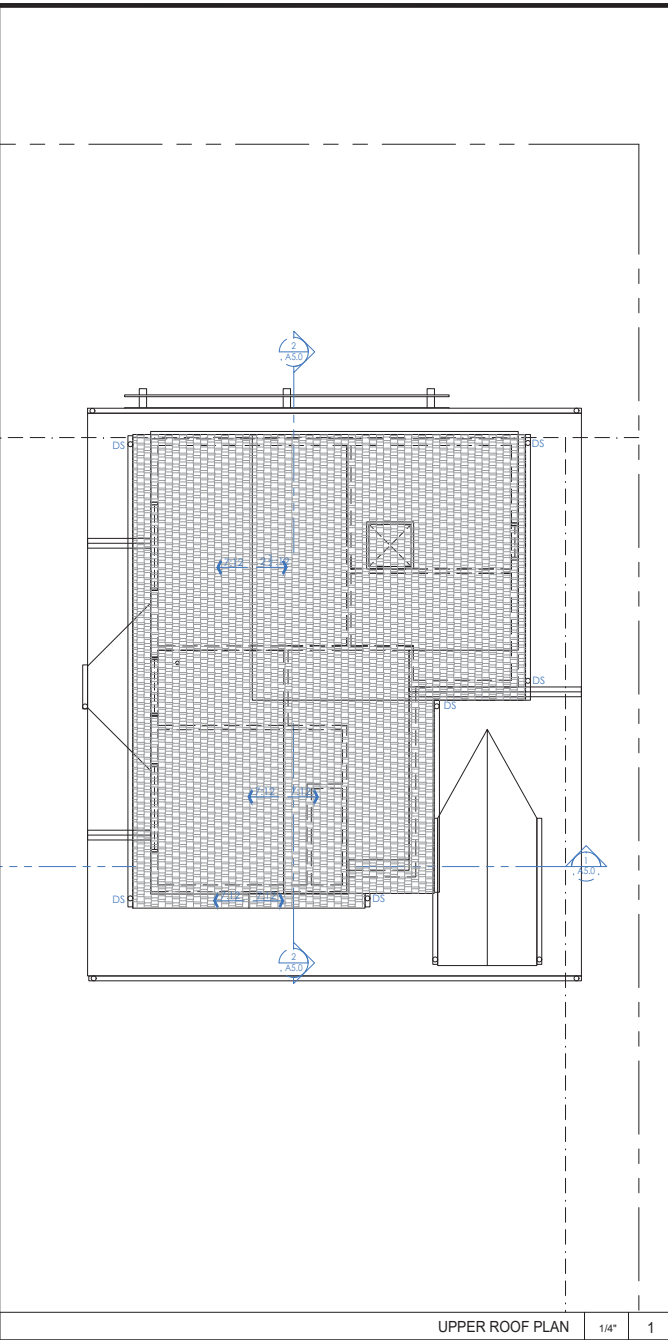


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LOWER ROOF PLAN 1/4" 1



UPPER ROOF PLAN 1/4" 1

ROOF GENERAL NOTES

ATTIC VENTILATION CALCULATIONS AND NOTES

(N) ASPHALT COMPOSITION SHINGLES @ 1 LAYER 1# ROOF FELT (EXCEPT FOR AT ROOF SLOPES BETWEEN 2-4:12, INSTALL 2 LAYERS) PER CRC 905.2.7--MIN. CLASS [C]--MANUF. [CERTAINEED]; STYLE: PRESIDENTIAL; COLOR: [BLACK]; LIFETIME EXPECTANCY--VERIFY FINAL SELECTION WITH OWNER PRIOR TO PLACING ORDER. INSTALL PER MANUF. WARRANTY INSTRUCTIONS AND ICC-ES EVALUATION REPORT #ESR-1389

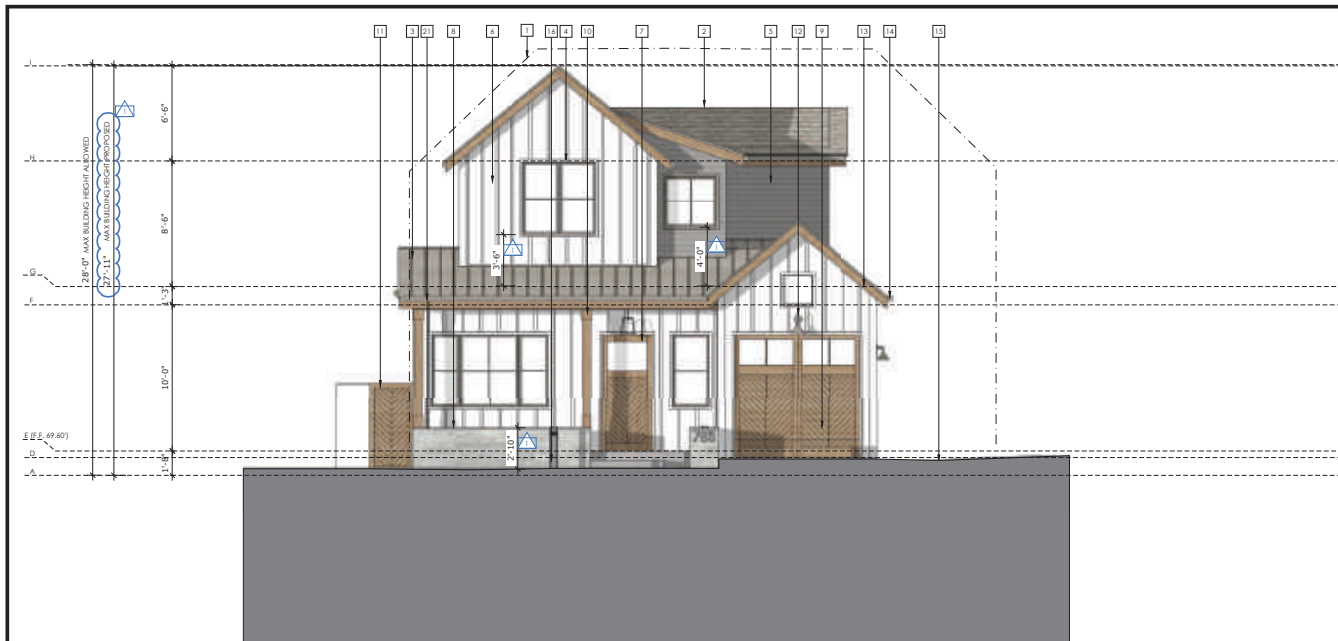
(N) STANDING SEAM METAL ROOF, MIN CLASS C--MANUF. [AEP SPAN OR SIMILAR]; STYLE: SELECT NARROW BATTEN; COVERAGE: [16']; GAUGE: [22]; COLOR: ZINC GREY--VERIFY FINAL SELECTION WITH OWNER PRIOR TO PLACING ORDER. INSTALL PER MANUF. WARRANTY INSTRUCTIONS AND [UES EVALUATION REPORT #0309]

DS DENOTES GUTTER DRAIN (3" DIA.) AND DOWNSPOUT (2" X 3") 26 GA ALUMINUM - PAINTED TO MATCH TRIM COLOR-- VERIFY SPEC. W/ OWNER. INSTALL PER MFR. INSTRUCTIONS

← DENOTES DIRECTION OF SLOPE FROM HIGH TO LOW--ROOF SLOPE APPROX., REFER TO ELEVATIONS FOR MAX HT AND VERTICAL CONTROL

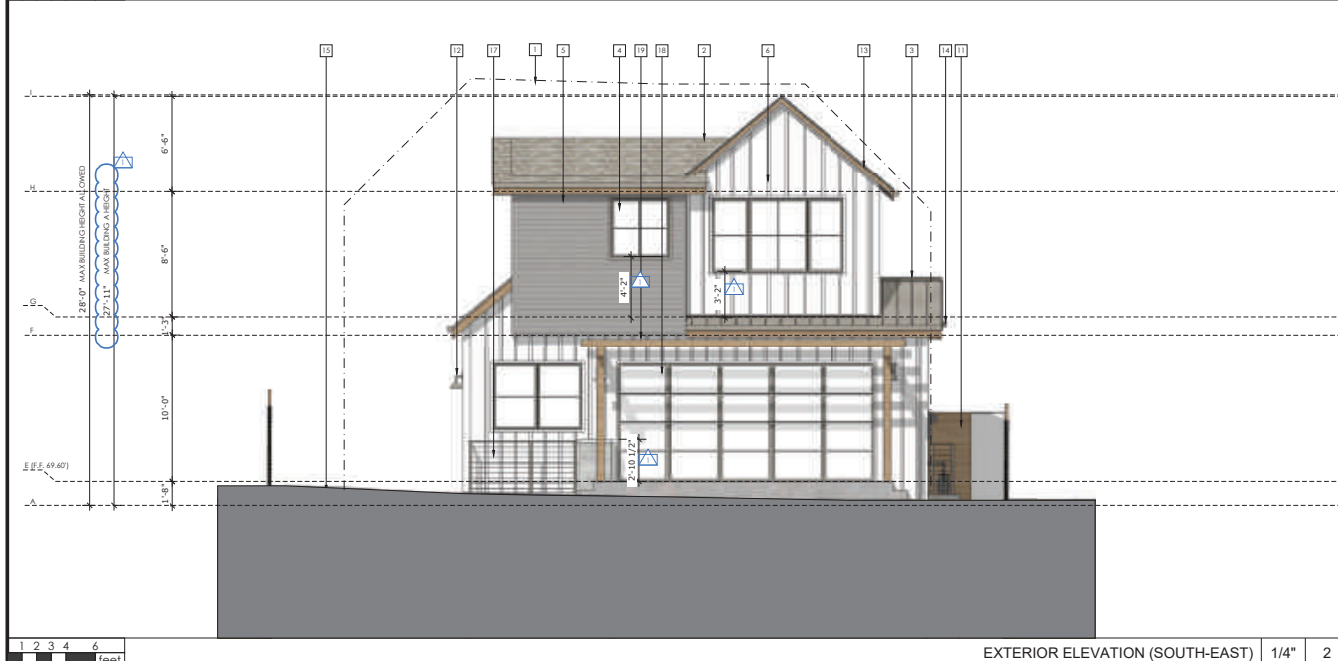
--- LINE OF BLDG. BELOW

ROOF PLAN LEGEND



- KEYNOTE = NUMBER OF KEYNOTE BELOW
- 1 DAYLIGHT PLANE AS DEFINED BY JURISDICTION
 - 2 ASPHALT COMP SHINGLE ROOFING--SEE ROOF PLAN FOR MORE INFO
 - 3 STANDING SEAM METAL ROOFING--SEE ROOF PLAN FOR MORE INFO
 - 4 ALUMINUM WINDOWS; ALL WEATHER 5000 SERIES
 - 5 FIELD PAINTED FIBER CEMENT SIDING w/ 1 LAYER TYVEK HOUSE WRAP--SIDING PROFILE: CHANNEL RUSTIC LAP w/ 1 LAYER TYVEK HOUSE WRAP--SIDING PROFILE: CHANNEL RUSTIC LAP
 - 6 FIELD PAINTED FIBER CEMENT BOARD AND BATTEN SIDING (PARO/PANEL OR EQUAL) w/ 1 LAYER TYVEK HOUSE WRAP. VERTICAL BATTENS TO BE 1/2 CEMENT FIBER TRIM @ 15' O.C.--SIDING TO CONFORM TO CRC TABLE 703.4--INSTALL PER MANUF. WARRANTY INSTRUCTIONS
 - 7 SOLID WOOD ENTRY DOOR WITH TRANSLUCENT GLASS; PINKYS IRON DOORS OR SIMILAR
 - 8 INTEGRAL COLOR CONCRETE PLANTER
 - 9 PAINT GRADE WOOD GARAGE DOOR WITH TEMPERED GLAZING PICTURE WINDOWS--SEE DOOR SCHEDULE FOR MORE INFO
 - 10 WOOD COLUMN
 - 11 STAIN GRADE CEDAR GATE
 - 12 EXTERIOR LIGHT, INSTALL PER MANUF. INSTRUCTIONS--MANUF.
 - 13 STAIN GRADE CEDAR TRIM--2x10 BARGEBOARD AND 1x2 DRIP EDGE
 - 14 PAINTED GUTTER
 - 15 HARDSCAPE--SEE SITE PLAN AND FINISHED FLOOR PLAN FOR MORE INFO
 - 16 GARDEN STREET LAMP BOLLARD LIGHTS
 - 17 FABRICATED METAL RAILING OVER LIGHT WELL 42" FROM ADJACENT FINISHED GRADE. RAILING MUST RESIST A CONCENTRATED LOAD OF 200LBS. APPLIED ANYWHERE ALONG THE TOP RAIL. INTERMEDIATE RAILS MUST BE PLACED SUCH THAT A 4" DIAMETER SPHERE CANNOT PASS THROUGH.
 - 18 ALUMINUM BI-FOLDING DOORS; LA CANTINA OR SIMILAR
 - 19 PATIO WOOD STRUCTURE
 - 20 ALUMINUM DOOR WITH TRANSLUCENT GLASS; PINKYS OR SIMILAR
 - 21 RAIN CHAIN

1 2 3 4 6 feet EXTERIOR ELEVATION (NORTH-WEST) 1/4" 1



- NOTES:
1. SEE 2(A)0.1a FOR PLUMBING GENERAL NOTES
 2. SEE 3(A)0.1a FOR MECHANICAL GENERAL NOTES
 3. SEE 3(A)0.1a FOR ELECTRICAL GENERAL NOTES
 4. SEE 4(A)0.1a FOR PLAN AND INTERIOR GENERAL NOTES
 5. EXTERIOR HARDSCAPE AND EXTERIOR STAIRS NOT SHOWN FOR CLARITY--SEE A0.3a FOR 3D MODEL VIEWS

KEYNOTES	-	-

ELEVATION GRID LINE KEY

A	AVERAGE NATURAL GRADE @ BLDG A = +/- 68.46'
B	BASEMENT FLOOR TOP OF STRUCTURE = +/- 58.33'
C	BASEMENT CEILING HEIGHT (U.N.O.) = +/- 68.33' (10')
D	GARAGE FLOOR TOP OF STRUCTURE = +/- 69.10'
E	1ST FLOOR TOP OF STRUCTURE = +/- 69.60'
F	1ST FLOOR CEILING HEIGHT (U.N.O.) = +/- 79.60' (10')
G	2ND FLOOR TOP OF STRUCTURE (U.N.O.) = +/- 80.85'
H	2ND FLOOR CEILING HEIGHT (U.N.O.) = +/- 90.33' (9.75')
I	PROPOSED BUILDING HEIGHT = +/- 95.91' (27.92')
J	MAX BUILDING HEIGHT ALLOWED = +/- 28'

1 2 3 4 6 feet EXTERIOR ELEVATION (SOUTH-EAST) 1/4" 2

ELEVATION GRID LINE KEY	-	-



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MONTEREY DEVELOPMENT
TWO NEW SINGLE FAMILY HOMES

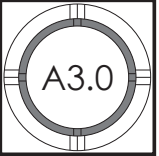
MENLO PARK, 785 PARTTRIDGE

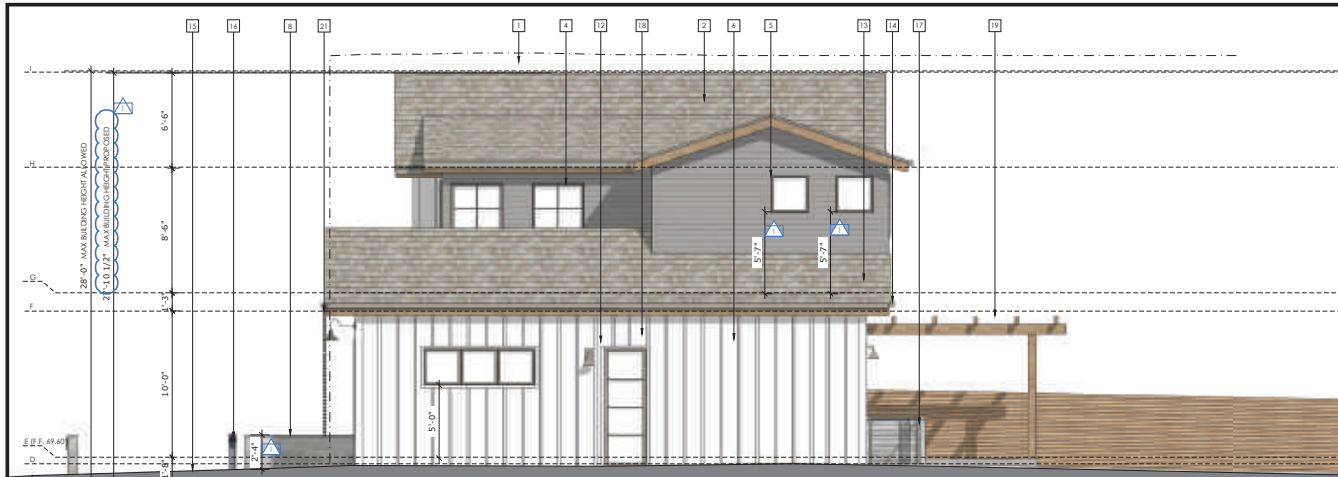
MONTEREY DEVELOPMENT, LLC



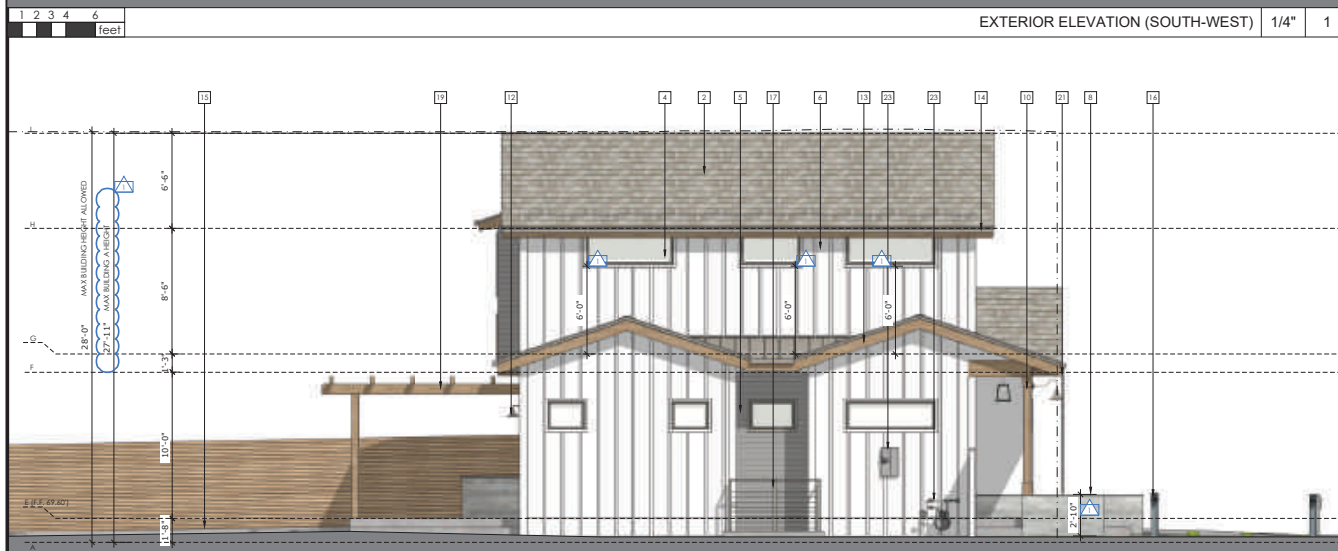
PROJECT NO.	DATE	DESCRIPTION	DRAWN BY	IN/RC
	2022.11.15	1ST PLANNING SUBMITTAL	IS/MC	IS/MC
	2022.12.16	PLANNING REVI SUBMITTAL SET		

EXTERIOR ELEVATIONS
BLDG. A





- KEYNOTE**
- 1 DAYLIGHT PLANE AS DEFINED BY JURISDICTION
 - 2 ASPHALT COMP SHINGLE ROOFING-SEE ROOF PLAN FOR MORE INFO
 - 3 STANDING SEAM METAL ROOFING-SEE ROOF PLAN FOR MORE INFO
 - 4 ALUMINUM WINDOWS, ALLWEATHER 5000 SERIES
 - 5 FIELD PAINTED FIBER CEMENT SIDING w/ 1 LAYER TYVEK HOUSE WRAP-SIDING PROFILE: CHANNEL RUSTIC LAP w/ 1 LAYER TYVEK HOUSE WRAP-SIDING PROFILE: CHANNEL RUSTIC LAP
 - 6 FIELD PAINTED FIBER CEMENT BOARD AND BATTEN SIDING (HARDFACE OR EQUAL) w/ 1 LAYER TYVEK HOUSE WRAP. VERTICAL BATTENS TO BE 1/2 CEMENT FIBER TRIM @ 15' O.C.-SIDING TO CONFORM TO CRC TABLE 703.4-INSTALL PER MANUF. WARRANTY INSTRUCTIONS
 - 7 SOLID WOOD ENTRY DOOR WITH TRANSLUCENT GLASS; PINKY'S IRON DOORS OR SIMILAR
 - 8 INTEGRAL COLOR CONCRETE PLASTER
 - 9 PAINT GRADE WOOD GARAGE DOOR WITH TEMPERED GLAZING PICTURE WINDOWS-SEE DOOR SCHEDULE FOR MORE INFO
 - 10 WOOD COLUMN
 - 11 STAIN GRADE CEDAR GATE
 - 12 EXTERIOR LIGHT, INSTALL PER MANUF. INSTRUCTIONS-MANUF.
 - 13 STAIN GRADE CEDAR TRIM-2x10 BARGEBOARD AND 1x2 DRIP EDGE
 - 14 PAINTED GUTTER
 - 15 HARDSCAPE-SEE SITE PLAN AND FINISHED FLOOR PLAN FOR MORE INFO
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 - 18 ALUMINUM BI-FOLDING DOORS; LA CANTINA OR SIMILAR
 - 19 PATIO WOOD STRUCTURE
 - 20 ALUMINUM DOOR WITH TRANSLUCENT GLASS; PINKY'S OR SIMILAR
 - 21 RAIN CHAIN
 - 22 (N) GAS METER FED BY UNDERGROUND TRENCH, DESIGNED BY OTHERS. VERIFY W/ UTILITY COMPANY & OWNER FOR FINAL LOCATION.
 - 23 (N) ELECTRICAL METER, VERIFY ALL REG. W/ PG&E, CITY INSPECTOR, AND OWNER IN FIELD PRIOR TO WORK.



- KEYNOTES**
- 1 SEE 2/AD.1.a FOR PLUMBING GENERAL NOTES
 - 2 SEE 3/AD.1.a FOR MECHANICAL GENERAL NOTES
 - 3 SEE 3/AD.1.a FOR ELECTRICAL GENERAL NOTES
 - 4 SEE 4/AD.1.a FOR PLAN AND INTERIOR GENERAL NOTES
 - 5 EXTERIOR HARDSCAPE AND EXTERIOR STAIRS NOT SHOWN FOR CLARITY-SEE AD.3.a FOR 3D MODEL VIEWS

ELEVATION GRID LINE KEY

A	AVERAGE NATURAL GRADE @ BLDG A = +/- 68.46'
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H	2ND FLOOR CEILING HEIGHT (U.N.O.) = +/- 90.33' (19.75')
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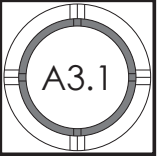
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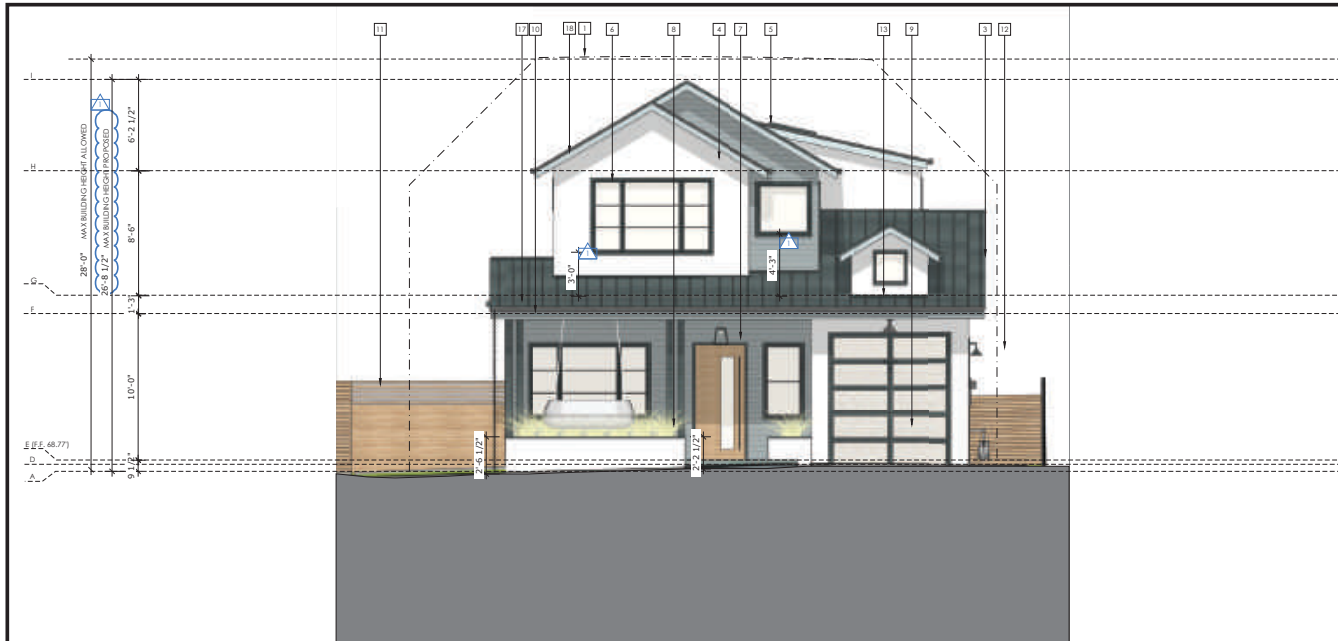
MONTEREY DEVELOPMENT
 TWO NEW SINGLE FAMILY HOMES
 MENLO PARK, 785 PARTTRIDGE
 MONTEREY DEVELOPMENT, LLC



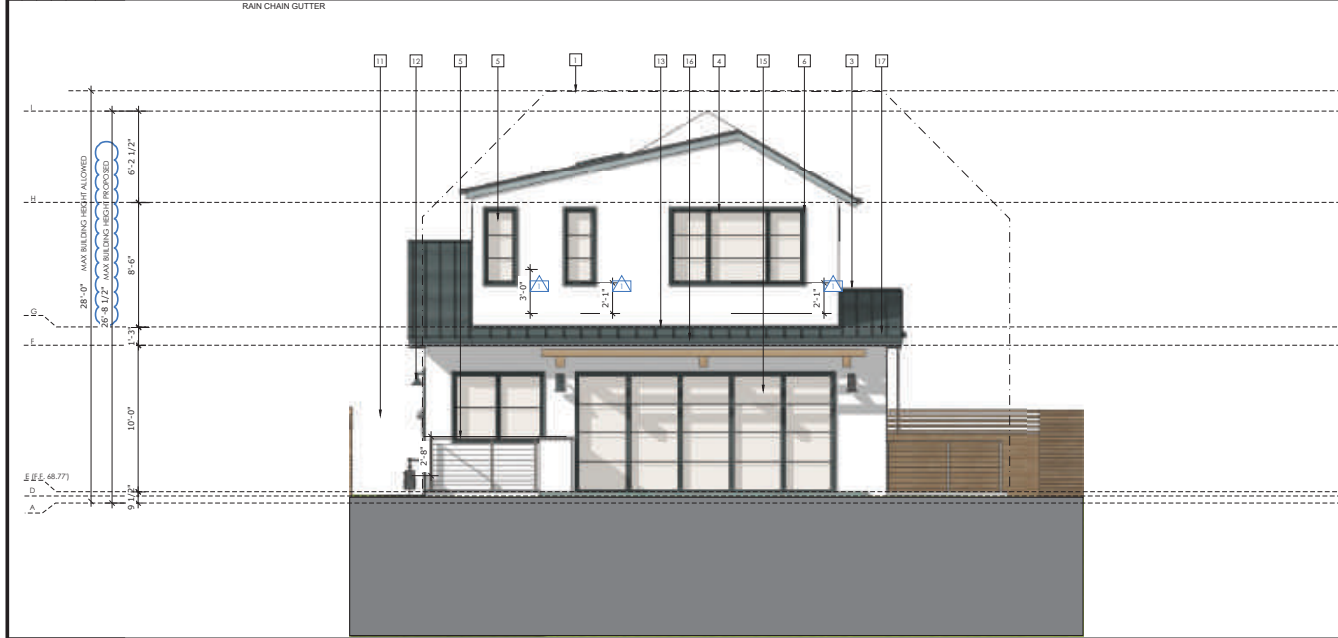
REVISION	DATE	DESCRIPTION	DRAWN BY	CHECKED BY
	2022.11.15	1ST PLANNING SUBMITTAL	NS/MC	NS/MC
	2022.12.16	PLANNING REVI SUBMITTAL SET	NS/MC	NS/MC

EXTERIOR ELEVATIONS BLDG. A





1 2 3 4 6 feet
RAIN CHAIN GUTTER
EXTERIOR ELEVATION (NORTH-WEST) 1/4" 1



1 2 3 4 6 feet
EXTERIOR ELEVATION (SOUTH-EAST) 1/4" 2

- # = NUMBER OF KEYNOTE BELOW
- 1 DAYLIGHT PLANE AS DEFINED BY JURISDICTION
 - 2 ASPHALT COMF SHINGLE ROOFING- SEE ROOF PLAN FOR MORE INFO
 - 3 STANDING SEAM METAL ROOFING- SEE ROOF PLAN FOR MORE INFO
 - 4 ALUMINUM WINDOWS, ALLWEATHER 5000 SERIES
 - 5 FIELD PAINTED FIBER CEMENT SIDING @ 1 LAYER TYVEK HOUSE WRAP- SIDING PROFILE: CHANNEL RUSTIC LAP @ 1 LAYER TYVEK HOUSE WRAP- SIDING PROFILE: CHANNEL RUSTIC LAP
 - 6 PAINTED STEEL TROWELED GIBBERION RESISTANT CEMENT PLASTER SYSTEM (SMOOTH FINISH) - 7/8" PLASTER @ METAL LATH @ 2 LAYERS GRADE 1' OR BETTER BUILDING PAPER, 3 COAT SYSTEM WITH 24 ga. WEEP SCREED AT WALL BASE AT LEAST 4" ABOVE GRADE OR 2" ABOVE HARDSCAPE- SEE DETAILS 7 & 8 & 2- DO NOT USE DOUBLE-ROLL- INSTALLATION FOR BUILDING PAPER
 - 7 SOLID WOOD ENTRY DOOR WITH TRANSLUCENT GLASS; PINKY'S IRON DOORS OR SIMILAR
 - 8 INTEGRAL COLOR CONCRETE PLANTER
 - 9 PAINT FINISHED ALUMINUM GARAGE DOOR WITH TEMPERED GLAZING PICTURE WINDOWS; SUSAS GARAGE DOOR OR SIMILAR
 - 10 STAIN-GRADE WOOD COLUMN, TYP.
 - 11 6'-0" TALL WOOD FENCE, VERIFY FINAL DESIGN W/ OWNER.
 - 12 EXTERIOR LIGHT, INSTALL PER MANUF. INSTRUCTIONS-MANUF.
 - 13 PAINTED GUTTER
 - 14 HARDSCAPE- SEE SITE PLAN AND FINISHED FLOOR PLAN FOR MORE INFO
 - 15 ALUMINUM BI-FOLDING DOORS; LA CANTINA OR SIMILAR
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 - 17 RAIN CHAIN
 - 18 FIELD PAINTED FIBER CEMENT TRIM- 2x10 BARGEBOARD AND 1/2 DRIP EDGE
 - 19 FABRICATED METAL RAILING OVER LIGHT WELL 42" FROM ADJACENT FINISHED GRADE. RAILING MUST RESIST A CONCENTRATED LOAD OF 200LBS. APPLIED ANYWHERE ALONG THE TOP RAIL. INTERMEDIATE RAILS MUST BE PLACED SUCH THAT A 4" DIAMETER SPHERE CANNOT PASS THROUGH.
- (N) GAS METER FED BY UNDERGROUND TRENCH, DESIGNED BY OTHERS. VERIFY W/ UTILITY COMPANY & OWNER FOR FINAL LOCATION.
- (E) ELECTRICAL METER, VERIFY ALL REG. W/ PG&E, CITY INSPECTOR, AND OWNER IN FIELD PRIOR TO WORK.

KEYNOTE	DESCRIPTION
1	DAYLIGHT PLANE AS DEFINED BY JURISDICTION
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4	ALUMINUM WINDOWS, ALLWEATHER 5000 SERIES
5	FIELD PAINTED FIBER CEMENT SIDING @ 1 LAYER TYVEK HOUSE WRAP- SIDING PROFILE: CHANNEL RUSTIC LAP @ 1 LAYER TYVEK HOUSE WRAP- SIDING PROFILE: CHANNEL RUSTIC LAP
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1	2022.11.15	1ST PLANNING SUBMITTAL
2	2022.12.16	PLANNING REVI SUBMITTAL SET

- NOTES:
1. SEE 2/AD.1a FOR PLUMBING GENERAL NOTES
 2. SEE 3/AD.1a FOR MECHANICAL GENERAL NOTES
 3. SEE 3/AD.1a FOR ELECTRICAL GENERAL NOTES
 4. SEE 4/AD.1a FOR PLAN AND INTERIOR GENERAL NOTES
 5. EXTERIOR HARDSCAPE AND EXTERIOR STAIRS NOT SHOWN FOR CLARITY- SEE A3.3a FOR 3D MODEL VIEWS

KEYNOTES	-	-

ELEVATION GRID LINE KEY	-	-
A	AVERAGE NATURAL GRADE @ BLDG.	8 +/- 68.25'
B	BASEMENT FLOOR TOP OF STRUCTURE	+/- 57.83'
C	BASEMENT CEILING HEIGHT (U.N.O.)	+/- 67.83' (10')
D	GARAGE FLOOR TOP OF STRUCTURE	+/- 68.35'
E	1ST FLOOR TOP OF STRUCTURE	+/- 68.77'
F	1ST FLOOR CEILING HEIGHT (U.N.O.)	+/- 78.77' (10')
G	2ND FLOOR TOP OF STRUCTURE (U.N.O.)	+/- 80.02'
H	2ND FLOOR CEILING HEIGHT (U.N.O.)	+/- 88.52' (8.5')
I	PROPOSED BUILDING HEIGHT	+/- 94.69' (26.71')
J	MAX BUILDING HEIGHT ALLOWED	+/- 28'

ELEVATION GRID LINE KEY - -



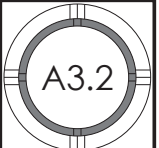
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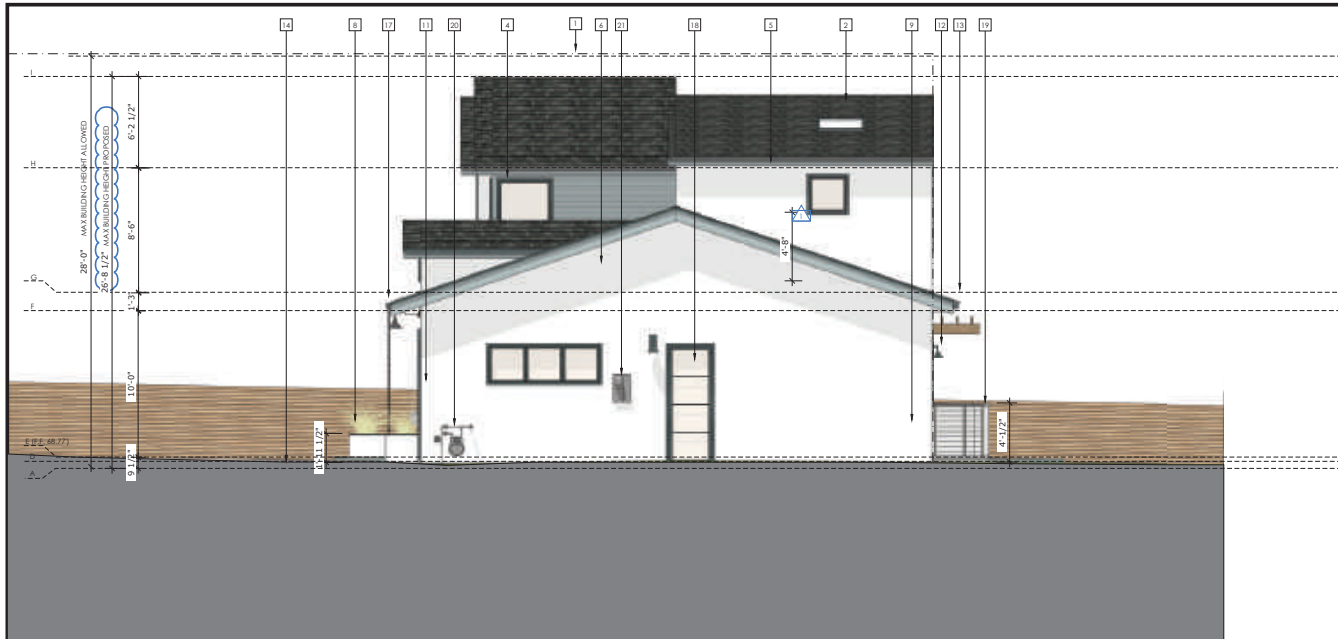
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TWO NEW SINGLE FAMILY HOMES
MENLO PARK, 785 PARTTRIDGE
MONTEREY DEVELOPMENT, LLC



REVISION	DATE	DESCRIPTION
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EXTERIOR ELEVATIONS
BLDG. B





EXTERIOR ELEVATION (SOUTH-WEST) 1/4" 1



EXTERIOR ELEVATION (NORTH-EAST) 1/4" 2

- KEY = NUMBER OF KEYNOTE BELOW
- 1 DAYLIGHT PLANE AS DEFINED BY JURISDICTION
 - 2 ASPHALT COMB SHINGLE ROOFING-SEE ROOF PLAN FOR MORE INFO
 - 3 STANDING SEAM METAL ROOFING-SEE ROOF PLAN FOR MORE INFO
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 - 5 STAIN GRADE CEDAR SIDING w/ 1 LAYER TYVEK HOUSE WRAP-SIDING PROFILE: CHANNEL RUSTIC LAP
 - 6 PAINTED STEEL TROWELED IGNITION RESISTANT CEMENT PLASTER SYSTEM (SMOOTH FINISH - 7/8" PLASTER ON METAL LATH w/ 2 LAYERS GRADE 1' OR BETTER BUILDING PAPER, 3 COAT SYSTEM WITH 26 Gd. WEEP SCREED AT WALL BASE AT LEAST 4" ABOVE GRADE OR 2" ABOVE HARDSCAPE-SEE DETAILS 7 & 8/A&2- DO NOT USE "DOUBLE-ROLL" INSTALLATION FOR BUILDING PAPER
 - 7 SOLID WOOD ENTRY DOOR WITH TRANSLUCENT GLASS; PINKY'S IRON DOORS OR SIMILAR
 - 8 INTEGRAL COLOR CONCRETE PLANTER
 - 9 PAINT FINISHED ALUMINUM GARAGE DOOR WITH TEMPERED GLAZING PICTURE WINDOWS; SUSAS GARAGE DOOR OR SIMILAR
 - 10 STAIN-GRADE WOOD COLUMBI TYP.
 - 11 4'-0" TALL WOOD FENCE, VERIFY FINAL DESIGN W/ OWNER
 - 12 EXTERIOR LIGHT, INSTALL PER MANUF. INSTRUCTIONS-MANUF.
 - 13 PAINTED GUTTER
 - 14 HARDSCAPE-SEE SITE PLAN AND FINISHED FLOOR PLAN FOR MORE INFO
 - 15 ALUMINUM BI-FOLDING DOORS; LA CANTINA OR SIMILAR
 - 16 PATIO WOOD STRUCTURE
 - 17 RAIN CHAIN
 - 18 ALUMINUM DOOR WITH TRANSLUCENT GLASS; PINKY'S OR SIMILAR
 - 19 FABRICATED METAL RAILING OVER LIGHT WELL 42" FROM ADJACENT FINISHED GRADE. RAILING MUST RESIST A CONCENTRATED LOAD OF 200LBS. APPLIED ANYWHERE ALONG THE TOP RAIL. INTERMEDIATE RAILS MUST BE PLACED SUCH THAT A 4" DIAMETER SPHERE CANNOT PASS THROUGH
 - 20 (N) GAS METER FED BY UNDERGROUND TRENCH, DESIGNED BY OTHERS. VERIFY W/ UTILITY COMPANY & OWNER FOR RAIL LOCATION.
 - 21 (N) ELECTRICAL METER, VERIFY ALL REQ. W/ PG&E, CITY INSPECTOR, AND OWNER IN FIELD PRIOR TO WORK.

PROJ. NO.	DATE	DESCRIPTION	DRAWN BY	REVISION
	2022.11.15	1ST PLANNING SUBMITTAL		
	2022.12.16	PLANNING REVI SUBMITTAL SET		

- NOTES:
1. SEE 2/A0.1a FOR PLUMBING GENERAL NOTES
 2. SEE 3/A0.1a FOR MECHANICAL GENERAL NOTES
 3. SEE 3/A0.1a FOR ELECTRICAL GENERAL NOTES
 4. SEE 4/A0.1a FOR PLAN AND INTERIOR GENERAL NOTES
 5. EXTERIOR HARDSCAPE AND EXTERIOR STAIRS NOT SHOWN FOR CLARITY-SEE A0.3a FOR 3D MODEL VIEWS

KEYNOTES	-	-

ELEVATION GRID LINE KEY

A	AVERAGE NATURAL GRADE @ BLDG. 8 = +/- 68.25'
B	BASEMENT FLOOR TOP OF STRUCTURE = +/- 57.83'
C	BASEMENT CEILING HEIGHT (U.N.O.) = +/- 67.83' (10')
D	GARAGE FLOOR TOP OF STRUCTURE = +/- 68.35'
E	1ST FLOOR TOP OF STRUCTURE = +/- 68.77'
F	1ST FLOOR CEILING HEIGHT (U.N.O.) = +/- 78.77' (10')
G	2ND FLOOR TOP OF STRUCTURE (U.N.O.) = +/- 80.02'
H	2ND FLOOR CEILING HEIGHT (U.N.O.) = +/- 88.52' (8.5')
J	PROPOSED BUILDING HEIGHT = +/- 94.69' (26.71')
	MAX BUILDING HEIGHT ALLOWED = +/- 28'

ELEVATION GRID LINE KEY	-	-



1005 S. Winchester Blvd
San Jose, CA 95128
P : (408) 998 - 0983

MONTEREY DEVELOPMENT
TWO NEW SINGLE FAMILY HOMES

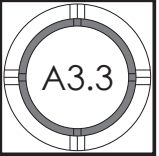
MENLO PARK, 785 PARTTRIDGE

MONTEREY DEVELOPMENT, LLC



PROJ. NO.	DATE	DESCRIPTION	DRAWN BY	REVISION
	2022.11.15	1ST PLANNING SUBMITTAL		
	2022.12.16	PLANNING REVI SUBMITTAL SET		

EXTERIOR ELEVATIONS
BLDG. B





EXTERIOR PERSPECTIVE FROM PARTRIDGE AVENUE - NORTH - 4



EXTERIOR PERSPECTIVE DOWN DRIVEWAY TOWARDS BLDG B - 1



EXTERIOR PERSPECTIVE FROM PARTRIDGE AVENUE - SOUTH - 5



EXTERIOR PERSPECTIVE TOWARDS BLDG B - 2



EXTERIOR REAR PERSPECTIVE OF BUILDING A - 6



EXTERIOR REAR PERSPECTIVE AT BACK OF LOT - BLDG B - 3



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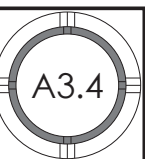
MONTEREY DEVELOPMENT
TWO NEW SINGLE FAMILY HOMES

MENLO PARK, 785 PARTRIDGE
MONTEREY DEVELOPMENT, LLC



REVISION	DATE	DESCRIPTION	DRAWN BY	CHECKED BY
	2022.11.15	1ST PLANNING SUBMITTAL	DS/MC	DS/MC
	2022.12.16	PLANNING REV1 SUBMITTAL SET	DS/MC	DS/MC

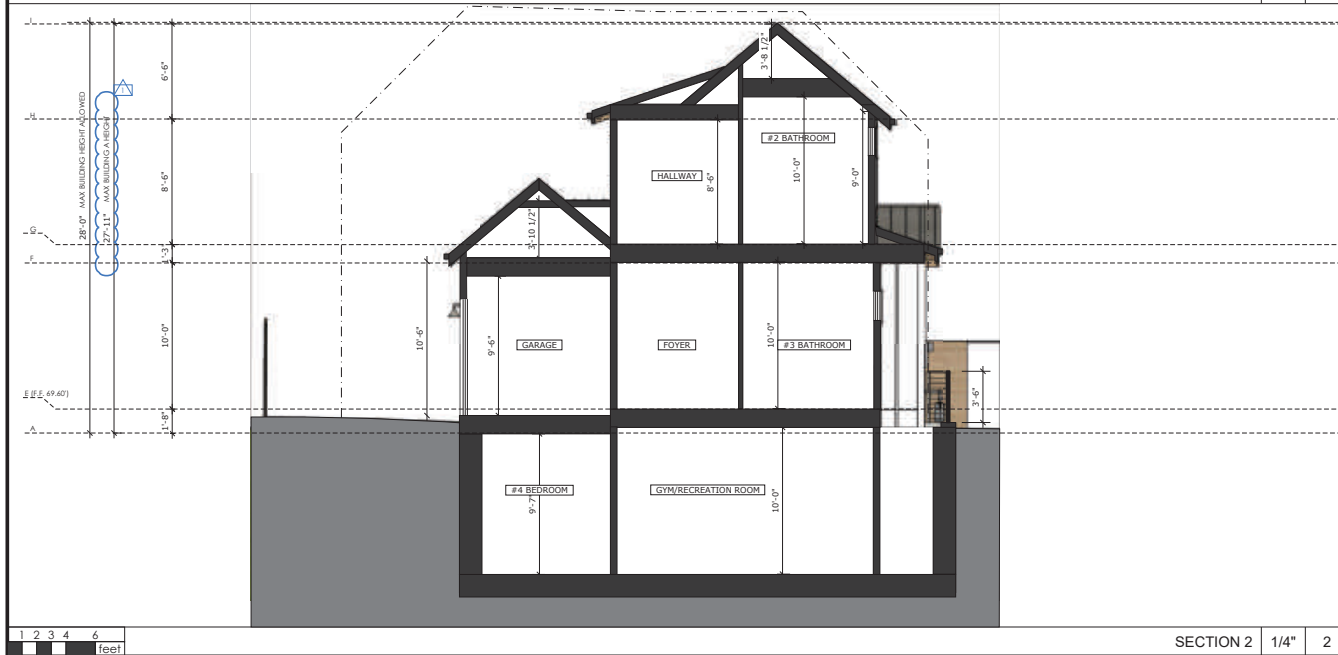
EXTERIOR PERSPECTIVES



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SECTION 2 1/4" 2



SECTION 2 1/4" 2

1 NUMBER OF KEYNOTE BELOW

- NOTES:
1. SEE 2/A0.1a FOR PLUMBING GENERAL NOTES
 2. SEE 3/A0.1a FOR MECHANICAL GENERAL NOTES
 3. SEE 3/A0.1a FOR ELECTRICAL GENERAL NOTES
 4. SEE 4/A0.1a FOR PLAN AND INTERIOR GENERAL NOTES
 5. SEE BID INSTRUCTIONS FOR INSULATION VALUES. INSULATION TO BE NOT LESS THAN AS INDICATED IN 124 BEFORE

KEYNOTES - -

ELEVATION GRID LINE KEY

A	AVERAGE NATURAL GRADE @ BLDG A = +/- 68.46'
B	BASEMENT FLOOR TOP OF STRUCTURE = +/- 58.33'
C	BASEMENT CEILING HEIGHT (U.N.C.) = +/- 68.33' (10')
D	GARAGE FLOOR TOP OF STRUCTURE = +/- 69.10'
E	1ST FLOOR TOP OF STRUCTURE = +/- 69.60'
F	1ST FLOOR CEILING HEIGHT (U.N.C.) = +/- 79.60' (10')
G	2ND FLOOR TOP OF STRUCTURE (U.N.C.) = +/- 80.85'
H	2ND FLOOR CEILING HEIGHT (U.N.C.) = +/- 90.33' (9.75')
I	PROPOSED BUILDING HEIGHT = +/- 93.91' (26.31')
J	MAX BUILDING HEIGHT ALLOWED = +/- 28'

ELEVATION GRID LINE KEY - -



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MONTEREY DEVELOPMENT
TWO NEW SINGLE FAMILY HOMES

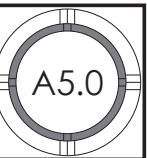
MENLO PARK, 785 PARTTRIDGE

MONTEREY DEVELOPMENT, LLC

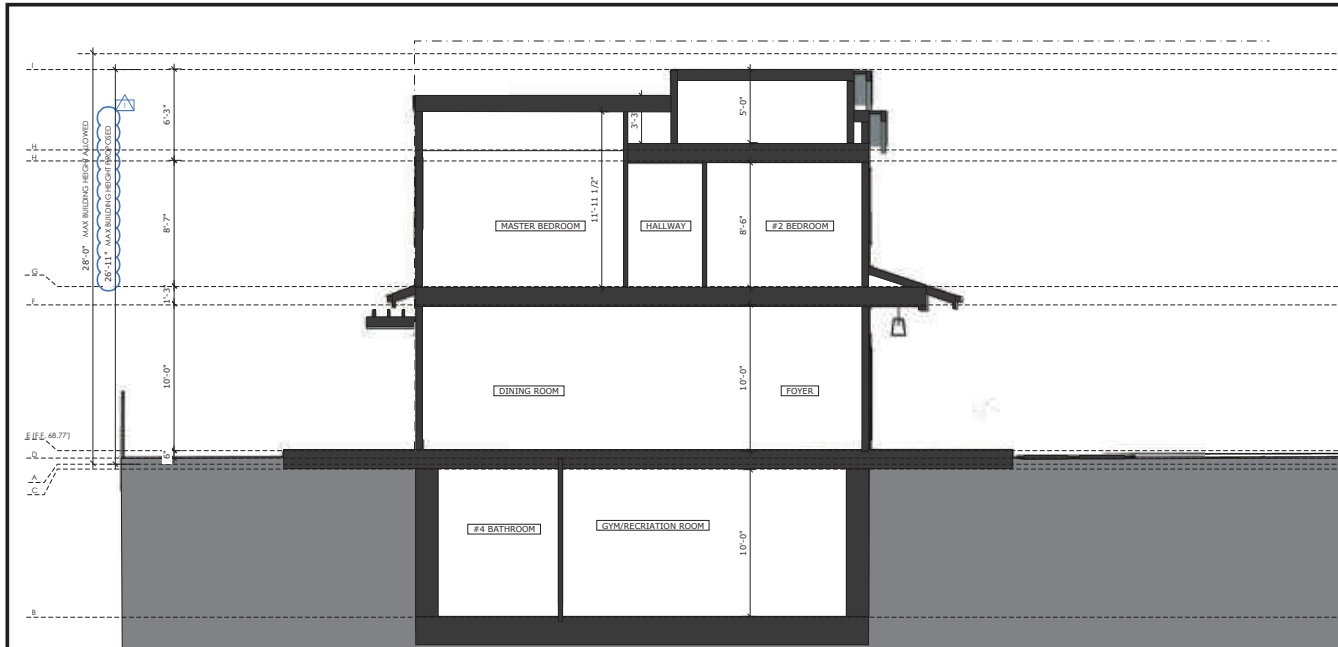


REVISION	DATE	DESCRIPTION	DRAWN BY	ISSUE
	2022.11.15	1ST PLANNING SUBMITTAL		
	2022.12.16	PLANNING REVI SUBMITTAL SET		

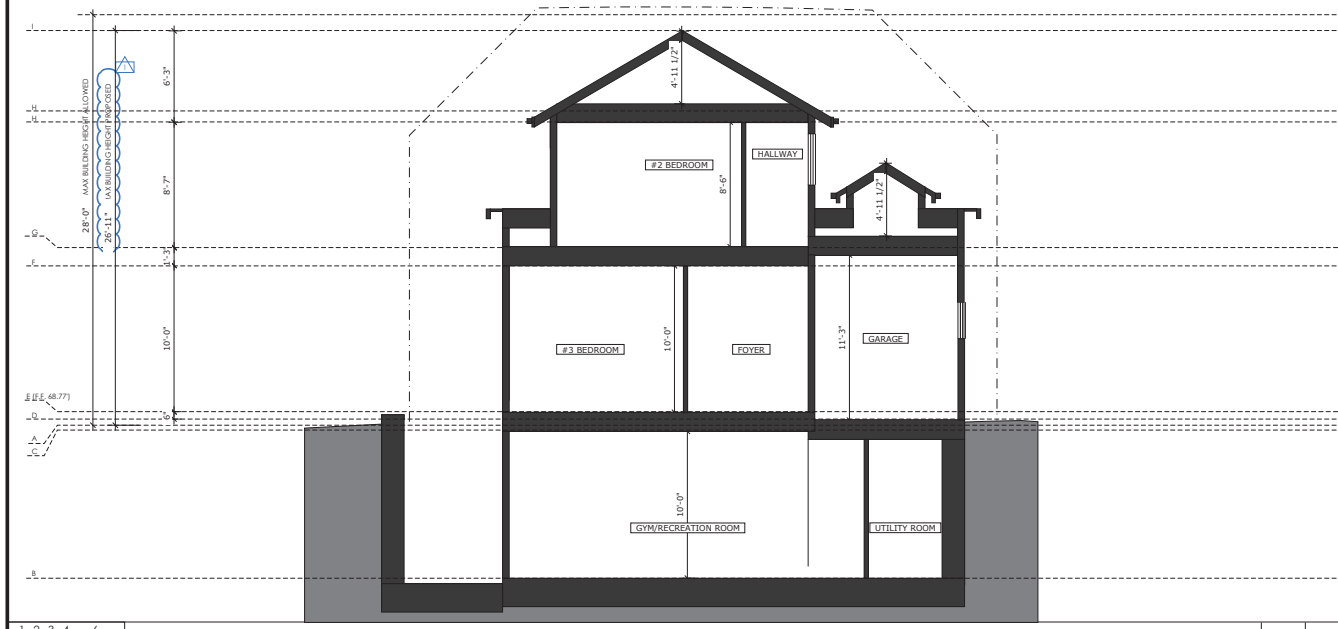
SECTIONS
BLDG A



STUDIO 5 SQUARED ARCHITECTURE, INC.



SECTION 1 1/4" 1



SECTION 1 1/4" 1

1 = NUMBER OF KEYNOTE BELOW

- NOTES:
1. SEE 2/A0.1a FOR PLUMBING GENERAL NOTES
 2. SEE 3/A0.1a FOR MECHANICAL GENERAL NOTES
 3. SEE 3/A0.1a FOR ELECTRICAL GENERAL NOTES
 4. SEE 4/A0.1a FOR PLAN AND INTERIOR GENERAL NOTES
 5. SEE BID INSTRUCTIONS FOR INSULATION VALUES. INSULATION TO BE NOT LESS THAN AS INDICATED IN 124 REPORT

KEYNOTES	-	-

ELEVATION GRID LINE KEY

A	AVERAGE NATURAL GRADE @ BLDG. S = +/- 68.25'
B	BASEMENT FLOOR TOP OF STRUCTURE = +/- 57.83'
C	BASEMENT CEILING HEIGHT (U.N.O.) = +/- 67.85' (10')
D	GARAGE FLOOR TOP OF STRUCTURE = +/- 68.35'
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F	1ST FLOOR CEILING HEIGHT (U.N.O.) = +/- 78.77' (10')
G	2ND FLOOR TOP OF STRUCTURE (U.N.O.) = +/- 80.02'
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ELEVATION GRID LINE KEY	-	-



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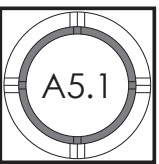
MONTEREY DEVELOPMENT
TWO NEW SINGLE FAMILY HOMES

MONTEREY DEVELOPMENT, LLC



PROJECT NO.	DATE	DESCRIPTION	DRAWN BY
	2022.11.15	1ST PLANNING SUBMITTAL	RS/MC
	2022.12.16	PLANNING REVI SUBMITTAL SET	RS/MC

SECTIONS
BLDG B





30302821

Adam Toh
Senior Arborist
785 Parridge Ave
Oakland CA 94610

Re: Tree protection for construction of new two-story at 785 Parridge Ave, Menlo Park, CA 94025

Dear Adam,

As your request, I have visited the property in person to evaluate the trees present with respect to the proposed project. The report below contains my analysis.

Summary

There are numerous trees on this property, and NUMBERS (numbering the property has various purposes. NUMBERS are recommended for various inspections of project features, as they are in their condition. NUMBERS on this property are recommended for removal, as they conflict with project features.

All other trees are in good condition and should be retained and protected as detailed in the Recommendations below. With proper protection, all are expected to survive and thrive during and after construction.

Prepared for Adam Toh by Aesculus Arboricultural Consulting on 3/30/2021

Species tolerance	Tree vitality	Distance from trunk (feet per inch trunk diameter)
Great	High	0.25
	Moderate	0.75
	Low	1.25
Adequate	High	0.75
	Moderate	1
	Low	1.25
Poor	High	1.25
	Moderate	1.75
	Low	2.5

It is important to note that some roots will not be visible but will be present outside the TPE. However, roots that are outside the TPE is unlikely to cause tree decline.

Some of the tree species listed here are not listed in Trees & Construction. My own evaluation of them based on my experience with the species is as follows:

Species	Estimated tolerance	Reason for tolerance rating
Pinus jeffreyi	2	Performs well in typical Bay Area conditions, but is relatively slow growing.
Ardisia	2	Performs well in Bay Area, but can be prone to dieback if cultural conditions are less than optimal.
Bay Laurel	2	Performs well in Bay Area, but can be prone to dieback if cultural conditions are less than optimal.

Palm

Because palms, yuccas, and other monocots (grasses) are morphologically very different from woody trees, they respond differently to root disturbance. All palm roots are adventitious, arising ascended from the root collar area, and most grow only in length, but not in girth.

Prepared for Adam Toh by Aesculus Arboricultural Consulting on 3/30/2021

Assignment and Limits of Report

I have been asked to write a report detailing impacts to trees from the proposed development on this property. This report may be used by my firm and other project members as needed in the form of stages of the project.

All observations were made from the ground with basic equipment. Highest soil elevations in certain exposures were performed. No project features had been staked at the time of my site visit.

Tree Regulations

In the City of Menlo Park, native oak trees are protected at 18 inches DBH (diameter at breast height, 4.3 feet above grade), and all other trees are protected at 15 inches DBH. Street trees are protected regardless of size.

This report will address only tree protection measures, and only for protected trees.

According to the Heritage Tree Ordinance Administrative Guidelines, the dollar value of replacement trees is determined as follows:

- One (1) #5 container - \$100
- One (1) #15 container - \$300
- One (1) 24-inch tree base - \$400
- One (1) 36-inch tree base - \$1,200
- One (1) 48-inch tree base - \$2,000
- One (1) 60-inch tree base - \$7,000

Observations

Trees

There are seven trees on or overhanging this property. Three are riparian palms (Petalocedrus, Douglas fir (Pseudotsuga), and redwood), and the rest are various species (Magnolia, T-11).

Monitoring pine #6 has red suspension beetle (Dendroctonus) adult damage in the basal trunk.

Prepared for Adam Toh by Aesculus Arboricultural Consulting on 3/30/2021

Palm species differ in their tolerance of root pruning, but all do much more tolerate than angiosperm trees. Optimal root cut level, given in distance from the trunk, is summarized in the following table extrapolated from (Borstner 2017):

Species	Non-irrigated	Irrigated	Region
California	40	45	9-11
Florida	40	45	12-14
Midwest	40	45	15-17
Southwest	40	45	18-20
Northwest	40	45	21-23
South	40	45	24-26
North	40	45	27-29

For palms and other monocots, root disturbance is typically a tree protection zone extending 30' beyond the edge of the trunk, (the more conservative distance used in this report).

Root and Foundations

Tree roots do not generally grow under houses, as foundation legislation requires these areas to be heavily compacted and dry. As discussed above, these conditions do not meet trees' needs for root colonization. Roots may grow under houses if foundations are poorly installed, or if trees are growing in contact with the foundation.

Species-Specific Issues

Monitoring pine - This species is highly susceptible to damage from red turpentine beetles (Dendroctonus valens). Beetles generally colonize trees in the spring, and a healthy tree should show signs of complete by the summer of the same year. For this reason, mature Monterey pines are generally not considered long-term trees even if apparently healthy.

None of the other species present have reliable pest or disease issues beyond those normally experienced by trees in general.

¹ Borstner, Timothy K. Publisher. MONKEY Transplanting Palms in the Landscape. (Original publication date April 1990; Revised June 2000; Revised December 2017). (14166) Edition. Available at: <http://www.aesculus.com/wordpress/wp-content/uploads/2017/03/20170301.pdf>

Prepared for Adam Toh by Aesculus Arboricultural Consulting on 3/30/2021

Bay Laurel #6 is growing through a lattice in the existing deck, and is beginning to be choked by the deck.

All other trees are in good condition.

Project Features

The existing house will be demolished, and new, smaller houses are proposed. A new driveway is proposed along the southeast property line, with five enclosed parking stalls between the new houses.

All utilities, grading, drainage, or fences are shown on the plans provided to me.

Assessed Cavity

Trees #1 and 9 - the proposed house closer to the street conflicts with these trees.

Trees #2 and 3 - the proposed driveway is within a substantial portion of these trees' TPEs.

Tree #4 - the proposed driveway conflicts with this tree.

Trees #5 and 8 - the proposed house further from the street conflicts with these trees.

Tree #6 - the proposed house further from the street and a portion of the proposed driveway is within this tree's TPE.

Trees #7 and 10 - no project features are proposed near this tree.

Tree #11 - although no work is shown near this tree, I assume the existing driveway approach will be removed as part of this project.

Tree #13 - although no work is shown near this tree, I assume the existing driveway approach will be removed as part of this project.

Testing and Analysis

Tree DBHs were taken using a diameter tape measure if trunks were accessible. The DBHs of trees with non-accessible trunks were estimated visually. All trees over 12 inches in DBH were inventoried.

Tree protection zones, Tree Protection Tree Map, and Tree Inventory Table for more detail.

Prepared for Adam Toh by Aesculus Arboricultural Consulting on 3/30/2021

Prepared for Adam Toh by Aesculus Arboricultural Consulting on 3/30/2021

Conclusions

Trees #1 and 9 - these trees must be removed to accommodate the proposed house closer to the street.

Trees #2 and 3 - these trees will likely undergo moderate to major impacts from installation of the new driveway and, to a lesser degree, the house closer to the street.

Tree #4 - this tree must be removed to accommodate the proposed driveway.

Trees #5 and 8 - this tree must be removed to accommodate the proposed house further from the street.

Tree #6 - this tree should be removed irrespective of project features, because it will likely die in the near future from red turpentine beetle damage.

Trees #7 and 10 - impacts to these trees will likely be minimal.

Tree #11 - this tree will likely undergo minor impacts from removal of the existing driveway approach (not shown on the project plans provided to me).

Recommendations

Design Phase

1. Look into removing the existing driveway approach and installing a new driveway approach.

Preconstruction Phase

1. Install tree protection fencing for trees #7 and 10 approximately as shown in the Tree Map below.
 - a. Tree protection fencing shall comprise 6' chain link fabric mounted on 1.5" diameter metal posts driven into the ground.
 - b. Place a 6" layer of wood chips inside tree protection fencing.
2. If needed, prune tree #11 for large vehicle access.

Construction Phase

1. Maintain tree protection fencing as needed throughout construction.

Post-Construction Phase

1. None

Sign ratings are based on my experience and empirical knowledge of each species.

Tree location data was collected using a GPS smartphone application and processed in GIS software to create the maps included in this report. Due to the error inherent in GPS data collection, and due also to slight differences between GPS data and CAD drawings, tree locations shown on the map below are approximate.

I visited the site once, on 2/23/2021. All observations and photographs in this report were taken at that site visit.

This report is based on the plans set titled "Two New Homes for Adam Toh and Yan Liu" dated 2/23/2021, provided to me electronically by the client.

Discussion

Use of Planting Aides (TPZs)

Tree roots grow where conditions are favorable, and their spatial arrangement is therefore unpredictable. Favorable conditions vary among species, but generally include the presence of moisture, and soft soil textures with low compaction.

Contrary to popular belief, roots of all tree species grow primarily in the top two feet of soil, with a small number of roots sometimes occurring at greater depths. Some species have taproots when young, but these without commonly disappear with age. At maturity, a tree's root system may extend out from the trunk farther than the tree is tall.

The optimal size of the area around a tree which should be protected from disturbance depends on the trees size, species, and age, as shown in the following table adapted from Trees & Construction, Volker and Clark, 1988.

Prepared for Adam Toh by Aesculus Arboricultural Consulting on 3/30/2021

Tree Map¹

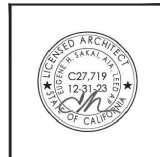


FOR PERMIT REVIEW ONLY - NOT FOR CONSTRUCTION



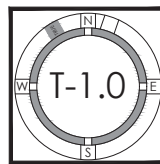
1000 S Winchester Blvd
San Jose, CA 95128
P: (408) 998-0983

22011 Monterey Development Residence TWO NEW SINGLE FAMILY HOMES
Menlo Park, 785 Parridge Avenue ADAM TOH & XIAOYAN LIU



DATE	DESCRIPTION	DATE	DESCRIPTION
2021.11.15	1ST PLANNING SUBMITTAL	MC	
2022.12.16	PLANNING REV. SUBMITTAL	CS/MC	

ARBORIST REPORT



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LEGEND

- STREET CENTERLINE
- PROPERTY LINE
- WOOD FENCE
- WATER METER
- WATER VALVE
- C&G CURB & GUTTER
- PP POWER POLE
- SSMH SANITARY SEWER MANHOLE
- FH FIRE HYDRANT
- GM GAS METER
- EM FOUND IRON PIPE
- ELECTRIC METER

EASTERLY CORNER OF LOT 9, BLOCK 1 6 MAPS 57

BASIS OF BEARINGS N 23°51'21" E 355.11'

WESTERLY CORNER OF LOT 16, BLOCK 2 6 MAPS 57

NOTES:
- ALL DISTANCES AND DIMENSIONS ARE SHOWN IN FEET AND DECIMALS THEREOF

- REFERENCED CITY OF MENLO PARK BM: BENCHMARK #7 EL: 65.71' (NAVD88)

- THE CALCULATED BEARING N 23°51'21" E BETWEEN FOUND IRON PIPES AT THE WESTERLY CORNER OF LOT 16 IN BLOCK 2 AND EASTERLY CORNER OF LOT 9 IN BLOCK 1, AS SHOWN UPON CERTAIN MAP ENTITLED "MAP OF PARTRIDGE SUBDIVISION", RECORDED IN BOOK 6 OF MAPS, AT PAGE 57, SAN MATEO COUNTY RECORDS, WAS USED AS THE BASIS OF BEARINGS SHOWN ON THIS MAP

- TOTAL LOT SIZE IS 8,000 SQ.FT.

SURVEYOR'S STATEMENT

I CERTIFY THAT THIS PARCEL'S BOUNDARY WAS ESTABLISHED BY ME OR UNDER MY SUPERVISION AND IS BASED ON A FIELD SURVEY IN CONFORMANCE WITH THE LAND SURVEYOR'S ACT. ALL MONUMENTS, USE OF THE CHARACTER AND OCCUPY THE POSITIONS INDICATED AND ARE SUFFICIENT TO ENSURE THE SURVEY TO BE RE-TRACED.

Sarah M. Parvin
SARAH M. PARVIN, PLS 6261



PARTRIDGE AVE. (60' WIDE)

PORTION OF LOT 18, BLOCK 2 6 MAPS 57

LOT 20, BLOCK 2 6 MAPS 57

POLARIS SURVEYORS			
668 BRENDA LEE DR. SAN JOSE, CA 95123 TEL (408) 691-8891			
TOPOGRAPHIC & BOUNDARY SURVEY			
APN: 071-413-010			
OWNER: ADAM TOH			
REVISIONS		PROJECT: 785 PARTRIDGE	
NO.	DATE	BY	LOCATION: 785 PARTRIDGE AVE, MENLO PARK, CA 94025
1			SCALE: 1" = 10'
2			DATE: 07/29/20 DRAWING NO.
3			TRACES
4			APPROVED

GENERAL NOTES:

- BUILDING WALLS AND TIES ARE PARALLEL WITH OR 90° TO PROPERTY BOUNDARIES.
- A "UNIT" MEANS THE ELEMENTS OF A CONDOMINIUM THAT ARE NOT OWNED IN COMMON WITH THE OTHER OWNERS OF CONDOMINIUM IN THE PROJECT.

EACH UNIT IS A CUBE OF AIRSPACE IDENTIFIED ON THIS CONDOMINIUM PLAN BY A SEPARATE UNIT NUMBER AND IS DELINEATED BY THE HORIZONTAL AND VERTICAL LIMITS SHOWN OR DESCRIBED HEREON. THE UPPER ELEVATION AND THE LOWER ELEVATION OF EACH UNIT RELATE TO THE BENCHMARK DESCRIBED HEREON. THE UPPER AND LOWER LIMITS OF SUCH ELEVATIONS ARE PLANES, EXTENDING TO THE HORIZONTAL LIMITS OF THE UNIT, WHICH PLANES, MEASURED PERPENDICULARLY ARE 40 FEET ABOVE AND 15 FEET BELOW THE FINISH FLOOR ELEVATIONS OF THE GROUND LEVEL AREA OF THE RESIDENTIAL STRUCTURAL CONSTRUCTED WITHIN SUCH UNIT. EACH UNIT INCLUDES THE LAND, BUILDING, AND THE UTILITIES CONTAINED WITHIN ITS BOUNDARIES.

- COMMON AREA SHALL MEAN AND REFER TO ALL OF THE LAND AND IMPROVEMENTS CONSTRUCTED THEREON, (EXCEPTING THE INDIVIDUAL CONDOMINIUM UNITS) TITLE TO WHICH IS HELD BY ALL OF THE OWNERS IN COMMON. THE COMMON AREA INCLUDES WITHOUT LIMITATION LAND, DRIVEWAYS, PARKING AREAS, PORCHES, PATIOS (EXCEPT THOSE PORTIONS OF THE LAN, PORCHES, AND PATIOS WHICH ARE INCLUDED WITH THE INDIVIDUAL UNITS); AND CONDUIT PIPES, PLUMBING, WIRES AND OTHER UTILITY INSTALLATIONS (EXCEPT THOSE PORTIONS OF SUCH PIPES, PLUMBING, WIRES AND OTHER UTILITY INSTALLATIONS WHICH ARE INCLUDED WITHIN THE INDIVIDUAL UNITS).

THE COMMON AREA IS SUBJECT TO NON-EXCLUSIVE EASEMENTS IN FAVOR OF EACH UNIT FOR INGRESS-EGRESS AND UTILITIES WITHIN THE AREA SO DESIGNATED HEREON.

- EXCLUSIVE USE COMMON AREA - EACH OF THE AREAS WITHIN THE COMMON, REFERRED TO AND DESIGNATED ON THE MAP AS "EXECUTIVE USE COMMON AREA", "YARD AREA", DESIGNATED AS "YA1", "YA2", AND UNCOVERED GARAGE AREA, DESIGNATED AS "G1", "G2", ALL AS SHOWN ON THE MAP ARE HEREBY SET ASIDE AND ALLOCATED FOR THE EXCLUSIVE USE AND ENJOYMENT OF THE OWNER OF THE UNIT TO WHICH THEY ARE CORRESPONDINGLY NUMBERED, INCLUDING ALL IMPROVEMENTS THEREON.

- THE INTERPRETING DEEDS AND OTHER DOCUMENTS IN CONNECTION WITH THIS SUBDIVISION MAP, THE EXISTING PHYSICAL BOUNDARIES OF THE UNIT OR OF A UNIT RECONSTRUCTED IN SUBSTANTIALLY ACCORDANCE WITH THE ORIGINAL PLANS THEREOF, SHALL BE CONCLUSIVELY PRESUMED TO BE ITS BOUNDARIES RATHER THAN THE BOUNDARIES AS EXPRESSED AND DELINEATED ON THIS PARCEL MAP, REGARDLESS OF SETTLING OR LATERAL MOVEMENT OF THE BUILDING OR REGARDLESS OF MINOR VARIANCE BETWEEN BOUNDARIES SHOWING ON SAID MAP AND THOSE OF THE BUILDING.

- ALL BUILDING WALLS AND ALL WALLS OF THE UNITS ARE RIGHT ANGLES, EXPECT AS SHOWN OTHERWISE.

- TIES TO BUILDINGS ARE TO GROUND LEVEL EXTERIOR WALLS.

- THE DISTINCTIVE BORDER INDICATES THE BOUNDARY OF THE LAND SUBDIVIDED BY THIS MAP.

- THE RIGHT OF EMERGENCY ACCESS TO THE PROVIDERS OF EMERGENCY SERVICE IS RESERVED ACROSS THE COMMON AREA.

- ALL DISTANCES ARE MEASURED IN FEET AND DECIMALS THEREOF.

TENTATIVE PARCEL MAP

FOR CONDOMINIUM PURPOSES (2 UNITS)
785 PARTRIDGE AVE.
APN: 071-413-010

BEING A SUBDIVISION OF THE NORTHWESTERLY 160 FEET OF LOT 19, BLOCK 2 AS DESIGNATED ON THE MAP ENTITLED "MAP OF PARTRIDGE SUBDIVISION NEAR MENLO PARK, SAN MATEO COUNTY, CALIFORNIA" FILED WITH THE OFFICE OF THE RECORDER OF THE COUNTY OF SAN MATEO, STATE OF CALIFORNIA, ON APRIL 6, 1909 IN BOOK 6 OF MAPS AR PAGE 57, CONSISTING OF ONE SHEET.

UTILITIES COMPANIES

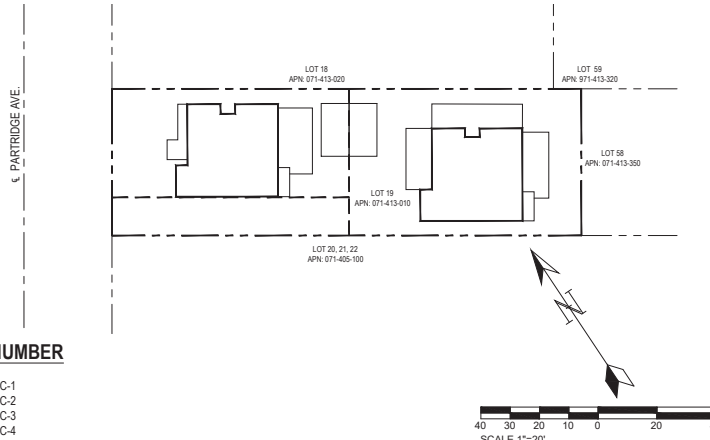
- | | |
|---------------------|------------------------------------|
| ELECTRICITY AND GAS | - PACIFIC GAS AND ELECTRIC COMPANY |
| WATER | - CALIFORNIA WATER SERVICE |
| SEWER | - WEST BAY SANITARY DISTRICT |
| CABLE AND INTERNET | - XIFINITY (COMCAST) |
| FIRE DISTRICT | - MENLO PARK FIRE DISTRICT |

OWNER / SUBDIVIDER

ADAM TOH
785 PARTRIDGE AVENUE
MENLO PARK, CA 94025

PROJECT ENGINEER

W.H. CIVIL ENGINEERING INC.
HONGBO YANG (C-88467)
25 MAULCHLY, SUITE 323
IRVINE, CA 92618
PHONE: 949-229-1413



SHEET INDEX

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ZONING

- | | | |
|-----------------|---|-------------------------------------|
| EXISTING ZONING | - | R2 (LOW DENSITY APARTMENT DISTRICT) |
| PROPOSED ZONING | - | R2 (LOW DENSITY APARTMENT DISTRICT) |

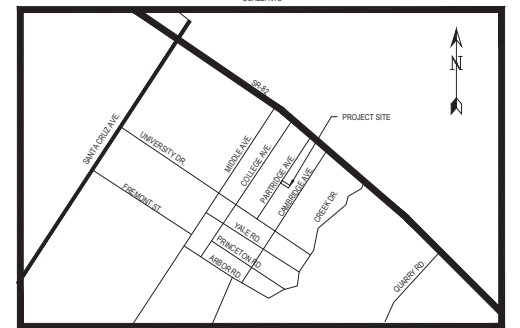
SCOPE OF WORK

A MINOR SUBDIVISION - SUBDIVIDE A SINGLE LOT INTO A TWO CONDOMINIUM UNITS LOT.

SCOPE OF WORK INCLUDE DEMOLISH EXISTING ONE-STORY, SINGLE FAMILY RESIDENCE AND CONSTRUCT TWO NEW TWO-STORY SINGLE FAMILY RESIDENCES.

FEMA FLOOD ZONE

ZONE X (OTHER AREAS)
PER PANEL 0308E OF MAP NUMBER 06081C0308E
EFFECTIVE DATE: OCTOBER 16, 2012
PROJECT AREA IS OUTSIDE THE FLOOD ZONE.



BENCHMARK

REFER TO CITY OF MENLO PARK BENCHMARK. BENCHMARK#7. ELEVATION 65.72' (NAVD 88)

BASIS OF BEARINGS

THE CALCULATED BEARING N 23°52'21" E BETWEEN FOUND IRON PIPES AT THE WESTERLY CORNER OF LOT 16 IN BLOCK 2 AND EASTERLY CORNER OF LOT 9 IN BLOCK 1, AS SHOWN UPON CERTAIN MAP ENTITLED "MAP OF PARTRIDGE SUBDIVISION", RECORDED IN BOOK 6 OF MAPS, AT PAGE 57, SAN MATEO COUNTY RECORDS , WAS USED AS THE BASIS OF BEARINGS SHOWN ON THIS MAP.

OWNER/SUBDIVIDER:		DESIGNER:	
ADAM TOH 785 PARTRIDGE AVE. MENLO PARK CA 94025		DESIGNED BY:	
		DRAFTED BY:	
		APPROVED BY:	
		CHECKED BY:	
		W.C.	
NO.	DATE	REVISIONS	APPROVED BY



PLANS PREPARED BY:

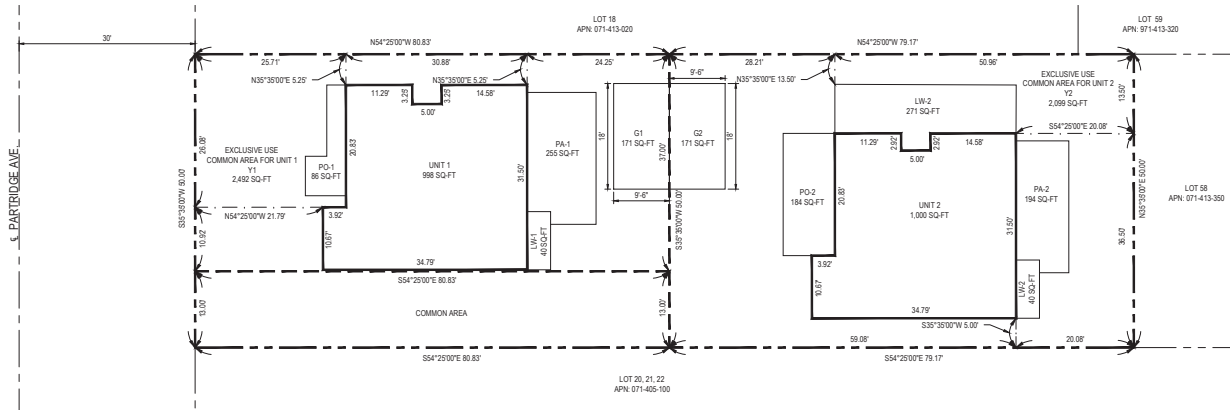
W.H. CIVIL ENGINEERING
25 MAULCHLY, SUITE 323
IRVINE, CA 92618
INFO@WHENGINEERINGGROUP.COM

HONGBO YANG
R.C.E. 028467 EXP. 3/31/2022

11/4/2022 DATE

785 PARTRIDGE AVE
TITLE SHEET
PROJECT ADDRESS
785 PARTRIDGE AVE,
MENLO PARK, CA, 94025

PROJECT NO.:
20209
SHEET NO.:
C-1



LEGEND

- PROPERTY LINE
- - - - - LIMIT OF COMMON AREA
- G1 UNCOVERED PARKING AREA FOR UNIT 1
- G2 UNCOVERED PARKING AREA FOR UNIT 2
- Y1 EXCLUSIVE USE YARD AREA FOR UNIT 1
- Y2 EXCLUSIVE USE YARD AREA FOR UNIT 2
- PA1 EXCLUSIVE USE PATIO AREA FOR UNIT 1
- PA2 EXCLUSIVE USE PATIO AREA FOR UNIT 2
- PO1 EXCLUSIVE USE POACH FOR UNIT 1
- PO2 EXCLUSIVE USE POACH FOR UNIT 2
- LW1 EXCLUSIVE USE LIGHTWELL FOR UNIT 1
- LW2 EXCLUSIVE USE LIGHTWELL FOR UNIT 2

TENTATIVE PARCEL MAP EXHIBIT

SCALE 1"=10'

SETBACK

FRONT (REQUIRED) = 20'
 PROPOSED = 21'-9"¹

SIDE (REQUIRED) = 5'
 PROPOSED = 5'-3"

REAR (REQUIRED) = 20'
 PROPOSED = 20'-1"

	BUILDING	LANDSCAPE (SQ-FT)	DRIVEWAY/HARDSCAPE (SQ-FT)
EXISTING	2,558	4,623	819
PROPOSED	3,068	2,583	2,349

ALL THE EXISTING BUILDINGS AND STRUCTURES WILL BE REMOVED.

BENCHMARK

REFER TO CITY OF MENLO PARK BENCHMARK. BENCHMARK#7. ELEVATION 65.72' (NAVD 88)

BASIS OF BEARINGS

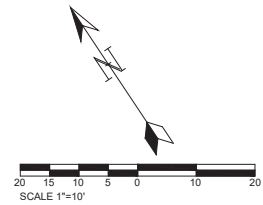
THE CALCULATED BEARING N 23°52'21" E BETWEEN FOUND IRON PIPES AT THE WESTERLY CORNER OF LOT 16 IN BLOCK 2 AND EASTERLY CORNER OF LOT 9 IN BLOCK 1, AS SHOWN UPON CERTAIN MAP ENTITLED "MAP OF PARTRIDGE SUBDIVISION", RECORDED IN BOOK 6 OF MAPS, AT PAGE 57, SAN MATEO COUNTY RECORDS, WAS USED AS THE BASIS OF BEARINGS SHOWN ON THIS MAP.

CONDOMINIUM SUBDIVISION SUMMARY

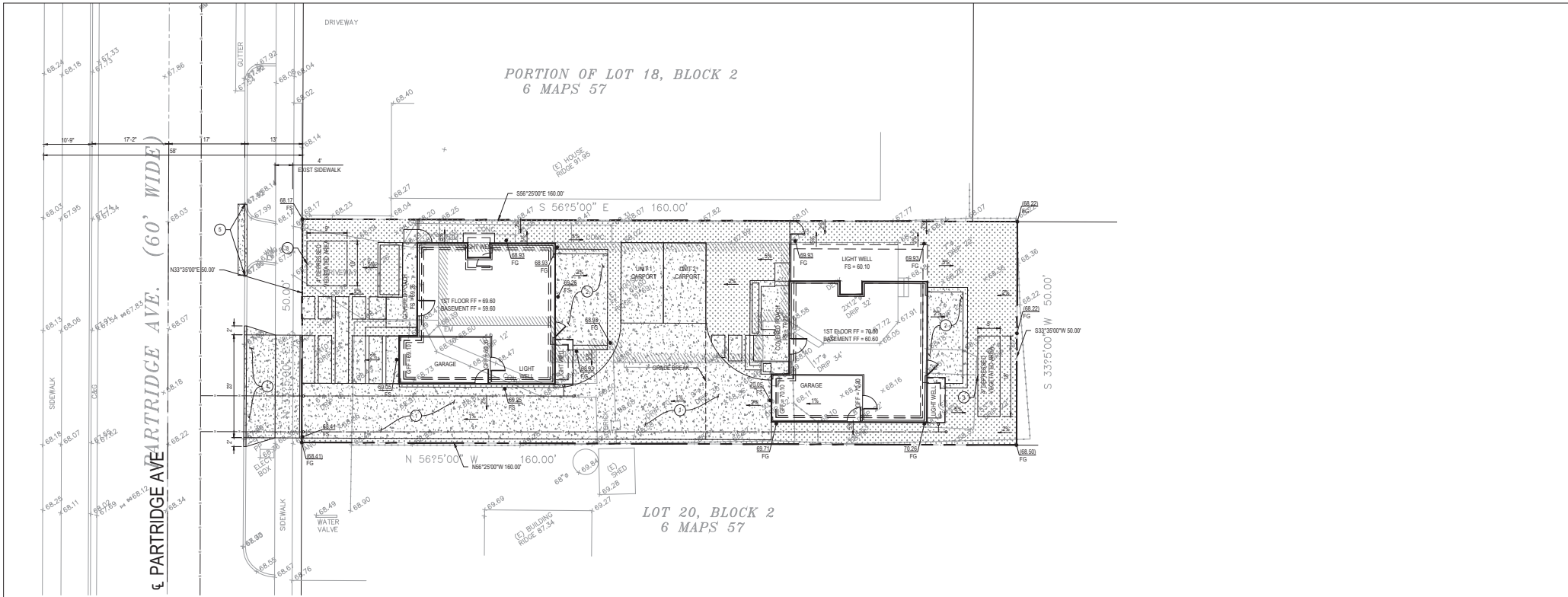
	UNIT SQUARE FOOTAGE	NUMBER OF ROOMS	EXCLUSIVE USE COMMON AREA (SQ-FT)	PARKING SPACE
UNIT 1	2,304	4	2,492	2
UNIT 2	2,315	4	2,099	2

IMPERVIOUS AREA SUMMARY

LOT AREA	=	8,000 SQ-FT
EXISTING IMPERVIOUS AREA	=	3,377 SQ-FT
EXISTING LANDSCAPE AREA	=	4,623 SQ-FT
POST DEVELOPMENT IMPERVIOUS AREA	=	5,417 SQ-FT
POST DEVELOPMENT LANDSCAPE AREA	=	2,583 SQ-FT



<p>OWNER/SUBDIVIDER:</p> <p>ADAM TOH 785 PARTRIDGE AVE. MENLO PARK, CA 94025</p>	<p>DESIGNER:</p> <p>DESIGNED BY:</p> <p>DRAFTED BY:</p>		<p>PLANS PREPARED BY:</p> <p>W.H. CIVIL ENGINEERING 25 MAUGHLY, SUITE 323 RYDINE, CA 92018 INFO@WHENGINEERINGGROUP.COM</p> <p>HONGBO YANG R.C.E. 038467 EXP. 3/31/2022</p>	<p>11/4/2022 DATE</p>	<p>PROJECT NO.: 20209</p> <p>SHEET NO.: C-2</p>										
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>REVISIONS</th> <th>APPROVED BY</th> <th>CHECKED BY</th> <th>W.C.</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>		NO.	DATE	REVISIONS	APPROVED BY	CHECKED BY	W.C.							<p>785 PARTRIDGE AVE TENTATIVE PARCEL MAP</p> <p>PROJECT ADDRESS 785 PARTRIDGE AVE. MENLO PARK, CA 94025</p>	
NO.	DATE	REVISIONS	APPROVED BY	CHECKED BY	W.C.										

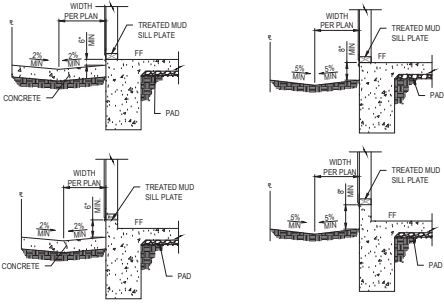


PRELIMINARY GRADING AND DRAINAGE PLAN

SCALE 1"=10'

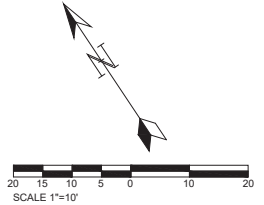
- CONSTRUCTION NOTE**
1. CONSTRUCT DRIVEWAY PER DETAIL
 2. CONSTRUCT HARDSCAPE PER DETAIL
 3. INSTALL 4" DEPRESSED VEGETATED AREA FOR RETENTION. SEE CITY STANDARD DR-18.
 4. CONSTRUCT NEW CONCRETE DRIVEWAY APPROACH PER CITY STANDARD DETAIL CG-14.
 5. REMOVE EXISTING DRIVEWAY APPROACH AND INSTALL NEW CURB AND GUTTER PER CITY STANDARD CG-2.

- LEGEND**
- 100ES SPOT ELEVATION
 - PROPOSED HARDSCAPE
 - PROPOSED LANDSCAPE
 - SD PROPOSED STORM DRAIN
 - PROPOSED FENCE
 - FLOW LINE
 - PROPERTY LINE
 - SURFACE SLOPE
 - STORM DRAIN SLOPE
 - SEWER LINE
 - WATER LINE
 - ELECTRICAL LINE
 - GAS LINE
 - DF DEEPEINED FOOTING
 - PAD PROPOSED PAD ELEVATION
 - FG PROPOSED FINISHED SURFACE
 - FF PROPOSED FINISHED FLOOR
 - INV INVERT OF PIPE
 - TG TOP OF GRATE
 - PL PROPERTY LINE
 - TW TOP OF WALL
 - TF TOP OF FOOTING
 - HP HIGH POINT
 - LP LOW POINT



DETAIL BUILDING DRAINAGE AT HARDSCAPE
NOT TO SCALE

DETAIL BUILDING DRAINAGE AT LANDSCAPE
NOT TO SCALE



OWNER/SUBDIVIDER:		DESIGNER:			
ADAM TOH 785 PARTRIDGE AVE. MENLO PARK, CA 94025		W.H. CIVIL ENGINEERING			
		DESIGNED BY:			
		DRAFTED BY:			
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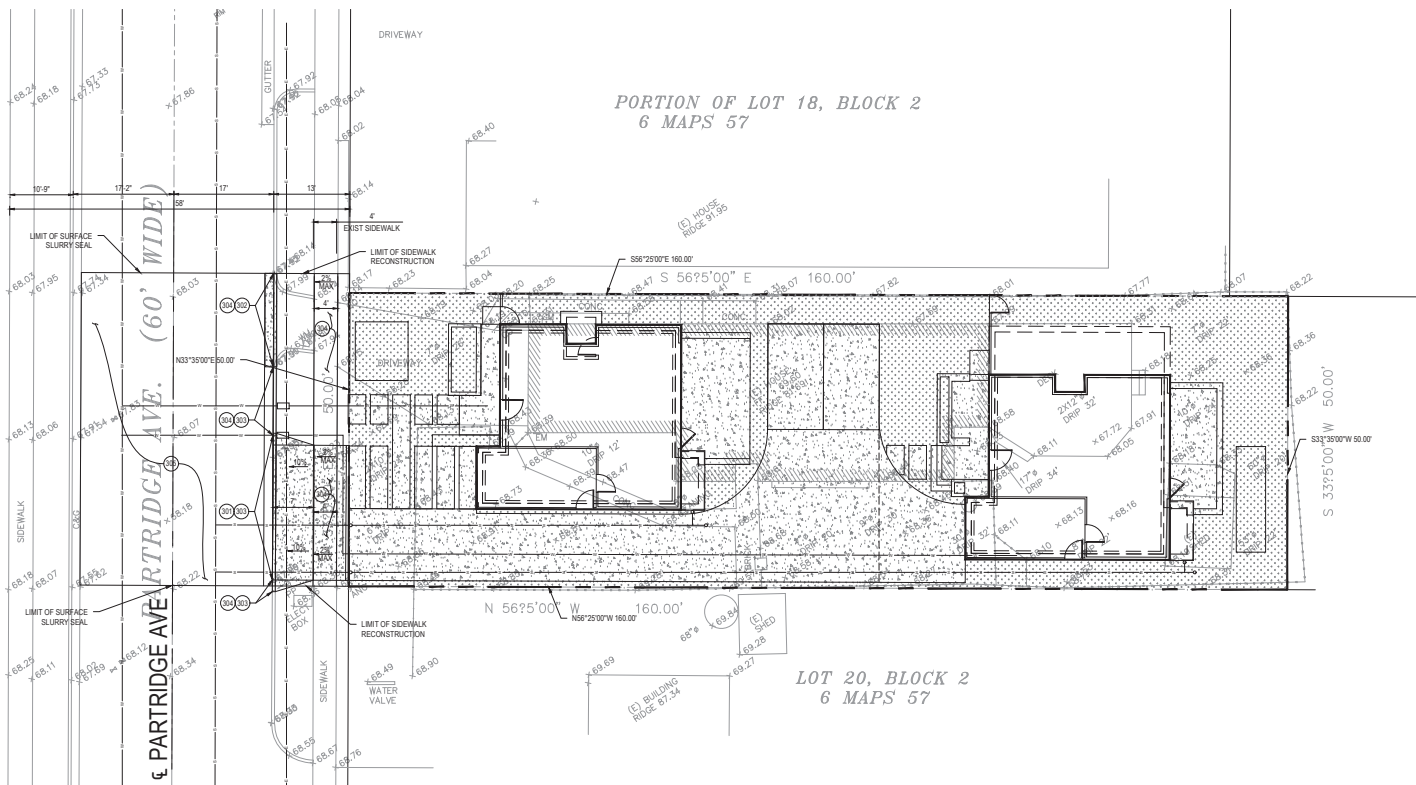


PLANS PREPARED BY:
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 INFO@WHENGINEERINGGROUP.COM
 11/4/2022
 HONGBO YANG
 R.C.E. 028467 EXP. 3/31/2022

**785 PARTRIDGE AVE
 PRELIMINARY GRADING
 & DRAINAGE PLAN
 PROJECT ADDRESS**
 785 PARTRIDGE AVE.
 MENLO PARK, CA 94025

PROJECT NO.:
20209

SHEET NO.:
C-3

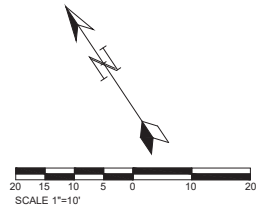


PRELIMINARY OFFSITE IMPROVEMENT PLAN

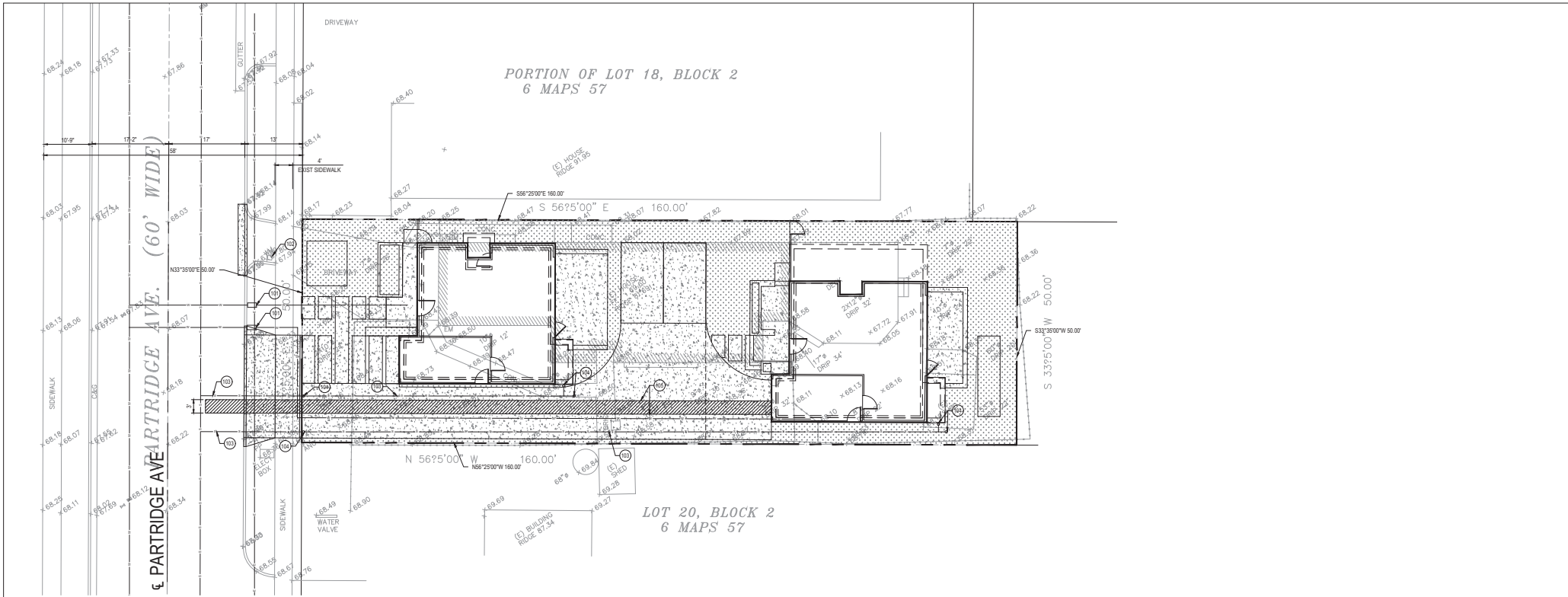
SCALE 1"=10'

STREET IMPROVEMENT CONSTRUCTION NOTE

- 301 CONSTRUCT NEW CONCRETE DRIVEWAY APPROACH PER CITY STANDARD DETAIL CG-14.
- 302 REMOVE EXISTING DRIVEWAY APPROACH.
- 303 REMOVE EXISTING CURB, GUTTER, AND SIDEWALK ALONG ENTIRE PROJECT FRONTAGE.
- 304 CONSTRUCT NEW CURB, GUTTER AND SIDEWALK PER CITY STANDARD DETAIL CG-2.
- 305 SLURRY SEAL EXISTING STREET SURFACE.



OWNER/SUBDIVIDER: ADAM TOH 785 PARTRIDGE AVE. MENLO PARK, CA 94025	DESIGNER		PLANS PREPARED BY: W.H. CIVIL ENGINEERING 25 MAUGHLY, SUITE 523 IRVINE, CA 92618 INFO@WHENGINEERINGGROUP.COM	785 PARTRIDGE AVE OFFSITE IMPROVEMENT PLAN PROJECT ADDRESS 785 PARTRIDGE AVE. MENLO PARK, CA 94025	PROJECT NO.:
	DESIGNED BY:				20209
	DRAFTED BY:				SHEET NO.:
	APPROVED BY:				C-4
	CHECKED BY:				
	W.C.				
NO.	DATE	REVISIONS	APPROVED BY	CHECKED BY:	W.C.



PRELIMINARY UTILITY PLAN

SCALE 1"=10'

UTILITY CONSTRUCTION NOTE

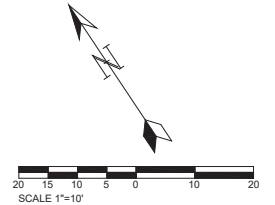
- 101 INSTALL 1/2" NEW WATER METER PER CITY STANDARD DETAIL WA-17.
- 102 REMOVE AND ABANDON EXISTING WATER METER.
- 103 INSTALL 6" DIA. C-400 PVC SEWER LATERAL PER WESTBAY SANITARY DISTRICT STANDARD DETAIL 6.
- 104 INSTALL SEWER CLEANOUT PER WESTBAY SANITARY DISTRICT STANDARD DETAIL 7.
- 105 3' WIDE JOINT TRENCH FOR DRY UTILITIES LATERAL. PER SEPARATE PLAN.

LEGEND

- SEWER LINE
- WATER LINE
- ELECTRICAL LINE
- ▨ JOINT TRENCH

NOTE:

ALL SEWER LATERAL TO HAVE MINIMUM 2% SLOPE.



OWNER/SUBDIVIDER:		DESIGNER:	
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NO.		DESIGNED BY:	
DATE		DRAFTED BY:	
REVISIONS		APPROVED BY:	
APPROVED BY:		CHECKED BY:	
W.C.		W.C.	



PLANS PREPARED BY:
W.H. CIVIL ENGINEERING
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 IRVINE, CA 92618
 INFO@WHENGINEERINGGROUP.COM
 11/4/2022
 HONGRO YANG
 R.C.E. 03847 EXP. 3/31/2022 DATE

**785 PARTRIDGE AVE
 PRELIMINARY UTILITY
 PLAN
 PROJECT ADDRESS
 785 PARTRIDGE AVE.
 MENLO PARK, CA 94025**

PROJECT NO.:
20209
 SHEET NO.:
C-5

NOTES:

1. ALL WORK SHALL COMPLY WITH 2010 CALTRANS STANDARD SPECIFICATION
2. CONCRETE SHALL BE CLASS A (B/C) CONTAINING NOT LESS THAN 564 LBS OF PORTLAND CEMENT PER CU YD (1" MINIMUM AGGREGATE SIZE AND A 4.0% COMPRESSIVE STRENGTH OF 3000 PSI); AGGREGATE BASE (AB) SHALL BE CLASS 2, CALTRANS STANDARD
3. CONCRETE FOR CURB, GUTTER, SIDEWALK, DRIVEWAY, AND OTHER SURFACE LEVEL SLABS SHALL CONTAIN ONE LB. OF AIR ENCLAVE PER CU YD. (AT BATCH PLANT)
4. AB SHALL BE COMPACTED TO NOT LESS THAN 90% RELATIVE COMPACTION, AS TESTED BY CITY APPROVED LAB AT CONTRACTOR'S EXPENSE.
5. WEAKENED PLANE JOINTS 2" DEEP FOR SIDEWALKS, 3" DEEP FOR 6" THICK DRIVEWAYS, SHALL BE SPACED AT 12' NOMINAL INTERVALS AND AT EACH SIDE OF DRIVEWAY AND PORTS OF DRIVEWAY WITH CURB CUTS EXCEEDING 2' AND AS DIRECTED BY THE DIRECTOR OF PUBLIC WORKS OR DESIGNER.
6. EXPOSED SURFACES OF SIDEWALK, DRIVEWAY, CURB AND GUTTER SHALL BE STEEL TROWEL FOLLOWED BY A MEDIUM BROOM FINISH. EXISTING DECORATIVE FINISHES SHALL BE MATCHED IN KIND.
7. EXCEPT AS SHOWN, ALL EXPOSED EDGES, INCLUDING AT WEAKENED PLANE JOINTS, SHALL BE TOOLED TO 1/4" RADIUS.
8. SIDEWALK SHALL BE SCORE MARKED AT 30' NOMINAL INTERVALS EACH WAY OR TO MATCH EXISTING SCORE MARKS. MONOLITHIC CURB, SIDEWALK AND DRIVEWAY SHALL HAVE A CONTINUOUS SCORE MARK 8" FROM CURB FACE.
9. ALL CONCRETE SHALL BE CURED BY KEEPING CONTINUOUSLY MOIST FOR THREE DAYS AFTER PLACEMENT EITHER BY SPRINKLING, COVERING WITH A WATERPROOF MEMBRANE, OR APPLYING TYPE I CONCRETE CURING COMPOUND.
10. EXPOSED CURB AND GUTTER SHALL BE APPROVED BY THE PUBLIC WORKS INSPECTOR PRIOR TO CONSTRUCTION AND PRIOR TO CONCRETE PLACEMENT.
11. ALL GUTTER LIPS SHALL BE FORMED AND TOOLED A 1/2" WIDE FULL DEPTH AC STREET STRUCTURAL SECTION SHALL BE PLACED ADJACENT TO THE GUTTER UP. THE AC SECTION SHALL BE MINIMUM OF 6" DEEP OR 1" THICKER THAN EXISTING AC, WHICHEVER IS GREATER.
12. ALL NEW PCC IMPROVEMENTS SHALL BE CONCRETE TO EXISTING AND ADJACENT PCC IMPROVEMENTS USING 12' LONG #4 OR #4 BROADTOP DOWELS AT 3' O.C. DOWELS TO BE GRADED (FOOD GRADE) OR PARTS EXCEED ONE (1) AND CAPED, TYPICAL AT ALL EXPANSION JOINTS.
13. STREET TREE WELLS AND GRATES SHALL BE PROVIDED IN ACCORDANCE WITH CITY REQUIREMENTS PER L-1. ALL SUBMITTALS SHALL BE APPROVED PRIOR TO PLACEMENT.
14. ALL PCC IMPROVEMENTS TO BE REMOVED SHALL BE SAW CUT ALONG EXISTING SCORELINE, NO SECTION TO BE REPLACED SHALL BE SMALLER THAN 30" IN EITHER LENGTH OR WIDTH. IF THE SAW CUT IN SIDEWALK OR DRIVEWAY FALLS WITHIN 30" OF A CONSTRUCTION JOINT, EXPANSION JOINT, COLD JOINT, OR EDGE, THE CONCRETE SHALL BE REMOVED TO THE JOINT OR EDGE. ALL SAW CUT RESIDUE SHALL BE DRY VACUUMED CONCURRENTLY WITH SAWING OPERATION.
15. NEW OR APPROVED RECYCLED IMPORTED FULL DEPTH AB SHALL BE PLACED UNDER ALL NEW OR REPLACEMENT CURB, GUTTER, SIDEWALK AND DRIVEWAY.
16. ALL FORM WORK AND REINFORCING MUST BE APPROVED BY THE PUBLIC WORKS INSPECTOR PRIOR TO CONCRETE PLACEMENT.

CITY OF MENLO PARK STANDARD DETAILS

GENERAL CONTRACTOR NOTES

APPROVED: *[Signature]*

DATE: 10/29/23 SCALE: 1/8" = 1'-0" SHEET: 1 OF 1

MONOLITHIC SIDEWALK, CURB AND GUTTER DETAIL

SIDEWALK, CURB & GUTTER WITH PLANTING STRIP

SIDEWALK AND CURB RETURN

All units are in metric. Non-metric units in brackets.

CITY OF MENLO PARK STANDARD DETAILS

NEW CONCRETE CURB, GUTTER AND SIDEWALK ADJACENT TO EXIST. CONCRETE

APPROVED: *[Signature]*

DATE: 10/29/23 SCALE: 1/8" = 1'-0" SHEET: 1 OF 1

DRIVEWAY WITH SEPARATE SIDEWALK

SECTION A-A

All units are in metric. Non-metric units are in brackets.

CITY OF MENLO PARK STANDARD DETAILS

DRIVEWAY WITH SEPARATE SIDEWALK

APPROVED: *[Signature]*

DATE: 10/29/23 SCALE: 1/8" = 1'-0" SHEET: 1 OF 1

UTILITY TRENCH

CITY OF MENLO PARK STANDARD DETAILS

APPROVED: *[Signature]*

DATE: 10/29/23 SCALE: 1/8" = 1'-0" SHEET: 1 OF 1

SANITARY SEWER LATERAL CONNECTIONS

CITY OF MENLO PARK STANDARD DETAILS

APPROVED: *[Signature]*

DATE: 10/29/23 SCALE: 1/8" = 1'-0" SHEET: 1 OF 1

WEST BAY SANITARY DISTRICT

SANITARY SEWER LATERAL CONNECTIONS

APPROVED BY: *[Signature]* DATE: 06-24-15

DATE: 06-24-15

WEST BAY SANITARY DISTRICT

SERVICE LATERAL CLEANOUT

APPROVED BY: *[Signature]* DATE: 06-24-15

DATE: 06-24-15

TYPICAL DRIVEWAY / HARDSCAPE SECTION

NOTES:

1. THIS DETAIL IS FOR REFERENCE ONLY TO ILLUSTRATE SOILS REPORT REQUIREMENTS. HARDSCAPE DESIGN BY OTHERS.
2. SEE SOILS REPORT FOR OVEREXCAVATION AND SUBGRADE PREPARATION REQUIREMENTS.

A	MIN. SLAB THICKNESS	DRIVEWAYS: 6"	HARDSCAPE: 4"
B	MIN. AGG. BASE THICKNESS	6"	4"
C	MIN. REINFORCEMENT (O.C. SPACING)	18" @ 12"	18" @ 12"
D	MAX. SAWCUT OR COLD-JT. SPACING	10'	8'
E	THICKENED EDGE DEPTH	12"	12"
F	THICKENED EDGE WIDTH	8"	8"

UNDISTURBED COMPETENT SOIL OR 90% COMPACTION MOISTURE CONDITION SOIL SUB GRADE PER SOIL ENGINEER RECOMMENDATION.

DETAIL TYPICAL DRIVEWAY / HARDSCAPE SECTION 1 2

NOT TO SCALE

OWNER/SUBDIVIDER:	DESIGNER:				
ADAM TOH					
785 PARTRIDGE AVE.	DESIGNED BY:				
MENLO PARK CA 94025					
	DRAFTED BY:				
NO.	DATE	REVISIONS	APPROVED BY:	CHECKED BY:	W.C.

NO.	DATE	REVISIONS	APPROVED BY:	CHECKED BY:	W.C.
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NO.	DATE	REVISIONS	APPROVED BY:	CHECKED BY:	W.C.
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PLANS PREPARED BY:

W.H. CIVIL ENGINEERING

25 MAUGHLIN, SUITE 323
REYNE, CA 94018
INFO@WHENGINEERINGGROUP.COM

11/4/2022 DATE

HONGBO YANG
P.E. 038467 EXP. 3/31/2022

PROJECT NO.: 20209

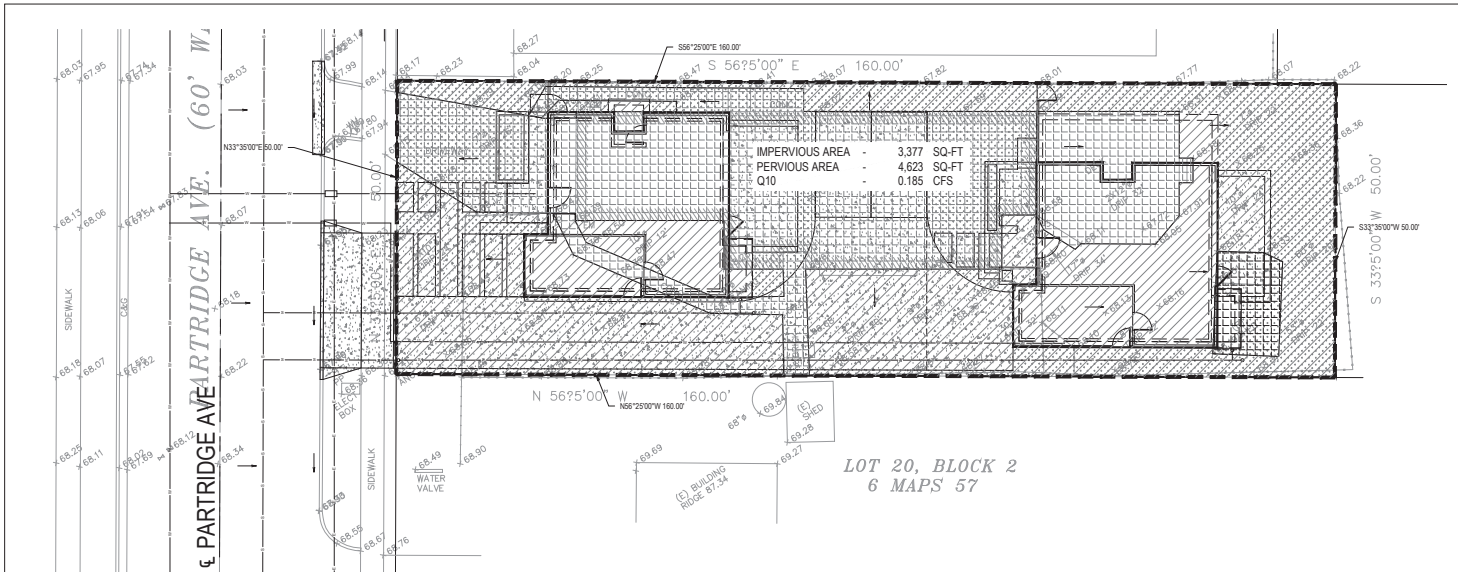
SHEET NO.: C-6

785 PARTRIDGE AVE

DETAILS PLAN

PROJECT ADDRESS

785 PARTRIDGE AVE.
MENLO PARK, CA. 94025

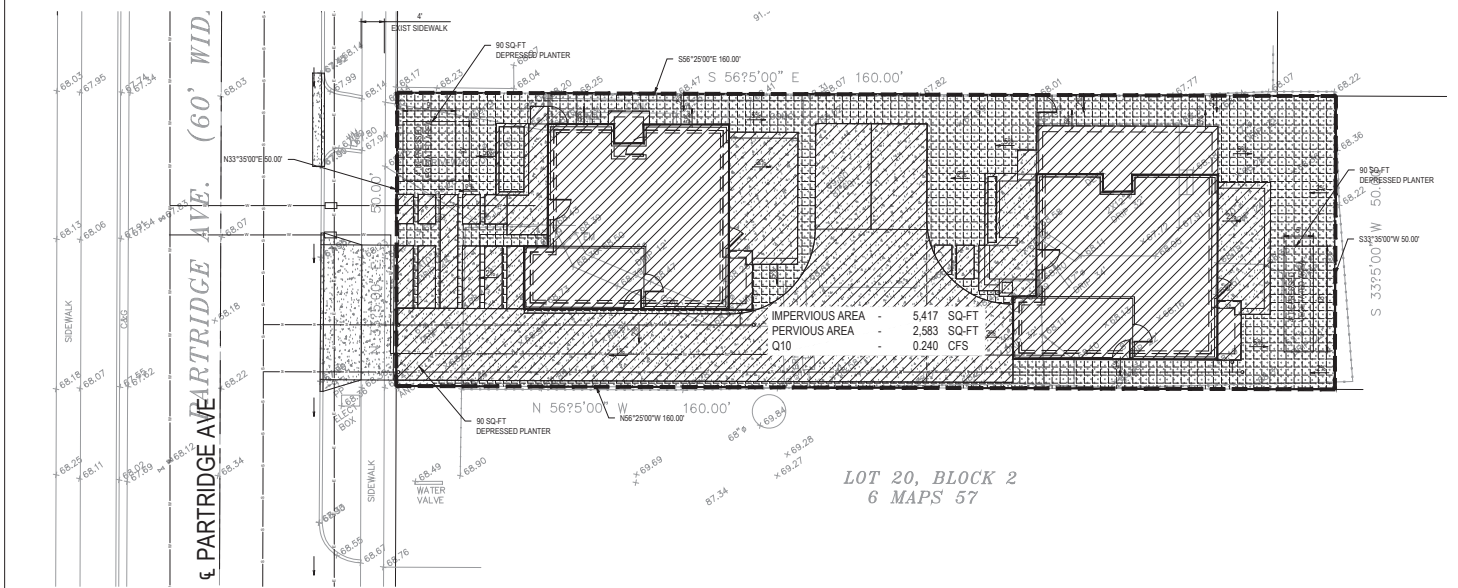


LEGEND

- IMPERVIOUS AREA
- PERVIOUS AREA
- DRAINAGE AREA
- FLOW DIRECTION

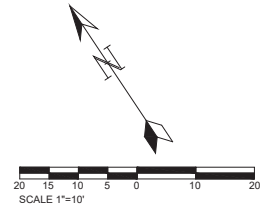
EXISTING HYDROLOGY MAP

SCALE 1"=10'



PROPOSED HYDROLOGY MAP

SCALE 1"=10'



OWNER/SUBDIVIDER:		DESIGNER:	
ADAM TOH			
785 PARTRIDGE AVE.		DESIGNED BY:	
MENLO PARK, CA 94025			
		DRAFTED BY:	
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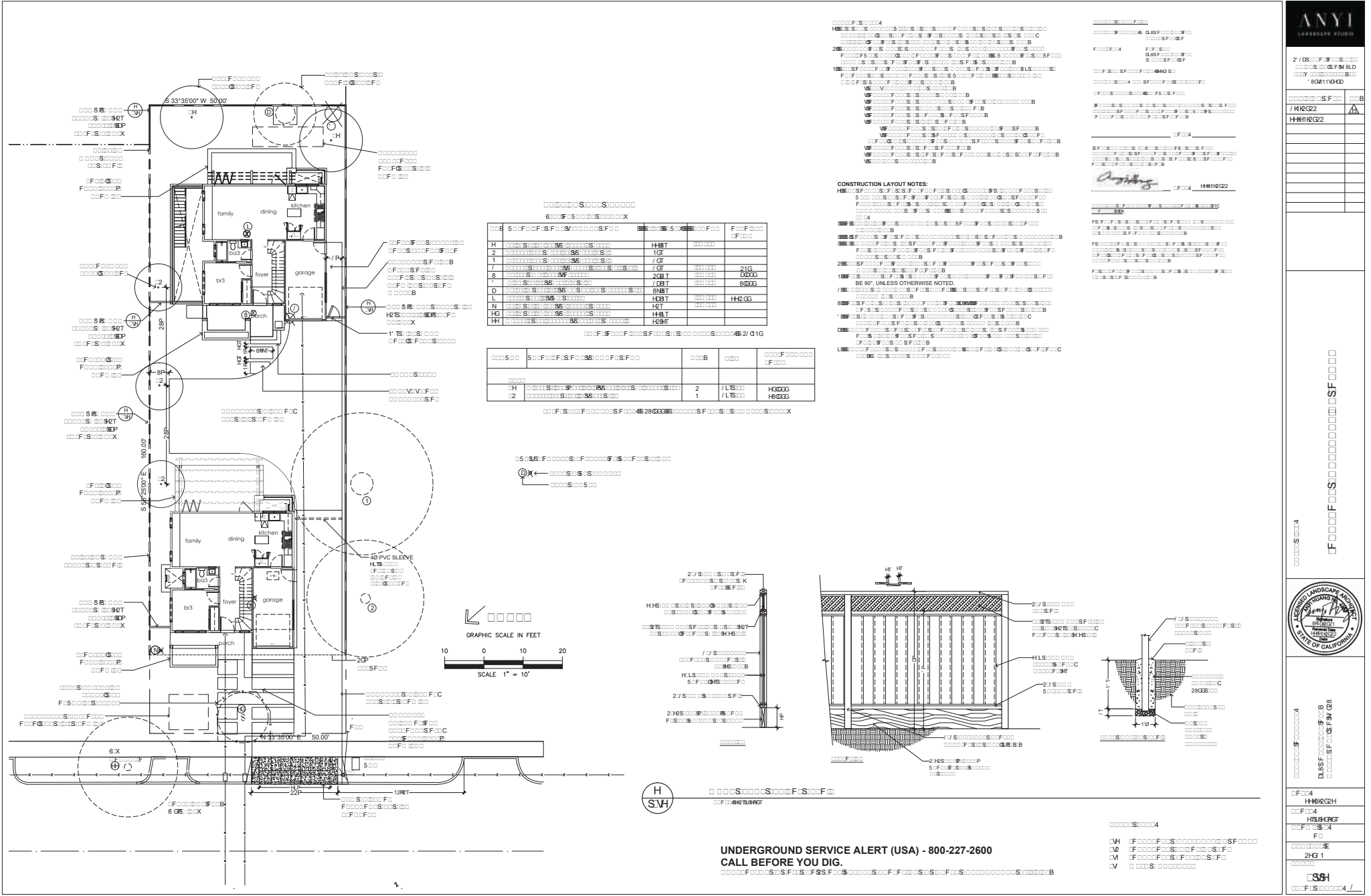
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785 PARTRIDGE AVE
 HYDROLOGY MAP
 PROJECT ADDRESS
 785 PARTRIDGE AVE.
 MENLO PARK, CA. 94025

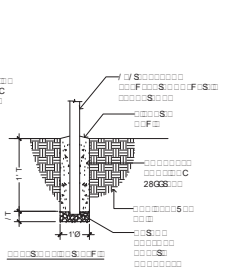
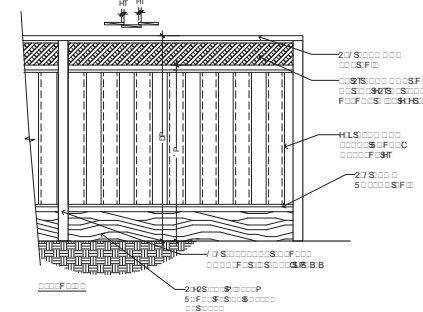
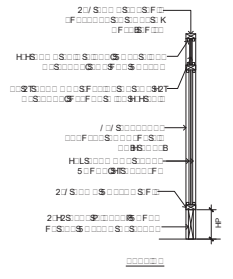
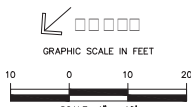
PROJECT NO.:
 20209

SHEET NO.:
 C-7




NO	DESCRIPTION	QTY	UNIT	REMARKS
1	CONCRETE	1	CF	
2	REINFORCING	2	LBS	
3	FORMWORK	3	SF	
4	BRICK	4	1000	
5	CEMENT	5	BB	
6	SAND	6	CY	
7	GRAVEL	7	CY	
8	PIPE	8	LF	
9	VALVE	9	EA	
10	CONCRETE	10	CF	
11	REINFORCING	11	LBS	
12	FORMWORK	12	SF	
13	BRICK	13	1000	
14	CEMENT	14	BB	
15	SAND	15	CY	
16	GRAVEL	16	CY	
17	PIPE	17	LF	
18	VALVE	18	EA	
19	CONCRETE	19	CF	
20	REINFORCING	20	LBS	
21	FORMWORK	21	SF	
22	BRICK	22	1000	
23	CEMENT	23	BB	
24	SAND	24	CY	
25	GRAVEL	25	CY	
26	PIPE	26	LF	
27	VALVE	27	EA	
28	CONCRETE	28	CF	
29	REINFORCING	29	LBS	
30	FORMWORK	30	SF	
31	BRICK	31	1000	
32	CEMENT	32	BB	
33	SAND	33	CY	
34	GRAVEL	34	CY	
35	PIPE	35	LF	
36	VALVE	36	EA	
37	CONCRETE	37	CF	
38	REINFORCING	38	LBS	
39	FORMWORK	39	SF	
40	BRICK	40	1000	
41	CEMENT	41	BB	
42	SAND	42	CY	
43	GRAVEL	43	CY	
44	PIPE	44	LF	
45	VALVE	45	EA	
46	CONCRETE	46	CF	
47	REINFORCING	47	LBS	
48	FORMWORK	48	SF	
49	BRICK	49	1000	
50	CEMENT	50	BB	
51	SAND	51	CY	
52	GRAVEL	52	CY	
53	PIPE	53	LF	
54	VALVE	54	EA	
55	CONCRETE	55	CF	
56	REINFORCING	56	LBS	
57	FORMWORK	57	SF	
58	BRICK	58	1000	
59	CEMENT	59	BB	
60	SAND	60	CY	
61	GRAVEL	61	CY	
62	PIPE	62	LF	
63	VALVE	63	EA	
64	CONCRETE	64	CF	
65	REINFORCING	65	LBS	
66	FORMWORK	66	SF	
67	BRICK	67	1000	
68	CEMENT	68	BB	
69	SAND	69	CY	
70	GRAVEL	70	CY	
71	PIPE	71	LF	
72	VALVE	72	EA	
73	CONCRETE	73	CF	
74	REINFORCING	74	LBS	
75	FORMWORK	75	SF	
76	BRICK	76	1000	
77	CEMENT	77	BB	
78	SAND	78	CY	
79	GRAVEL	79	CY	
80	PIPE	80	LF	
81	VALVE	81	EA	
82	CONCRETE	82	CF	
83	REINFORCING	83	LBS	
84	FORMWORK	84	SF	
85	BRICK	85	1000	
86	CEMENT	86	BB	
87	SAND	87	CY	
88	GRAVEL	88	CY	
89	PIPE	89	LF	
90	VALVE	90	EA	
91	CONCRETE	91	CF	
92	REINFORCING	92	LBS	
93	FORMWORK	93	SF	
94	BRICK	94	1000	
95	CEMENT	95	BB	
96	SAND	96	CY	
97	GRAVEL	97	CY	
98	PIPE	98	LF	
99	VALVE	99	EA	
100	CONCRETE	100	CF	

NO	DESCRIPTION	QTY	UNIT	REMARKS
1	CONCRETE	1	CF	
2	REINFORCING	2	LBS	
3	FORMWORK	3	SF	
4	BRICK	4	1000	
5	CEMENT	5	BB	
6	SAND	6	CY	
7	GRAVEL	7	CY	
8	PIPE	8	LF	
9	VALVE	9	EA	
10	CONCRETE	10	CF	
11	REINFORCING	11	LBS	
12	FORMWORK	12	SF	
13	BRICK	13	1000	
14	CEMENT	14	BB	
15	SAND	15	CY	
16	GRAVEL	16	CY	
17	PIPE	17	LF	
18	VALVE	18	EA	
19	CONCRETE	19	CF	
20	REINFORCING	20	LBS	
21	FORMWORK	21	SF	
22	BRICK	22	1000	
23	CEMENT	23	BB	
24	SAND	24	CY	
25	GRAVEL	25	CY	
26	PIPE	26	LF	
27	VALVE	27	EA	
28	CONCRETE	28	CF	
29	REINFORCING	29	LBS	
30	FORMWORK	30	SF	
31	BRICK	31	1000	
32	CEMENT	32	BB	
33	SAND	33	CY	
34	GRAVEL	34	CY	
35	PIPE	35	LF	
36	VALVE	36	EA	
37	CONCRETE	37	CF	
38	REINFORCING	38	LBS	
39	FORMWORK	39	SF	
40	BRICK	40	1000	
41	CEMENT	41	BB	
42	SAND	42	CY	
43	GRAVEL	43	CY	
44	PIPE	44	LF	
45	VALVE	45	EA	
46	CONCRETE	46	CF	
47	REINFORCING	47	LBS	
48	FORMWORK	48	SF	
49	BRICK	49	1000	
50	CEMENT	50	BB	
51	SAND	51	CY	
52	GRAVEL	52	CY	
53	PIPE	53	LF	
54	VALVE	54	EA	
55	CONCRETE	55	CF	
56	REINFORCING	56	LBS	
57	FORMWORK	57	SF	
58	BRICK	58	1000	
59	CEMENT	59	BB	
60	SAND	60	CY	
61	GRAVEL	61	CY	
62	PIPE	62	LF	
63	VALVE	63	EA	
64	CONCRETE	64	CF	
65	REINFORCING	65	LBS	
66	FORMWORK	66	SF	
67	BRICK	67	1000	
68	CEMENT	68	BB	
69	SAND	69	CY	
70	GRAVEL	70	CY	
71	PIPE	71	LF	
72	VALVE	72	EA	
73	CONCRETE	73	CF	
74	REINFORCING	74	LBS	
75	FORMWORK	75	SF	
76	BRICK	76	1000	
77	CEMENT	77	BB	
78	SAND	78	CY	
79	GRAVEL	79	CY	
80	PIPE	80	LF	
81	VALVE	81	EA	
82	CONCRETE	82	CF	
83	REINFORCING	83	LBS	
84	FORMWORK	84	SF	
85	BRICK	85	1000	
86	CEMENT	86	BB	
87	SAND	87	CY	
88	GRAVEL	88	CY	
89	PIPE	89	LF	
90	VALVE	90	EA	
91	CONCRETE	91	CF	
92	REINFORCING	92	LBS	
93	FORMWORK	93	SF	
94	BRICK	94	1000	
95	CEMENT	95	BB	
96	SAND	96	CY	
97	GRAVEL	97	CY	
98	PIPE	98	LF	
99	VALVE	99	EA	
100	CONCRETE	100	CF	




**UNDERGROUND SERVICE ALERT (USA) - 800-227-2600
CALL BEFORE YOU DIG.**



2 / DE / 2014
/ H12G22
H12G22

CONSTRUCTION LAYOUT



CONTRACT NO. 14-001
PROJECT NO. 14-001

DATE: 12/15/14

SCALE: AS SHOWN

**Studio S Squared Architecture, Inc.**

1000 S Winchester Blvd.

San Jose, CA 95128

ph: (408) 998-0983

fax: (408) 404-0144

www.studios2arch.com

January 26, 2023

**City of Menlo Park
Planning Department**701 Laurel Street
Menlo Park , CA 94025**Attn: Fahteen Khan****785 PARTRIDGE PROJECT DESCRIPTION**

(revised 01 / 17 / 23)

BACKGROUND

The project site is located at 785 Partridge Avenue, between El Camino Real and University Drive in the Allied Arts area. The block of this Partridge Ave does not have a single predominant architectural style, although bungalow, ranch, traditional residential and craftsman styles are common. Nearby buildings are generally one- and two-story in size. Most of the lots have front and rear homes. The lot size of 785 Partridge Ave is 8,000 s.f. and is zoned R - 2. The lot is of substandard width, 50', rather than 65'. The existing property contains a one-story, single-family home. The adjacent parcels are both multi - family properties with apartments.

PROPOSAL

The purpose of this application is to request consideration of this project to demolish the existing home and build two new single-family residences, each with a full basement. The homes will have 4 bedrooms and 4 bathrooms with approximately 2,400 sf of living space. We are proposing a single car garage and one additional uncovered parking space for each single - family residence, similar to what other projects have done on this street. The lot coverage and FAL are within the zoning allowances. The driveway will have permeable pavers. The architectural styles of the two proposed homes compliment each other but are not a direct copy through the use of similar floor plans, massing and similar

material palettes. The front home (building A) is designed as traditional farmhouse, evident in its use of exposed wood elements and white tones, front porch, and gabled roofs with roof slopes of this style. Board and batten siding is prevalent at the exterior with the introduction of horizontal grey siding on the second floor at the upper stairwell and rear master bath and closet. Aluminum windows are proposed with traditional cross muntins where applicable, typical of the farmhouse style with 2" painted wood trim for a simplistic look. The garage door is designed as a wood, carriage - style door with a beautiful herringbone pattern below high window panes for privacy. The front door is similar in style with a high window for privacy, using the wood exterior to bring attention to the entry. The rear home (building B) is designed in a transitional style, with traditional forms complimentary of the front house, but introducing contemporary details and materials for a modern refresh on the traditional farmhouse home. The home is covered in a smooth trowel light-colored stucco with a sea - foam blue horizontal siding at the front entry and the stairwell at the second floor. Standing seam metal roof is proposed above the first floor, reinforcing the contemporary approach to a traditional roof form with asphalt shingle roof at the second. The garage door is a modern aluminum frame with non - translucent window panes for privacy. The front door is designed as a custom wood door with a sleek vertical window pane. Aluminum windows are proposed throughout the home with horizontal muntins for a modern aesthetic. Both homes have been designed with the larger 2nd story egress windows to the front and rear of the property. On the 2nd story, smaller windows are at the sides, to address privacy issues. We are requesting a use permit for light well encroachments into the required setbacks as a result of the substandard lot width, the need to provide 2 uncovered parking spaces, and providing a driveway to the rear home. Light-wells are required for fire escape from basement rooms, and cannot be allocated on the driveway side of the property due to the property and homes' footprints. At the first home (bldg. A), a lightwell protrudes 2' - 0" into the required setback at a length of 7'-0". This is to provide egress to the basement as well as proper daylight into the spaces below. The lightwell is designed as narrow as possible while still not sacrificing on necessary egress and providing adequate light below. At the rear home (bldg B.), we are requesting a 4' - 11" encroachment into the rear setback at a width of 9' - 11". This lightwell is required to provide egress and daylight into the adjacent bedroom.

NEIGHBORHOOD COMMUNICATION

The property owners have initiated correspondence with the neighbors, and have mailed information and plans directly to the neighbors, encouraging neighbors to contact us about the project. The only communication received from neighbors includes Liz Poux at 775 Partridge Avenue who has concerns about asbestos during demolition of the existing home, and Andrea Wilson of the Peninsula Volunteers at 817 Partridge Avenue asked to share the proposed fence cost. Thank you very much for your review and continued assistance with our project . Please do not hesitate to call our office should you have any questions.

Thank you for your review. Please do not hesitate to call our office should you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to read 'EHS', with a small dot at the end.

Eugene H. Sakai, AIA, LEED AP

President, Studio S² Architecture, Inc.

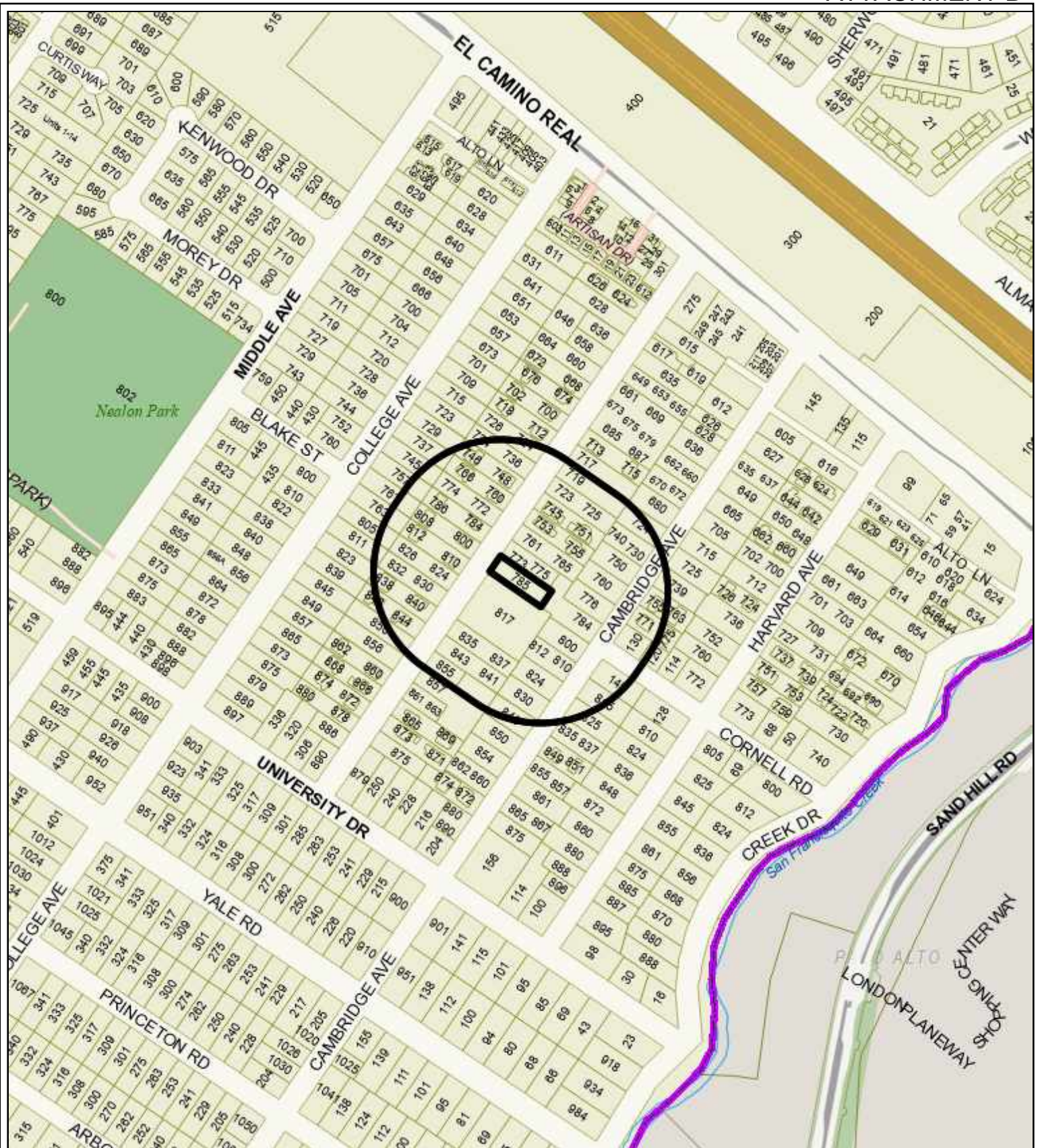
cc: Xiaoyan Liu (silva.liux@gmail.com)
Adam Toh (simsimrealty@gmail.com)

LOCATION: 785 Partridge Avenue	PROJECT NUMBER: PLN2021-00032	APPLICANT: Calvin Smith	OWNER: Xiaoyan Liu
---------------------------------------	--------------------------------------	--------------------------------	---------------------------

<p>CONDITIONS OF APPROVAL:</p> <p>1. Approve the use permit subject to the following standard conditions:</p> <ul style="list-style-type: none"> a. The applicant shall be required to apply for a building permit within one year from the date of approval (by February 6, 2024) for the use permit to remain in effect. b. Development of the project shall be substantially in conformance with the plans prepared by Studio S Squared consisting of 32 plan sheets, dated received July 8, 2022 and approved by the Planning Commission on December 16, 2022, except as modified by the conditions contained herein, subject to review and approval of the Planning Division. c. Prior to building permit issuance, the applicant shall comply with all Sanitary District, Menlo Park Fire Protection District, and utility companies’ regulations that are directly applicable to the project. d. Prior to building permit issuance, the applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project. e. Prior to building permit issuance, the applicant shall submit a plan for any new utility installations or upgrades for review and approval by the Planning, Engineering and Building Divisions. All utility equipment that is installed outside of a building and that cannot be placed underground shall be properly screened by landscaping. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes. f. Simultaneous with the submittal of a complete building permit application, the applicant shall submit plans indicating that the applicant shall remove and replace any damaged and significantly worn sections of frontage improvements. The plans shall be submitted for review and approval of the Engineering Division. g. Simultaneous with the submittal of a complete building permit application, the applicant shall submit a Grading and Drainage Plan for review and approval of the Engineering Division. The Grading and Drainage Plan shall be approved prior to the issuance of grading, demolition or building permits. h. Heritage trees in the vicinity of the construction project shall be protected pursuant to the Heritage Tree Ordinance and the arborist report prepared by Aesculus Arboricultural Consulting, dated March 26, 2021. i. Prior to building permit issuance, the applicant shall pay all fees incurred through staff time spent reviewing the application. j. The applicant or permittee shall defend, indemnify, and hold harmless the City of Menlo Park or its agents, officers, and employees from any claim, action, or proceeding against the City of Menlo Park or its agents, officers, or employees to attack, set aside, void, or annul an approval of the Planning Commission, City Council, Community Development Director, or any other department, committee, or agency of the City concerning a development, variance, permit, or land use approval which action is brought within the time period provided for in any applicable statute; provided, however, that the applicant’s or permittee’s duty to so defend, indemnify, and hold harmless shall be subject to the City’s promptly notifying the applicant or permittee of any said claim, action, or proceeding and the City’s full cooperation in the applicant’s or permittee’s defense of said claims, actions, or proceedings. <p>2. Approve the use permit subject to the following project-specific conditions:</p>

785 Partridge Avenue – Exhibit C: Conditions of Approval

LOCATION: 785 Partridge Avenue	PROJECT NUMBER: PLN2021-00032	APPLICANT: Calvin Smith	OWNER: Xiaoyan Liu
CONDITIONS OF APPROVAL:			
a. Simultaneous with the submittal of a complete building permit application, the applicant shall submit revised plans showing one 24-inch box size street tree on the street frontage from the City's approved list of trees subject to review and approval of the Engineering Division.			



City of Menlo Park
 Location Map
 785 Partridge Avenue



Scale: 1:4,000

Drawn By: FNK

Checked By: KTP

Date: 2/6/2023

Sheet: 1

	PROPOSED PROJECT	EXISTING PROJECT	ZONING ORDINANCE
Lot area	8,000.0 sf	8,000.0 sf	7,000.0 sf min.
Lot width	50.0 ft.	50.0 ft.	65.0 ft. min.
Lot depth	160.0 ft.	160.0 ft.	100.0 ft. min.
Setbacks			
Front	21.8 ft.	30.5 ft.	20.0 ft. min.
Rear	20.0 ft.	51.0 ft.	20.0 ft. min.
Side (left)	5.3 ft.	4.9 ft.	5.0 ft. min.
Side (right)	5.0 ft.	18.1 ft.	10.0 ft. min.
Building coverage	2,621 sf 32.8 %	1,965 sf 24.6 %	2,800 sf max. 35.0 % max.
FAL (Floor Area Limit)	3,199 sf 39.9 %	1,788 sf 22.3 %	3,200 sf max. 40.0 % max.
2 nd Floor FAL	1,199 sf 14.9 %	n/a	1,200.0 sf max 15.0 % max
Landscaping	5,379 sf 67.2 %	6,035.2 sf 75.4 %	3,200 sf min. 40.0 % min.
Square footage by floor	Front Unit 787.6 sf/1st 600.2 sf/2 nd 916.3 sf/basement 212.5 sf/garage 321.4 sf/porch Rear Unit 787.6 sf/1st 598.6 sf/2 nd 929.0 sf/basement 212.5 sf/garage 299.5 sf/porch	1,403 sf/1 st 385 sf/garage 177 sf/porch	
Square footage of buildings	5,665.2 sf	1,965 sf	
Building height (unit A)	27.9 ft.	15.5 ft.	28.0 ft. max.
Building height (unit B)	27.7 ft.		
Parking	2 covered, 2 uncovered	2 covered	1 covered/1 uncovered per unit
Note: Areas shown highlighted indicate a nonconforming or substandard situation.			

Trees					
Heritage trees*	7	Non-heritage trees	4	New trees	5
Heritage trees proposed for removal	4	Non-heritage trees proposed for removal	1	Total number of trees*	15

*Includes one street tree (tree #11) and two trees (tree #2 and 3) on a neighboring property.



3/26/2021

Adam Toh
Simsim Realty
565 Fairbanks Ave
Oakland CA 94610

Re: Tree protection for construction of two new homes at 785 Partridge Ave, Menlo Park, CA 94025

Dear Adam,

At your request, I have visited the property referenced above to evaluate the trees present with respect to the proposed project. The report below contains my analysis.

Summary

There are NUMBERA trees on this property, and NUMBERB overhanging the property from adjacent properties. NUMBERD are recommended for removal irrespective of project features, as they are in poor condition. NUMBERC on this property are recommended for removal, as they conflict with project features.

All other trees are in good condition and should be retained and protected as detailed in the Recommendations, below. With proper protection, all are expected to survive and thrive during and after construction.

Assignment and Limits of Report

I have been asked to write a report detailing impacts to trees from the proposed pool installation on this property. This report may be used by my client and other project members as needed to inform all stages of the project.

All observations were made from the ground with basic equipment. No root collar excavations or aerial inspections were performed. No project features had been staked at the time of my site visit.

Tree Regulations

In the City of Menlo Park, native oak trees are protected at 10 inches DBH (diameter at breast height, 4.5 feet above grade), and all other trees are protected at 15 inches DBH. Street trees are protected regardless of size.

This report will address only tree protection measures, and only for protected trees.

According to the Heritage Tree Ordinance Administrative Guidelines, the dollar value of replacement trees is determined as follows:

- One (1) #5 container – \$100
- One (1) #15 container – \$200
- One (1) 24-inch tree box – \$400
- One (1) 36-inch tree box – \$1,200
- One (1) 48-inch tree box – \$5,000
- One (1) 60-inch tree box – \$7,000

Observations

Trees

There are eleven trees on or overhanging this property. Three are pineapple guavas (*Feijoa sellowiana*), two are Douglas-firs (*Pseudotsuga menziesii*), and the rest are various species (Images 1-11).

Monterey pine #6 has red turpentine beetle (*Dendroctonus valens*) damage in the lower trunk.

Bay laurel #8 is growing through a cutout in the existing deck, and is beginning to be girdled by the deck.

All other trees are in good condition.

Project Features

The existing house will be demolished, and two new, smaller houses are proposed. A new driveway is proposed along the southwest property line, with two uncovered parking stalls between the new houses.

No utilities, grading, drainage, or fences are shown on the plans provided to me.

Potential Conflicts

Trees #1 and 9 - the proposed house closer to the street conflicts with these trees.

Trees #2 and 3 - the proposed driveway lies within a substantial portion of these trees' TPZs.¹

Tree #4 - the proposed driveway conflicts with this tree.

Trees #5 and 8 - the proposed house farther from the street conflicts with these trees.

Tree #6 - the proposed house farther from the street and a portion of the proposed driveway lie within this tree's TPZ.

Trees #7 and 10 - no project features are proposed near this tree.

Tree #11 - although no work is shown near this tree, I assume the existing driveway approach will be removed as part of this project.

Testing and Analysis

Tree DBHs were taken using a diameter tape measure if trunks were accessible. The DBHs of trees with non-accessible trunks were estimated visually. All trees over 12 inches in DBH were inventoried.

¹ Tree protection zones. See Discussion, Tree Map, and Tree Inventory Table for more detail.

Vigor ratings are based on tree appearance and experiential knowledge of each species.

Tree location data was collected using a GPS smartphone application and processed in GIS software to create the maps included in this report. Due to the error inherent in GPS data collection, and due also to slight differences between GPS data and CAD drawings, tree locations shown on the map below are approximate.

I visited the site once, on 2/27/2021. All observations and photographs in this report were taken at that site visit.

This report is based on the plan set titled "Two New Homes for: Adam Toh and Yan Liu," dated 2/9/2021, provided to me electronically by the client.

Discussion

Tree Protection Zones (TPZ's)

Tree roots grow where conditions are favorable, and their spatial arrangement is therefore unpredictable. Favorable conditions vary among species, but generally include the presence of moisture, and soft soil texture with low compaction.

Contrary to popular belief, roots of all tree species grow primarily in the top two feet of soil, with a small number of roots sometimes occurring at greater depths. Some species have taproots when young, but these almost universally disappear with age. At maturity, a tree's root system may extend out from the trunk farther than the tree is tall.

The optimal size of the area around a tree which should be protected from disturbance depends on the tree's size, species, and vigor, as shown in the following table (adapted from *Trees & Construction*, Matheny and Clark, 1998):

Species tolerance	Tree vitality	Distance from trunk (feet per inch trunk diameter)
Good	High	0.5
	Moderate	0.75
	Low	1
Moderate	High	0.75
	Moderate	1
	Low	1.25
Poor	High	1
	Moderate	1.25
	Low	1.5

It is important to note that some roots will almost certainly be present outside the TPZ; however, root loss outside the TPZ is unlikely to cause tree decline.

Some of the tree species present here are not evaluated in Trees & Construction. My own evaluation of them based on my experience with the species is as follows:

Species	Estimated tolerance	Reason for tolerance rating
Pineapple guava	2	Performs well in typical Bay Area conditions, but is relatively slow growing
Avocado	2	Performs well in Bay Area, but can be prone to dieback if cultural conditions are less than optimal
Bay laurel	2	Performs well in Bay Area, but can be prone to dieback if cultural conditions are less than optimal

Palms

Because palms, yuccas, and other monocots (grasses) are morphologically very different from woody trees, they respond differently to root disturbance. All palm roots are adventitious, arising as needed from the root initiation zone, and roots grow only in length but not in girth.

Palm species differ in their tolerance of root pruning, but all are much more tolerant than angiosperm trees. Optimal root ball sized, given in distance from the trunk, is summarized in the following table (reproduced from Broschat 2017)²:

Table 1. Average percentage of cut roots branching in four different root-length classes.

Species	Root-stub length (inches)			Avg. no. of new roots
	<6	6-12	12-24	
<i>Cocos nucifera</i>	47	61	50	20
<i>Phoenix reclinata</i>	0	2	8	62
<i>Roystonea regia</i>	1	6	24	97
<i>Sabal palmetto</i>	1	1	3	196
<i>Syagrus romanzoffiana</i>	3	41	49	13
<i>Washingtonia robusta</i>	2	14	31	144

Data from Broschat and Donselman (1984; 1990b).

For palms and other monocots not addressed in this table, I specify a tree protection zone extending 24" beyond the edge of the trunk, the most conservative distance tested in this study.

Roots and Foundations

Tree roots do not generally grow under houses, as foundation installation requires these areas to be heavily compacted and dry. As discussed above, these conditions do not meet trees' needs for root colonization. Roots may grow under houses if foundations are poorly installed, or if trees are growing in contact with the foundation.

Species-Specific Issues

Monterey pine – this species is highly susceptible to damage from red turpentine beetles (*Dendroctonus valens*). Beetles generally colonize trees in the spring, and a heavily colonized tree may die completely by the summer of the same year. For this reason, mature Monterey pines are generally not considered long-term trees even if apparently healthy.

None of the other species present have notable pest or disease issues beyond those normally experienced by trees in general.

² Broschat, Timothy K. Publication #CIR1047: Transplanting Palms in the Landscape. Original publication date April 1992. Revised June 2009. Reviewed December 2017. UF IFAS Extension. Available at <https://edis.ifas.ufl.edu/pdf/files/EP/EP00100.pdf>

Conclusions

Trees #1 and 9 - these trees must be removed to accommodate the proposed house closer to the street.

Trees #2 and 3 - these trees will likely undergo moderate to major impacts from installation of the new driveway and, to a lesser degree, the house closer to the street.

Tree #4 - this tree must be removed to accommodate the proposed driveway.

Trees #5 and 8 - this tree must be removed to accommodate the proposed house farther from the street.

Tree #6 - this tree should be removed irrespective of project features, because it will likely die in the near future from red turpentine beetle damage.

Trees #7 and 10 - impacts to these trees will likely be minimal.

Tree #11 - this tree will likely undergo minor impacts from removal of the existing driveway approach (not shown on the project plans provided to me).

Recommendations

Design Phase

1. Look into removing the existing driveway approach and installing a new driveway approach.

Preconstruction Phase

1. Install tree protection fencing for trees #7 and 10 approximately as shown in the Tree Map, below.
 - a. Tree protection fencing shall comprise 6' chain link fabric mounted on 1.5" diameter metal posts driven into the ground.
 - b. Place a 6" layer of wood chips inside tree protection fencing.
2. If needed, prune tree #11 for large vehicle access.

Construction Phase

1. Maintain tree protection fencing as needed throughout construction.

Post-Construction Phase

1. None

Tree Inventory Table

Tree #	Common Name	Species	DBH (in.)	Protected by city	Vitality (0-3)	Species Construction Tolerance (1-3)	TPZ radius (ideal; feet from center of trunk)	Expected Impacts	Remove	Notes
1	Pineapple guava	Feijoa sellowiana	11.5		3	2	8.6	Incompatible with new house	X	-
2	Douglas-fir	Pseudotsuga menziesii	30.0	X	3	1	30.0	Moderate from new driveway and house footprint		-
3	Douglas-fir	Pseudotsuga menziesii	40.0	X	3	1	40.0	Moderate from new driveway and house		-
4	Canary Island date palm	Phoenix canariensis	40.0	X	3	N/A	3.7	Incompatible with new driveway	X	As this tree is a palm, the TPZ radius is 2' beyond the edge of the trunk.
5	Avocado	Persea americana	20.4	X	3	2	15.3	Incompatible with new house	X	-

Tree #	Common Name	Species	DBH (in.)	Protected by city	Vitality (0-3)	Species Construction Tolerance (1-3)	TPZ radius (ideal; feet from center of trunk)	Expected Impacts	Remove	Notes
6	Monterey pine	Pinus radiata	47.3	X	1	2	59.1	REMOVE	X	Tree should be removed irrespective of construction activities, as it has red turpentine beetle damage
7	New Zealand cabbage tree	Cordyline australis	59.5	X	3	N/A	4.5	Negligible		As this tree is a monocot, the TPZ radius is 2' beyond the edge of the trunk.
8	Bay laurel	Laurus nobilis	17.3	X	3	2	13.0	Incompatible with new house	X	-
9	Pineapple guava	Feijoa sellowiana	12.0		3	2	9.0	Major from new house and hardscape	X	-
10	Pineapple guava	Feijoa sellowiana	11.8		3	2	8.9	Negligible		-

Tree #	Common Name	Species	DBH (in.)	Protected by city	Vitality (0-3)	Species Construction Tolerance (1-3)	TPZ radius (ideal; feet from center of trunk)	Expected Impacts	Remove	Notes
11	Southern magnolia	Magnolia grandiflora	12.1	X	3	1	12.1	Minor from removal of curb. May require some pruning for large vehicle access		-

Supporting Photographs

Image 1: pineapple guava #1



Image 2: Douglas-firs #2 (right) and 3



Image 3: Canary Island date palm #4



Image 4: avocado #5



Image 5: Monterey pine #6



Image 6: beetle damage to Monterey pine #6



Image 6: New Zealand cabbage tree #7



Image 7: bay laurel #8



Image 8: trunk of bay laurel #8 growing up through deck



Image 9: pineapple guava #9



Image 10: pineapple guava #10



Image 11: southern magnolia #11



Respectfully submitted,



Katherine Naegele

Consulting Arborist

Aesculus Arboricultural Consulting, LLC

Master of Forestry, UC Berkeley

ISA Certified Arborist #WE-9658A

ISA Tree Risk Assessment Qualified

American Society of Consulting Arborists, Member

Cell: 650 209-0631



Terms of Assignment

The following terms and conditions apply to all oral and written reports and correspondence pertaining to the consultations, inspections, and activities of Aesculus Arboricultural Consulting:

1. All property lines and ownership of property, trees, and landscape plants and fixtures are assumed to be accurate and reliable as presented and described to the consultant, either orally or in writing. The consultant assumes no responsibility for verification of ownership or locations of property lines, or for results of any actions or recommendations based on inaccurate information.
2. It is assumed that any property referred to in any report or in conjunction with any services performed by Aesculus Arboricultural Consulting is in accordance with any applicable codes, ordinances, statutes, or other governmental regulations, and that any titles and ownership to any property are assumed to be good and marketable. The existence of liens or encumbrances has not been determined, and any and all property is appraised and/or assessed as though free and clear, under responsible ownership and competent management.
3. All reports and other correspondence are confidential and are the property of Aesculus Arboricultural Consulting and its named clients and their assigns or agents. Possession of this report or a copy thereof does not imply any right of publication or use for any purpose, without the express permission of the consultant and the client to whom the report was issued. Loss, removal, or alteration of any part of a report invalidates the entire appraisal/evaluation.
4. The scope of any report or other correspondence is limited to the trees and conditions specifically mentioned in those reports and correspondence. Aesculus Arboricultural Consulting assumes no liability for the failure of trees or parts of trees, inspected or otherwise. The consultant assumes no responsibility to report on the condition of any tree or landscape feature not specifically requested by the named client.
5. All inspections are limited to visual examination of accessible parts, without dissection, excavation, probing, boring or other invasive procedures, unless otherwise noted in the report, and reflect the condition of those items and features at the time of inspection. No warranty or guarantee is made, expressed or implied, that problems or deficiencies of the plants or the property will not occur in the future, from any cause. The consultant shall not be responsible for damages caused by any tree defects, and assumes no responsibility for the correction of defects or tree related problems.
6. The consultant shall not be required to provide further documentation, give testimony, be deposed, or to attend court by reason of this appraisal/report unless subsequent contractual arrangements are made, including payment of additional fees for such services as set forth by the consultant or in the fee schedule or contract.
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8. Any report and the values, observations, and recommendations expressed therein represent the professional opinion of the consultant, and the fee for services is in no manner contingent upon the reporting of a specified value nor upon any particular finding.
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From: pouxliz@aol.com
To: [Khan, Fahteen N](#)
Subject: Re: Permit/Xiaoyan Liu/785 Partridge Avenue
Date: Wednesday, February 1, 2023 7:33:16 AM
Attachments: [CMP_Email_Logo_100dpi_05d92d5b-e8e3-498f-93a6-d0da509bd60211111111.png](#)

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Hi Fateen,

1. The Arborist doesn't seem to mention the 2 Douglas Fir trees on the neighbor's property, besides just listing them. My concern has been and continues to be the potential impact of digging a basement on the root system of these 2 trees. We all know that the drought has stressed trees and atmospheric rivers create situations that cause trees to topple. The arborist report states that root systems are hard to predict and we have 2 enormous trees. If we disturb the root systems and further weaken the trees, I know which way the trees will topple, in the direction of the root disturbance which puts the trees falling on my roof.

My concern is not trees falling during construction. It is likely to happen a few years later when the owner has sold the houses to others and then we all squabble over whose fault the trees' demise and resulting damage is. But for me, it is clear that what is decided now is material and has a direct relation to any eventual problems related to those 2 trees and from what I can tell, the arborist doesn't address the safety of those trees.

2. I will be asking the Committee how to reduce construction vibrations and how the project will protect my foundation since my property is particularly long and I'm concerned about potential damage to my property due to the digging and pounding that will happen as the project lays their foundations.

3. Window/Privacy issues were a problem with the previous plan. I have not yet had time to look at the exact placement of windows but the previous version had the new owners looking directly into our bedrooms or living room. If this is still the case, I protest. Recessing a wall to move the window further back does not change the view when windows are face-to-face. I have lots of wall space between my windows where the new project can allow everyone to maintain privacy.

Please submit these concerns to the Committee. I plan to attend the meeting to hear the response but just in case there is a technical problem with my ability to get to the Zoom meeting, I would like to make sure that my concerns are officially noted.

Thank you.

Sincerely,
Liz

-----Original Message-----

From: Khan, Fahteen N <FNKhan@menlopark.gov>
To: Elizabeth Poux <pouxliz@aol.com>
Cc: Calvin Smith <calvin@studios2arch.com>
Sent: Tue, Jan 31, 2023 2:01 pm
Subject: RE: Permit/Xiaoyan Liu/785 Partridge Avenue

Hi Liz,

I wanted to clarify about the lightwell - during the building permit stage it will be ensured

and verified that no neighboring structures are negatively impacted by its construction. A geotech report is done and submitted to the City for review, additionally a structural engineer will also review the proposal and ensure that the proposed lightwell (amongst other construction related items) are not negatively impacting both the neighbors and the proposed residence.

As for the asbestos concern during demo, when it is caught the applicant will need to notify the Bay Area Air Quality Management and remediation will be done. I am also attaching the arborist report ahead of time from staff report publication. Let us know if you still have concerns over the trees. I believe that has been taken care of through the heritage tree removal process.

Your email seemed more of a request of information which is why I will not be adding this as a correspondence to the staff report for the project. Absent of hearing from you we will not attach this email. If you have comments on the project that you wish be attached to the staff report then you will need to send it to us by tomorrow morning, no later than 10 am. If it come after the deadline, we will send it to the Planning Commission via email, not as an attachment to the staff report. Or you are more than welcome to attend the meeting and voice out concerns.

Let me know,
Fahteen



Fahteen N. Khan
Associate Planner
City Hall - 1st Floor
701 Laurel St
tel 650-330-6739
menlopark.gov

*Note our emails have changed to @menlopark.gov

From: Khan, Fahteen N
Sent: Monday, January 30, 2023 5:35 PM
To: 'Elizabeth Poux' <pouxliz@aol.com>
Cc: 'Calvin Smith' <calvin@studios2arch.com>
Subject: RE: Permit/Xiaoyan Liu/785 Partridge Avenue

Hi Liz,

In order to attend via zoom you will need to go to the City's website. Home>Agenda>Planning Commission>Select the Agenda for the hearing, it will have information on accessing zoom. We are doing hybrid, so you can attend in-person as well, if you want to. However, it is the last item on the agenda so I don't know when we will be discussing it at the meeting.

Your concerns on the lightwell and how it you will be protected, is something that the applicant can talk to you about. I believe they reached out to you and other neighbors. However, you are welcome to bring it up at the meeting and the applicant can answer to that, and Planning Commission can weigh in on it.

As for the arborist report it will be published with the staff report on Thursday. They did redesign the houses. Please see attached plans excerpt and let me and the applicant know if you have any concerns. The second floor is set back further than the first floor, which should help with privacy.

-Fahteen

From: Elizabeth Poux [<mailto:pouxliz@aol.com>]
Sent: Saturday, January 28, 2023 7:42 AM
To: Khan, Fahteen N <FNKhan@menlopark.gov>
Subject: Re: Permit/Xiaoyan Liu/785 Partridge Avenue

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Hi Fahteen,

Thanks for the email. Yes, I received a notice in the mail but it didn't say how to find the zoom address. Can you please tell me how? I and my husband are planning on attending this meeting.

Also, how do I find out whether my concerns about protecting the ground so that my foundation is not affected by digging and construction vibrations has been addressed. The massive hole will be just 10 feet from my foundation and there will be lots of vibrations to disturb the ground where I am. Are there special precautions that they can make?

I have also written about protecting the large trees on the neighbor's property on the other side so that it won't fall over (eventually). For these reasons, I am not so keen on a basement. I understand that an arborist was supposed to examine the trees. How can I read the report?

I'm also wondering if my window placement concerns have been addressed. They had a window looking directly into my daughter's bedroom, a huge staircase window looking onto my property and another one looking into my living room. How can I know that we can maintain our privacy?

Do I need to make some sort of formal request to state my concerns? If so, how?

Thanks in advance for your help with my questions.

Liz

Sent from my iPhone